

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
144	(44X-R&530B)RS	LAKE	50	1
		ILLINOIS	CONTRACT NO. 60V06	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE VILLAGES OF BUFFALO GROVE, LONG GROVE AND VERNON HILLS

TRAFFIC DATA

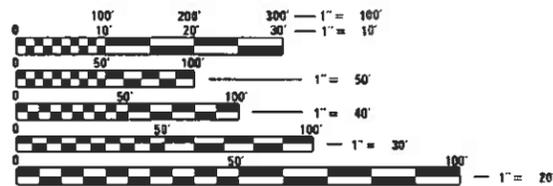
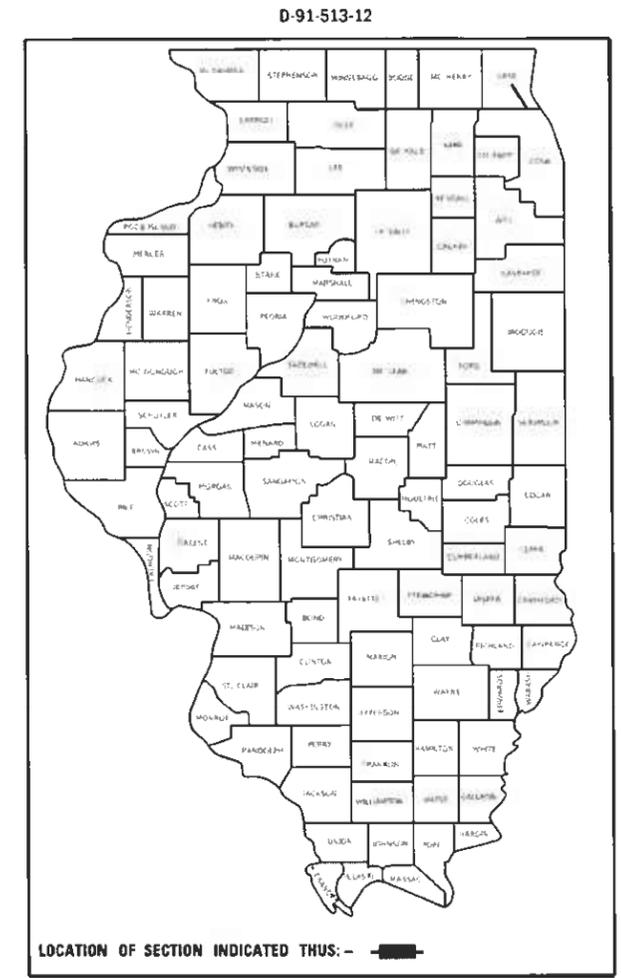
2019 ADT = 47900

POSTED SPEED LIMIT = 35-45 MPH

PROPOSED
HIGHWAY PLANS

FAP ROUTE 344: IL 83 (MCHENRY RD)
US 45 TO BUFFALO GROVE RD
SECTION (44X-R&530B)RS
PROJECT: NHPP-GM3G(849)
PRECAST CONCRETE PANELS AND CLASS B PATCHING
LAKE COUNTY

C-91-513-12

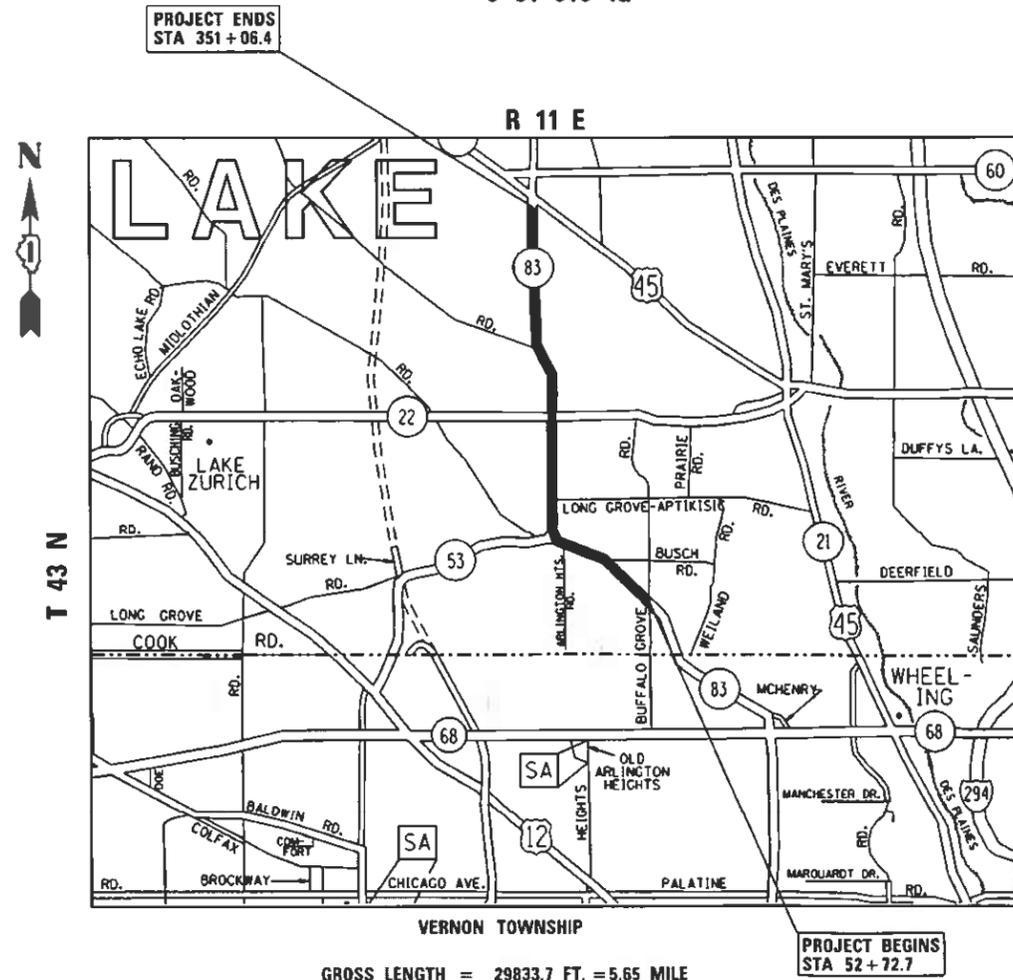


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 60V06



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED June 23 2020
Anthony J. Wingle REGIONAL ENGINEER
August 14, 2020
Scott A. Elk
ENGINEER OF DESIGN AND ENVIRONMENT
August 14, 2020
James J. ...
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
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5	TYPICAL SECTIONS
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17-25	DETECTOR LOOPS REPLACEMENT PLANS
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45	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
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STATE STANDARDS

<u>STANDARD NO</u>	<u>DESCRIPTION</u>
420001-09	PAVEMENT JOINTS
420101-06	24' (7.2 m) JOINTED PCC PAVEMENT
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS >45 MPH TO 55 MPH
701422-10	LANE CLOSURE, MULTILANE, FOR SPEEDS >45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS > 45 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W or 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF BUFFALO GROVE, LONG GROVE AND VERNON HILLS.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND REISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

THE RESIDENT ENGINEER SHALL CONTACT AREA TRAFFIC ENGINEER WALTER CZARNY VIA EMAIL AT walter.czarny@illinois.gov A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT: Kalpana.Kannan-Hosadurga@illinois.gov A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. walter.czarny@illinois.gov

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF ONSTRUCTION.

ALL TEMPORARY PAVEMENT MARKINGS SHALL BE TYPE III TAPE

FILE NAME :	USER NAME : gorengetab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	REV-SEP
pw\planroom.dot.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI51312\Drawings\Design\DI51312-shr-ipln.dgn		CHECKED -	REVISED -			344	(44X-R&530B)RS	LAKE	50	2	
Default	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED -			CONTRACT NO. 60V06			ILLINOIS FED. AID PROJECT		
	PLOT DATE = 6/22/2020					SCALE:	SHEET	OF	SHEETS	STA.	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 0006				
CODE NO	ITEM	UNIT		0006 80% FED				
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	3240	3240				
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	62360	62360				
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	11100	11100				
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	1300	1300				
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	700	700				
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	1600	1600				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	100	100				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	100	100				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4471	4471				
X0326767	PROFILE DIAMOND GRINDING CONCRETE PAVEMENT	SO YD	2120	2120				
X0327772	PRECAST CONCRETE PAVEMENT SLABS 10"	SO FT	13608	13608				
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	31423	31423				
* X2700003	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	15000	15000				
X4423015	DOWEL BARS 1 1/2" RETROFIT	EACH	1890	1890				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	20792	20792				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE 0006				
CODE NO	ITEM	UNIT		0006 80% FED				
* X7800956	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID	FOOT	15000	15000				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3155	3155				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	100	100				
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	15	15				
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	360	360				
Ø Z0076600	TRAINEES	HOUR	500	500				
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500				

FILE NAME :	USER NAME : gongolao	DESIGNED -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 FROM US RTE 45 TO BUFFALO GROVE RD
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R&S30B)RS	LAKE	50	4
CONTRACT NO. 60V06			REV-SEP	

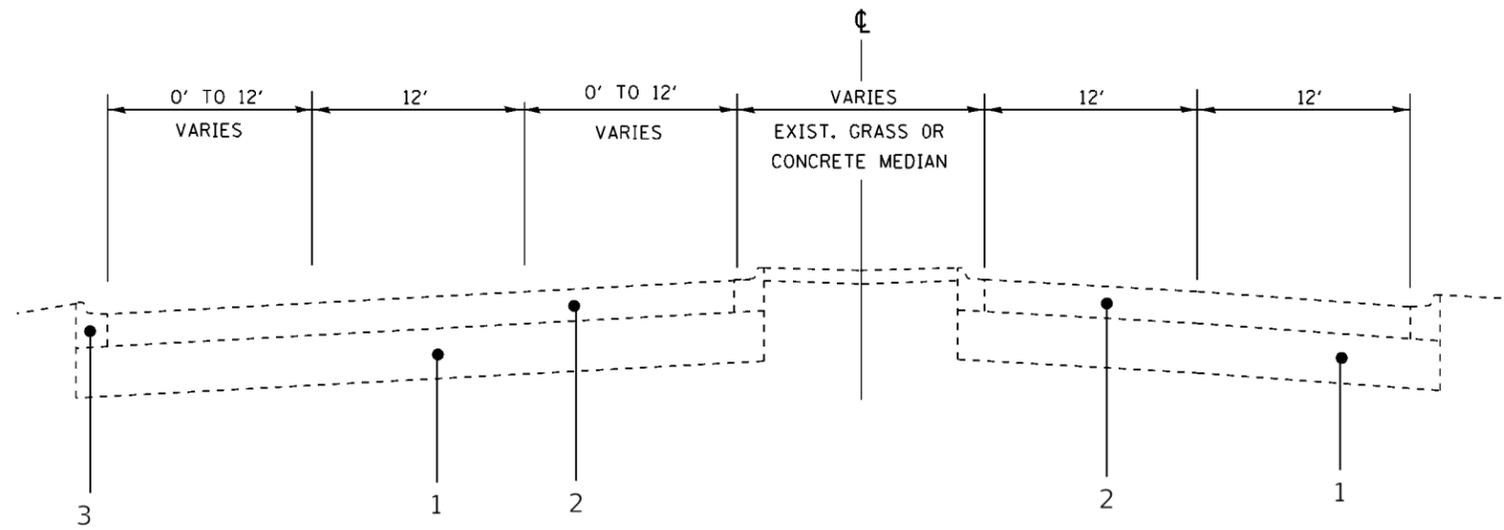
* SPECIALTY ITEM

Ø 0042

REV-SEP

SCALE: SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



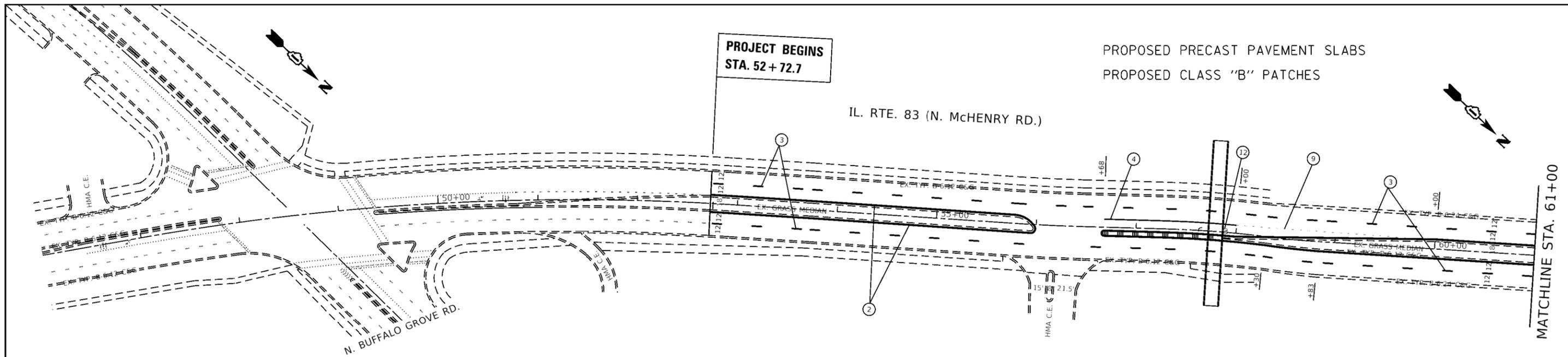
LEGEND

- 1 EXISTING AGGREGATE SUBGRADE, 12"
- 2 EXISTING P.C.C. PAVEMENT, ± 9 1/4"
- 3 EXISTING CURB AND GUTTER

EXISTING TYPICAL SECTION
 IL 83 (MCHENRY RD)
 STA 52+72.7 TO STA 351+06.4

PRECAST CONCRETE PAVEMENT SLAB SCHEDULE

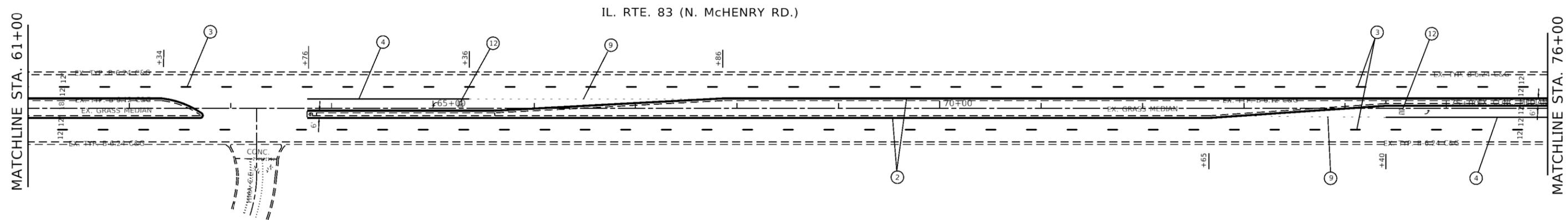
IL 83 (MCHENRY RD)						
	WIDTH FT	LENGTH FT	NB		SB	
			LANE NO 1	LANE NO 2	LANE NO 1	LANE NO 2
BUFFALO GROVE RD	12	6	6	10	7	9
(TO IL 53)	12	9	1	1	1	1
	12	15		1		
IL 53 TO IL 22	12	6	7	4	13	9
	12	9	1	1	2	2
	12	15			1	
GILMER RD	12	6	5	3	8	10
(TO IL 25)	12	9	1	1	3	4
	12	15		1		1
GILMER RD	12	6	8	2	16	18
	12	9	1		3	3
	12	15			1	1



LEGEND

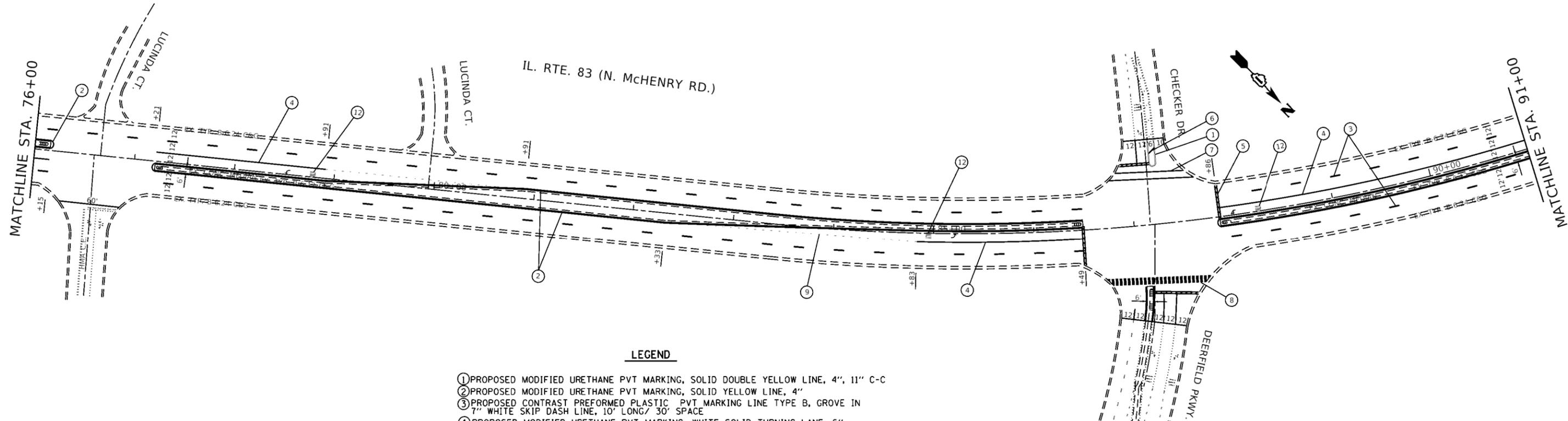
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PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



FILE NAME =	USER NAME = gorenautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD) ROADWAY PLAN.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\planroom.dot,illinois.gov\IDOT\Documents\IDOT Offices\District 1\Projects\0151312\Drawings\Design\0151312-shr-plan.dgn		CHECKED -	REVISED -			344	(44X-R & 530B)RS	LAKE	50	6	
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -			CONTRACT NO. 60V06					
Default	PLOT DATE = 6/23/2020					SCALE: 1"=50'	SHEET	OF	SHEETS	STA. 52+72.7	TO STA. 76+00

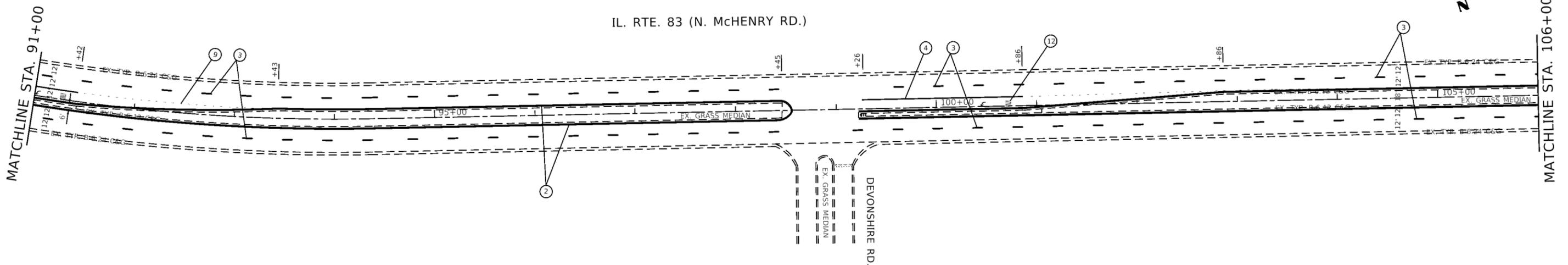
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PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



