

# STATE OF ILLINOIS

## DEPARTMENT OF TRANSPORTATION

| FAP<br>ROUTE                | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
|-----------------------------|----------|-----------|-----------------|--------------|
| 313                         | (8R)RS-2 | HENDERSON | 34              | 1            |
| ILLINOIS CONTRACT NO. 68F10 |          |           |                 |              |

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2. GENERAL NOTES & HMA MIXTURE REQUIREMENTS
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- 7-11. SCHEDULE OF QUANTITIES
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- 13-18. TYPICAL SECTIONS /CONSTRUCTION SEQUENCE
- 19-23. STAGING DETAILS
- 24-25. GENERAL PLAN & DETAIL
26. PAVEMENT MARKING DETAIL
- 27-34. CADD STANDARDS

### HIGHWAY STANDARDS

|           |           |
|-----------|-----------|
| 630001-12 | 701422-10 |
| 630301-09 | 701426-09 |
| 631031-15 | 701428-01 |
| 642001-02 | 701456-05 |
| 701011-04 | 701901-08 |
| 701201-05 | 725001-01 |
| 701306-04 | 780001-05 |
| 701311-03 | 781001-04 |
| 701411-09 | 782006    |
| 701421-08 |           |

### DESIGN DESIGNATION (US34)

#### OTHER PRINCIPAL ARTERIAL

2017 ADT:

US 34 = 11,900 (MU=1,400)

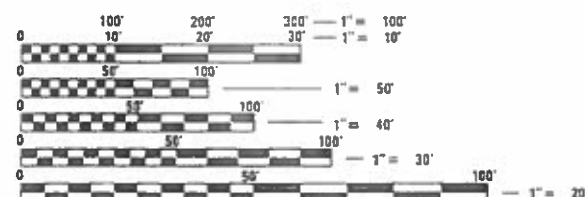
CHINN AVE. = 325 (MU=15)

RAMP "A" = 100 (MU=6)

RAMP "B" = 275 (MU=3)

RAMP "C" = 100 (MU=10)

RAMP "D" = UNKNOWN



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT ENGINEER: RICH DOTSON (309)671-3455  
PROJECT MANAGER: CLIFF OLTMAN (309)671-3471

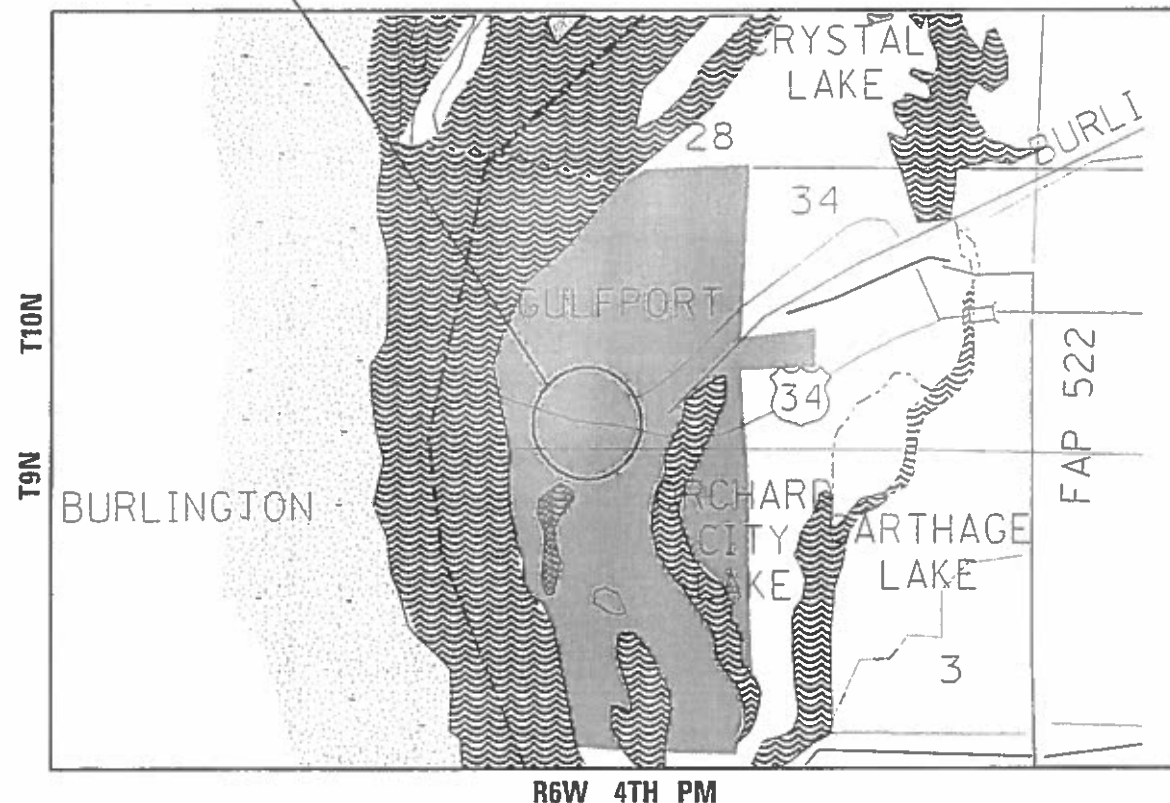
CATALOG NO. 035764-00D  
CONTRACT NO. 68F10

# PROPOSED HIGHWAY PLANS

FAP ROUTE 313 (US 34)  
SECTION (8R)RS-2  
PROJECT NHPP-STP-EUQD(541)  
3P  
HENDERSON COUNTY

C-94-062-19

PROJECT LOCATION  
US34 /CHINN AVE.  
INTERCHANGE



GROSS LENGTH = 5,861.6 FT. = 1.1 MILE (US34=1160', RAMP "A"=386.9', RAMP "B"=455.6',  
RAMP "C"=376.7', RAMP "D"=1015.1', RDWY "E"=602.8', RDWY "F"=520.53', CHINN AVE.=1344')  
NET LENGTH = 1160 FT. (US34) = 0.22 MILE

D-94-044-19



### PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF THE FOLLOWING IMPROVEMENTS AT THE INTERCHANGE OF US34 WITH CHINN AVE/2CND ST. NEAR GULFPORT, (INCLUDING THE RAMPS AND CHINN AVE): HMA SURFACE REMOVAL, PARTIAL DEPTH PATCHING, CENTERLINE JOINT REPAIR, PLACEMENT OF LEVELING BINDER AND SURFACE, SHOULDER REPAIR, GUARDRAIL, AND PAVEMENT MARKINGS.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 31 2019  
Rensel A. Barnett RSO  
REGIONAL ENGINEER  
June 14 2019  
EA EK  
ENGINEER OF DESIGN AND ENVIRONMENT  
June 14 2019  
Paul P. Chaf  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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AVAILABILITY OF ELECTRONIC FILES

MicroStation and GEOPAK files of this project will be made available to the Contractor after contract award. If there is a conflict between the electronic files and the printed contract plans and documents, the printed contract plans and documents shall take precedence over the electronic files. The Contractor shall accept all risk associated with using the electronic files and shall hold the Department harmless for any errors or omissions in the electronic files and the data contained therein. Errors or delays resulting from the use of the electronic files by the Contractor shall not result in an extension of time for any interim or final completion date or shall not be considered cause for additional compensation. The Contractor shall not use, share, or distribute these electronic files except for the purpose of constructing this contract. Any claims by third parties due to use or errors shall be the responsibility of the Contractor. The Contractor shall include this disclaimer with the transfer of these electronic files to any other parties and shall include appropriate language binding them to similar responsibilities.

COMMITMENTS

There are no commitments for this contract

PROPERTY OWNER ACCESS REQUIREMENTS

Access must be maintained to all existing properties during construction per Article 107.09 unless arrangements are made in writing by the Contractor with the property owners with a copy to the Engineer for short-term closures.

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20 mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

- Location:
- 2, 3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
  - Multi-Lane Divided Roadways – outside edge of pavement in both directions
  - Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format “XXX (XX + X00)”, where X represents the pavement station

This work will not be paid for separately, but will be included in the cost of the associated pavement and/or overlay pay items.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

| Mixture Use(s):                          | Surface Course 1.5" | Level Binder 1"  | Shoulders (Surface Lift) | Incidental Surface (if needed) | Shoulder/Gore Repairs (3", 2.5" lifts) | Partial Depth Patch (3"), Joint Repair |
|--|---------------------|------------------|--------------------------|--------------------------------|--|--|
| AC/PG:                                   | SBS or SBR 76-28    | SBS or SBR 76-22 | PG 64-22                 | PG 64-22                       | PG 64-22                               | PG 64-22                               |
| Design Air Voids:                        | 4.0% @ N=70         | 4.0% @ N=50      | 4.0% @ N=50              | 4.0% @ N=50                    | 4.0% @ N=50                            | 4.0% @ N=50                            |
| Mixture Composition: (Mixture Gradation) | IL 9.5              | IL 4.75          | IL 9.5FG                 | IL 9.5                         | IL 9.5                                 | IL 9.5                                 |
| Friction Aggregate:                      | Mix D               | N.A.             | Mix C                    | Mix C                          | Mix C                                  | Mix C                                  |
| Quality Management Program:              | QCQA                | QCQA             | QCQA                     | QCQA                           | QCQA                                   | QCQA                                   |

Note: 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more thn 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.  
2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y./in., unless otherwise noted.  
3) Sublot size for PFP mix will be 1000 tons, unless otherwise agreed to by the Engineer and the paving contractor.

|  |                               |            |           |   |   |                    |                    |          |                           |              |           |
|--|-------------------------------|------------|-----------|---|---|--------------------|--------------------|----------|---------------------------|--------------|-----------|
|  | USER NAME = oltmancr          | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | GENERAL NOTES<br>HMA MIXTURE REQUIREMENTS |                    | F.A. RTE.          | SECTION  | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|  |                               | DRAWN -    | REVISED - |   |   |                    | 313                | (8R)RS-2 | HENDERSON                 | 34           | 2         |
|  | PLOT SCALE = 100.0000 ' / in. | CHECKED -  | REVISED - |   |   |                    | CONTRACT NO. 68F10 |          |                           |              |           |
|  | PLOT DATE = 5/31/2019         | DATE -     | REVISED - |   | SCALE:                                    | SHEET OF 34 SHEETS | STA.               | TO STA.  | ILLINOIS FED. AID PROJECT |              |           |

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| CODE NO. | ITEM   | UNIT   | TOTAL QUANTITY | CONSTRUCTION CODE |           |
|----------|--|--|----------------|-------------------|-----------|
|          |  |  |                | NHPP              | STP       |
|          |  |  |                | 80% FED           | 80% FED   |
|          |  |  |                | 20% STATE         | 20% STATE |
|          |  |  |                | ROADWAY           | ROADWAY   |
|          |  |  |                | 0005              | 0005      |
| 25000750 | MOWING   | ACRE   | 8              | 6                 | 2         |
| 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON  | 1196.5         | 942               | 254.5     |
| 40600982 | HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT             | SQ YD  | 584.2          | 584.2             |           |
| 40603415 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5FG, N50              | TON  | 733.7          | 557.6             | 176.1     |
| 40603540 | POLYMERIZED HOT - MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON  | 2062.5         | 1553.6            | 508.9     |
| 44000159 | HOT - MIX ASPHALT SURFACE REMOVAL, 2 1/2"                  | SQ YD  | 13607.8        | 13607.8           |           |
| 48101200 | AGGREGATE SHOULDERS, TYPE B                                | TON  | 89.6           |                   | 89.6      |
| 48203009 | HOT - MIX ASPHALT SHOULDERS, 3"                            | SQ YD  | 1793.8         | 1793.8            |           |
| 48203019 | HOT - MIX ASPHALT SHOULDERS, 5 1/2"                        | SQ YD  | 1254.5         | 1254.5            |           |
| *        | 63000001   | STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS  | FOOT           | 12.5              | 12.5      |
| *        | 63100085   | TRAFFIC BARRIER TERMINAL, TYPE 6                   | EACH           | 1                 | 1         |
| *        | 63100167   | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH           | 1                 | 1         |
|          | 63200310   | GUARDRAIL REMOVAL                                  | FOOT           | 45                | 45        |
|          | 64200116   | SHOULDER RUMBLE STRIPS, 16 INCH                    | FOOT           | 786               | 786       |

\*= SPECIALTY ITEM

|  |                              |            |           |   |                       |                           |          |           |              |           |
|--|------------------------------|------------|-----------|---|-----------------------|---------------------------|----------|-----------|--------------|-----------|
|  | USER NAME : oltmanor         | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | F.A. RTE.                 | SECTION  | COUNTY    | TOTAL SHEETS | SHEET NO. |
|  |                              | DRAWN -    | REVISED - |   |                       | 313                       | (8R)R5-2 | HENDERSON | 34           | 3         |
|  | PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |   |                       | CONTRACT NO. 68F10        |          |           |              |           |
|  | PLOT DATE : 6/3/2019         | DATE -     | REVISED - |   |                       | ILLINOIS FED. AID PROJECT |          |           |              |           |

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| CODE NO. | ITEM   | UNIT   | TOTAL QUANTITY | CONSTRUCTION CODE |           |
|----------|--|--------|----------------|-------------------|-----------|
|          |  |        |                | NHPP              | STP       |
|          |  |        |                | 80% FED           | 80% FED   |
|          |  |        |                | 20% STATE         | 20% STATE |
|          |  |        |                | ROADWAY           | ROADWAY   |
|          |  |        |                | 0005              | 0005      |
|          |  |        |                |                   |           |
| 67100100 | MOBILIZATION   | L SUM  | 1              | 0.75              | 0.25      |
|          |  |        |                |                   |           |
| 70107025 | CHANGEABLE MESSAGE SIGN                                | CAL DA | 120            | 120               |           |
|          |  |        |                |                   |           |
| 70300100 | SHORT-TERM PAVEMENT MARKING                            | FOOT   | 15330          | 15330             |           |
|          |  |        |                |                   |           |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL                    | SQ FT  | 5109.5         | 5109.5            |           |
|          |  |        |                |                   |           |
| *        | 72501000 TERMINAL MARKER - DIRECT APPLIED              | EACH   | 1              | 1                 |           |
|          |  |        |                |                   |           |
| *        | 78009004 MODIFIED URETHANE PAVEMENT MARKING - LINE 4"  | FOOT   | 17433.4        | 12762.4           | 4671      |
|          |  |        |                |                   |           |
| *        | 78009006 MODIFIED URETHANE PAVEMENT MARKING - LINE 6"  | FOOT   | 600            | 600               |           |
|          |  |        |                |                   |           |
| *        | 78009008 MODIFIED URETHANE PAVEMENT MARKING - LINE 8"  | FOOT   | 1914.8         | 1914.8            |           |
|          |  |        |                |                   |           |
| *        | 78009012 MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT   | 425            | 425               |           |
|          |  |        |                |                   |           |
| *        | 78009024 MODIFIED URETHANE PAVEMENT MARKING - LINE 24" | FOOT   | 43             | 43                |           |
|          |  |        |                |                   |           |
| *        | 78100100 RAISED REFLECTIVE PAVEMENT MARKER             | EACH   | 46             | 46                |           |
|          |  |        |                |                   |           |
| *        | 78200005 GUARDRAIL REFLECTORS, TYPE A                  | EACH   | 2              | 2                 |           |
|          |  |        |                |                   |           |
| *        | 78200011 BARRIER WALL RELFLECTORS, TYPE C              | EACH   | 12             | 12                |           |
|          |  |        |                |                   |           |
|          | 78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL     | EACH   | 44             | 44                |           |
|          |  |        |                |                   |           |

\*= SPECIALTY ITEM

|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME : oltmanor         | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 6/3/2019         | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

| F.A. RTE. | SECTION  | COUNTY             | TOTAL SHEETS | SHEET NO. |
|-----------|----------|--------------------|--------------|-----------|
| 313       | (8R)RS-2 | HENDERSON          | 34           | 4         |
|           |          | CONTRACT NO. 68F10 |              |           |
| ILLINOIS  |          | FED. AID PROJECT   |              |           |



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| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE |           |
|------------|---|-------|----------------|-------------------|-----------|
|            |   |       |                | NHPP              | STP       |
|            |   |       |                | 80% FED           | 80% FED   |
|            |   |       |                | 20% STATE         | 20% STATE |
|            |   |       |                | ROADWAY           | ROADWAY   |
|            |   |       |                | 0005              | 0005      |
|            |   |       |                |                   |           |
| X0300004   | BITUMINOUS MATERIALS (TACK COAT), SPECIAL       | POUND | 37586.6        | 30288.7           | 7297.9    |
|            |   |       |                |                   |           |
| X0556100   | PARTIAL DEPTH PATCHING (SPECIAL)                | SQ YD | 1399.9         | 1399.9            |           |
|            |   |       |                |                   |           |
| X0800001   | SHOULDER REMOVAL (SPECIAL)                      | SQ YD | 3048.5         | 3048.5            |           |
|            |   |       |                |                   |           |
| X4400196   | HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL      | SQ YD | 755.6          | 755.6             |           |
|            |   |       |                |                   |           |
| X4401198   | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ YD | 11113.4        | 5521.2            | 5592.2    |
|            |   |       |                |                   |           |
| X4405030   | LONGITUDINAL PARTIAL DEPTH REMOVAL 3"           | FOOT  | 2829.4         | 2829.4            |           |
|            |   |       |                |                   |           |
| X4420900   | LONGITUDINAL PARTIAL DEPTH PATCHING             | TON   | 105.6          | 105.6             |           |
|            |   |       |                |                   |           |
| X4421000   | PARTIAL DEPTH PATCHING                          | TON   | 374.9          | 374.9             |           |
|            |   |       |                |                   |           |
| X7010216   | TRAFFIC CONTROL AND PROTECTION (SPECIAL)        | L SUM | 1              | 0.75              | 0.25      |
|            |   |       |                |                   |           |
| * X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5"       | FOOT  | 17433.4        | 12762.4           | 4671      |
|            |   |       |                |                   |           |
| * X7830074 | GROOVING FOR RECESSED PAVEMENT MARKING 7"       | FOOT  | 600            | 600               |           |
|            |   |       |                |                   |           |
| * X7830076 | GROOVING FOR RECESSED PAVEMENT MARKING 9"       | FOOT  | 1914.8         | 1914.8            |           |
|            |   |       |                |                   |           |
| * X7830078 | GROOVING FOR RECESSED PAVEMENT MARKING 13"      | FOOT  | 425            | 425               |           |
|            |   |       |                |                   |           |
| * X7830090 | GROOVING FOR RECESSED PAVEMENT MARKING 25"      | FOOT  | 43             | 43                |           |
|            |   |       |                |                   |           |

|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME : oltmanor         | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
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DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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|           |          | CONTRACT NO. 68F10 |              |           |
| ILLINOIS  |          | FED. AID PROJECT   |              |           |

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|                               | DRAWN -    | REVISED - |
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| PLOT DATE = 6/3/2019          | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

## SUMMARY OF QUANTITIES

| F.A.<br>RTE. | SECTION  | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO. |
|--------------|----------|--------------------|-----------------|--------------|
| 313          | (8R)RS-2 | HENDERSON          | 34              | 6            |
|              |          | CONTRACT NO. 68F10 |                 |              |
| ILLINOIS     |          | FED. AID PROJECT   |                 |              |

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| PATCHING TABLE       |                |                    |               |               |                                     |                        |        | NOTES   |  |
|----------------------|----------------|--------------------|---------------|---------------|-------------------------------------|------------------------|--------|---|--|
| LOCATION             | WIDTH (LANE)   | LENGTH (ALONG STA) | AREA<br>SQ FT | AREA<br>SQ YD | X0556100                            | X4421000               |        |   |  |
|                      |                |                    |               |               | PARTIAL DEPTH<br>PATCHING (SPECIAL) | PARTIAL DEPTH PATCHING |        |   |  |
|                      |                |                    |               |               |                                     | 3"                     | 5 1/2" |   |  |
|                      | FT             | FT                 |               |               | SQ YD                               | TON                    | TON    |   |  |
| WESTBOUND            |                |                    |               |               |                                     |                        |        | ADJACENT TO BRIDGE APPROACH PAVEMENT: DRIVING LANE = 12', PASSING LANE = 15', AND RAMP A TURN LANE = 5.4' TO 5.8' |  |
| 21+38.98 TO 21+48.98 | 32.6           | 10.0               | 326.0         | 36.2          | 36.2                                | 6.1                    |        |   |  |
| 20+50                | 10.0           | 23.5               | 235           | 26.1          | 26.1                                | 4.4                    |        |   | CENTERLINE TO EDGE OF TURN LANE  |
| 18+00                | 36.0           | 6.0                | 216           | 24.0          | 24.0                                | 4.0                    |        |   | ALL 3 LANES  |
| 17+00                | 12.0           | 15.0               | 180           | 20.0          | 20.0                                | 3.4                    |        |   | PASSING LANE   |
| 16+75                | 12.0           | 6.0                | 72            | 8.0           | 8.0                                 | 1.3                    |        |   | DRIVING LANE   |
| 16+25 TO16+40        | 6.0            | 15.0               | 90            | 10.0          | 10.0                                | 1.7                    |        |   | 1/2 PASSING LANE   |
| 15+90                | 24.0           | 6.0                | 144           | 16.0          | 16.0                                | 2.7                    |        |   | DRIVING AND PASSING LANE   |
| 15+20                | 12.0           | 12.0               | 144           | 16.0          | 16.0                                | 2.7                    |        |   | DRIVING LANE   |
| 13+75 TO 14+60       | 6.0            | 85.0               | 510           | 56.7          | 56.7                                | 9.5                    |        |   | 1/2 PASSING LANE   |
| 12+25                | 12.0           | 10.0               | 120           | 13.3          | 13.3                                | 2.2                    |        |   | DRIVING LANE   |
| 11+40                | 12.0           | 6.0                | 72            | 8.0           | 8.0                                 | 1.3                    |        |   | DRIVING LANE   |
| 11+00                | 12.0           | 3.0                | 36            | 4.0           | 4.0                                 | 0.7                    |        |   | DRIVING LANE   |
| 10+70                | 12.0           | 3.0                | 36            | 4.0           | 4.0                                 | 0.7                    |        |   | DRIVING LANE   |
| 9+82.75 TO 9+92.75   | 38.5           | 10.0               | 385.0         | 42.8          | 42.8                                | 7.2                    |        |   | ADJACENT TO BRIDGE APPROACH PAVEMENT: DRIVING LANE = 10.5', PASSING LANE = 13.5', AND RAMP D TURN LANE = 14.5'     |
| 9+82.75 TO 13+38.16  | 5.11 TO 17.0   | 355.41             | 3929.1        | 436.6         | 436.6                               |                        | 134.5  |   | GORE OF RAMP B: PRESTAGE I BUILD UP, WIDTHS INCLUDE 1.5' ON EACH SIDE OF JOINTS FOR RAMP B AND WB DRIVING LANE     |
| EAST BOUND           |                |                    |               |               |                                     |                        |        |   |  |
| 9+82.75 TO 12+15.09  | 12.88 TO 30.62 | 232.34             | 5053.4        | 561.5         | 561.5                               |                        | 172.9  |   | GORE OF RAMP D: PRESTAGE I BUILD UP, WIDTHS INCLUDE 1.5' ON EACH SIDE OF JOINTS FOR RAMP D AND EB DRIVING LANE     |
| 9+82.75 TO 9+92.75   | 38.5           | 10.0               | 385.0         | 42.8          | 42.8                                | 7.2                    |        |   | ADJACENT TO BRIDGE APPROACH PAVEMENT: DRIVING LANE = 10.5', PASSING LANE = 13.5', AND RAMP B ENTRANCE LANE = 14.5' |
| 13+50                | 12.0           | 12.0               | 144.0         | 16.0          | 16.0                                | 2.7                    |        |   | DRIVING LANE   |
| 15+00                | 6.0            | 18.0               | 108.0         | 12.0          | 12.0                                | 2.0                    |        | 1/2 PASSING LANE  |  |
| 18+00                | 12.0           | 12.0               | 144.0         | 16.0          | 16.0                                | 2.7                    |        | PASSING LANE  |  |
| 21+38.98 TO 21+48.98 | 27.0           | 10.0               | 270.0         | 30.0          | 30.0                                | 5.0                    |        | ADJACENT TO BRIDGE APPROACH PAVEMENT: DRIVING LANE = 12', PASSING LANE = 15'                                      |  |
| TOTAL                |                |                    |               |               | 1399.9                              | 67.5                   | 307.4  |   |  |
| GRAND TOTAL          |                |                    |               |               | 1399.9                              | 374.9                  |        |   |  |

