

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	1
		ILLINOIS	CONTRACT NO. 62F83	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED
IN THE VILLAGES OF NILES AND
SKOKIE.

TRAFFIC DATAHOWARD ST.

ADT (2014) = 13,500

POSTED SPEED LIMIT = 35 MPH

**PROPOSED
HIGHWAY PLANS**

**F.A.U. ROUTE 1334 – HOWARD ST.
W. OF LEHIGH AVE. TO NEW GROSS POINT RD.**

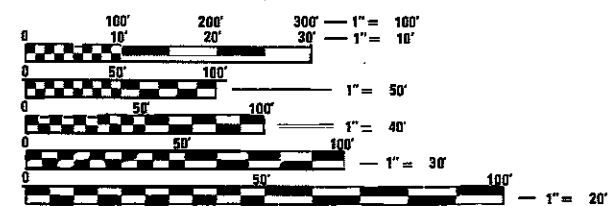
SECTION: 2017-049RS

RESURFACING (3P); PEDESTRIAN RAMPS

PROJECT: STP-JM2Q(072)

COOK COUNTY

C-91-060-18

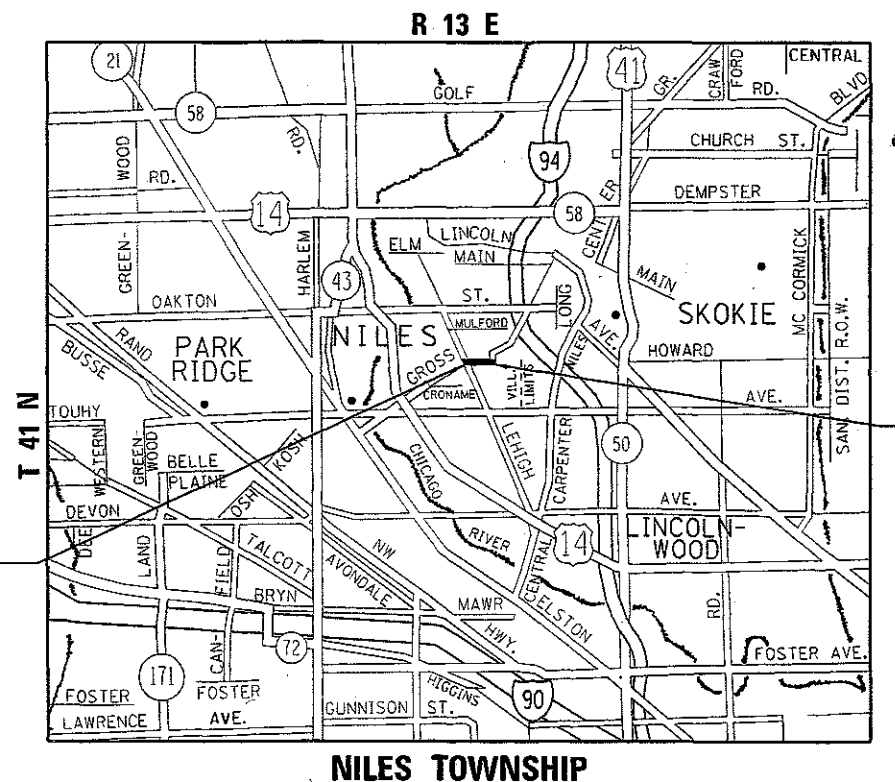


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER J. ALAIN MIDY (847) 221-3056
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F83

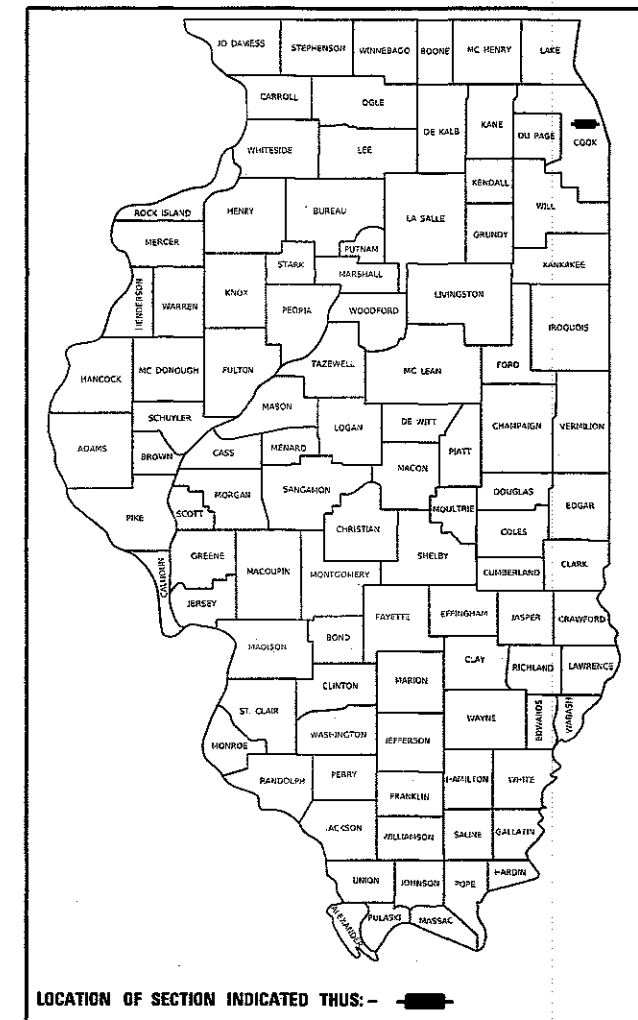


IMPROVEMENT BEGINS
STA. 14+13.4

IMPROVEMENT
ENDS STA. 32+44

GROSS & NET LENGTH = 1787.6 FT. = 0.338 MILES

D-91-211-18



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED MARCH 13 2018
Anthony P. [Signature] REGIONAL ENGINEER
May 11 2018
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT
May 11 2018
[Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
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5	TYPICAL SECTION
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10	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
13	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
15	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
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20	DRIVEWAY ENTRANCE SIGNING (TC-26)
21	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-7)

HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
442201-03	CLASS C AND D PATCHES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, > 15' AWAY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS < 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W, 2W, WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGES OF NILES AND SKOKIE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, IDOT'S AREA TRAFFIC FIELD ENGINEER VIA EMAIL AT DON.CHIARUGI@ILLINOIS.GOV OR AT (847)741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

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PLOT DATE = 2/28/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	2
CONTRACT NO. 62F83				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	80% FED 20% STATE 0005	CONSTRUCTION TYPE CODE 0005			
CODE NO	ITEM	UNIT						
20200100	EARTH EXCAVATION	CU YD	5	5				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	15	15				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1	1				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1	1				
25200110	SODDING, SALT TOLERANT	SO YD	15	15				
25200200	SUPPLEMENTAL WATERING	UNIT	1	1				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	9233	9233				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	21	21				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	565	565				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	259	259				
40601003	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	95	95				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1150	1150				
42001300	PROTECTIVE COAT	SO YD	95	95				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	623	623				
42400800	DETECTABLE WARNINGS	SO FT	73	73				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	13679	13679				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	80% FED 20% STATE 0005	CONSTRUCTION TYPE CODE 0005			
CODE NO	ITEM	UNIT						
44000600	SIDEWALK REMOVAL	SO FT	623	623				
44002215	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3 3/4"	SO YD	237	237				
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	4	4				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	105	105				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	128	128				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5	5				
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3				
67100100	MOBILIZATION	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2104	2104				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	967	967				

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USER NAME = pyzhanovskir

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PLLOT SCALE = 100,000' / 1in.

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DATE -

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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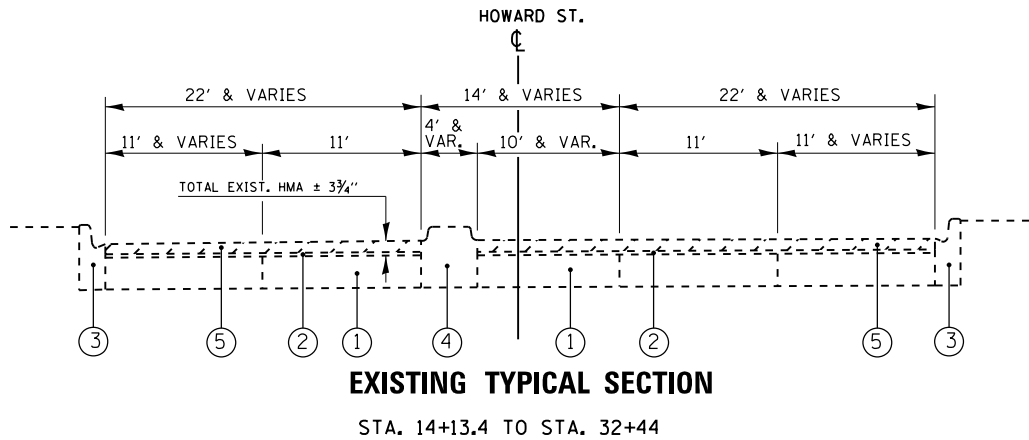
F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
21 3

CONTRACT NO. 62F83

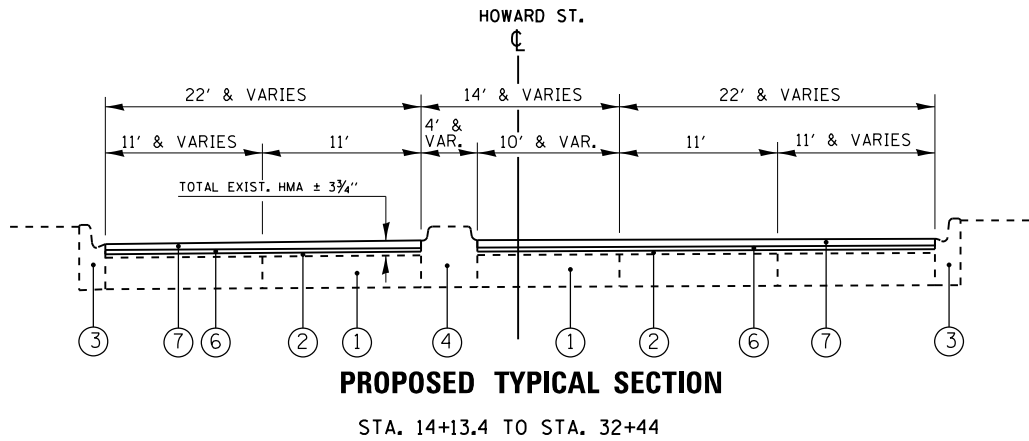
SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	80% FED 20% STATE 0005	CONSTRUCTION TYPE CODE 0005																
CODE NO	ITEM	UNIT																			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	450	450						△	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	600	600						
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1925	1925								X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	33	33					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1440	1440								X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	3431	3431					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	210	210								Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	213	213					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1117	1117							△	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	20	20					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	406	406								Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	2	2					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	450	450								Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	154.2	154.2					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1925	1925																	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1440	1440																	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	210	210																	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1117	1117																	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	406	406																	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	77	77																	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	77	77																	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1209	1209																	
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1						7											

FILE NAME =		USER NAME = jpr2010wsktrb	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		PLOT SCALE = 100,0000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 62F83				FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
		PLOT DATE = 3/20/2018	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.			