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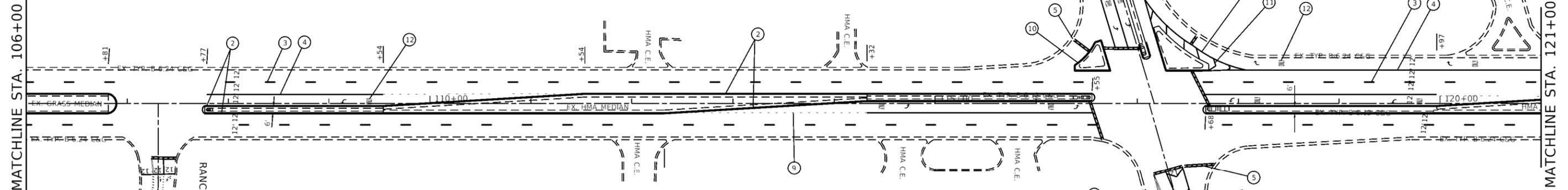
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)			
ROADWAY PLAN			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. 76+00 TO STA. 106+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R & 530B)RS	LAKE	50	7
CONTRACT NO. 60V06				
ILLINOIS FED. AID PROJECT				

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

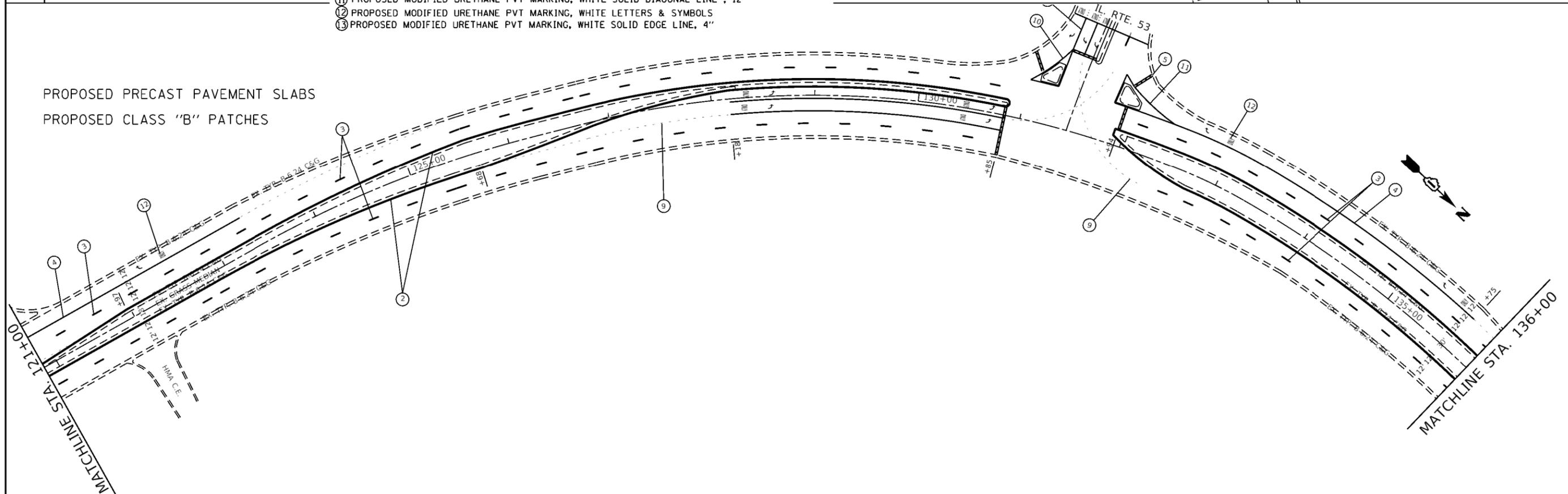
IL. RTE. 83 (N. MCHENRY RD.)



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PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



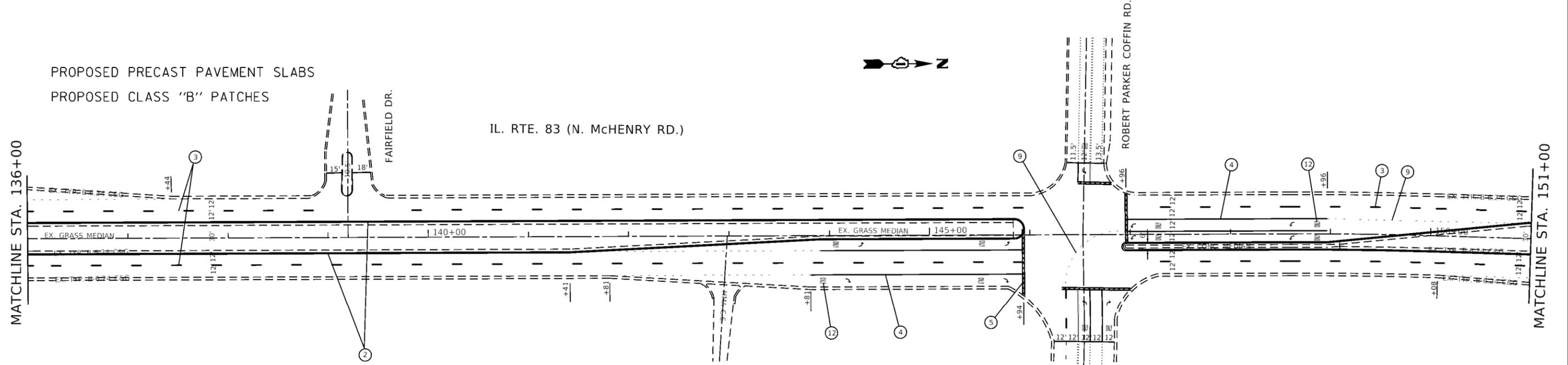
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STATE OF ILLINOIS
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IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN.

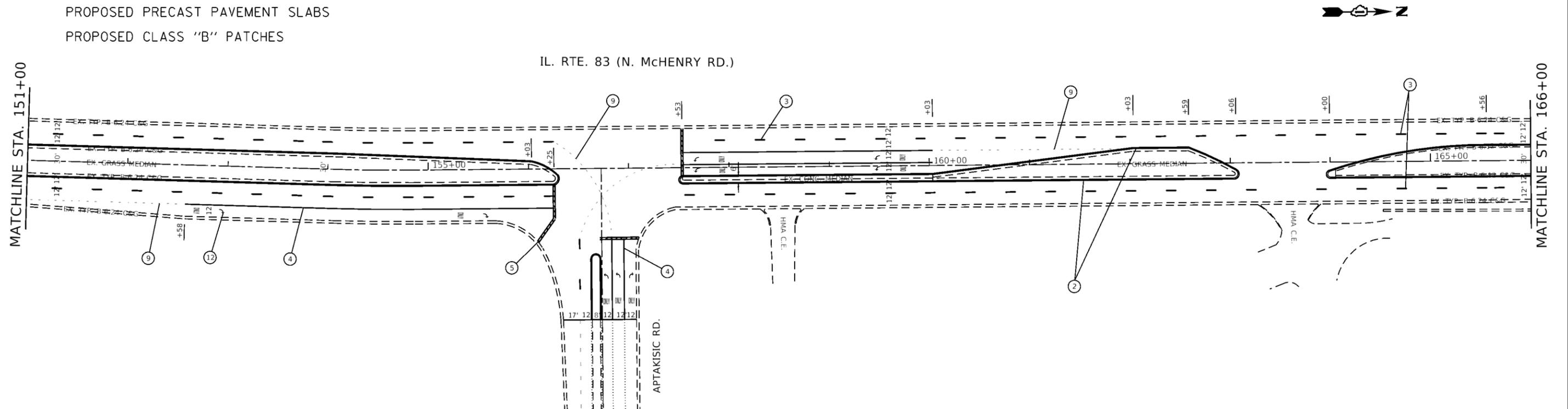
SCALE: 1"=50' SHEET OF SHEETS STA. 106+00 TO STA. 136+00

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344	(44X-R & 530B/RS)	LAKE	50	8
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ILLINOIS FED. AID PROJECT				



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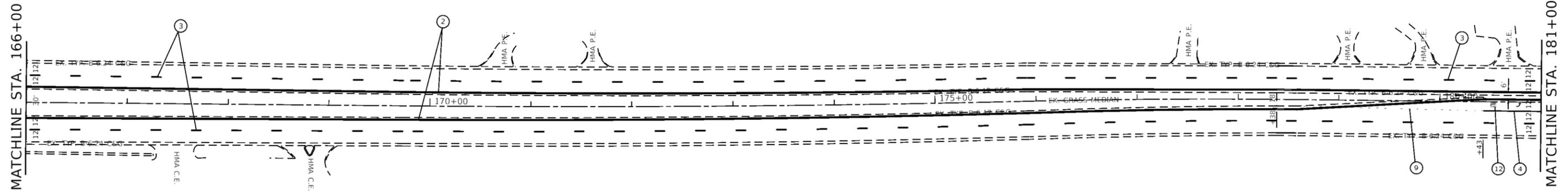


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PLOT SCALE = 100.0000' / 1in.	CHECKED :	REVISED :	SCALE: 1"=50'			SHEET	OF	SHEETS	STA. 136+00	TO STA. 166+00	CONTRACT NO. 60V06
Default	PLOT DATE = 6/23/2020	DATE :									ILLINOIS FED. AID PROJECT

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



IL. RTE. 83 (N. McHENRY RD.)



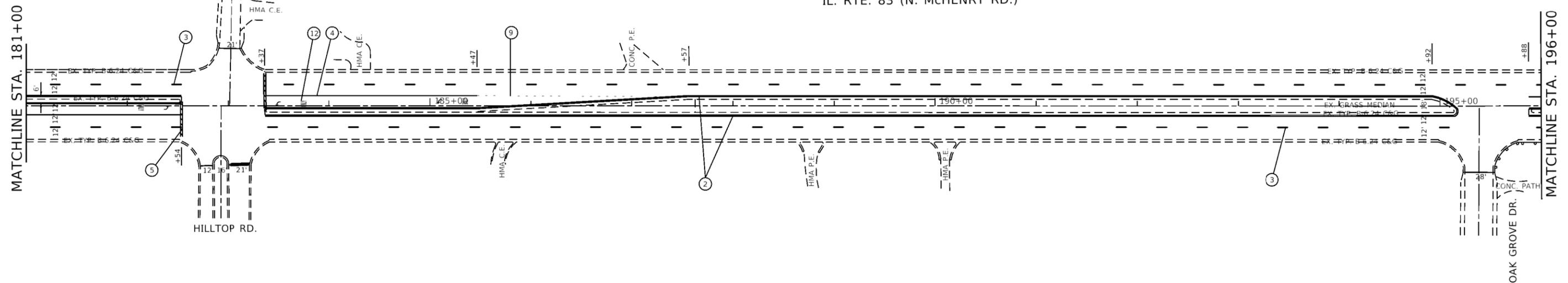
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 PROPOSED CLASS "B" PATCHES



IL. RTE. 83 (N. McHENRY RD.)

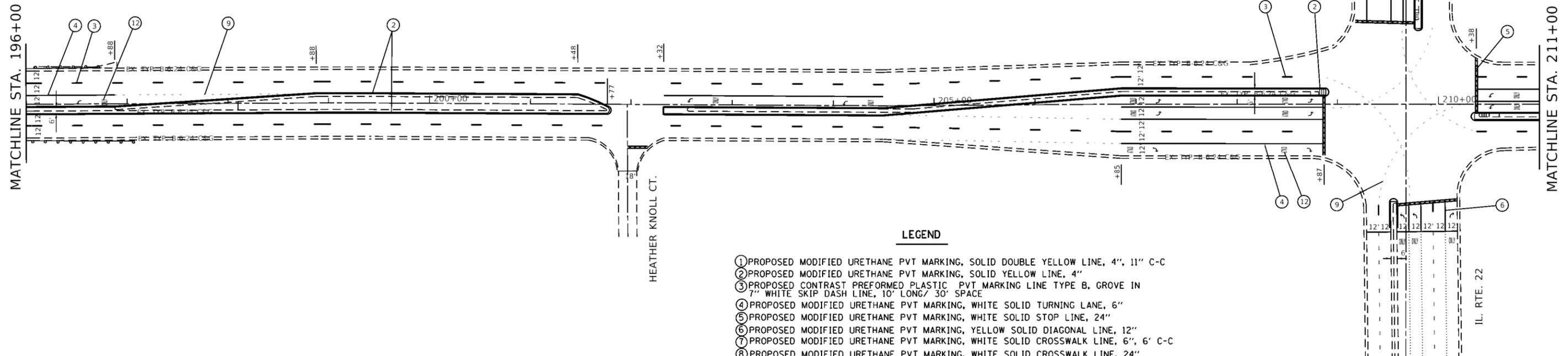


FILE NAME =	USER NAME = gorenautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD) ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pwi\planroom.dot,illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI51312\DRAWING\Design\DI51312-shr-plan.dgn		CHECKED -	REVISED -			344	(44X-R & 530BRS)	LAKE	50	10	
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -			CONTRACT NO. 60V06					
Default	PLOT DATE = 6/23/2020					ILLINOIS FED. AID PROJECT					

SCALE: 1"=50' SHEET OF SHEETS STA. 166+00 TO STA. 196+00

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. MCHENRY RD.)

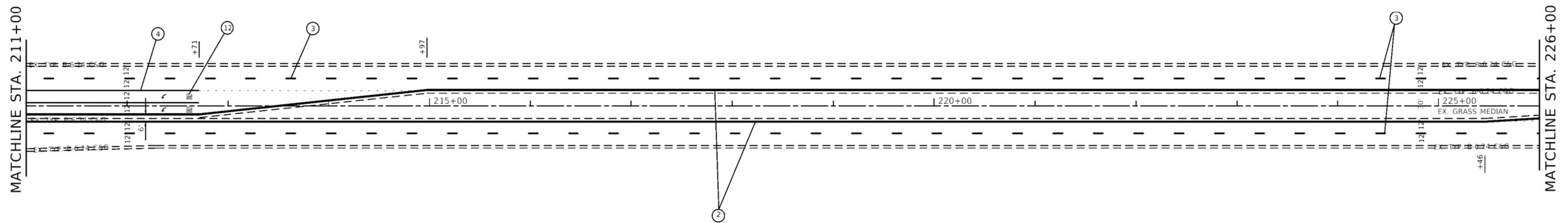


LEGEND

- ① PROPOSED MODIFIED URETHANE PVT MARKING, SOLID DOUBLE YELLOW LINE, 4", 11" C-C
- ② PROPOSED MODIFIED URETHANE PVT MARKING, SOLID YELLOW LINE, 4"
- ③ PROPOSED CONTRAST PREFORMED PLASTIC PVT MARKING LINE TYPE B, GROVE IN 7" WHITE SKIP DASH LINE, 10" LONG/ 30" SPACE
- ④ PROPOSED MODIFIED URETHANE PVT MARKING, WHITE SOLID TURNING LANE, 6"
- ⑤ PROPOSED MODIFIED URETHANE PVT MARKING, WHITE SOLID STOP LINE, 24"
- ⑥ PROPOSED MODIFIED URETHANE PVT MARKING, YELLOW SOLID DIAGONAL LINE, 12"
- ⑦ PROPOSED MODIFIED URETHANE PVT MARKING, WHITE SOLID CROSSWALK LINE, 6", 6" C-C
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- ⑬ PROPOSED MODIFIED URETHANE PVT MARKING, WHITE SOLID EDGE LINE, 4"

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. MCHENRY RD.)



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	PLOT DATE = 6/23/2020	DATE -	REVISED -

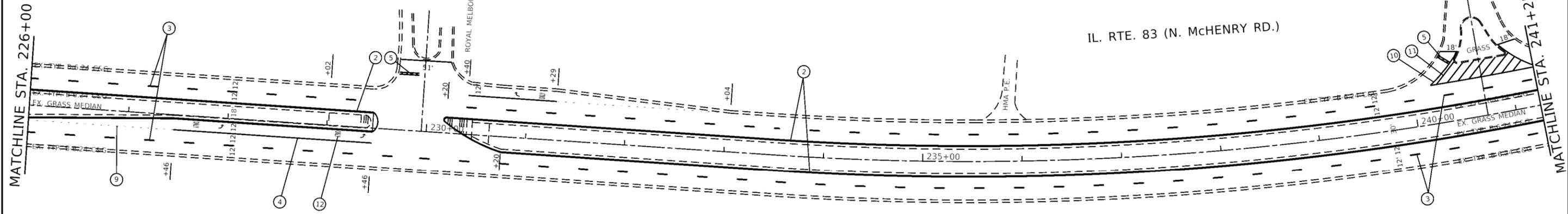
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. 196+00 TO STA. 226+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44R-R & 530B)RS	LAKE	50	11
CONTRACT NO. 60V06			ILLINOIS FED. AID PROJECT	

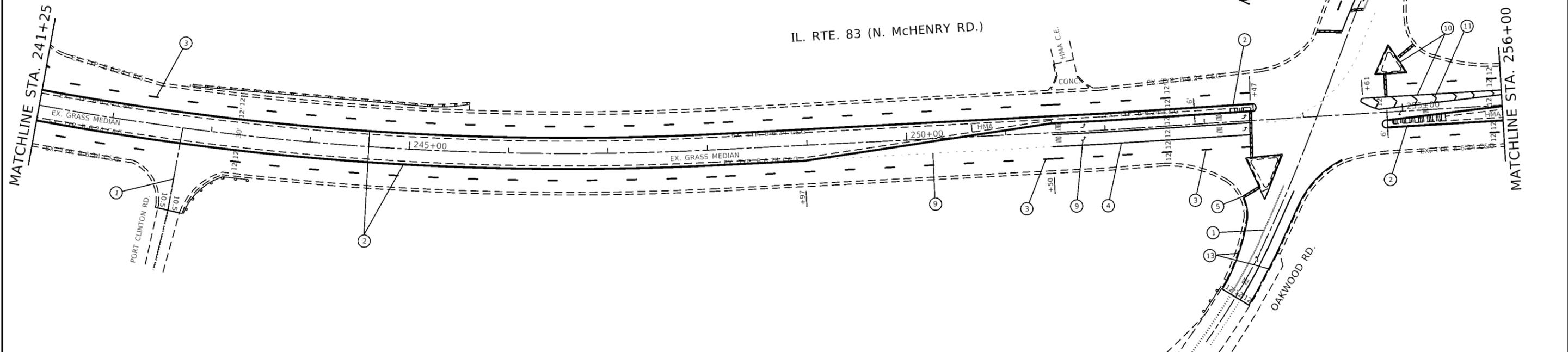
PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



LEGEND

- ① PROPOSED MODIFIED URETHANE PVT MARKING, SOLID DOUBLE YELLOW LINE, 4", 11" C-C
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- ③ PROPOSED CONTRAST PREFORMED PLASTIC PVT MARKING LINE TYPE B, GROVE IN 7" WHITE SKIP DASH LINE, 10' LONG/ 30' SPACE
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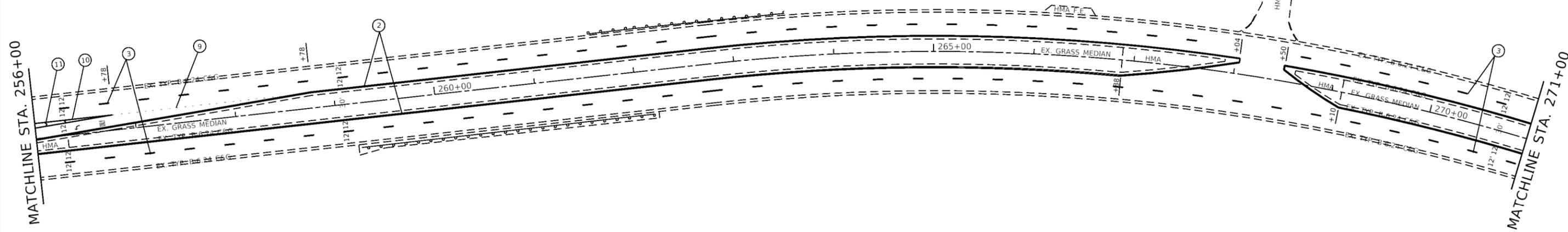
PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES



FILE NAME =	USER NAME = gorenautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD) ROADWAY PLAN	F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pwi\planroom.dot.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI51312\Design\DI51312-shr-plan.dgn						344	(44X-R & 530B)RS	LAKE	50	12	
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 60V06					
	PLOT DATE = 6/23/2020	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=50'	SHEET	OF	SHEETS	STA. 236+00	TO STA. 256+25	

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. McHENRY RD.)