| LONGITUDINAL JOINT REPAIR |          |    |          |       |   |       |   |                                 |  |
|---------------------------|----------|----|----------|-------|---|-------|---|---------------------------------|--|
| LOCATION                  |          |    |          | WIDTH | X4405030 -<br>LONGITUDINAL<br>PARTIAL DEPTH<br>REM., 3" | AREA  | X4420900 - LONGITUDINAL<br>PARTIAL DEPTH PATCHING | NOTES                           |  |
|                           |          |    |          |       |   |       |   |                                 |  |
| WEST BOUND                |          |    |          |       |   |       |   |                                 |  |
| STA                       | 9+92.75  | TO | 21+38.98 | 2.0   | 1146.2  | 254.7 | 42.8  | CENTERLINE                      |  |
| STA                       | 16+77.00 | TO | 21+38.98 | 2.0   | 462.0   | 102.7 | 17.2  | LT EDGE OF TURN LANE            |  |
| EAST BOUND                |          |    |          |       |   |       |   |                                 |  |
| STA                       | 9+92.75  | TO | 21+38.98 | 2.0   | 1146.2  | 254.7 | 42.8  | CENTERLINE                      |  |
| STA                       | 16+21.00 | TO | 16+96.00 | 2.0   | 75.0  | 16.7  | 2.8   | RT EDGE OF DRIVING LANE/ RAMP C |  |
| TOTALS =                  |          |    |          |       | 2829.4  |       | 105.6   |                                 |  |

|  |                               |            |           |   |  |  |           |                 |                    |              |                           |
|--|-------------------------------|------------|-----------|---|--|--|-----------|-----------------|--------------------|--------------|---------------------------|
|  | USER NAME = oltmancr          | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |  |  | F.A. RTE. | SECTION         | COUNTY             | TOTAL SHEETS | SHEET NO.                 |
|  |                               | DRAWN -    | REVISED - |   |  |  | 313       | (8R)RS-2        | HENDERSON          | 34           | 7                         |
|  | PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |   |  |  |           |                 | CONTRACT NO. 68F10 |              |                           |
|  | PLOT DATE = 5/31/2019         | DATE -     | REVISED - |   |  |  | SCALE:    | SHEET OF SHEETS | STA.               | TO STA.      | ILLINOIS FED. AID PROJECT |

MODEL Default  
FILE Name: pavplanroom.dwt\\links\gov\PIWDOT\Documents\DOT Office\District 6 alius\acullosortgulfport\Ill4.dgn

| PAVEMENT TABLE |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
|----------------|-----------|----|-----------|---------|---------------|-----------------------------------|--------------------------------|---|--|--------------------------|---|------------------------|--|--------|------------------------|--|----------------------|-----------------------------|
| LOCATION       |           |    | LENGTH    | WIDTH   | AREA          | 40600982                          | 44000159                       | X4400196  | X4401198                                   | Z0004542                 | 40600827  |                        | 40603540                                     |        |                        | X0300004                                     |                      | Z0034105                    |
|                |           |    |           |         |               | HOT-MIX ASPHALT<br>REM BUTT JOINT | HMA SURFACE<br>REMOVAL<br>1/2" | HMA SURFACE<br>REMOVAL, SPECIAL<br>(JOINT TRIM) | HMA SURFACE<br>REMOVAL, VAR.<br>DEPTH (3") | HMA REMOVAL<br>(SPECIAL) | POLYMERIZED LEVELING BINDER<br>COURSE (MACHINE METHOD), IL-4.75,<br>N50 |                        | POLYMERIZED HMA SURFACE COURSE, MIX "D", N70 |        |                        | BITUMINOUS MATERIALS (TACK COAT),<br>SPECIAL |                      | MATERIAL TRANSFER<br>DEVICE |
|                |           |    |           |         |               |                                   |                                |   |  |                          | 1"  | QTY. FOR JOINT<br>TRIM | 1 1/2"                                       | 2"     | QTY. FOR JOINT<br>TRIM | MILLED<br>0.08 LB/ SQ FT                     | FOG<br>0.08 LB/SQ FT |                             |
|                |           |    | FT        | FT      | SQ YD         | SQ YD                             | SQ YD                          | SQ YD   | SQ YD                                      | SQ YD                    | TON   | TON                    | TON  | TON    | TON                    | POUND  | POUND                | TON                         |
| WESTBOUND      |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 9+82.75   | TO | 9+92.75   | 10.00   | 45.1 - 45.3   | 50.2                              | 48.6                           |   | 2.2  | 1.7                      | 2.8   | 0.1                    | 4.2  |        | 0.2                    | 37.8   | 37.8                 | 7.3                         |
| STA            | 9+92.75   | TO | 10+76.94  | 84.19   | 45.3 - 47.0   | 431.7                             |                                | 417.7   | 18.7                                       | 14.0                     | 24.2  | 1.0                    | 36.3   |        | 1.6                    | 324.3  | 324.3                | 63.1                        |
| STA            | 10+76.94  | TO | 12+75.00  | 198.06  | 31.0 - 35.0   | 726.22                            |                                | 693.2   | 22.0                                       | 33.0                     | 40.7  | 1.2                    | 61.0   |        | 1.8                    | 538.7  | 538.7                | 104.8                       |
| STA            | 12+75.00  | TO | 16+76.94  | 401.94  | 27.0          | 1205.82                           |                                | 1138.8  | 44.7                                       | 67.0                     | 67.5  | 2.5                    | 101.3  |        | 3.8                    | 900.3  | 900.3                | 175.1                       |
| STA            | 16+76.94  | TO | 17+33.95  | 57.01   | 53.0 - 39     | 291.4                             |                                | 281.9   | 9.5  | 9.5                      | 16.3  | 0.5                    | 24.5   |        | 0.8                    | 216.6  | 216.6                | 42.1                        |
| STA            | 17+33.95  | TO | 19+84.39  | 250.44  | 39.0          | 1085.2                            |                                | 1043.5  | 41.7                                       | 41.7                     | 60.8  | 2.3                    | 91.2   |        | 3.5                    | 811.4  | 811.4                | 157.8                       |
| STA            | 19+84.39  | TO | 21+38.98  | 154.59  | 39 - 32.8     | 616.6                             |                                | 590.9   | 25.8                                       | 25.8                     | 34.5  | 1.4                    | 51.8   |        | 2.2                    | 462.5  | 462.5                | 89.9                        |
| STA            | 21+38.98  | TO | 21+48.98  | 10.00   | 32.4 - 32.8   | 36.2                              | 34.6                           |   | 1.7  | 1.7                      | 2.0   | 0.1                    | 3.0  |        | 0.1                    | 27.3   | 27.3                 | 5.3                         |
| EASTBOUND      |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 9+82.75   | TO | 9+92.75   | 10.00   | 52.88 - 53.58 | 59.1                              | 57.5                           |   | 2.2  | 1.7                      | 3.3   | 0.1                    | 5.0  |        | 0.2                    | 44.2   | 44.2                 | 8.6                         |
| STA            | 9+92.75   | TO | 10+96.19  | 103.44  | 53.58 - 63.0  | 671.7                             |                                | 654.4   | 23.0                                       | 17.2                     | 37.6  | 1.3                    | 56.4   |        | 1.9                    | 500.2  | 500.2                | 97.3                        |
| STA            | 10+96.19  | TO | 21+38.98  | 1042.79 | 27.0          | 3128.4                            | 2954.6                         |   | 115.9                                      | 173.8                    | 175.2   | 6.5                    | 262.8  |        | 9.7                    | 2335.8                                       | 2335.8               | 454.2                       |
| STA            | 21+38.98  | TO | 21+48.98  | 10.00   | 27.0          | 30.0                              | 28.3                           |   | 1.1  | 1.7                      | 1.7   | 0.1                    | 2.5  |        | 0.1                    | 22.4   | 22.4                 | 4.4                         |
| RAMP A         |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 6+02.50   | TO | 7+49.14   | 146.64  | 16.0          | 260.7                             |                                | 260.7   | 16.3                                       |                          | 14.6  | 0.9                    | 12.3   |        | 1.4                    | 199.4  | 199.4                | 29.2                        |
| STA            | 7+49.14   | TO | 8+30.00   | 80.86   | 16.0 TO 12.0  | 125.8                             |                                | 125.8   | 9.0  |                          | 7.0   | 0.5                    | 6.8  |        | 0.8                    | 97.0   | 97.0                 | 15.1                        |
| STA            | 8+30.00   | TO | 8+82.00   | 52.00   | 12.0          | 69.3                              |                                | 69.3  | 5.8  |                          | 3.9   | 0.3                    | 4.4  |        | 0.5                    | 54.1   | 54.1                 | 9.1                         |
| STA            | 8+82.00   | TO | 9+16.89   | 34.89   | 18.0 TO 13.0  | 60.1                              |                                | 60.1  | 3.9  |                          | 3.4   | 0.2                    | 2.9  |        | 0.3                    | 46.1   | 46.1                 | 6.8                         |
| STA            | 9+16.89   | TO | 9+36.89   | 20.00   | 13.0 - 12.0   | 27.8                              |                                |   | 2.2  | 27.8                     | 1.6   | 0.1                    |  | 2.7    | 0.2                    | 21.6   | 21.6                 | 4.6                         |
| RAMP B         |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 0+00.00   | TO | 0+20.00   | 20.00   | 12.0 - 12.4   | 27.1                              |                                |   | 2.2  | 27.1                     | 1.5   | 0.1                    |  | 2.7    | 0.2                    | 21.1   | 21.1                 | 4.5                         |
| STA            | 0+20.00   | TO | 1+99.22   | 179.22  | 12.4 - 16.0   | 282.8                             |                                | 282.8   | 17.9                                       |                          | 15.8  | 1.0                    | 23.8   |        | 1.5                    | 216.5  | 216.5                | 42.1                        |
| STA            | 1+99.22   | TO | 6+55.56   | 456.34  | 16.0          | 811.3                             |                                | 811.3   | 50.7                                       |                          | 45.4  | 2.8                    | 68.1   |        | 4.3                    | 620.6  | 620.6                | 120.7                       |
| RAMP C         |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 0+00.00   | TO | 0+37.91   | 37.91   | 12.0          | 50.5                              |                                | 50.5  | 4.2  |                          | 2.8   | 0.2                    | 4.2  |        | 0.4                    | 39.4   | 39.4                 | 7.7                         |
| STA            | 0+37.91   | TO | 0+95.00   | 57.09   | 12.0 - 16.0   | 88.8                              |                                | 88.8  | 6.3  |                          | 5.0   | 0.4                    | 7.5  |        | 0.5                    | 68.5   | 68.5                 | 13.3                        |
| STA            | 0+95.00   | TO | 3+02.52   | 207.52  | 16.0          | 368.9                             |                                | 368.9   | 23.1                                       |                          | 20.7  | 1.3                    | 31.0   |        | 1.9                    | 282.2  | 282.2                | 54.9                        |
| STA            | 3+02.52   | TO | 3+60.00   | 57.48   | 16.0 - 75.0   | 183.4                             |                                | 183.4   | 6.4  |                          | 10.3  | 0.4                    | 15.4   |        | 0.5                    | 136.7  | 136.7                | 26.6                        |
| RAMP D         |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 3+23.26   | TO | 9+14.34   | 591.08  | 16.0          | 1058.6                            |                                | 1058.6  | 65.7                                       |                          | 59.3  | 3.7                    | 88.9   |        | 5.5                    | 809.5  | 809.5                | 157.4                       |
| STA            | 9+14.34   | TO | 10+21.50  | 107.16  | 12.0 - 16.0   | 166.7                             |                                | 166.7   | 11.9                                       |                          | 9.3   | 0.7                    | 14.0   |        | 1.0                    | 128.6  | 128.6                | 25.0                        |
| STA            | 10+21.50  | TO | 10+80.00  | 58.5    | 24.0          | 156.0                             |                                | 156.0   | 9.8  |                          | 8.7   | 0.5                    | 13.1   |        | 0.8                    | 119.3  | 119.3                | 23.2                        |
| STA            | 10+80.00  | TO | 11+00.00  | 20      | 24.0          | 53.3                              |                                |   | 3.3  | 53.3                     | 3.0   | 0.2                    |  | 5.2    | 0.3                    | 40.8   | 40.8                 | 8.7                         |
| STA            | 11+00.00  | TO | 12+25.00  | 125     | 24.0          | 333.3                             |                                |   | 20.8                                       | 333.3                    |   |                        |  | 37.3   | 2.3                    | 255.0  | 255.0                | 39.7                        |
| ROADWAY E      |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 2+33.06   | TO | 6+00.00   | 366.94  | 24.0          | 978.5                             |                                |   | 61.2                                       | 978.5                    | 54.8  | 3.4                    |  | 109.6  | 6.8                    | 748.6  | 748.6                | 174.7                       |
| STA            | 6+00.00   | TO | 7+50.00   | 150     | 24.0 - 33.0   | 475.0                             |                                |   | 25.0                                       | 475.0                    | 26.6  | 1.4                    |  | 53.2   | 2.8                    | 360.0  | 360.0                | 84.0                        |
| STA            | 7+50.00   | TO | 7+80.00   | 30      | 33.0 - 42.0   | 125.0                             |                                |   | 5.0  | 125.0                    | 7.0   | 0.3                    |  | 14.0   | 0.6                    | 93.6   | 93.6                 | 21.8                        |
| STA            | 7+80.00   | TO | 8+00.00   | 20      | 42.0 - 55.0   | 107.8                             |                                |   | 3.3  | 107.8                    | 6.0   | 0.2                    |  | 12.1   | 0.4                    | 80.0   | 80.0                 | 18.7                        |
| STA            | 8+00.00   | TO | 8+15.00   | 15      | 55.0 - 67.0   | 23.3                              |                                |   | 2.5  | 23.3                     | 1.3   | 0.1                    |  | 2.6    | 0.3                    | 18.6   | 18.6                 | 4.3                         |
| STA            | 8+15.00   | TO | 8+52.00   | 37      | 67.0 - 130    | 404.9                             |                                |   | 6.2  | 404.9                    | 22.7  | 0.3                    |  | 45.4   | 0.7                    | 296.0  | 296.0                | 69.1                        |
| STA            | 8+52.00   | TO | 8+62.00   | 10      | 130 - 175     |                                   | 338.9                          |   | 1.7  |                          | 19.0  | 0.1                    |  | 38.0   | 0.2                    | 245.2  | 245.2                | 57.2                        |
| ROADWAY F      |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 6+64.17   | TO | 10+70.00  | 405.83  | 24.0          | 1082.2                            |                                |   | 67.6                                       | 1082.2                   | 60.6  | 3.8                    |  | 121.2  | 7.6                    | 827.9  | 827.9                | 193.2                       |
| STA            | 10+70.00  | TO | 11+10.00  | 40      | 24.0 - 34.0   | 128.9                             |                                |   | 6.7  | 128.9                    | 7.2   | 0.4                    |  | 14.4   | 0.7                    | 97.6   | 97.6                 | 22.8                        |
| STA            | 11+10.00  | TO | 11+40.00  | 30      | 34.0 - 55.0   | 148.3                             |                                |   | 5.0  | 148.3                    | 8.3   | 0.3                    |  | 16.6   | 0.6                    | 110.4  | 110.4                | 25.8                        |
| STA            | 11+40.00  | TO | 11+51.70  | 11.7    | 55.0 - 58.5   | 73.8                              |                                |   | 2.0  | 73.8                     | 4.1   | 0.1                    |  | 8.3    | 0.2                    | 54.5   | 54.5                 | 12.7                        |
| STA            | 11+51.70  | TO |           | 10      | 58.5 - 79     |                                   | 76.4                           |   | 1.7  |                          | 4.3   | 0.1                    |  | 8.6    | 0.2                    | 56.2   | 56.2                 | 13.1                        |
| CHINN AVENUE   |           |    |           |         |               |                                   |                                |   |  |                          |   |                        |  |        |                        |  |                      |                             |
| STA            | 94+50.00  | TO | 95+83.00  | 133.00  | 46.0          | 596.0                             |                                |   | 596.0                                      |                          | 33.4  |                        |  | 66.8   |                        | 429.12                                       | 429.12               | 100.1                       |
| STA            | 95+83.00  | TO | 103+34.00 | 751.00  | 28.0          | 2336.4                            |                                |   |  | 2336.4                   | 130.8   |                        |  | 261.7  |                        | 1682.24                                      | 1682.24              | 392.5                       |
| STA            | 103+34.00 | TO | 105+00.00 | 166.00  | 46.0          | 709.1                             |                                |   | 709.1                                      |                          | 39.7  |                        |  | 79.4   |                        | 510.56                                       | 510.56               | 119.1                       |
| STA            | 105+00.00 | TO | 107+90.00 | 290     | 28.0          | 902.2                             |                                |   | 902.2                                      |                          | 50.5  |                        |  | 101.0  |                        | 649.6  | 649.6                | 151.6                       |
| TOTALS         |           |    |           |         |               | 584.2                             | 11457.9                        | 755.6   | 8533.1                                     | 388.7                    | 1155.3  | 41.1                   | 992.4  | 1000.7 | 69.4                   | 15638.1                                      | 15638.1              | 3259.0                      |
| GRAND TOTAL    |           |    |           |         |               | 584.2                             | 11457.9                        | 755.6   | 8533.1                                     | 388.7                    | 1196.5  |                        |  | 2062.5 |                        | 31276.3                                      |                      | 3259.0                      |

|                               |            |           |
|-------------------------------|------------|-----------|
| USER NAME = oltmancr          | DESIGNED - | REVISED - |
|                               | DRAWN -    | REVISED - |
| PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 5/31/2019         | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET OF SHEETS STA. TO STA.

|                    |          |           |                  |           |
|--------------------|----------|-----------|------------------|-----------|
| F.A. RTE.          | SECTION  | COUNTY    | TOTAL SHEETS     | SHEET NO. |
| 313                | (8R)RS-2 | HENDERSON | 34               | 8         |
| CONTRACT NO. 68F10 |          |           |                  |           |
|                    |          | ILLINOIS  | FED. AID PROJECT |           |

