

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

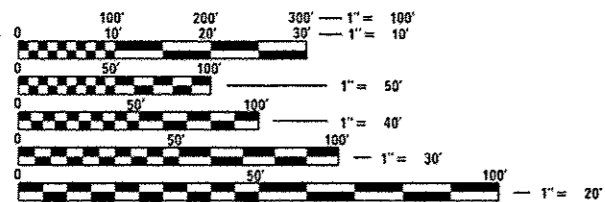
F.A.U. ROUTE 2845: HARWOOD AVENUE
1) 183RD ST. TO ELM RD. 2) ELM RD. TO DIXIE HWY
SECTION: 3160A-RS&N
1) RESURFACING
2) CHANNELIZATION
COOK COUNTY
C-91-506-12

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	1
ILLINOIS			CONTRACT NO. 60T99	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS IMPROVEMENT IS LOCATED IN THE VILLAGE OF HOMEWOOD

TRAFFIC DATA
2010 ADT = 2,300
SPEED LIMIT = 25 MPH

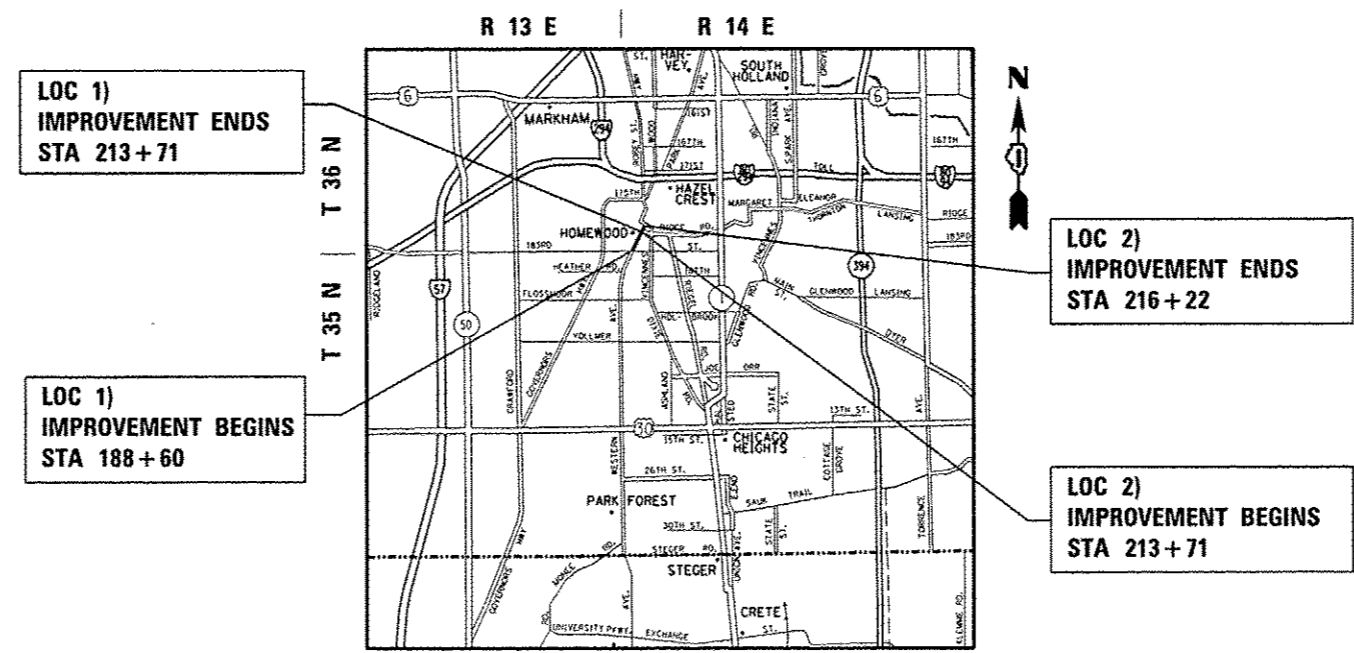


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: J. ALAIN MIDY (847)221-3056
PROJECT MANAGER: ISSAM RAYYAN

CONTRACT NO. 60T99



GROSS AND NET LENGTH = 2762 FT. = 0.523 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 27 2013

John D. Baramezzi
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 10 2013
John D. Baramezzi PE
ENGINEER OF DESIGN AND ENVIRONMENT

May 10 2013
Chris Osman PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
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27	CROSS SECTIONS

LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-06	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
604001-03	FRAME AND LIDS TYPE I
604036-02	GRATE TYPE B
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-04	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-03	OFF-ROAD OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
780001-03	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF HOMEWOOD.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED ACCORDING TO ARTICLE 205.04 OF THE STANDARD SPECIFICATIONS.

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING, RESURFACING OR OVERLAYING.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

FILE NAME: c:\pwwork\midyja\0267284\PI1811	USER NAME: midyja	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HARWOOD AVENUE				P.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	shl-gennote.dgn	DRAWN: -	REVISED: -					INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES				2845	3160A-RS&N	COOK	27	2	
		CHECKED: -	REVISED: -	SCALE:				SHEET NO.	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT			
		DATE: 3/20/2013	REVISED: -									CONTRACT NO. 60T99					

SUMMARY OF QUANTITIES			100% STATE URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	LOC 1 0005	LOC 2 0004				
20200100	EARTH EXCAVATION	CU YD	20		20				
20400800	FURNISHED EXCAVATION	CU YD	10		10				
20800150	TRENCH BACKFILL	CU YD	3		3				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	24		24				
21101655	TOPSOIL FURNISH AND PLACE, 14"	SQ YD	125		125				
25000210	SEEDING, CLASS 2A	ACRE	0.03		0.03				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2.7		2.7				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2.7		2.7				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2.7		2.7				
25100630	EROSION CONTROL BLANKET	SQ YD	132		132				
25200110	SODDING, SALT TOLERANT	SQ YD	149		149				
28000400	PERIMETER EROSION BARRIER	FOOT	155		155				
28000510	INLET FILTERS	EACH	1		1				
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SO YD	62		62				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	8.6	8	0.6				
40600300	AGGREGATE (PRIME COAT)	TON	41	38	3				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15.3	14.2	1.1				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	422	391	31				

SUMMARY OF QUANTITIES			100% STATE URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	LOC 1 0005	LOC 2 0004				
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	153	116	37				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	857	796	61				
42001300	PROTECTIVE COAT	SO YD	273	29	244				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	53	7	46				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1140	15	1125				
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SO FT	128		128				
42400800	DETECTABLE WARNINGS	SO FT	20		20				
44000100	PAVEMENT REMOVAL	SO YD	180		180				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	10050	9360	690				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	20	7	13				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	236		236				
44000600	SIDEWALK REMOVAL	SO FT	805	15	790				
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SO YD	25	25					
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	215	200	15				
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	100	100					

FILE NAME *	USER NAME * mdyja	DESIGNED -	REVISED -
DATE * 3/29/2013	PLLOT SCALE * 100,000' / 1" IN	DRAWN -	REVISED -
	PLLOT DATE * 3/29/2013	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HARWOOD AVENUE
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 2845	SECTION 3160A-RS&N	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 3
CONTRACT NO. 60T99				

Rel.

* SPECIALTY ITEMS

SUMMARY OF QUANTITIES			100% STATE URBAN CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	LOC 1 0005	LOC 2 0004			
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	300	300				
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	21		21			
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1		1			
60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1		1			
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1		1			
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	4	3	1			
60266600	VALVE BOXES TO BE ADJUSTED	EACH	7	6	1			
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6, 12	FOOT	266		266			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	3	3			
67100100	MOBILIZATION	L SUM	1	0.5	0.5			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	1		1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2319	2295	24			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	482.9	482.9				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5165	5060	105			

SUMMARY OF QUANTITIES			100% STATE URBAN CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	LOC 1 0005	LOC 2 0004			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1080	1080				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	386	180	206			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	170	124	46			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	255	255				
* 72000100	SIGN PANEL - TYPE 1	SO FT	57.5	15	42.5			
* 72900100	METAL POST-TYPE A	FOOT	52.5	21	31.5			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	482.9	482.9				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5165	5060	105			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1080	1080				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	386	180	206			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	170	124	46			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	42	42				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	376	376				
X0326863	BRICK SIDEWALK	SO FT	20		20			
X0326864	BRICK SIDEWALK REMOVAL	SO FT	437	45	392			
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1		1			
X5537700	STORM SEWERS TO BE CLEANED 10"	FOOT	260		260			

FILE NAME *	USER NAME * mdyjo	DESIGNED -	REVISED -
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PLOT SCALE * 100.0000 1/16		CHECKED -	REVISED -
PLOT DATE * 3/29/2013		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

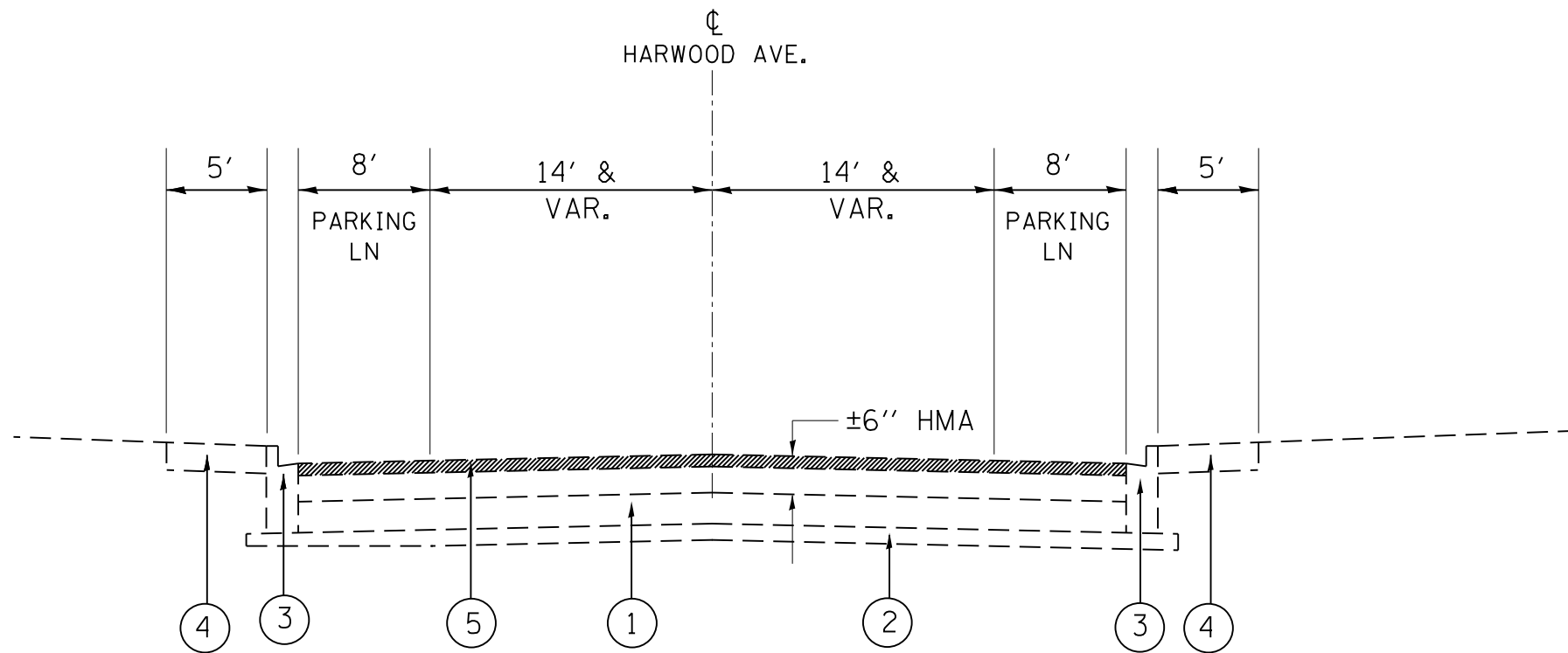
**HARWOOD AVENUE
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

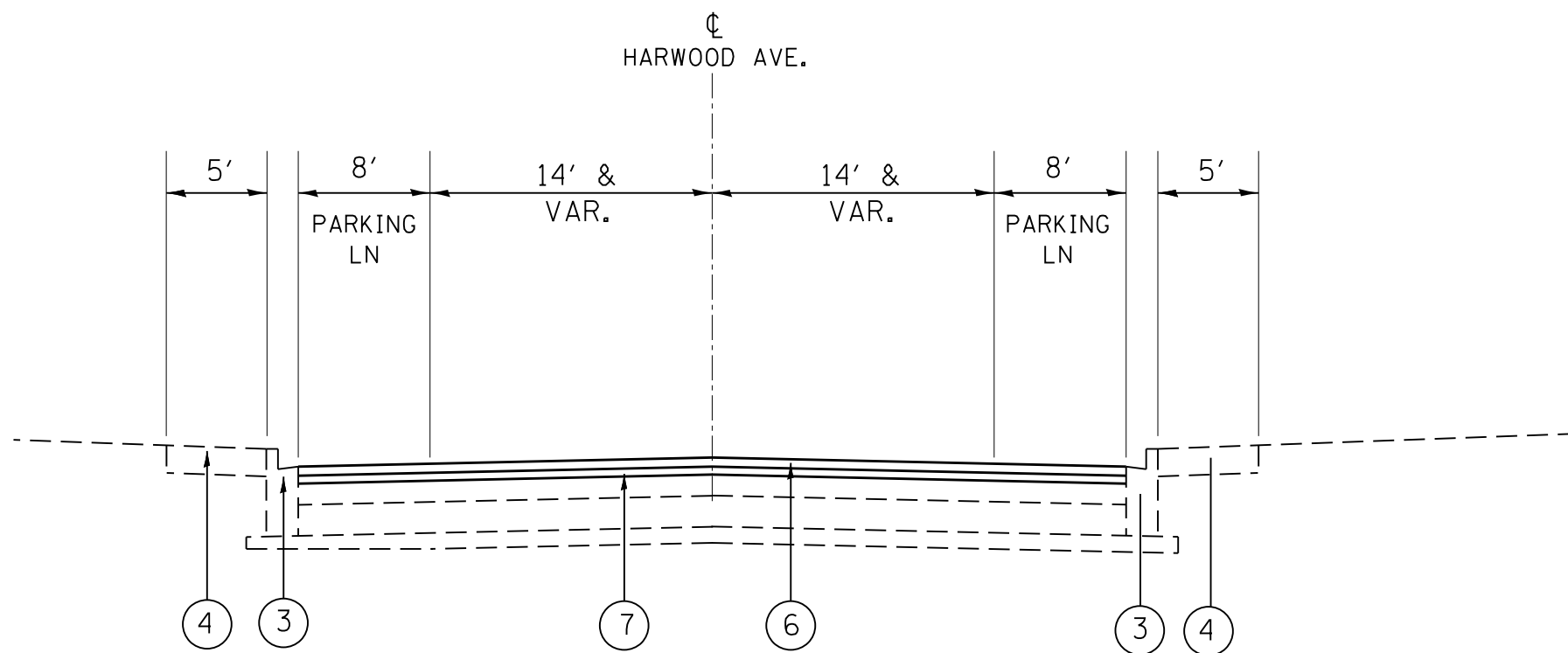
F.A.U. RTE. 2845	SECTION 3160A-RS&N	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 4
CONTRACT NO. 60T99				

Rev.