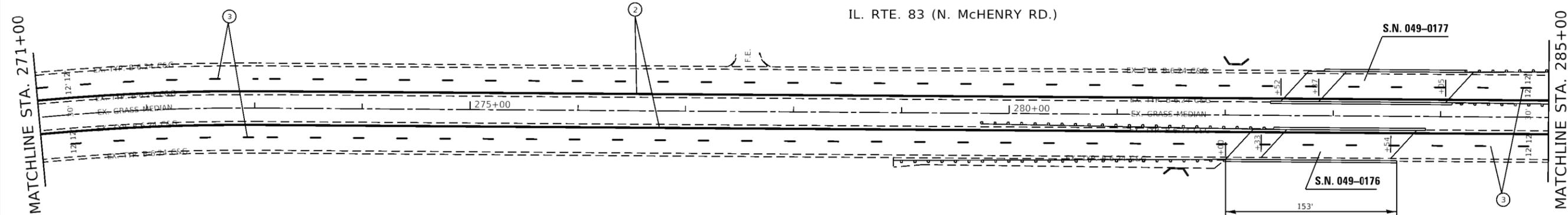


LEGEND

- ① PROPOSED MODIFIED URETHANE PVT MARKING, SOLID DOUBLE YELLOW LINE, 4", 11" C-C
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PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. McHENRY RD.)



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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 6/23/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

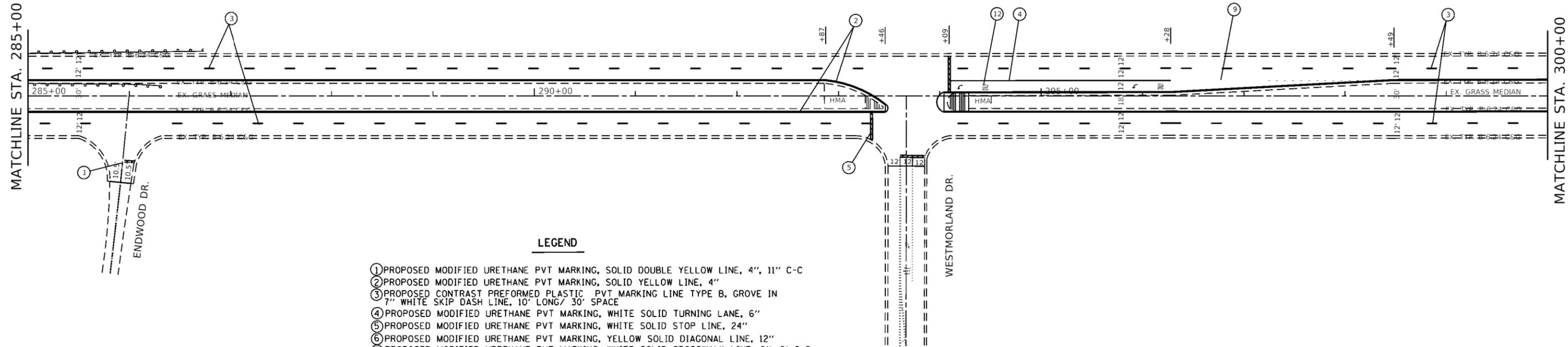
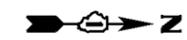
**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN**

SCALE: SHEET OF SHEETS STA. 256+25 TO STA. 285+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R & 530B)RS	LAKE	50	13
CONTRACT NO. 60V06				
ILLINOIS FED. AID PROJECT				

PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. MCHENRY RD.)

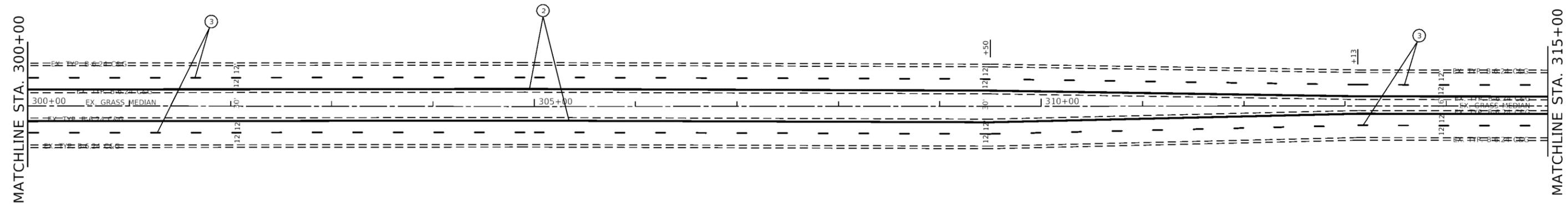
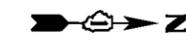


LEGEND

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PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

IL. RTE. 83 (N. MCHENRY RD.)



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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 6/23/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. 285+00 TO STA. 315+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R & 530B)RS	LAKE	50	14
CONTRACT NO. 60V06			ILLINOIS FED. AID PROJECT	

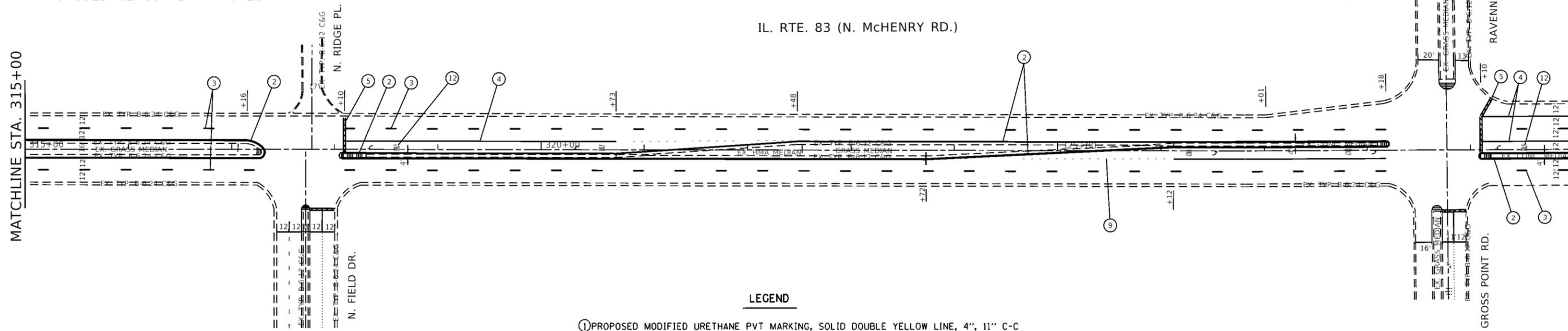
PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

MATCHLINE STA. 315+00

IL. RTE. 83 (N. McHENRY RD.)



MATCHLINE STA. 330+00



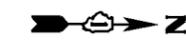
LEGEND

- ① PROPOSED MODIFIED URETHANE PVT MARKING, SOLID DOUBLE YELLOW LINE, 4", 11" C-C
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- ⑬ PROPOSED MODIFIED URETHANE PVT MARKING, WHITE SOLID EDGE LINE, 4"

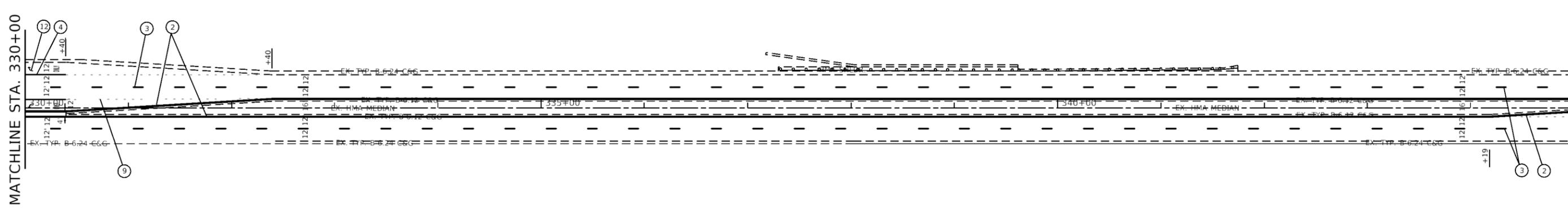
PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

MATCHLINE STA. 330+00

IL. RTE. 83 (N. McHENRY RD.)



MATCHLINE STA. 345+00



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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 6/23/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. 315+00 TO STA. 345+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R & 530B)RS	LAKE	50	15
CONTRACT NO. 60V06				
ILLINOIS FED. AID PROJECT				

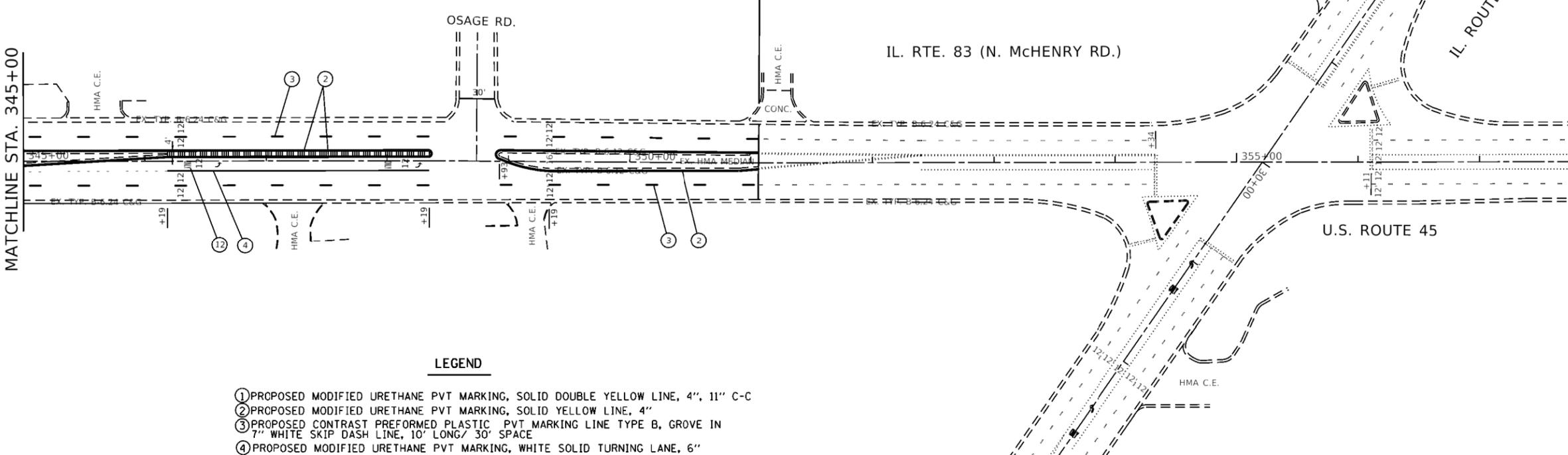
PROPOSED PRECAST PAVEMENT SLABS
 PROPOSED CLASS "B" PATCHES

PROJECT ENDS
 STA. 351+06.4

MATCHLINE STA. 345+00



IL. RTE. 83 (N. MCHENRY RD.)



LEGEND

- ① PROPOSED MODIFIED URETHANE PVT MARKING, SOLID DOUBLE YELLOW LINE, 4", 11" C-C
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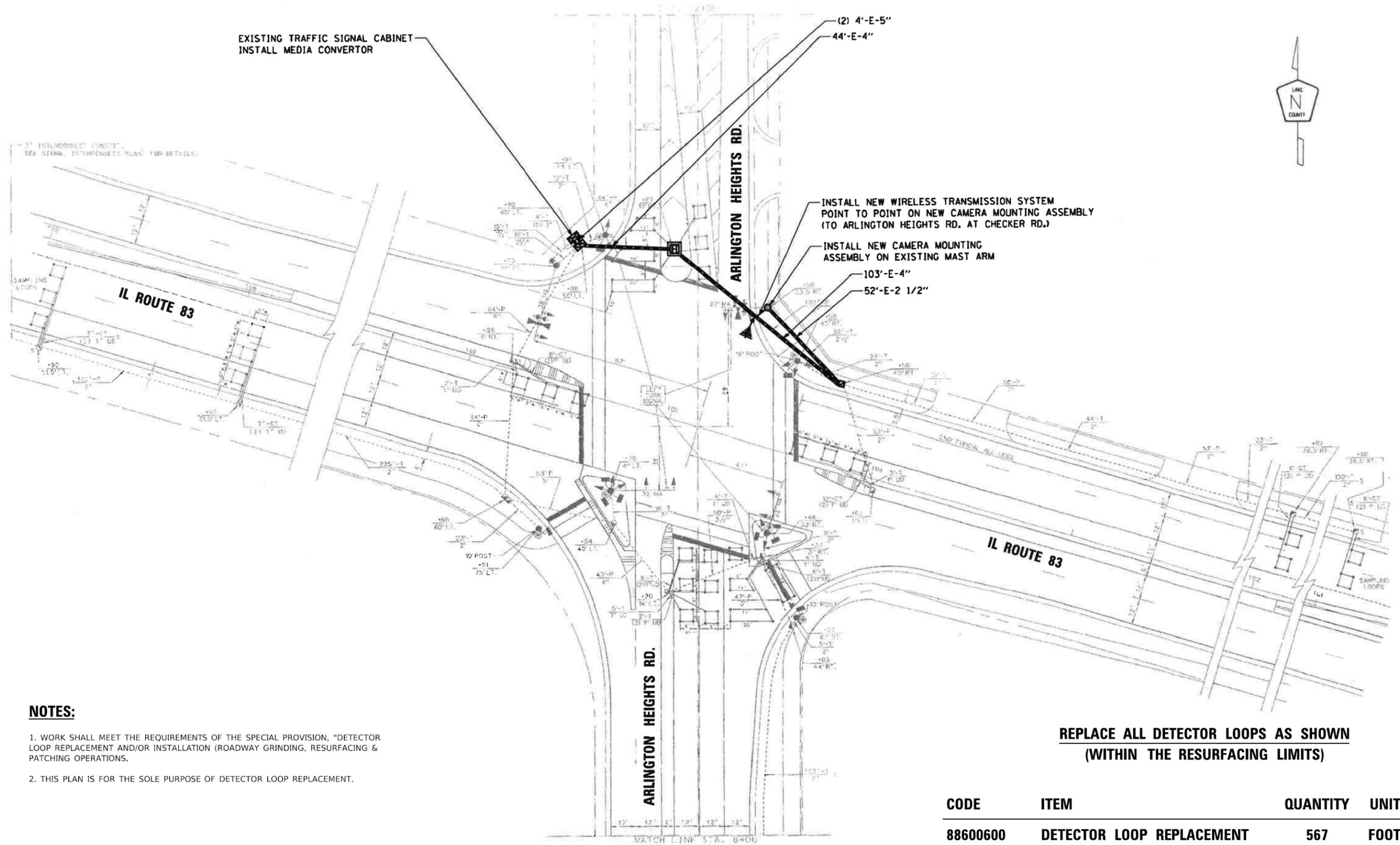
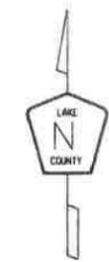
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	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
Default	PLOT DATE = 6/23/2020	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
 ROADWAY PLAN

SCALE: 1"=50' SHEET OF SHEETS STA. 345+00 TO STA. 351+06.4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	144X-R & 530BIRS	LAKE	50	16
CONTRACT NO. 60V06			ILLINOIS FED. AID PROJECT	



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	567	FOOT

MODEL: Default
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USER NAME = gorenautab	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/23/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
DETECTOR LOOP REPLACEMENT, IL ROUTE 83 AT ARLINGTON HIGHTS RD**

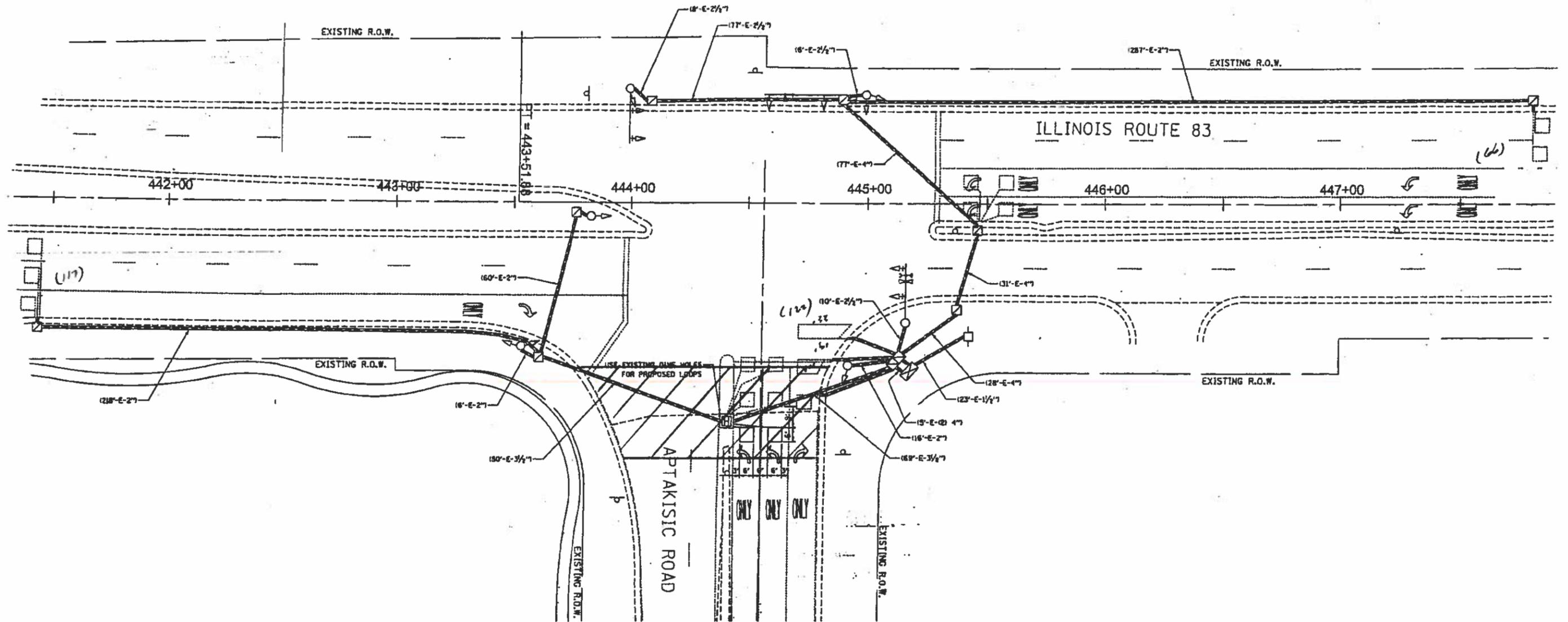
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION (44X-R&530B)RS	COUNTY LAKE	TOTAL SHEETS 50	SHEET NO. 18
			CONTRACT NO. 60V06	
ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



*** NOTE:**
The existing detector loop locations, sizes and quantities shown on the plans are to be used by the Contractor to help estimate and reinstall any existing detector loop as needed if inadvertently milled or saw cut while installing butt joints.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	*305	FOOT	DETECTOR LOOP, REPLACEMENT

**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

USER NAME = gorenautab	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/23/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
DETECTOR LOOP REPLACEMENT. IL ROUTE 83 AT APTAKISIC RD**

SCALE: SHEET OF SHEETS STA. TO STA.

