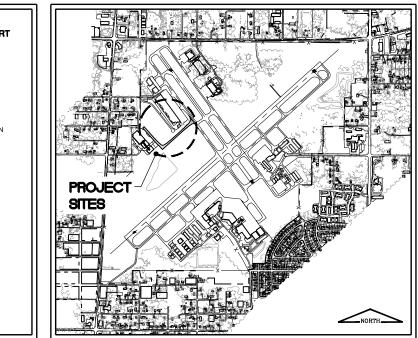
WAUKEGAN PORT DISTRICT WAUKEGAN, LAKE COUNTY, ILLINOIS

CONSTRUCTION PLANS FOR WAUKEGAN NATIONAL AIRPORT

RECONSTRUCT TAXIWAY D

ILLINOIS PROJECT: UGN-5190 S.B.G. PROJECT: 3-17-SBGP-XXX

APRIL 18, 2025



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES, PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE BELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JUBISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811 WAUKEGAN NATIONAL AIRPORT WAUKEGAN PORT DISTRICT VAUKEGAN 2601 Plane Rest Drive NATIONAL Waukegan, Illinois 60087 ATRPOR' Telephone: 847.244.0055 Fax: 847.244.3813 Min APPROVED BY SKIP GOSS, GENERAL MANAGER APRIL 18 DATE 2025

J.U.L.I.E.

now what's below.

Call before you dig

JOINT UTILITY LOCATING

www.illinois1call.com

INFORMATION FOR EXCAVATORS



LOCATION MAP

SITE PLAN

WA082 TOTAL SHEETS = 47



CALL J.U.L.I.E. BEFORE EXCAVATING AT 811 WAUKEGAN NATIONAL AIRPORT

> TOWNSHIP: 46 NORTH RANGE: 12 EAST SECTION: 31 AND 32 COUNTY: LAKE TOWNSHIP: BENTON

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY D DESIGN AIRCRAFT GROUP III

	© Copyright CMT, Inc.	24006512-00
CRAWFORD, MURPHY & T		
CONSULTING ENGINEERS License No. 184-000613	5	
SUBMITTED BY	A	
DATE	CRAIG/LOUDEN, P.E. APRIL 18	2025

	SUMMARY OF QUANTITIES					
ASE BID						
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORI QUANTIT		
AR108108	1/C #8 5 KV UG CABLE	FOOT	450			
AR108158	1/C #8 5 KV UG CABLE IN UD	FOOT	1,460			
AR108706	1/C #6 COUNTERPOISE	FOOT	1,460			
AR110011	1" DIRECTIONAL BORE	FOOT	100			
AR110552	EXTEND 2-WAY DUCT	FOOT	20			
AR110906	REMOVE ELECTRICAL HANDHOLE	EACH	1			
AR125565	SPLICE CAN	EACH	1			
AR125411	MITL-STAKE MOUNTED-LED	EACH	3			
AR125941	ADJUST STAKE MOUNTED LIGHT	EACH	5			
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	1			
AR125961	RELOCATE STAKE MOUNTED LIGHT	EACH	13			
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	6			
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	1			
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1			
AR150520	MOBILIZATION	L SUM	1			
AR152410	UNCLASSIFIED EXCAVATION	CU YD	5,500			
AR152480	SHOULDER ADJUSTMENT	SQ YD	4,100			
AR152540	SOIL STABILIZATION FARBIC	SQ YD	8,750			
AR156510	SILT FENCE	FOOT	1,090			
AR156520	INLET PROTECTION	EACH	13			
AR201910	REMOVE & REPLACE BIT. PAVEMENT	SQ YD	210			
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	2,950			
AR209606	CRUSHED AGG. BASE COURSE - 6"	SQ YD	10,200			
AR401610	BITUMINOUS SURFACE COURSE	TON	390			
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	9,750			
AR501511	11" PCC PAVEMENT	SQ YD	8,110			
AR501530	PCC TEST BATCH	EACH	1			
AR602510	BITUMINOUS PRIME COAT	GALLON	495			
AR603510	BITUMINOUS TACK COAT	GALLON	120			
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	2,570			
AR620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	3,610			
AR620900	PAVEMENT MARKING PBEAGK BONDER	SQ FT	420			
AR701006	6" PVC STORM SEWER	FOOT	92			
AR705506	6" PERFORATED UNDERDRAIN	FOOT	2.605			
AR705635		EACH	2			
AR705900		FOOT	885			
AR705900		EACH	6			
AR705904 AR901510	SEEDING	ACRE	1			
AR901510 AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	1			

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46	CROSS SECTIONS - TAXIWAY D1 - SHEET 6
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	HM	IA MIXTURE REC	QUIREMENT TA	ABLE	
ITEM	RUNWAY/TAXIWAY (60K + LBS.) NDES	PG BINDER	AGGREGATE QUALITY	MAX RAP	DENSIT
401: HMA SURFACE	N50 @ 3.0%	SBS PG 70-28	А	0 (N/A)	I NUC

REHABILITATE TAXIWAY PAVEMENT
APRIL 18, 2025
OWNER
WAUKEGAN NATIONAL AIRPORT
MARK DATE DESCRIPTION
CMT PROJECT NO: 24006512.00
CAD DWG FILE: DESIGNED BY: CMJ
DRAWN BY: JRO
CHECKED BY: CAL APPROVED BY: CAL COPYRIGHT:
SHEET TITLE
SUMMARY OF
QUANTITIES AND
QUANTITIES AND INDEX TO SHEETS



CONSULTANTS

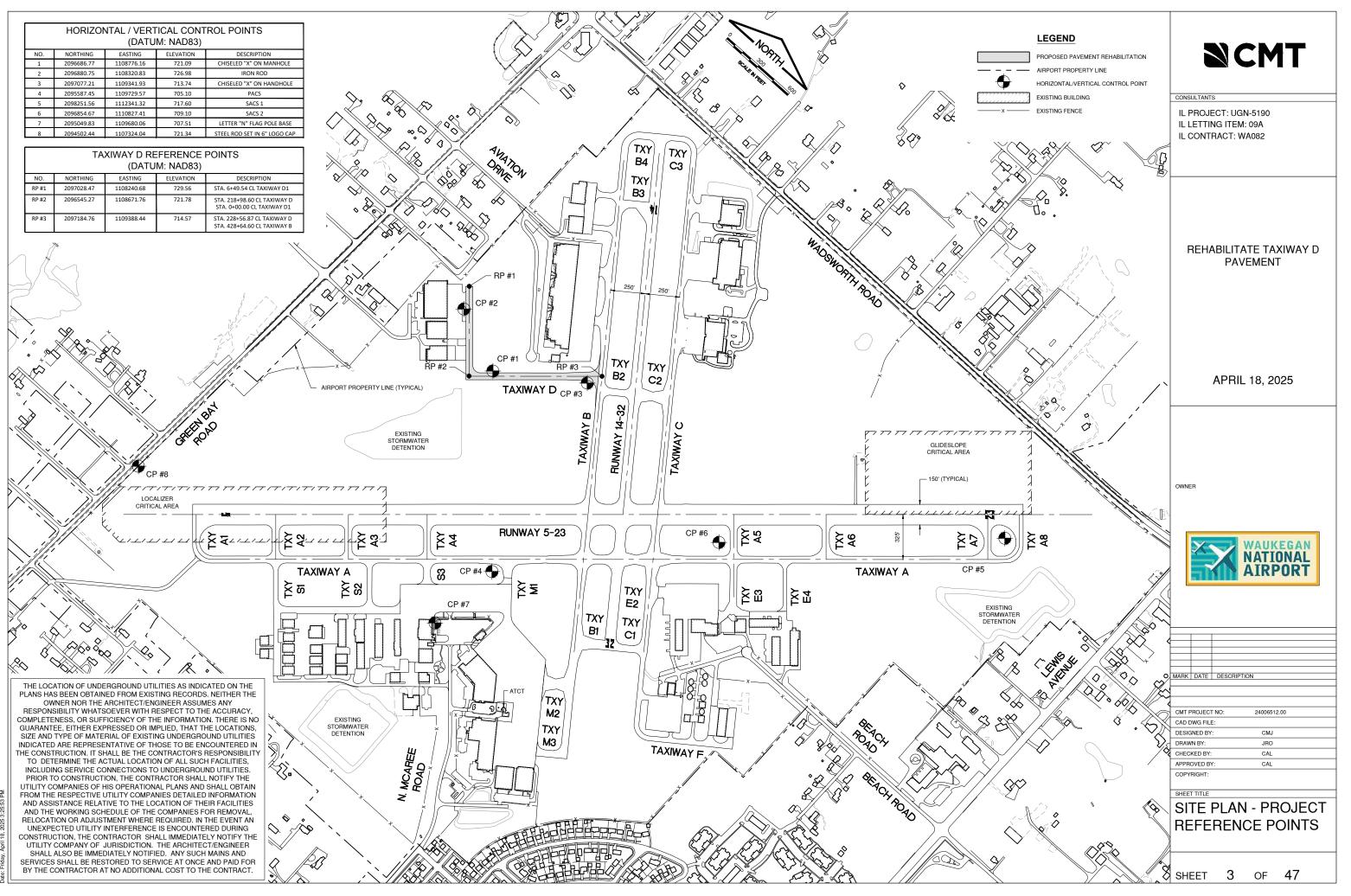
IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

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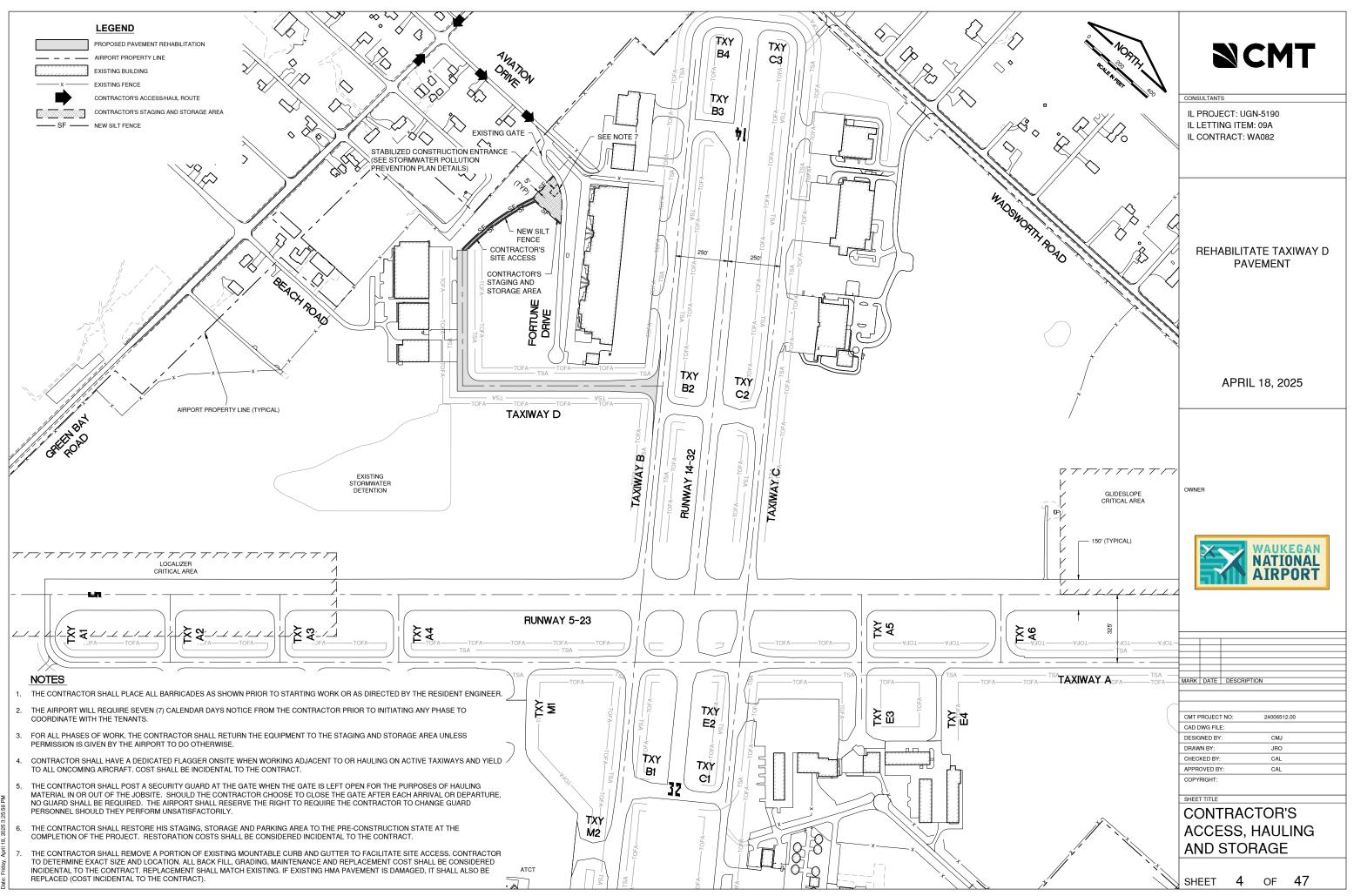
8, 2025