MODEL Default  
FILE Name: pavlabanrom.dallinks.gov\PIWDOT\Documents\DOT Office\Director - Projects\Squad 6 al\US34culportgulfportC114.dgn

| SHOULDER TABLE                         |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
|--|-----------|-------|-----------|-------|---------|--------------|-------|----------------------------------|-----------------------------|-----------------------------------|---|-----------------------------------|-------|-------------------|--|-------|--------------------------------|
| LOCATION                               |           |       |           |       | LENGTH  | WIDTH        | AREA  | X0800001                         | 48203019                    | 44000159                          | X4401198                                  | 40603415                          |       | 48203009          | X0300004                                     |       | 48101200                       |
|  |           |       |           |       |         |              |       | SHOULDER<br>REMOVAL<br>(SPECIAL) | HMA<br>SHOULDERS, 5<br>1/2" | HMA SURFACE<br>REMOVAL, 2<br>1/2" | HMA SURFACE<br>REMOVAL, VAR DEPTH<br>(3") | HMA SURFACE COURSE, IL-9.5FG, N50 |       | HMA SHOULDERS, 3" | BITUMINOUS MATERIALS (TACK COAT),<br>SPECIAL |       | AGGREGATE<br>SHOULDERS, TYPE B |
|  |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
|  |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| FT                                     | FT        | SQ YD | SQ YD     | SQ YD | SQ YD   | SQ YD        | SQ YD | 2 1/2"<br>TON                    | 3"<br>TON                   | SQ YD                             | 0.08 LB/SQ FT<br>POUND                    | 0.08 LB/ SQ FT<br>POUND           | TON   |                   |  |       |                                |
| EASTBOUND                              |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 10+96.79  | TO    | 12+15.09  | RT    | 118.30  | 18.0 - 27.62 | 299.8 |                                  |                             | 299.8                             |   | 42.0                              |       |                   | 215.9  |       |                                |
| STA                                    | 12+15.09  | TO    | 16+21.00  | RT    | 405.91  | 17.62 - 8.0  | 577.7 | 645.4                            | 645.4                       | 577.7                             |   | 80.9                              |       |                   | 880.7  | 232.3 |                                |
| STA                                    | 16+30.00  | TO    | 16+90.00  | RT    | 74.00   | 6.0          | 49.3  | 61.7                             | 61.7                        | 49.3                              |   | 6.9                               |       |                   | 79.9   | 22.2  |                                |
| STA                                    | 16+90.00  | TO    | 17+15.50  | RT    | 25.50   | 6.0 - 10.0   | 22.7  | 26.9                             | 26.9                        | 22.7                              |   | 3.2                               |       |                   | 35.7   | 9.7   |                                |
| STA                                    | 17+15.50  | TO    | 17+73.00  | RT    | 57.50   | 10.0         | 63.9  | 73.5                             | 73.5                        | 63.9                              |   | 8.9                               |       |                   | 98.9   | 26.5  |                                |
| WESTBOUND                              |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 9+82.75   | TO    | 10+76.96  | LT    | 94.21   | 6.0          | 62.8  | 62.8                             |                             |                                   |   | * 8.8                             |       | 62.8              | 45.2   | 22.6  |                                |
| STA                                    | 12+75.00  | TO    | 13+38.16  | LT    | 63.16   | 8.0 - 14.0   | 77.2  |                                  |                             | 77.2                              |   | 10.8                              |       |                   | 55.6   |       |                                |
| STA                                    | 13+38.16  | TO    | 16+45.00  | LT    | 306.84  | 10.0         | 340.9 | 392.1                            | 392.1                       | 392.1                             |   | 54.9                              |       |                   | 564.6  | 141.1 |                                |
| STA                                    | 17+33.95  | TO    | 20+50.00  | LT    | 316.05  | 6.0          | 210.7 | 210.7                            |                             |                                   |   | * 29.5                            |       | 210.7             | 75.9   | 75.9  |                                |
| RAMP A                                 |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 5+50.00   | TO    | 9+36.89   | RT    | 386.89  | 6.0          | 257.9 | 257.9                            |                             |                                   |   | * 36.1                            |       | 257.9             | 185.7  | 92.9  |                                |
| STA                                    | 6+25.00   | TO    | 8+50.00   | LT    | 225     | 4.0          | 100.0 |                                  |                             | 100.0                             |   | 14.0                              |       |                   | 72.0   |       |                                |
| GORE AT RAMP A (US 34 STATIONING)      |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 16+45.00  | TO    | 16+76.94  | LT    | 31.94   | 22.0 - 6.0   | 49.7  | 55.0                             | 55.0                        | 49.7                              |   | 7.0                               |       |                   | 75.4   | 19.8  |                                |
| RAMP B                                 |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 0+00.00   | TO    | 6+55.56   | RT    | 655.56  | 6.0          | 437.0 | 437.0                            |                             |                                   |   | * 61.2                            |       | 437.0             | 314.7  | 157.3 |                                |
| STA                                    | 0+57.00   | TO    | 4+03.00   | LT    | 346     | 4.0          | 190.4 |                                  |                             | 190.4                             |   | 26.7                              |       |                   | 137.1  |       |                                |
| RAMP C                                 |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 0+00.00   | TO    | 3+02.52   | RT    | 302.52  | 6.0          | 201.7 | 201.7                            |                             |                                   |   | * 28.2                            |       | 201.7             | 145.2  | 72.6  |                                |
| STA                                    | 1+32.00   | TO    | 3+23.00   | LT    | 191     | 4.0          | 84.9  |                                  |                             | 84.9                              |   | 11.9                              |       |                   | 8.6  |       |                                |
| STA                                    | 3+23.00   | TO    | 3+39.00   | LT    | 16      | 4.0 - 14.0   | 11.0  |                                  |                             | 11.0                              |   | 1.5                               |       |                   | 1.1  |       |                                |
| RAMP D                                 |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 1+97.00   | TO    | 2+26.00   | RT    | 29      | 4.5 - 6.0    | 16.9  | 16.9                             |                             |                                   |   | * 2.4                             |       | 16.9              | 12.18  | 6.1   |                                |
| STA                                    | 2+26.00   | TO    | 7+09.00   | RT    | 483     | 6.0          | 322.0 | 322.0                            |                             |                                   |   | * 45.1                            |       | 322.0             | 231.8  | 115.9 |                                |
| STA                                    | 7+09.00   | TO    | 10+21.50  | RT    | 312.5   | 6.0          | 208.3 | 208.3                            |                             |                                   |   | * 29.2                            |       | 208.3             |  |       |                                |
| STA                                    | 10+21.50  | TO    | 10+51.50  | RT    | 30      | 4.0 - 6.0    | 16.7  | 16.7                             |                             |                                   |   | * 2.3                             |       | 16.7              | 12.0   |       |                                |
| STA                                    | 10+51.50  | TO    | 10+80.00  | RT    | 28.5    | 4.0          | 12.7  | 12.7                             |                             |                                   |   | * 1.8                             |       | 12.7              | 9.1  |       |                                |
| STA                                    | 10+80.00  | TO    | 11+00.00  | RT    | 20      | 4.0          | 8.9   | 8.9                              |                             |                                   |   | * 1.2                             |       | 8.9               | 6.4  |       |                                |
| STA                                    | 11+00.00  | TO    | 12+25.00  | RT    | 125     | 4.0          | 55.6  |                                  |                             |                                   |   |                                   |       |                   | 40.0   |       |                                |
| STA                                    | 4+41.81   | TO    | 8+72.00   | LT    | 430.19  | 4.0          | 191.2 |                                  |                             | 191.2                             | 55.6                                      | 26.8                              | 8.6   |                   | 137.7  |       |                                |
| STA                                    | 10+21.50  | TO    | 10+51.50  | LT    | 30      | 6.0 - 4.0    | 16.7  | 16.7                             |                             |                                   |   | * 2.3                             |       | 16.7              | 12.0   |       |                                |
| STA                                    | 10+51.50  | TO    | 10+80.00  | LT    | 28.5    | 4.0          | 12.7  | 12.7                             |                             |                                   |   | * 1.8                             |       | 12.7              | 9.1  |       |                                |
| STA                                    | 10+80.00  | TO    | 11+00.00  | LT    | 20      | 4.0          | 8.9   | 8.9                              |                             |                                   |   | * 1.2                             |       | 8.9               | 6.4  |       |                                |
| STA                                    | 11+00.00  | TO    | 12+25.00  | LT    | 125     | 4.0          | 55.6  |                                  |                             | 55.6                              |   |                                   | 9.3   |                   | 40.0   |       |                                |
| GORE OF RAMP C & D (RAMP D STATIONING) |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 8+72.00   | TO    | 8+96.00   | LT    | 24.00   | 14.0 TO 6.0  | 40.0  |                                  |                             | 40.0                              |   | 5.6                               |       |                   | 28.8   |       |                                |
| ROADWAY E                              |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 2+33.06   | TO    | 8+62.00   | RT    | 696.91  | 6.0          | 464.6 |                                  |                             |                                   | 464.6                                     |                                   | 78.1  |                   | 334.5  |       |                                |
| STA                                    | 2+33.06   | TO    | 8+14.00   | LT    | 666.94  | 6.0          | 444.6 |                                  |                             |                                   | 444.6                                     |                                   | 74.7  |                   | 320.1  |       |                                |
| STA                                    | 8+14.00   | TO    | 8+62.00   | LT    | 30.00   | 6.0 - 4.0    | 16.7  |                                  |                             |                                   | 16.7                                      |                                   | 2.8   |                   | 12.0   |       |                                |
| ROADWAY F                              |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 6+64.17   | TO    | 12+11.70  | RT    | 547.53  | 4.0          | 243.3 |                                  |                             |                                   | 243.3                                     |                                   | 40.9  |                   | 175.2  |       |                                |
| STA                                    | 6+64.17   | TO    | 11+62.00  | LT    | 565.83  | 4.0          | 251.5 |                                  |                             |                                   | 251.5                                     |                                   | 42.2  |                   | 181.1  |       |                                |
| CHINN AVENUE                           |           |       |           |       |         |              |       |                                  |                             |                                   |   |                                   |       |                   |  |       |                                |
| STA                                    | 94+50.00  | TO    | 107+55.00 | RT    | 1330.00 | 4.0          | 591.1 |                                  |                             |                                   | 591.1                                     |                                   | 99.3  |                   | 425.6  |       | 50.5                           |
| STA                                    | 107+78.00 | TO    | 107+90.00 | RT    | 60.00   | 4.0          | 26.7  |                                  |                             |                                   | 26.7                                      |                                   | 4.5   |                   | 19.2   |       | 2.3                            |
| STA                                    | 95+44.00  | TO    | 102+90.00 | LT    | 746.00  | 4.0          | 331.6 |                                  |                             |                                   | 331.6                                     |                                   | 55.7  |                   | 238.7  |       | 28.3                           |
| STA                                    | 105+30.00 | TO    | 107+53.00 | LT    | 223.00  | 4.0          | 99.1  |                                  |                             |                                   | 99.1                                      |                                   | 16.7  |                   | 71.4   |       | 8.5                            |
| TOTALS                                 |           |       |           |       |         |              |       | 3048.5                           | 1254.5                      | 2149.9                            | 2580.3                                    | 301.0                             | 432.7 | 1793.8            | 5315.4                                       | 994.9 | 89.6                           |
| GRAND TOTAL                            |           |       |           |       |         |              |       | 3048.5                           | 1254.5                      | 2149.9                            | 2580.3                                    | 733.7                             |       | 1793.8            | 6310.3                                       |       | 89.6                           |

\* SURFACE COURSE OVER HMA SHOULDERS 3"

MODEL: Definitive  
FILE NAME: D:\

|                     |                               |           |   |    |        |      |                    |                           |           |              |           |
|---------------------|-------------------------------|-----------|---|----|--------|------|--------------------|---------------------------|-----------|--------------|-----------|
| USER NAME = noltera | DESIGNED -                    | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |    |        |      | F.A. RTE.          | SECTION                   | COUNTY    | TOTAL SHEETS | SHEET NO. |
|                     | DRAWN -                       | REVISED - |   |    |        |      | 313                | (8)RS-2                   | HENDERSON | 34           | 9         |
|                     | PLOT SCALE = 140,0000 ' / in. | CHECKED - |   |    |        |      | CONTRACT NO. 68F10 |                           |           |              |           |
|                     | PLOT DATE = 5/31/2019         | DATE -    |   |    |        |      | REVISED -          | ILLINOIS FED. AID PROJECT |           |              |           |
|                     | SCALE:                        | SHEET     |   | OF | SHEETS | STA. | TO STA.            |                           |           |              |           |

MODEL Default  
FILE Name: pavplanroom.dwt\\links\gov\PI\\DOT\Documents\DOT\_Offices\District\_4\Projects\Squad\_6\_a\US34\callout\gulfport\CH.dgn

| 63200310 - GUARDRAIL REMOVAL |             |
|------------------------------|-------------|
| LOCATION                     | LENGTH (FT) |
| STA 21+04 TO 21+49 RT        | 45.0        |

| 78200011 - BARRIER WALL REFLECTORS, TY C |      |
|--|------|
| LOCATION                                 | EACH |
| STA 9+82.75 TO 21+48.98                  | 12.0 |

| 78100100 - RAISED REFLECTIVE PAVEMENT MARKER |      |
|--|------|
| LOCATION                                     | EACH |
| WESTBOUND                                    | 15   |
| EASTBOUND                                    | 15   |
| TURN LANE RAMP A                             | 9    |
| TURN LANE RAMP D                             | 7    |
| TOTAL  | 46   |

| 78300200 - RAISED REFLECTIVE PAVEMENT MARKER REMOVAL |      |
|--|------|
| LOCATION   | EACH |
| WESTBOUND  | 23   |
| EASTBOUND  | 21   |
| TOTAL  | 44   |

| 70107025 - CHANGEABLE MESSAGE SIGN |         |
|------------------------------------|---------|
| LOCATION                           | CAL DAY |
| WEST BOUND                         | 60.0    |
| EASTBOUND                          | 60.0    |
| TOTAL                              | 120.0   |

| 64200116 - SHOULDER RUMBLE STRIPS, 16 INCH |      |
|--|------|
| LOCATION                                   | FOOT |
| WESTBOUND                                  |      |
| STA 13+38 TO 16+45 LT                      | 307  |
| EASTBOUND                                  |      |
| STA 12+15 TO 16+21 RT                      | 406  |
| STA 17+00 TO 17+73                         | 73   |
| TOTAL                                      | 786  |

| GUARDRAIL ITEMS    |  |                                   |  |                               |                                     |  |
|--------------------|--|-----------------------------------|--|-------------------------------|-------------------------------------|--|
| LOCATION           | 63000001   | 63100085                          | 63100167   | 78200005                      | 72501000                            | Z0001002                               |
|                    | STEEL PLATE BEAM<br>GUARDRAIL, TY A, 6'<br>POSTS | TRAFFIC BARRIER<br>TERMINAL, TY 6 | TRAFFIC BARRIER<br>TERMINAL, TY 1<br>(SPECIAL) TANGENT | GUARDRAIL<br>REFLECTORS, TY A | TERMINAL MARKER -<br>DIRECT APPLIED | GUARDRAIL AGGREGATE<br>EROSION CONTROL |
| EASTBOUND          | FT   | EACH                              | EACH   | EACH                          | EACH                                | TON                                    |
| STA 20+50 TO 21+49 | 12.5   | 1.0                               | 1.0  | 2.0                           | 1.0                                 | 24.0                                   |

| LOCATION | 67100100     | 25000750 | XZ013798                       | Z0013798               | X7010216                                     |
|----------|--------------|----------|--------------------------------|------------------------|--|
|          | MOBILIZATION | MOWING   | CONSTRUCTION<br>STATION LAYOUT | CONSTRUCTION<br>LAYOUT | TRAFFIC CONTROL AND<br>PROTECTION, (SPECIAL) |
|          | L SUM        | ACRE     | L SUM                          | L SUM                  | LSUM   |
| JOBSITE  | 1.0          | 8.0      | 1.0                            | 1.0                    | 1.0  |

|  |                               |            |           |   |        |       |    |                           |          |           |                 |              |
|--|-------------------------------|------------|-----------|---|--------|-------|----|---------------------------|----------|-----------|-----------------|--------------|
|  | USER NAME = oltmancr          | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |        |       |    | F.A.<br>RTE.              | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
|  | DRAWN -                       | DRAWN -    | REVISED - |   |        |       |    | 313                       | (8R)RS-2 | HENDERSON | 34              | 10           |
|  | PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |   |        |       |    | CONTRACT NO. 68F10        |          |           |                 |              |
|  | PLOT DATE = 5/31/2019         | DATE -     | REVISED - |   |        |       |    | ILLINOIS FED. AID PROJECT |          |           |                 |              |
|  |                               |            |           |   | SCALE: | SHEET | OF | SHEETS                    | STA.     | TO        | STA.            |              |

MODEL Default  
FILE Name: pavplanroom.dwt\\links\gov\PWIDOT\Documents\DOT\_Offices\District\_4\Projects\Squad\_6\_a\US34\culportgulfportC114.dgn

| LOCATION                        |          |    |           | PAVEMENT MARKING |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
|---------------------------------|----------|----|-----------|------------------|--------------|--|-----------------|-------------------------------------|-------------|-----------------|-------------------------------------|-------------|--------------|--|-------------|--|-----------------------------------|--|
|                                 |          |    |           | 78009004         |              | X7830070                                     | 78009006        | X7830074                            | 78009008    |                 | X7830076                            | 78009012    |              | X7830078                                 | 78009024    | X7830090                                 | 70300100                          | 70300150                                     |
|                                 |          |    |           | LINE 4"          |              | GROOVING FOR<br>RECESSED PAVT<br>MARKING, 5" | LINE 6"         | GROOVING FOR<br>RECESSED PVT MKG 7" | LINE 8"     |                 | GROOVING FOR<br>RECESSED PVT MKG 9" | LINE 12"    |              | GROOVING FOR<br>RECESSED PVT<br>MKG, 13" | LINE 24"    | GROOVING FOR<br>RECESSED PVT MKG,<br>25" | SHORT TERM<br>PAVEMENT<br>MARKING | SHORT TERM<br>PAVEMENT<br>MARKING<br>REMOVAL |
|                                 |          |    |           | WHITE SOLID      | YELLOW SOLID |  | WHITE SKIP-DASH |                                     | WHITE SOLID | WHITE SKIP-DASH |                                     | WHITE SOLID | YELLOW SOLID |  | WHITE SOLID |  |                                   |  |
|                                 |          |    |           | FT               | FT           | FT   | FT              | FT                                  | FT          | FT              | FT                                  | FT          | FT           | FT                                       | FT          | FT                                       | SQ FT                             |  |
| WESTBOUND                       |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 9+82.75  | TO | 10+76.00  | 93.3             | 93.3         | 186.5  |                 |                                     | 186.5       |                 | 186.5                               | 86.0        |              | 86.0                                     |             |  | 373                               | 124.3  |
| STA                             | 10+76.00 | TO | 13+39.00  |                  | 263.0        | 263.0  |                 |                                     | 526.0       |                 | 526.0                               | 24.0        |              | 24.0                                     |             |  | 1052                              | 350.6  |
| STA                             | 13+39.00 | TO | 17+33.00  |                  | 394.0        | 394.0  |                 |                                     | 394.0       |                 | 394.0                               | 44.0        |              | 44.0                                     |             |  | 1576                              | 525.3  |
| STA                             | 17+33.00 | TO | 19+60.00  | 227.0            | 227.0        | 454.0  |                 |                                     | 227.0       |                 | 227.0                               |             |              |  |             |  | 908                               | 302.6  |
| STA                             | 19+60.00 | TO | 21+49.00  | 189.0            | 189.0        | 378.0  |                 |                                     |             | 47.3            | 47.3                                |             |              |  |             |  | 756                               | 252.0  |
| CENTERLINE STA 9+82.75 TO 21+49 |          |    |           |                  |              |  | 300             | 300                                 |             |                 |                                     |             |              |  |             |  |                                   |  |
| EAST BOUND                      |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 9+82.75  | TO | 12+15.00  |                  | 232.3        | 232.3  |                 |                                     | 466         |                 | 466                                 | 221         |              | 221                                      |             |  | 929                               | 309.6  |
| STA                             | 12+15.00 | TO | 16+21.00  | 406.0            | 406.0        | 812.0  |                 |                                     |             |                 |                                     |             |              |  |             |  | 1624                              | 541.3  |
| STA                             | 16+21.00 | TO | 21+49.00  | 478.0            | 528.0        | 1006.0                                       |                 |                                     |             |                 |                                     |             |              |  |             |  | 2112                              | 703.9  |
| CENTERLINE STA 9+82.75 TO 21+49 |          |    |           |                  |              |  | 300             | 300                                 |             |                 |                                     |             |              |  |             |  |                                   |  |
| RAMP A                          |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 5+50.00  | TO | 9+37.00   | 387.0            | 326.0        | 713.0  |                 |                                     | 68.0        |                 | 68.0                                |             | 26.0         | 26.0                                     |             |  | 1000.00                           | 333.3  |
| RAMP B                          |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 0+00.00  | TO | 6+55.60   | 655.6            | 404.0        | 1059.6                                       |                 |                                     |             |                 |                                     |             |              |  |             |  | 1000.00                           | 333.3  |
| RAMP C                          |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 0+00.00  | TO | 3+51.00   | 363              | 272          | 635  |                 |                                     |             |                 |                                     |             | 24.0         | 24.0                                     | 28.0        | 28.0                                     | 1000.00                           | 333.3  |
| RAMP D                          |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 2+10.00  | TO | 12+25.00  | 1236             | 1083         | 2319   |                 |                                     |             |                 |                                     |             |              |  |             |  | 1000.00                           | 333.3  |
| ROADWAY E                       |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 2+83.00  | TO | 8+61.00   | 1221             | 1095         | 2316   |                 |                                     |             |                 |                                     |             |              |  | 15.0        | 15.0                                     | 1000.00                           | 333.3  |
| ROADWAY F                       |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 6+64.00  | TO | 11+85.00  | 1027             | 967          | 1994   |                 |                                     |             |                 |                                     |             |              |  |             |  | 1000.00                           | 333.3  |
| CHINN AVENUE                    |          |    |           |                  |              |  |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| STA                             | 94+50.00 | TO | 107+90.00 | 2369             | 2302         | 4671   |                 |                                     |             |                 |                                     |             |              |  |             |  |                                   |  |
| TOTAL =                         |          |    |           | 8651.9           | 8781.5       | 17433.4                                      | 600.0           | 600.0                               | 1867.5      | 47.3            | 1914.8                              | 375.0       | 50.0         | 425.0                                    | 43.0        | 43.0                                     | 15330.0                           | 5109.5                                       |
| GRAND TOTAL =                   |          |    |           | 17433.4          |              | 17433.4                                      | 600.0           | 600.0                               | 1914.8      |                 | 1914.8                              | 425.0       |              | 425.0                                    | 43.0        | 43.0                                     | 15330.0                           | 5109.5                                       |

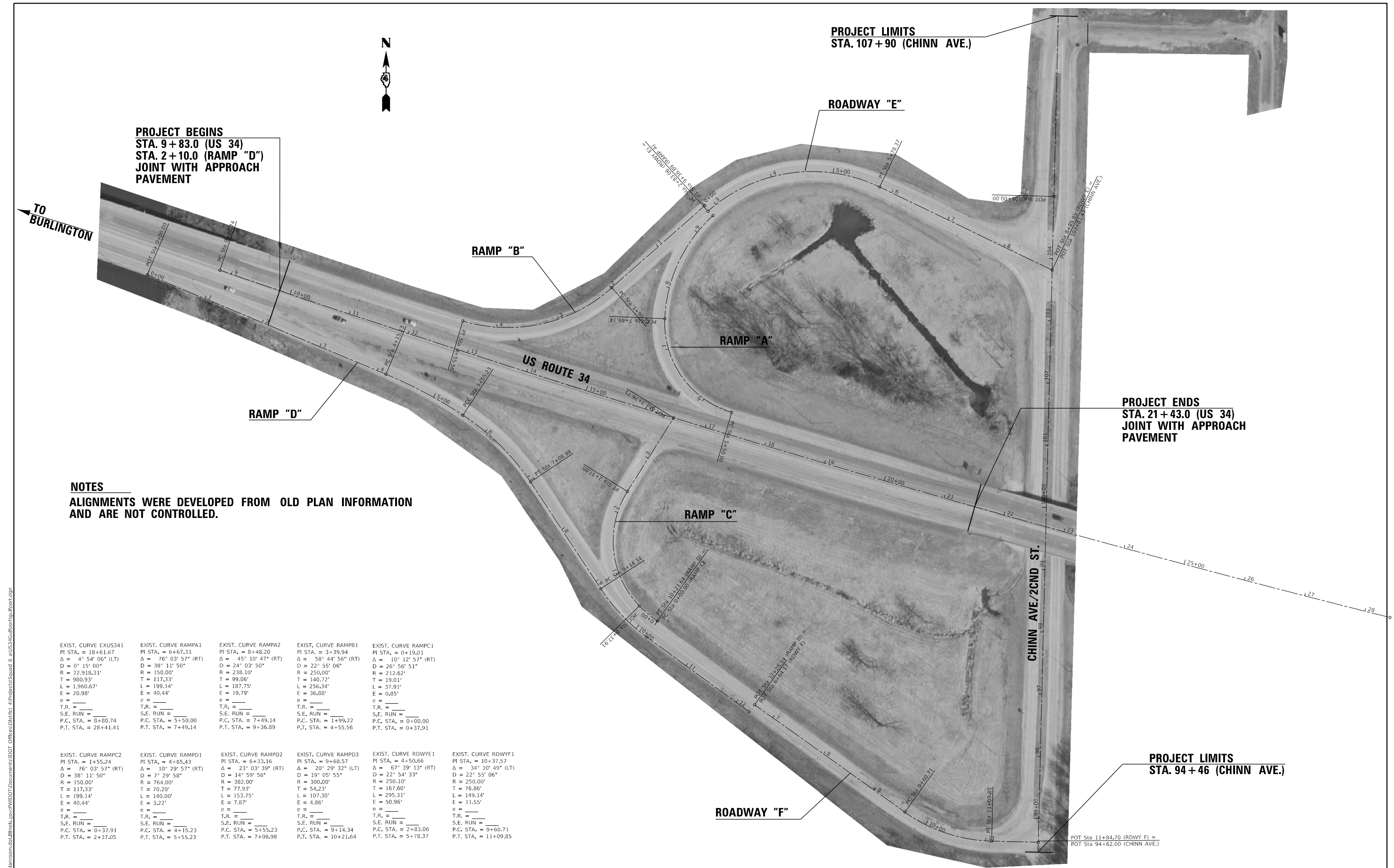
|            |                    |          |   |         |   |
|------------|--------------------|----------|---|---------|---|
| USER NAME  | = altmancr         | DESIGNED | - | REVISED | - |
|            |                    | DRAWN    | - | REVISED | - |
| PLOT SCALE | = 140,0000 ' / in. | CHECKED  | - | REVISED | - |
| PLOT DATE  | = 5/31/2019        | DATE     | - | REVISED | - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET OF SHEETS STA. TO STA.

|                           |          |           |                 |              |
|---------------------------|----------|-----------|-----------------|--------------|
| F.A.<br>RTE.              | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
| 313                       | (8R)RS-2 | HENDERSON | 34              | 11           |
| CONTRACT NO. 68F10        |          |           |                 |              |
| ILLINOIS FED. AID PROJECT |          |           |                 |              |

MODEL Default  
FILE Name: p:\pub\anr\room\dot\illinois\dot\Documents\DOT Office\District 6\al\US34\cullington\front.dgn



PROJECT BEGINS  
STA. 9+83.0 (US 34)  
STA. 2+10.0 (RAMP "D")  
JOINT WITH APPROACH  
PAVEMENT

PROJECT LIMITS  
STA. 107+90 (CHINN AVE.)