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- LEGEND**
- ① EXISTING PCC PAVEMENT (10" AND VARIES)
 - ② EXISTING HMA SURFACE COURSE REMAINING, ± 1½"
 - ③ EXISTING COMB. CURB AND GUTTER TYPE B-6.12
 - ④ EXISTING CONCRETE MEDIAN
 - ⑤ PROPOSED HMA SURFACE REMOVAL, 2¼"
 - ⑥ PROPOSED POLY. LEVELING BINDER (MM), IL-4.75, N50, ¾"
 - ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1½"



HMA MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%) @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, (IL 9.5 mm)	4% @ 70 GYR	QC/QA
POLYMERIZED LEVELING BINDER, (MM), IL 4.75, N50	3.5% @ 50 GYR	QC/QA
PAVEMENT PATCHING		
HMA REPLACEMENT OVER PATCHES (HMA Binder IL-19.0)	4% @ 70 GYR	QC/QA
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA)		

NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO. YD. / IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76 -22" AND FOR NON POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

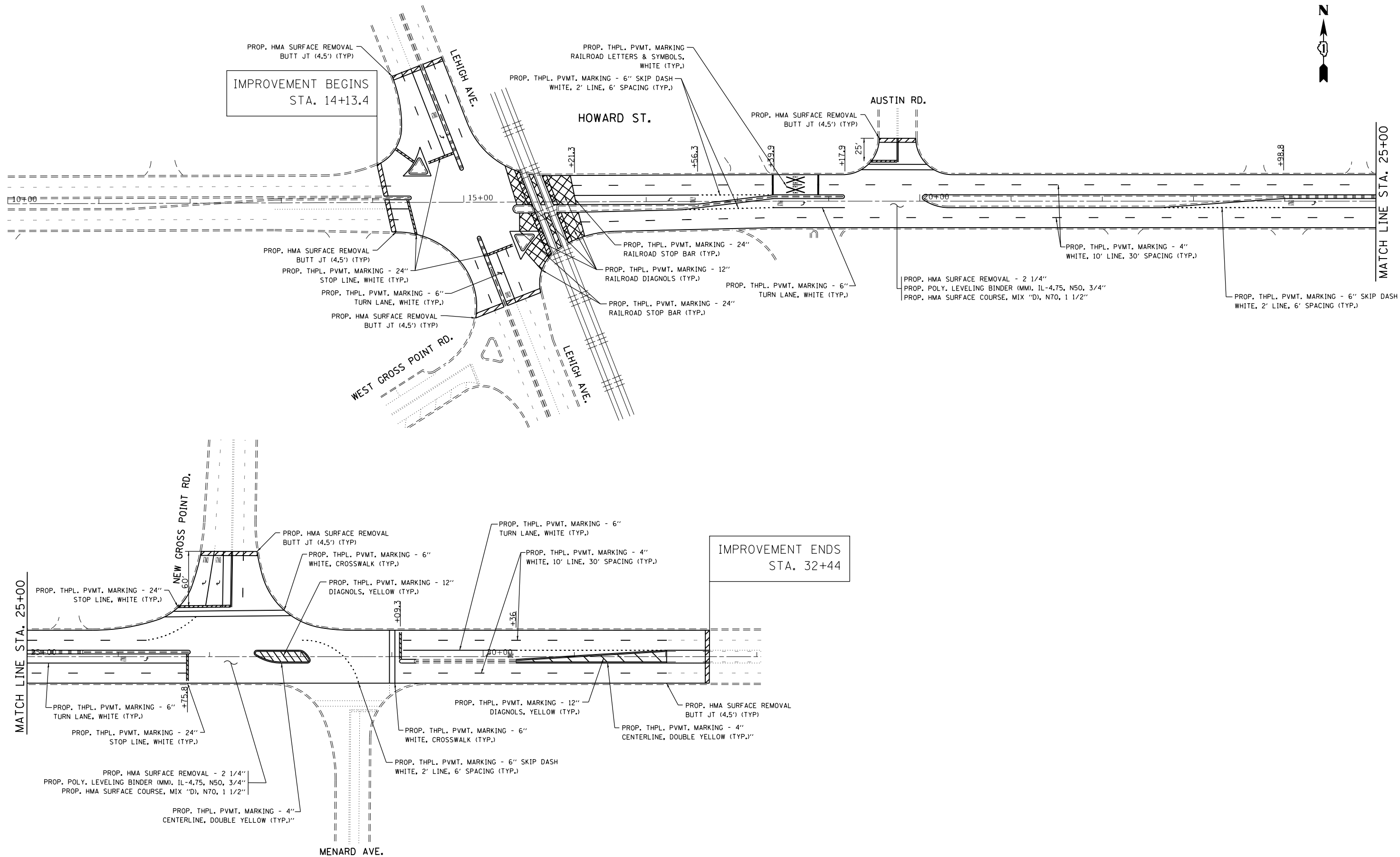
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

**THE CONTRACTOR SHALL PATCH
FIRST BEFORE MILLING**

	USER NAME = pyrzanowski	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION HOWARD ST. (W/O LEHIGH AVE. – NEW GROSS POINT RD.)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -					REVISED -	1334	2017-049RS	COOK	21	5
	PLOT SCALE = 100,0000 ' / in.	CHECKED -					REVISED -	CONTRACT NO. 62F83				
	PLOT DATE = 3/19/2018	DATE -					REVISED -	ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

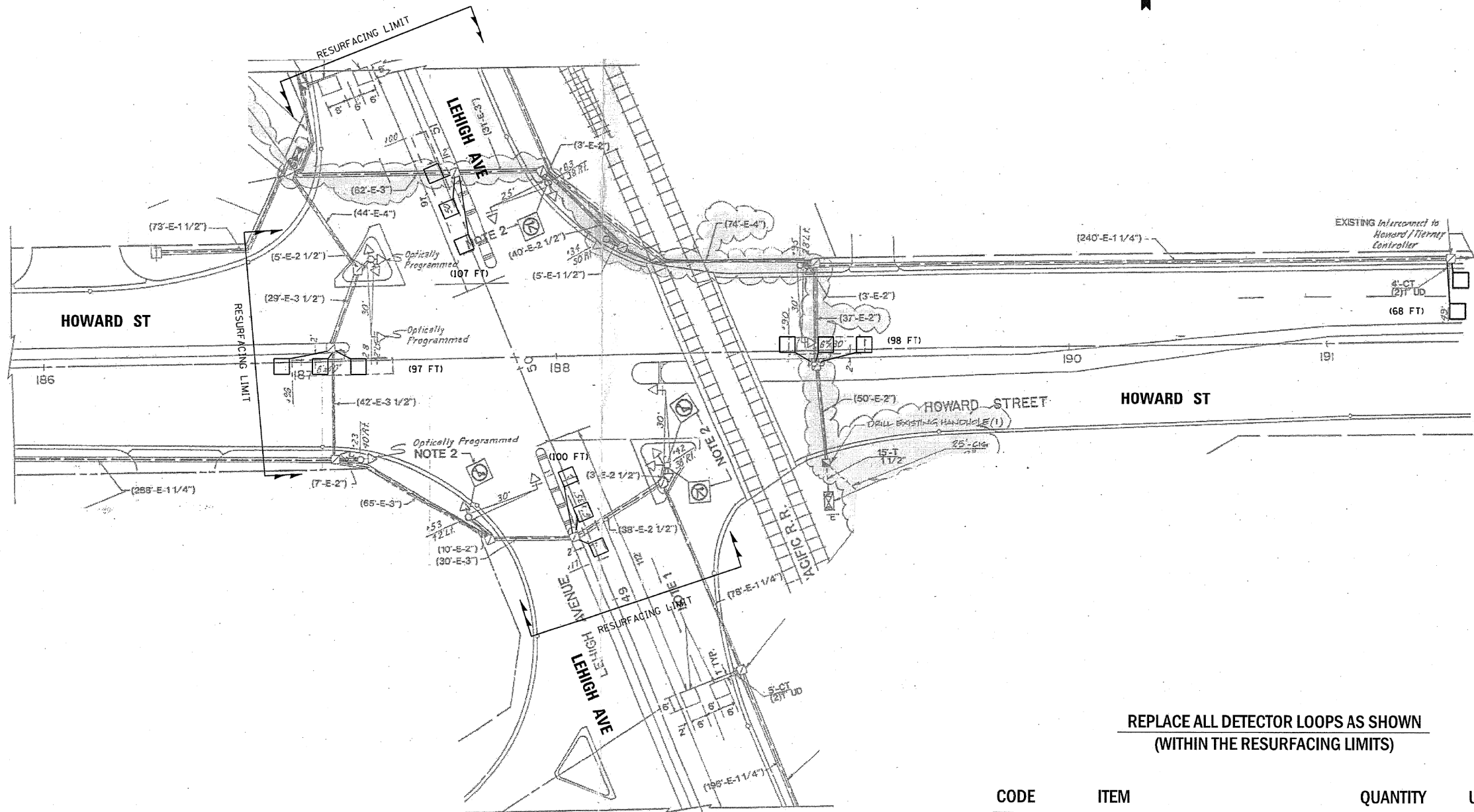
ROADWAY PLAN
HOWARD ST. (W/O LEHIGH AVE. - NEW GROSS POINT RD.)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	6
CONTRACT NO. 62F83				
ILLINOIS FED. AID PROJECT				

NOTES:

1. Work shall meet the requirements of the special provision,
"Detector Loop Replacement and/or Installation (Roadway Grinding,
Resurfacing & Patching Operations".
2. This plan is for the sole purpose of detector loop replacement.



CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	470	FOOT

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PLOT DATE =	3/16/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP PLAN
HOWARD ST. AT LEHIGH AVE.

