



Illinois Department of Transportation

Office of Intermodal Project Implementation / Division of Aeronautics
1 Langhorne Bond Drive / Springfield, Illinois 62707-8415

June 10, 2022

SUBJECT: Quincy Regional Airport
Quincy, Illinois
Adams County
Illinois Project Number: UIN-4953
AIP Project Number: 3-17-0085-TBD
Contract No. QI064
Item No. 05A, June 17, 2022 Letting
Addendum B

NOTICE TO PROSPECTIVE BIDDERS

Attached is an addendum to the plans or proposal. This addendum involves revised and/or added material.

Reason for Addendum:
Correction to notes

To All Plan Holders:
The following changes to the bid documents dated 4/22/2022 are included in Addendum B:

1. Correct typo on plan sheet 22.

Plan Changes:

- Sheet 22, General Grooving Notes
 - REVISE: Note 3 to read "...NO MORE THAN 18 INCHES..."

Specification Changes:

None

Schedule of Prices Changes:

None

Prime contractors must utilize the enclosed material when preparing their bid and must include any changes to the Schedule of Prices in their bid.

Questions on this addendum may be directed to Wes loerger of CMT at 217-787-8050.

Attachments

- Revised Plan Sheet 22

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

BID ISSUE
APRIL 22, 2022

RECONSTRUCT RUNWAY 4/22
PHASE 3

OWNER



CITY OF QUINCY
QUINCY REGIONAL AIRPORT
QUINCY, IL

B 6/10/22 ADDENDUM B

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0085-XX

IL PROJ. NO: UIN-XXXX

CMT PROJECT NO: 18002001

CAD DWG FILE: 18002001-RWY22-CP501.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: CHK

APPROVED BY: APR

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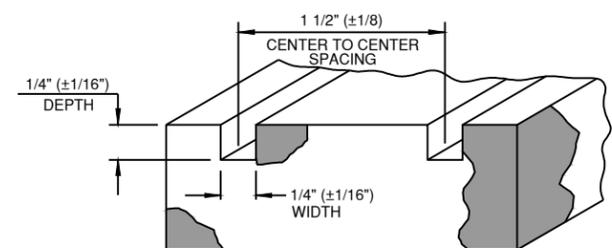
SHEET TITLE

GROOVING DETAILS

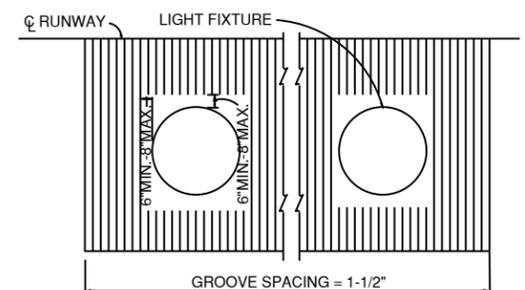
CP502
SHEET 22 OF 62

GENERAL GROOVING NOTES:

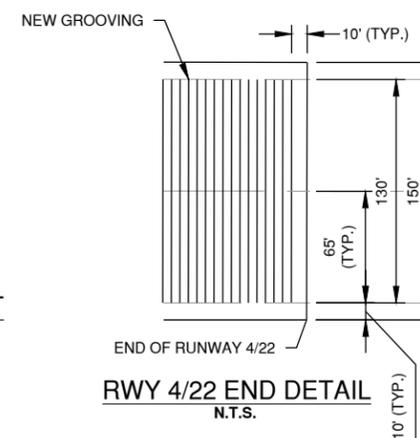
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SECURE AN APPROPRIATE WATER SOURCE FOR GROOVING OPERATIONS.
- GROOVES SHALL RUN TRANSVERSE TO DIRECTION OF TRAFFIC FLOW. GROOVES SHALL EXTEND OUTWARD 65' FROM RUNWAY CENTERLINE.
- GROOVES SHALL BE SAWED NO CLOSER THAN 6 INCHES AND NO MORE THAN 18 INCHES FROM IN-PAVEMENT LIGHT FIXTURES.
- THE CONTRACTOR SHALL GROOVE A TEST SECTION TO DEMONSTRATE THAT THE EQUIPMENT AND OPERATOR WILL PROVIDE THE REQUIRED CONFIGURATION WITHIN THE PRESCRIBED TOLERANCES.
- THE FOLLOWING TOLERANCES WILL BE PERMITTED FOR P-401:
 - ±1-1/2" IN ALIGNMENT PER 75' GROOVE.
 - ±1/16" IN DEPTH OF GROOVE.
 - +1/16" -0" WIDTH OF GROOVE.
 - 1/8", +0" IN CENTER SPACING BETWEEN GROOVES
 - 60% OR MORE OF THE GROOVES SHALL NOT BE LESS THAN 1/8" DEPTH
- CONTRACTOR SHALL NOT AT ANY TIME, BE ALLOWED TO DUMP THE GROOVING SLURRY SEAL IN THE AIRFIELD GRASS AREA. GROOVING SHALL USE A VACUUM TRUCK TO REMOVE ALL SLURRY.
- CLEANUP IS EXTREMELY IMPORTANT AND SHALL BE CONTINUOUS THROUGHOUT GROOVING OPERATIONS. THE WASTE MATERIAL COLLECTED DURING THE GROOVING OPERATION MUST BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY.
- GROOVING SHALL NOT COMMENCE UNTIL THE ASPHALT SURFACE HAS CURED FOR THIRTY (30) DAYS. SURFACE SHALL BE THOROUGHLY FLUSHED AND CLEARED PRIOR TO PERMANENT PAVEMENT MARKING.
- AREAS WHERE GROOVING IS PERFORMED SHALL RECEIVE A DOUBLE APPLICATION OF PAVEMENT MARKING. THE SECOND APPLICATION SHALL BE APPLIED OPPOSITE TO THE FIRST APPLICATION.



SAW-CUT GROOVING CONFIGURATION
N.T.S.



TYPICAL GROOVING DETAIL ADJACENT TO INPAVEMENT LIGHTING
N.T.S.



RWY 4/22 END DETAIL
N.T.S.

BITUMINOUS PAVEMENT GROOVING DETAIL
N.T.S.