PROJECT ENDS  
STA. 21+43.0 (US 34)  
JOINT WITH APPROACH  
PAVEMENT

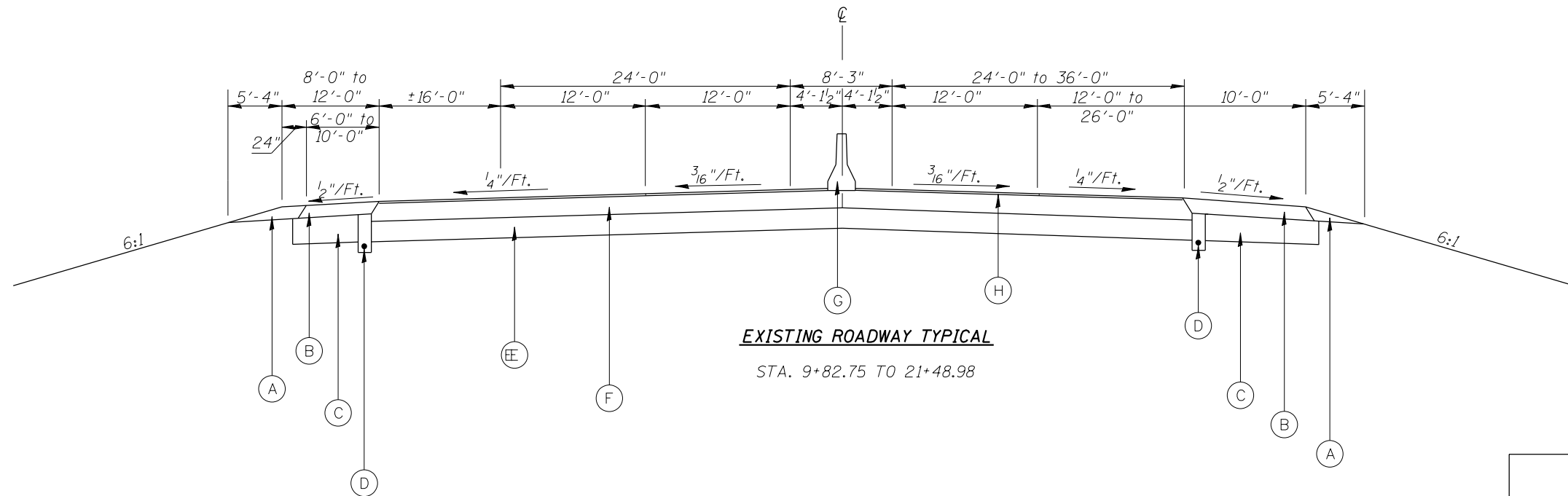
PROJECT LIMITS  
STA. 94+46 (CHINN AVE.)

NOTES  
ALIGNMENTS WERE DEVELOPED FROM OLD PLAN INFORMATION  
AND ARE NOT CONTROLLED.

|   |  |   |  |   |   |  |   |   |  |
|---|--|---|--|---|---|--|---|---|--|
| EXIST. CURVE EXUS341<br>PI STA. = 18+61.67<br>Δ = 4° 54' 06" (LT)<br>D = 0° 15' 00"<br>R = 22,918.31'<br>T = 980.93'<br>L = 1,960.67'<br>E = 20.98'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 8+80.74<br>P.T. STA. = 28+41.41 | EXIST. CURVE RAMP A1<br>PI STA. = 6+67.33<br>Δ = 76° 03' 57" (RT)<br>D = 38° 11' 50"<br>R = 150.00'<br>T = 117.33'<br>L = 199.14'<br>E = 40.44'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 5+50.00<br>P.T. STA. = 7+49.14 | EXIST. CURVE RAMP A2<br>PI STA. = 8+48.20<br>Δ = 45° 10' 47" (RT)<br>D = 24° 03' 50"<br>R = 238.10'<br>T = 99.06'<br>L = 187.75'<br>E = 19.79'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 7+49.14<br>P.T. STA. = 9+36.89 | EXIST. CURVE RAMP B1<br>PI STA. = 3+39.94<br>Δ = 58° 44' 56" (RT)<br>D = 22° 55' 06"<br>R = 250.00'<br>T = 140.72'<br>L = 256.34'<br>E = 36.88'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 1+99.22<br>P.T. STA. = 4+55.56 | EXIST. CURVE RAMP C1<br>PI STA. = 0+19.01<br>Δ = 10° 12' 57" (RT)<br>D = 26° 56' 51"<br>R = 212.62'<br>T = 19.01'<br>L = 37.91'<br>E = 0.85'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 0+00.00<br>P.T. STA. = 0+37.91 | EXIST. CURVE RAMP D1<br>PI STA. = 4+85.43<br>Δ = 10° 29' 57" (RT)<br>D = 7° 29' 58"<br>R = 764.00'<br>T = 70.20'<br>L = 140.00'<br>E = 2.22'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 4+15.23<br>P.T. STA. = 5+55.23 | EXIST. CURVE RAMP D2<br>PI STA. = 6+33.16<br>Δ = 23° 03' 39" (RT)<br>D = 14° 59' 56"<br>R = 382.00'<br>T = 77.93'<br>L = 153.75'<br>E = 7.87'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 5+55.23<br>P.T. STA. = 7+08.98 | EXIST. CURVE RAMP D3<br>PI STA. = 9+68.57<br>Δ = 20° 29' 32" (LT)<br>D = 19° 05' 55"<br>R = 300.00'<br>T = 54.23'<br>L = 107.30'<br>E = 4.86'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 9+14.34<br>P.T. STA. = 10+21.64 | EXIST. CURVE RDWYF1<br>PI STA. = 4+50.66<br>Δ = 67° 39' 13" (RT)<br>D = 22° 54' 33"<br>R = 250.10'<br>T = 167.60'<br>L = 295.31'<br>E = 50.96'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 2+83.06<br>P.T. STA. = 5+78.37 | EXIST. CURVE RDWYF2<br>PI STA. = 10+37.57<br>Δ = 34° 10' 49" (LT)<br>D = 22° 55' 06"<br>R = 250.00'<br>T = 76.86'<br>L = 149.14'<br>E = 11.55'<br>e = _____<br>T.R. = _____<br>S.E. RUN = _____<br>P.C. STA. = 9+60.71<br>P.T. STA. = 11+09.85 |
|---|--|---|--|---|---|--|---|---|--|

|   |  |                 |  |              |  |         |  |                           |          |           |                 |              |
|---|--|-----------------|--|--------------|--|---------|--|---------------------------|----------|-----------|-----------------|--------------|
| STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |  |                 |  | LINE DIAGRAM |  |         |  | F.A.P<br>RTE.             | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
|   |  |                 |  |              |  |         |  | 313                       | (8R)RS-2 | HENDERSON | 34              | 12           |
|   |  |                 |  |              |  |         |  | CONTRACT NO. 68F10        |          |           |                 |              |
|   |  |                 |  |              |  |         |  | ILLINOIS FED. AID PROJECT |          |           |                 |              |
| SCALE:  |  | SHEET OF SHEETS |  | STA.         |  | TO STA. |  |                           |          |           |                 |              |





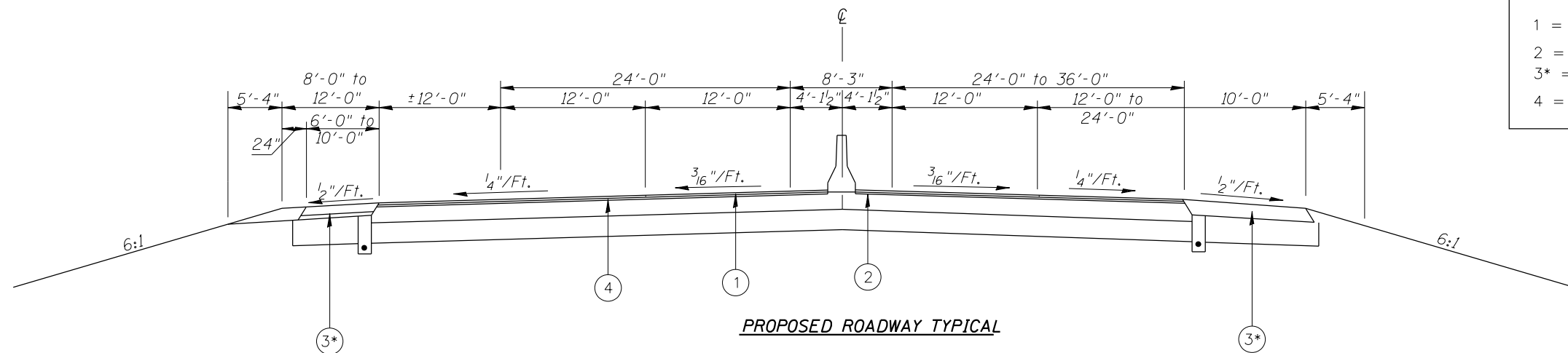
**EXISTING ROADWAY TYPICAL**

STA. 9+82.75 TO 21+48.98

## LEGEND

- A = EXISTING AGGREGATE SHOULDERS
- B = EXISTING HMA SHOULDERS, 8"
- C = EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
- D = EXISTING PIPE UNDERDRAIN, 4"
- E = EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 12"
- F = EXISTING FULL DEPTH HMA PAVEMENT, 11"
- G = EXISTING MEDIAN CONCRETE BARRIER
- H = EXISTING HMA SURFACE COURSE MIX"D" CLASS I, TY 2, 1 1/2"

- 1 = PLOYMERIZED HMA SURF. COURSE MIX "D" N70, 1 1/2"
- 2 = POLYMERIZED L.B. (MACHINE METHOD), IL-4.75, N50, 1"
- 3\* = HMA SHOULDER: 5 1/2" / HMA SURF. CRSE. IL-9.5FG N50
- 4 = HMA SURFACE REMOVAL 2 1/2"



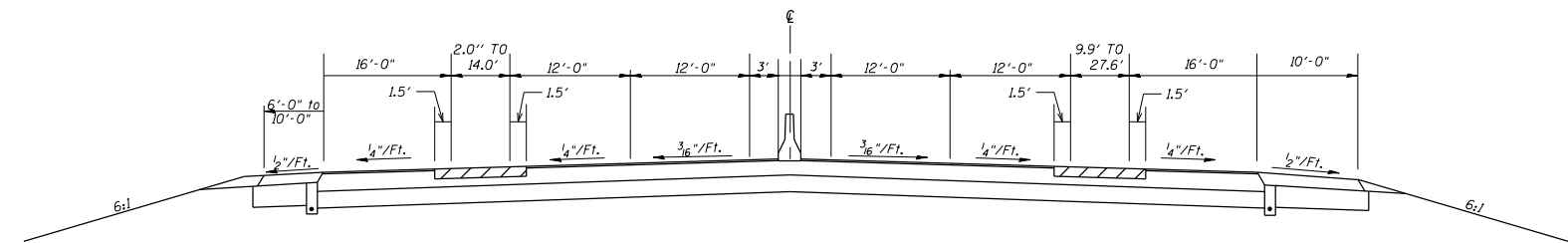
**PROPOSED ROADWAY TYPICAL**

STA 9+82.75 TO 21+48.98

\* PRESTAGE SHOULDERS ARE HMA SHOUDLER 5 1/2",STAGE  
2 MILL 2 1/2" AND PLACE HMA SURF. COURSE IL-9.5FG,N50  
SEE STAGE CONSTRUCTION SEQUENCE FOR FURTHER INFORMATION

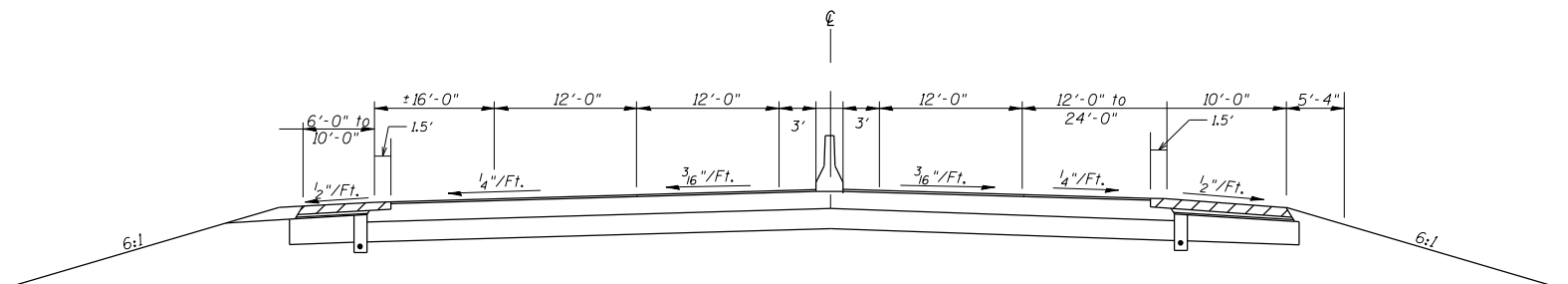
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|--|-------------------------------|------------|-----------|---|---|--|--|--|-----------------|----------|---------------------------|-----------------|--------------|
| FILE NAME =  | USER NAME = oltmanor          | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS /CONSTRUCTION SEQUENCE |  |  |  | F.A.P.<br>RTE.  | SECTION  | COUNTY                    | TOTAL<br>SHEETS | SHEET<br>NO. |
| p:\planroom\dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 4\Projects\Squad 6\BURN4Gulfport\gulfportCliff.dgn |                               | CHECKED -  | REVISED - |   |   |  |  |  | 313             | (8R)RS-2 | HENDERSON                 | 34              | 13           |
| Default  | PLOT SCALE = 140.0000 ' / in. | DATE -     | REVISED - |   | SCALE:                                  |  |  |  | SHEET OF SHEETS |          | STA. TO STA.              |                 |              |
|  | PLOT DATE = 6/3/2019          |            |           |   |   |  |  |  |                 |          | ILLINOIS FED. AID PROJECT |                 |              |
| CONTRACT NO. 68F10   |                               |            |           |   |   |  |  |  |                 |          |                           |                 |              |

## PRE-STAGE 1



### PROPOSED PARTIAL DEPTH PATCHING AT GORES

WB: STA 9+82.75 TO 13+38.16  
EB: STA 9+82.75 TO 12+15.09



PROPOSED SHOULDER IMPROVEMENTS

WB: 13+38.16 TO 16+45  
EB: 12+15.09 TO 16+21  
EB: 16+30 TO 17+73

PARTIAL DEPTH PATCHING, 5½"  
PLACE PARTIAL DEPTH PATCHING (TONS)  
- 1ST LIFT = 3.0" HMA SURF. COURSE IL-9.5 MIX"C" N50  
- 2ND LIFT = 2.5" HMA SURF. COURSE IL-9.5 MIX"C" N50

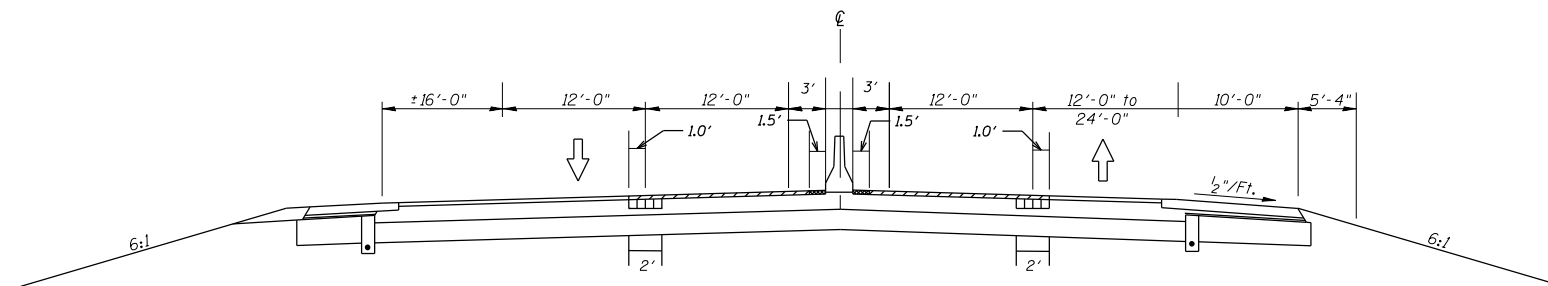
SHOULDER REMOVAL (SPECIAL)  
PLACE HMA SHOULDERS, 5 1/2"

|  |                              |            |           |  |  |  |  |  |  |  |  |                |         |           |                 |              |
|--|------------------------------|------------|-----------|--|--|--|--|--|--|--|--|----------------|---------|-----------|-----------------|--------------|
| FILE NAME =  | USER NAME = oltmanor         | DESIGNED - | REVISED - |  |  |  |  |  |  |  |  | F.A.P.<br>RTE. | SECTION | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
| p:\planroom\dot.illinois.gov\PIDOT\Documents\DOT Offices\District 4\Projects\SquadRAWS\4Gulfport\gulfportCliff.dgn |                              | DRAWN      | REVISED - |  |  |  |  |  |  |  |  | 313            | (8)RS-2 | HENDERSON | 34              | 14           |
|  | PLOT SCALE = 140.0000' / in. | CHECKED -  | REVISED - |  |  |  |  |  |  |  |  |                |         |           |                 |              |
| Default  | PLOT DATE = 6/3/2019         | DATE -     | REVISED - |  |  |  |  |  |  |  |  |                |         |           |                 |              |

### STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

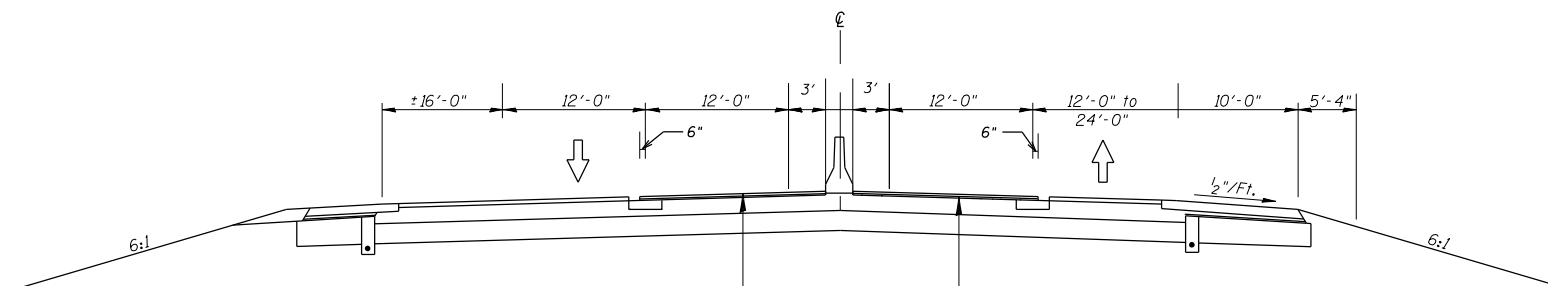
| TYPICAL SECTIONS /CONSTRUCTION SEQUENCE |       |    |        |      |         |  |                           |  |
|---|-------|----|--------|------|---------|--|---------------------------|--|
| SCALE:                                  | SHEET | OF | SHEETS | STA. | TO STA. |  | ILLINOIS FED. AID PROJECT |  |
|   |       |    |        |      |         |  | CONTRACT NO. 6BF10        |  |

## STAGE 1



LOCATION

WB &amp; EB PL: STA 9+82.75 TO 21+48.98



LOCATION

WB &amp; EB PL: STA 9+82.75 TO 21+48.98



HMA SURFACE REMOVAL 2 1/2"



HMA REMOVAL (SPECIAL)

|  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|
|  |  |  |  |  |  |  |
|--|--|--|--|--|--|--|

LONGITUDINAL JOINT REPAIR - 3"

①

POLYMERIZED LEVELING BINDER COURSE (MACHINE METHOD) IL-4.75 N50, 1"

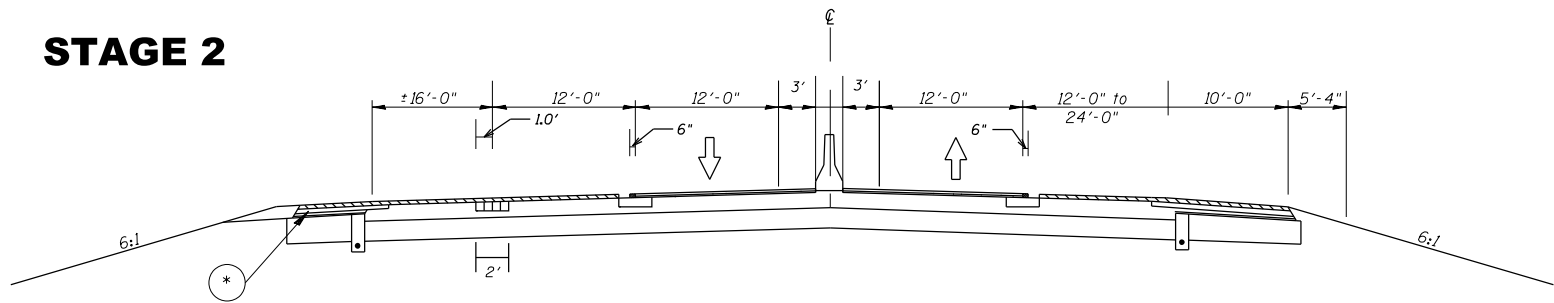
②

POLYMERIZED HMA SURFACE COURSE MIX "D" N70, 1 1/2"