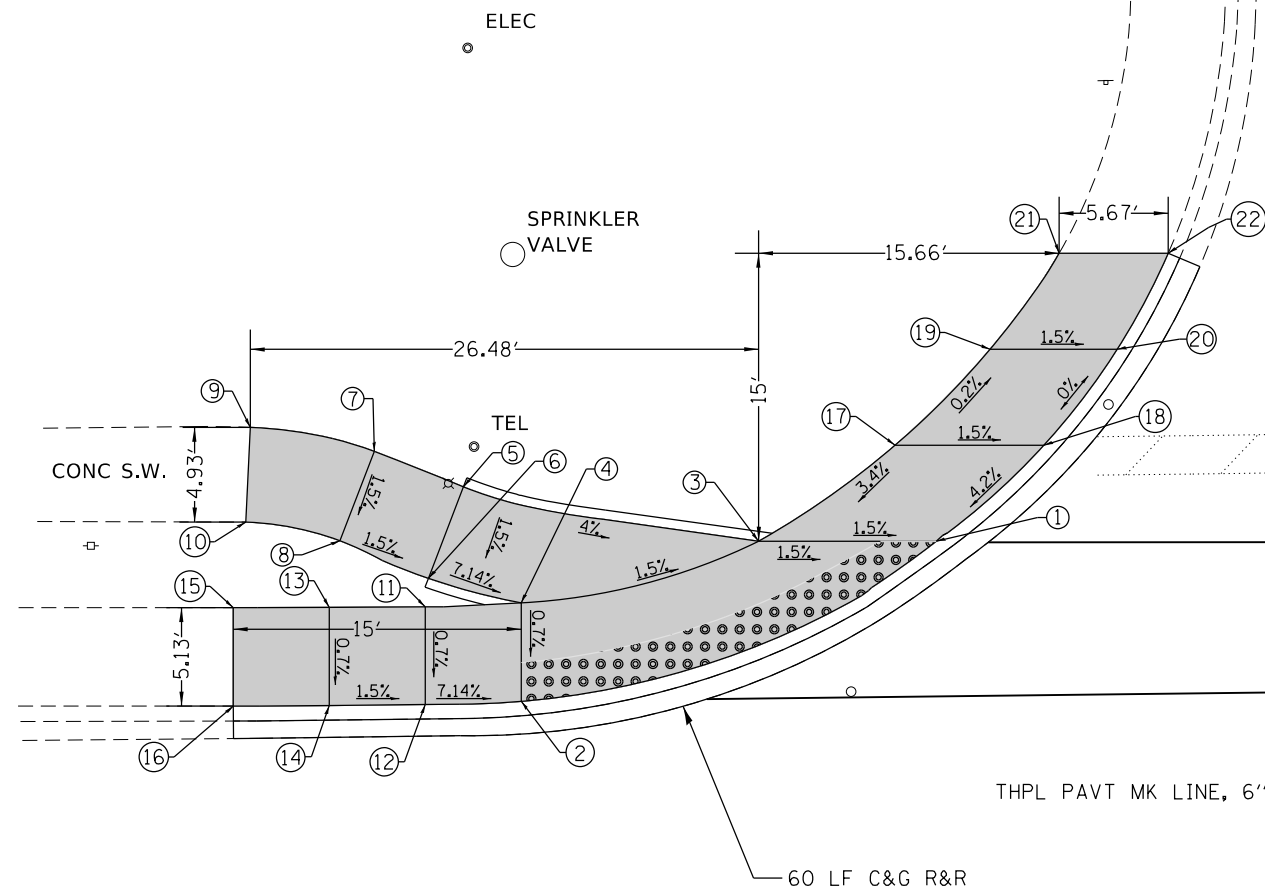
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	7
CONTRACT NO. 62F83				
ILLINOIS FED. AID PROJECT				

1. Work shall meet the requirements of the special provision, "Detector Loop Replacement and/or Installation (Roadway Grinding, Resurfacing & Patching Operations)".
2. This plan is for the sole purpose of detector loop replacement.



CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	739	FOOT



ADA RAMP ELEVATION TABLE							
POINT NO.:	ELEVATION	POINT NO.:	ELEVATION	POINT NO.:	ELEVATION	POINT NO.:	ELEVATION
1	623.8554	6	624.5428	11	624.5428	16	624.5741
2	624.15	7	624.6943	12	624.507	17	624.2874
3	623.9941	8	624.6178	13	624.6178	18	624.1719
4	624.1838	9	624.9231	14	624.582	19	624.2709
5	624.6193	10	624.8822	15	624.7235	20	624.1719
						21	624.3284
						22	624.1307

REFERENCE BENCHMARK ELEV 626.78

BENCHMARK : BOLT WITH ARROW OF FIRE HYDRANT ON AUSTIN
±300' NORTH OF HOWARD

LEGEND

XX.XX'

EXISTING LENGTH

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



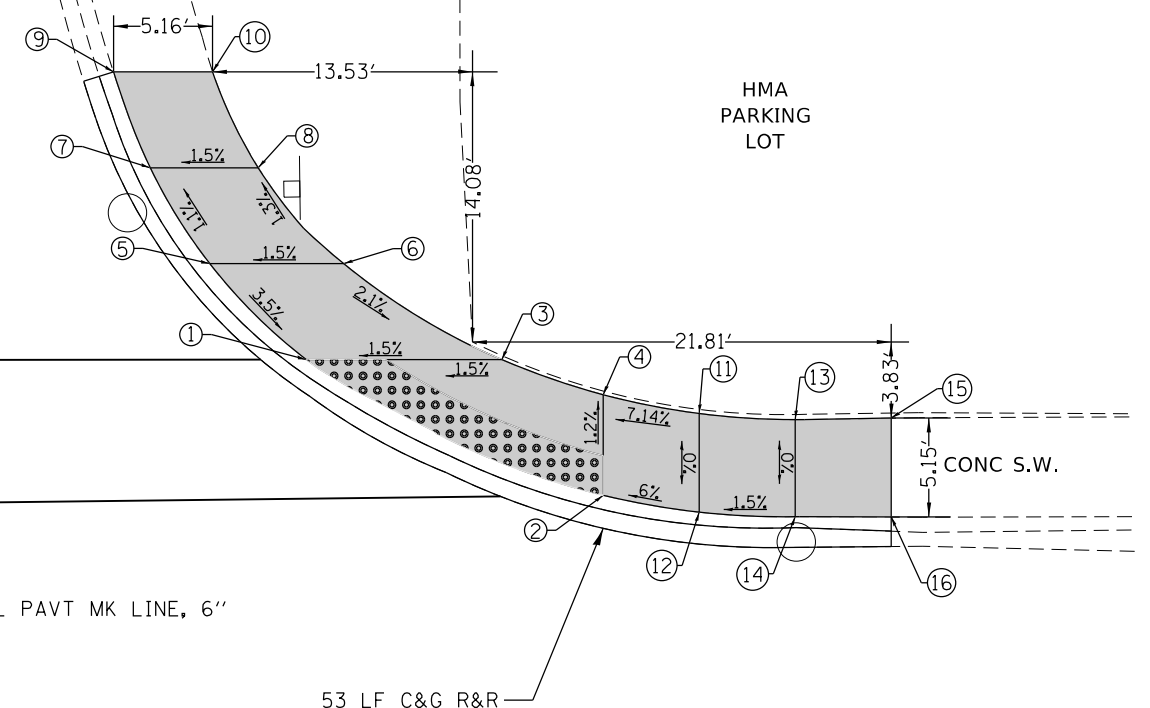
PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD



ADA RAMP ELEVATION TABLE							
POINT NO.:	ELEVATION	POINT NO.:	ELEVATION	POINT NO.:	ELEVATION		
1	624.0671	6	624.4194	11	624.6566	16	624.8916
2	624.3506	7	624.2484	12	624.6566		
3	624.2186	8	624.3324	13	624.7316		
4	624.2996	9	624.1645	14	624.7316		
5	624.3159	10	624.3131	15	624.9983		

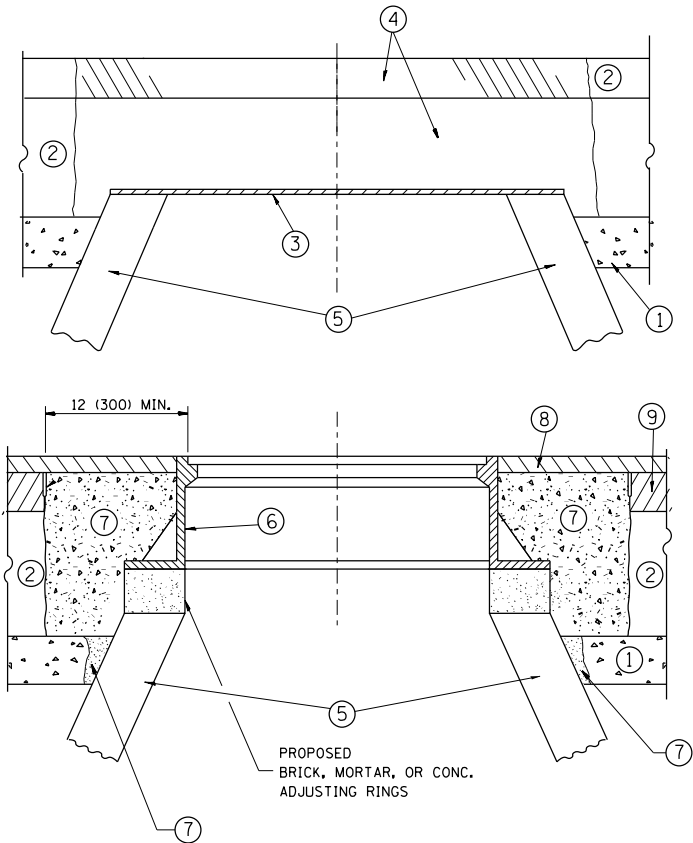
NORTHEAST CORNER
HOWARD ST. AT AUSTIN RD.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP DETAILS
HOWARD ST. AT AUSTIN RD.