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CMT F	MT PROJECT NO: 24006512.00					
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DESIG	ESIGNED BY: CMJ					
DRAW	RAWN BY: JRO					



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GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS
- PRIOR TO THE NOTICE TO PROCEED. THE CONTRACTOR SHALL SUBMIT TO 2. THE AIRPORT THROUGH THE RESIDENT ENGINEER. FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE 3. RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE 4. CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SLIPPI IERS
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL 1. ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION. CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- 2. ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE. THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- З. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFE AND RESIDENT ENGINEER ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

ЛQN

Path: Date:

TOTAL CONTRACT TIME SHALL BE 99 CALENDAR DAYS

WORK SHALL BE COMPLETED IN TWO SUB-PHASES AS SHOWN ON THESEQUENCE OF CONSTRUCTION PLAN SHEETS.

COMPLETE INITIAL MOBILIZATION, STAGING AREA PREPARATION AND LAYOUT IN ORDER TO PROCEED WITH THE REMOVAL OF THE EXISTING PAVEMENT AND EBOSION CONTROL MEASURES, INSTALL THE NEW PCC PAVEMENT AND COMPLETE EARHTWORK, LANDSCAPING AND GENERAL IMPROVEMENTS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND 2. AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING З. AIRCRAFT AT ALL TIMES

4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID
- 2. EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
- 3. EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE
- 4. PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA
- 5. IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE BOUTES. 2. AND GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND 3. TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY COUNTY TOWNSHIP OB LD O T
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED 4. AND FLAGGED PER SECTION 70-08 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES 5. (AGGREGATE, ASPHALT, FTC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE. THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
- CONTRACTOR WORK CREWS MUST MAINTAIN BADIO CONTACT 6 WITH THE WAUKEGAN AIR TRAFFIC CONTROL TOWER GROUND CONTROL (121.65 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA) THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE BADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
- THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS 7. SHOWN ON THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL 8 EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE 9. STORED AT THE STAGING AREAS
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN 10. ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE
- 11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.B. TITLE 14 PART 77 - OBJECTS AFFECTINGNAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING 12 RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN BADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL 13. CONSTRUCTION AREAS AND HAUL BOUTES WHICH WILL BE OPENED TO VEHICLE AND AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE 14. CONTRACTOR FOR HAUL BOADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE 15. CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION 16. ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT 1. ENGINEER IF ANY WILDLIEF IS SEEN ENTERING THE AIRPORT
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE 2. CONTRACTOR IS NOT WORKING
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM 2. VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN. AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO 2 THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS
- FOR ANY FOUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED
- 5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
- 2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN 1. FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS, NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE LITILITY BY THE CONTRACTOR AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION

12. PENALTIES

NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

- 14. RUNWAY AND TAXIWAY
- ALL BUNWAYS, TAXIWA 1 AIRPORT TRAFFIC DUR CONSTRUCTION ACTIV

2.

IF ANY RUNWAY OR TA CONTRACTOR AND APP USE MARKING, LIGHTIN REQUIREMENTS OF FA

15. MARKING AND SIGNS F

- BABBICADES AND SIGN ACCESS ROUTE AS DET SHEETS.
- 16. HAZARD MARKING AND THE CONTRACTOR SH ASSOCIATED LIGHTING STOCKPILES, AND HIS/

ALL CONSTRUCTION E ACCORDANCE WITH FA **OB LATEST EDITION AT** PROPERTY. THE MAXIN

3. BARRICADES SHALL BE CONSTRUCTION ACTIV

THE CONTRACTOR SHA WORK DAY TO INSURE THE RED LIGHTS AND F

17. WORK ZONE LIGHTING

1

2.

- WORK PERFORMED BY SHALL BE DONE UNDER FOR PROPER CONSTRU
- LIGHTS SHALL CONSIST FLOODLIGHTS AND/OR ILLUMINATE THE WORK ONLY IN ADDITION TO (NOT INTERFERE WITH UNDER INSUFFICIENT A JUDGEMENT, SHALL BE LIGHTING IS PROVIDED NOT BE ACCEPTABLE MADE

18. PROTECTION

- 1. ALL WORK REQUIRED IN THE RUNWAY TO BE CLO
 - ALL WORK REOLIBED OF 2. TAXIWAY OBJECT FREE

19. OTHER LIMITATIONS OF IF. DURING CONSTRUC 1 AIRPORT, THE CONTRA OF ALL VEHICLES, PER

- AIRPORT BROKEN CONCRETE, BI 2 MISCELLANEOUS DEBF UNLESS OTHERWISE S
- THE CONTRACTOR WIL AIRSPACE FOR THE CO THAT SPECIFIED ON TH UP TO 12 WEEKS TO CO
- THE CONTRACTOR SH EXISTING CIRCUITS PR CONSTRUCTION AS SP

20. AIRFIELD EDGE LIGHTS

- CONTRACTOR SHALL C ON CLOSED TAXIWAYS USE THE METHOD AND LIGHTS SHALL MEET TH INCIDENTAL TO THE COM FIXTURES AS A MEANS SERVICE SHALL NOT BE
- 2. CONTRACTOR SHALL TU REGULATOR AND LOCK-CLOSURES. CONTRACTO WITH THE AIRPORT MAN **BE-OPENING THE BUNW** COORDINATE WITH AIRE **RE-ENERGIZE THE RUN**

AY VISUAL AIDS	
IAYS, AND APRONS SHALL BE KEPT OPEN TO RING CONSTRUCTION EXCEPT AS NOTED IN THE VITY PLAN.	
AXIWAY CLOSURES ARE REQUESTED BY THE PPROVED BY THE AIRPORT, THE CONTRACTOR SHALL NG AND SIGNS THAT FOLLOWING THE AA AC 150/5370-2G OR LATEST EDITION.	CONSULTANTS
FOR ACCESS ROUTES INS SHALL BE USED ALONG THE CONTRACTOR'S ETAILED ON THE CONSTRUCTION ACTIVITY PLAN	IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082
D LIGHTING HALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND G OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY	
A CONSTRUCTION EQUIPMENT. EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN AA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C T ALL TIMES WHILE OPERATING ON AIRPORT INUM EQUIPMENT HEIGHT IS 25'.	
E PLACED AT THE LOCATIONS SHOWN ON THE VITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.	REHABILITATE TAXIWAY D PAVEMENT
IALL INSPECT THE BARRICADES ONCE DURING EACH E PROPER PLACEMENT AND PROPER OPERATION OF FLAG PLACEMENT.	
G FOR NIGHTTIME CONSTRUCTION Y THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS ER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW	
RUCTION METHODS AND INSPECTION. ST OF VEHICLE OR MOVABLE POLE MOUNTED R SPOTLIGHTS OF SUFFICIENT NUMBER TO IK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL I AIR OPERATIONS. ANY WORK BEING PERFORMED ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S SE STOPPED UNTIL SUCH TIME AS ADDITIONAL	APRIL 18, 2025
UNTIL PROPER INSPECTION AND TESTING CAN BE NSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE OSED. ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE E AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED. ON CONSTRUCTION	OWNER
CTION, AN EMERGENCY IS DECLARED BY THE IACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT RSONNEL AND EQUIPMENT IF DIRECTED BY THE BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER IRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY,	WAUKEGAN NATIONAL AIRPORT
SPECIFIED. ILL BE RESPONSIBLE FOR COORDINATING THE ONSTRUCTION EQUIPMENT THAT IS TALLER THAN 'HE PLANS WITH THE FAA. THIS PROCESS MAY TAKE 'OMPLETE.	
HALL BE RESPONSIBLE FOR MEGGAR TESTING ALL RIOR TO CONSTRUCTION AND FOLLOWING PECIFIED IN THE CONTRACT DOCUMENTS.	
S AND SIGNS	MARK DATE DESCRIPTION
COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT MATERIALS USED TO COVER THE SIGNS AND E ENGINEER'S AND AIRPORT'S APPROVAL. COST ONTRACT. REMOVING LAMPS FROM ENERGIZED TO REMOVE THE LIGHTS OR FIXTURES FROM E ACCEPTABLE. URN OFF RUNWAY/TAXIWAY EDGE LIGHTING (-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT LI VAULT. DURING ALL RUNWAY/TAXIWAY FOR SHALL COORDINATE ACCESS TO THE VAULT NAGER/RESIDENT ENGINEER PRIOR TO WAY/TAXIWAY, THE CONTRACTOR SHALL IPORT MANAGER/RESIDENT ENGINEER TO IWAY/TAXIWAY CIRCUIT.	CMT PROJECT NO: 24006512.00 CAD DWG FILE: DESIGNED BY: CMJ DRAWN BY: JRO CHECKED BY: CAL APPROVED BY: CAL COPYRIGHT: SHEET TITLE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL
	PLAN GENERAL NOTES - 1
	1

SHEET 5 OF 47

PHASING NOTES (ALL PHASES)

- 1. THE INTENT OF THE CONSTRUCTION ACTIVITY PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT MANAGER
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL 2. CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT EXCEPT WHERE SPECIFICALLY SHOWN IN THE PLANS
- 3. CONTRACTOR MAY REQUEST TO COMBINE WORK PHASES/AREAS. THE AIRPORT WILL DETERMINE IF THE REQUEST IS ACCEPTABLE
- PRIOR TO REOPENING A CLOSED RUNWAY THE ENTIRE RUNWAY SAFETY AREA (RSA), AND THE TAXIWAY OBJECT 4 FREE AREA (TOFA) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE IS NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF IS THREE (3) INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN THREE (3) PERCENT. STEEL PLATES MAY BE REQUIRED TO MEET CRITERIA. IF NECESSARY, TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT
- THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SAFETY SCHEDULE, STRICT ADHERENCE TO THE APPROVED 5. SCHEDULE WILL BE ENFORCED TO AVOID CONFLICT WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON THE AIRPORT OPERATIONS.
- 6. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT MANAGER AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S
- 7. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS. THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE ATCT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
- 8. TEMPORARY CLOSED TAXIWAY MARKERS ARE ONLY REQUIRED WHEN THE TAXIWAY WILL REMAIN CLOSED FOR 3 CONSECUTIVE DAYS OR MORE
- IF A RUNWAY OR TAXIWAY IS TO REOPEN PRIOR TO FINAL TOPSOILING AND GRADING, THE MAXIMUM SIGN BASE 9. EDGE DROP OFF SHALL BE 3-INCHES AND MAXIMUM SLOPES SHALL BE 3%. THE SURROUNDING AREA ADJACENT TO THE SIGN BASE SHALL BE SMOOTHLY GRADED.
- 10. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES, CONTRACTOR SHALL RELOCATE EQUIPMENT TO PROVIDE MINIMAL DISRUPTIONS TO THE ACTIVE AIRCRAFT MOVEMENT AREA. FAA AND AIRPORT ROAD(S) SHALL NOT BE USED AS A HAUL ROUTE BY THE CONTRACTOR WITHOUT PRIOR APPROVAL
- 11. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF THE AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN
- 12. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF THE CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENT AT ANY TIME DURING THE PROJECT WITH FAA AND IDA APPROVAL.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIA PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE TAXIWAY SITE DUE TO ADJACENT BUILDING CONSTRUCTION