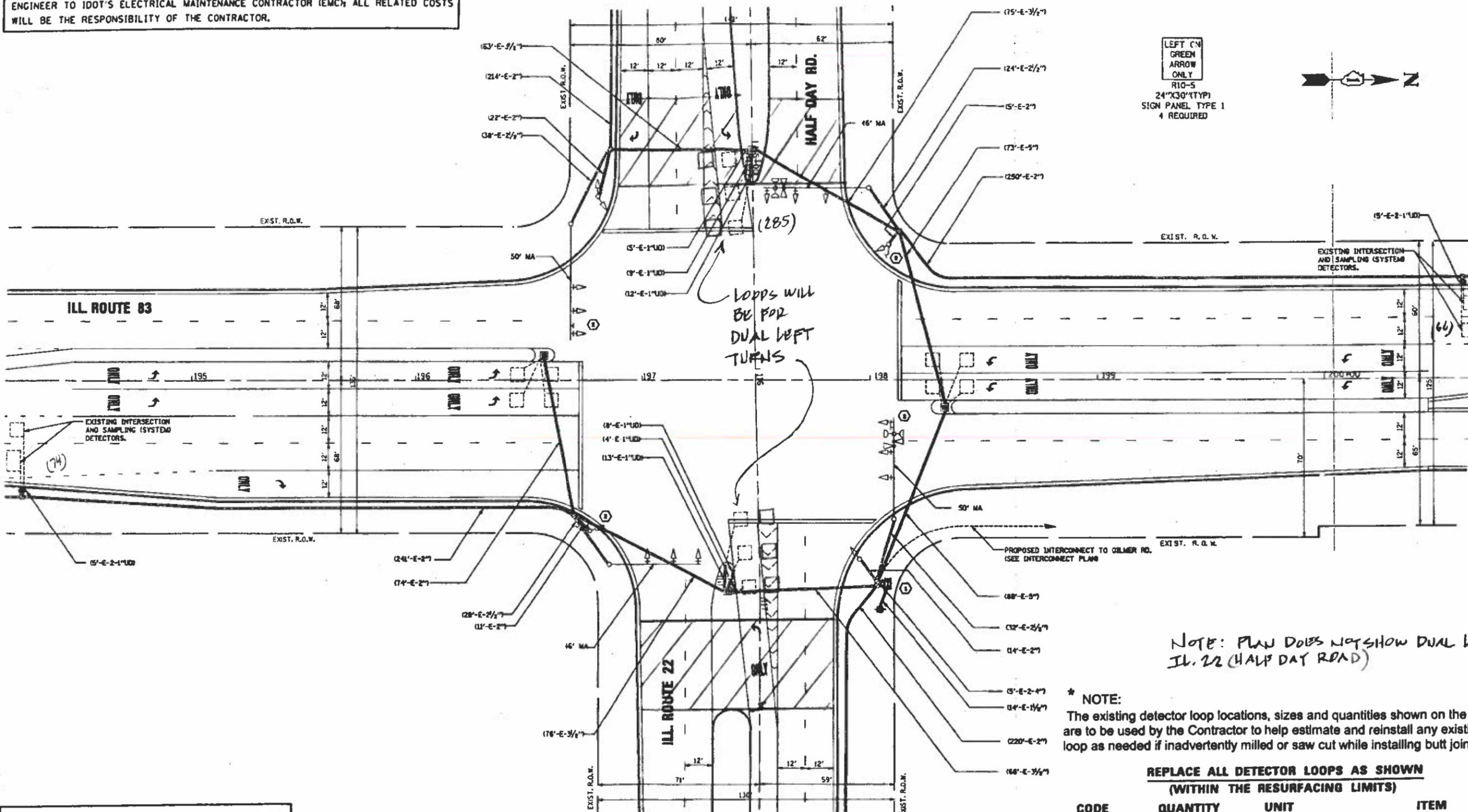
F.A.P. RTE. 344	SECTION 144X-R&S30BIRS	COUNTY LAKE	TOTAL SHEETS 50	SHEET NO. 21
			CONTRACT NO. 60V06	
ILLINOIS FED. AID PROJECT				

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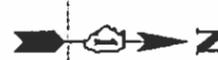
WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



LEFT CN GREEN ARROW ONLY
R10-5
24"X30" (TYP)
SIGN PANEL TYPE 1
4 REQUIRED



NOTE: PLAN DOES NOT SHOW DUAL LT ON IL. 22 (HALF DAY ROAD)

* NOTE:
The existing detector loop locations, sizes and quantities shown on the plans are to be used by the Contractor to help estimate and reinstall any existing detector loop as needed if inadvertently milled or saw cut while installing butt joints.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	*425	FOOT	DETECTOR LOOP, REPLACEMENT

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

MODEL: Default
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DATE: 6/23/2020

USER NAME = gorenautab	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/23/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
DETECTOR LOOP REPLACEMENT. IL ROUTE 83 AT ILL ROUTE 22

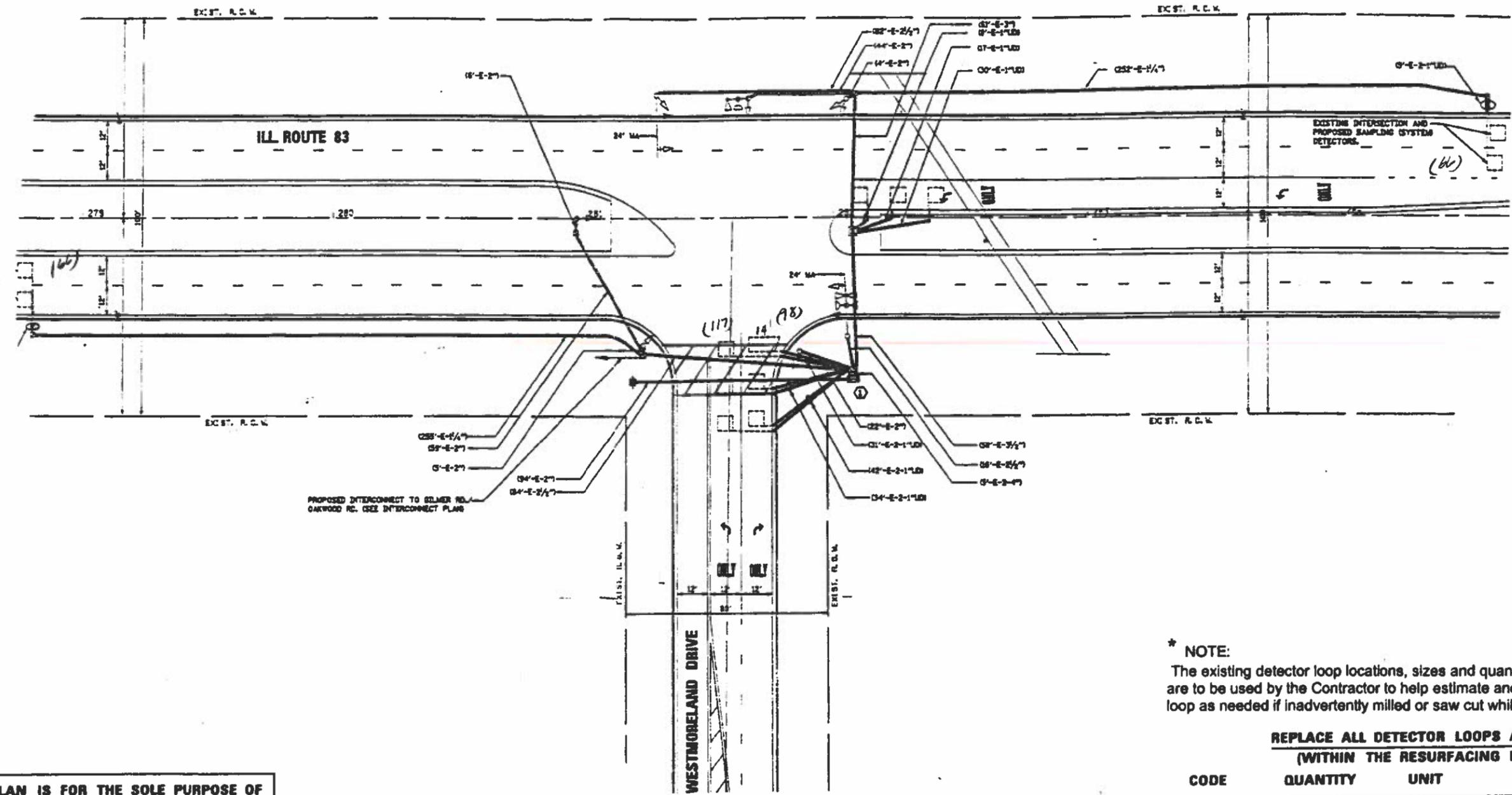
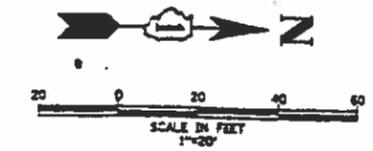
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION 144X-R&530BRS	COUNTY LAKE	TOTAL SHEETS 50	SHEET NO. 23
CONTRACT NO. 60V06				
ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	➔	➔
SIGNAL HEAD	➔	➔
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	⊞	⊞
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	⊞	⊞



PROPOSED INTERCONNECT TO SILVER RD. OAKWOOD RD. (SEE INTERCONNECT PLAN)

* NOTE:
The existing detector loop locations, sizes and quantities shown on the plans are to be used by the Contractor to help estimate and reinstall any existing detector loop as needed if inadvertently milled or saw cut while installing butt joints.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	*347	FOOT	DETECTOR LOOP, REPLACEMENT

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

USER NAME = gorenautab	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000" / in.	CHECKED -	REVISED -
PLOT DATE = 6/23/2020	DATE -	REVISED -

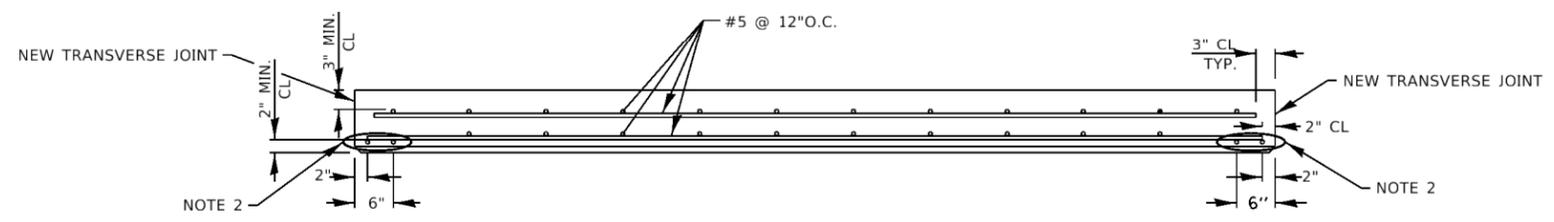
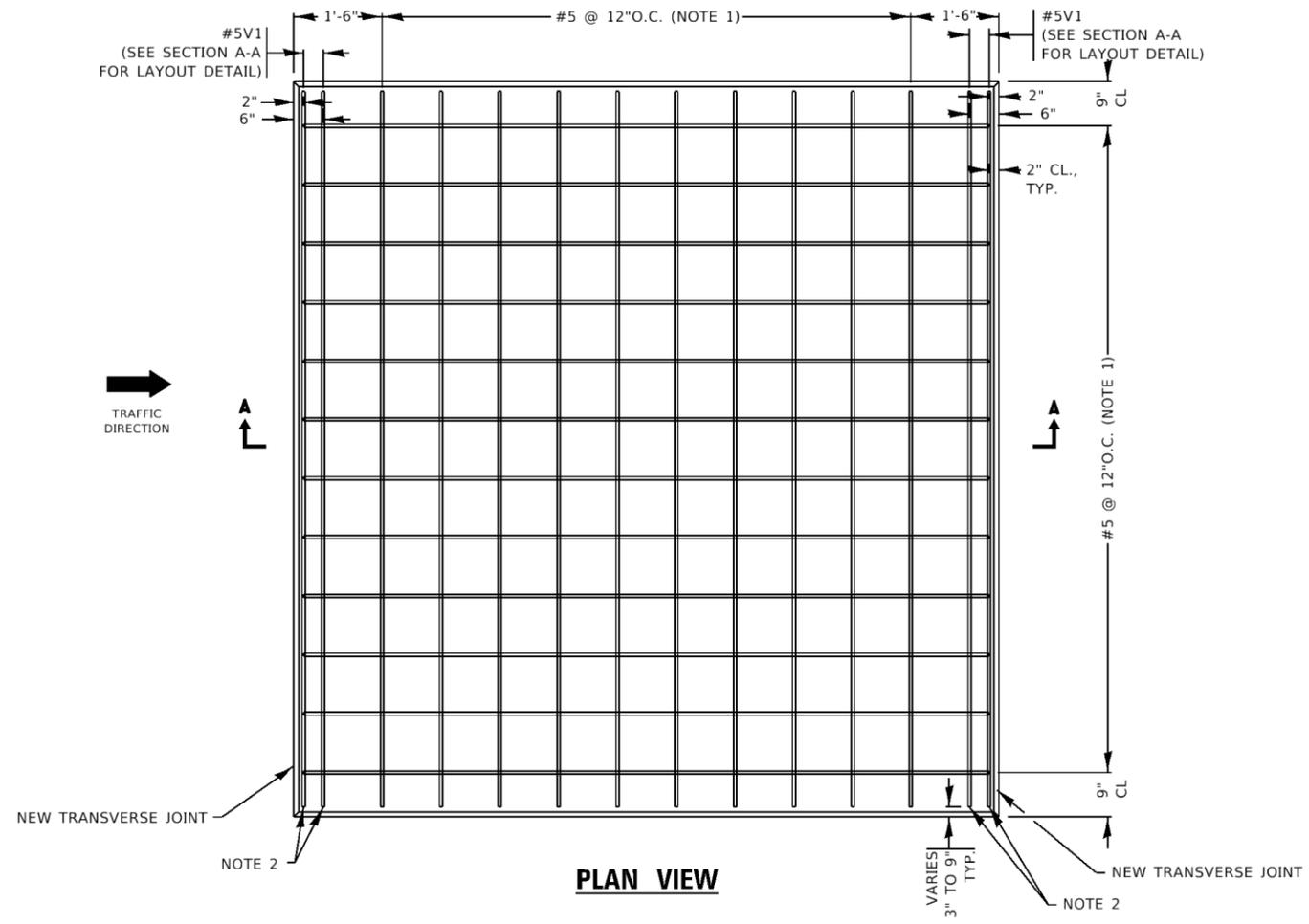
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (MCHENRY RD) (US 45 TO BUFFALO GROVE RD)
DETECTOR LOOP REPLACEMENT. IL ROUTE 83 AT WESTMORELAND DR**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	144X-R&530BIRS	LAKE	50	25

CONTRACT NO. 60V06

MODEL: Default
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 6/23/2020 10:00:00 AM



REINFORCEMENT SECTION A-A
 ALL BARS ARE TRIM TO FIT #5 BAR
 SAW CUTS OFF LONGITUDINAL EDGES SHALL BE NO MORE THAN 6" OFF THE EDGES

- NOTES:**
1. FOR ALL CUSTOM SLABS OF TRAPEZOID SHAPES, THIS REINFORCEMENT SHALL BE LAID OUT IN A PERPENDICULAR GRID PATTERN, NOT SKEWED.
 2. THIS REINFORCEMENT SHALL BE PARALLEL TO THE NEW TRANSVERSE JOINT.