NOTE : ONCE BUTT JOINTS ARE REMOVED, THEN PERFORM THE PARTIAL DEPTH PATCH ADJACENT TO BRIDGE APPROACH PAVEMENT

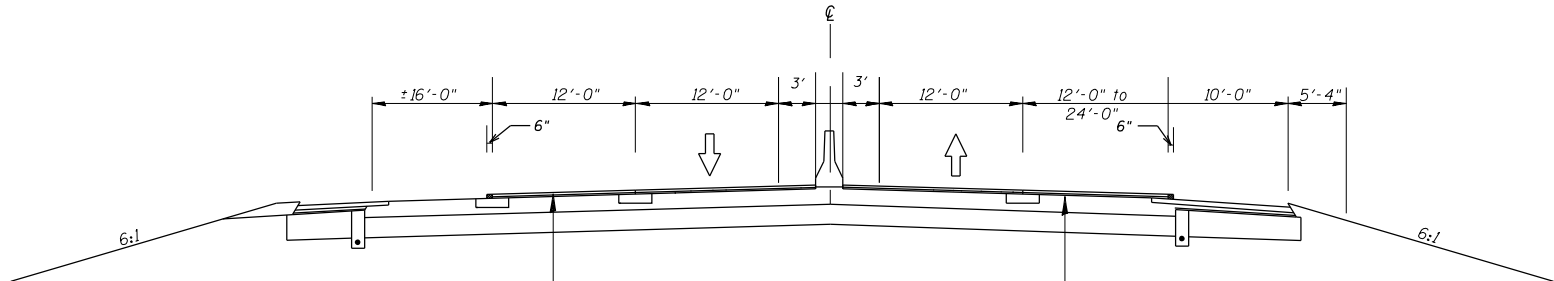
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| FILE NAME :   | USER NAME = oltmancr | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS /CONSTRUCTION SEQUENCE |       |    |        | F.A.P. RTE. | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| p:\planroom.dot\illinois.gov\PIWIDOT\Documents\DOT Offices\District 4\Projects\Squad 6\Drawings\4Gulfport\gulfportCliff.dgn |                      |            |           |   |   |       |    |        | 313         | (8)RS-2 | HENDERSON                 | 34           | 15        |
| PLOT SCALE = 140.0000' / in.  | CHECKED -            | REVISED -  |           |   | CONTRACT NO. 68F10                      |       |    |        |             |         |                           |              |           |
| PLOT DATE = 6/3/2019  | DATE -               | REVISED -  |           |   | SCALE:                                  | SHEET | OF | SHEETS | STA.        | TO STA. | ILLINOIS FED. AID PROJECT |              |           |

STAGE 2



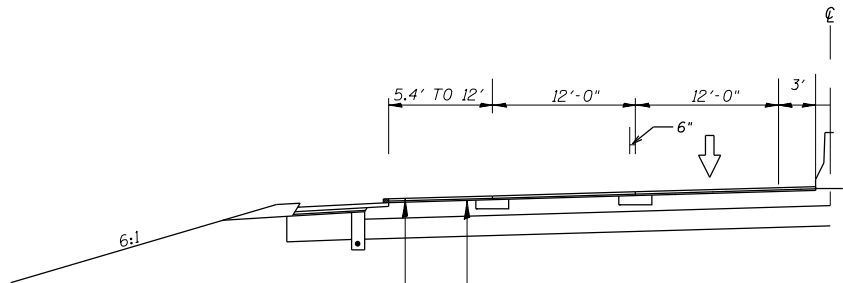
LOCATION

WB & EB DL: STA 9+82.75 TO 21+48.98



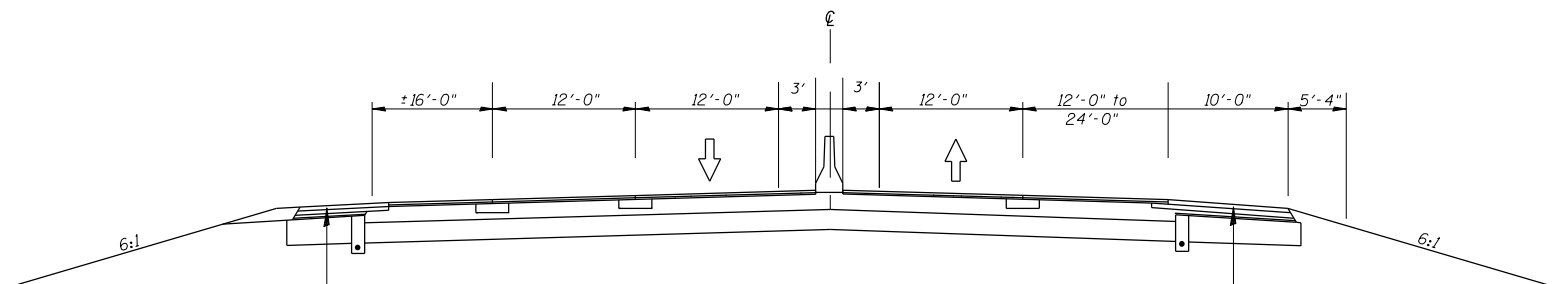
LOCATION

WB & EB DL: STA 9+82.75 TO 21+48.98



LOCATION

TURN LANE FOR RAMP A  
STA 16+77 TO 21+38.98



LOCATION

WB & EB DL: STA 9+82.75 TO 21+48.98



HMA SURFACE REMOVAL, SPECIAL (JOINT TRIM)  
TRIM PASSING LANE/ DRIVING LANE JOINT



HMA SURFACE REMOVAL, 2 1/2"



LONGITUDINAL JOINT REPAIR, 3"  
BETWEEN TURN LANE FOR RAMP A AND WBDL  
STA 16+77 TO 21+38.98



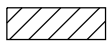
SHOULDER REMOVAL (SPECIAL) ON OUTSIDE SHOULDER OF TURN LANE FOR  
RAMP A, (US 34 STA 17+33.95 TO 21+48.98/ RAMP A STA 5+50 TO 6+02.5)



POLYMERIZED LEVELING BINDER COURSE (MACHINE METHOD) IL-4.75 N50, 1"



POLYMERIZED HMA SURFACE COURSE MIX "D" N70, 1 1/2"



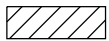
HMA SURFACE REMOVAL, SPECIAL (JOINT TRIM)  
TRIM DRIVING LANE/ OUTSIDE SHOULDER JOINT  
AND DRIVING LANE AND TURN LANE FOR RAMP A



POLYMERIZED LEVELING BINDER COURSE (MACHINE METHOD) IL-4.75 N50, 1"



POLYMERIZED HMA SURFACE COURSE MIX "D" N70, 1 1/2"



HMA SURFACE REMOVAL, SPECIAL (JOINT TRIM)  
TRIM DRIVING LANE/ OUTSIDE SHOULDER JOINT  
AND DRIVING LANE AND TURN LANE FOR RAMP A



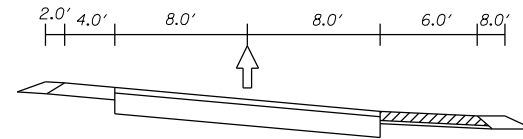
HMA SURFACE COURSE, IL-9.5FG, N50, 2 1/2"

NOTE : ONCE BUTT JOINTS ARE REMOVED, THEN PERFORM THE PARTIAL  
DEPTH PATCH ADJACENT TO BRIDGE APPROACH PAVEMENT

|  |                             |            |           |   |   |                 |                |          |                           |                 |              |
|--|-----------------------------|------------|-----------|---|---|-----------------|----------------|----------|---------------------------|-----------------|--------------|
| FILE NAME =  | USER NAME = oltmanor        | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS /CONSTRUCTION SEQUENCE |                 | F.A.P.<br>RTE. | SECTION  | COUNTY                    | TOTAL<br>SHEETS | SHEET<br>NO. |
| p:\planroom\dotillinois.gov\PWIDOT\Documents\IDOT Offices\District 4\Projects\Squad 6\Drawings\4Gulfport\gulfportCliff.dgn |                             | CHECKED -  | REVISED - |   |   |                 | 313            | (8R)RS-2 | HENDERSON                 | 34              | 16           |
| Default  | PLOT SCALE = 1/4" = 100'-0" | DATE -     | REVISED - |   | SCALE:                                  | SHEET OF SHEETS | STA.           | TO STA.  | CONTRACT NO. 68F10        |                 |              |
|  | PLOT DATE = 6/3/2019        |            |           |   |   |                 |                |          | ILLINOIS FED. AID PROJECT |                 |              |

### STAGE 3

### STAGE 3A



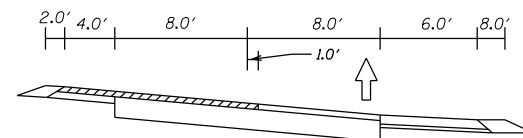
LOCATION

RIGHT SHOULDER  
RAMP A: STA 6+02.5 TO 9+36.89  
RAMP B: STA 0+00 TO 6+55.56  
RAMP C: STA 0+00 TO 3+02.52  
RAMP D: STA 1+97 TO 7+09



SHOULDER REMOVAL (SPECIAL)  
HOT-MIX ASPHALT SHOULDERS, 3"  
HMA SURFACE COURSE, IL-9.5FG, N50, 2 1/2"

### STAGE 3B

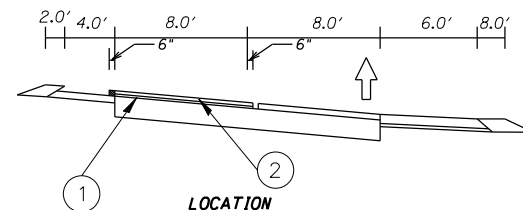


LOCATION

RAMP A : 6+02.5 TO 9+36.89  
RAMP B : 0+00 TO 6+55.56  
RAMP C : 0+00 TO 3+39  
RAMP D : 1+97 TO 12+25

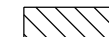


HMA SURFACE REMOVAL, 2 1/2"



LOCATION

RAMP A : 6+02.5 TO 9+36.89  
RAMP B : 0+00 TO 6+55.56  
RAMP C : 0+00 TO 3+39  
RAMP D : 1+97 TO 12+25



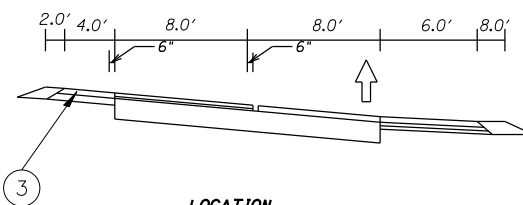
HMA SURFACE REMOVAL SPECIAL (JOINT TRIM)

①

POLYMERIZED LEVELING BINDER COURSE (MACHINE METHOD) IL-4.75 N50 1"

(2)

POLYMERIZED HMA SURFACE COURSE MIX "D" N70, 1 1/2"



LOCATION

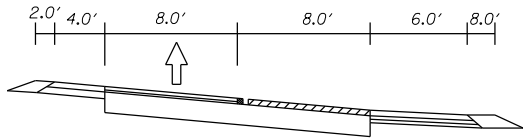
RAMP A : 6+02.5 TO 9+36.89  
RAMP B : 0+00 TO 6+55.56  
RAMP C : 0+00 TO 3+39  
RAMP D : 1+97 TO 12+25



HMA SURFACE COURSE, IL-9.5FG, N50 2 1/2"

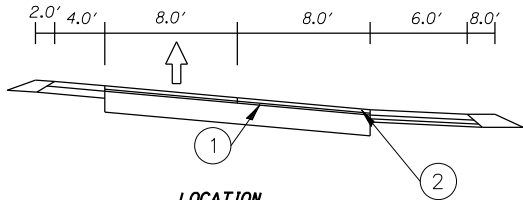
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| FILE NAME =  | USER NAME = oltmancr          | DESIGNED - | REVISED - | <div>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</div> | <div>TYPICAL SECTIONS /CONSTRUCTION SEQUENCE</div> | F.A.P. RTE.        | SECTION | COUNTY    | TOTAL SHEETS | SHEET NO. |         |
| p:\planroom\dot.illinois.gov\PWIDOT\Documents\DOT Offices\District 4\Projects\Squad 60R\B34\Gulfport\gulfportCliff.dgn |                               | 60R\B34    | REVISED - |   |  | 313                | (B)RS-2 | HENDERSON | 34           | 17        |         |
| Default  | PLOT SCALE = 140.0000 ' / in. | CHECKED -  | REVISED - |   |  | CONTRACT NO. 68F10 |         |           |              |           |         |
|  | PLOT DATE = 6/3/2019          | DATE -     | REVISED - |   |  | SCALE:             | SHEET   | OF        | SHEETS       | STA.      | TO STA. |

STAGE 3



LOCATION

RAMP A : 6+02.5 TO 9+36.89  
RAMP B : 0+00 TO 6+55.56  
RAMP C : 0+00 TO 3+39  
RAMP D : 1+97 TO 12+25



LOCATION

RAMP A : 6+02.5 TO 9+36.89  
RAMP B : 0+00 TO 6+55.56  
RAMP C : 0+00 TO 3+39  
RAMP D : 1+97 TO 12+25



HMA SURFACE REMOVAL, 2 1/2"



HMA SURFACE REMOVAL SPECIAL (JOINT TRIM)

1

POLYMERIZED LEVELING BINDER COURSE (MACHINE METHOD) IL-4.75 N50 1"

2

POLYMERIZED HMA SURFACE COURSE MIX "D" N70, 1 1/2"

|  |                              |            |           |   |   |  |                           |  |                |          |           |                 |              |
|--|------------------------------|------------|-----------|---|---|--|---------------------------|--|----------------|----------|-----------|-----------------|--------------|
| FILE NAME =  | USER NAME = oltmanor         | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS /CONSTRUCTION SEQUENCE |  |                           |  | F.A.P.<br>RTE. | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |
| p:\planroom\dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 4\Projects\Squad 4\BURN4Gulfport\gulfportCliff.dgn |                              | CHECKED -  | REVISED - |   |   |  |                           |  | 313            | (8R)RS-2 | HENDERSON | 34              | 18           |
| Default  | PLOT SCALE = 140.0000' / in. |            | REVISED - |   | CONTRACT NO. 68F10                      |  |                           |  |                |          |           |                 |              |
|  | PLOT DATE = 6/3/2019         | DATE -     | REVISED - |   | SCALE:                                  |  | ILLINOIS FED. AID PROJECT |  |                |          |           |                 |              |
|  |                              |            |           |   | SHEET OF SHEETS                         |  | STA. TO STA.              |  |                |          |           |                 |              |



## STAGE 1



## CONSTRUCTION AREA

### ROADWAY E -

**RAMP B -**

— **RAMP A**

— **RAMP D**

### RAMP C

## ROADWAY F



## STAGE 1

## CONSTRUCTION

### REMOVE EXISTING RPM'S WITHIN ALLOWED CLOSURE

**MILL 2 1/2" ON EB & WB PASSING LANE = 15' WIDE (13.5' P.L. + 1.5' D.L.), AND HMA REMOVAL (SPECIAL) ADJACENT TO BARRIER WALL (1.5')**

**PERFORM PARTIAL DEPTH PATCHING IN EB & WB PASSING LANES AND  
JOINT REPAIR ALONG WB & EB CENTERLINE (2' WIDE, 1.0' EACH SIDE OF JOINT)**

**PLACE 1" HMA LEVELING BINDER ON PASSING LANES & INSIDE SHOUDLER EXTENDING 6" INTO DRIVING LANE (1.5' INSIDE SHOULDER + 13.5' P.L. + 6" D.L. = 15.5')**

**PLACE 1 1/2" HMA SURF. COURSE MIX"D" N50 ON PASSING LANES & INSIDE SHOULDER  
EXTENDING 6" INTO DRIVING LANE (1.5' INSIDE SHOULDER + 13.5' P.L. + 6" D.L. = 15.5')**

## PLACE BARRIER WALL REFLECTORS

**PLACE SHORT TERM PAVEMENT MARKINGS, 4" ON CENTERLINE AND INSIDE EDGELINE  
PER ART. 703.03**

## TRAFFIC

**COMPLETE THIS WORK UTILIZING TRAFFIC CONTROL  
AND PROTECTION STANDARDS 701422.**

|                               |            |           |
|-------------------------------|------------|-----------|
| USER NAME = oltmancr          | DESIGNED - | REVISED - |
|                               | DRAWN -    | REVISED - |
| PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 6/3/2019          | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

## STAGING DETAILS

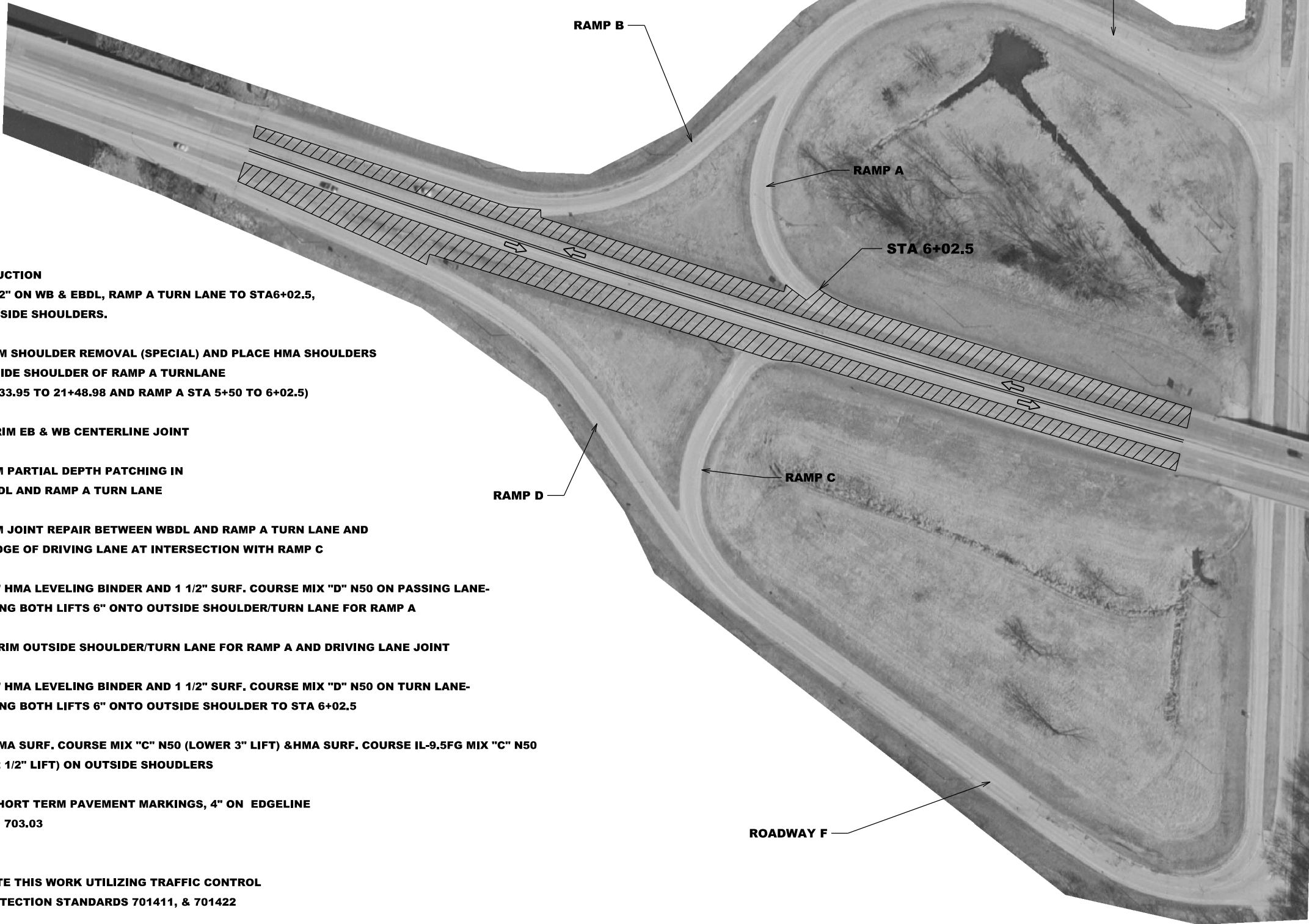
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|--------|-------|----|--------|------|---------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. |
|--------|-------|----|--------|------|---------|

|              |          |                    |                  |              |
|--------------|----------|--------------------|------------------|--------------|
| F.A.<br>RTE. | SECTION  | COUNTY             | TOTAL<br>SHEETS  | SHEET<br>NO. |
| 313          | (8R)RS-2 | HENDERSON          | 34               | 20           |
|              |          | CONTRACT NO. 68F10 |                  |              |
|              |          | ILLINOIS           | FED. AID PROJECT |              |



STAGE 2

CONSTRUCTION AREA



STAGE 2

CONSTRUCTION

MILL 2 1/2" ON WB & EBDL, RAMP A TURN LANE TO STA6+02.5, AND OUTSIDE SHOULDERS.