* SPECIALTY ITEMS



**EXISTING TYPICAL SECTION
HARWOOD AVENUE
STA 185+65 TO STA 213+71**



**PROPOSED TYPICAL SECTION
HARWOOD AVENUE
STA 185+65 TO STA 213+71**

LEGEND

- ① EXISTING HMA BASE COURSE, ± 8 1/4"
- ② EXISTING STABILIZED AGGREGATE SUBBASE, 4"
- ③ EXISTING COMBINATION CONCRETE C&G TYPE B-6.12
- ④ EXISTING P.C.C. SIDEWALK, 5"
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50 3/4"
- ⑧ PROPOSED COMBINATION CONCRETE C&G, TYPE B-6.12
- ⑨ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑩ PROPOSED P.C.C. SIDEWALK, 5"
- ⑪ PROPOSED TOPSOIL, 4"
- ⑫ PROPOSED TOPSOIL, 14"

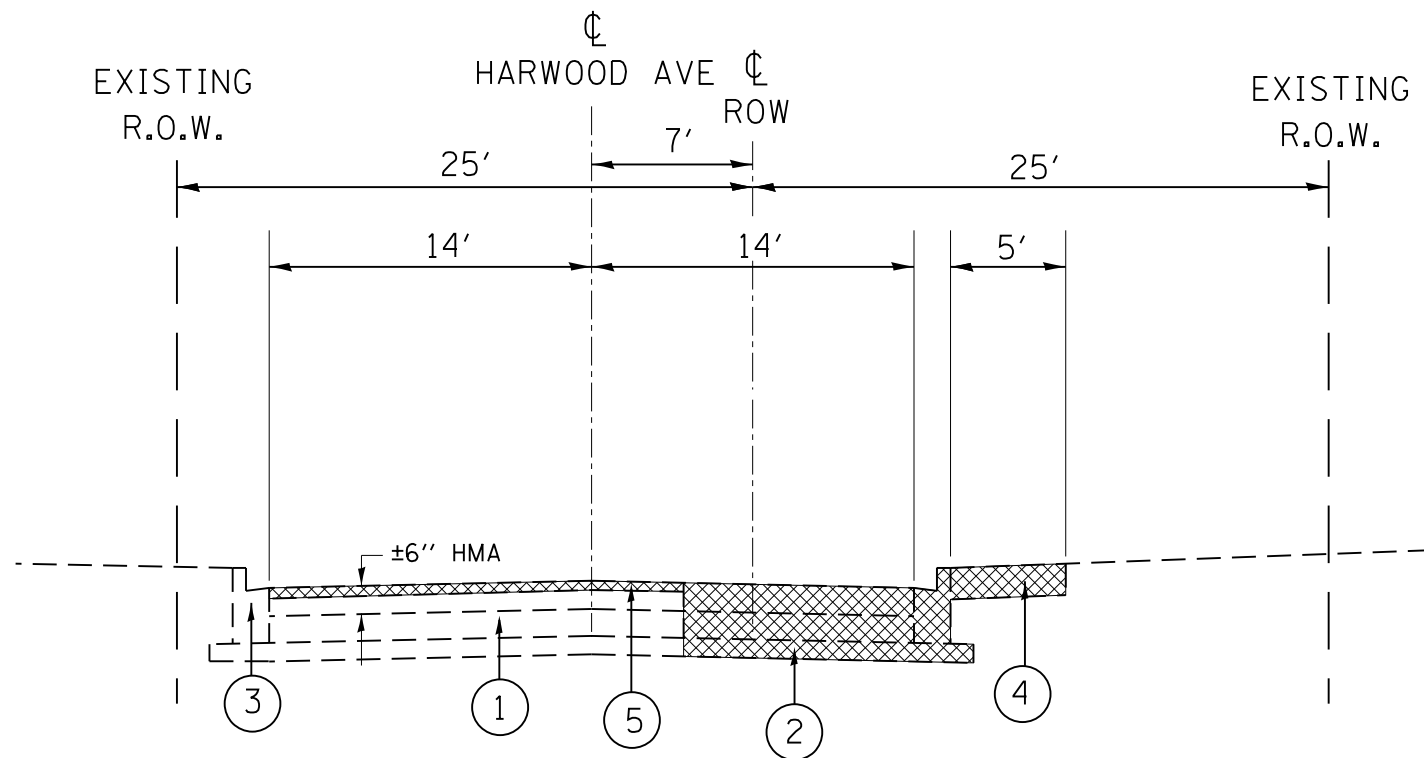


**CONTRACTOR SHALL MILL FIRST
BEFORE PAVING**

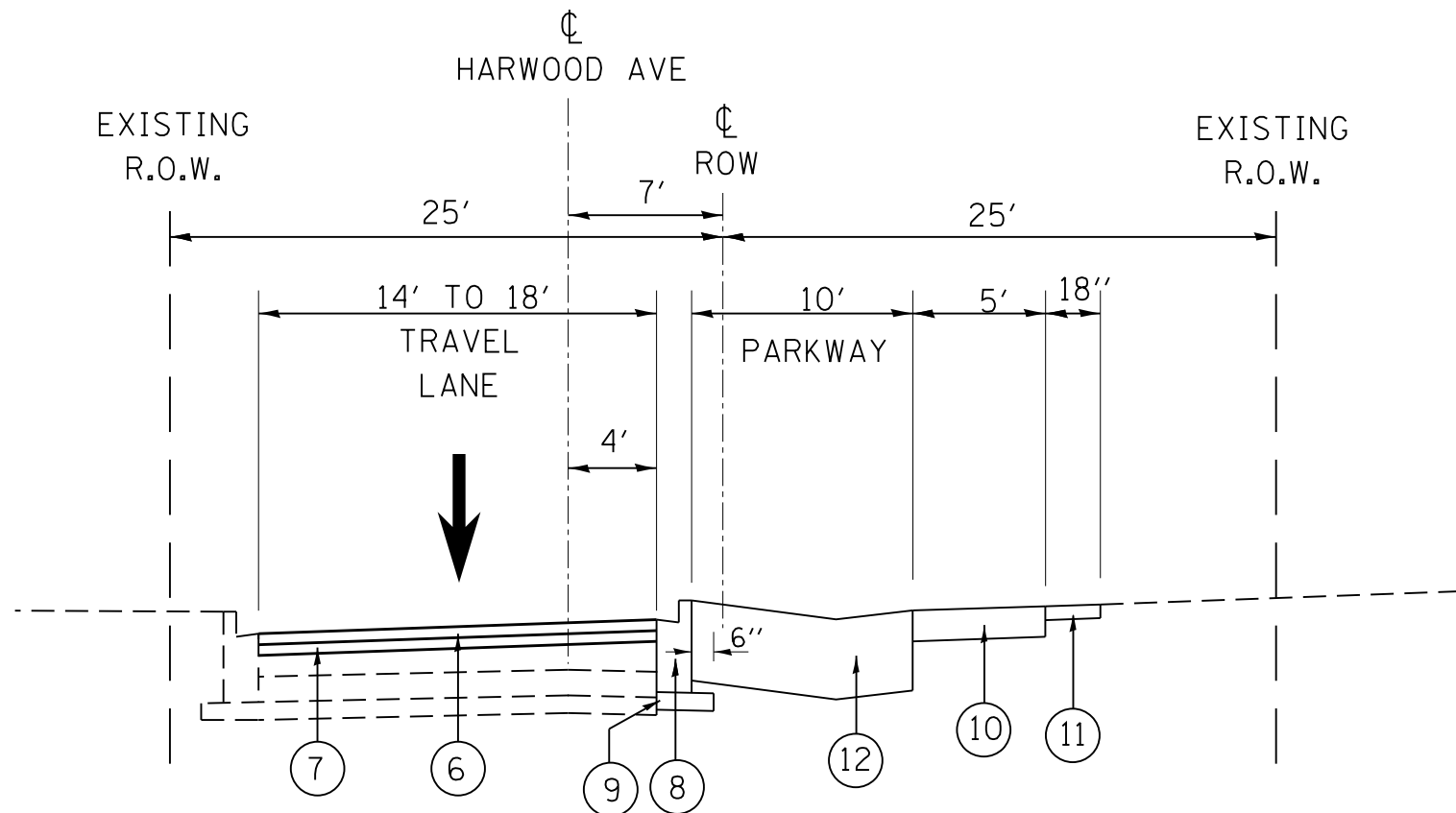
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 1 1/2"	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR USE IF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS HARWOOD AVE (ELM ST - 183rd ST)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	2845	3160A-RS&N	COOK	27	6
	PLOT DATE = 4/27/2013	CHECKED -	REVISED -												
		DATE -	REVISED -												



**EXISTING TYPICAL SECTION
HARWOOD AVENUE
STA 213+71 TO STA 216+22**



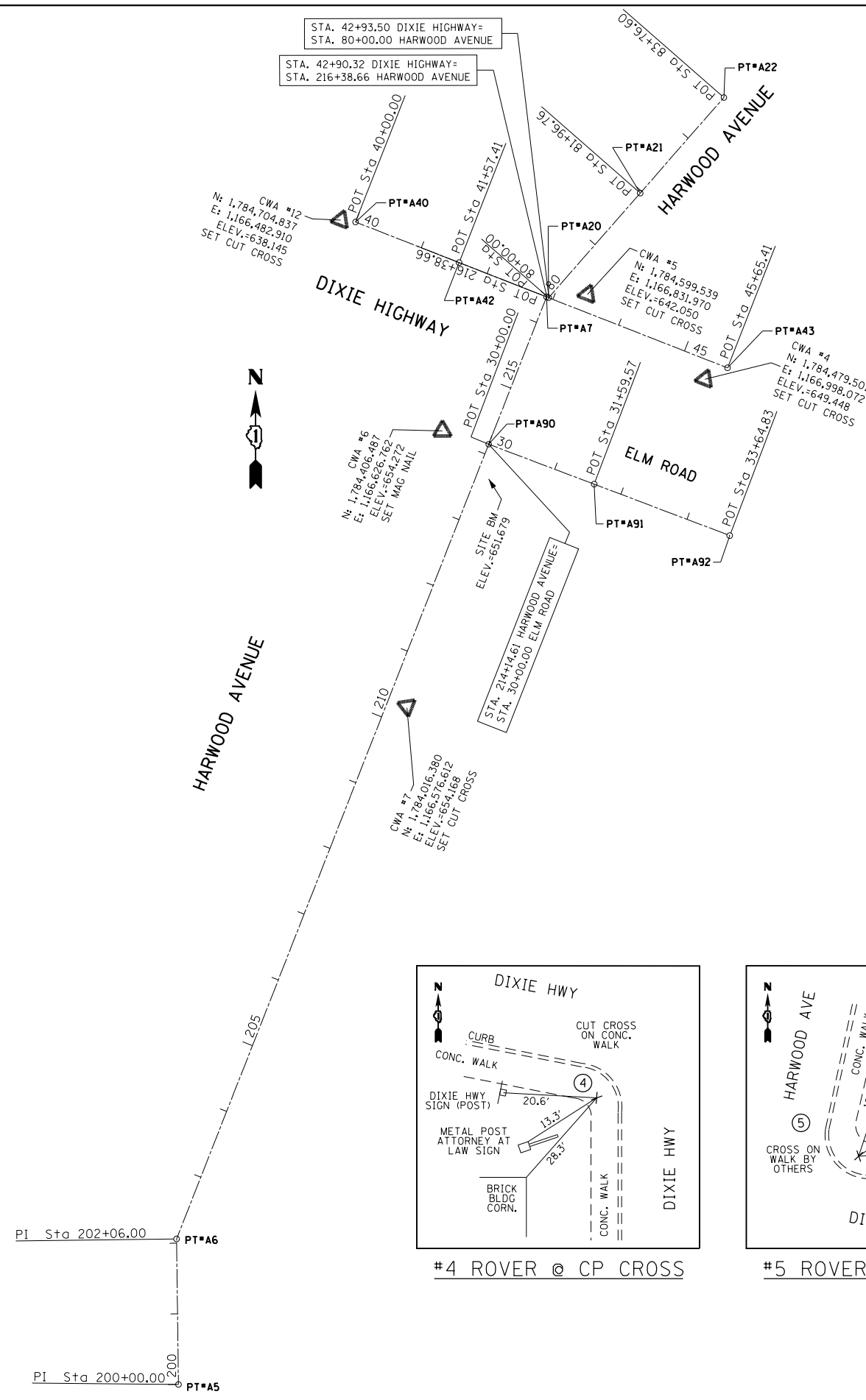
**PROPOSED TYPICAL SECTION
HARWOOD AVENUE
STA 213+71 TO STA 216+22**

LEGEND

- ① EXISTING HMA BASE COURSE, ± 8 1/4"
- ② EXISTING STABILIZED AGGREGATE SUBBASE, 4"
- ③ EXISTING COMBINATION CONCRETE C&G TYPE B-6.12
- ④ EXISTING P.C.C. SIDEWALK, 5"
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50 3/4"
- ⑧ PROPOSED COMBINATION CONCRETE C&G, TYPE B-6.12
- ⑨ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑩ PROPOSED P.C.C. SIDEWALK, 5"
- ⑪ PROPOSED TOPSOIL, 4"
- ⑫ PROPOSED TOPSOIL, 14"



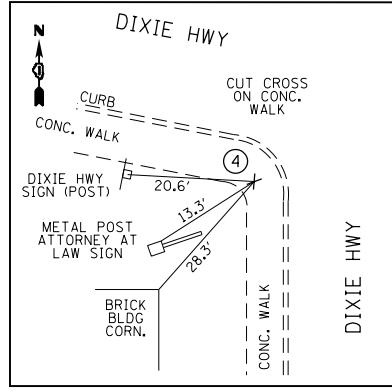
FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS HARWOOD AVE (ELM ST - DIXIE HWY)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
Default	2118111-sht-typical.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	2845	3160A-RS&N	COOK	27	7
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -												
	PLOT DATE = 4/27/2013	DATE -	REVISED -												
											ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T99	



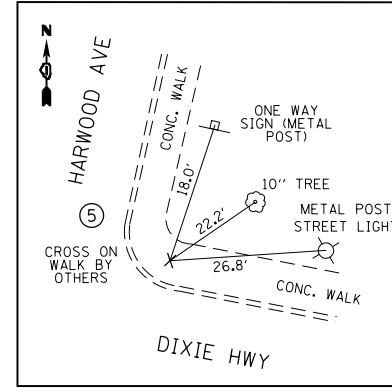
BENCHMARK

N.E. BOLT ON FIRE HYDRANT @ S.E. CORNER OF ELM ROAD AND HARWOOD AVE.

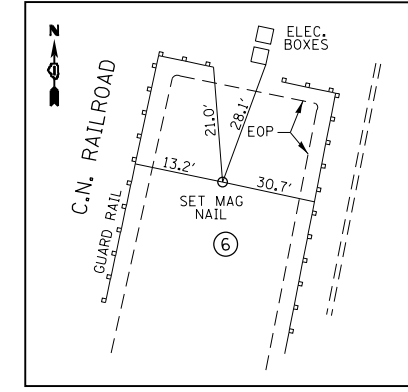
HARWOOD AVE. (SOUTH)	PT# A5	N 1783056.5261	E 1166253.2684	STA. 200+00
	PT# A6	N 1783262.5089	E 1166250.6059	STA. 202+06
	PT# A7	N 1784596.0866	E 1166774.1489	STA. 216+38.66
HARWOOD AVE. (NORTH)	PT# A20	N 1784594.9227	E 1166777.1161	STA. 80+00
	PT# A21	N 1784743.0032	E 1166906.6863	STA. 81+96.76
ELM ST.	PT# A22	N 1784878.3427	E 1167025.1082	STA. 83+76.60
	PT# A90	N 1784387.5294	E 1166692.2725	STA. 30+00
	PT# A91	N 1784330.9831	E 1166841.4892	STA. 31+59.57
DIXIE HWY	PT# A92	N 1784258.2460	E 1167033.4312	STA. 33+64.83
	PT# A40	N 1784702.1054	E 1166503.8833	STA. 40+00
	PT# A41	N 1784644.6207	E 1166650.4247	STA. 41+57.41
	PT# A42	N 1784495.6262	E 1167030.2451	STA. 45+65.41