CUSTOM SLAB TYPICAL REINFORCEMENT DETAIL

MODEL - Default
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 PROJECT - 60V06 - ILLINOIS STATE ROADWAY IMPROVEMENT DISTRICT 1 PROJECT
 DATE - 6/19/2020

USER NAME = gorengautab	DESIGNED - O. PATEL	REVISED - D.G. 6-14
	DRAWN -	REVISED - D.G. 9-16
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/19/2020	DATE - 10-25-2013	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

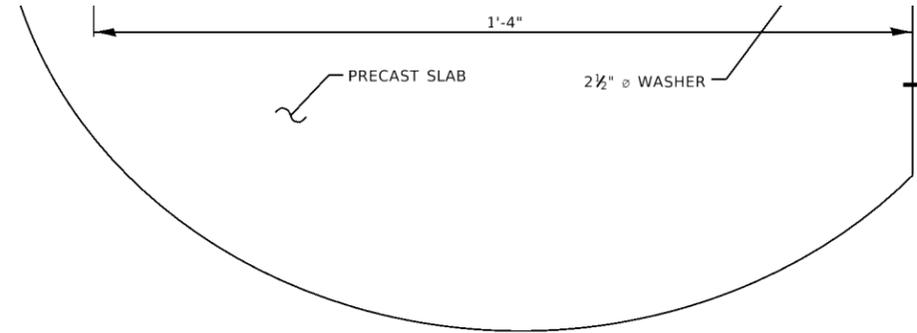
PRECAST CONCRETE PAVEMENT SLABS			
SCALE: NONE	SHEET 3	OF 19 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)R5	LAKE	50	28
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				

**ISOLATED PLACEMENT WITH NARROW MOUTH
PREDRILLED HOLES IN ADJACENT PAVEMENT.**

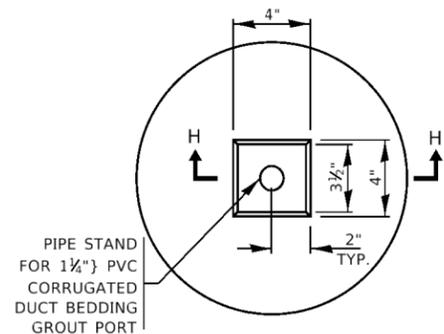
PAVEMENT SLABS		F.A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		344	(44X-R&530B)RS	LAKE	50	31
		BD 57		CONTRACT NO. 60V06		
ETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

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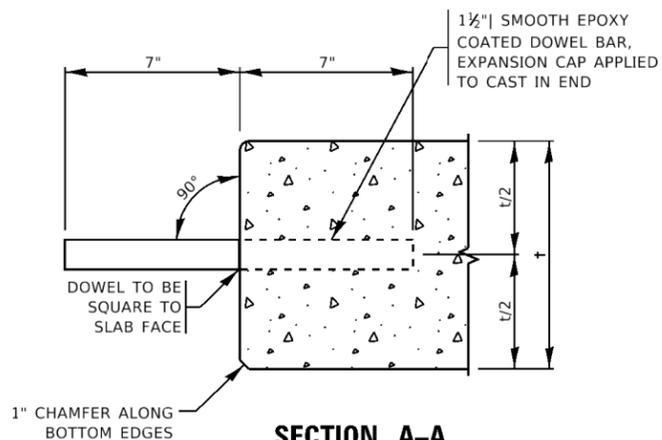


DETAIL B
TRANSVERSE NARROW MOUTH SLOT
DETAIL FOR ISOLATED SLABS

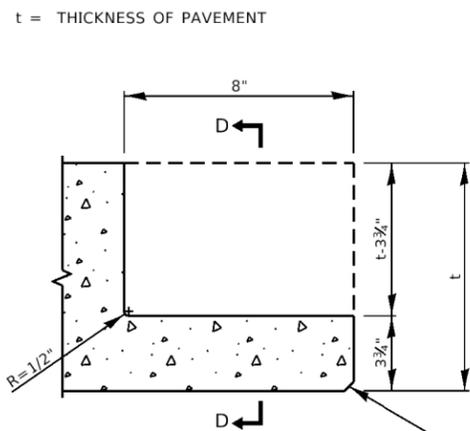
USER NAME = gorengautab	DESIGNED - O. PATEL	REVISED - D.G. 6-14	D
DRAWN -	CHECKED -	REVISED - D.G. 9-16	
PLOT SCALE = 100.0204' / in.	DATE - 10-25-2013	REVISED -	
PLOT DATE = 6/19/2020	REVISED -		



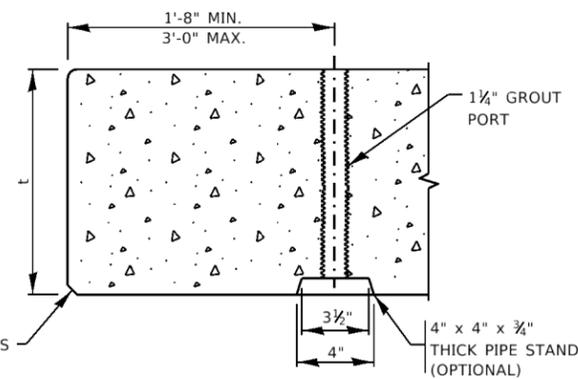
DETAIL A
GROUT PIPE STAND



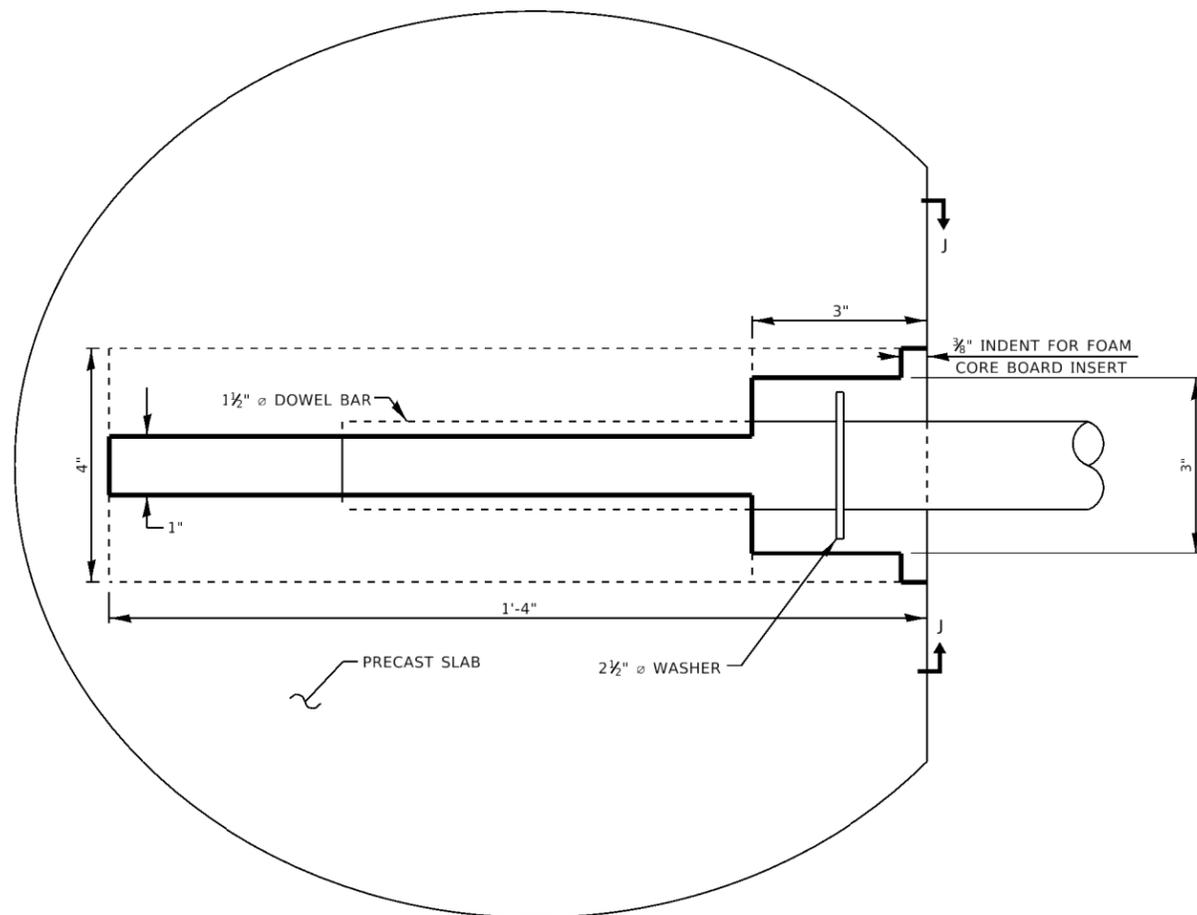
SECTION A-A
TRANSVERSE JOINT-DOWEL BAR (EMBEDDED INTO STANDARD PRECAST PAVEMENT SLAB FOR BOTH ISOLATED AND CONSECUTIVE PLACEMENT-TYP.)



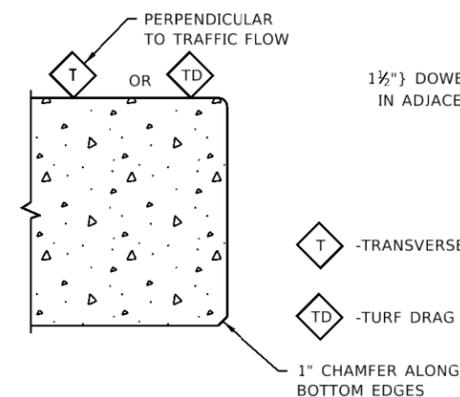
SECTION B-B
TRANSVERSE WIDE MOUTH OPEN SLOT DETAIL FOR CONSECUTIVE STANDARD SLABS



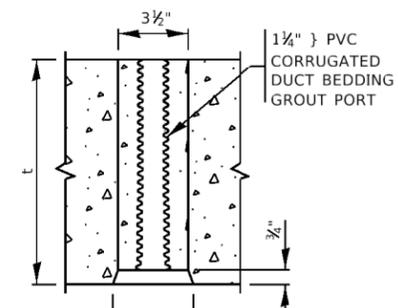
SECTION C-C
GROUT CHANNEL & PORT LOCATION



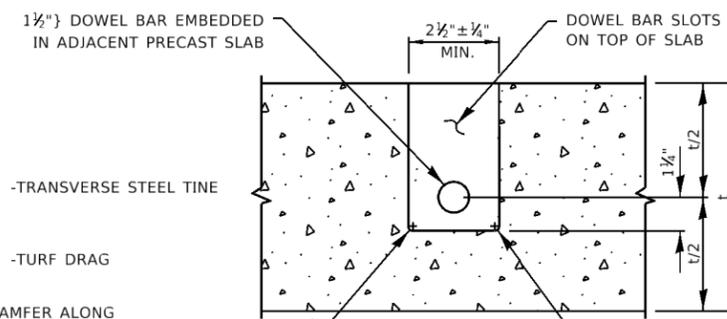
DETAIL B
TRANSVERSE NARROW MOUTH SLOT
DETAIL FOR ISOLATED SLABS



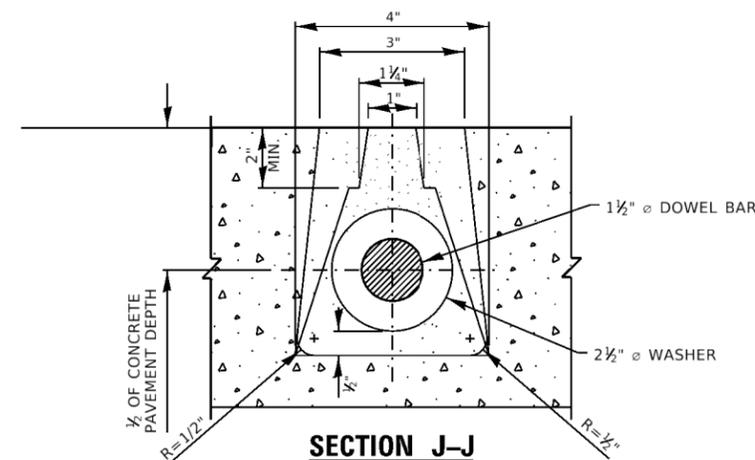
FINISH SCHEDULE



SECTION H-H
PIPE STAND ELEVATION



SECTION D-D
DOWEL BAR SECTION FOR WIDE MOUTH OPEN SLOTS



SECTION J-J
3" TAPER TO 4"x16"
LONG DOWEL SLOT

FABRICATION DETAILS

MODEL - Default
 FILE - Model - 344\pavbarcon.dwg
 ILLINOIS.gov\PIV\DOT\Documents\DOT - Offices\District 1\Projects\0113131\CAD\Drawings\Design\DCS\Std.dwg

USER NAME = gorenautab	DESIGNED - O. PATEL	REVISED - D.G. 6-14
	DRAWN -	REVISED - O.G. 9-16
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/19/2020	DATE - 10-25-2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE PAVEMENT SLABS

SCALE: NONE SHEET 7 OF 19 SHEETS STA. TO STA.

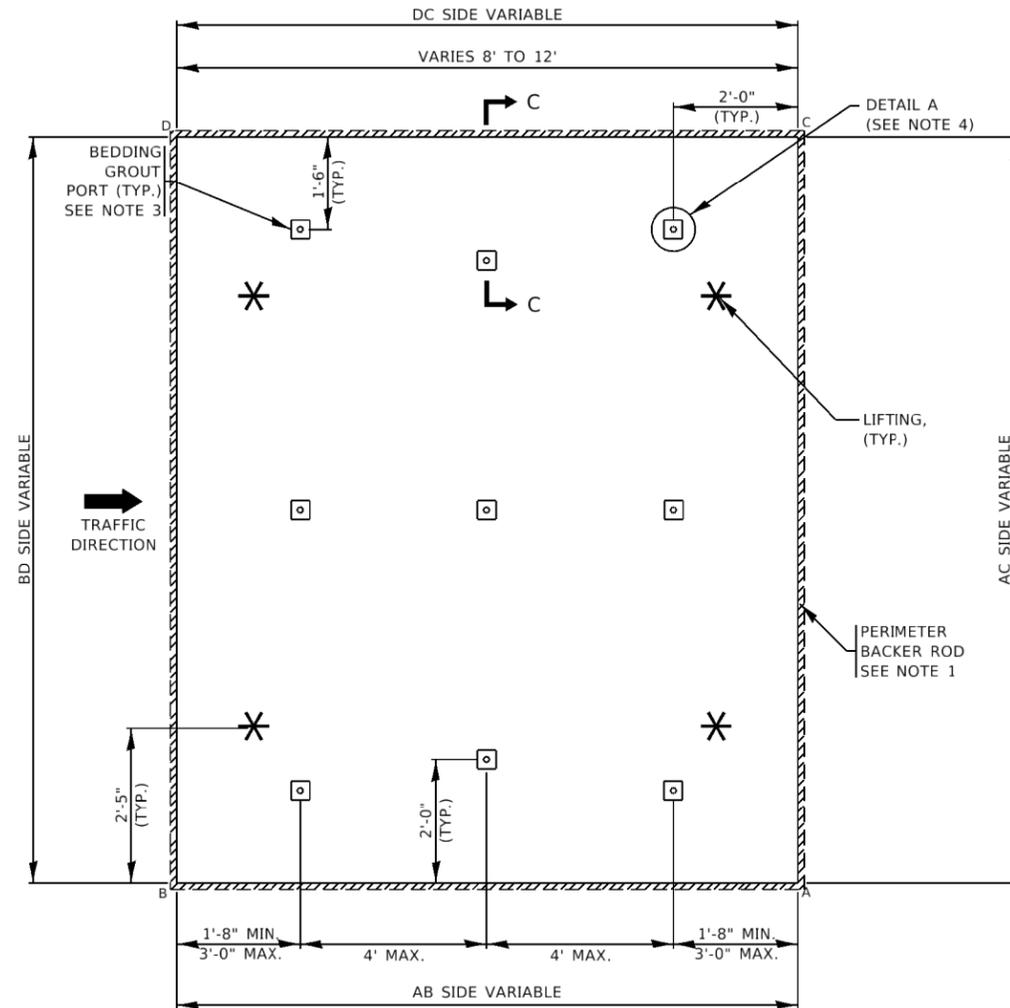
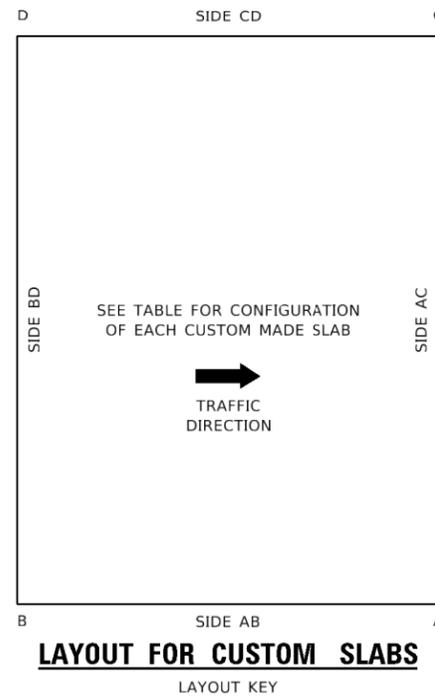
F.A.P. RTE. 344	SECTION (44X-R&530B)RS	COUNTY LAKE	TOTAL SHEETS 50	SHEET NO. 32
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				

FOR NON STANDARD SLABS, UPON COMPLETION BY THE CONTRACTOR A SLAB LAYOUT WILL BE ADDED WITH SLAB DIMENSIONS TO INCLUDE BUT NOT BE LIMITED TO THE TABLE SHOWN BELOW.

EXAMPLE	ROUTE	STATION NUMBER	MAINLINE LANE NO.	RAMP ID.	RAMP LANE NO.	MARK NO.	LANE TYPE	VARIABLES (FT.)				* AB SIDE	* BD SIDE	* CD SIDE	* AC SIDE	AREA (SQ. FT.)	VOLUME (CU. FT.)	WEIGHT (TONS)	DIAGONALS (FT.)	
								AB (FT.)	AC (FT.)	BD (FT.)	CD (FT.)								AD	BC

MAINLINE LANE NO.: LANE NO. 1 IS ADJACENT TO MEDIAN SHOULDER.
 RAMP LANE NO.: LANE NO. 1 IS ADJACENT TO INSIDE SHOULDER
 MARK NO.: EACH PANEL SHALL BE INDIVIDUALLY MARKED FOR CORRECT PLACEMENT.
 LANE TYPE: "OUT" IN THIS COLUMN INDICATES OUTSIDE LANE.
 "MID" IN THIS COLUMN INDICATES MIDDLE LANE.
 "IN" IN THIS COLUMN INDICATES INSIDE LANE

*** LEGEND**
 DB= DOWEL BAR EMBEDDED
 DS= DOWEL SLOT
 ST= SLOT OR HOLE FOR STITCHED TIE BAR
 RD= FIELD RETROFITTED DOWEL BARS



NOTES:

1. A FOAM BACKER ROD SHALL BE PLACED AROUND THE OUTSIDE PERIMETER OF THE SLAB AT THE BOTTOM OF THE JOINTS BEFORE THE SLAB HAS BEEN SET AND BEFORE BEDDING GROUT OR POLYURETHANE LEVELING FILL IS APPLIED. THE BACKER ROD SHALL NOT BE REQUIRED WHEN ANY SLAB IS LEVELLED WITH A FLOWABLE FILL.
2. EITHER SINGLE DIAMOND BLADED SAWS OR DIAMOND BLADED GANG SAWS SHALL BE USED TO MAKE THE SAW CUTS PERPENDICULAR TO THE TRANSVERSE (NONSKewed) JOINT LINE TO ALLOW FOR DOWEL BAR PLACEMENTS WITHIN THE SPECIFIED TOLERANCES.
3. SEE NOTE 8 ON SHEET 1 FOR LOCATING BEDDING GROUT PORTS.
4. SEE SHEET 7 FOR SECTION DETAILS.