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	9
		CONTRACT NO. 62F83		
		ILLINOIS	FED. AID PROJECT	



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

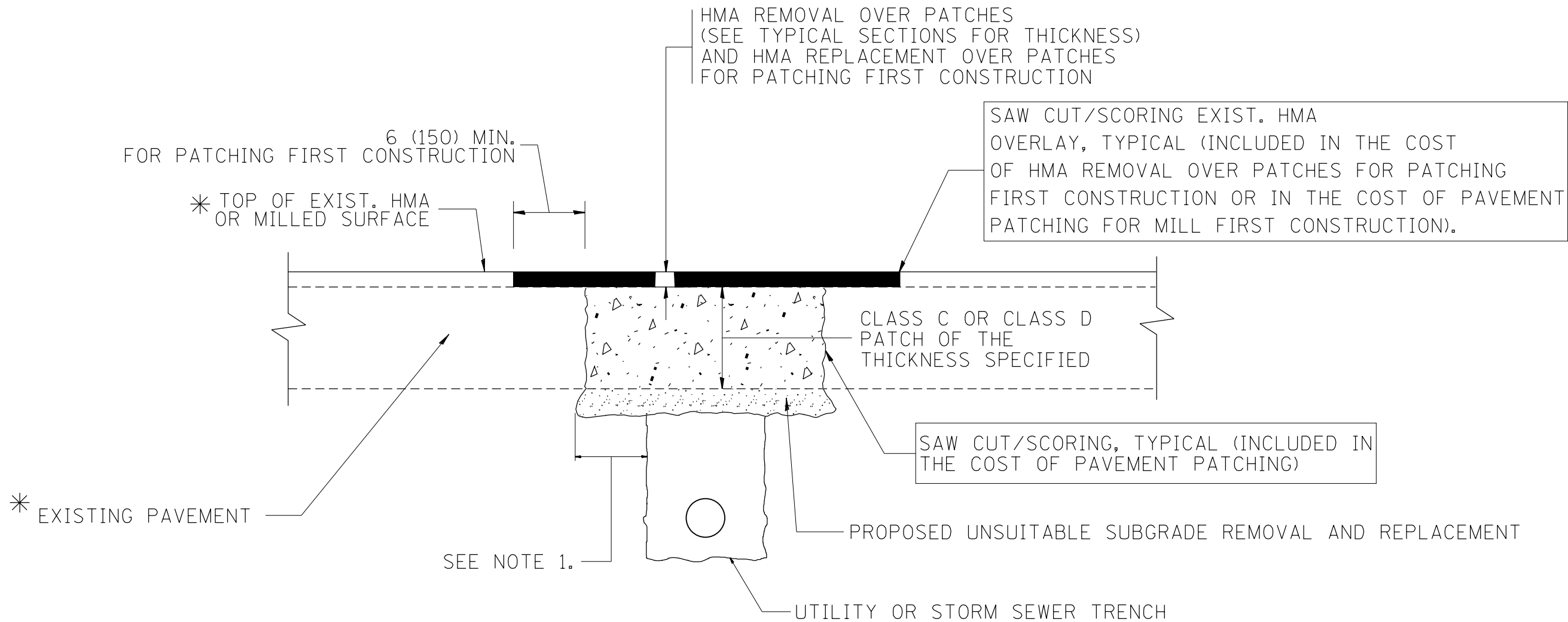
THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pyrczenowski	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\DI2118\Frame\Design\DistStd.dgn			REVISED - R. BORO 01-01-07					1334	2017-049R5	COOK	21	10
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11		BD600-03 (BD-8)			CONTRACT NO. 62F83				
	PLOT DATE = 2/26/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

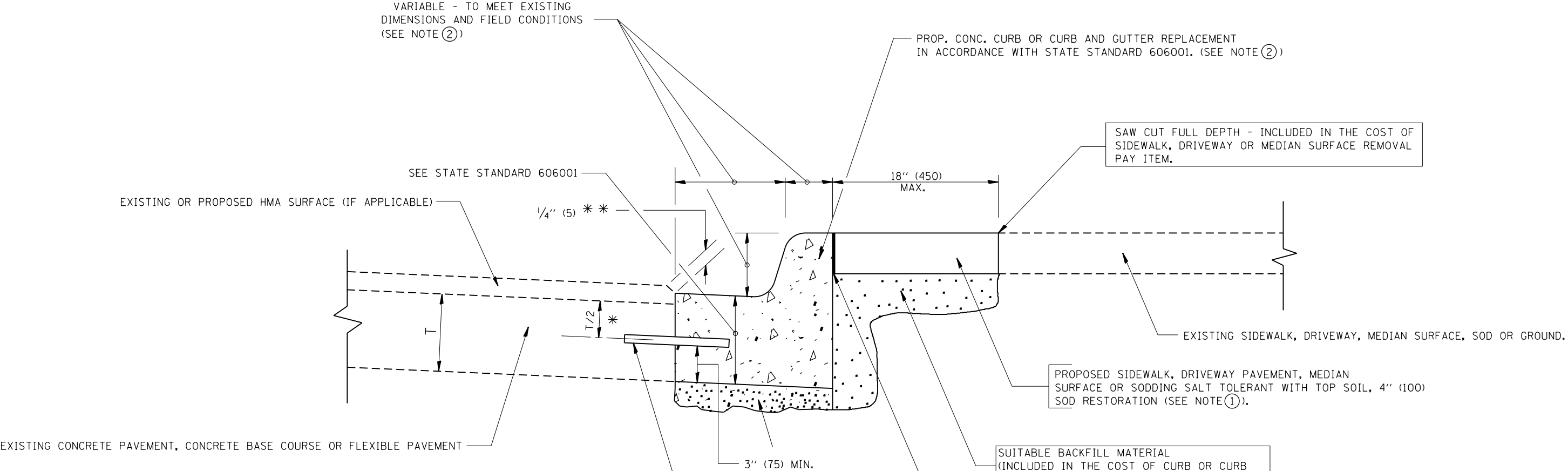
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pyzenowskirb	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI2118\RAM\Design\DistStd.dgn			REVISED - R. BORO 01-01-07					1334	2017-049R5	COOK	21	11
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07					BD400-04 (BD-22) CONTRACT NO. 62F83				
	PLOT DATE = 2/26/2018	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

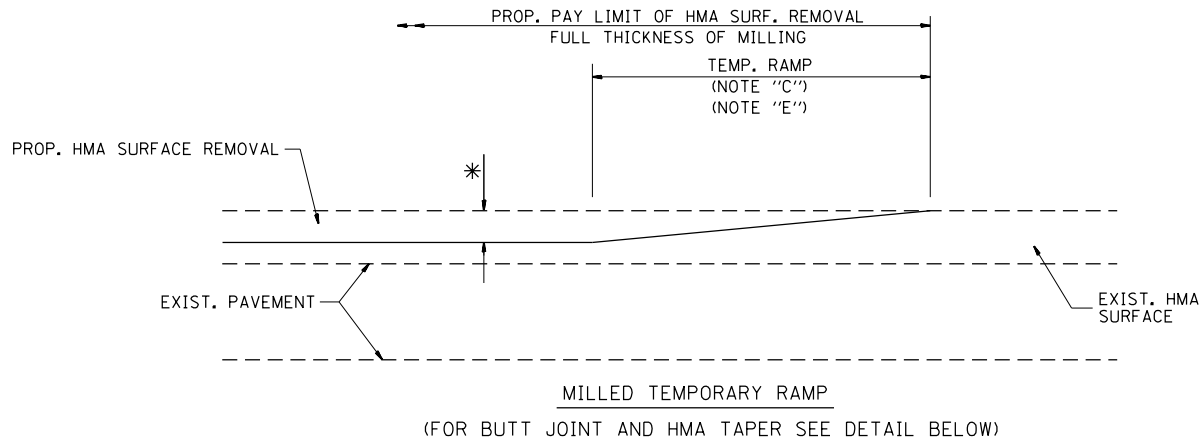
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

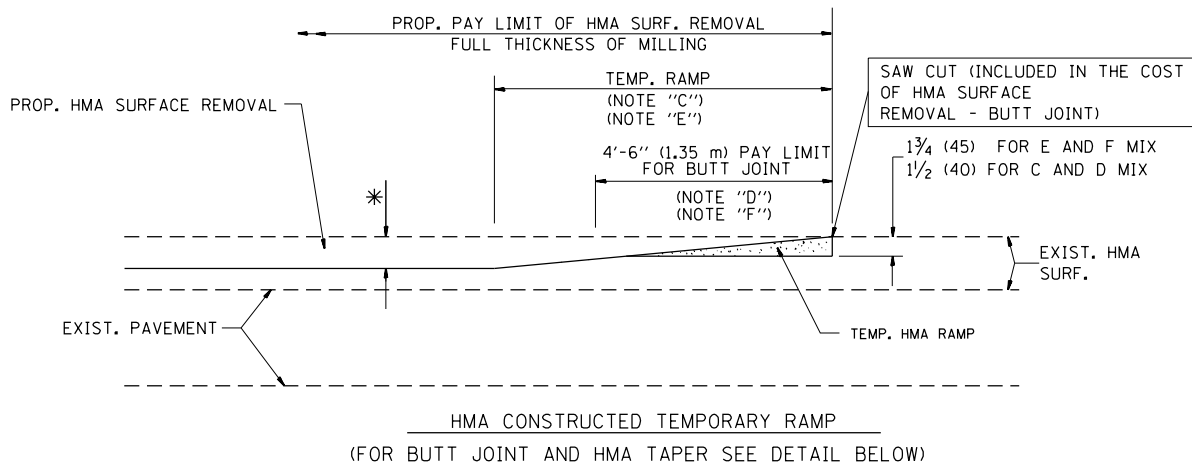
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

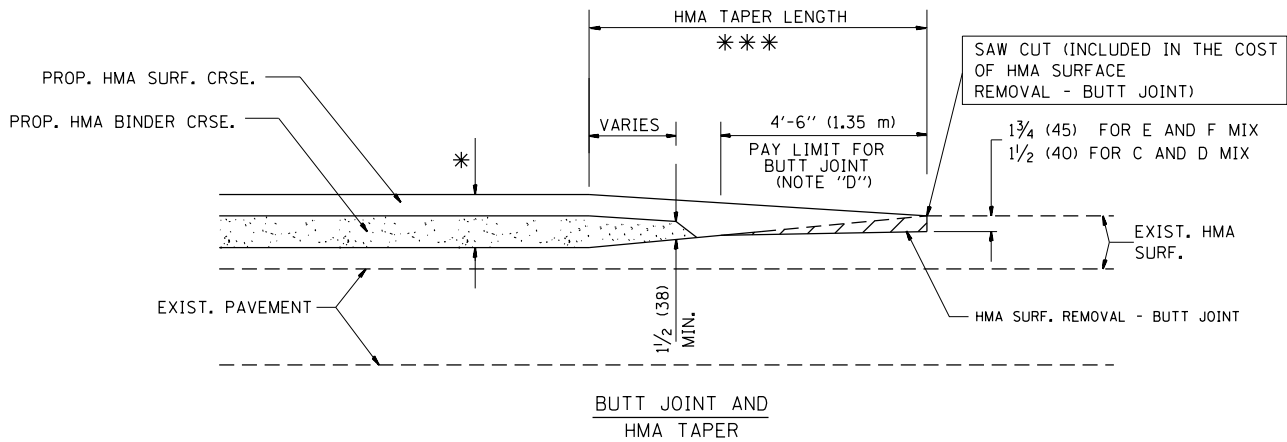
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			REVISED - A. ABBAS 03-21-97					1334	2017-049R5	COOK	21	12
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01					BD600-06 (BD-24)		CONTRACT NO. 62F83		
	PLOT DATE = 2/26/2018	DATE - 03-11-94	REVISED - R. BORO 12-15-09					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
SCALE: NONE				SHEET NO. 1 OF 1 SHEETS			STA. TO STA.					