 CONSTRUCT PERIMETER FENCING PHASE 5 (NORTHWEST ARFA)

GROUND CONTROL FREQUENCY: 121.65 MHz

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

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CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

OWNER



MARK	DATE	DESCRIPTION
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SHEET 6 OF 47

NOTES

- THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK OR AS DIRECTED BY THE RESIDENT ENGINEER
- 2. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
- FOR ALL PHASES OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND 3. STORAGE AREA UNLESS PERMISSION IS GIVEN BY THE AIRPORT TO DO OTHERWISE.
- 4. THE CONTRACTOR SHALL POST A SECURITY GUARD AT THE GATE WHEN THE GATE IS LEFT OPEN FOR THE PURPOSES OF HAULING MATERIAL IN OR OUT OF THE JOBSITE. SHOULD THE CONTRACTOR CHOOSE TO CLOSE THE GATE AFTER EACH ARRIVAL OR DEPARTURE, NO GUARD SHALL BE REQUIRED. THE AIRPORT SHALL RESERVE THE RIGHT TO REQUIRE THE CONTRACTOR TO CHANGE GUARD PERSONNEL SHOULD THEY PERFORM UNSATISFACTORILY.
- 5. THE CONTRACTOR SHALL RESTORE HIS STAGING, STORAGE AND PARKING AREA TO THE PRE-CONSTRUCTION STATE AT THE COMPLETION OF THE PROJECT. RESTORATION COSTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 6. PHASES 1A AND 1B CAN BE PERFORMED CONCURRENTLY.



1A	ALL RUNWAYS OPEN	TAXIWAY D			
1B	ALL RUNWAYS OPEN	TAXIWAY D TAXIWAY B BETWEEN RUNWAY 5/23 AND WCAF TAXIWAY B2			
NOTE: PHASE 1B SHALL BE LIMITED TO 10 CALENDAR DAYS. THE TOTAL TAXIWAY B CLOSURE (PHASE 1B AND 2A) SHALL BE LIMITED TO 30 DAYS.					

LEGEND

- AIRPORT PROPERTY LINE FAA CRITICAL POINT
- (A)EXISTING BUILDING
- EXISTING FENCE
- PHASE 1A WORK AREA
- PHASE 1B WORK AREA
- PHASE 1A LOW PROFILE BARRICADES
- PHASE 1B LOW PROFILE BARRICADES -

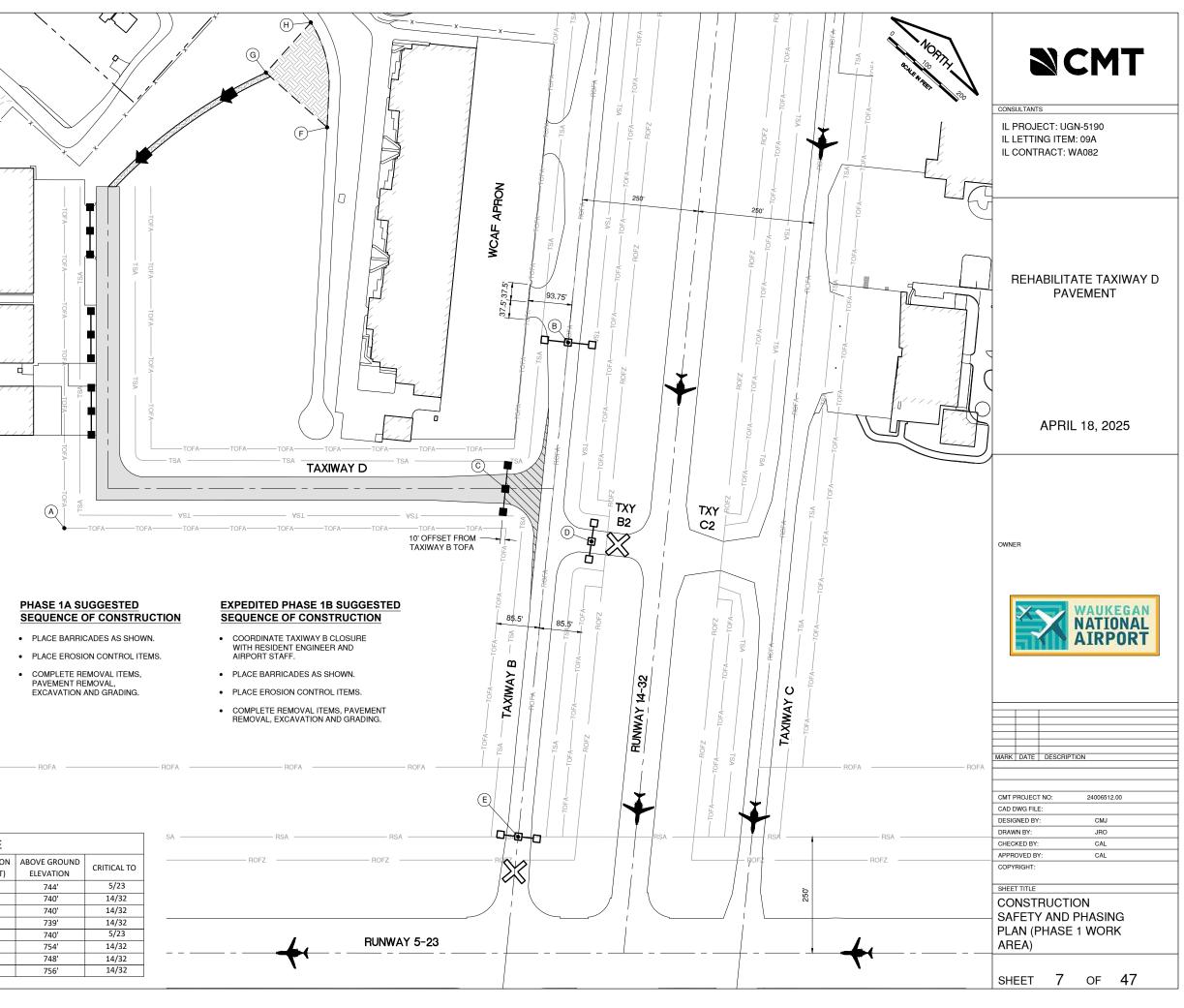
AIRCRAFT MOVEMENT AREA

-TSA

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- TAXIWAY OBJECT FREE AREA TAXIWAY SAFETY AREA
 - CONTRACTOR'S ACCESS ROUTE

 - TEMPORARY CLOSED TAXIWAY MARKER



Path: Date:

NOTES

- 1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK OR AS DIRECTED BY THE RESIDENT ENGINEER.
- 2. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
- 3. FOR ALL PHASES OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA UNLESS PERMISSION IS GIVEN BY THE AIRPORT TO DO OTHERWISE.
- 4. THE CONTRACTOR SHALL POST A SECURITY GUARD AT THE GATE WHEN THE GATE IS LEFT OPEN FOR THE PURPOSES OF HAULING MATERIAL IN OR OUT OF THE JOBSITE. SHOULD THE CONTRACTOR CHOOSE TO CLOSE THE GATE AFTER EACH ARRIVAL OR DEPARTURE, NO GUARD SHALL BE REQUIRED. THE AIRPORT SHALL RESERVE THE RIGHT TO REQUIRE THE CONTRACTOR TO CHANGE GUARD PERSONNEL SHOULD THEY PERFORM UNSATISFACTORILY.
- THE CONTRACTOR SHALL RESTORE HIS STAGING, 5. STORAGE AND PARKING AREA TO THE PRE-CONSTRUCTION STATE AT THE COMPLETION OF THE PROJECT. RESTORATION COSTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 6. PHASE 2A AND 2B CAN BE PERFORMED CONCURRENTLY.

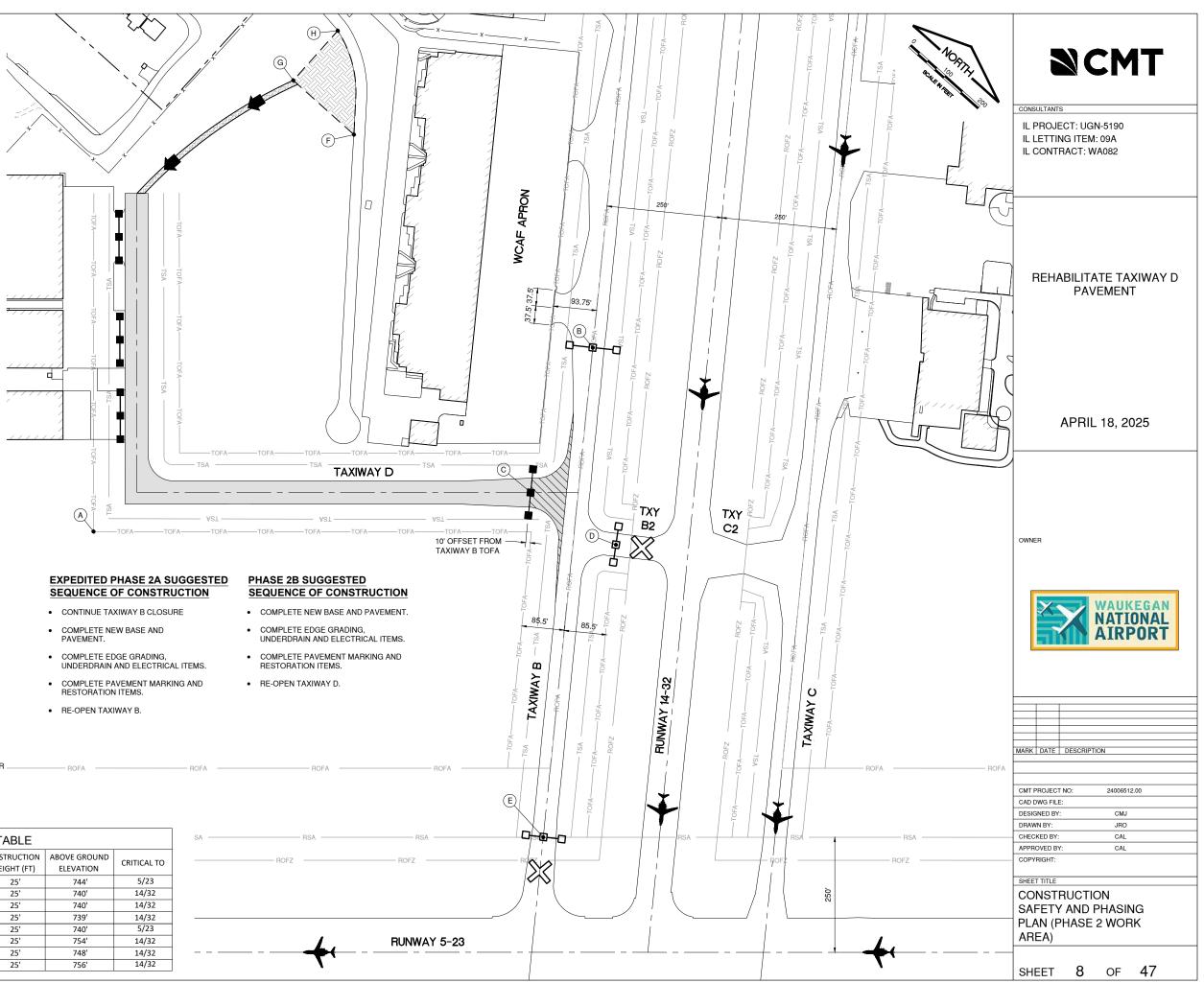
PHASE	FACILITY STATUS	- OPEN	FACILITY STATUS - CLOSED
2A	ALL RUNWAYS	OPEN	TAXIWAY D TAXIWAY B BETWEEN RUNWAY 5/23 AND WCAF TAXIWAY B2
2B	ALL RUNWAYS (DPEN	TAXIWAY D
	TOTAL TAXIWAY B C	LOSURE	ED TO 20 CALENDAR DAYS. : (PHASE 1B AND 2A) SHALL 30 DAYS. END
		AIRPOR	RT PROPERTY LINE
	A	FAA CF	RITICAL POINT
	· · · · · · · · · · · · · · · · · · ·	EXISTI	NG BUILDING
	x	EXISTI	NG FENCE