MODEL - Default
 FILE - Model - 344\pavbarcon.dwg
 PROJECT - 344\pavbarcon.dwg
 USER - gorenautab
 DATE - 6/19/2020

USER NAME = gorenautab	DESIGNED - O. PATEL	REVISED - D.G. 6-14
	DRAWN -	REVISED - D.G. 9-16
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/19/2020	DATE - 10-25-2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE PAVEMENT SLABS			
SCALE: NONE	SHEET 8	OF 19 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)RS	LAKE	50	33
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				

INSTALLATION GENERAL NOTES

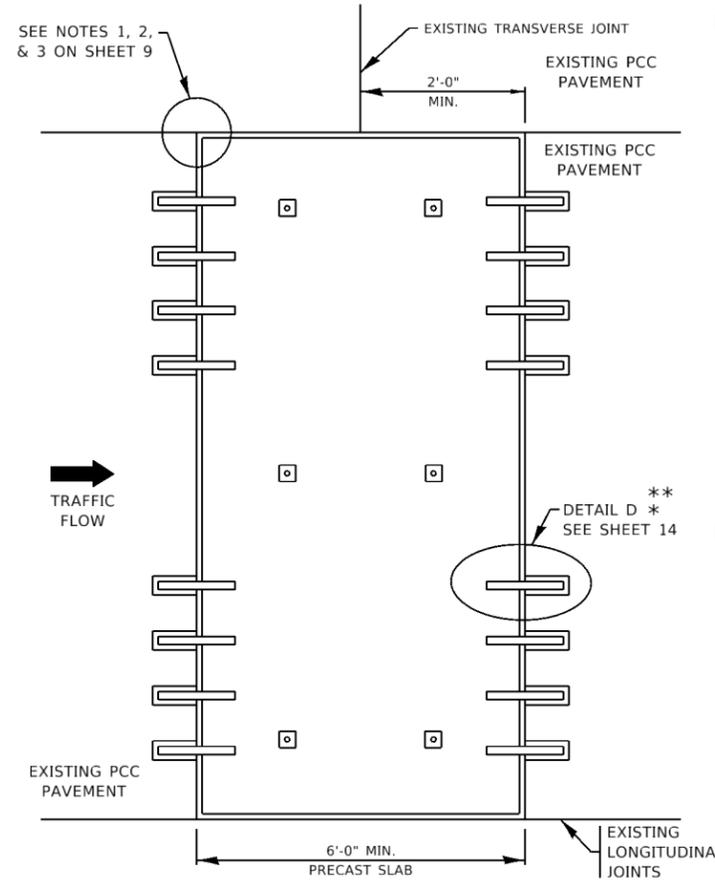
14. EPOXY COATED DOWEL BARS SHALL COMPLY WITH THE REQUIREMENTS OF ARTICLE 1006.06 (b) OF THE STANDARD SPECIFICATIONS. ANY ADDITIONAL MATERIAL REQUIRED FOR DOWEL BAR RETROFITTING SHALL BE IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "DOWEL BAR RETROFIT".
 15. EPOXY COATED TIE BARS FOR STITCHING SHALL COMPLY WITH THE REQUIREMENTS OF ARTICLE 1006.10 OF THE STANDARD SPECIFICATIONS.
 16. THE BACKER ROD USED AS A SEAL RESERVOIR GASKET AROUND THE PERIMETER OF A SLAB, NEAR THE TOP OF THE JOINTS, SHALL BE A CLOSED-CELL. PLASTIC FOAM ROD COMPATIBLE WITH THE SEALANT AND THE ELEVATED TEMPERATURES OF FINAL JOINT SEALANT APPLICATION. A CLOSED CELL PLASTIC FOAM BACKER ROD OF 3/8 " DIAMETER SHALL BE PINNED OR NAILED TO THE FINISHED BASE AROUND THE PERIMETER OF EACH OPENING BEFORE THE PANELS ARE SET.
- EQUIPMENT**
17. FOR BASE PREPARATION, A MECHANICALLY-CONTROLLED SCREEDING DEVICE OR STRAIGHTEDGE DEVICE CAPABLE OF GRADING FULLY COMPACTED FINE AGGREGATE USED AS THE LEVELING SAND TO A TOLERANCE OF 1/8 INCH PER 6 FT. LENGTHS OF PLACEMENT.
 18. CHIPPING HAMMERS SHALL BE HAND HELD AND HAVE A MAXIMUM WEIGHTS OF 30 LBS. PRIOR TO ANY HANDLE MODIFICATION WHERE APPLICABLE.
 19. WITH ANY FIELD RETROFITTING OF DOWEL BARS, A TEMPLATE SHALL BE ROUTINELY USED FOR ALL STANDARD SLABS IN ORDER TO LOCATE AND ALIGN THE SAWCUTS CONSISTENTLY. EITHER SINGLE DIAMOND BLADED SAWS OR DIAMOND BLADED GANG SAWS SHALL BE USED TO MAKE SAW CUTS PERPENDICULAR TO THE TRANSVERSE (NONSKEWED) JOINT LINE TO ALLOW FOR DOWEL BAR PLACEMENTS WITHIN THE FOLLOWING TOLERANCES:
 - ± 1/2 INCH OF THE MIDDLE OF THE CONCRETE SLAB DEPTH.
 - ± 1/2 INCH OF BEING CENTERED OVER THE TRANSVERSE JOINT
 - ± 1/4 " FROM PARALLEL TO THE CENTERLINE OVER 12 INCHES OF THE BAR
 - ± 1/4 " FROM PARALLEL TO THE ROADWAY SURFACE OVER 12 INCHES OF THE BAR
 SAWCUTS SAWED ACROSS SKEWED JOINTS SHOULD ALLOW EQUAL LENGTH OF THE DOWEL BAR TO BE PLACED ACROSS THE TRANSVERSE JOINT. THE ALIGNMENT OF SAWCUTS MUST BE PARALLEL TO THE ROADWAY CENTERLINE, REGARDLESS OF TRANSVERSE JOINT SKEW.
 20. WITH ANY FIELD INSERTIONS OF DOWEL BARS INTO PREDRILLED HOLES, THE DRILLING MACHINE SHALL BE IN ACCORDANCE WITH ARTICLE 442.03(g) OF THE STANDARD SPECIFICATIONS. HAND HELD DRILLING TOOLS WILL NOT BE ALLOWED.
 21. THE COMPRESSOR FOR AIR BLASTING SHALL HAVE A MINIMUM CAPACITY OF 120 CFM. THE COMPRESSED AIR SHALL BE FREE FROM OIL AND OTHER CONTAMINANTS.
 22. CONSOLIDATION EQUIPMENT USED TO CONSOLIDATE THE CONCRETE REPAIR MATERIAL IN THE RETROFITTED DOWEL BAR SLOTS SHALL BE INTERNAL VIBRATORS WITH A MAXIMUM DIAMETER OF 1 INCH AND SHALL HAVE A RESILIENT COVERING THAT WILL NOT DAMAGE THE EPOXY COATED REINFORCEMENT DURING USE. ANY VIBRATORS OR RODS USED FOR CONSOLIDATION OF THE REPAIR MATERIAL FOR NARROW MOUTH SLOTS SHALL HAVE A DIAMETER OF LESS THAN 1 INCH.
 23. BATCHING EQUIPMENT FOR FLOWABLE FILL SHALL HAVE DEVICES DESIGNED TO MEASURE THE SPECIFIED QUANTITIES OF EACH COMPONENT MATERIAL, AND MIXING SHALL BE OF SUFFICIENT DURATION TO INSURE UNIFORM CONSISTENCY OF THE MIXTURE. NO WATER WILL BE ADDED TO THE FLOWABLE FILL MIXTURE AFTER BATCHING. WATER CONTENT SHALL BE MAINTAINED SUCH THAT COMPRESSIVE STRENGTHS ARE ACHIEVED AND A UNIFORM, FLOWABLE MIXTURE IS DEVELOPED THAT IS ESSENTIALLY SELF-LEVELLING WHEN PLACED.
 24. EQUIPMENT FOR HIGH-DENSITY FOAM INJECTION SHALL INCLUDE A TRUCK MOUNTED PUMPING UNIT CAPABLE OF INJECTING THE POLYURETHANE BETWEEN THE CONCRETE AND THE SLAB SUBBASE. THE PUMP SHALL BE CAPABLE OF CONTROLLING THE RATE OF RISE OF THE PAVEMENT SLAB. A LEVELING UNIT SHALL BE PROVIDED TO ENSURE THE SLABS ARE RAISED TO AN EVEN PLANE, WITH VERTICAL ELEVATION DIFFERENCE ACROSS ANY CORNER NOT TO EXCEED 1/4 INCH.
 25. EQUIPMENT FOR MIXING AND PUMPING ANY GROUT/ADHESIVE MATERIALS FOR BEDDING THE SLABS, RETROFITTING DOWEL BARS, OR CROSS STITCHING TIE BARS SHALL BE IN ACCORDANCE WITH THE MATERIAL MANUFACTURER'S INSTRUCTIONS AND THE SPECIFICATIONS.
- REMOVAL /INSTALLATION:**
26. PERIMETER SAWCUTTING OF THE REMOVAL AREA AND SAWCUTTING OF THE DOWEL BAR SLOTS SHALL NOT BE CARRIED OUT MORE THAN (1) WEEK IN ADVANCE OF THE EXPECTED DATE OF REPAIR. THE CONTRACTOR SHALL USE A TEMPLATE TO PRECISELY DELINEATE THE LIMITS OF THE AREAS TO BE REPAIRED AS DEFINED ON THE CONTRACT DOCUMENTS AND APPROVED SHOP DRAWINGS, WITHIN A TOLERANCE OF 1/2 INCH. REPAIRS SHALL BE NO LESS THAN THE FULL WIDTH OF A LANE AND THE FULL DEPTH OF CONCRETE.

27. REMOVAL OF EXISTING PAVEMENT SHALL BE IN ACCORDANCE WITH SECTION 440 OF THE STANDARD SPECIFICATIONS EXCEPT AS FOLLOWS:
 - A. THE OUTER LIMITS OF THE REPAIR AREA WILL BE SAWCUT FULL DEPTH AND SHALL NOT EXTEND (OVERCUT) BY MORE THAN 10 INCHES INTO THE ADJACENT CONCRETE THAT IS TO REMAIN IN PLACE. OVERCUTS SHALL BE FILLED WITH A PRODUCT ACCEPTABLE TO THE DEPARTMENT. THE OUTER LIMITS FOR REPAIR SHALL BE MARKED OUT BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY SAWCUTTING.
 - B. REMOVAL OF CONCRETE WITHIN THE PERIMETER SAWCUTS SHALL BE BY THE LIFT-OUT METHOD, AND CONCRETE BETWEEN SAWCUTS FOR DOWEL BAR RETROFITS SHALL BE REMOVED USING JACKHAMMER AND HAND TOOLS. THE CONTRACTOR SHALL ENSURE THAT REMOVALS ARE CARRIED OUT WITHOUT DAMAGING THE ADJACENT CONCRETE PAVEMENT OR ASPHALT SHOULDER OR DISTURBING THE UNDERLYING BASE. HEAVY BREAKING EQUIPMENT SUCH AS HOE RAMS SHALL NOT BE USED IN THE REMOVAL OPERATION. THE CONCRETE PAVEMENT SHALL NOT BE BROKEN IN PLACE.
 - C. IF DURING THE REMOVAL PROCESS THE ADJACENT CONCRETE IN THE SAME LANE OR IN AN ADJACENT LANE THAT CAN ONLY BE REPAIRED DURING NIGHT TIME LANE CLOSURES, IS DAMAGED OR CRACKED DUE TO THE CONTRACTOR'S REMOVAL PROCEDURE, THE DAMAGED AREA SHALL BE CUT BACK FULL DEPTH TO SOUND CONCRETE AND REPLACED WITH PRECAST SLABS AT THE CONTRACTOR'S EXPENSE. IF CONCRETE IN THE ADJOINING LANE IS DAMAGED DURING THE REMOVAL PROCESS AND WEEKEND REPAIRS ARE POSSIBLE, THE DAMAGED CONCRETE SHALL BE REPAIRED IN ACCORDANCE SECTION 442 OF THE STANDARD SPECIFICATIONS AT THE CONTRACTOR'S EXPENSE. ASPHALT SHOULDER DAMAGED DURING THE REMOVAL PROCESS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL PROVIDE A PROPOSAL FOR REPAIRS TO THE ENGINEER FOR DEPARTMENT APPROVAL.
 - D. DISPOSAL OF EXCAVATED MATERIALS FROM THE REMOVAL OF CONCRETE SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AT THE CONTRACTOR'S EXPENSE.
 - E. ALL SLURRY FROM SAW CUTTING OPERATIONS SHALL BE THOROUGHLY SCRAPED AND REMOVED FROM THE PAVEMENT SURFACE BEFORE THE PAVEMENT IS OPENED TO TRAFFIC. DISPOSAL OF SLURRY SHALL BE IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AT THE CONTRACTORS EXPENSE.
28. ANY AREAS OF SUBBASE WHICH ARE BELOW THE REQUIRED ELEVATION OF THE FINISHED SUBBASE, SHALL BE BUILT UP TO GRADE WITH SATISFACTORY COMPACTED GRANULAR MATERIAL.
29. LEVELING MATERIAL PLACED BEFORE SLAB INSTALLATION SHALL BE EITHER A FLOWABLE FILL OR A FINE AGGREGATE MEETING THE REQUIREMENTS OF THIS CONTRACT DOCUMENT. FLOWABLE FILL SHALL BE USED AS A LEVELING MATERIAL ONLY ON TANGENT PAVEMENT SECTIONS. GRADE CONTROL SHALL BE ESTABLISHED FOR ALL LEVELING MATERIAL USING STRINGLINES, LASER GUIDANCE, OR OTHER APPROVED METHODS. THE TEMPERATURE OF THE FLOWABLE FILL MIXTURE AS MANUFACTURED AND DELIVERED SHALL BE AT LEAST 50° F. NONFLOWABLE FILL WILL BE ALLOWED IF THE ANTICIPATED AIR TEMPERATURE WILL BE 36°F OR LESS WITHIN 24 HOURS OF SLAB PLACEMENT. THE FLOWABLE FILL MUST OBTAIN FINAL SET BEFORE THE PAVEMENT MAY BE OPENED TO TRAFFIC.
30. WHEN FLOWABLE FILL IS USED AS THE LEVELING MATERIAL WITH SLAB INSTALLATION. A PERIMETER BACKER ROD WILL NOT BE REQUIRED AROUND THE PERIMETER OF THE SLAB.
31. LEVELING MATERIAL PLACED IMMEDIATELY AFTER SLAB INSTALLATION SHALL ONLY BE A HIGH-DENSITY POLYURETHANE FOAM MEETING THE REQUIREMENTS OF THIS CONTRACT DOCUMENT. PLACEMENT OF POLYURETHANE FOAM SHALL FILL ALL VOIDS BENEATH THE PRECAST PANELS THAT MAY BE PRESENT AFTER PLACING THE PANELS OVER THE PREPARED SUBBASE AND LEVELING AGGREGATE. PLACEMENT OF THE POLYURETHANE SHALL UTILIZE THE UNDERSLAB GROUT PORT HOLES AS SHOWN ON THE PLANS. THE PORT HOLES ARE TO BE FILLED WITH THE DOWEL BAR BACKFILLING MATERIAL.
32. FOLLOWING PROPER REMOVAL OF EXISTING PAVEMENTS AND ACCEPTABLE BASE PREPARATION/LEVELING, THE CONTRACTOR SHALL HAVE ALL EQUIPMENT REQUIRED FOR PANEL INSTALLATION ON-SITE PRIOR TO BEGINNING PANEL INSTALLATION. LIFTING AND TRANSPORTING EQUIPMENT SHALL NOT DAMAGE THE PREPARED SUBBASE/LEVELING MATERIALS PRIOR TO OR DURING PANEL INSTALLATION. PRIOR TO SLAB INSTALLATION, ALL VERTICAL SURFACES OF SURROUNDING PAVEMENT SHALL BE COATED WITH A BOND BREAKER SUCH AS FORM OIL OR A CURING COMPOUND.
33. PANELS SHALL BE INSTALLED ONE AT A TIME, AND SHALL BE INSTALLED IN SUCH A MANNER THAT THE SUBBASE/LEVELING MATERIAL OR ANY REMAINING PAVEMENT IS NOT DAMAGED DURING INSTALLATION. DURING PLACEMENT OF THE SLABS, USE TIE OFF ROPES TO AVOID CHIPPING OR SPALLING EDGES OF THE PRECAST UNITS. USE WOOD SHIMS OR WEDGES TO GUIDE THE SLAB INTO THE CORRECT POSITION. THE USE OF STEEL PRY BARS THAT CHIP EDGES SHOULD BE AVOIDED.