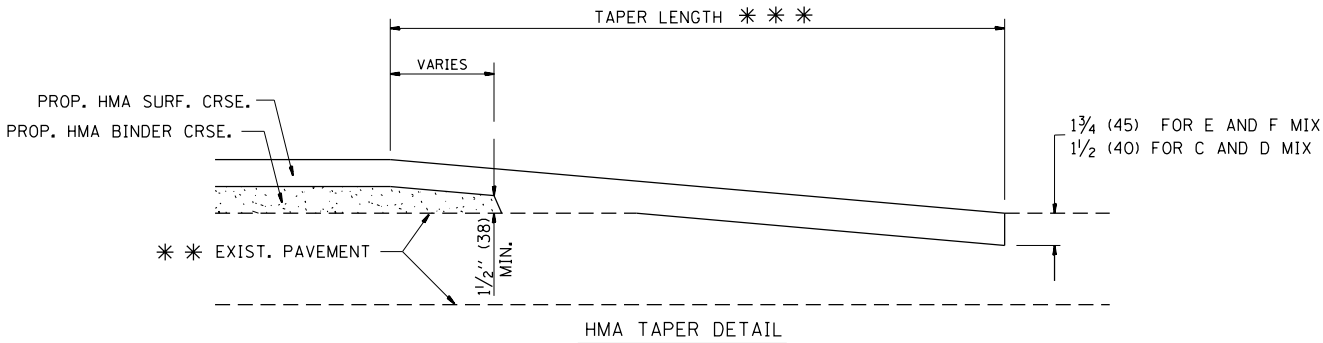
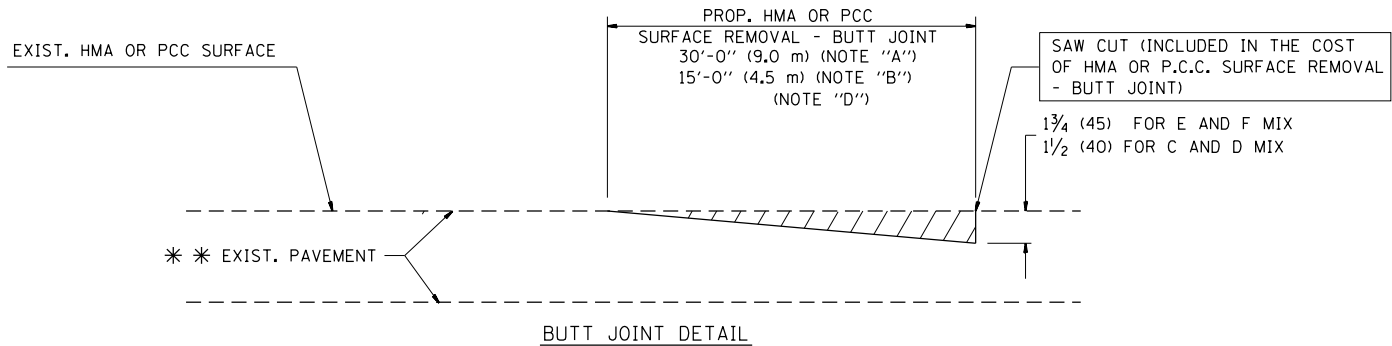
OPTION 1



OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

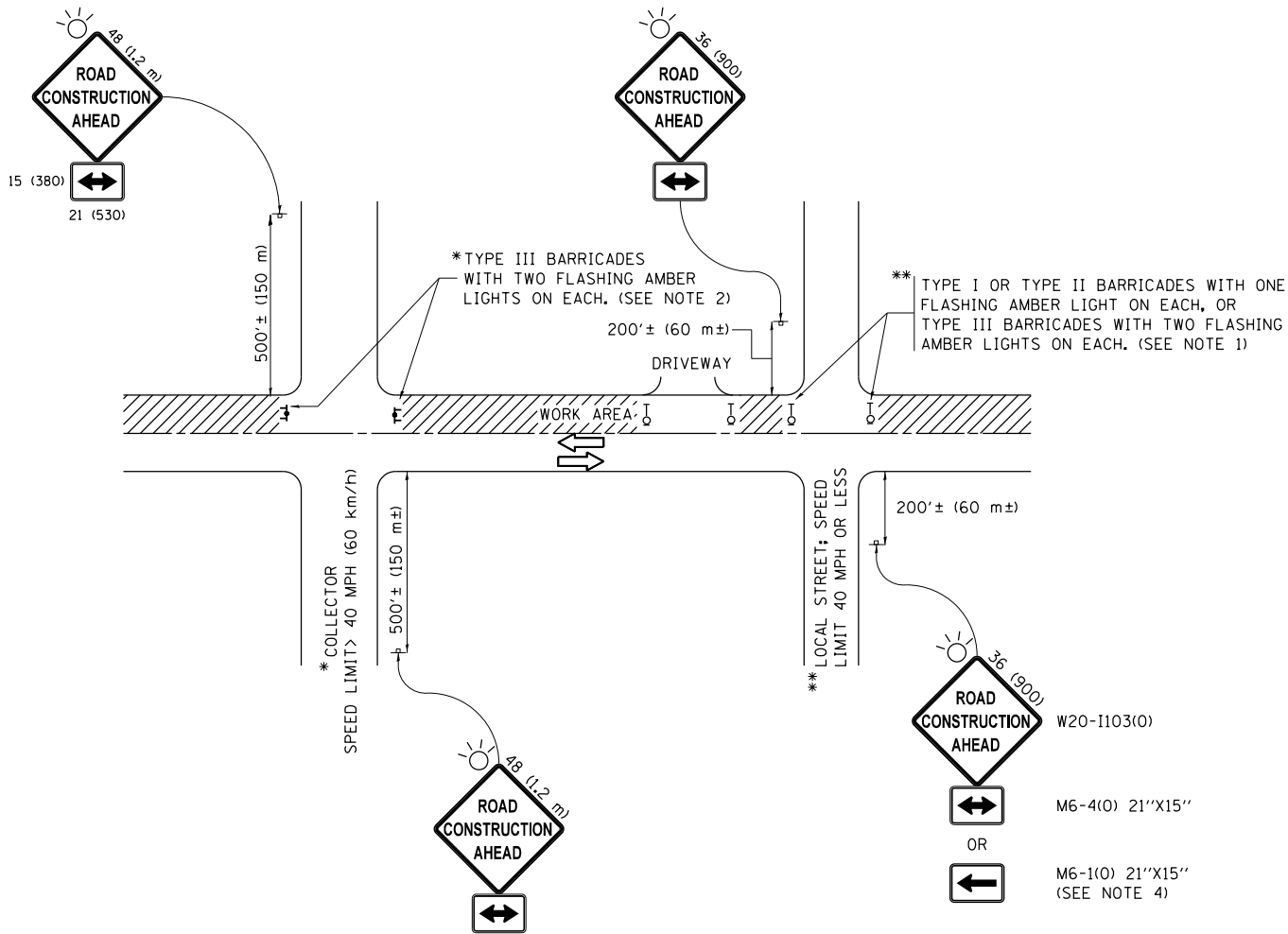
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01	
PLOT DATE = 2/26/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049R5	COOK	21	13
BD400-05 BD32		CONTRACT NO. 62F83		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

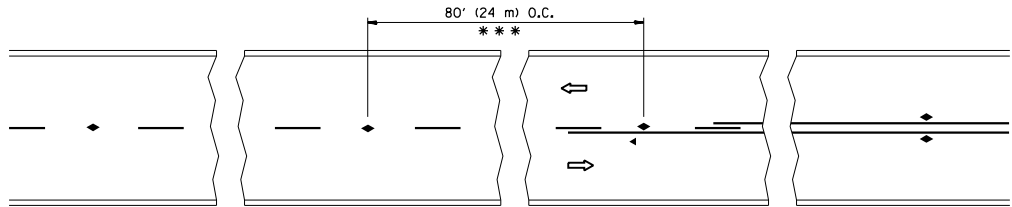
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Default	PLOT DATE = 2/26/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

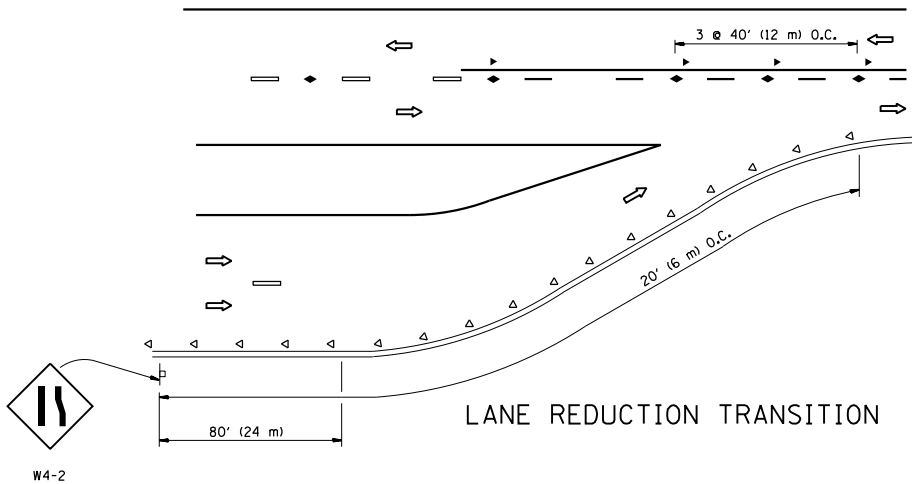
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049RS	COOK	21	14
TC-10		CONTRACT NO. 62F83		
ILLINOIS		FED. AID PROJECT		