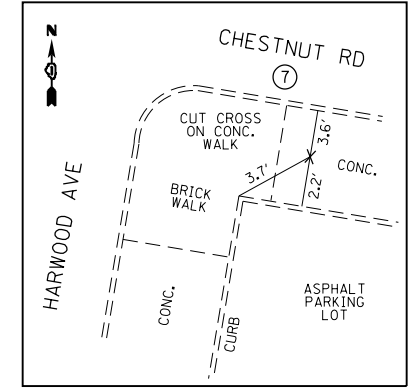
#4 ROVER @ CP CROSS



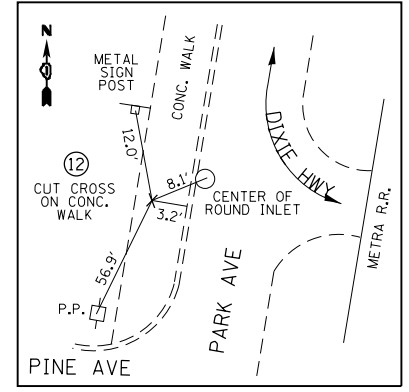
#5 ROVER @ CP CROSS



#6 ROVER @ CP MAG NAIL



#7 ROVER @ CP CROSS



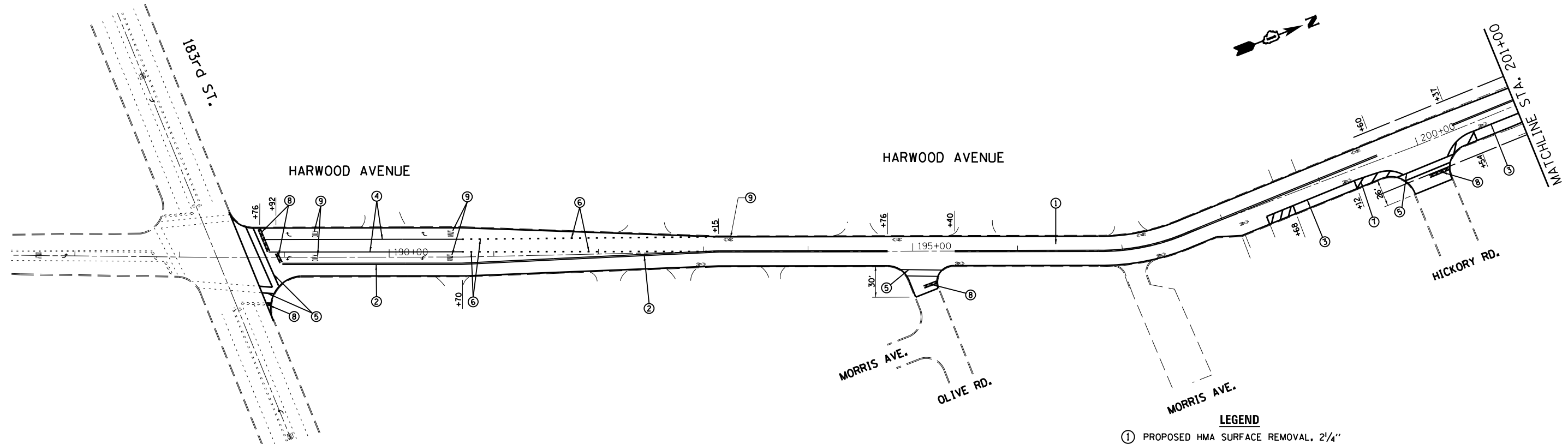
#12 ROVER @ CP CROSS

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -
c:\pwork\pwork\midyja\0267296\PI18111	sh-t-ATB.dgn	DRAWN -	REVISED -
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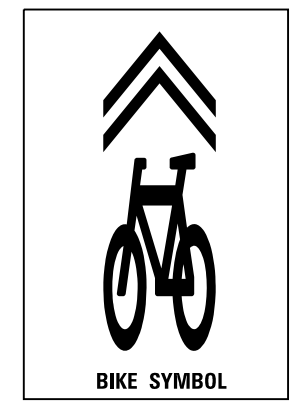
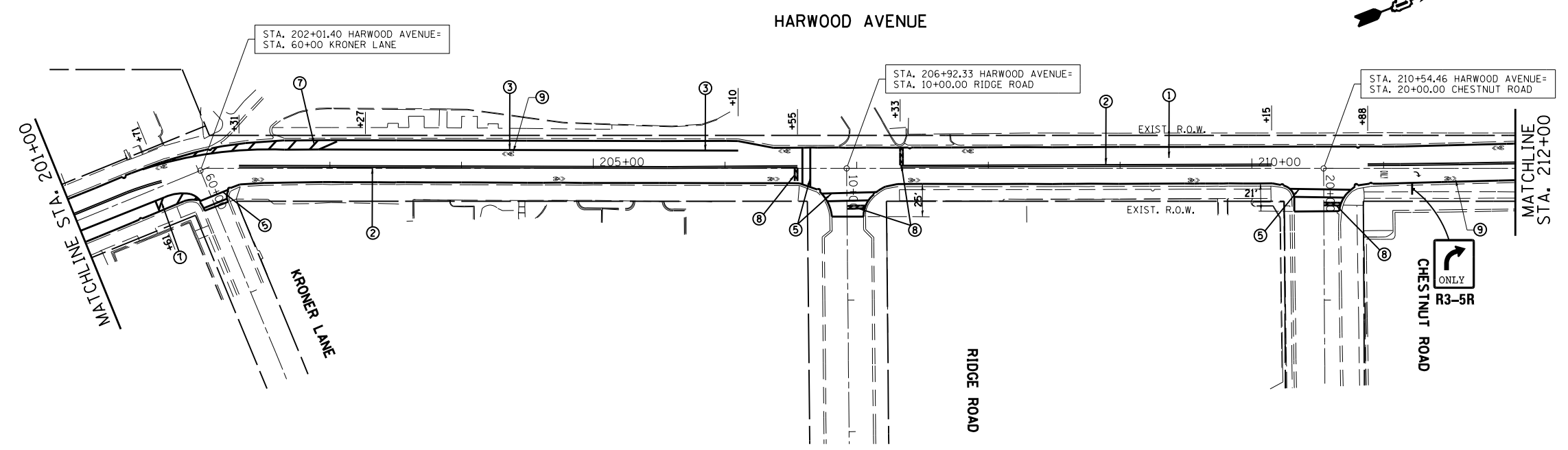
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES, AND BENCHMARK			
HARWOOD AVE (ELM RD - DIXIE HWY)			
SCALE: 1"=100'	SHEET	OF SHEETS	STA. TO STA.

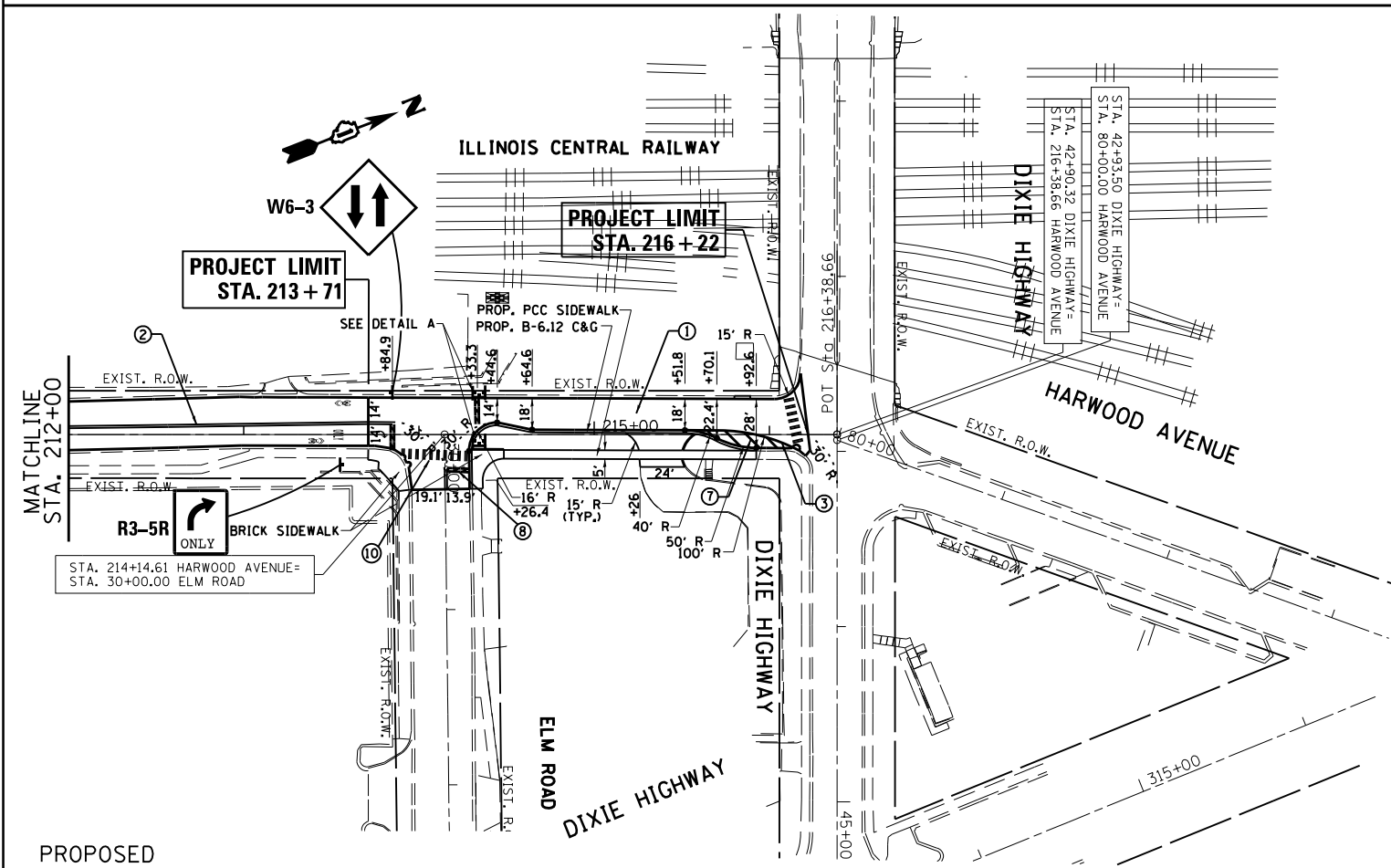
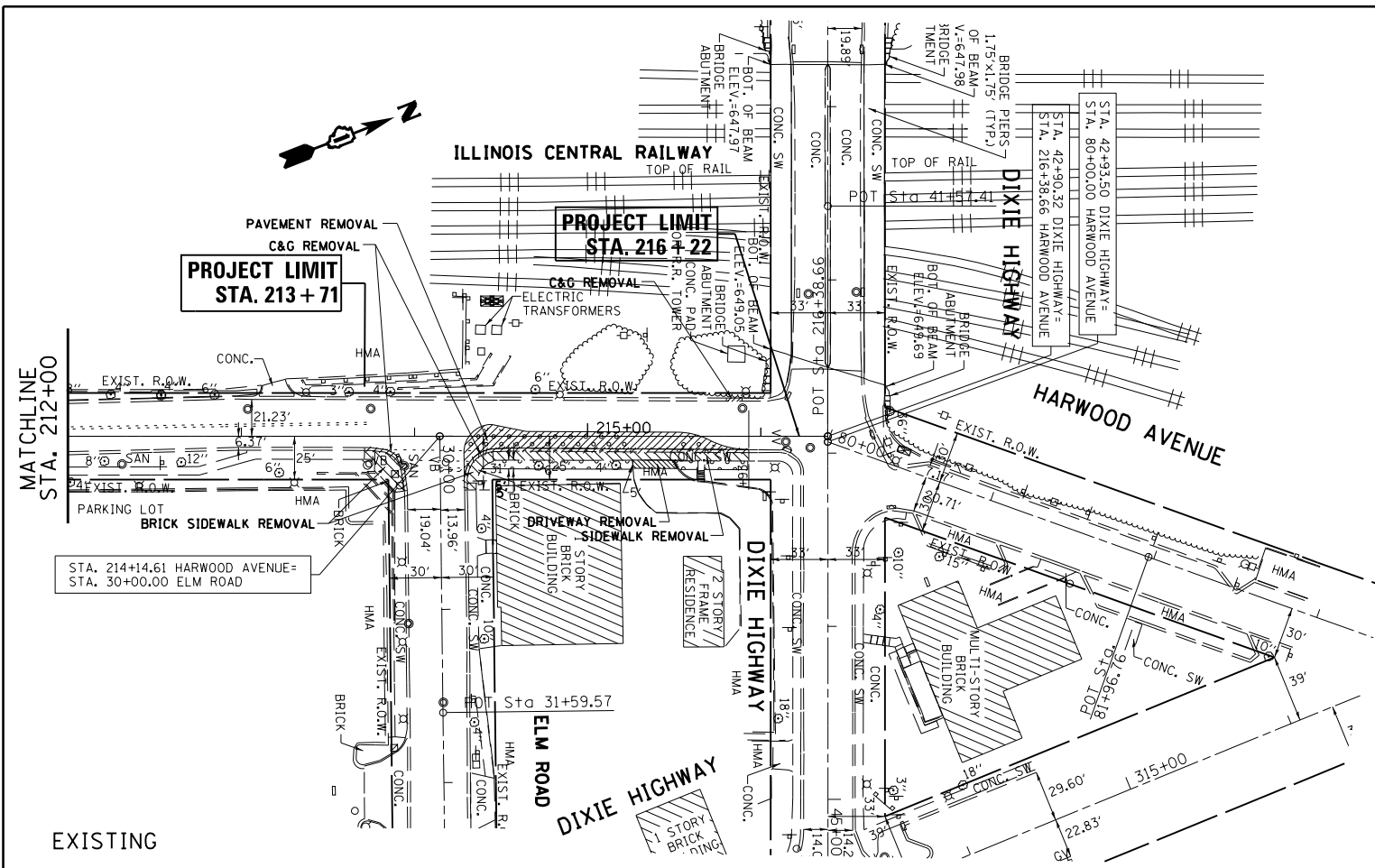
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	8
CONTRACT NO. 60T99				
ILLINOIS FED. AID PROJECT				



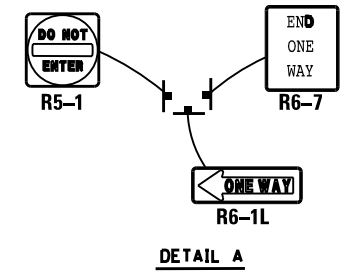
- LEGEND**
- ① PROPOSED HMA SURFACE REMOVAL, 2 1/4"
 - ② PROPOSED HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
 - ③ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
 - ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE CENTERLINE, YELLOW (TYPICAL)
 - ⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, WHITE (TYPICAL)
 - ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE LINE, WHITE (TYPICAL)
 - ⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", PEDESTRIAN CROSSWALK, WHITE (TYPICAL)
 - ⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", 2' LINE-6' SPACING, WHITE (TYPICAL)
 - ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", SHOULDER DIAGONALS, WHITE (TYPICAL)
 - ⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP LINE, WHITE (TYPICAL)
 - ⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYPICAL)
 - ⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12" CROSSWALK, WHITE (TYPICAL)



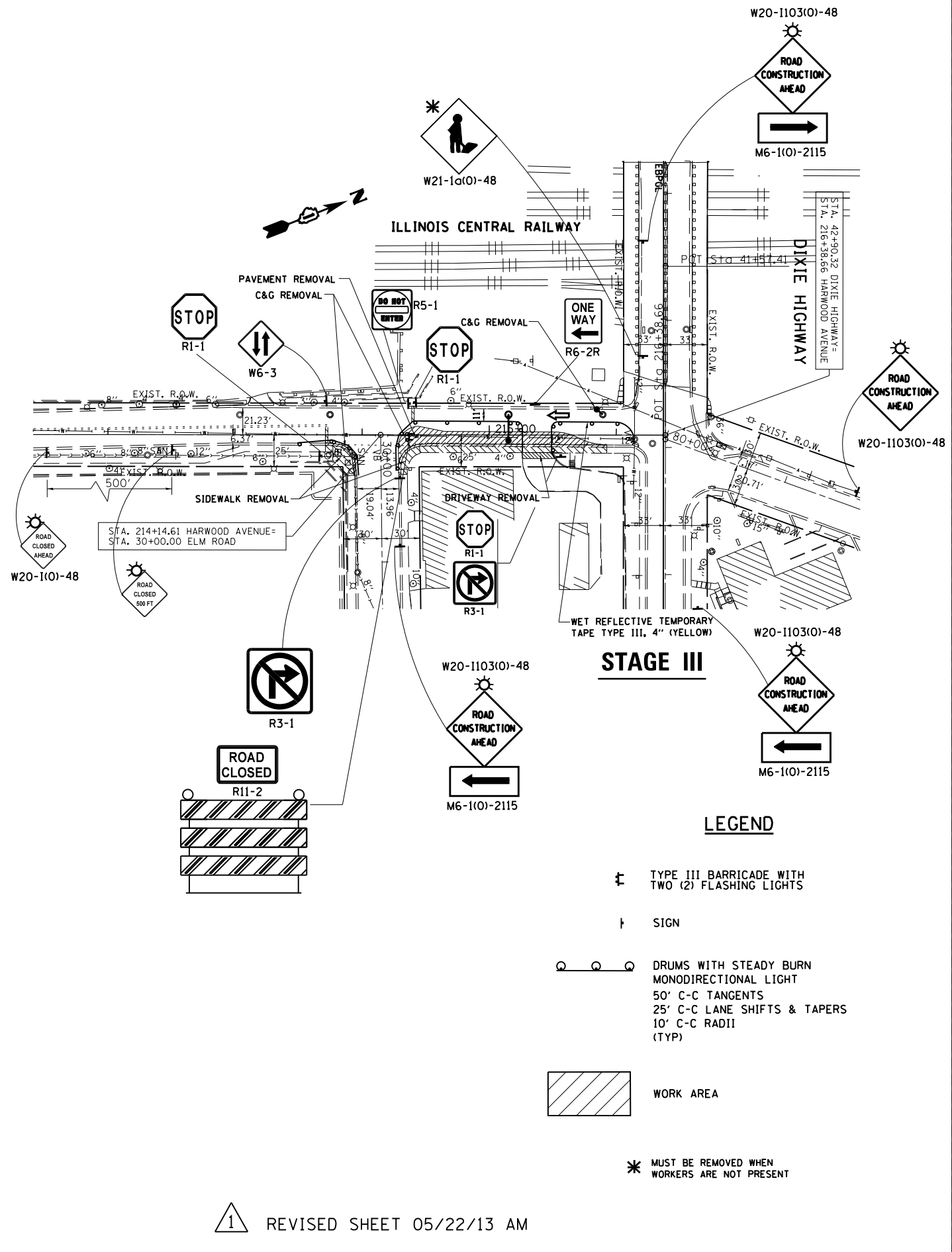
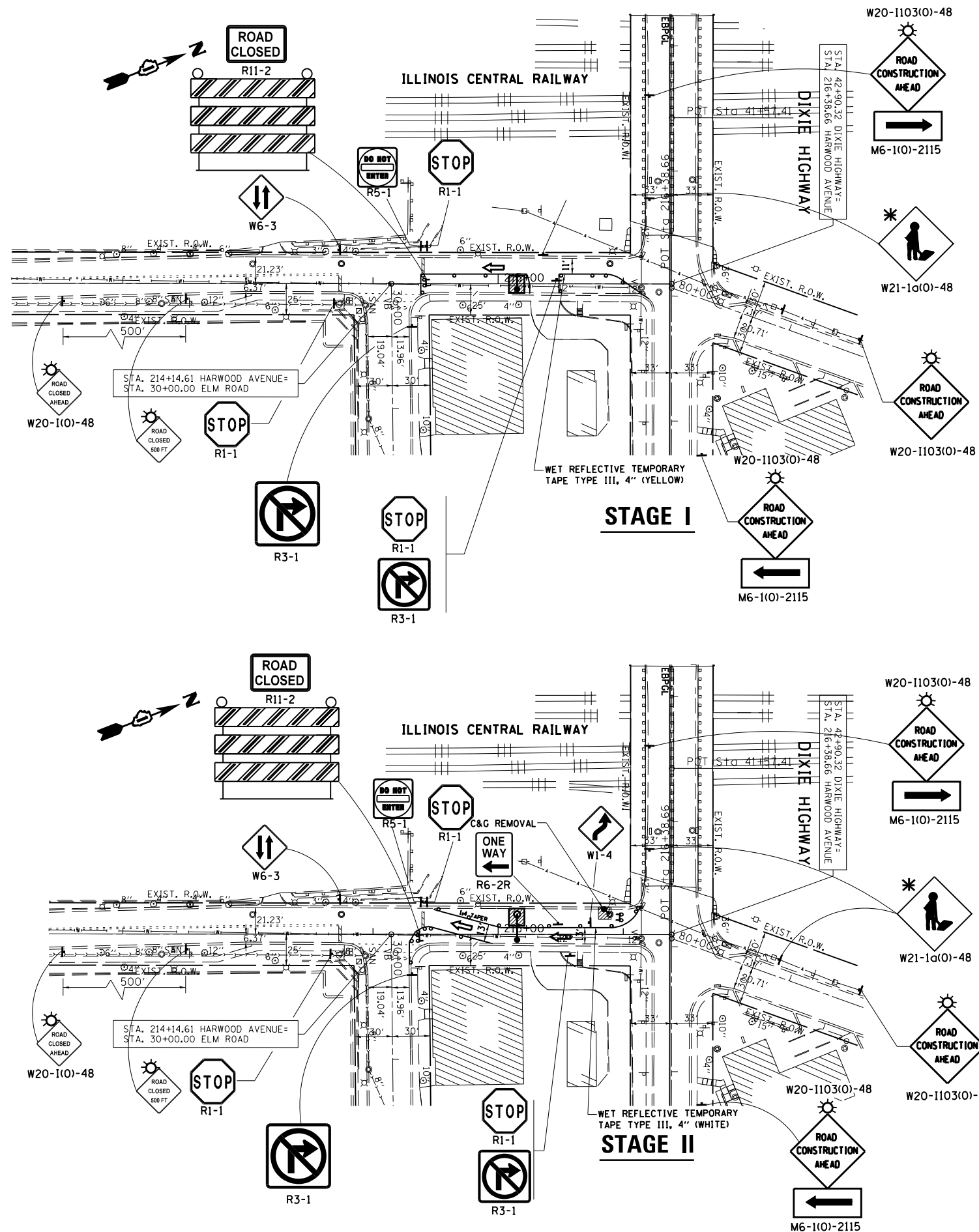
FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED ROADWAY & PAVEMENT MARKING PLAN HARWOOD AVE. (ELM ST. - DIXIE HWY)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	et:\pwork\pwork\midyja\0267284\PI18111\sh-typical.dgn	DRAWN -	REVISED -			2845	3160A-RS&N	COOK	27	9	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60T99					
	PLOT DATE = 4/27/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