PERFORM SHOULDER REMOVAL (SPECIAL) AND PLACE HMA SHOULDERS ON OUTSIDE SHOULDER OF RAMP A TURNLANE (STA 17+33.95 TO 21+48.98 AND RAMP A STA 5+50 TO 6+02.5)

JOINT TRIM EB & WB CENTERLINE JOINT

PERFORM PARTIAL DEPTH PATCHING IN WB & EBDL AND RAMP A TURN LANE

PERFORM JOINT REPAIR BETWEEN WBDL AND RAMP A TURN LANE AND RIGHT EDGE OF DRIVING LANE AT INTERSECTION WITH RAMP C

PLACE 1" HMA LEVELING BINDER AND 1 1/2" SURF. COURSE MIX "D" N50 ON PASSING LANE- EXTENDING BOTH LIFTS 6" ONTO OUTSIDE SHOULDER/TURN LANE FOR RAMP A

JOINT TRIM OUTSIDE SHOULDER/TURN LANE FOR RAMP A AND DRIVING LANE JOINT

PLACE 1" HMA LEVELING BINDER AND 1 1/2" SURF. COURSE MIX "D" N50 ON TURN LANE- EXTENDING BOTH LIFTS 6" ONTO OUTSIDE SHOULDER TO STA 6+02.5

PLACE HMA SURF. COURSE MIX "C" N50 (LOWER 3" LIFT) &HMA SURF. COURSE IL-9.5FG MIX "C" N50 (UPPER 2 1/2" LIFT) ON OUTSIDE SHOUDLERS

PLACE SHORT TERM PAVEMENT MARKINGS, 4" ON EDGELINE PER ART. 703.03

TRAFFIC

COMPLETE THIS WORK UTILIZING TRAFFIC CONTROL AND PROTECTION STANDARDS 701411, & 701422

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|                               |            |           |
|-------------------------------|------------|-----------|
| USER NAME = oltmancr          | DESIGNED - | REVISED - |
|                               | DRAWN -    | REVISED - |
| PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 6/3/2019          | DATE -     | REVISED - |

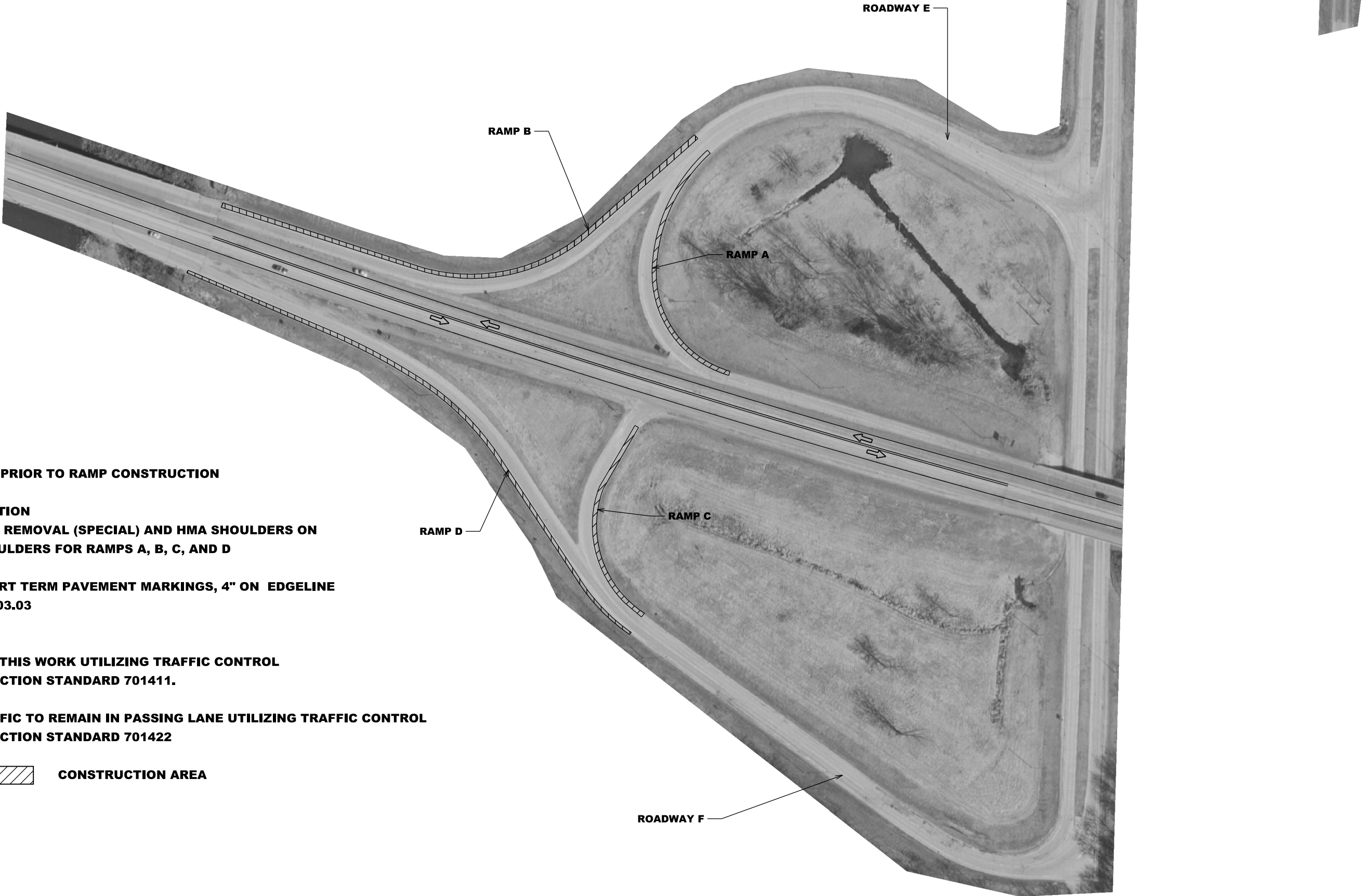
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STAGING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A. RTE.                 | SECTION  | COUNTY    | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|-----------|--------------|-----------|
| 313                       | (8R)RS-2 | HENDERSON | 34           | 21        |
| CONTRACT NO. 68F10        |          |           |              |           |
| ILLINOIS FED. AID PROJECT |          |           |              |           |

STAGE 3A



STAGE 3A - PRIOR TO RAMP CONSTRUCTION

CONSTRUCTION  
SHOULDER REMOVAL (SPECIAL) AND HMA SHOULDERS ON  
RIGHT SHOULDERS FOR RAMPS A, B, C, AND D

PLACE SHORT TERM PAVEMENT MARKINGS, 4" ON EDGELINE  
PER ART. 703.03

TRAFFIC  
COMPLETE THIS WORK UTILIZING TRAFFIC CONTROL  
AND PROTECTION STANDARD 701411.

US 34 TRAFFIC TO REMAIN IN PASSING LANE UTILIZING TRAFFIC CONTROL  
AND PROTECTION STANDARD 701422

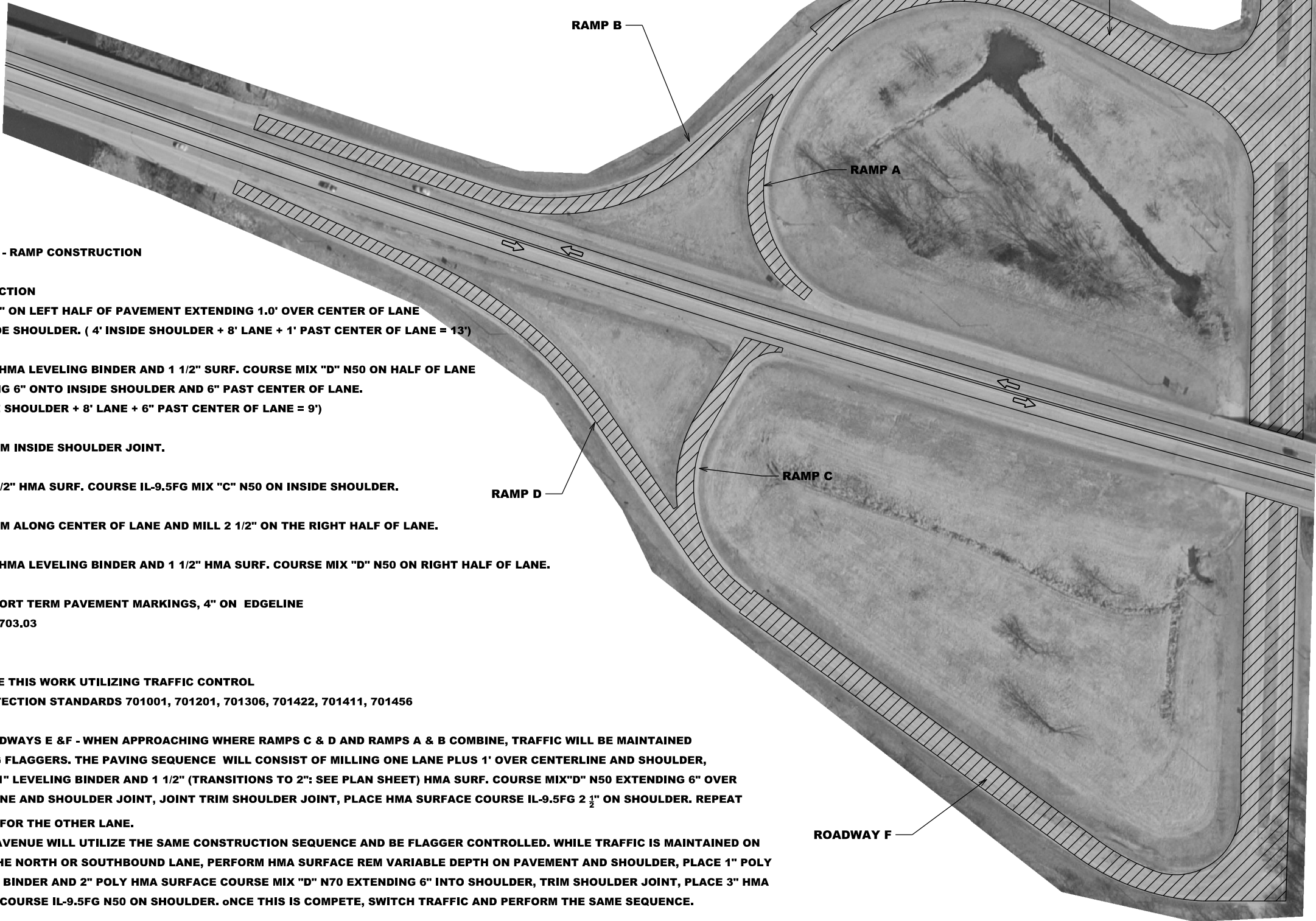
 CONSTRUCTION AREA

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|--|-------------------------------|------------|-----------|---|--------------------|----------|-----------|--------|--------------|---------|--------|-----------------|--------------|
|  | USER NAME = dltmancr          | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | STAGING DETAILS    |          |           |        | F.A.<br>RTE. | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|  |                               | DRAWN -    | REVISED - |   | 313                | (8R)RS-2 | HENDERSON | 34     | 22           |         |        |                 |              |
|  | PLOT SCALE = 140,0000 ' / in. | CHECKED -  | REVISED - |   | CONTRACT NO. 68F10 |          |           |        |              |         |        |                 |              |
|  | PLOT DATE = 6/3/2019          | DATE -     | REVISED - |   | SCALE:             | SHEET    | OF        | SHEETS | STA.         | TO      | STA.   |                 |              |

STAGE 3B

CONSTRUCTION AREA



STAGE 3B - RAMP CONSTRUCTION

CONSTRUCTION

MILL 2 1/2" ON LEFT HALF OF PAVEMENT EXTENDING 1.0' OVER CENTER OF LANE AND INSIDE SHOULDER. ( 4' INSIDE SHOULDER + 8' LANE + 1' PAST CENTER OF LANE = 13')

PLACE 1" HMA LEVELING BINDER AND 1 1/2" SURF. COURSE MIX "D" N50 ON HALF OF LANE EXTENDING 6" ONTO INSIDE SHOULDER AND 6" PAST CENTER OF LANE. (6" INSIDE SHOULDER + 8' LANE + 6" PAST CENTER OF LANE = 9')

JOINT TRIM INSIDE SHOULDER JOINT.

PLACE 2 1/2" HMA SURF. COURSE IL-9.5FG MIX "C" N50 ON INSIDE SHOULDER.

JOINT TRIM ALONG CENTER OF LANE AND MILL 2 1/2" ON THE RIGHT HALF OF LANE.

PLACE 1" HMA LEVELING BINDER AND 1 1/2" HMA SURF. COURSE MIX "D" N50 ON RIGHT HALF OF LANE.

PLACE SHORT TERM PAVEMENT MARKINGS, 4" ON EDGELINE PER ART. 703.03

TRAFFIC

COMPLETE THIS WORK UTILIZING TRAFFIC CONTROL AND PROTECTION STANDARDS 701001, 701201, 701306, 701422, 701411, 701456

\*FOR ROADWAYS E & F - WHEN APPROACHING WHERE RAMPS C & D AND RAMPS A & B COMBINE, TRAFFIC WILL BE MAINTAINED UTILIZING FLAGGERS. THE PAVING SEQUENCE WILL CONSIST OF MILLING ONE LANE PLUS 1' OVER CENTERLINE AND SHOULDER, PLACING 1" LEVELING BINDER AND 1 1/2" (TRANSITIONS TO 2": SEE PLAN SHEET) HMA SURF. COURSE MIX"D" N50 EXTENDING 6" OVER CENTERLINE AND SHOULDER JOINT, JOINT TRIM SHOULDER JOINT, PLACE HMA SURFACE COURSE IL-9.5FG 2 1/2" ON SHOULDER. REPEAT PROCESS FOR THE OTHER LANE.

\*\*CHINN AVENUE WILL UTILIZE THE SAME CONSTRUCTION SEQUENCE AND BE FLAGGER CONTROLLED. WHILE TRAFFIC IS MAINTAINED ON EITHER THE NORTH OR SOUTHBOUND LANE, PERFORM HMA SURFACE REM VARIABLE DEPTH ON PAVEMENT AND SHOULDER, PLACE 1" POLY LEVELING BINDER AND 2" POLY HMA SURFACE COURSE MIX "D" N70 EXTENDING 6" INTO SHOULDER, TRIM SHOULDER JOINT, PLACE 3" HMA SURFACE COURSE IL-9.5FG N50 ON SHOULDER. ONCE THIS IS COMPETE, SWITCH TRAFFIC AND PERFORM THE SAME SEQUENCE.

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|------------|--------------------|----------|---|
| USER NAME  | = oltmancr         | DESIGNED | - |
|            |                    | DRAWN    | - |
| PLOT SCALE | = 140,0000 ' / in. | CHECKED  | - |
| PLOT DATE  | = 6/3/2019         | DATE     | - |
|            |                    | REVISED  | - |

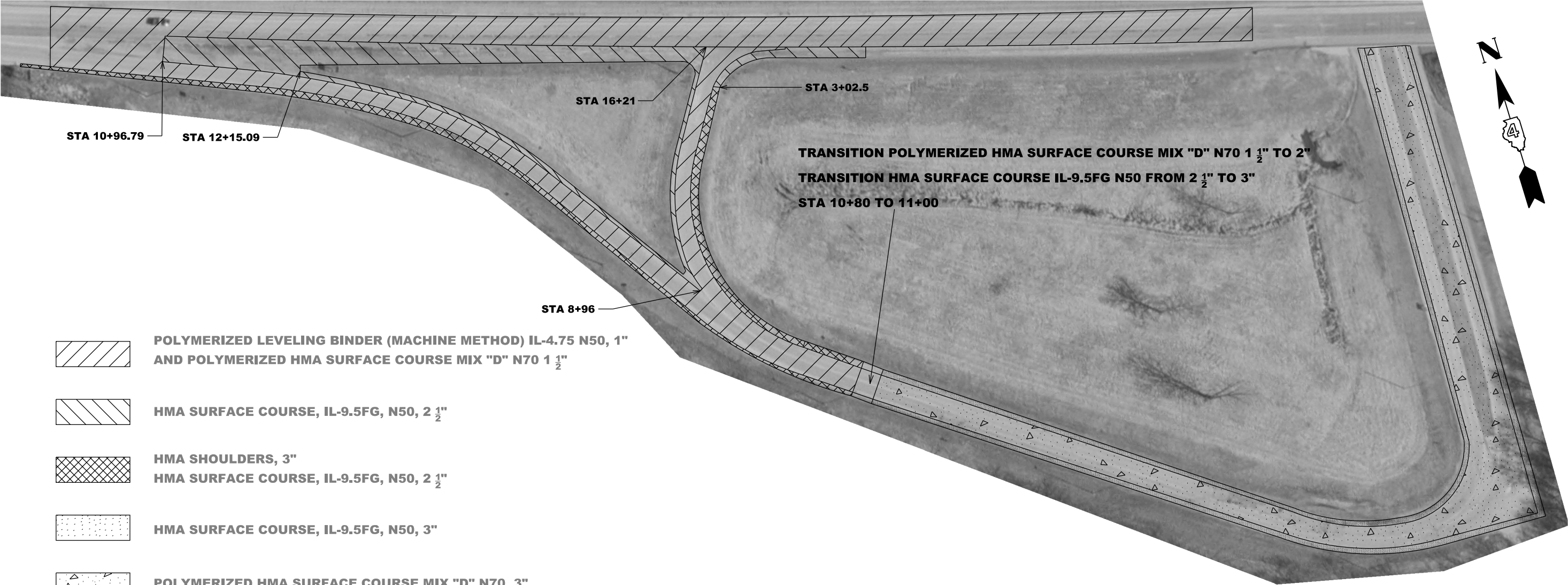
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

| STAGING DETAILS |       |    |         |
|-----------------|-------|----|---------|
| SCALE:          | SHEET | OF | SHEETS  |
|                 | STA.  |    | TO STA. |

| F.A. RTE.          | SECTION  | COUNTY    | TOTAL SHEETS     | SHEET NO. |
|--------------------|----------|-----------|------------------|-----------|
| 313                | (8R)RS-2 | HENDERSON | 34               | 23        |
| CONTRACT NO. 68F10 |          |           |                  |           |
|                    |          | ILLINOIS  | FED. AID PROJECT |           |



EAST BOUND

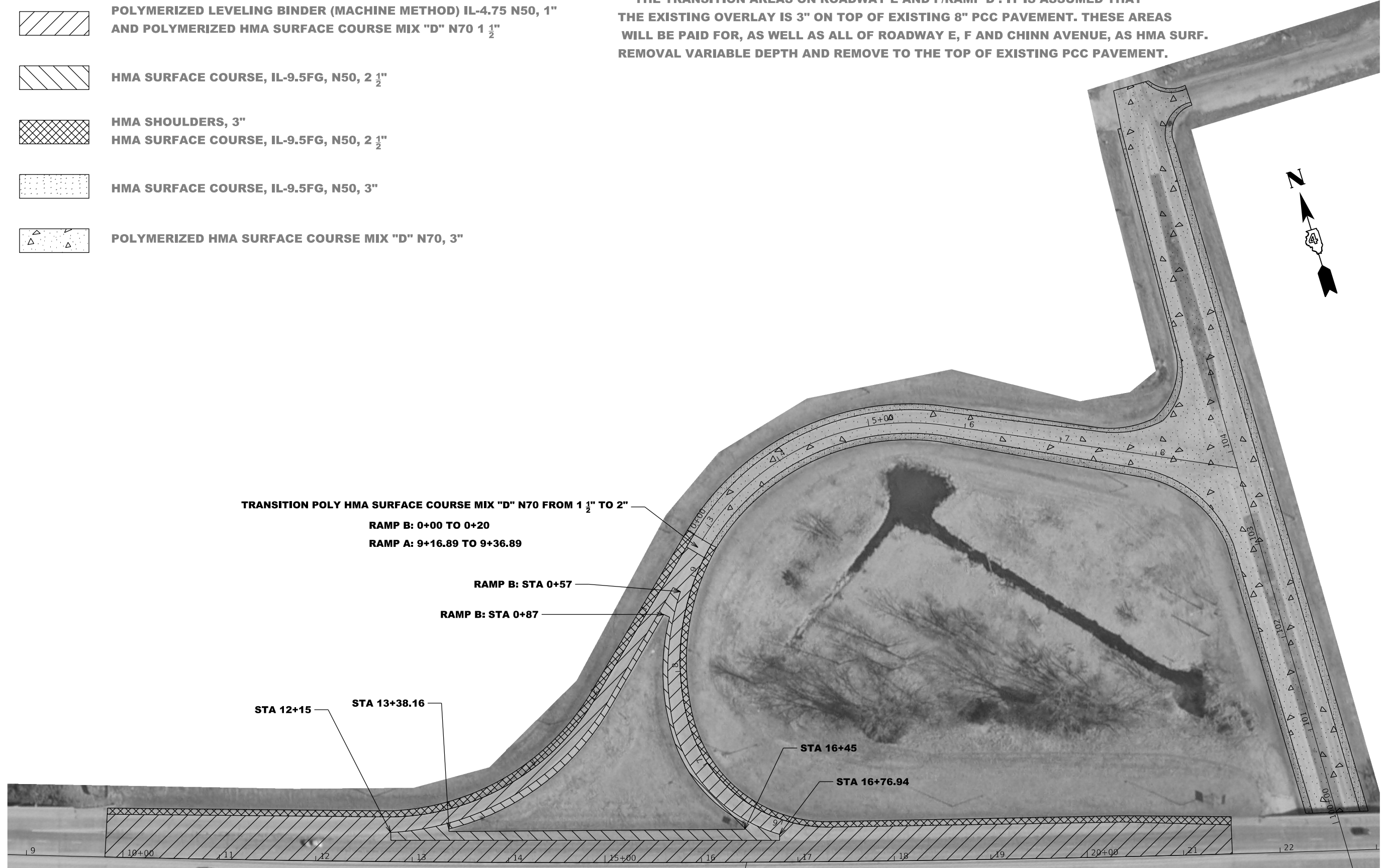
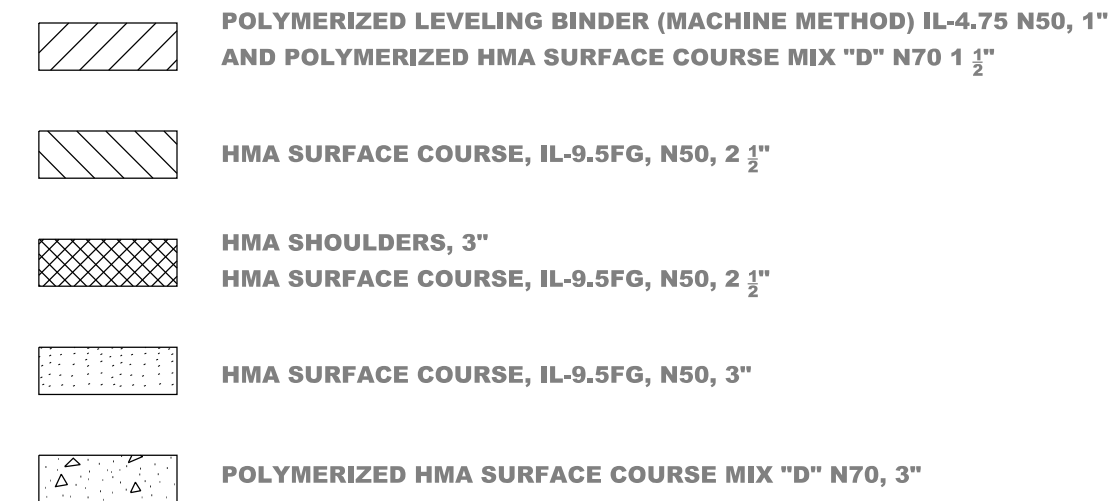


**\*\* THE TRANSITION AREAS ON ROADWAY E AND F/RAMP D : IT IS ASSUMED THAT THE EXISTING OVERLAY IS 3" ON TOP OF EXISTING 8" PCC PAVEMENT. THESE AREAS WILL BE PAID FOR, AS WELL AS ALL OF ROADWAY E, F AND CHINN AVENUE, AS HMA SURF. REMOVAL VARIABLE DEPTH AND REMOVE TO THE TOP OF EXISTING PCC PAVEMENT.**