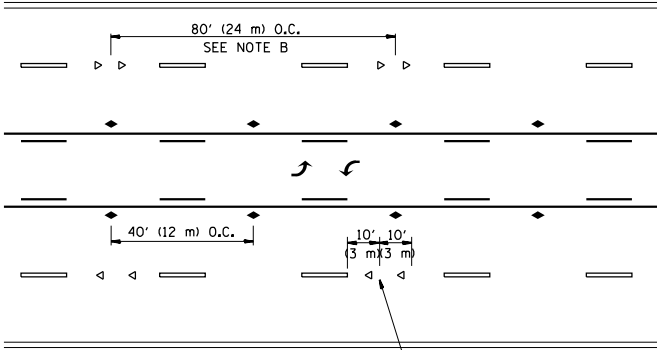


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

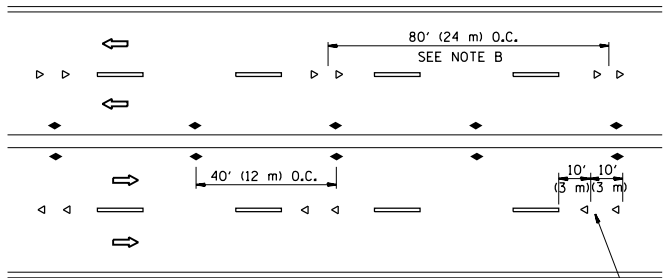


LANE REDUCTION TRANSITION



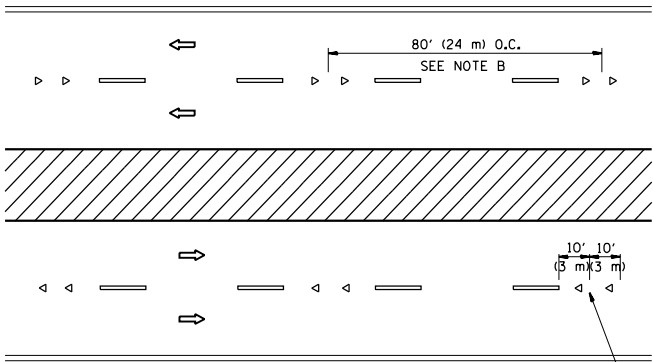
SEE NOTE A

TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

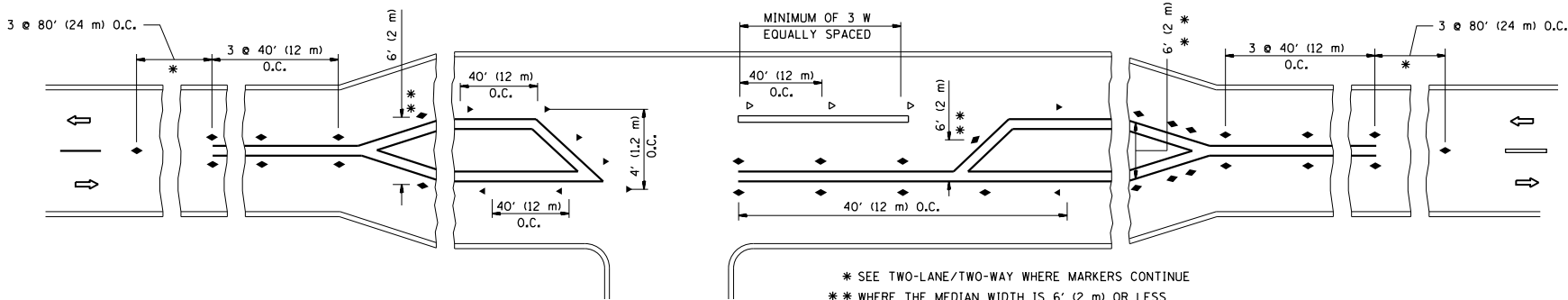
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS
USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI2118\RAM\Note\Design\DistStd.dgn	USER NAME = pyrazanowskirb	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99						1334	2017-049R5	COOK	21	15
	PLOT DATE = 2/26/2018	DATE -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.				TC-11 CONTRACT NO. 62F83				
			REVISED - C. JUCIUS 09-09-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



4' (1.2 m) WIDE MEDIANS ONLY



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					1334	2017-049RS	COOK	21	16
					TC-13		CONTRACT NO. 62F83		
SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

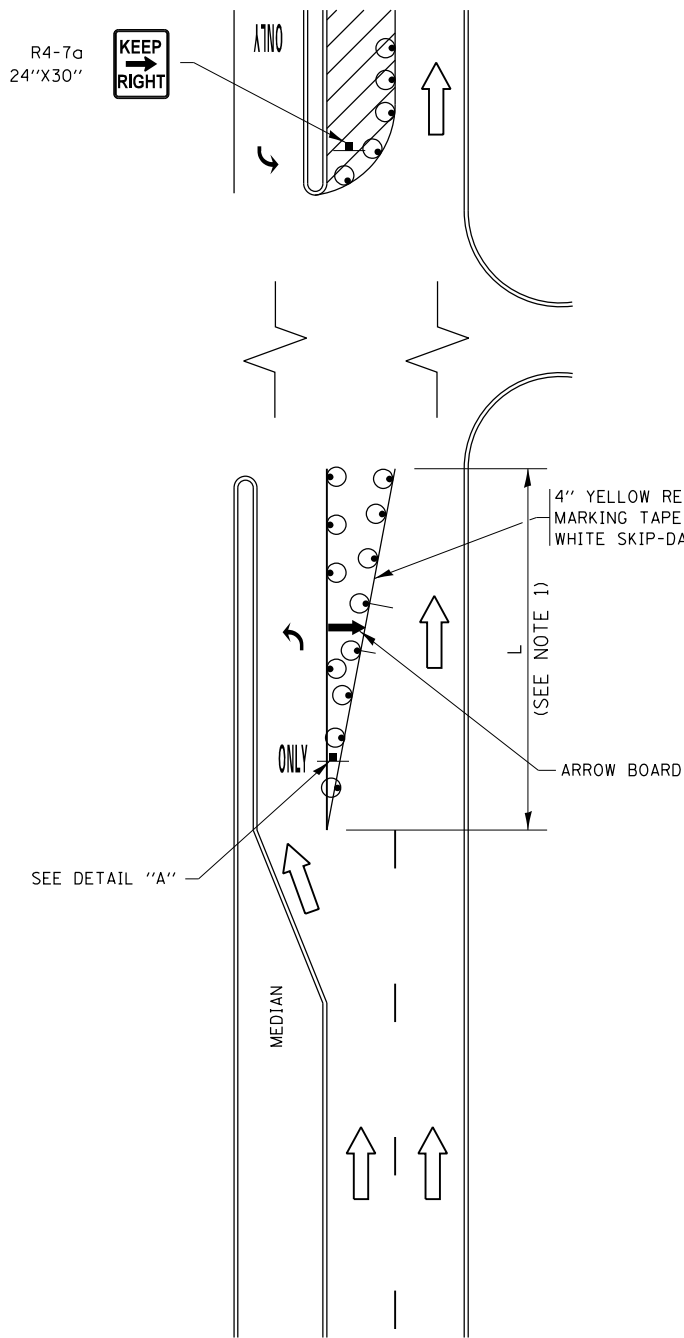
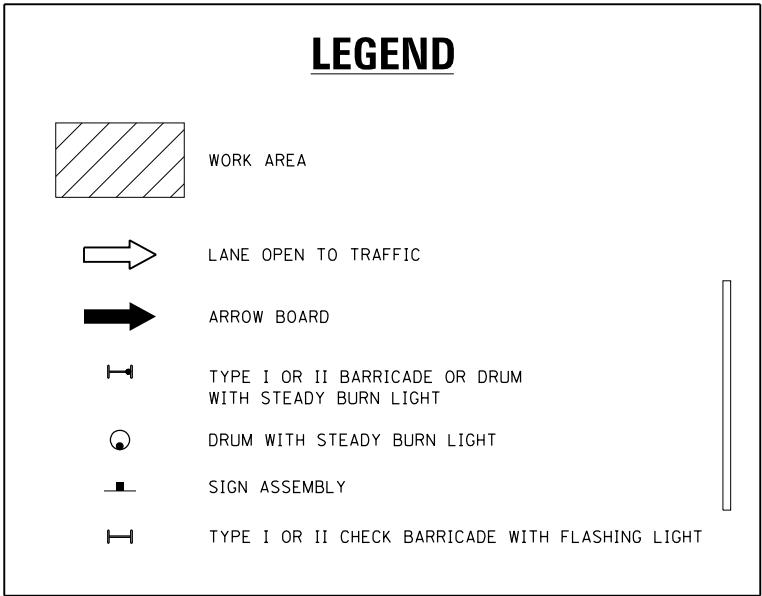


FIGURE 1

LEGEND



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

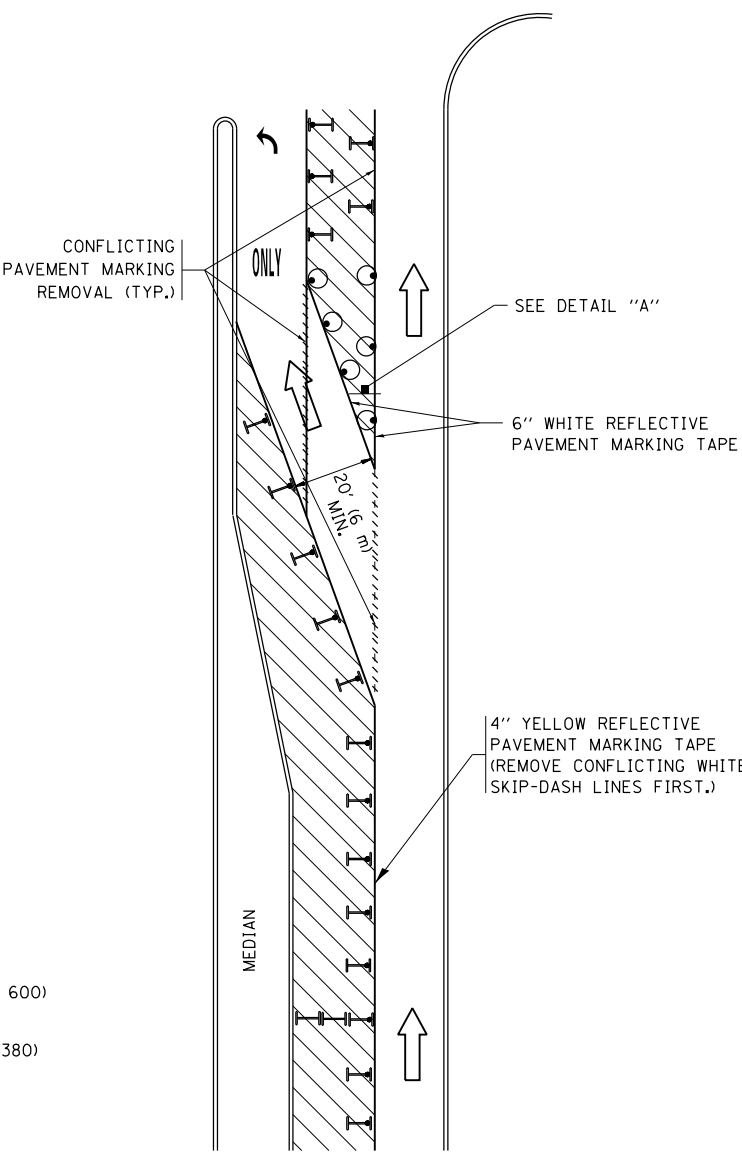
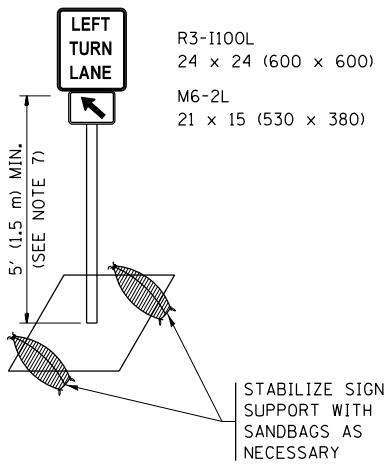