- LEGEND**
- ① PROPOSED HMA SURFACE REMOVAL, 2 1/4"
 - PROPOSED HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
 - PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
 - ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE CENTERLINE, YELLOW (TYPICAL)
 - ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, WHITE (TYPICAL)
 - ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE LINE, WHITE (TYPICAL)
 - ⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", PEDESTRIAN CROSSWALK, WHITE (TYPICAL)
 - ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", 2' LINE-6' SPACING, WHITE (TYPICAL)
 - ⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", SHOULDER DIAGONALS, WHITE (TYPICAL)
 - ⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP LINE, WHITE (TYPICAL)
 - ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYPICAL)
 - ⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12" CROSSWALK, WHITE (TYPICAL)

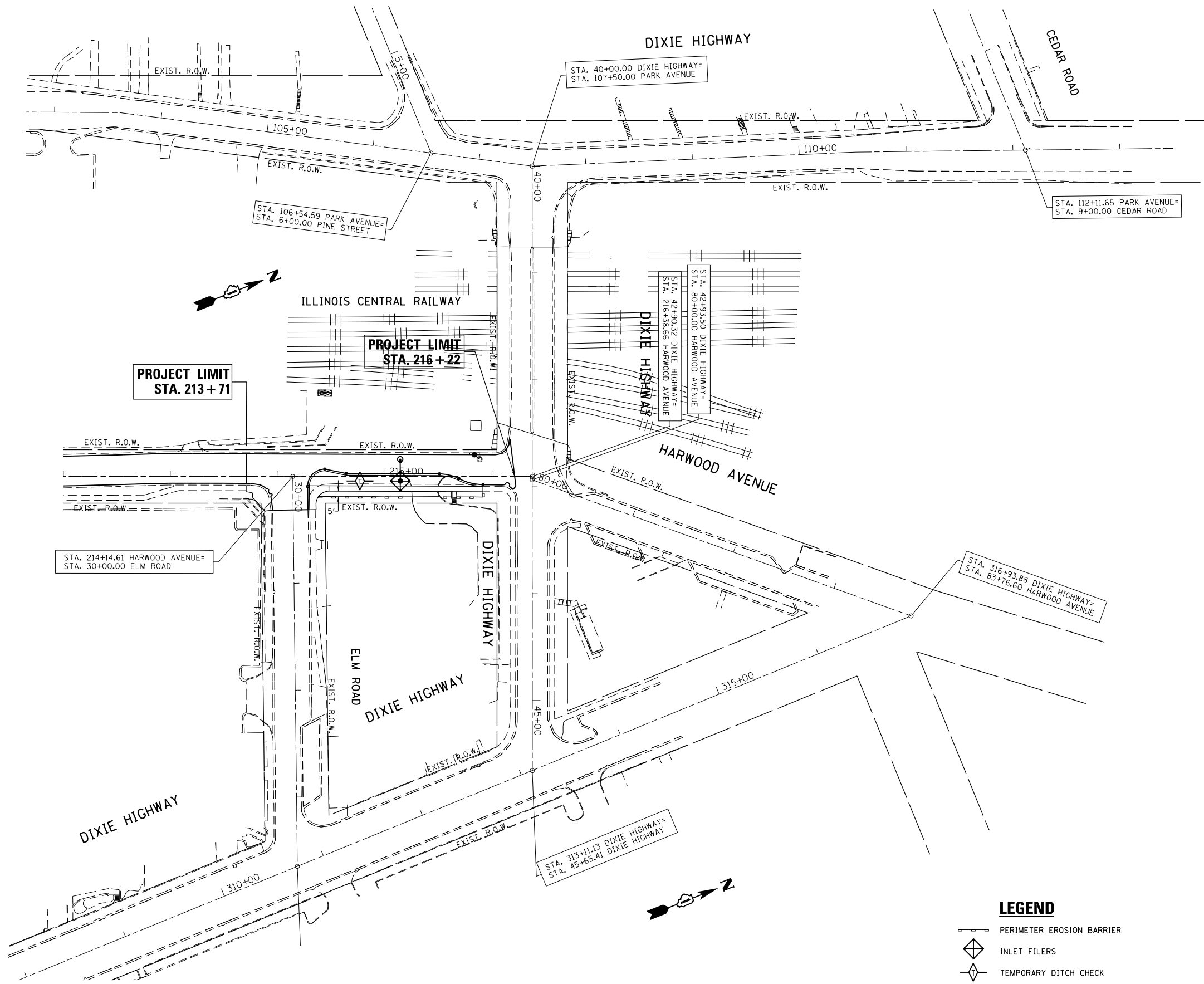


FILE NAME =	USER NAME = midjja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING & PROPOSED ROADWAY & PAVEMENT MARKING PLAN HARWOOD AVE. (ELM ST. - DIXIE HWY)	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			2845	3160A-RS&N	COOK	27	10	
	PLOT DATE = 4/27/2013	CHECKED -	REVISED -			CONTRACT NO. 60199					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



1 REVISED SHEET 05/22/13 AM




FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGING AND TRAFFIC CONTROL HARWOOD AVENUE	F.A.U. RT#.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 100.0011 / in.	CHECKED -	REVISED -			CONTRACT NO. 60T99					
	PLOT DATE = 5/22/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



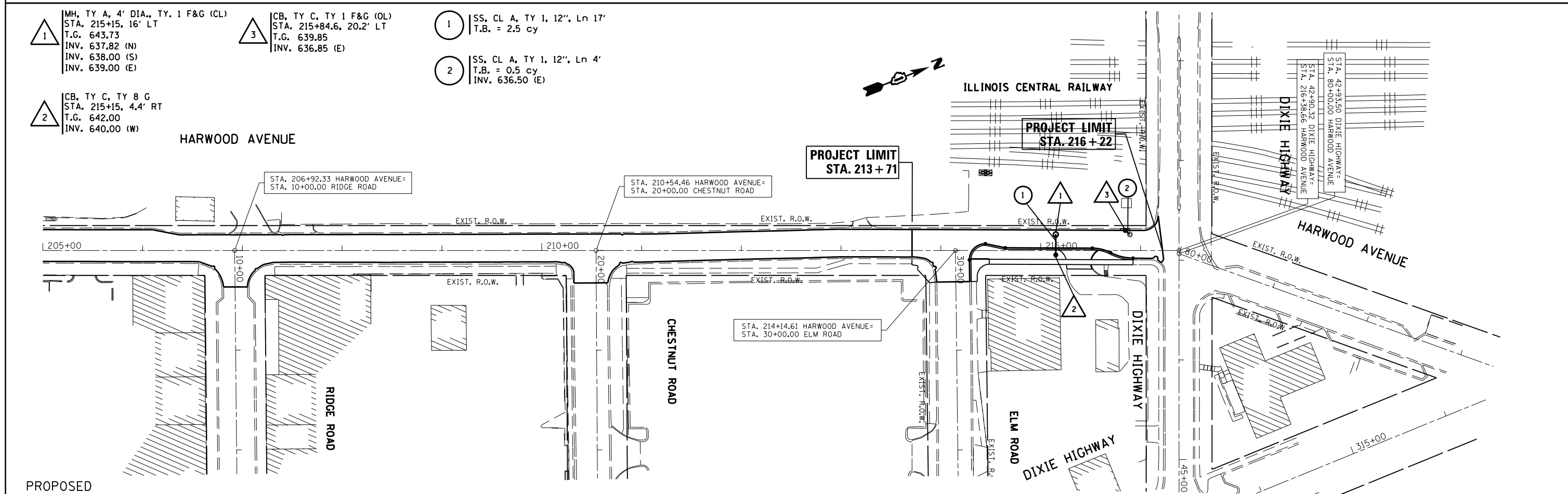
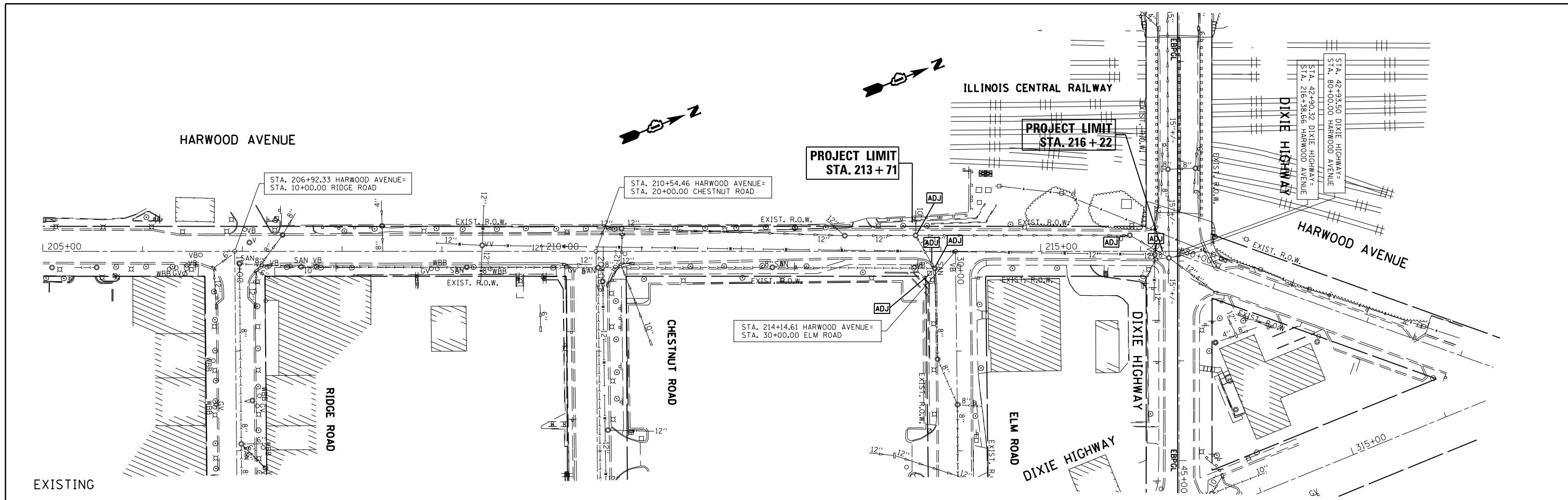
NOTES

1. TEMPORARY EROSION CONTROL SEEDING TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS, REFER TO SPECIAL PROVISIONS.
2. THE CONTRACTORS SHALL PROVIDE & MAINTAIN INLET PROTECTION AT ALL PROPOSED DRAINAGE STRUCTURES AND AT OTHER LOCATIONS DIRECTED BY THE ENGINEER AND/OR SHOWN ON THE PLANS. INLET PROTECTION SHALL BE PLACED BEFORE THE AREA INVOLVED HAS BEEN DISTURBED.
3. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARDS SPECIFICATION AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH A TIME AS DIRECTED BY THE ENGINEER.
4. AT ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND COVER SHALL REMAIN.
5. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATIONS NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. PERIMETER EROSION BARRIER SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
6. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES, DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
7. THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURES PRIOR TO STRIPPING EXISTING VEGETATION.