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|--|---------------------------------|--------------|-------------|---|------------------------|-------|----|--------|------|--------------|---------|-----------|------------------|--------------|
|  | USER NAME   = oltmancr          | DESIGNED   - | REVISED   - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | GENERAL PLAN & DETAILS |       |    |        |      | F.A.<br>RTE. | SECTION | COUNTY    | TOTAL<br>SHEETS  | SHEET<br>NO. |
|  | PLOT SCALE   = 140,0000 ' / in. | DRAWN   -    | REVISED   - |   |                        |       |    |        |      | 313          | (8)RS-2 | HENDERSON | 34               | 24           |
|  | PLOT DATE   = 6/3/2019          | CHECKED   -  | REVISED   - |   | CONTRACT NO. 68F10     |       |    |        |      |              |         |           |                  |              |
|  |                                 | DATE   -     | REVISED   - |   | SCALE:                 | SHEET | OF | SHEETS | STA. | TO           | STA.    | ILLINOIS  | FED. AID PROJECT |              |

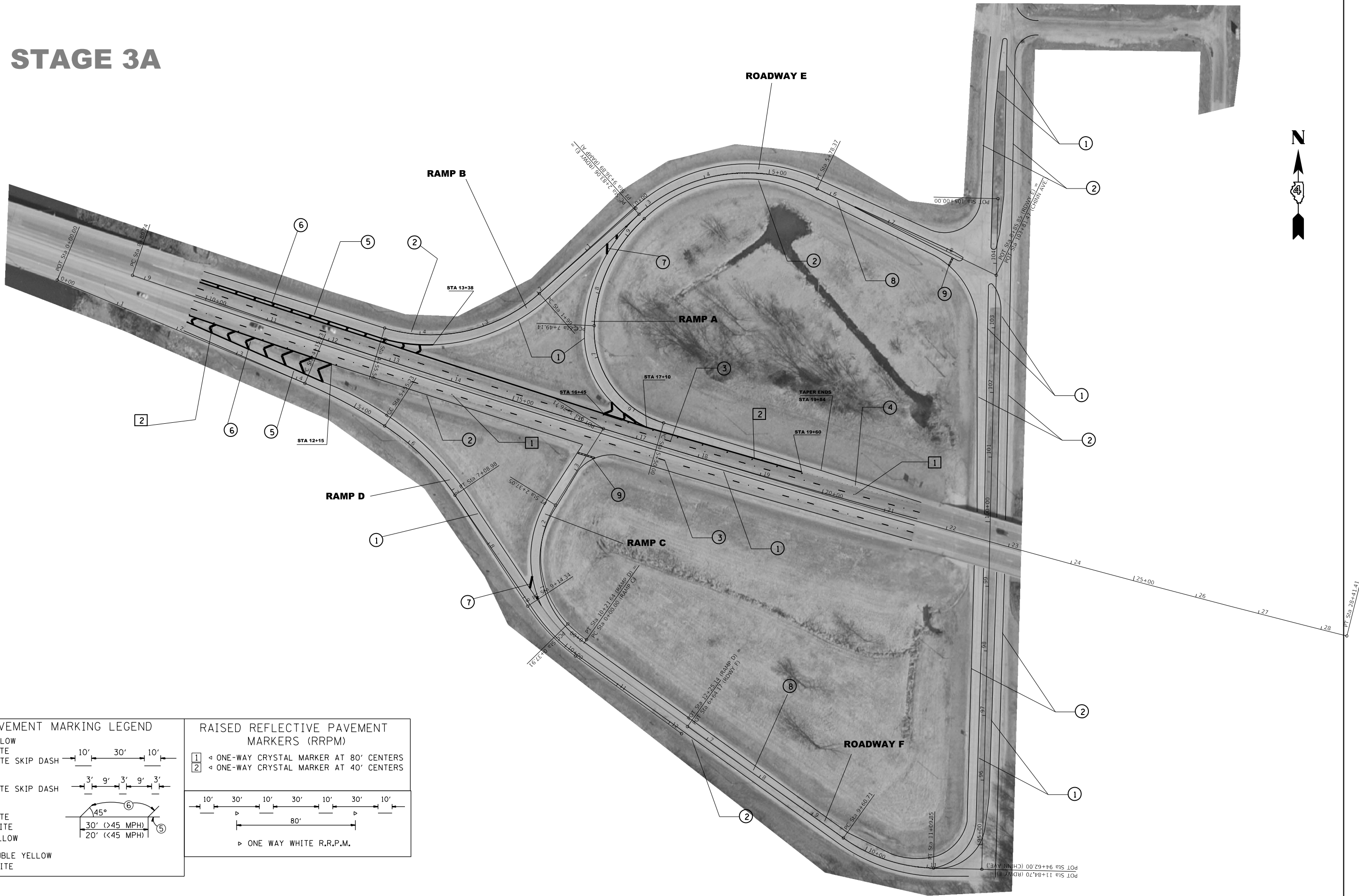
**\*\* THE TRANSITION AREAS ON ROADWAY E AND F/RAMP D : IT IS ASSUMED THAT THE EXISTING OVERLAY IS 3" ON TOP OF EXISTING 8" PCC PAVEMENT. THESE AREAS WILL BE PAID FOR, AS WELL AS ALL OF ROADWAY E, F AND CHINN AVENUE, AS HMA SURF. REMOVAL VARIABLE DEPTH AND REMOVE TO THE TOP OF EXISTING PCC PAVEMENT.**



# WEST BOUND

|                |                              |                 |              |   |                        |                    |         |           |              |           |
|----------------|------------------------------|-----------------|--------------|---|------------------------|--------------------|---------|-----------|--------------|-----------|
| FILE NAME: DMS | USER NAME = noltera          | DESIGNED -      | REVISED -    | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | GENERAL PLAN & DETAILS | F.A. RTE.          | SECTION | COUNTY    | TOTAL SHEETS | SHEET NO. |
|                |                              | DRAWN -         | REVISED -    |   |                        | 313                | (B)RS-2 | HENDERSON | 34           | 25        |
|                | PLOT SCALE = 140,0000' ± in. | CHECKED -       | REVISED -    |   |                        | CONTRACT NO. 68F10 |         |           |              |           |
|                | PLOT DATE = 5/31/2019        | DATE -          | REVISED -    |   |                        |                    |         |           |              |           |
|                | SCALE:                       | SHEET OF SHEETS | STA. TO STA. |   |                        |                    |         |           |              |           |

STAGE 3A



| PAVEMENT MARKING LEGEND |                    |
|-------------------------|--------------------|
| ①                       | 4" YELLOW          |
| ②                       | 4" WHITE           |
| ③                       | 6" WHITE SKIP DASH |
| ④                       | 8" WHITE SKIP DASH |
| ⑤                       | 8" WHITE           |
| ⑥                       | 12" WHITE          |
| ⑦                       | 12" YELLOW         |
| ⑧                       | 4" DOUBLE YELLOW   |
| ⑨                       | 24" WHITE          |

| RAISED REFLECTIVE PAVEMENT MARKERS (RRPM) |                                       |
|---|---------------------------------------|
| ①   | ONE-WAY CRYSTAL MARKER AT 80' CENTERS |
| ②   | ONE-WAY CRYSTAL MARKER AT 40' CENTERS |

|   |                        |
|---|------------------------|
| ① | ONE WAY WHITE R.R.P.M. |
|---|------------------------|

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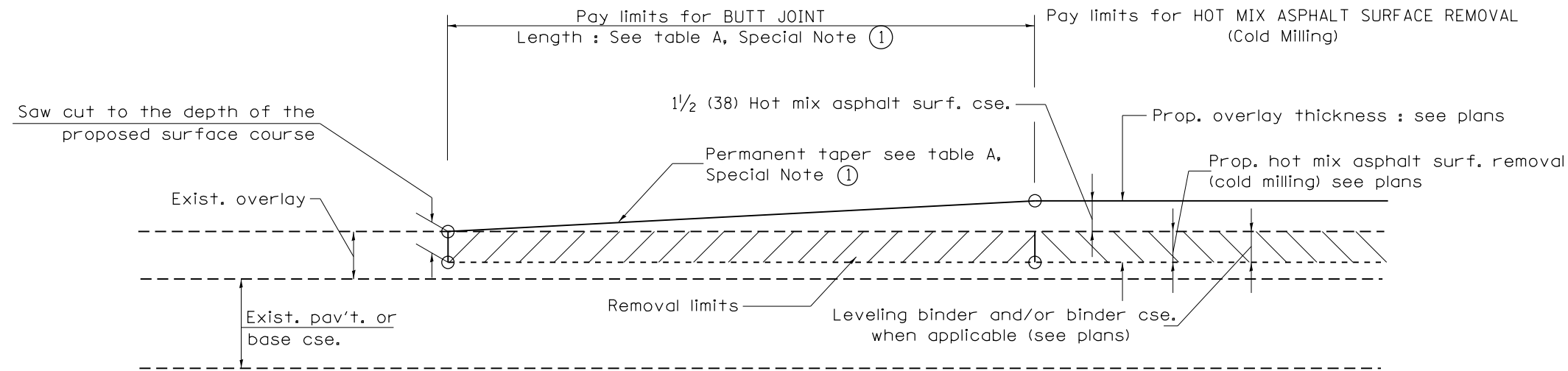
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| USER NAME  | = altmancr         | DESIGNED | - | REVISED | - |
| DRAWN      | -                  | REVIS    | - | REVIS   | - |
| PLOT SCALE | = 140,0000 ' / in. | CHECKED  | - | REVISED | - |
| PLOT DATE  | = 5/31/2019        | DATE     | - | REVISED | - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A. RTE.                 | SECTION  | COUNTY    | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|-----------|--------------|-----------|
| 313                       | (8R)RS-2 | HENDERSON | 34           | 26        |
| CONTRACT NO. 68F10        |          |           |              |           |
| ILLINOIS FED. AID PROJECT |          |           |              |           |



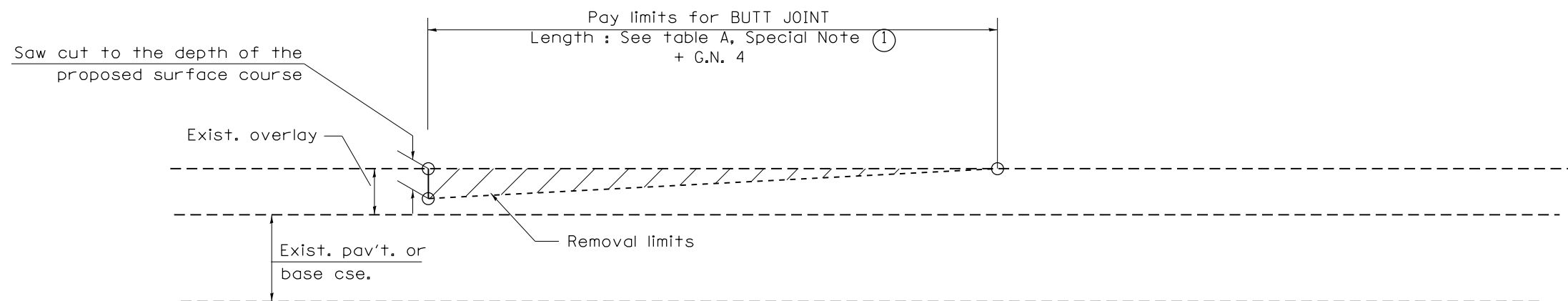
**CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

**TABLE A  
TAPER RATES**

| SPECIAL NOTE NUMBER | ELEMENT                   | MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS | ALL OTHERS |
|---------------------|---------------------------|---|------------|
| ①                   | BUTT JOINT TAPER RATE     | 1:480                                     | 1:240      |
| ②                   | TEMPORARY RAMP TAPER RATE | 1:80                                      | 1:40       |

**GENERAL NOTES**

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.

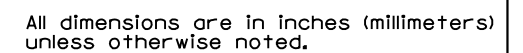


**CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

All dimensions are in inches (millimeters) unless otherwise noted.

|          |                                  |        |          |                     |      |   |   |                     |                           |           |              |           |
|----------|----------------------------------|--------|----------|---------------------|------|---|---|---------------------|---------------------------|-----------|--------------|-----------|
| 01-01-97 | RENUM. C-23.01, NEW REVISION BOX | T.P.   | 08-21-13 | MAJOR MODIFICATIONS | R.D. | <div>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</div> <div>NOT TO SCALE</div> | <div>BUTT JOINTS</div> <div>SHT. 1 OF 3<br/>CADD STD. 406101-D4</div> | F.A. RTE.           | SECTION                   | COUNTY    | TOTAL SHEETS | SHEET NO. |
| 04-01-97 | CORRECTION TO DEPTH              | J.A.   | 02-29-16 | MINOR CORRECTIONS   | R.D. |   |   | 313                 | (8R)RS-2                  | HENDERSON | 34           | 27        |
| 09-15-05 | REVISED DESIGNER NOTE            | M.M.A. | 04-12-16 | MINOR CORRECTIONS   | R.D. |   |   | CONTRACT NO. 68F10  |                           |           |              |           |
| 10-16-06 | REVISED TO 2007 SPEC.            | M.A.   | 02-14-17 | ADDED NOTE 5        | R.D. |   |   |                     |                           |           |              |           |
|          |                                  |        |          |                     |      |   |   | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |           |              |           |

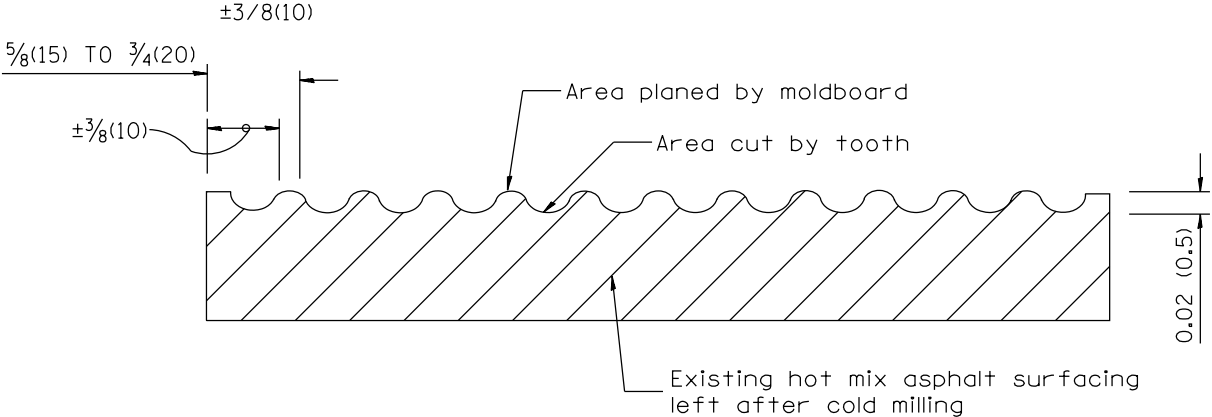
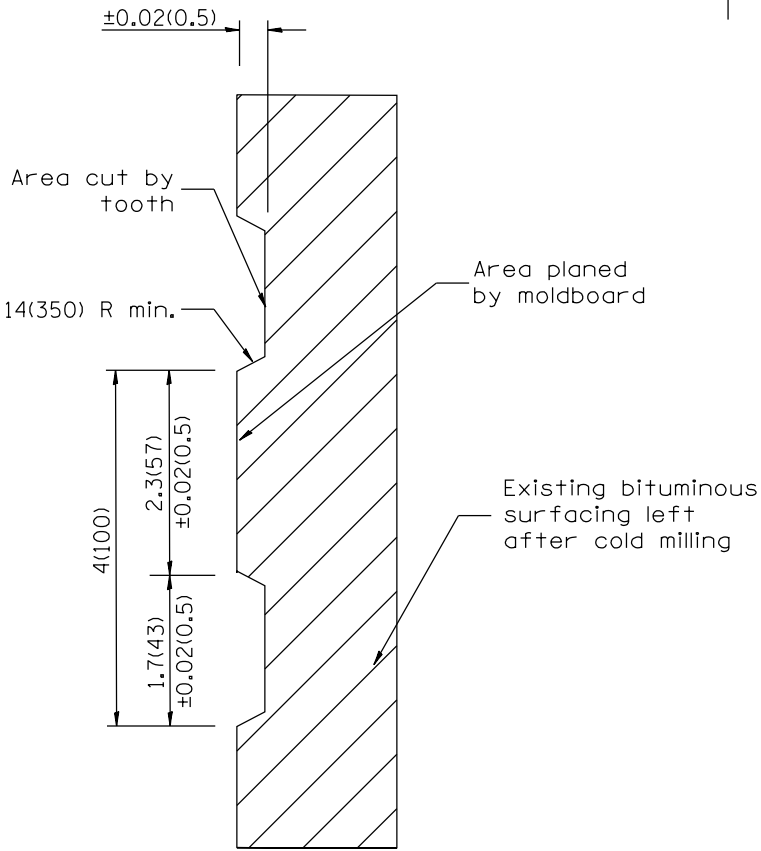
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DESIGNER NOTES:



General notes:

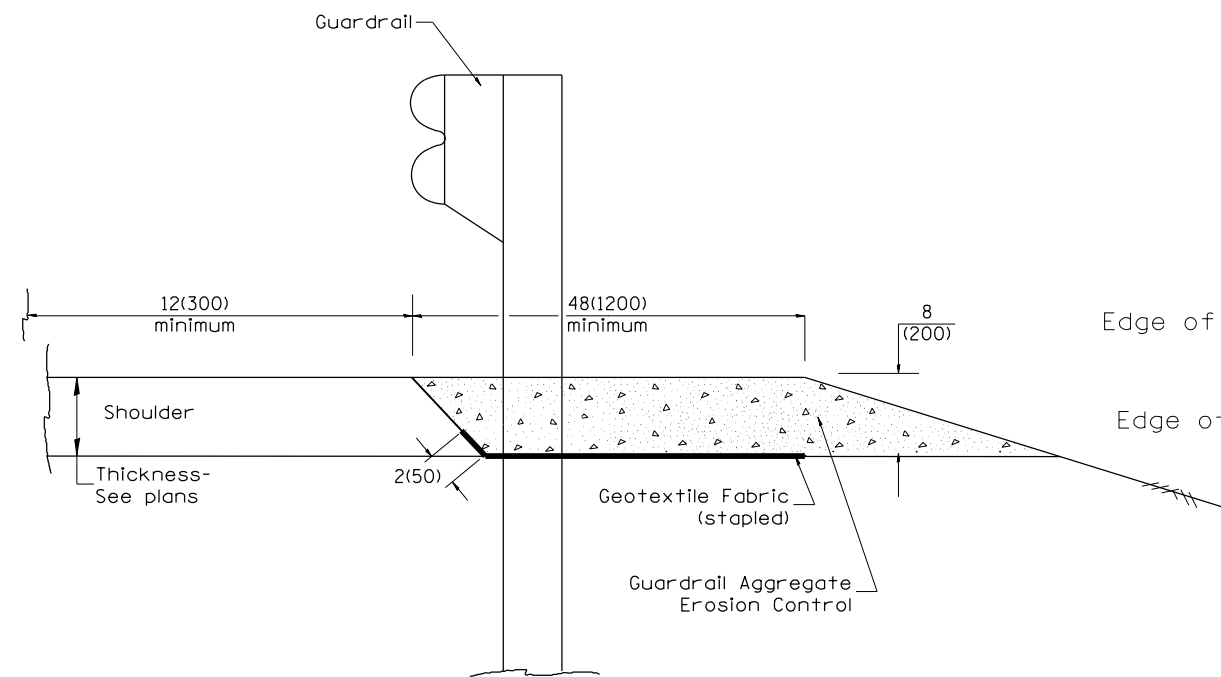
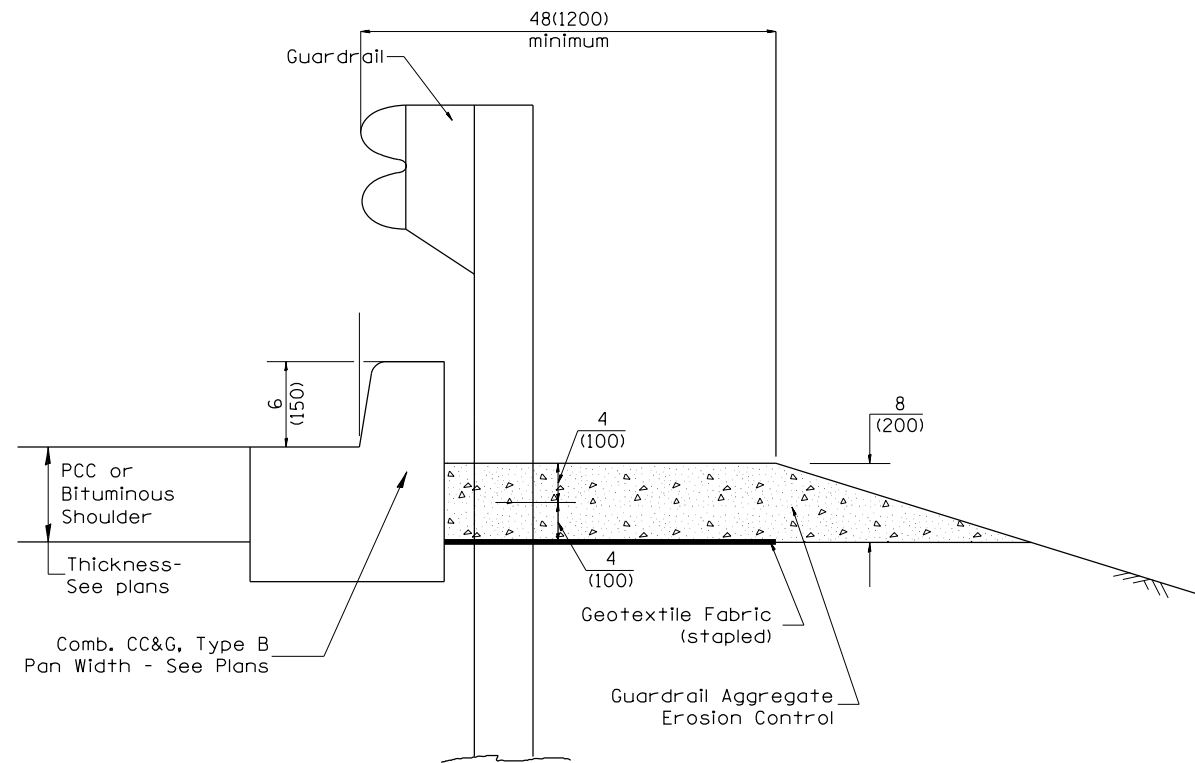
1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.

All dimensions are in inches (millimeters) unless otherwise noted.