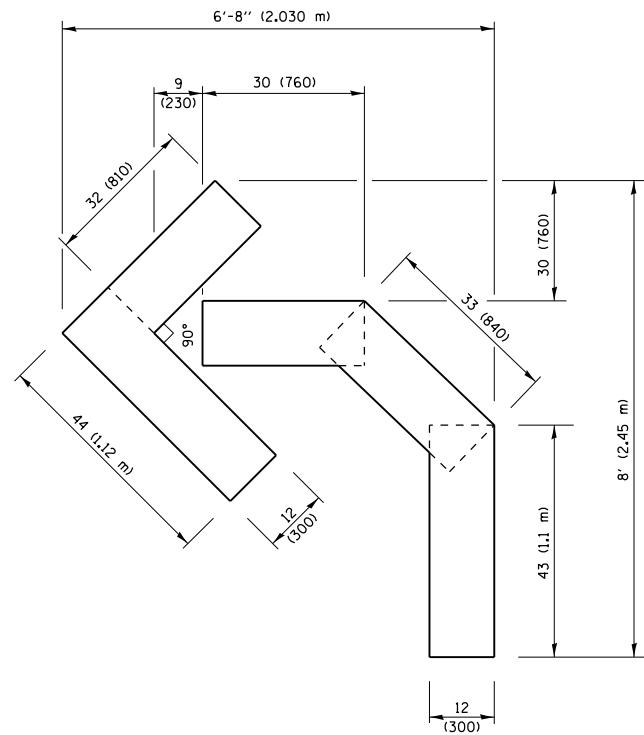
FIGURE 2



DETAIL A

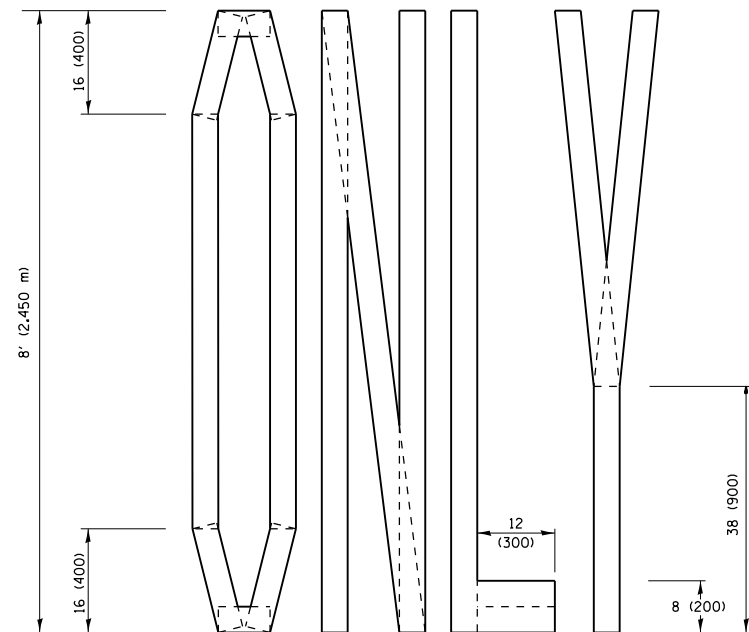
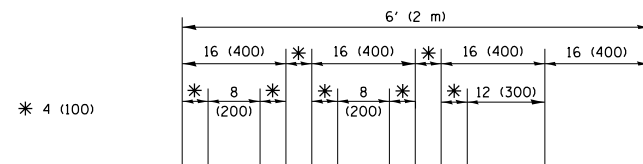
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = pyzenowskirb	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\1084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\0121\1084EBID\Design\As-Built\HOUSEH 11-07-95	REVISOR = A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			1334	2017-049R5	COOK	21	17
PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62F83		
Default	PLOT DATE = 2/26/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			ILLINOIS FED. AID PROJECT				
SCALE: NONE						SHEET 1	OF 1	SHEETS	STA.	TO STA.



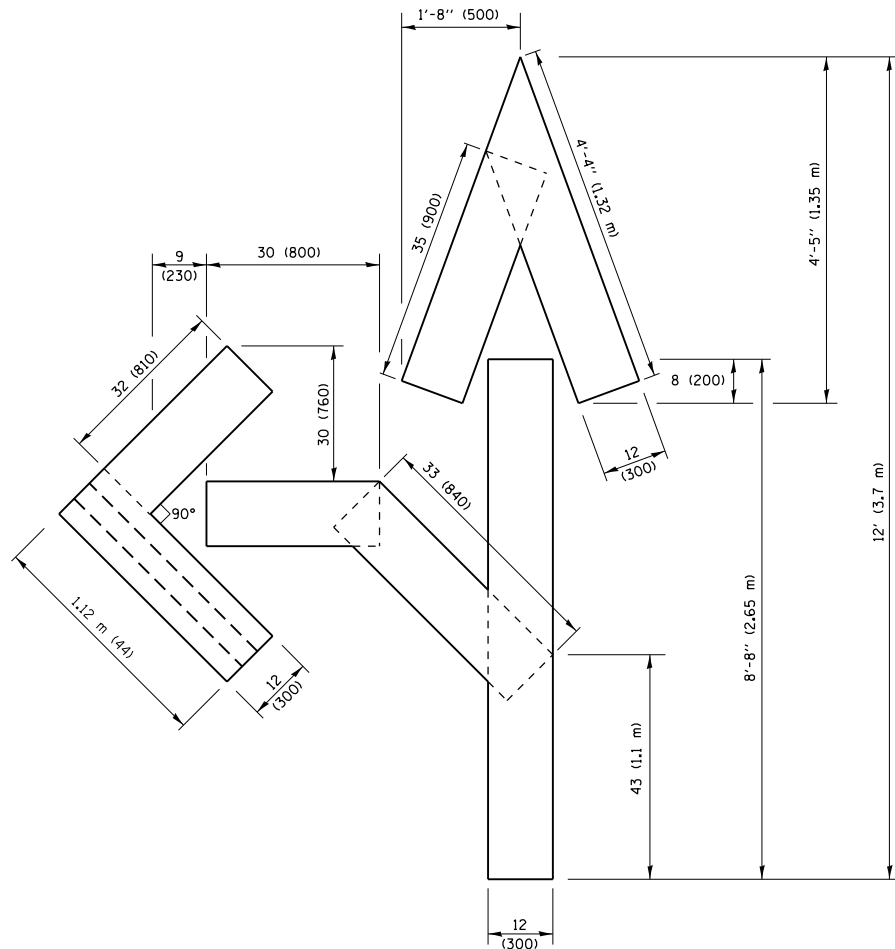
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