34. IMMEDIATELY AFTER THE SLAB HAS BEEN SET AND LEVELLED, SURVEY THE VERTICAL ELEVATION ACROSS ALL CORNERS TO VERIFY THE VERTICAL DIFFERENCE ABOVE ADJACENT CONCRETE IS 1/4 INCH ACROSS ALL CORNERS. ALL PRECAST SLABS SHALL RECEIVE A PROFILE DIAMOND GRIND THAT MAY INCLUDE UP TO 4' OF THE SURROUNDING PAVEMENT TO MAINTAIN SLOPE.
35. IF A SET PRECAST SLAB IS OPENED TO TRAFFIC BEFORE ANY GROUTING OPERATIONS, THE CONTRACTOR SHALL MEET THE FOLLOWING REQUIREMENTS:
 - i) DURING INSTALLATION, INCOMPRESSIBLE SHIMS, APPROVED BY THE ENGINEER, SHALL BE PLACED IN THE DOWNSTREAM TRAVERSE JOINT TO CORRECT AND MAINTAIN HORIZONTAL ALIGNMENT OF THE SLAB. INCLUDE SHIMS ON THE LONGITUDINAL JOINT FOR SLABS SUBJECTED TO TURNING PRESSURES. THE TOTAL THICKNESS OF SHIMS USED IN ANY JOINT SHALL BE EQUAL TO OR LESS THAN 3/8 ". SHIMS SHALL BE REMOVED BEFORE JOINT SEALING OPERATIONS. EQUIPMENT AND METHODS USED FOR REMOVING SHIMS SHALL BE SUCH AS TO PREVENT CRACKING, SHATTERING OR SPALLING OF PAVEMENT REMAINING IN PLACE. THE WORK TO REMOVE THE SHIMS SHALL ALSO BE INCLUDED IN THE COST OF THE PRECAST SLAB.
 - ii) ASPHALT SHOULDERS SHALL BE BACKFILLED TO MAINTAIN HORIZONTAL ALIGNMENT.
 - iii) WIDE MOUTH DOWEL SLOTS LEFT OPEN SHALL BE TEMPORARILY FILLED WITH A COMPRESSION SEAL APPROVED BY THE ENGINEER TO WITHIN 1 INCH FLUSH WITH THE PAVEMENT SURFACE.
 - iv) NARROW MOUTH DOWEL SLOTS MAY BE LEFT OPEN.
 - v) ALL GROUTING MEETING THE REQUIREMENTS OF THIS CONTRACT SHALL BE COMPLETED WITHIN 48 HOURS OF EACH SLAB'S PLACEMENT.
36. PRIOR TO DOWEL BAR PLACEMENT, THE TRANSVERSE JOINT SHALL BE CAULKED WITH A SILICONE SEALANT AT THE BOTTOM AND SIDES OF THE SLOT. THE CAULKING FILLER SHOULD NOT BE PLACED ANY FARTHER THAN 1/2 INCH OUTSIDE EITHER SIDE OF THE JOINT, AND APPLIED SUFFICIENTLY TO PREVENT ANY PATCHING MATERIAL FROM ENTERING THE JOINT AT THE BOTTOM OR SIDES OF THE SLOT. EXCESSIVE SEALANT AROUND THE SLOT DOES NOT ALLOW THE CONCRETE PATCHING MATERIAL TO BOND TO THE SIDES OF THE SLOT. BEFORE PLACEMENT, THE DOWEL BARS SHOULD BE LIGHTLY COATED WITH PARTING COMPOUND AND FULLY RETROFITTED DOWEL BARS PLACED ON A CHAIR THAT WILL PROVIDE A MINIMUM 1/2 INCH CLEARANCE BETWEEN THE BOTTOM OF THE DOWEL AND THE BOTTOM OF THE SLOT. FOR ANY DOWEL BARS INSERTED INTO PREDRILLED EPOXIED HOLES, AN APPURATUS CAPABLE OF MAINTAINING VERTICAL ALIGNMENT OF THE DOWEL AND TO PROVIDE A MINIMUM 1/2 INCH CLEARANCE BETWEEN THE BOTTOM OF THE DOWEL AND THE BOTTOM OF THE SLOT SHAL BE PROVIDED BY THE CONTRACTOR. A 3/8 INCH THICK FOAM INSERT SHOULD BE PLACED AT THE MIDDLE OF THE DOWEL TO MAINTAIN THE TRANSVERSE JOINT. THE FOAM INSERT SHOULD FIT TIGHTLY AROUND THE DOWEL, THE BOTTOM, AND THE EDGES OF THE SLOT, AND BE UP TO THE SURFACE OF THE EXISTING CONCRETE SURFACE. THE FOAM INSERT SHOULD BE CAPABLE OF REMAINING IN A VERTICAL POSITION AND HELD TIGHTLY TO ALL EDGES DURING PLACEMENT OF THE PATCH. IF FOR ANY REASON THE FOAM INSERT SHIFTS DURING PLACEMENT OF THE CONCRETE PATCHING MATERIAL, THE WORK SHALL BE REJECTED AND REDONE AT THE CONTRACTOR'S EXPENSE.
37. PLACEMENT OF HARDWARE GROUT/ADHESIVES:
 - A. DOWEL BARS - THE PLACEMENT OF ANY APPROVED BACKFILL MATERIAL FOR DOWEL BAR RETROFITTING OR FOR DOWEL BAR INSERTIONS SHALL BE IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "DOWEL BAR RETROFIT". THE PAVEMENT WILL NOT BE OPENED TO TRAFFIC UNTIL THE BACKFILL MATERIAL AROUND THE PAVEMENT HARDWARE OBTAINS 3,000 PSI COMPRESSIVE STRENGTH. ALL CONCRETE SURFACES WITHIN THE SLOT SHALL BE SOLID, FREE FROM LOOSE OR UNSOUND FRAGMENTS. BEFORE GROUTING, SANDBLAST ALL EXPOSED SURFACES IN THE DOWEL BAR SLOT FOLLOWED BY AIR BLASTING TO REMOVE ANY DUST, RESIDUE OR DEBRIS LEFT IN THE SLOT. UPON COMPLETION OF THE RETROFITTING WORK, THE GROUT OR CONCRETE PATCH MATERIAL SHALL FILL ALL SLOTS TO THE SURFACE OF THE EXISTING PAVEMENTS. ANY SLOTS INSUFFICIENTLY FILLED BELOW EXISTING PAVEMENT SURFACES SHALL BE REDONE AT THE CONTRACTOR'S EXPENSE.
 - B. TIE BARS - A FOAM BOARD GASKET SHALL BE INSERTED INTO THE LONGITUDINAL JOINT AT THE STITCHING LOCATION AND THE TIEBAR HOLE PREDRILLED THROUGH THE GASKET. AFTER PREDRILLED HOLES ARE AIR BLASTED, PRESSURE INJECT THE APPROVED ADHESIVE INTO THE PREDRILLED HOLES, LEAVING SOME VOLUME FOR THE BAR TO OCCUPY THE HOLE. INSERT THE TIEBAR INTO THE HOLE, LEAVING ABOUT 1 INCH FROM THE TOP OF THE TIE BAR TO THE PAVEMENT SURFACE. REMOVE EXCESS ADHESIVE AND FINISH FLUSH WITH THE PAVEMENT SURFACE.
 - C. FILL LIFTING INSERT HOLES AND GROUT PORTS WITH THE APPROVED GROUT USED FOR DOWEL BAR RETROFITTING.
38. PLACEMENT OF UNDERSEALING GROUT SHALL FILL ALL VOIDS BENEATH THE PRECAST PANELS AND GROUT PORT HOLES THAT MAY BE PRESENT AFTER PLACING THE PANELS OVER THE PREPARED SUBBASE AND LEVELING AGGREGATE. PLACEMENT OF THE UNDERSEALING GROUT SHALL UTILIZE THE UNDERSLAB GROUT PORT HOLES AS SHOWN ON THE PLANS. PLACEMENT OF UNDERSEALING GROUT SHALL NOT OCCUR UNTIL AFTER ALL HARDWARE DEVICES ARE PLACED AND GROUTED. IF UNDERSEALING GROUT FILLS ANY LONGITUDINAL JOINT TO WITHIN 9" OF THE SLAB SURFACE, A 9" SAW CUT OF THE JOINT SHALL BE REQUIRED DURING INSTALLATION. IF UNDERSEALING GROUT FILLS ANY TRANSVERSE JOINT TO WITHIN 9" OF THE SLAB SURFACE, THEN A 9" SAW CUT OF THE JOINT SHALL BE REQUIRED FOLLOWED BY REMOVAL AND FULL RETROFITTING OF ALL SEVERED DOWEL BARS ACROSS THE JOINT.
39. AFTER INSTALLATION AND GROUTING IS COMPLETED ALL LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SEALED IN ACCORDANCE WITH ARTICLE 420.12 OF THE STANDARD SPECIFICATIONS

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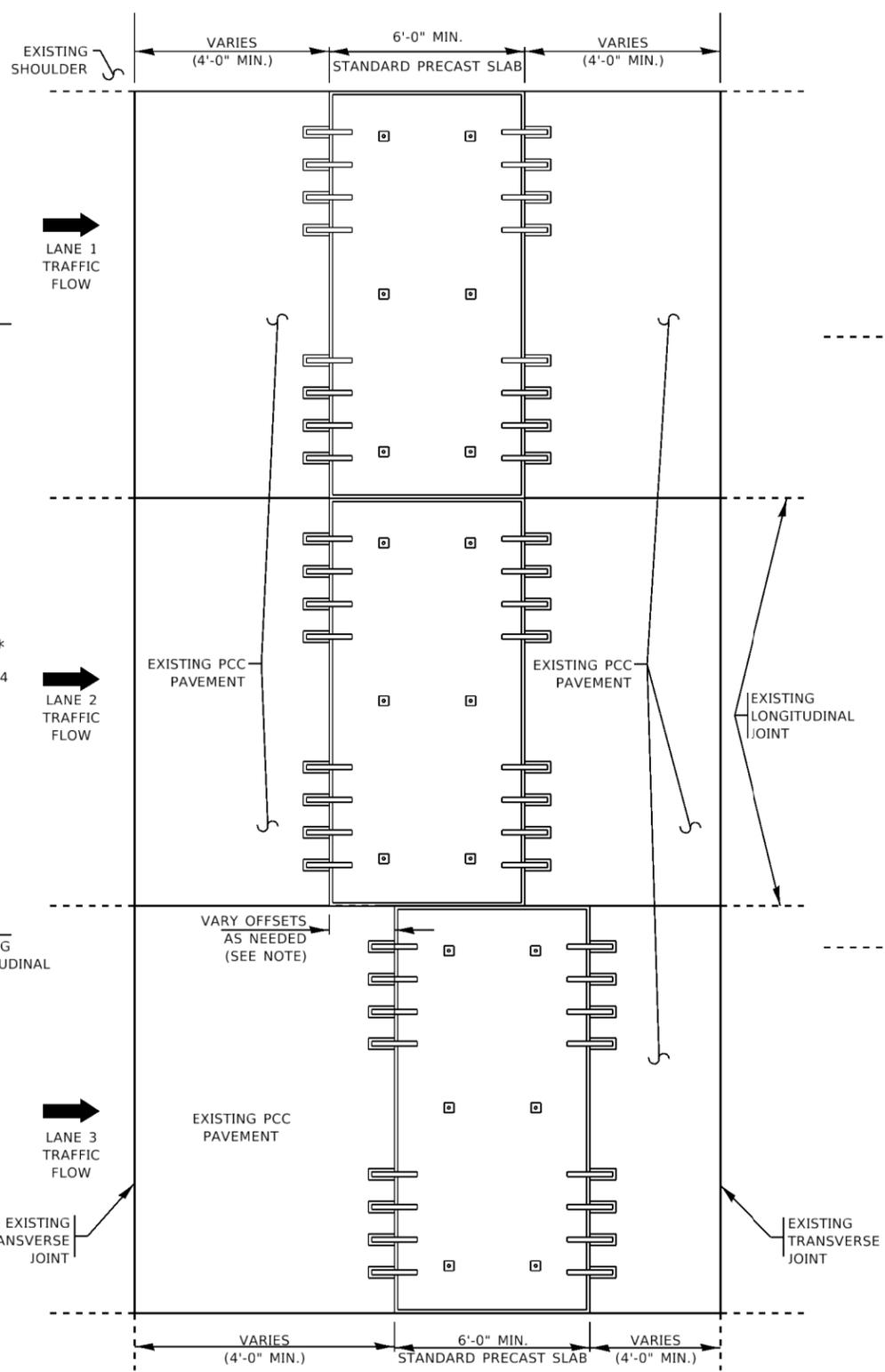
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	DRAWN -	REVISED - D.G. 9-16				344	(44X-R6530B)RS	LAKE	50	35	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - D.G. 8-19			BD 57		CONTRACT NO. 60V06			
	PLOT DATE = 6/19/2020	DATE -	REVISED -			SCALE: NONE	SHEET 10 OF 19 SHEETS	STA. TO STA.	ILLINOIS / FED. AID PROJECT		



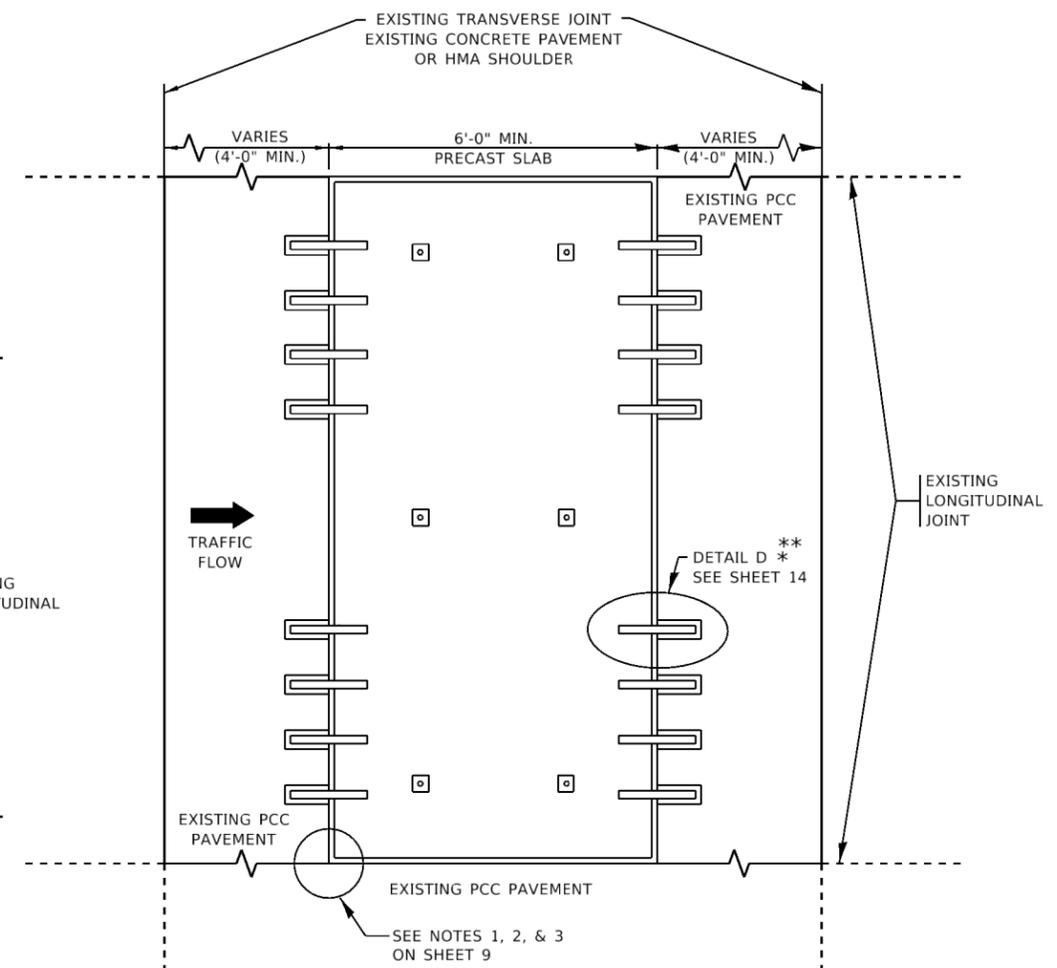
**SINGLE LANE JOINT REPLACEMENT
DROP IN PANEL FOR STANDARD
PRECAST SLABS**

NOTE:
WHEN THE TRANSVERSE JOINTS OF ANY PRECAST SLAB CANNOT BE ALIGNED WITH TRANSVERSE JOINTS IN ADJACENT LANES, A MAXIMUM 2'-0" OFFSET BETWEEN JOINTS SHALL BE PROVIDED. FOR DOWEL BARS FULLY RETROFITTED IN THE FIELD, REFER TO DETAIL C ON SHEET 13.

** FOR DOWEL BARS TO BE SLID INTO PRE-DRILLED HOLES IN THE FIELD, REFER TO DETAIL G ON SHEET 17.



**MULTIPLE LANE MID SLAB DROP IN PANEL
FOR STANDARD PRECAST PANELS**



**SINGLE LANE MID SLAB DROP
IN PANEL FOR STANDARD
PRECAST PANELS**

INSTALLATION OF ISOLATED STANDARD PRECAST SLABS

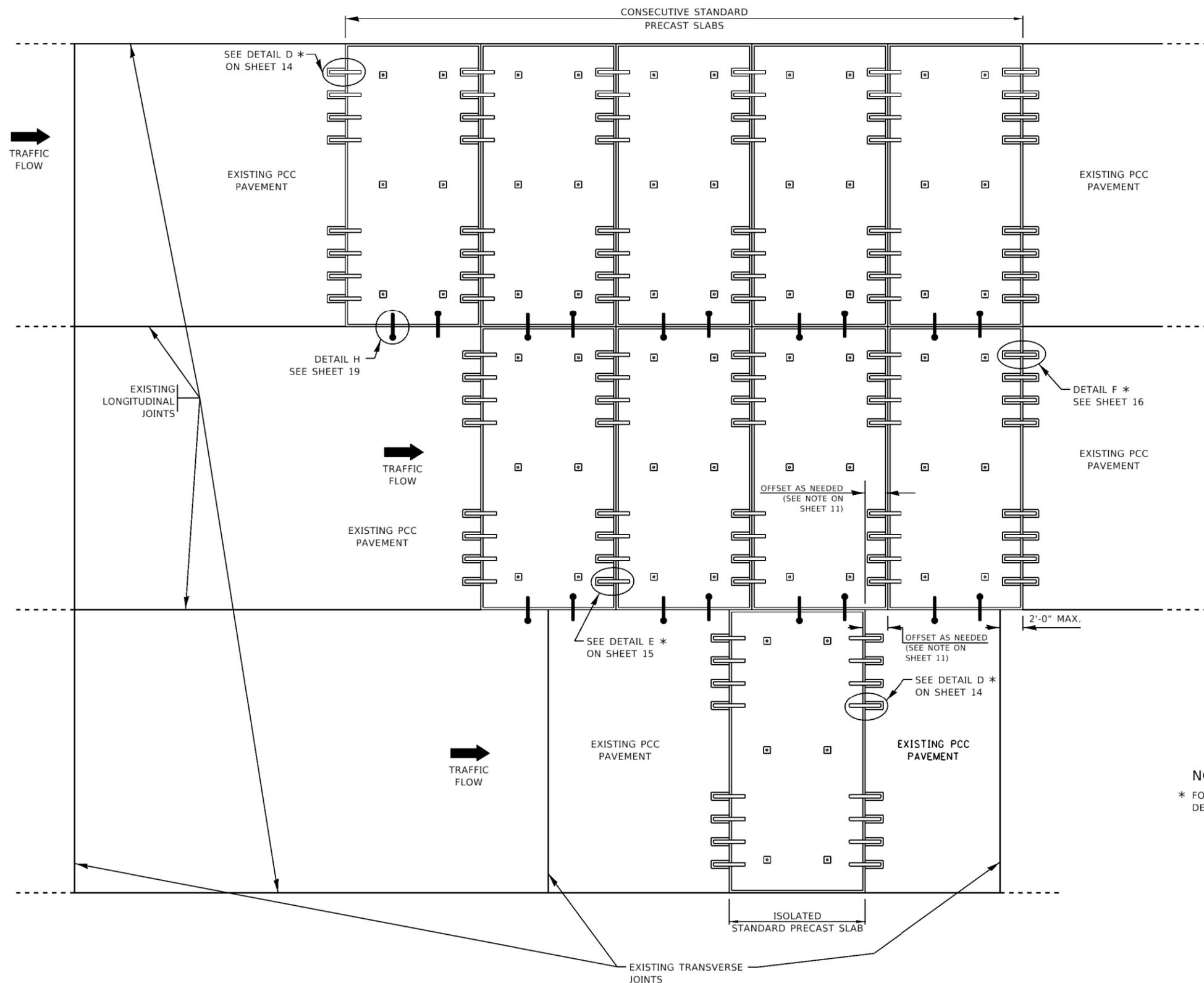
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PLOT DATE = 6/19/2020	DATE - 10-25-2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PRECAST CONCRETE PAVEMENT SLABS			
SCALE: NONE	SHEET 11 OF 19 SHEETS	STA.	TO STA.

F.A.P. RTE. 344	SECTION (44X-R&530B)RS	COUNTY LAKE	TOTAL SHEETS 50	SHEET NO. 36
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				



NOTE:
 * FOR DOWEL BARS FULLY RETROFITTED IN THE FIELD, REFER TO
 DETAIL C ON SHEET 13.