LEGEND

-  PERIMETER EROSION BARRIER
-  INLET FILERS
-  TEMPORARY DITCH CHECK

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EROSION CONTROL PLAN HARWOOD AVE. (ELM ST. - DIXIE HWY)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p1dot\midyja\0267296\PI18111	sh1-eros.dgn	DRAWN -	REVISED -		2845	3160A-RS&N	COOK	27	12			
\$MODELNAME\$	PLOT DATE = 3/27/2013	CHECKED -	REVISED -		SCALE: 1"=50'			CONTRACT NO. 60T99				
		DATE -	REVISED -		SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				

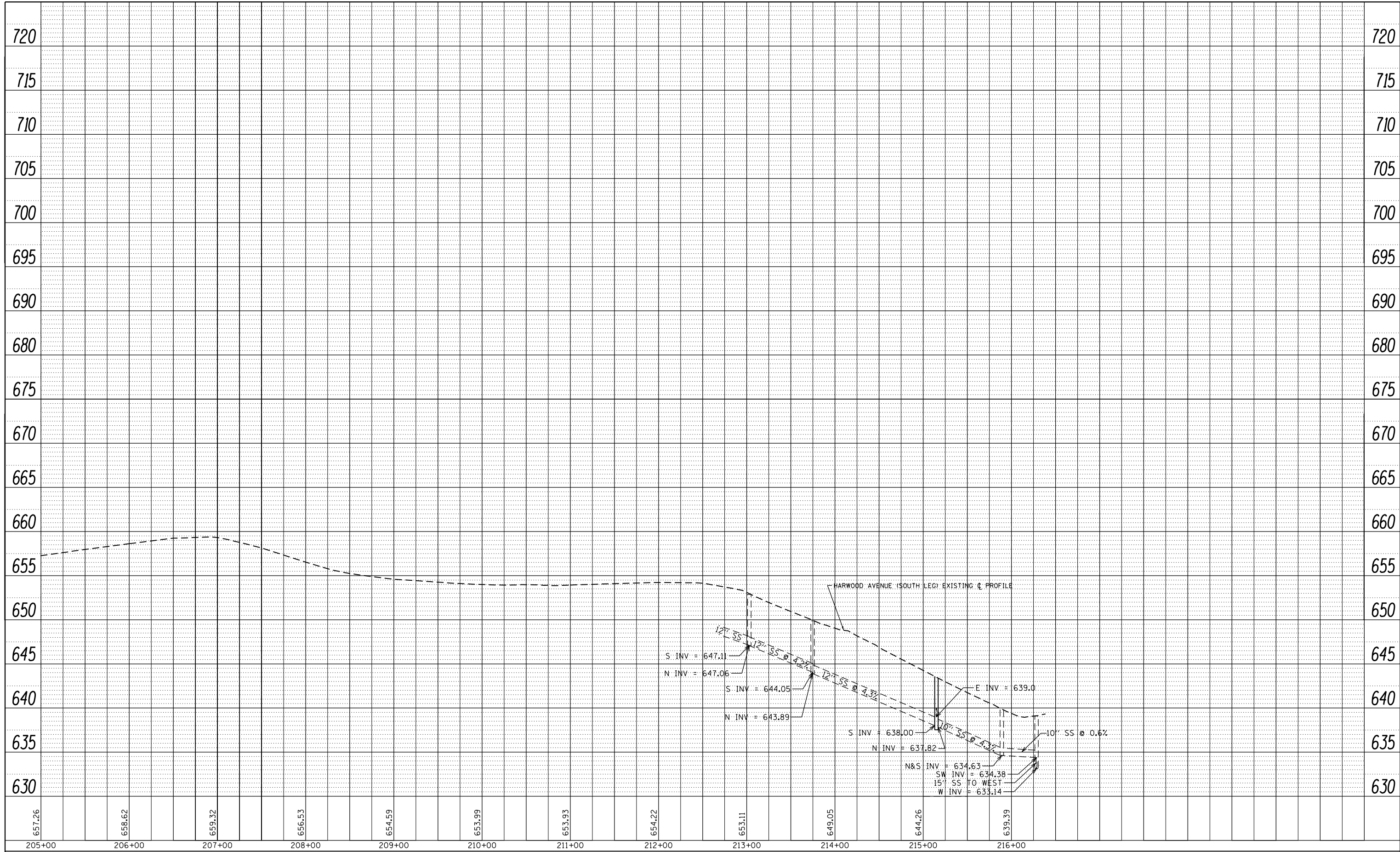


- 1 MH, TY A, 4' DIA., TY. 1 F&G (CL)
STA. 215+15, 16' LT
T.G. 643.73
INV. 637.82 (N)
INV. 638.00 (S)
INV. 639.00 (E)
- 2 CB, TY C, TY 8 G
STA. 215+15, 4.4' RT
T.G. 642.00
INV. 640.00 (W)
- 3 CB, TY C, TY 1 F&G (OL)
STA. 215+84.6, 20.2' LT
T.G. 639.85
INV. 636.85 (E)
- 1 SS, CL A, TY 1, 12", Ln 17'
T.B. = 2.5 cy
- 2 SS, CL A, TY 1, 12", Ln 4'
T.B. = 0.5 cy
INV. 636.50 (E)

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING & PROPOSED DRAINAGE PLAN HARWOOD AVE. (ELM ST. - DIXIE HWY)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pwork\midot\midyja\0267296\PI18111-sht-drain.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			2845	3160A-RS&N	COOK	27	13	
\$MODELNAME\$	PLOT DATE = 3/28/2013	CHECKED -	REVISED -			CONTRACT NO. 60199					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
CARD FILE NAME	ALIGNED	
	CHECKED	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE NOTATIONS CHECKED	

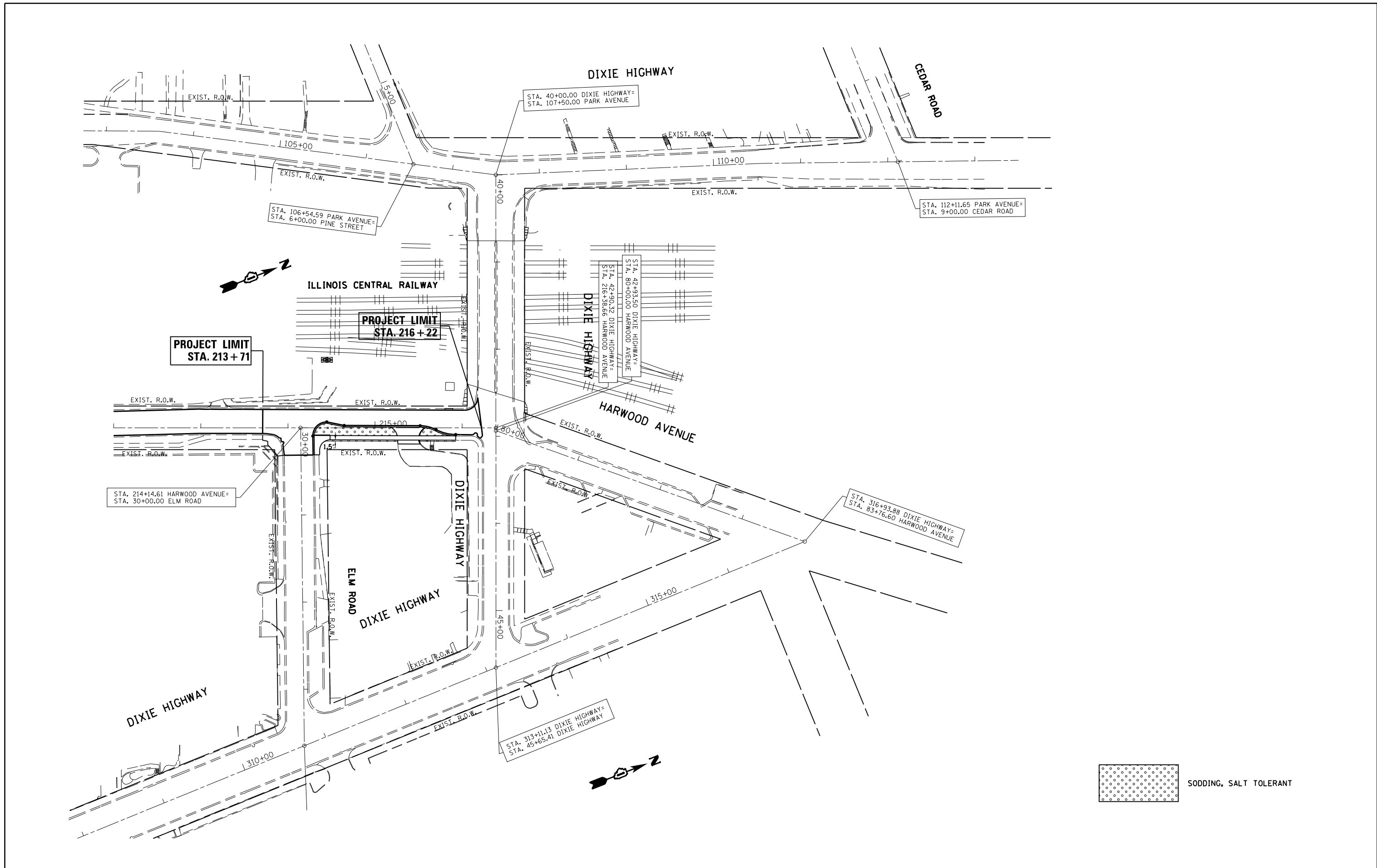


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c:\pwork\pwork\midyja\0267296\PI18111-ent-drain.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

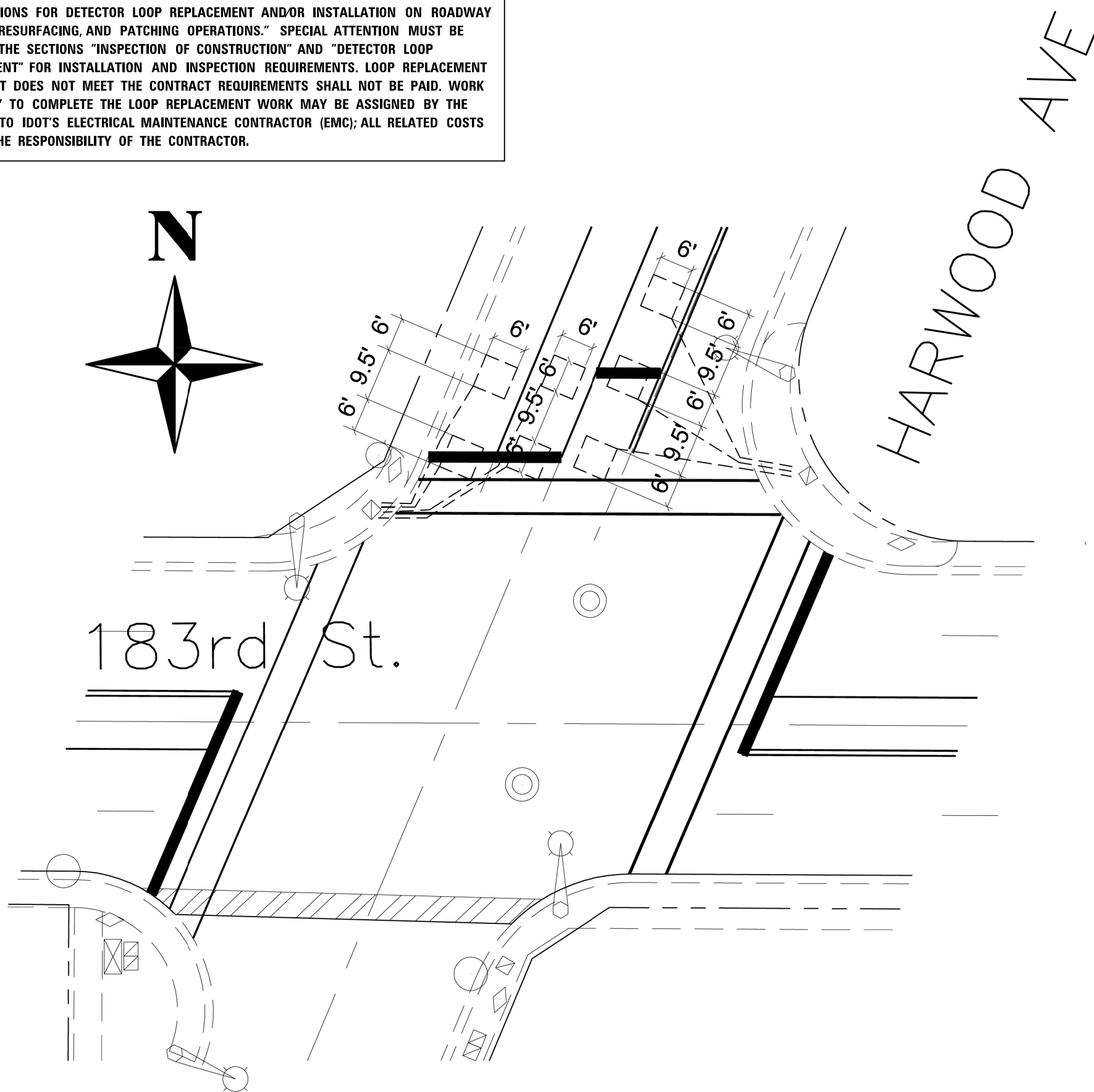
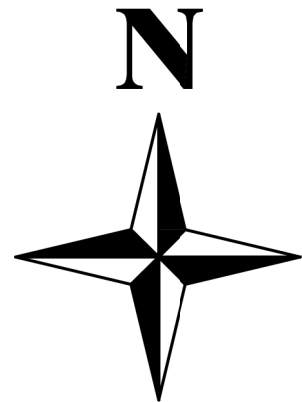
DRAINAGE PROFILE			
HARWOOD AVE. (ELM ST. - DIXIE)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	14
CONTRACT NO. 60T99				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = midvja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LANDSCAPING PLAN HARWOOD AVE. (ELM ST. - DIXIE HWY)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	et:\pw\work\p\midot\midvja\0267284\PI18111-sht-typical.dgn	DRAWN -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	27	15
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -									CONTRACT NO. 60T99	
	PLOT DATE = 4/27/2013	DATE -	REVISED -									ILLINOIS FED. AID PROJECT	

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS." SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.



TRAFFIC SIGNAL LEGEND

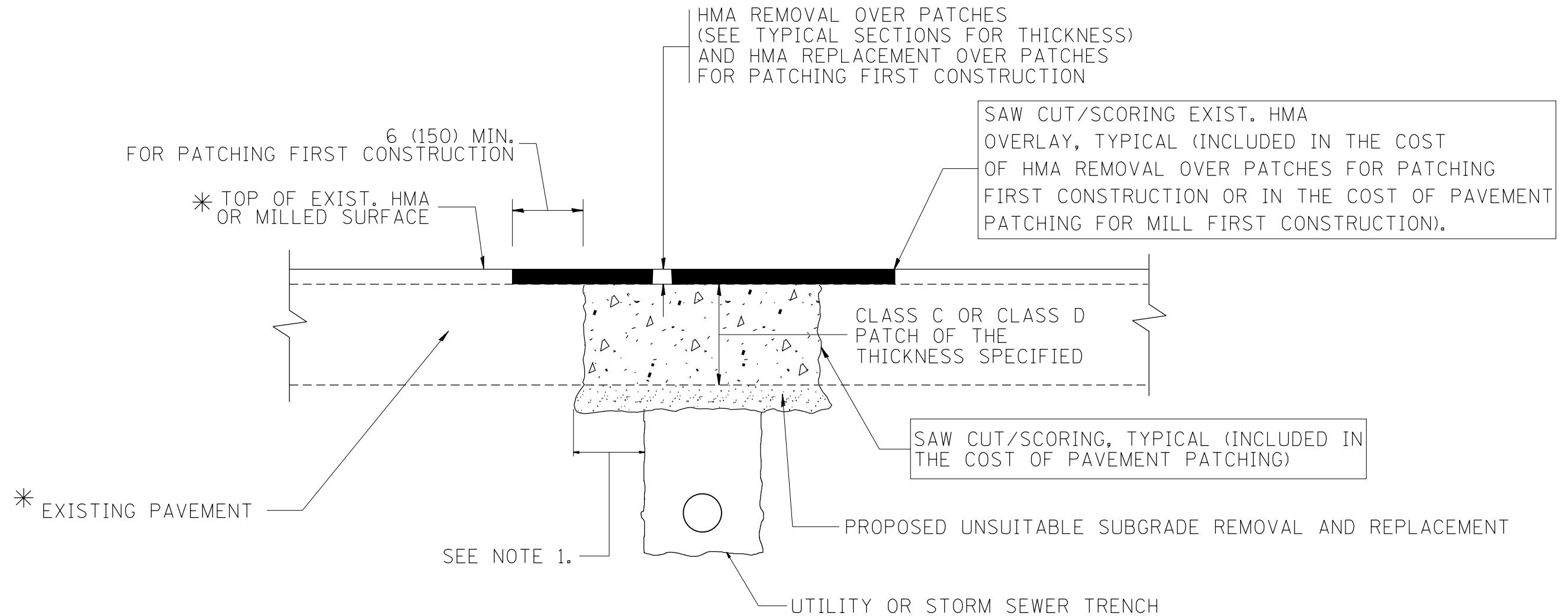
	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY.