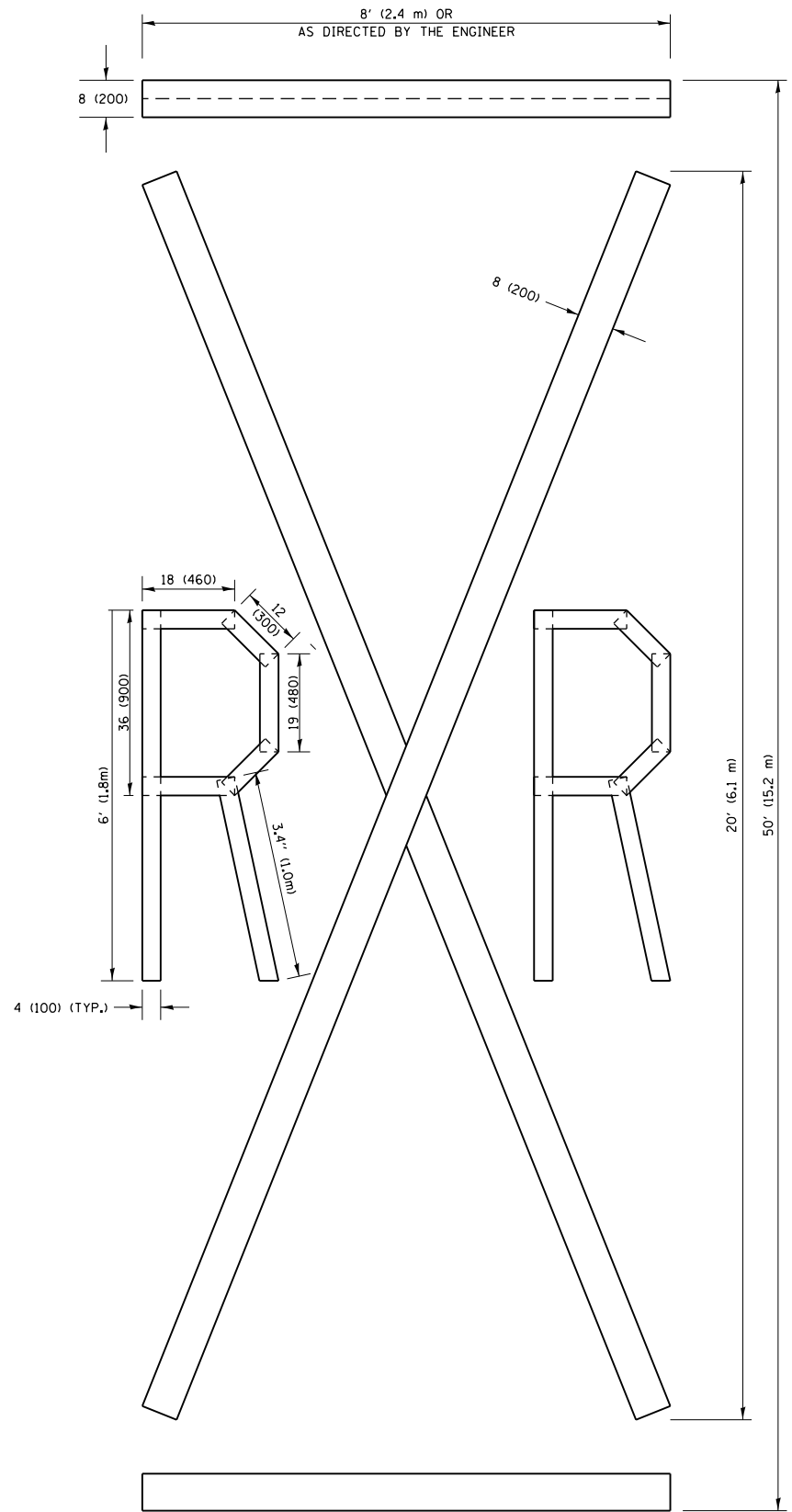


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

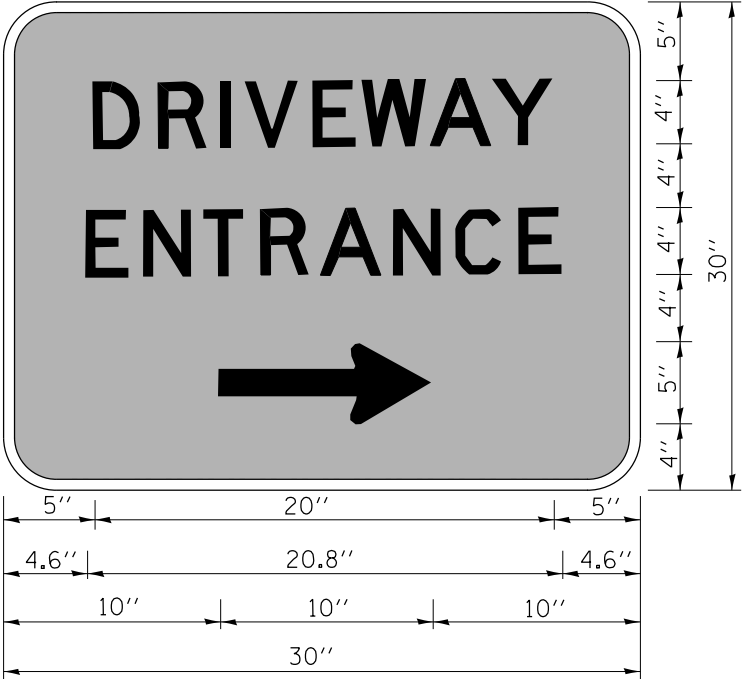
FILE NAME =	USER NAME = pyrozenowskirb	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\1084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI21\084EBIDINTEG\Design\DistStd.dgn			REVISED -E. GOMEZ 08-28-00
			REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1334	2017-049R5	COOK	21	18
TC-16 CONTRACT NO. 62F83				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

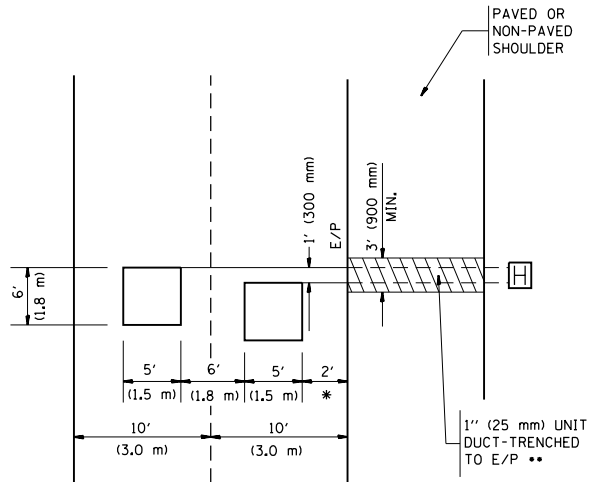
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = p:\11\084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI21\084EBID\Design\DistStd.dgn	USER NAME = pyrzezowski	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					1334	2017-049R5	COOK	21	20
	PLOT DATE = 2/26/2018	DATE -	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TC-26 CONTRACT NO. 62F83				
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.



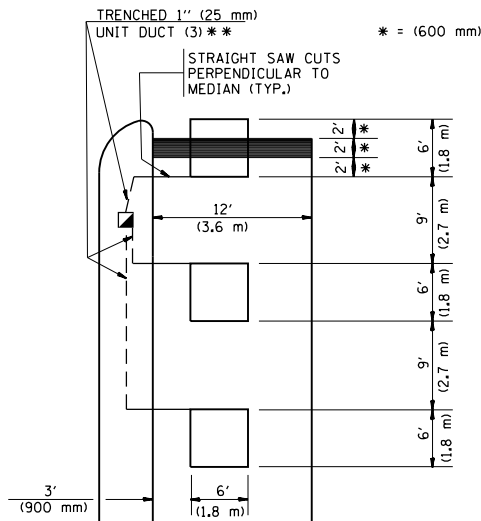
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

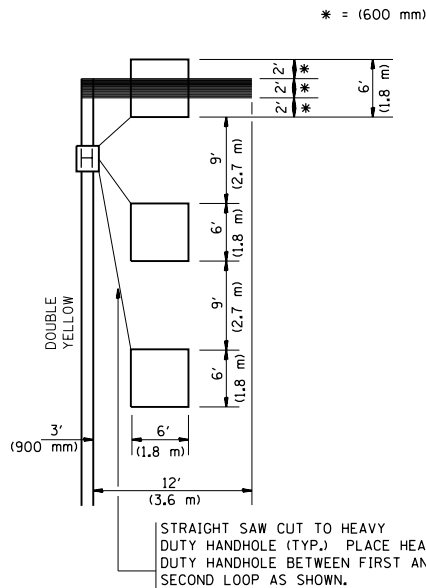


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

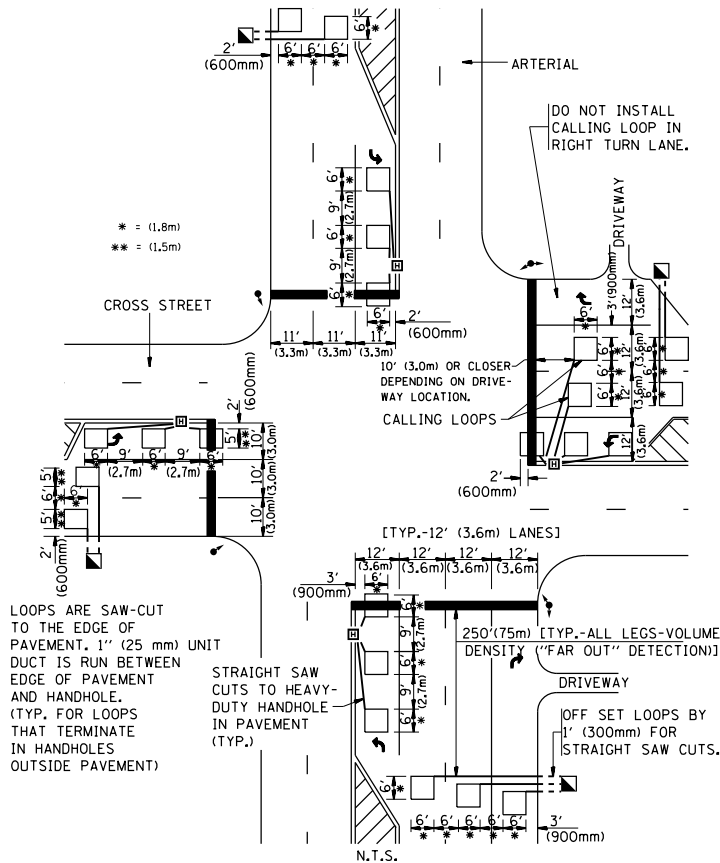
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



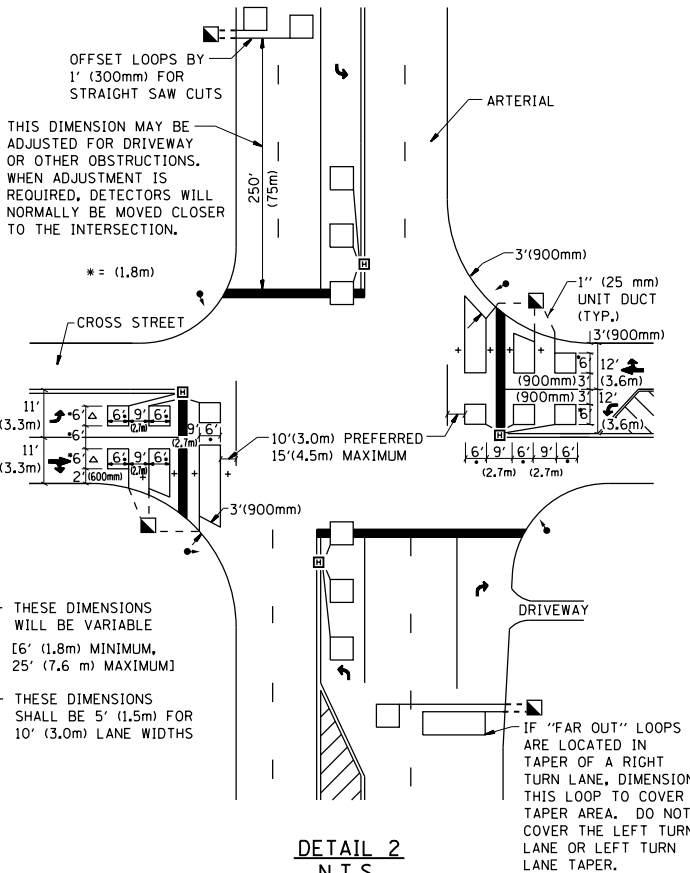
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = pyrozenowskirb	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div>	DISTRICT 1 – DETECTOR LOOP INSTALLATION			F.A.U.	SECTION	COUNTY	TOTAL	SHEET
p:\w\11084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI2118\Drawings\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -					RTE.			SHEETS	NO.
PLOT SCALE = 100.0000' / 1"		DATE -	REVISED -		1334	2017-049R5	COOK	21	21			
PLOT DATE = 2/26/2018					TS-07			CONTRACT NO. 62F83				
					SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		
				FED. ROAD DIST. NO. 1			ILLINOIS			FED. AID PROJECT		