- EXISTING FENCE
- PHASE 1A WORK AREA
- ////// PHASE 1B WORK AREA
 - PHASE 1A LOW PROFILE BARRICADES
- PHASE 1B LOW PROFILE BARRICADES
 - AIRCRAFT MOVEMENT AREA

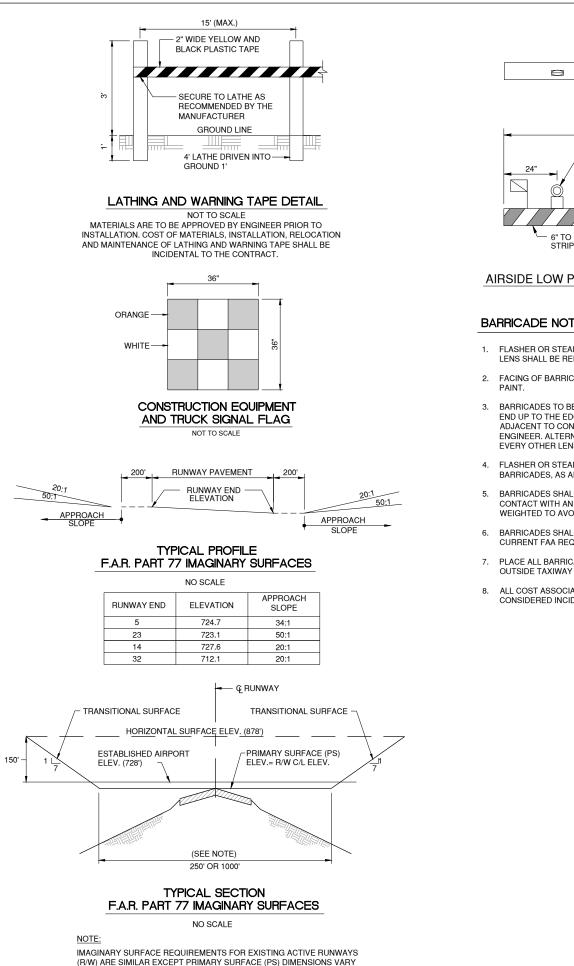
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- -----TOFA--TAXIWAY OBJECT FREE AREA
- TAXIWAY SAFETY AREA _____TSA -
 - CONTRACTOR'S ACCESS ROUTE
 - TEMPORARY CLOSED TAXIWAY MARKER

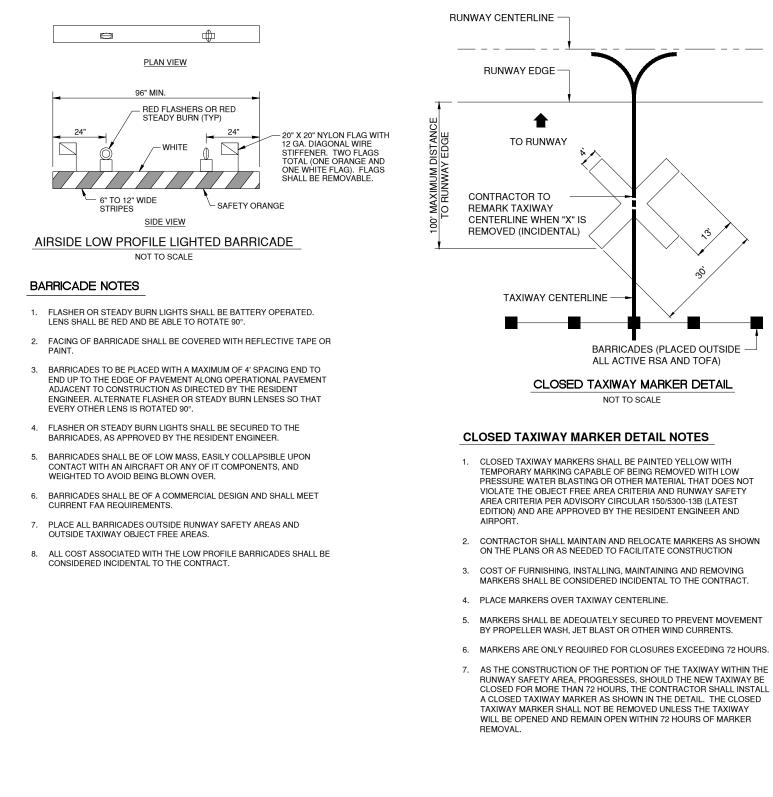
FAA CRITICAL POINT TABLE						
LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT (FT)	ABOVE GROUND ELEVATION	CRITICAL TO	
N 042°25'16.3"	W087°52'21.3"	719'	25'	744'	5/23	
N 042°25'26.8"	W087°52'14.4"	715'	25'	740'	14/32	
N 042°25'23.3"	W087°52'12.3"	715'	25'	740'	14/32	
N 042°25'23.5"	W087°52'09.7"	714'	25'	739'	14/32	
N 042°25'17.8"	W087°52'05.6"	715'	25'	740'	5/23	
N 042°25'26.4"	W087°52'25.6"	729'	25'	754'	14/32	
N 042°25'26.4"	W087°52'23.3"	723'	25'	748'	14/32	
N 042°25'27.8"	W087°52'25.6"	731'	25'	756'	14/32	
	N 042°25'16.3" N 042°25'26.8" N 042°25'23.3" N 042°25'23.5" N 042°25'17.8" N 042°25'26.4" N 042°25'26.4"	LATITUDE LONGITUDE N 042°25'16.3" W087°52'21.3" N 042°25'26.8" W087°52'14.4" N 042°25'23.3" W087°52'12.3" N 042°25'23.5" W087°52'09.7" N 042°25'17.8" W087°52'05.6" N 042°25'26.4" W087°52'25.6" N 042°25'26.4" W087°52'23.3"	LATITUDE LONGITUDE GROUND ELEVATION N 042°25'16.3" W087°52'21.3" 719' N 042°25'26.8" W087°52'14.4" 715' N 042°25'23.3" W087°52'12.3" 715' N 042°25'23.5" W087°52'12.3" 715' N 042°25'23.5" W087°52'09.7" 714' N 042°25'17.8" W087°52'05.6" 715' N 042°25'26.4" W087°52'23.3" 723'	LATITUDE LONGITUDE GROUND ELEVATION OBSTRUCTION HEIGHT (FT) N 042°25'16.3" W087°52'21.3" 719' 25' N 042°25'26.8" W087°52'14.4" 715' 25' N 042°25'23.3" W087°52'12.3" 715' 25' N 042°25'23.3" W087°52'12.3" 715' 25' N 042°25'23.5" W087°52'09.7" 714' 25' N 042°25'17.8" W087°52'05.6" 715' 25' N 042°25'26.4" W087°52'23.3" 723' 25'	LATITUDE LONGITUDE GROUND ELEVATION OBSTRUCTION HEIGHT (FT) ABOVE GROUND ELEVATION N 042°25'16.3" W087°52'21.3" 719' 25' 744' N 042°25'26.8" W087°52'14.4" 715' 25' 740' N 042°25'23.3" W087°52'12.3" 715' 25' 740' N 042°25'23.5" W087°52'09.7" 714' 25' 739' N 042°25'17.8" W087°52'05.6" 715' 25' 740' N 042°25'26.4" W087°52'25.6" 729' 25' 740' N 042°25'26.4" W087°52'23.3" 723' 25' 744'	



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RUNWAY 14-32 250' PS (125' LT & RT OF CENTERLINE) RUNWAY 5-23 1000' PS (500' LT & RT OF CENTERLINE)







CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

OWNER



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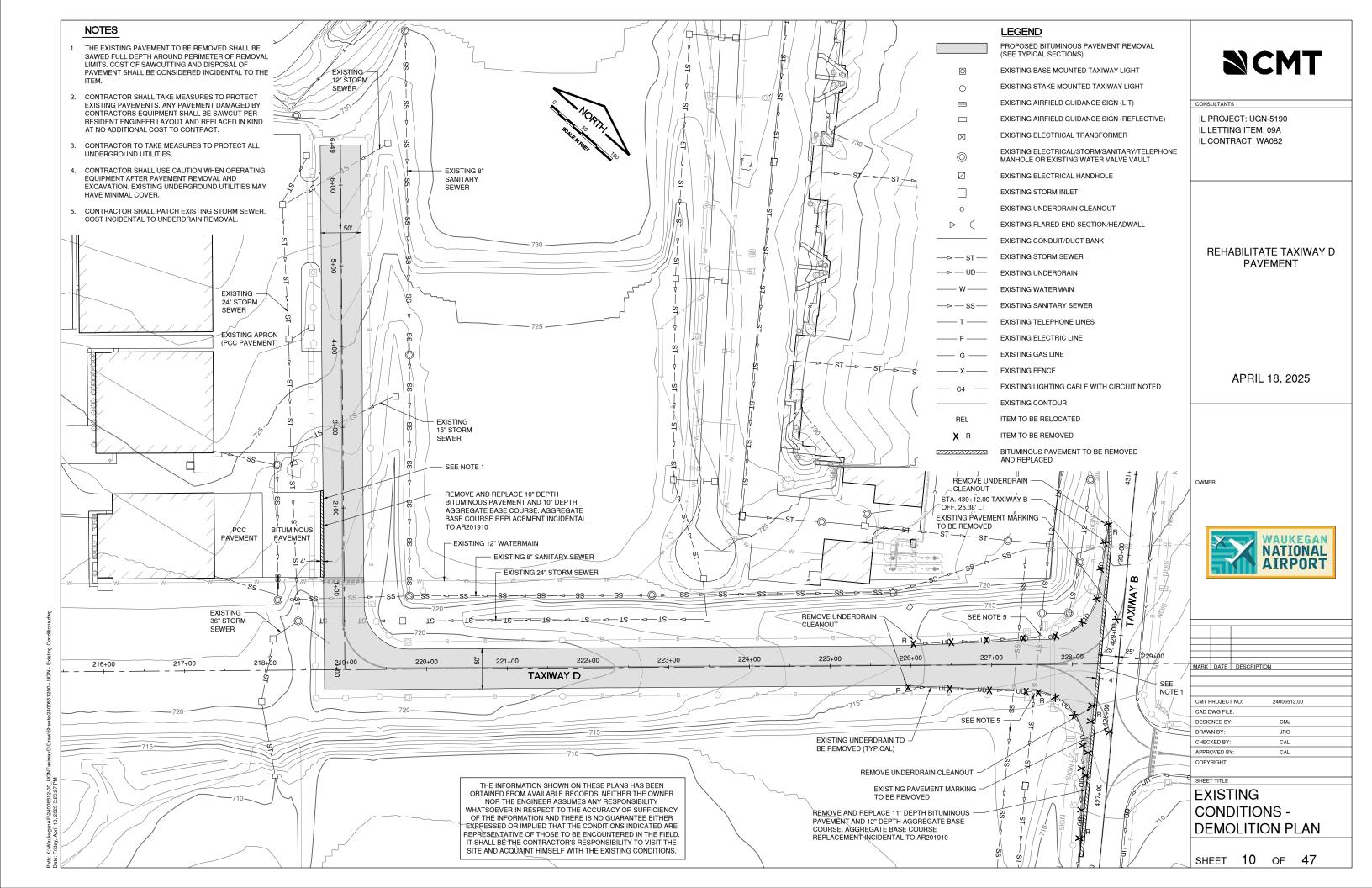
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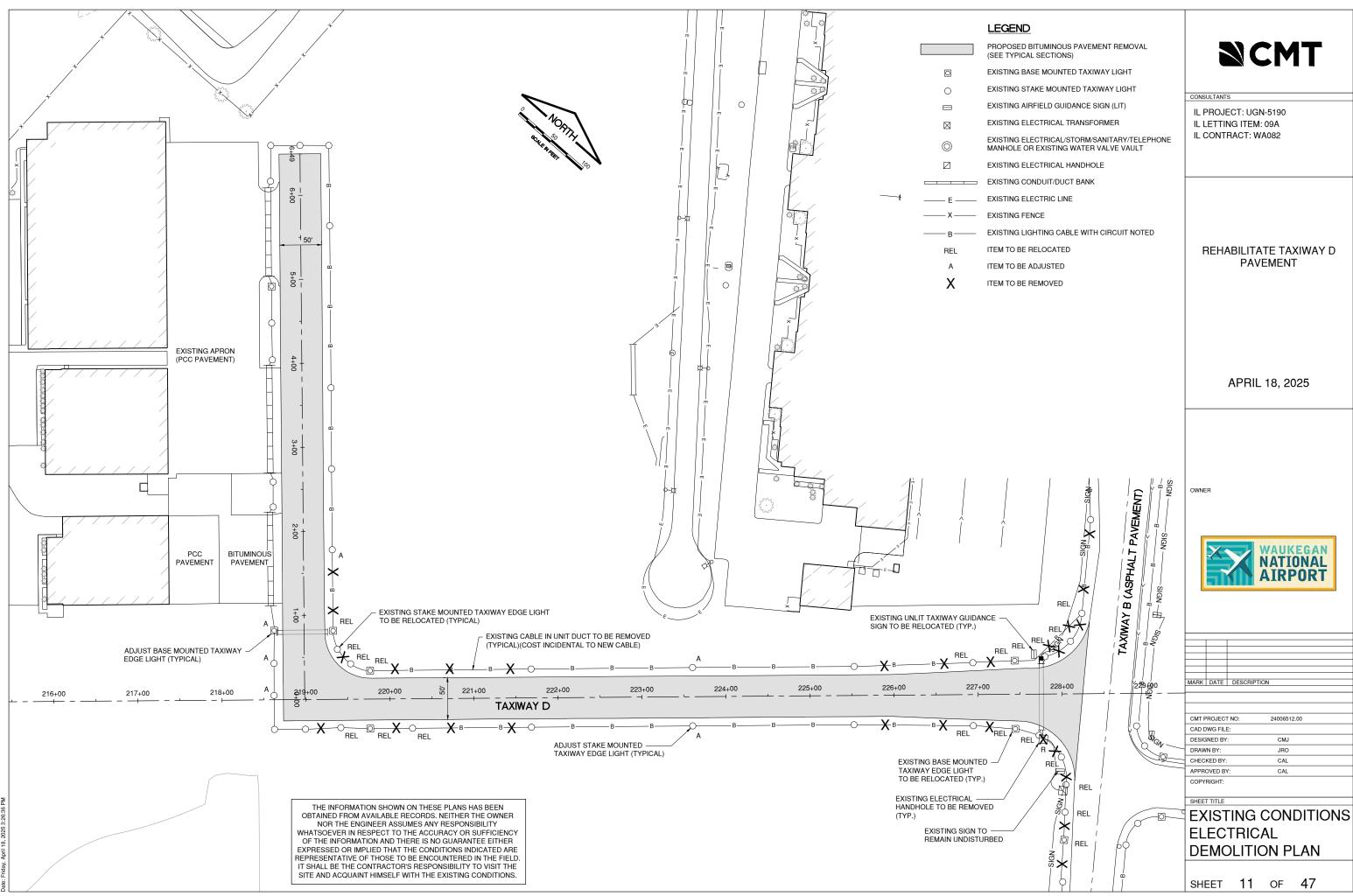
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CONSTRUCTION SAFETY AND PHASING PLAN DETAILS

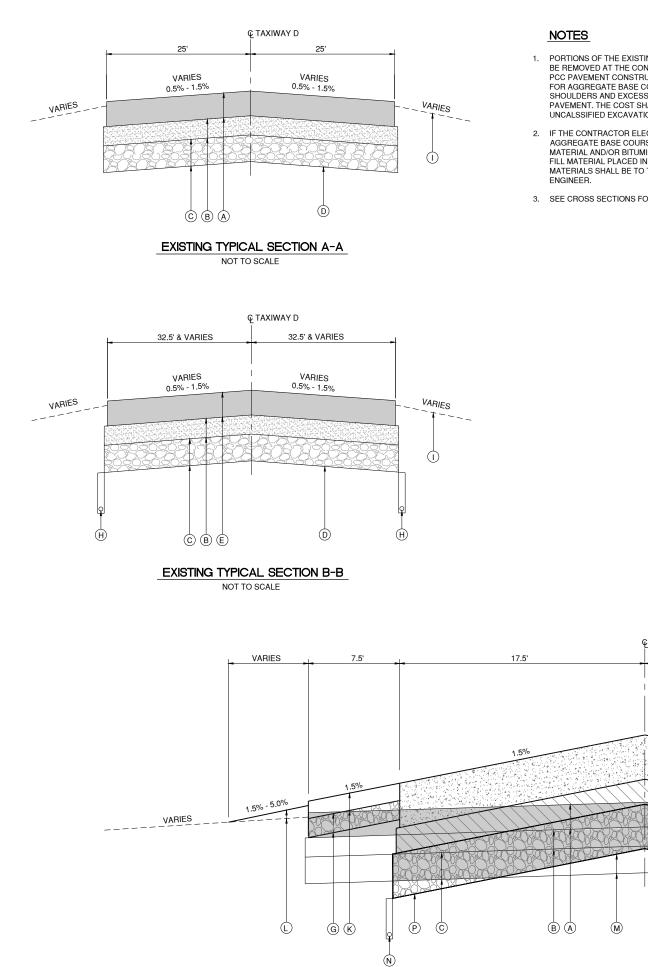
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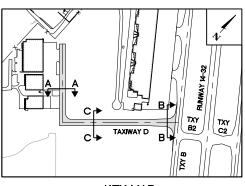




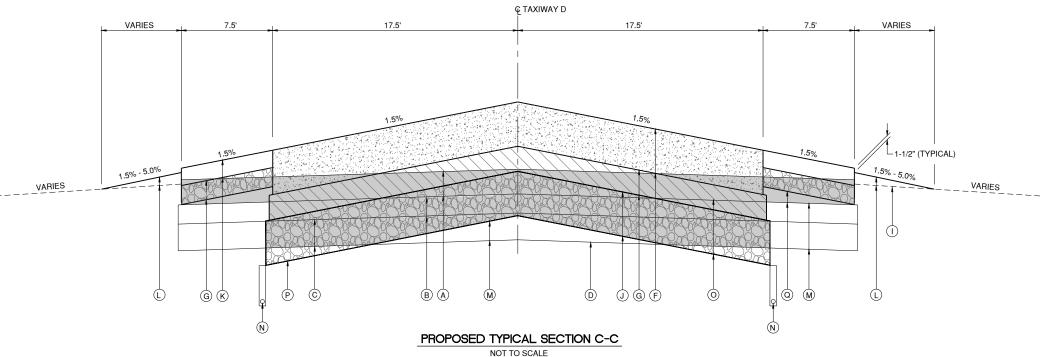
- PORTIONS OF THE EXISTING AGGREGATE BASE COURSE MAY BE REMOVED AT THE CONTRACTOR'S OPTION TO FACILITATE PCC PAVEMENT CONSTRUCTION. NO PAYMENT WILL BE MADE FOR AGGREGATE BASE COURSE REMOVAL BENEATH NEW SHOULDERS AND EXCESS REMOVAL BENEATH NEW PCC PAVEMENT. THE COST SHALL BE CONSIDERED INCIDENTAL TO UNCALSSIFIED EXCAVATION.
- 2. IF THE CONTRACTOR ELECTS TO REMOVE EXISTING AGGREGATE BASE COURSE, THE EXCAVATED AGGREGATE MATERIAL AND/OR BITUMINOUS MILLINGS SHALL BE USED AS FILL MATERIAL PLACED IN LIFTS. ALL COMPACTION OF THESE MATERIALS SHALL BE TO THE SATISFACTION OF THE RESIDENT
- 3. SEE CROSS SECTIONS FOR UNCLASSIFIED EXCAVATION LIMITS.

LEGEND

- (A) EXISTING 6.5" BITUMINOUS PAVEMENT (TO BE REMOVED)
- В EXISTING 4" CRUSHED AGGREGATE BASE COURSE (TO BE REMOVED)
- \odot EXISTING 8" AGGREGATE BASE COURSE (TO BE REMOVED)
- D EXISTING SUBGRADE
- E
- F NEW 11" PCC PAVEMENT (P-501)
- G NEW 6" CRUSHED AGGREGATE BASE COURSE (P-209)
- H EXISTING UNDERDRAIN TO BE REMOVED
- EXISTING GROUNDLINE
- J NEW 12" POROUS GRANULAR EMBANKMENT (P-208)
- K NEW 4" HMA SHOULDER (2 @ 2" LIFTS HMA SURFACE COURSE)(P-401) NEW TACK COAT (P-603) BETWEEN LIFTS AND PRIME COAT, BENEATH HMA (P-602)
- NEW TURF SHOULDER ADJUSTMENT SEEDING (T-901) AND MULCHING (T-908)
- M EXISTING AGGREGATE BASE COURSE (SEE NOTES 1 AND 2)
- (\mathbb{N}) NEW UNDERDRAIN (P-705)
- 0 NEW UNCLASSIFIED EXCAVATION (P-152)(SEE NOTE 3) P NEW SOIL STABILIZATION FABRIC (P-152)
- Q







EXISTING 6.5" - 13" VARIABLE DEPTH BITUMINOUS PAVEMENT (TO BE REMOVED)

NEW BITUMINOUS MILLINGS (INCIDENTAL TO CRUSHED AGGREGATE BASE COURSE)



CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

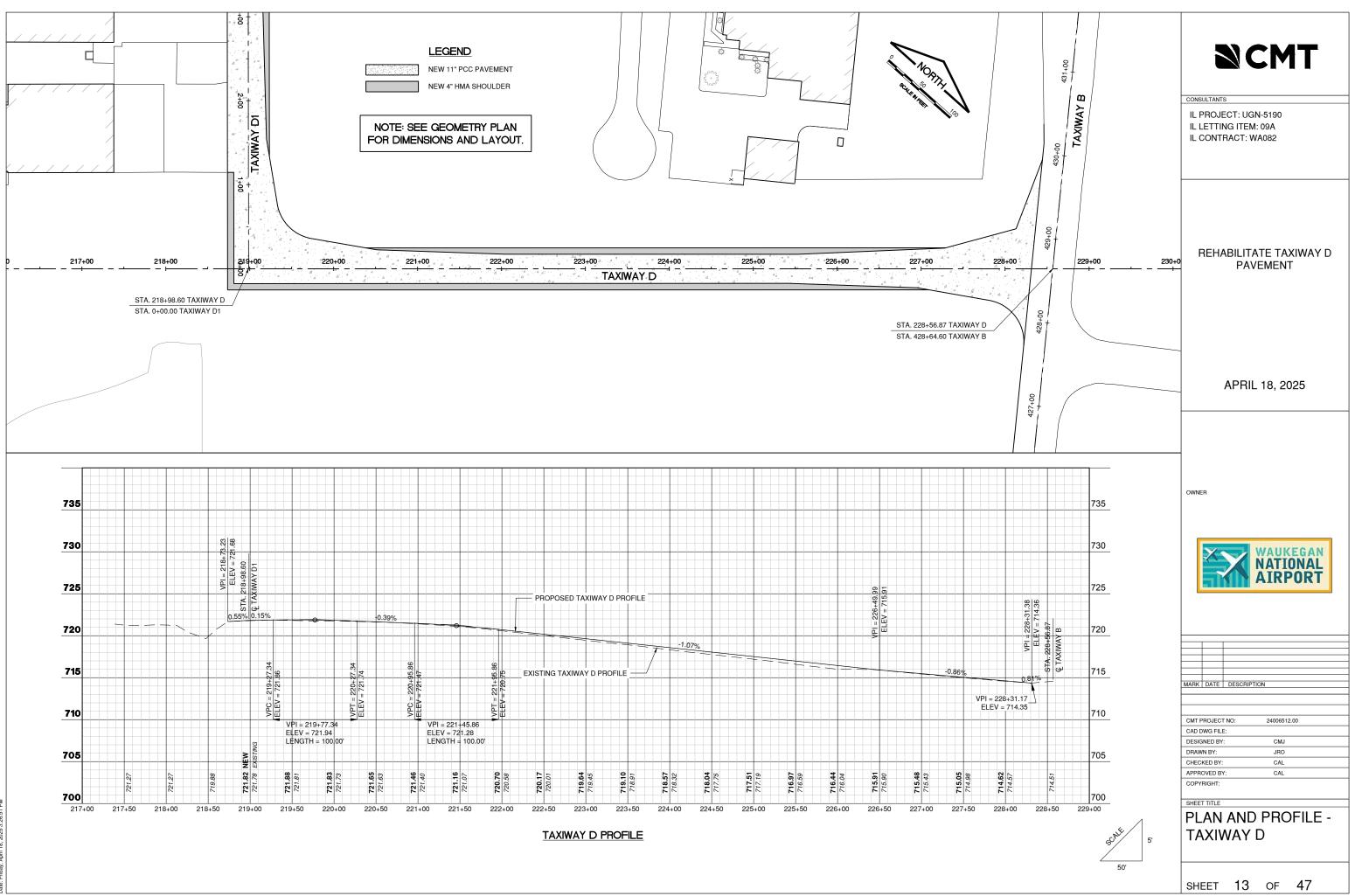
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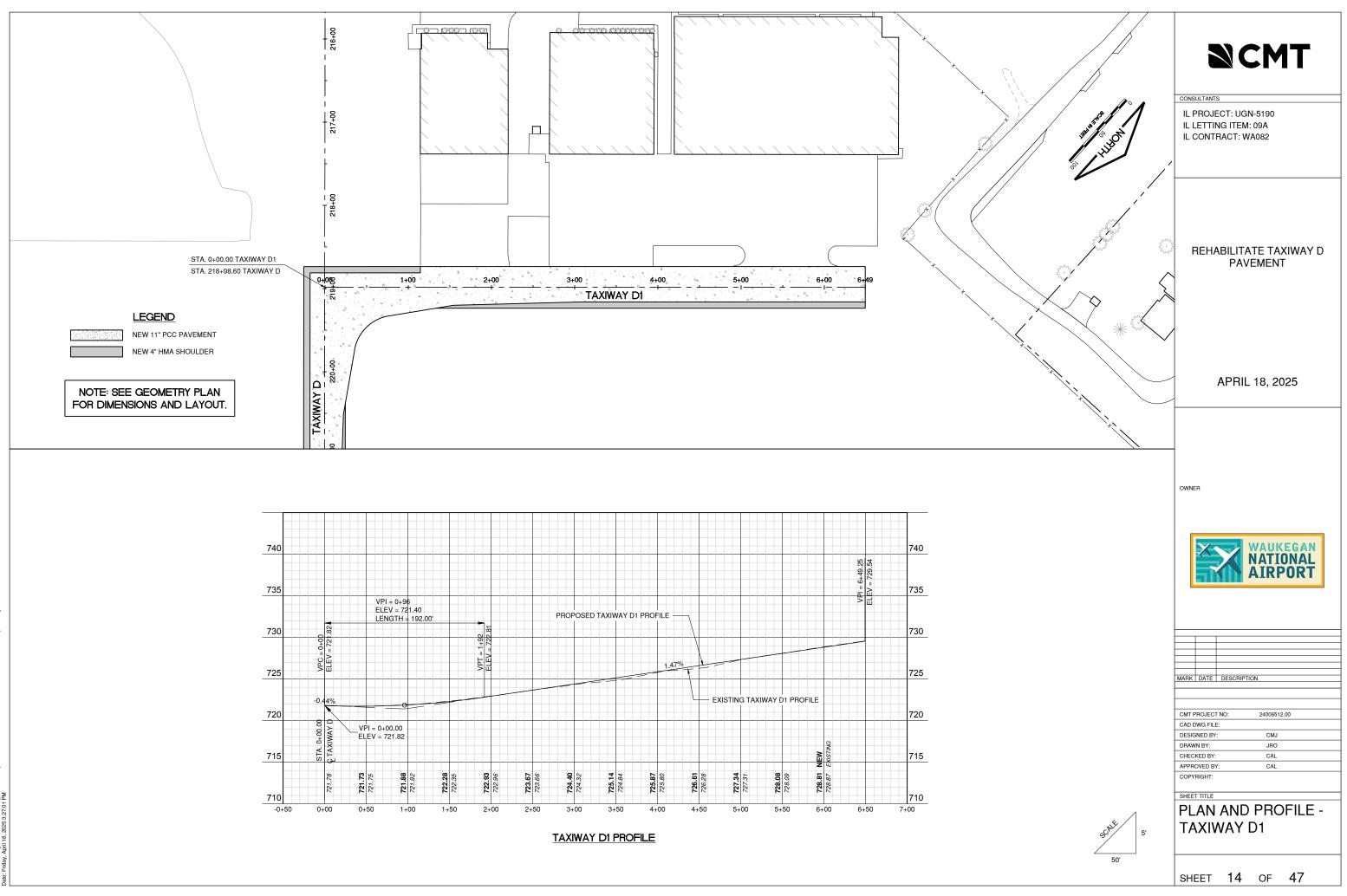
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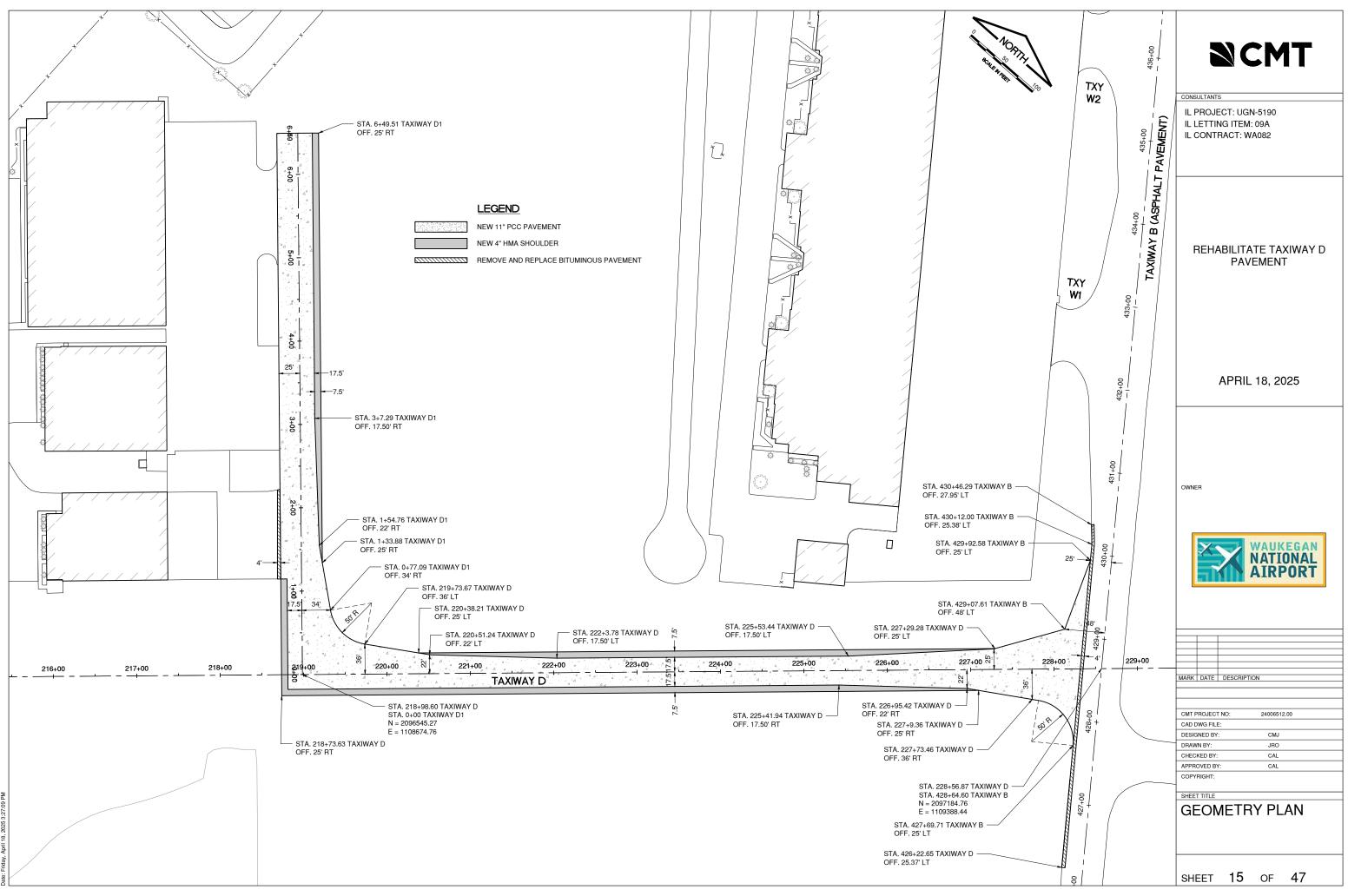