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMIT)

CODE	QUANTITY	UNIT	ITEM
88600600	376	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN HARWOOD AVE.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pw\work\pidot\midyja\0267284\PI18111	sh-typical.dgn	DRAWN -	REVISED -			2845	3160A-RS&N	COOK	27	16	
\$MODELNAME\$	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60T99					
	PLOT DATE = 3/27/2013	DATE -	REVISED -			SCALE:	SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

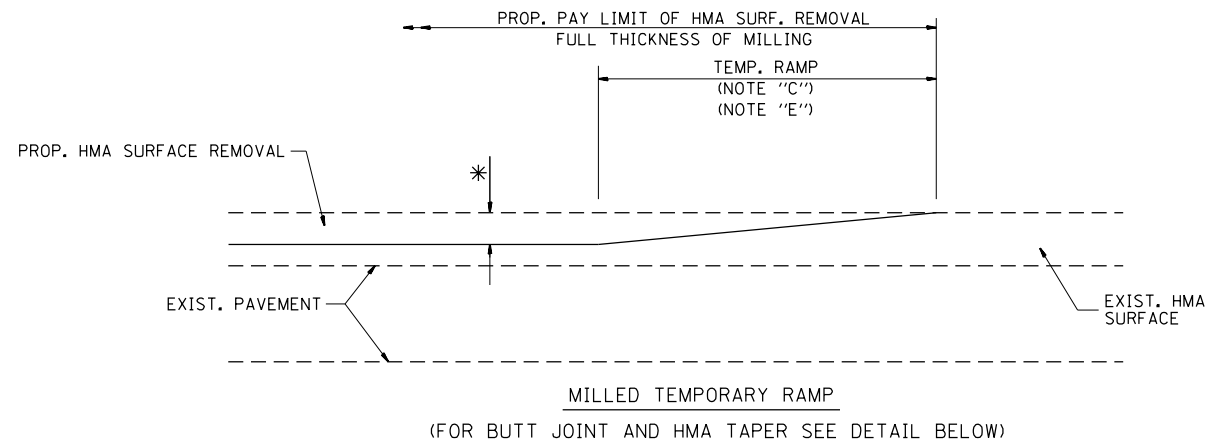
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = midyja	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
et:\pwork\pwork\midyja\0267284\DistStd.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 3/27/2013	DATE - 10-25-94	REVISED - K. ENG 10-27-08

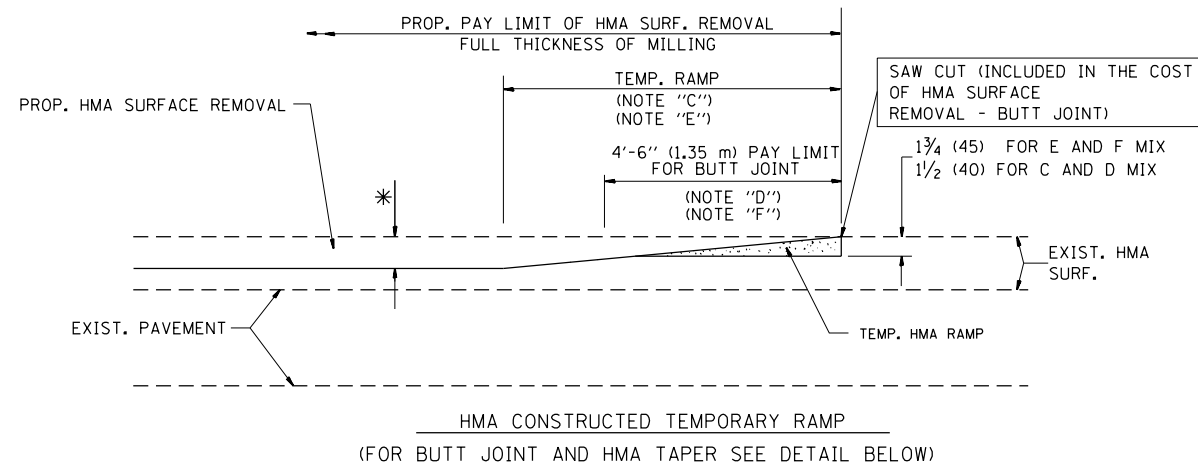
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	17
BD400-04 (BD-22)			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

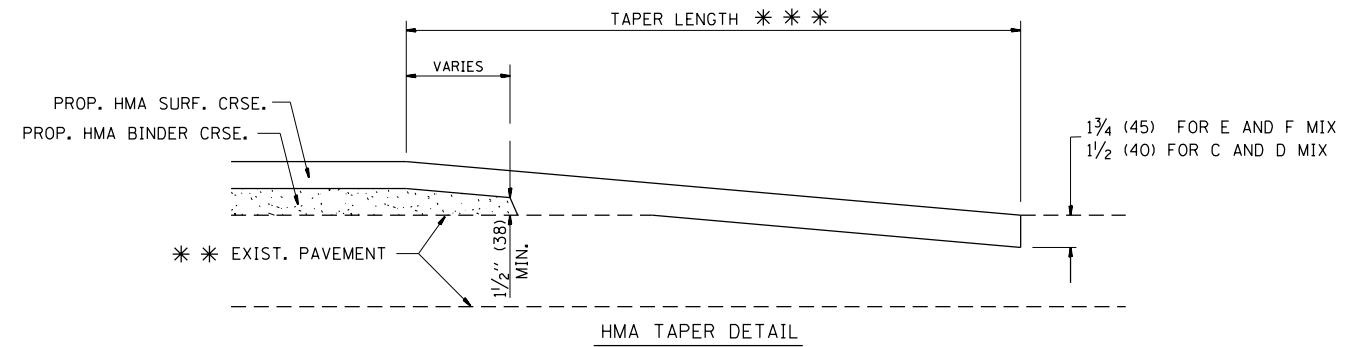
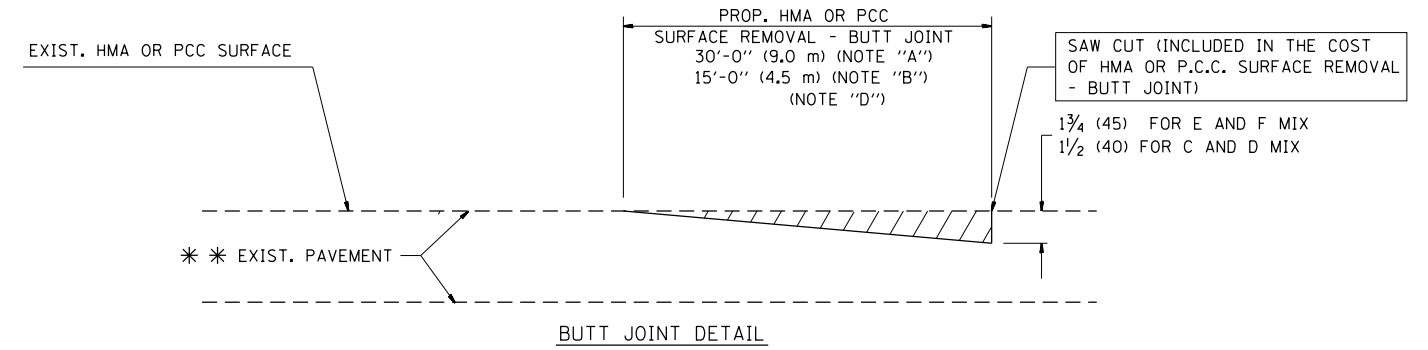


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

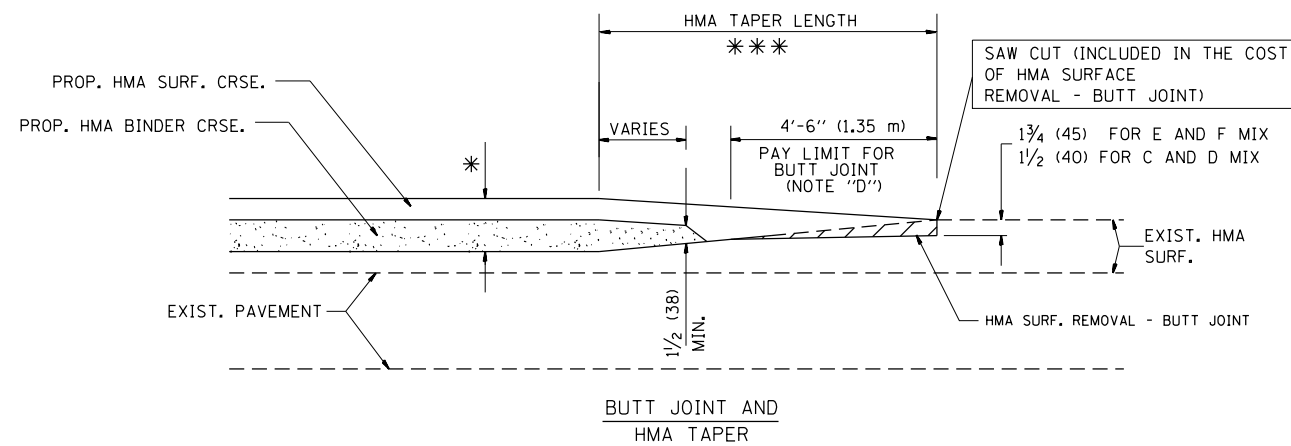
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



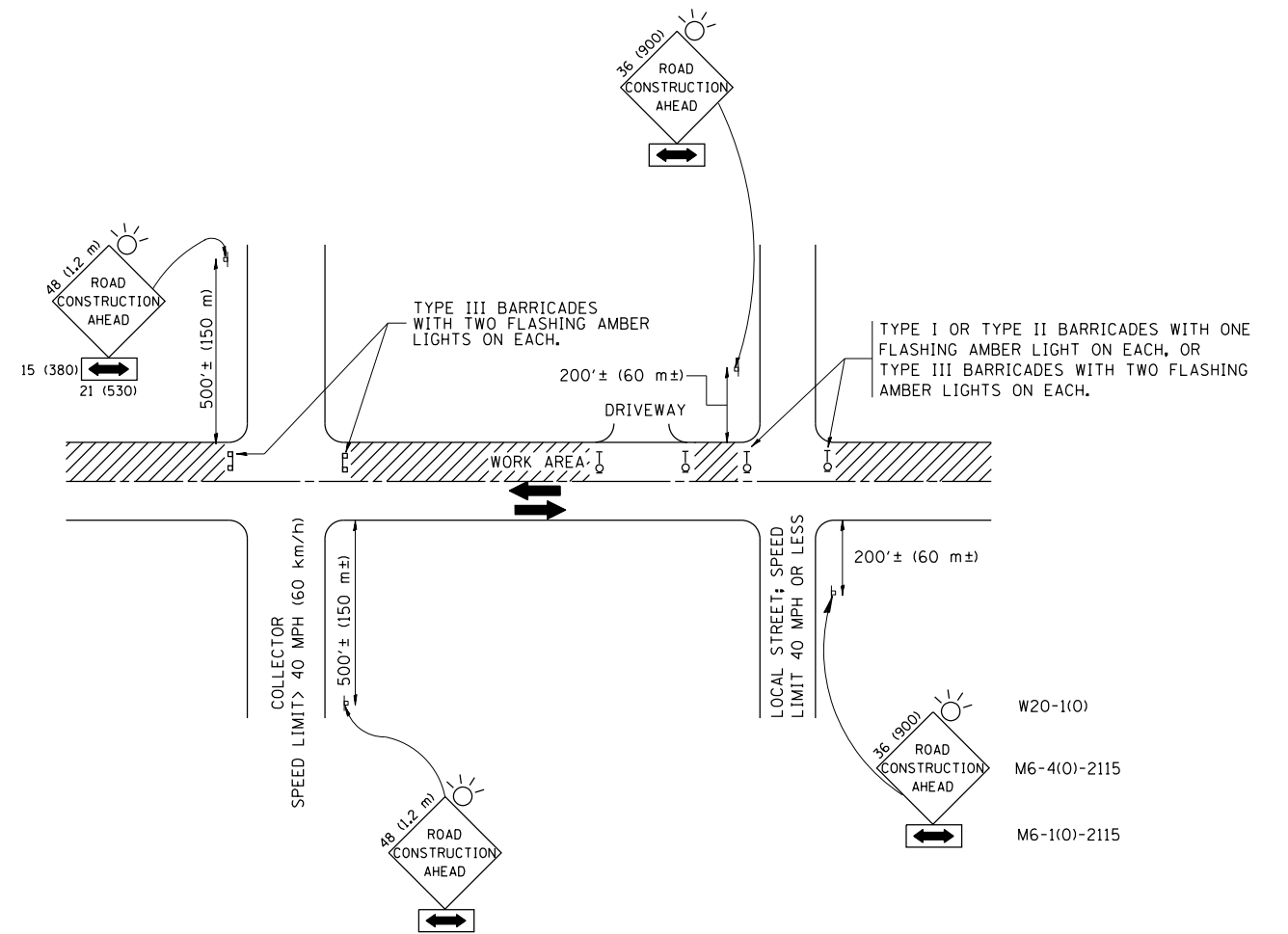
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = midyja	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
et:\pw\work\p\midot\midyja\0267284\DistStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/27/2013	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	18
BD400-05 BD32		CONTRACT NO. 60T99		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

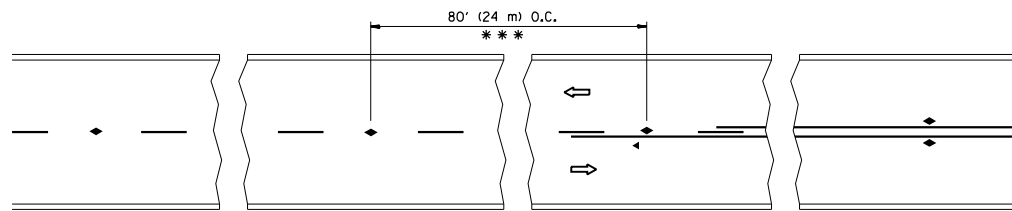
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 3/27/2013	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

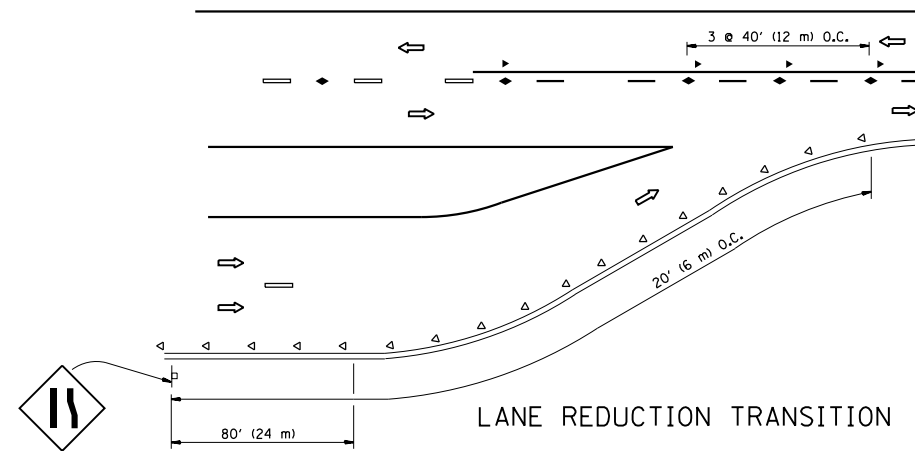
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

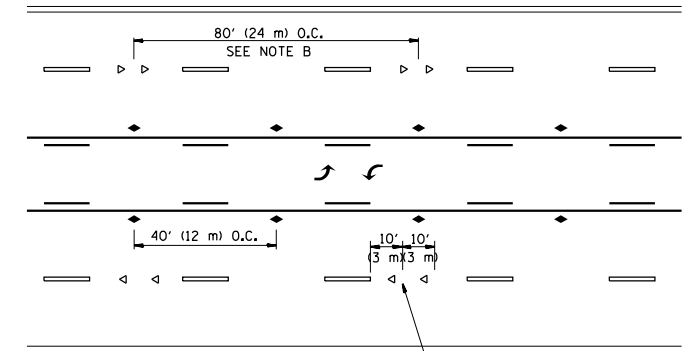


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

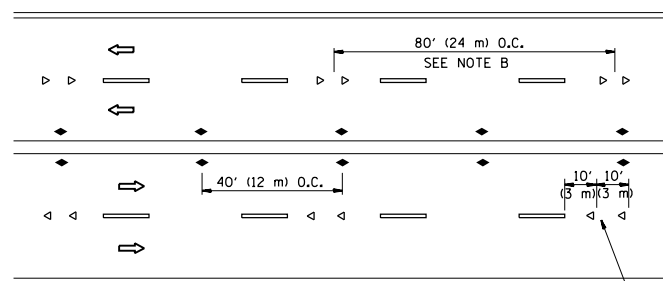
TWO-LANE/TWO-WAY



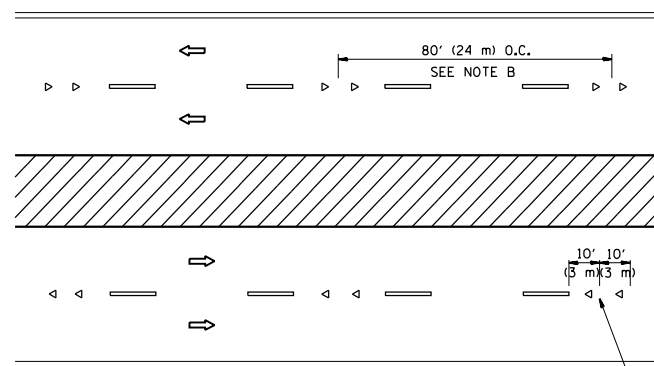
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

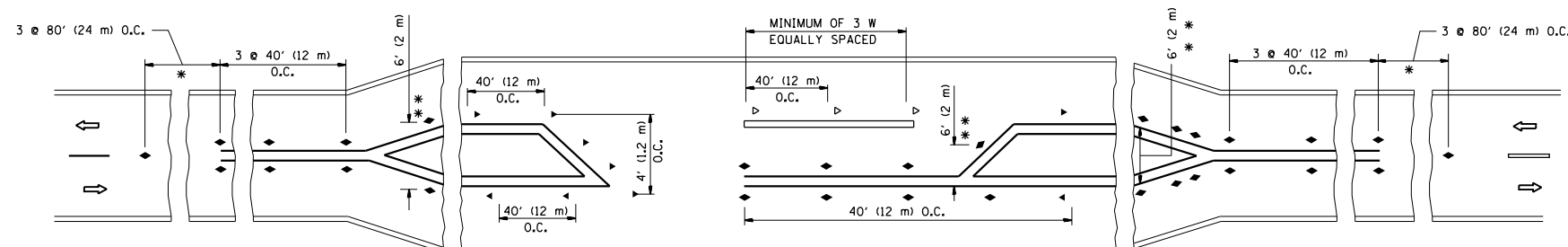
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