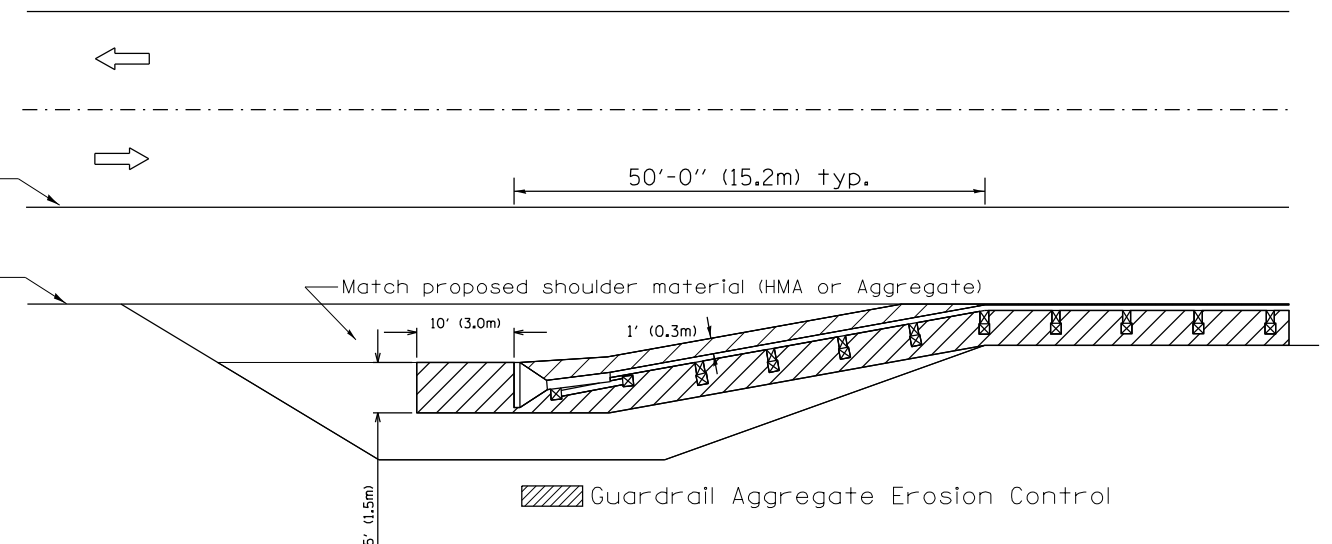
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|--------------|--------------------------------------|------|--|--|--|---|--|---|----------|-----------|-----------------|--------------|--|
| 01-01-97     | RENUM. C-104.01, NEW REVISION BOX    | T.P. |  |  |  | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING) | F.A.<br>RTE.                                  | SECTION  | COUNTY    | TOTAL<br>SHEETS | SHEET<br>NO. |  |
| 04-20-98     | REMOVED MILLING DETAIL FROM STANDARD | J.A. |  |  |  |   |  | 313   | (8R)RS-2 | HENDERSON | 34              | 30           |  |
| 09-08-98     | CORRECT NOTE LEADER PLACEMENT        | R.W. |  |  |  |   |  | CONTRACT NO. 68F10                            |          |           |                 |              |  |
| 10-16-06     | REVISED TO 2007 SPEC.                | M.A. |  |  |  |   |  | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |          |           |                 |              |  |
|              |                                      |      |  |  |  |   |  | CADD STD. 440001-D4                           |          |           |                 |              |  |
| NOT TO SCALE |                                      |      |  |  |  |   |  |   |          |           |                 |              |  |

DESIGNER NOTES:

- 1.
- 2.
- 3.
- 4.
- 5.
- 6.
- 7.
- 8.

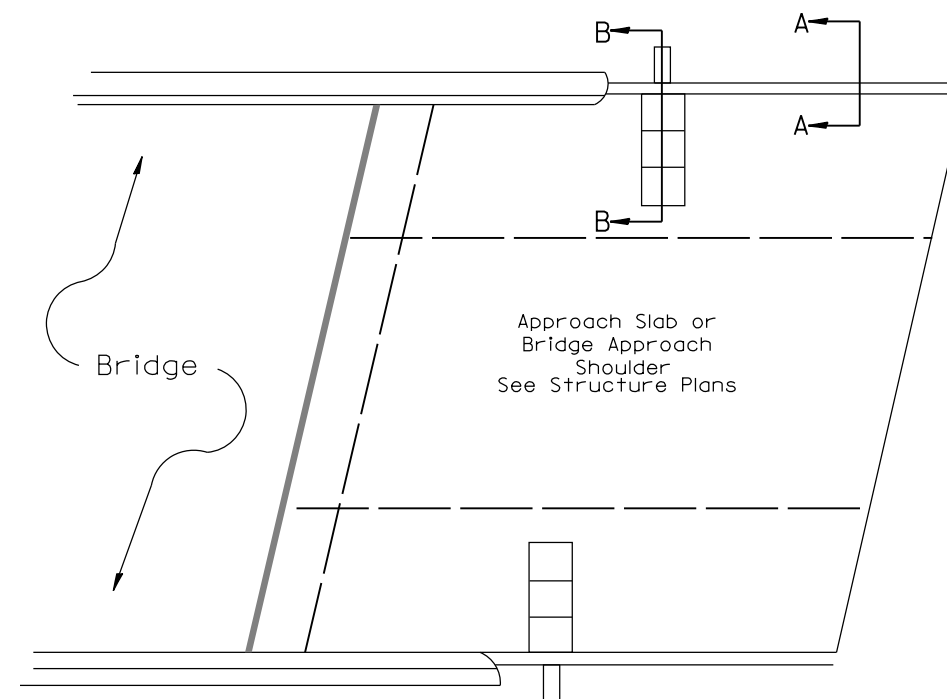
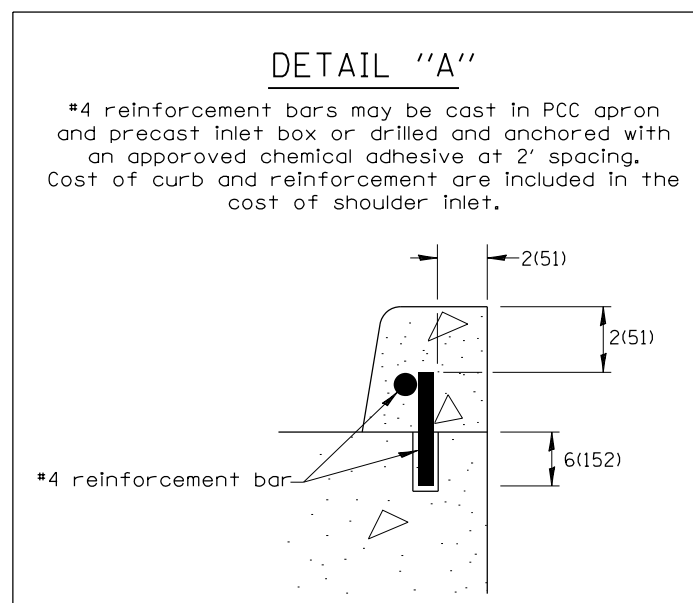


1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
  - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
  - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.

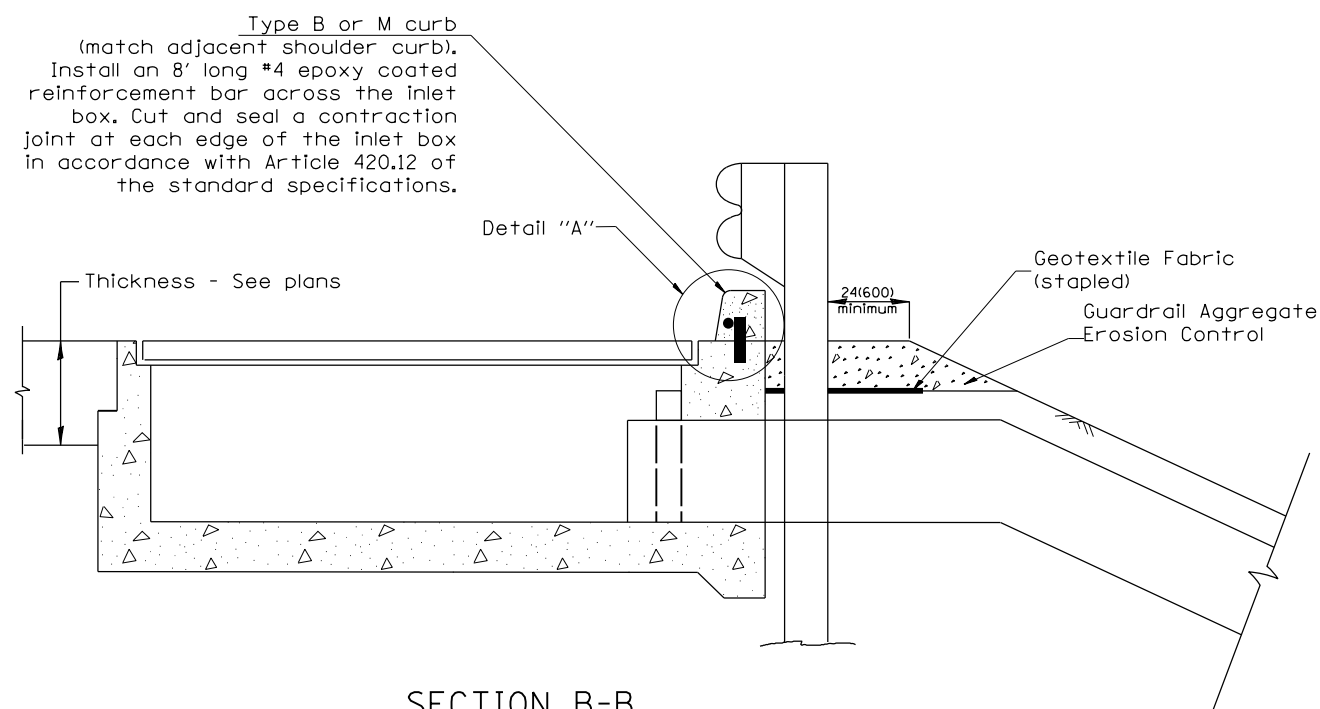


All dimensions are in inches (millimeters) unless otherwise noted.

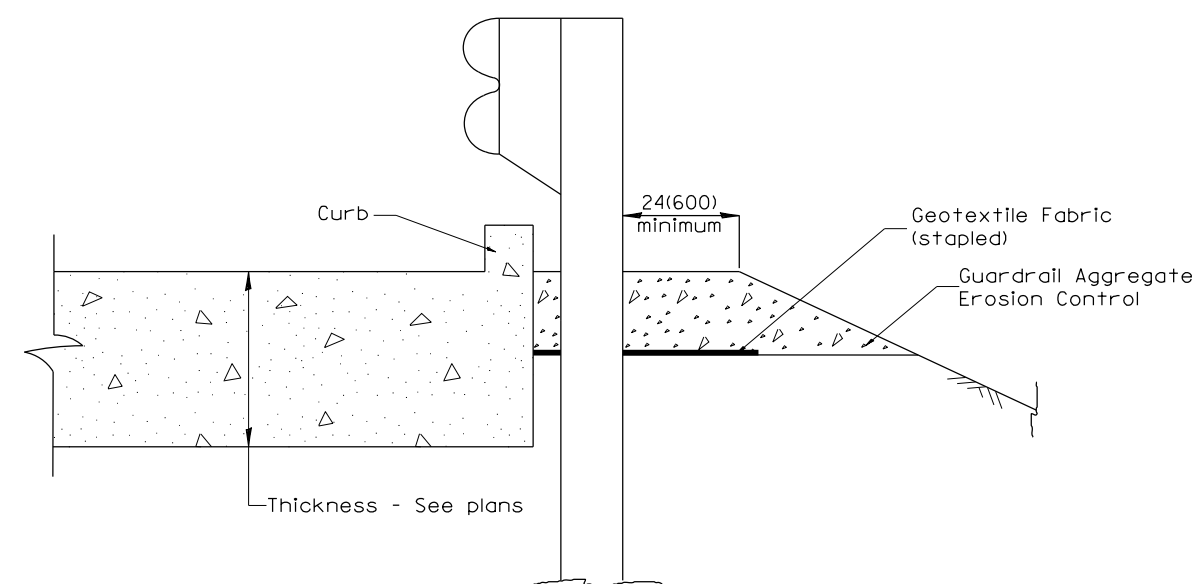
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|----------|--------------------------------|------|---------|-----------------------|------|--|--|-----------|----------|--------------------|--------------|-----------|
| 03-07-11 | ADDED DETAIL SHOWING PLAN VIEW | R.D. | 5-30-18 | CHANGE B CURB TO CC&G | R.D. | <div style="text-align: center;"> <b>STATE OF ILLINOIS</b><br/> <b>DEPARTMENT OF TRANSPORTATION</b> </div> | <div style="text-align: center;"> <b>GUARDRAIL EROSION CONTROL TREATMENTS</b> </div> | F.A. RTE. | SECTION  | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 08-10-12 | REVISED CURB "B" AND AGGREGATE | R.D. |         |                       |      |  |  | 313       | (8R)RS-2 | HENDERSON          | 34           | 31        |
| 07-15-15 | ADDRESSED SHOULDER INLET CURB  | R.D. |         |                       |      |  |  |           |          | CONTRACT NO. 68F10 |              |           |
| 01-26-17 | REVISED                        | R.D. |         |                       |      |  |  |           |          |                    |              |           |



**PLAN VIEW**  
**APPROACH SLAB OR SHOULDER PLACEMENT**



**SECTION B-B**  
**TYPICAL SECTION AT INLETS**  
**TYPE E, F & G (HIGHWAY STANDARD 610001)**

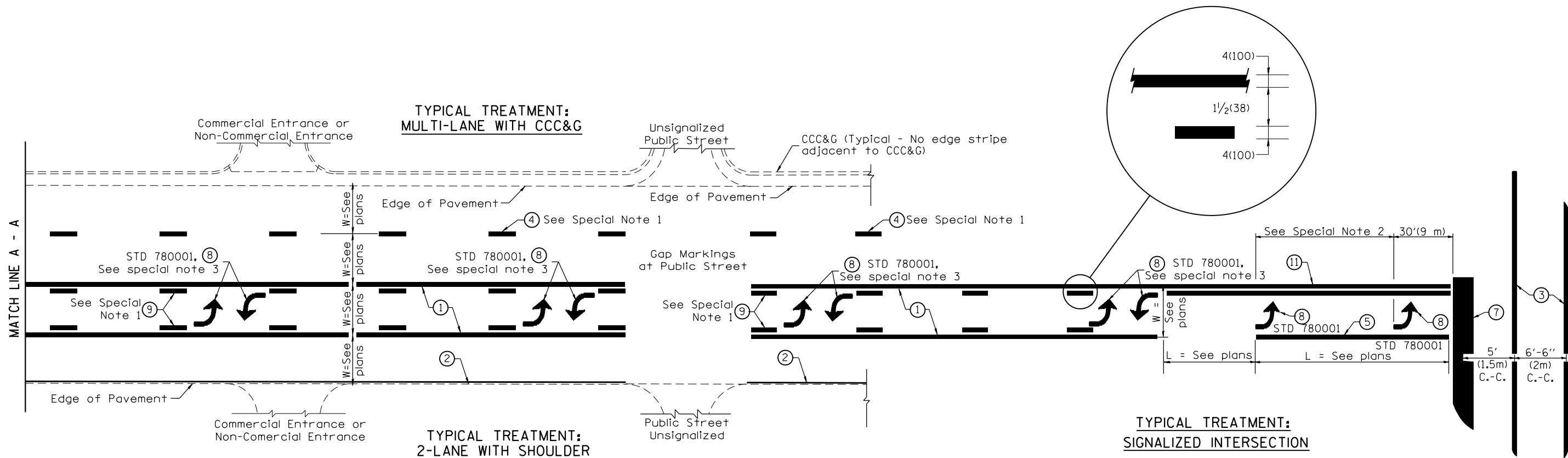


**SECTION A-A**  
**TYPICAL SECTION WITH BRIDGE APPROACH CURB**

All dimensions are in inches (millimeters) unless otherwise noted.

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DESIGNER NOTES:  
1. Include State Standard 780001 (Typical Pavement Markings)



**FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION**

**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑪ 4(100) Double Solid (Yellow) See Table A

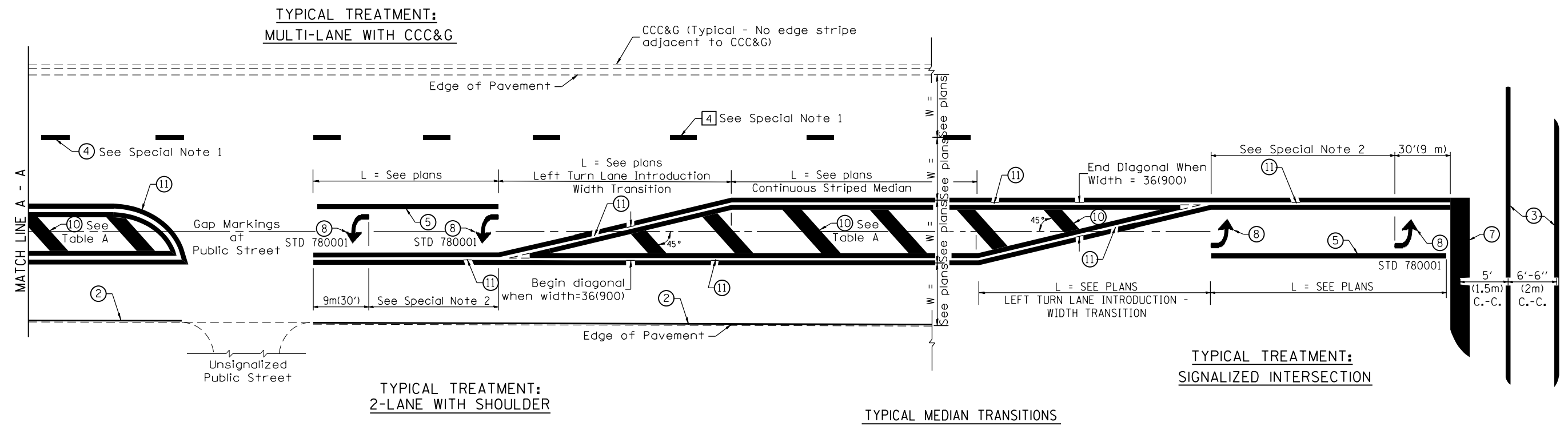
**SPECIAL NOTES**

- Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
- The following shall apply to arrows located in one-way left turn lanes:
  - A minimum of two (2) arrows is required.
  - The maximum spacing between arrows is 80' (24 m).
  - Arrows shall be evenly spaced if three (3) or more are required.
- The following shall apply to arrow pairs located in two-way left turn lanes:
  - A minimum of two (2) arrow pairs is required.
  - The maximum spacing between arrow pairs is 200' (61 m).
  - Arrow pairs shall be evenly spaced if three (3) or more are required.
  - The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

**GENERAL NOTES**

- Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
- See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
- Refer to Article 780.13 for letter, number and symbol areas (sq. ft.).
- Areas are grooved 1" beyond each edge for the following symbols:  
Through Arrow= 14.8 sq. ft.  
Large Left or Right Arrow= 21.9 sq. ft.  
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.  
Wrong Way Arrow= 29.5 sq. ft.  
Railroad Crossing Symbol= 69.8 sq. ft.  
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

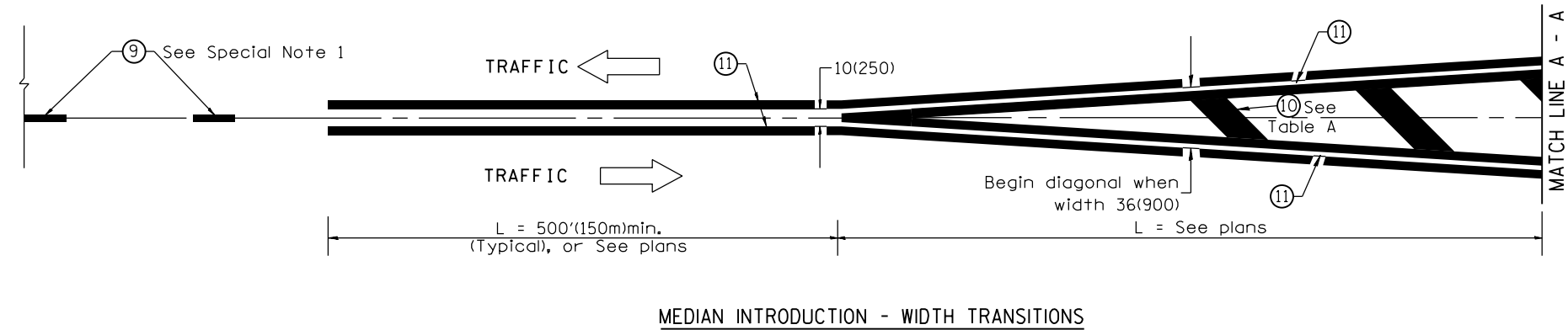
|          |                                  |      |          |                       |      |   |                                    |                           |         |        |                 |              |
|----------|----------------------------------|------|----------|-----------------------|------|---|------------------------------------|---------------------------|---------|--------|-----------------|--------------|
| 01-01-97 | RENUM. F-8.03, NEW REVISION BOX  | T.P. | 10-16-06 | REVISED TO 2007 SPEC. |      | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | TYPICAL PAVEMENT MARKINGS          | F.A.<br>RTE.              | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| 02-07-97 | ADD BI DIRECTIONAL DIMENSION     | J.A. | 2/29/16  | ADDED GROOVING AREAS  | R.D. |   |                                    |                           |         |        |                 |              |
| 10-97    | CORRECT BI DIRECTIONAL DIMENSION | J.A. |          |                       |      |   |                                    |                           |         |        |                 |              |
| 08-02    | ADD CROSSWALK DMNS. WITH T.S.    | M.A. |          |                       |      |   |                                    |                           |         |        |                 |              |
|          |                                  |      |          |                       |      | NOT TO SCALE                                      | SHT. 1 OF 2<br>CADD STD. 780001-D4 | ILLINOIS FED. AID PROJECT |         |        |                 |              |



# FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

**TABLE A**  
**RECOMMENDED SPACING BETWEEN DIAGONAL LINES**

| SPEED LIMIT RANGE          | CONTINUOUS | INTERSECTION CHANNELIZATION<br>(Includes Width Transitions for<br>Median and Left Turn Lane<br>Introductions) |
|----------------------------|------------|---|
| Less Than 30 mph (50 km/h) | 50' (15m)  | 15' (5m)  |
| 30 - 45 mph (50 - 70 km/h) | 75' (23m)  | 20' (6m)  |
| Over 45 mph (70 km/h)      | 150' (46m) | 30' (9m)  |



All dimensions are in inches (millimeters) unless otherwise noted.