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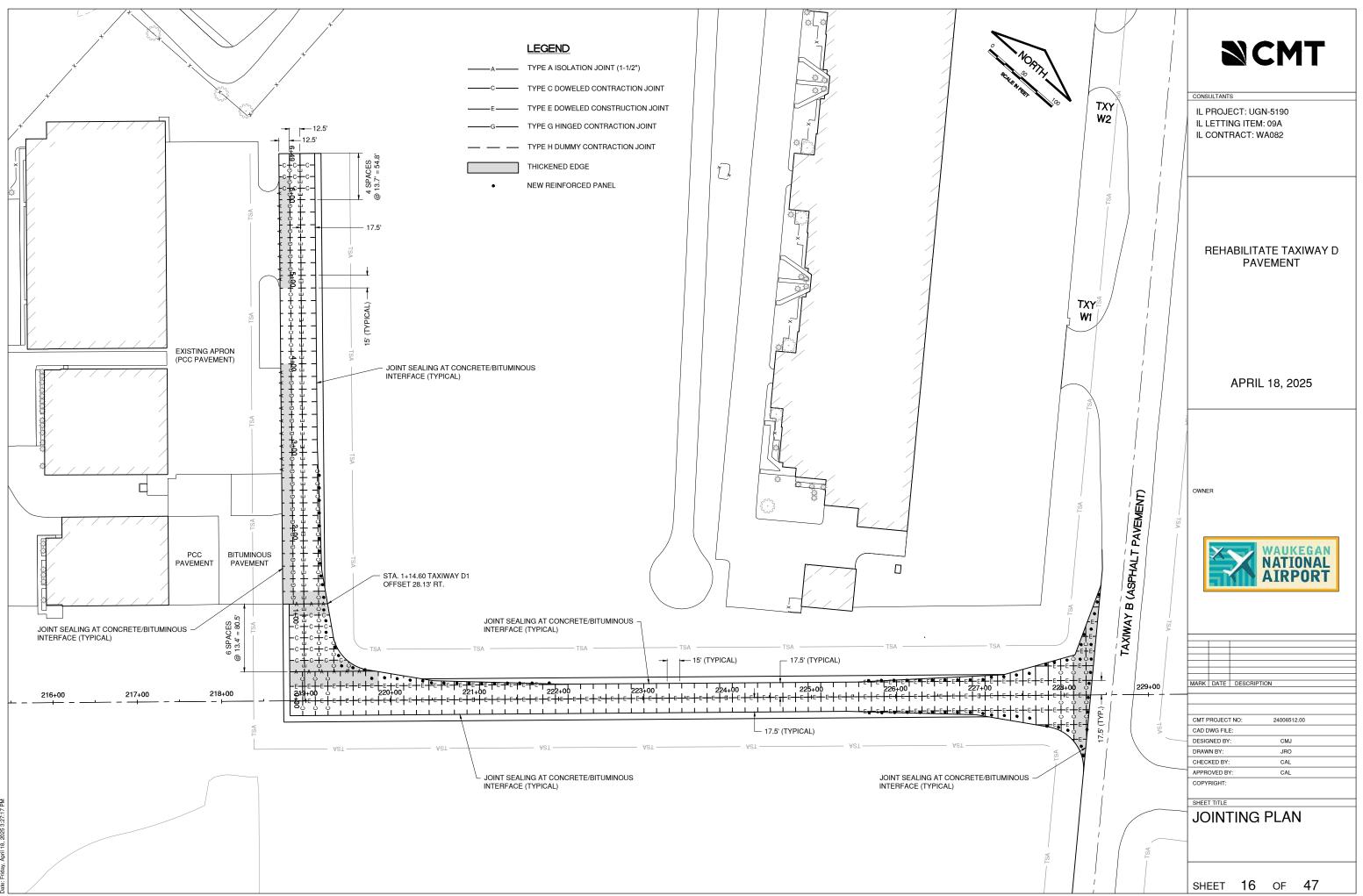
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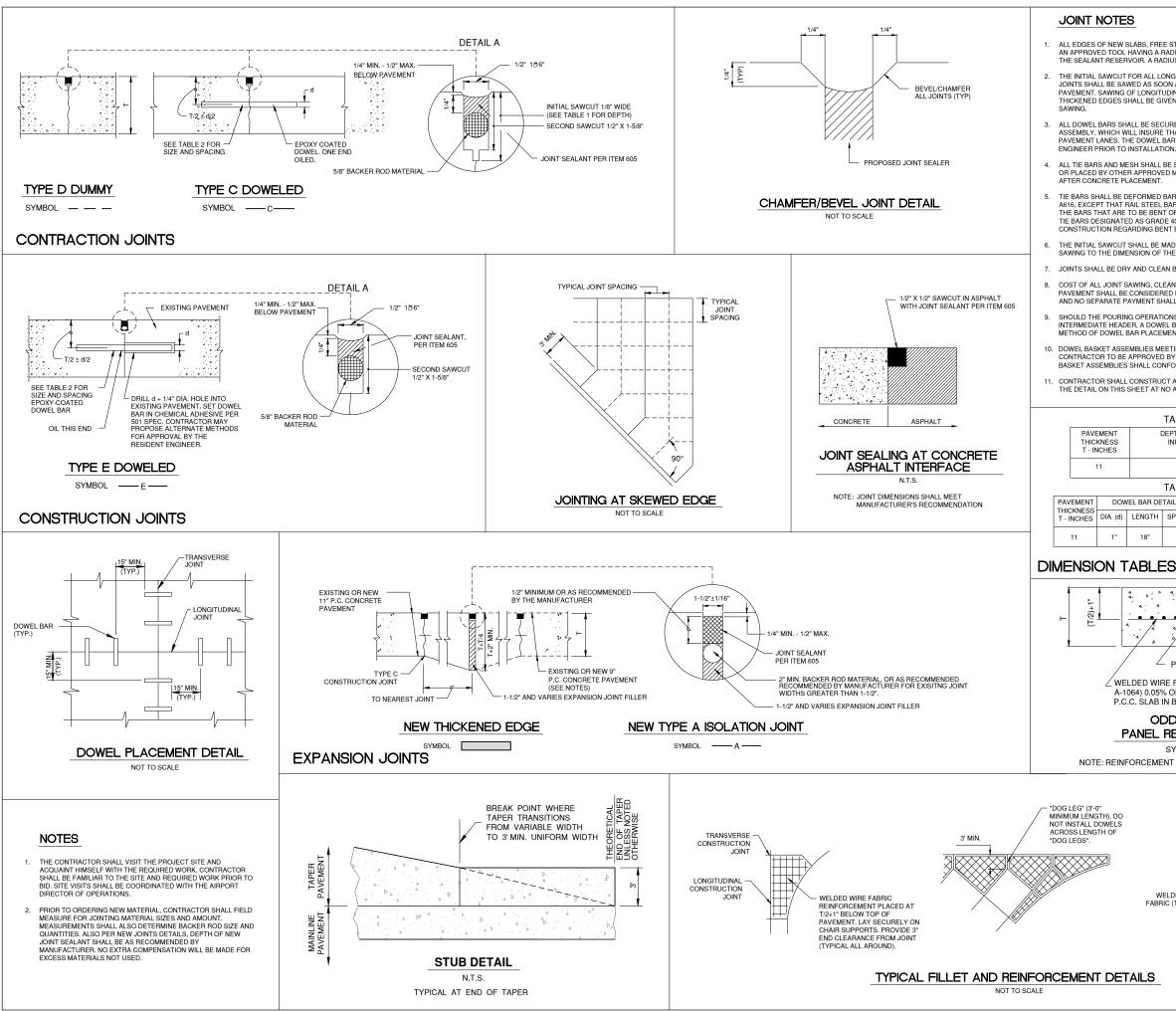




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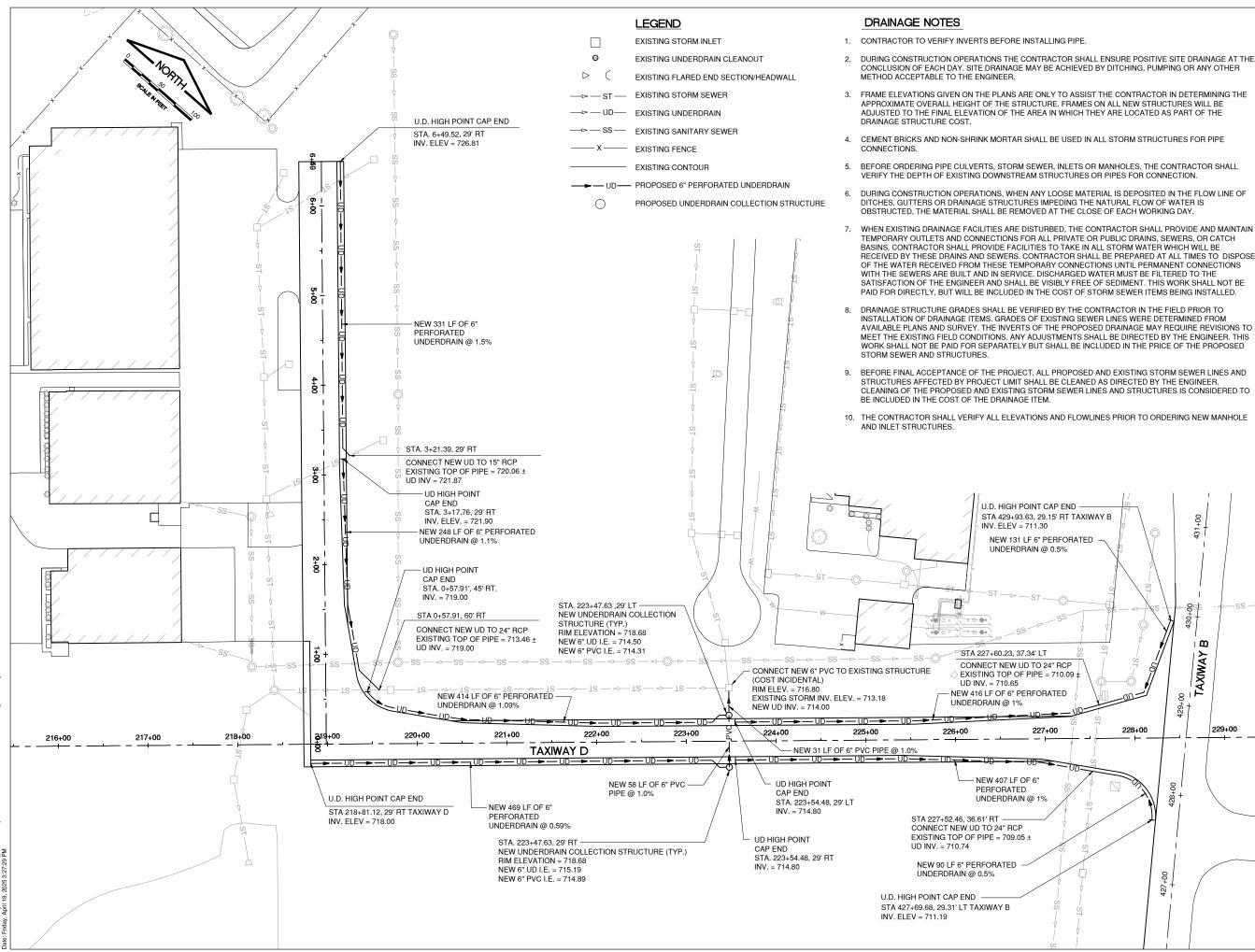


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ALL EDGES OF NEW SLABS. FREE STANDING OR CLOSURE. SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT CONSULTANTS ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR IL PROJECT: UGN-5190 ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. IL LETTING ITEM: 09A IL CONTRACT: WA082 ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED. 7. JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN. REHABILITATE TAXIWAY D COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM PAVEMENT AND NO SEPARATE PAYMENT SHALL BE MADE. SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED. 10. DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2. 11. CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER HE DETAIL ON THIS SHEET AT NO ADDITIONAL COST TABLE 1 APRIL 18, 2025 DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T= $(T/4) \pm 1/4"$ 2.75" TABLE 2 DOWEL BAR DETAILS TIE BAR DETAILS T - INCHES DIA. (d) LENGTH SPACING BAR SIZE LENGTH SPACING 18" 12" #5 30" 30" OWNER 2" MIN. - A -- 4 6" MAX. 4 · FROM WAUKEGAN JOINT 4 NATIONAL 4-AIRPORT - P.C.C. PAVEMENT WELDED WIRE FABRIC FLAT STOCK (ASTM A-1064) 0.05% OF CROSS SECTIONAL AREA OF P.C.C. SLAB IN BOTH DIRECTIONS ODD SHAPED PANEL REINFORCEMENT SYMBOL . NOTE: REINFORCEMENT SHALL NOT CROSS ANY JOINT MARK DATE DESCRIPTION CMT PROJECT NO: 24006512.00 CAD DWG FILE: DESIGNED BY CMJ JRO DRAWN BY CHECKED BY CAL APPROVED BY CAL "DOG LEG" COPYRIGHT WELDED WIRE FABRIC (TYPICAL) SHEET TITLE JOINTING DETAILS