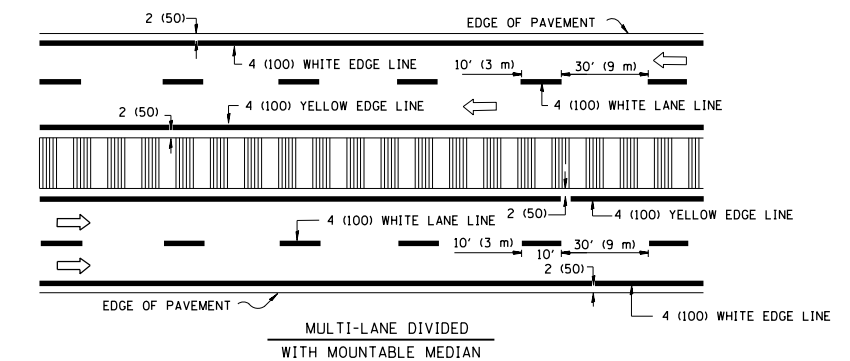
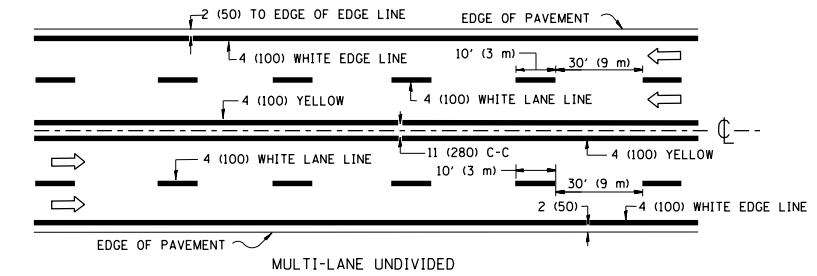
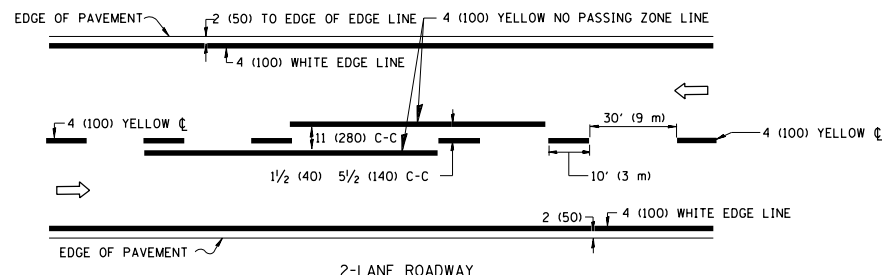
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/27/2013	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

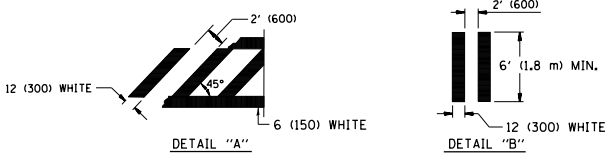
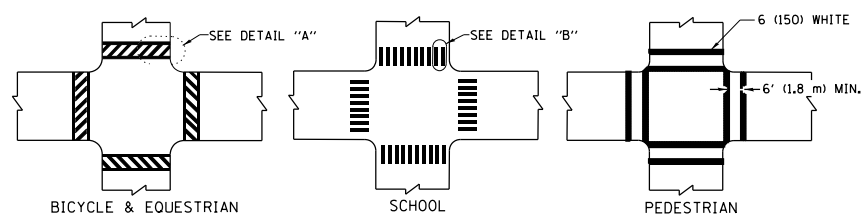
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

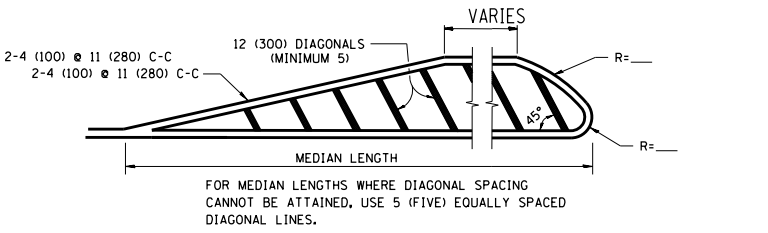
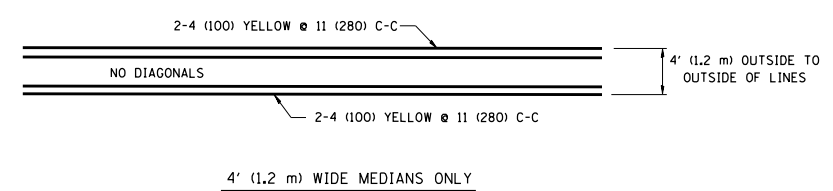


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

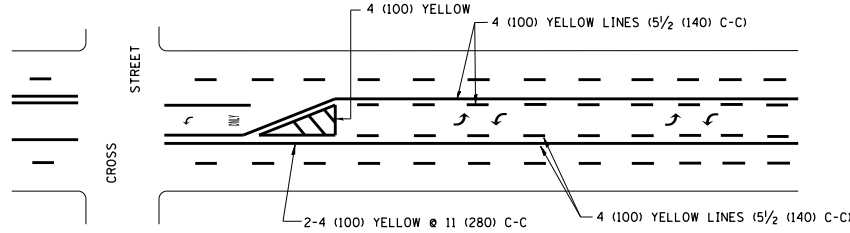


TYPICAL CROSSWALK MARKING

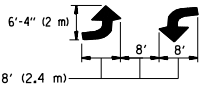


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

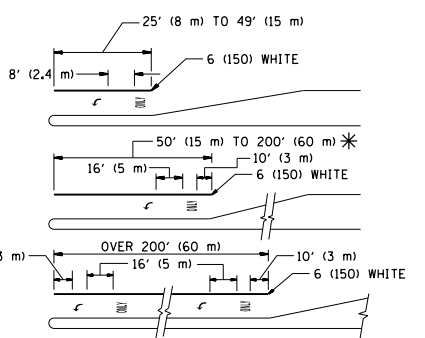


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

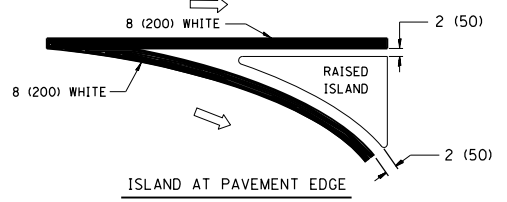
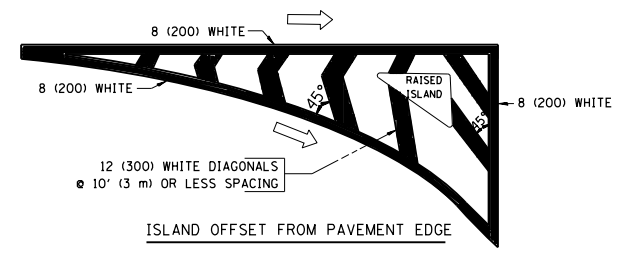


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

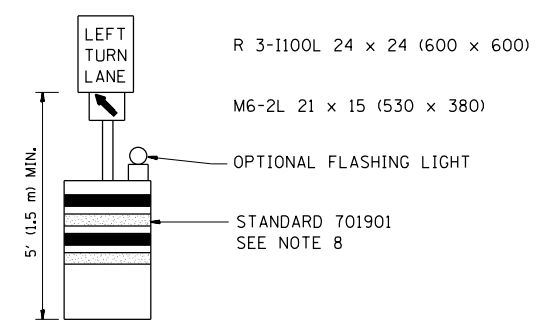
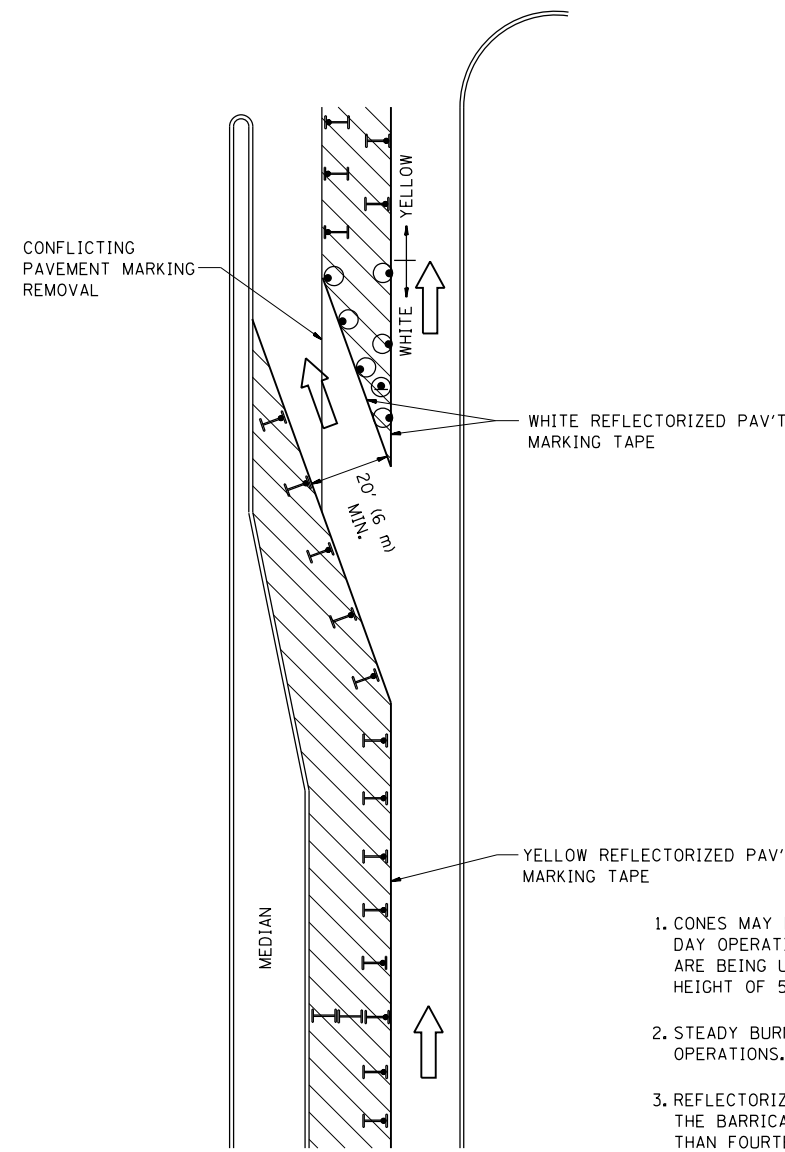
All dimensions are in inches (millimeters) unless otherwise shown.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	21
TC-13		CONTRACT NO. 60T99		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

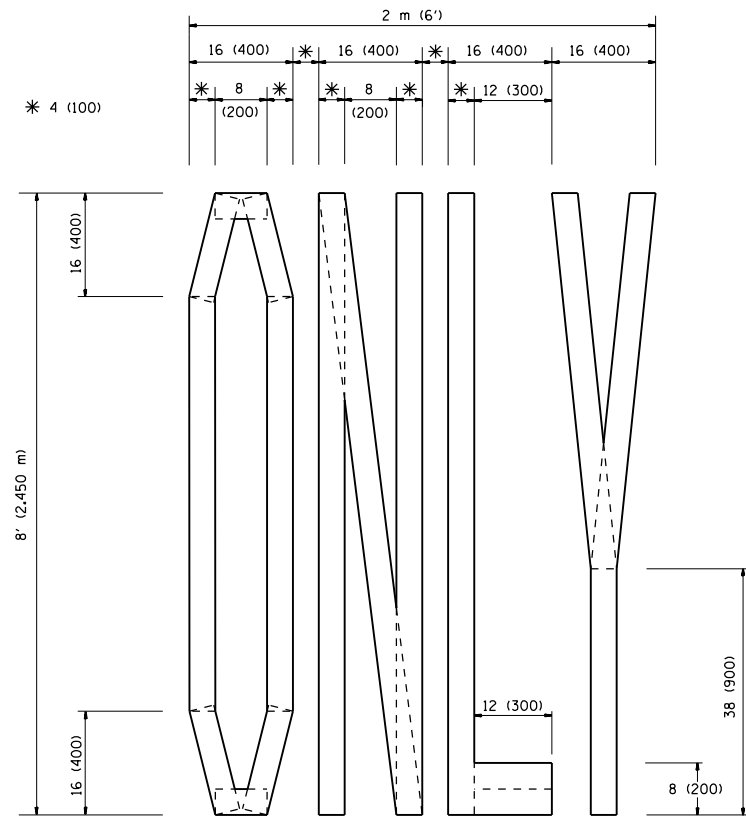
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	PLOT DATE = 3/27/2013	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

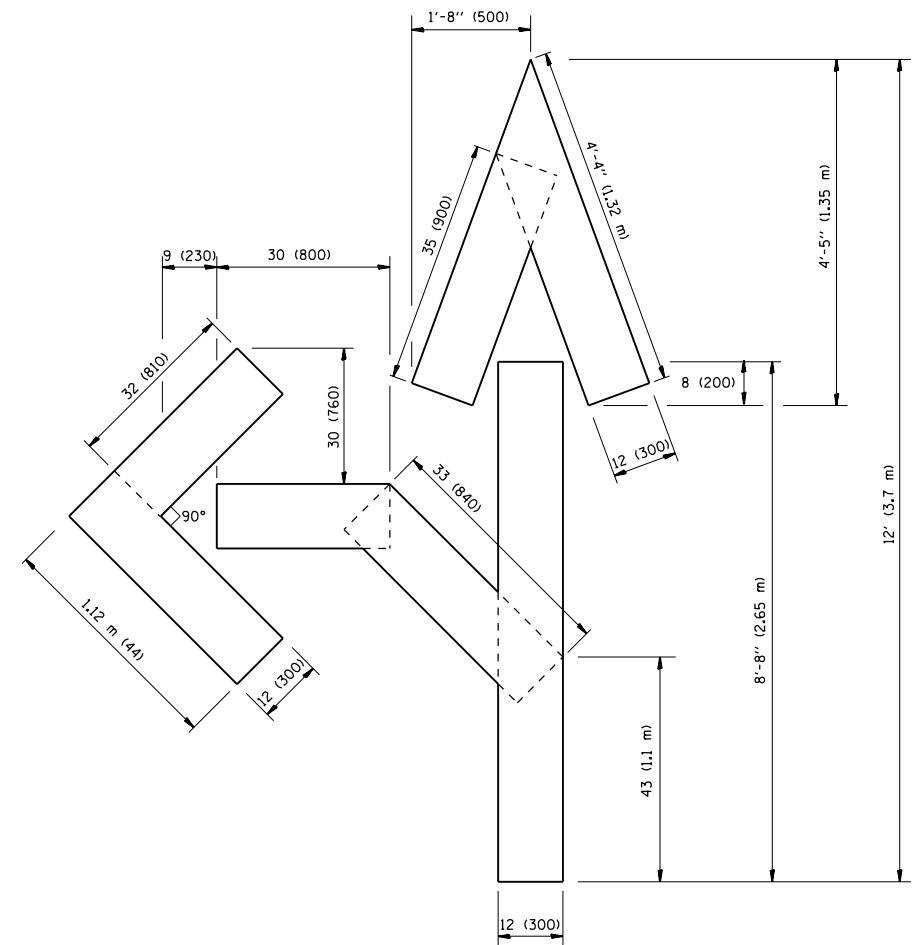
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

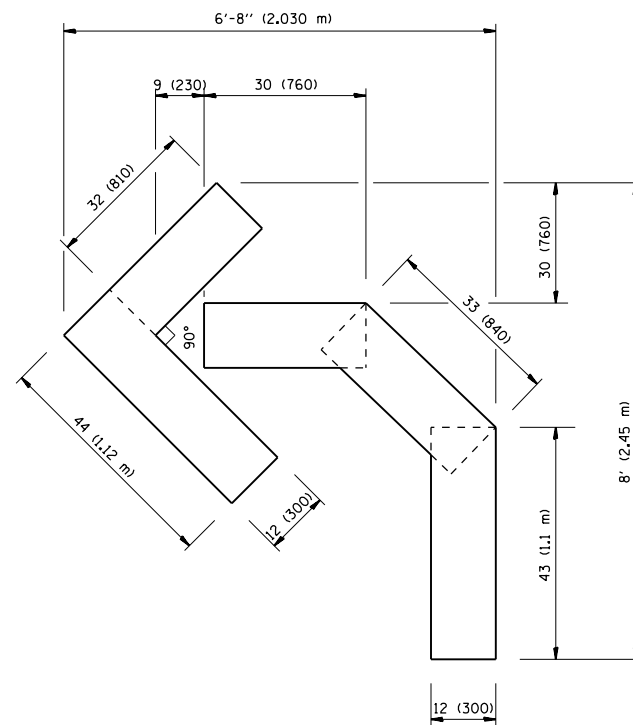
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2845	3160A-RS&N	COOK	27	23
TC-14		CONTRACT NO. 60799		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