INSTALLATION OF CONSECUTIVE STANDARD PRECAST SLABS

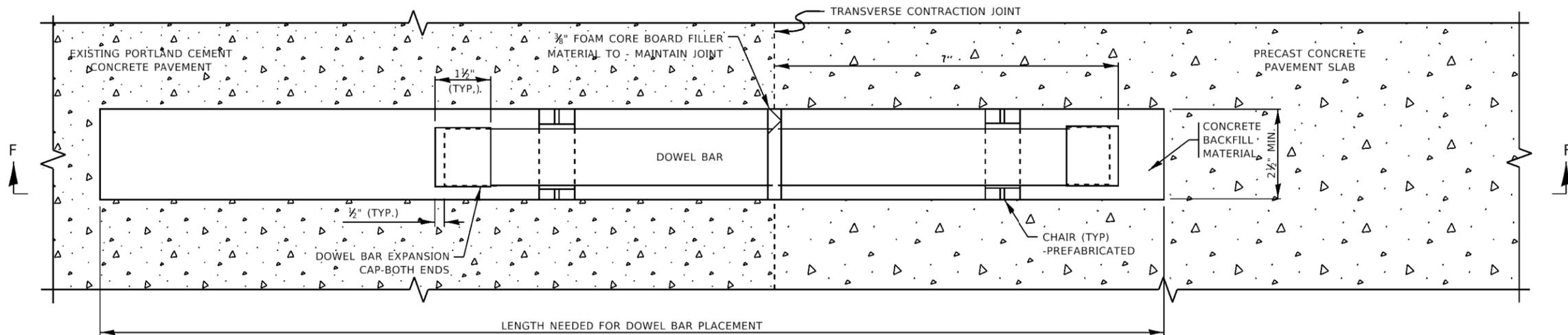
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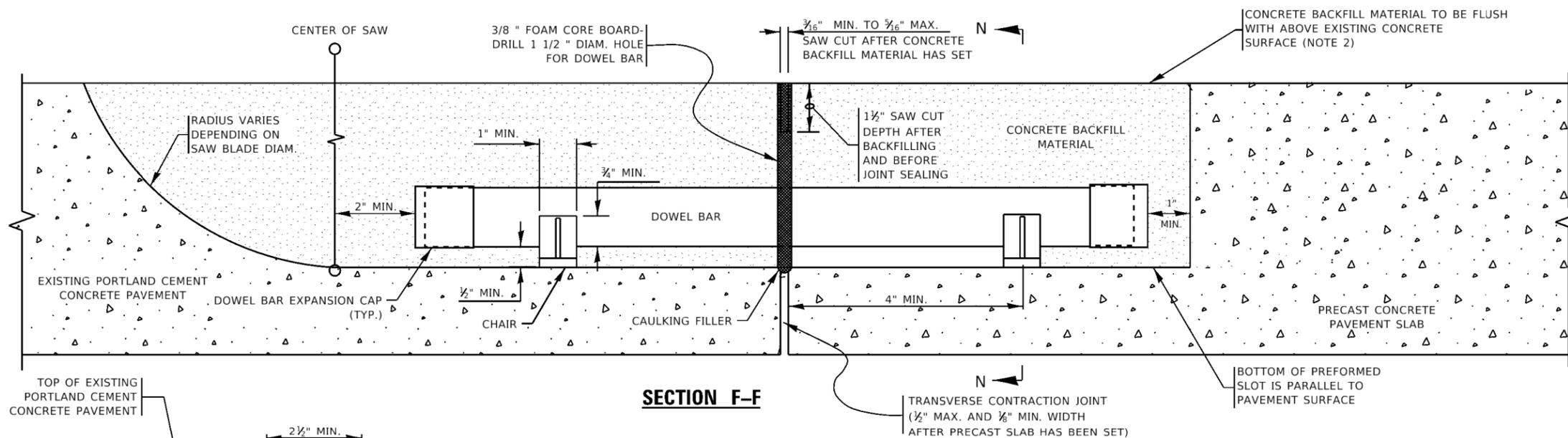
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE PAVEMENT SLABS			
SCALE: NONE	SHEET 12 OF 19 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)RS	LAKE	50	37
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				

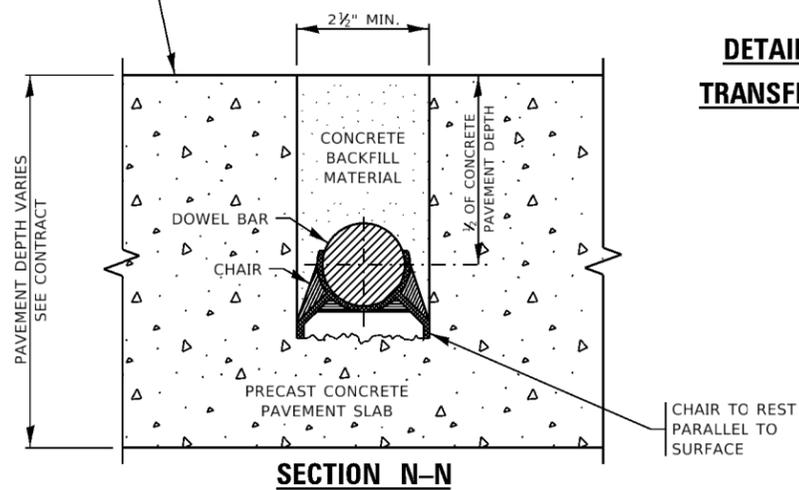


PLAN VIEW

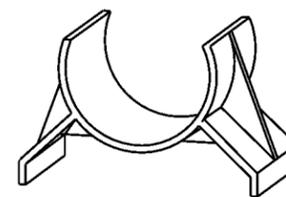


SECTION F-F

DETAIL-F, WIDE MOUTH DOWEL BAR PLACEMENT DETAIL FOR THE LAST TRANSFER JOINT OF CONSECUTIVELY PLACED STANDARD PRECAST PANELS



SECTION N-N



CHAIR DETAIL

NOTES:

1. PLACE FOAM CORE BOARDS TO THE TOP OF PATCH.
2. UPON COMPLETION, THE FINISHED SURFACE OF THE CONCRETE BACKFILL MATERIAL SHALL NOT BE BELOW THE EXISTING CONCRETE SURFACE.

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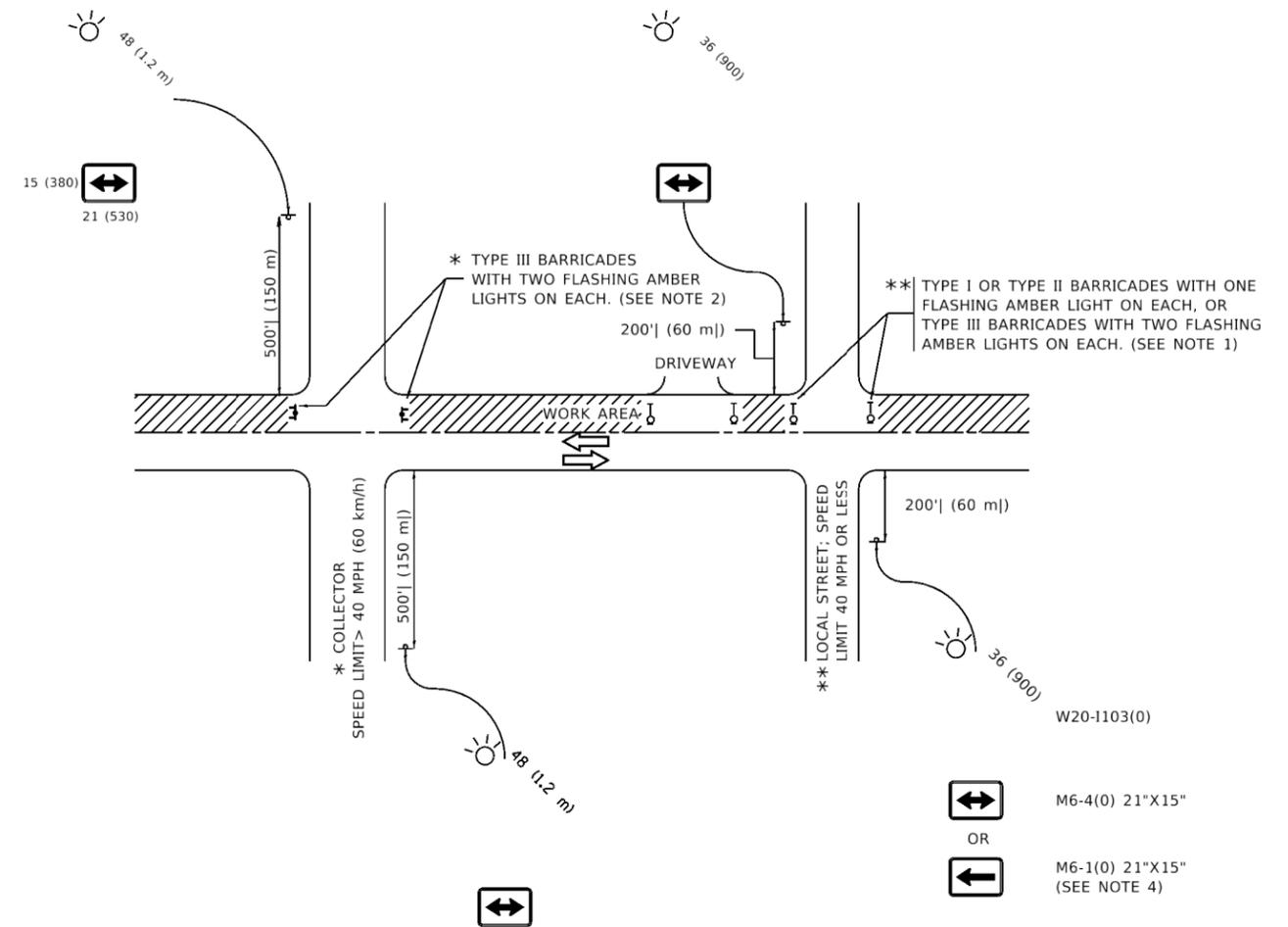
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PLOT DATE = 6/19/2020	DATE - 10-25-2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PRECAST CONCRETE PAVEMENT SLABS

SCALE: NONE SHEET 16 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)R5	LAKE	50	41
BD 57		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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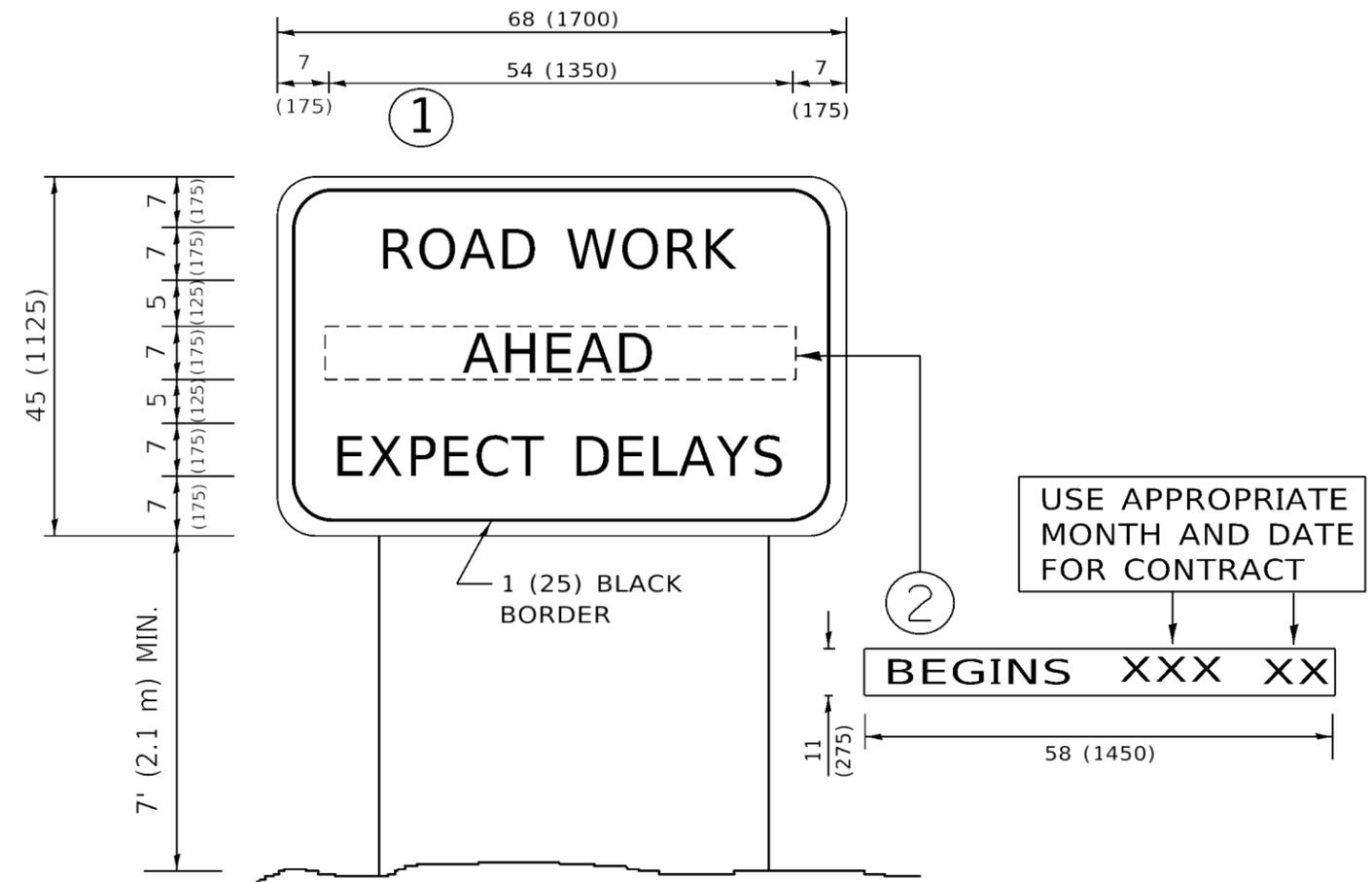
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PLOT DATE = 6/19/2020	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6&5308)RS	LAKE	50	45
TC-10		CONTRACT NO. 60V06		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

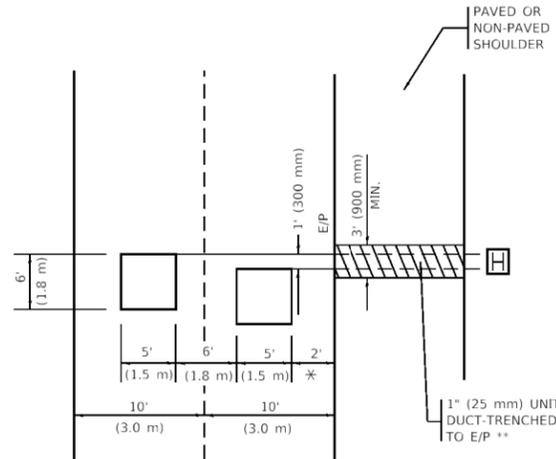
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)RS	LAKE	50	49
TC-22			CONTRACT NO. 60V06	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

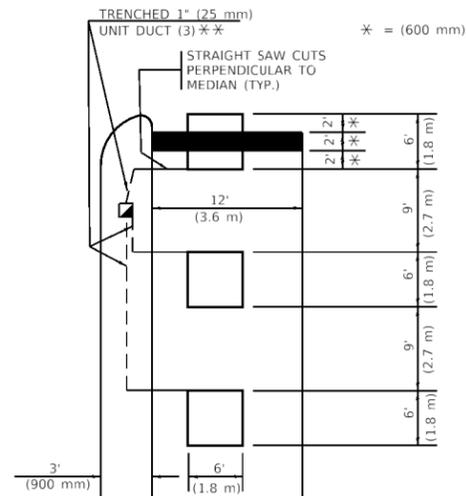


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

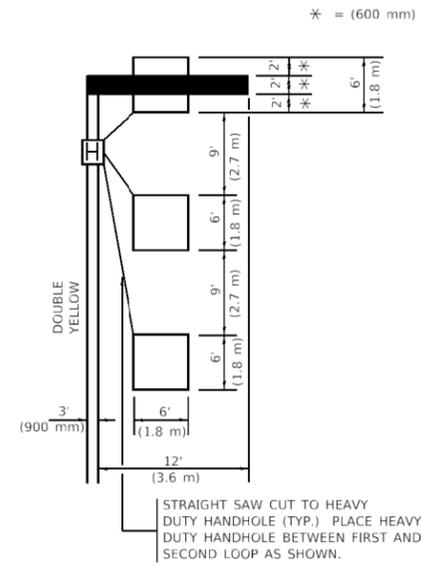


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

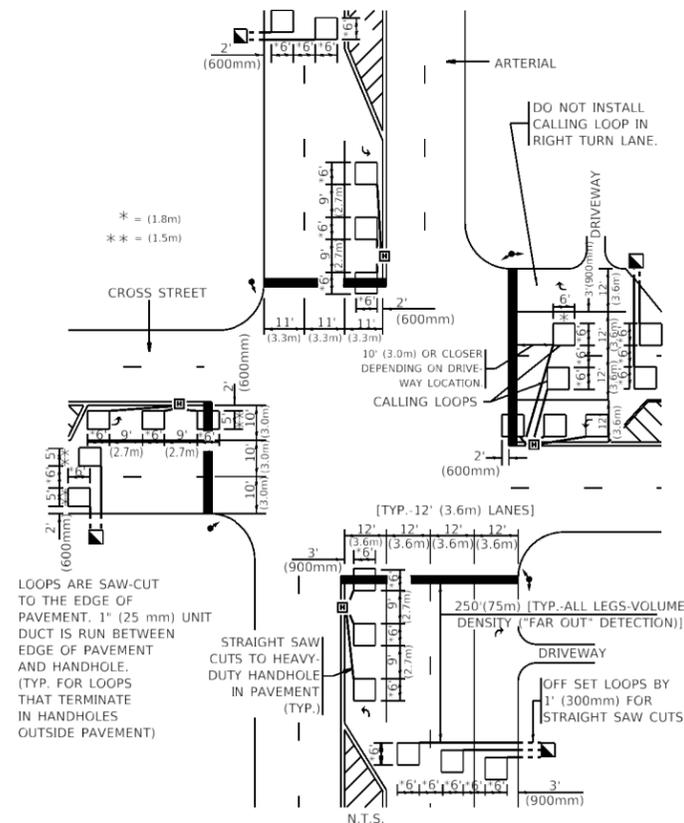
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

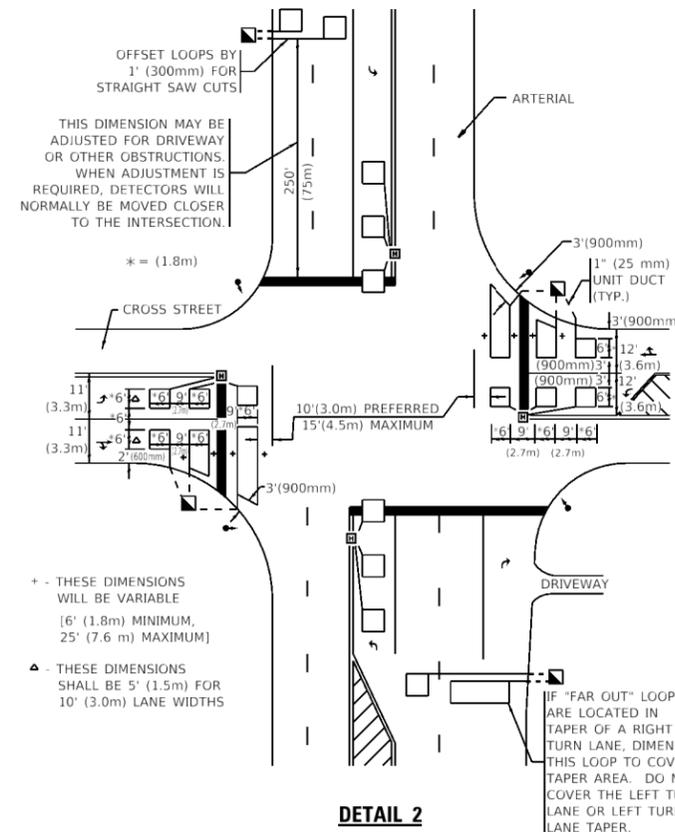
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1

N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2

N.T.S.

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PLOT DATE = 6/19/2020	DATE -	REVISION -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(44X-R6530B)RS	LAKE	50	50
TS-07			CONTRACT NO. 60V06	
ILLINOIS FED. AID PROJECT				