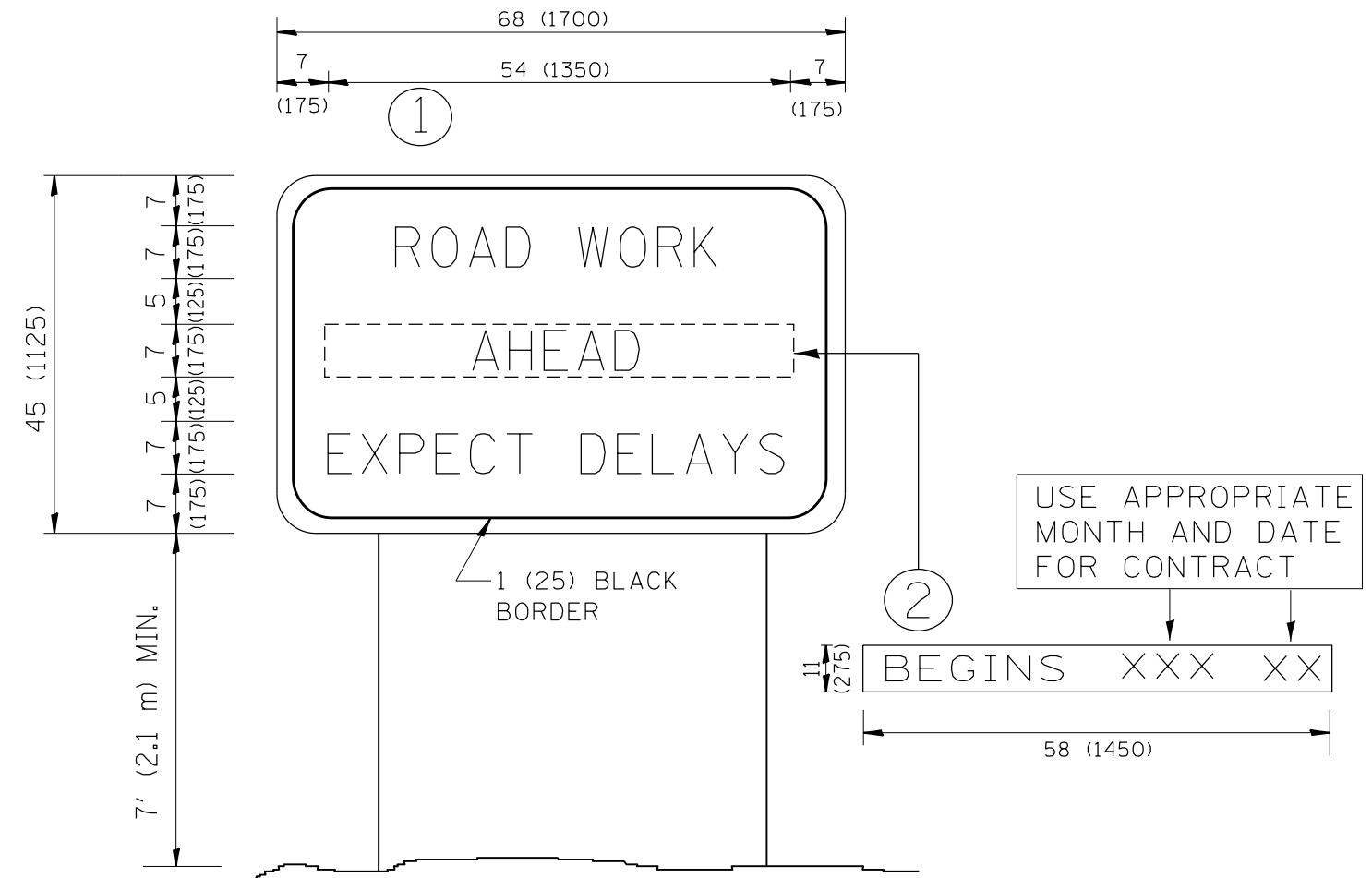
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	PLOT DATE = 3/27/2013	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	23
TC-16			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

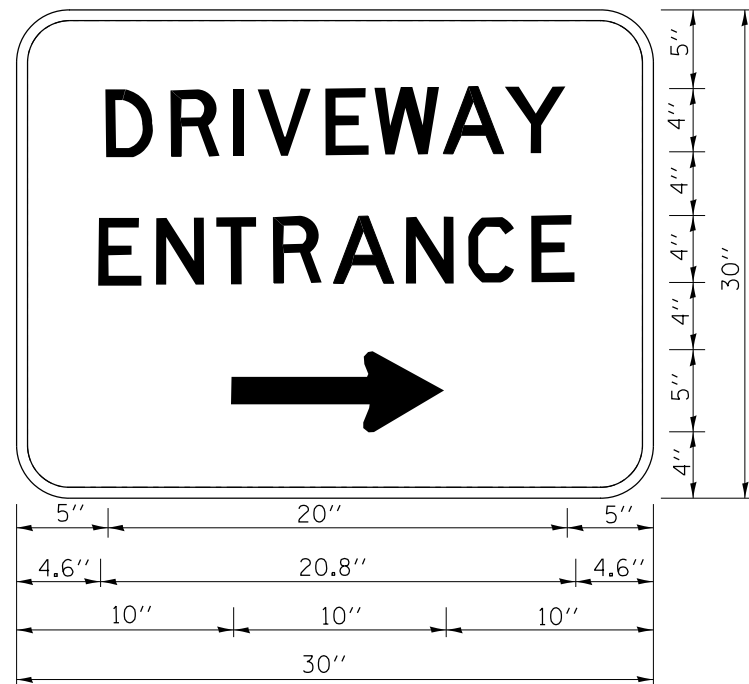
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	PLOT DATE = 3/27/2013	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	24
TC-22			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED - C. JUCIUS 02-15-07
et:\pwork\pwork\midyja\0267284\DistStd.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

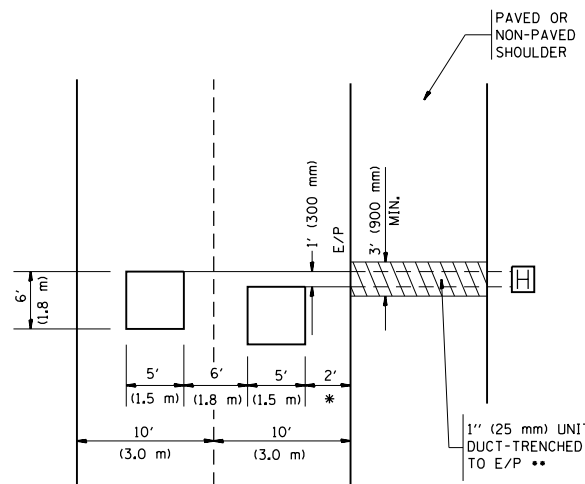
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	25
TC-26			CONTRACT NO. 60T99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

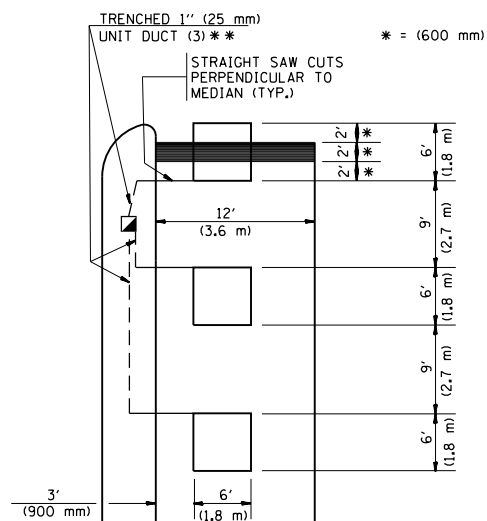


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

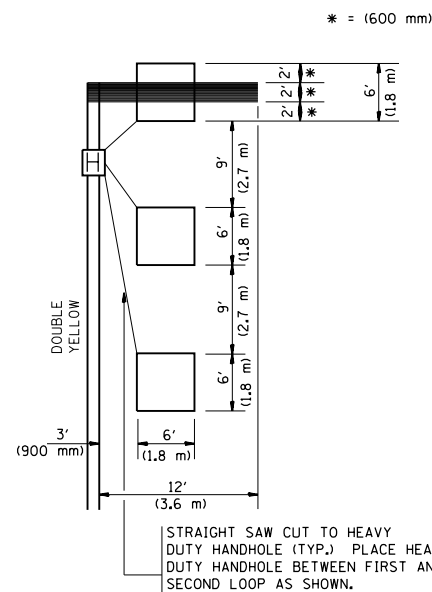
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

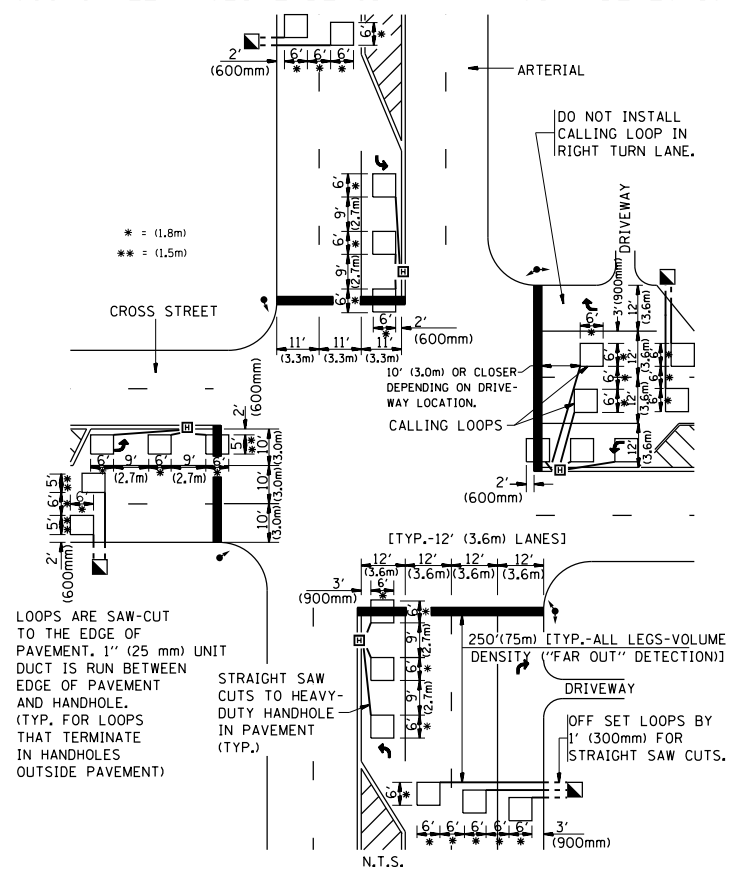
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



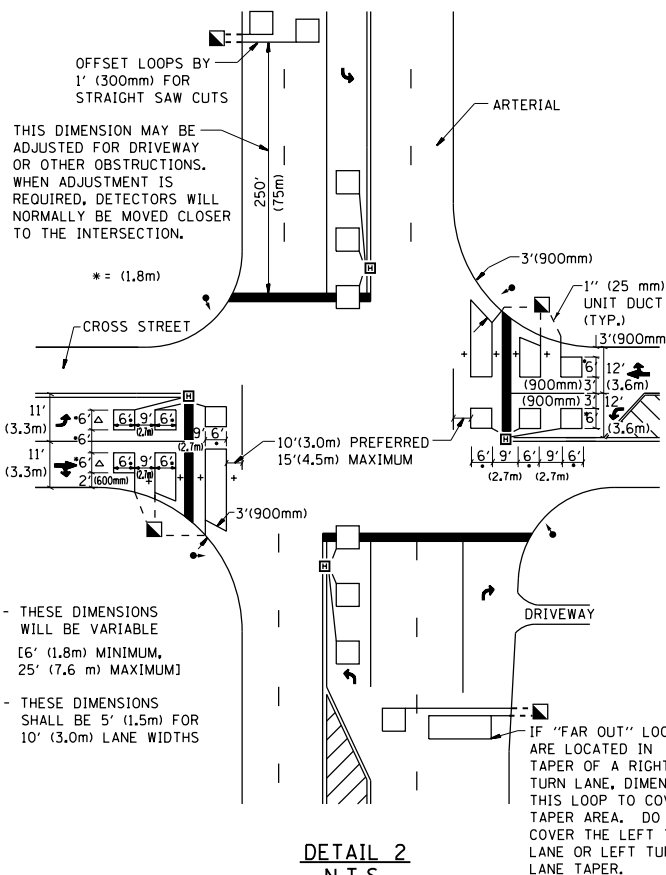
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -
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		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

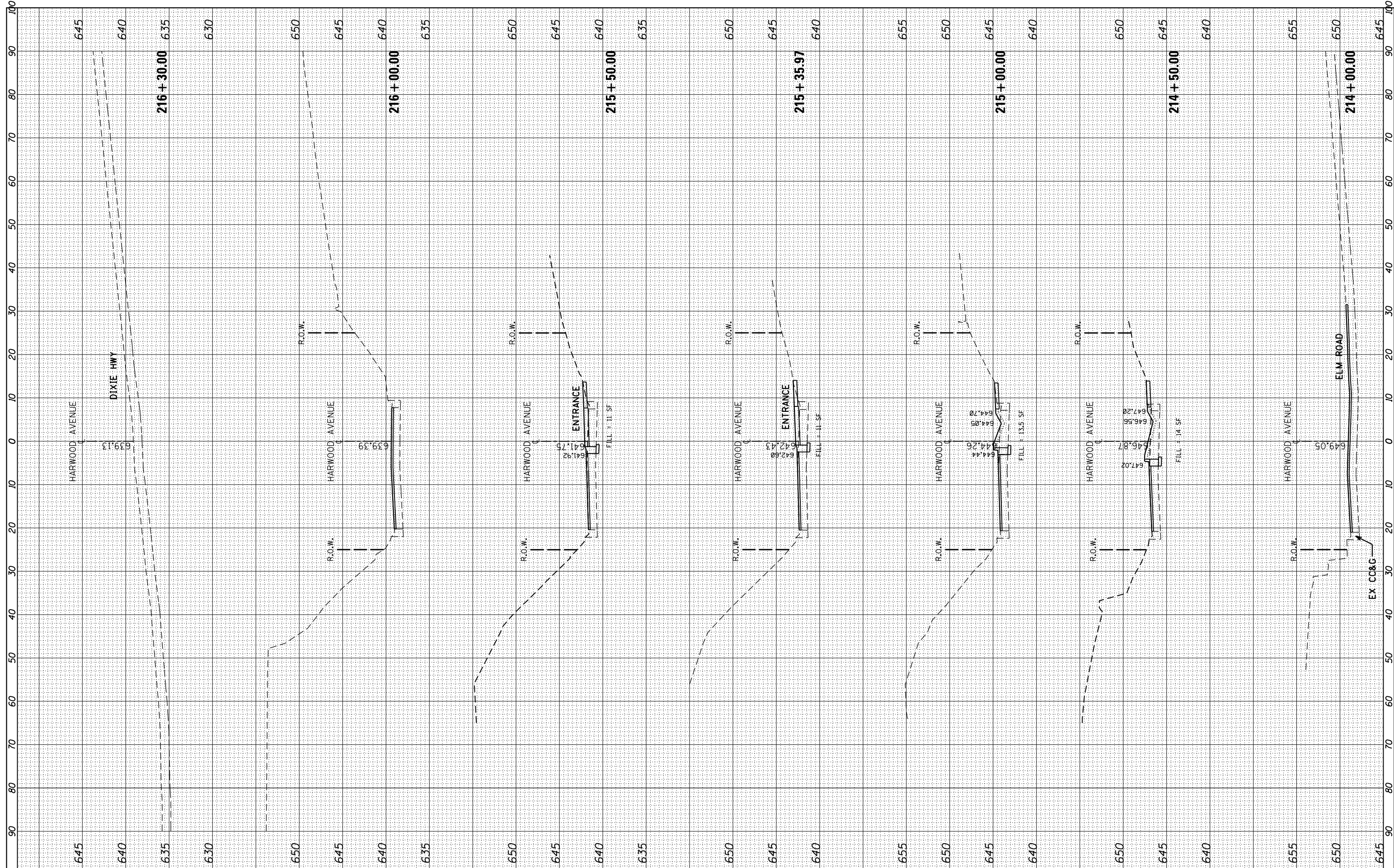
**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	26
TS-07		CONTRACT NO. 60799		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		



FILE NAME = P118111-sh1-xssht-SHarwood.dgn

USER NAME = miduja
 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 3/27/2013

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HARWOOD AVENUE
 CROSS SECTIONS

SCALE: SHEET NO. OF SHEETS STA. 215+00.00 TO STA. 216+30.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2845	3160A-RS&N	COOK	27	27
CONTRACT NO. 60T99				