SHEET 17 OF 47



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CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

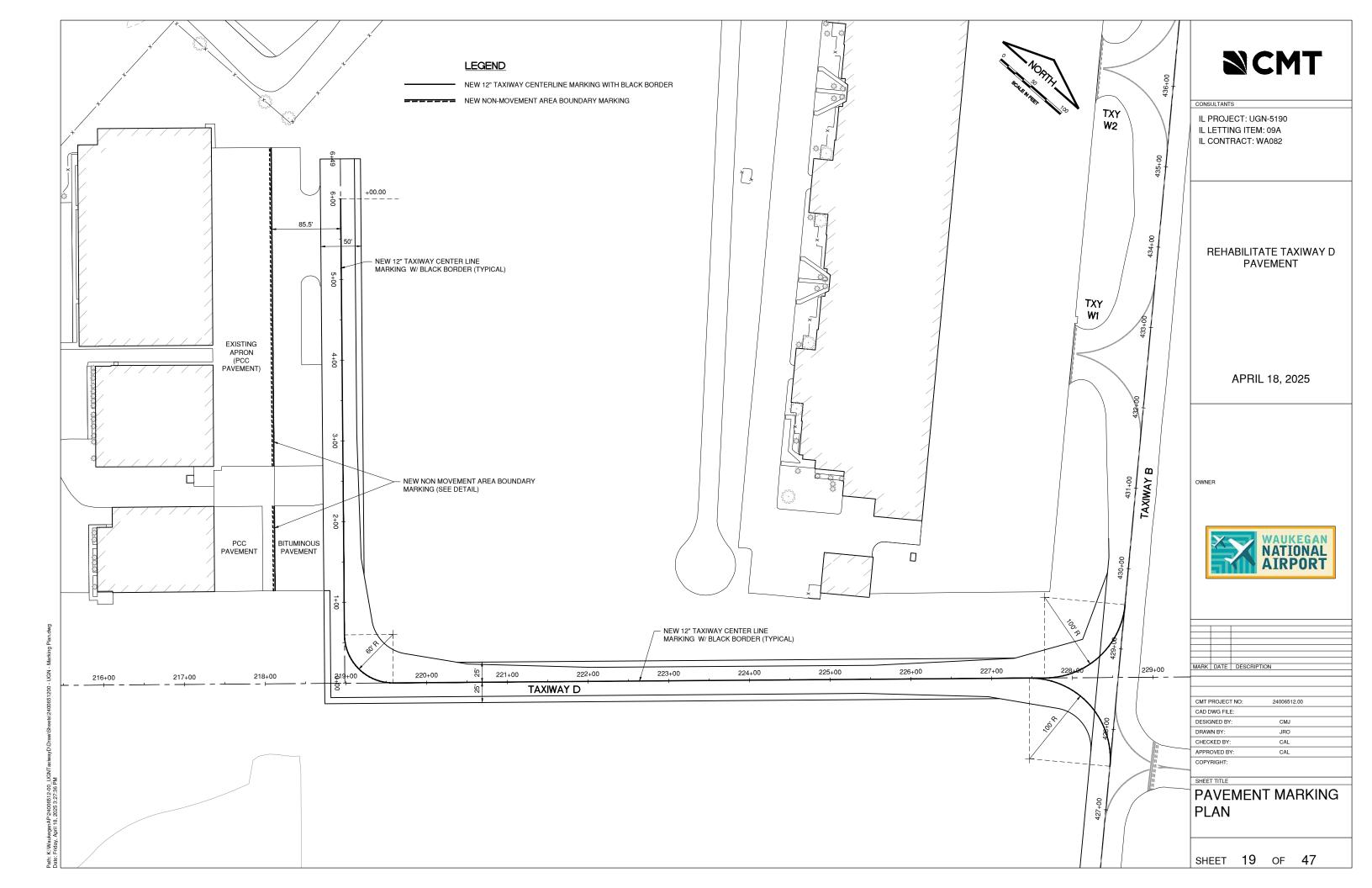
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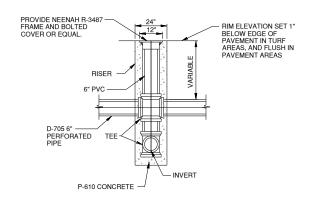
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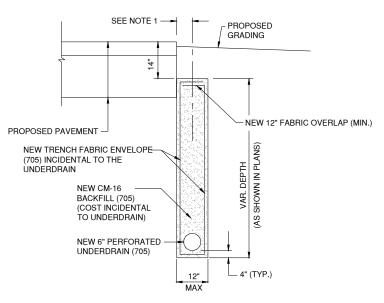


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TAXIWAY B	
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	SHEET 18 OF 47





UNDERDRAIN COLLECTION STRUCTURE (CS) DETAIL NOT TO SCALE



UNDERDRAIN DETAIL - PAVED AREAS

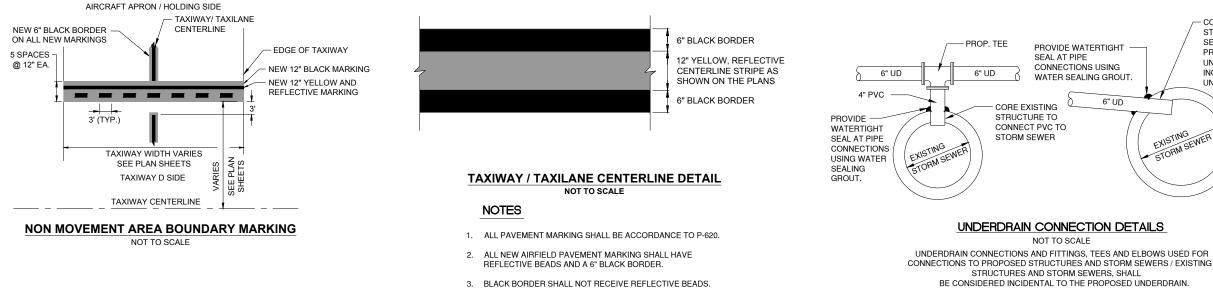
NOT TO SCALE

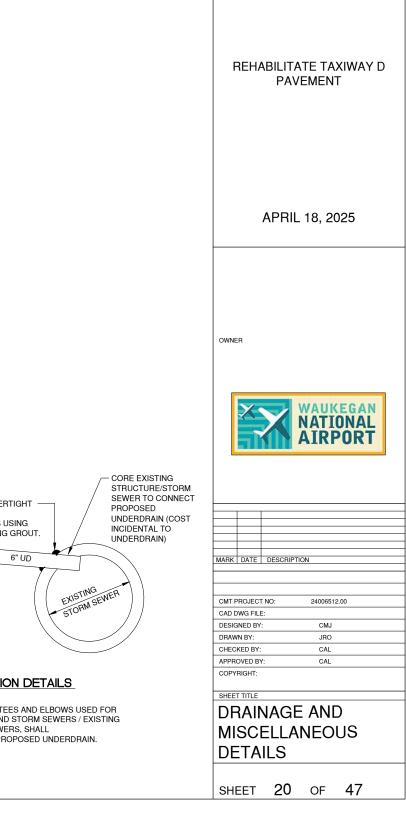
- 1. THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.
- 2. THE SPOILS FROM THE 6" UNDERDRAIN CONSTRUCTION SHALL BE REMOVED DAILY.

STORM SEWER/UNDERDRAIN NOTES

NOTES

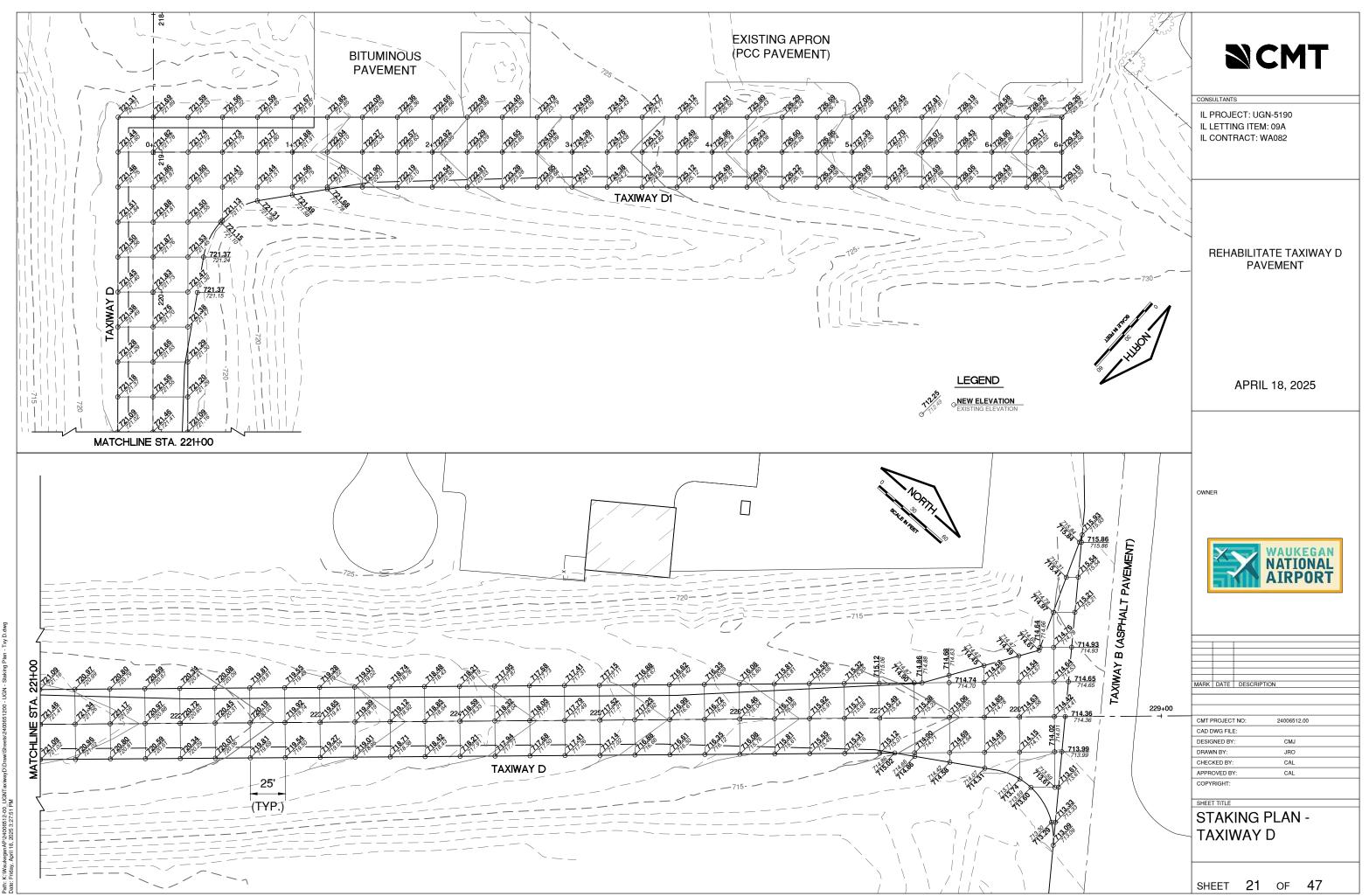
- 1. CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
- 2. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- 3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
- 4. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
- 5. CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER PIPE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.



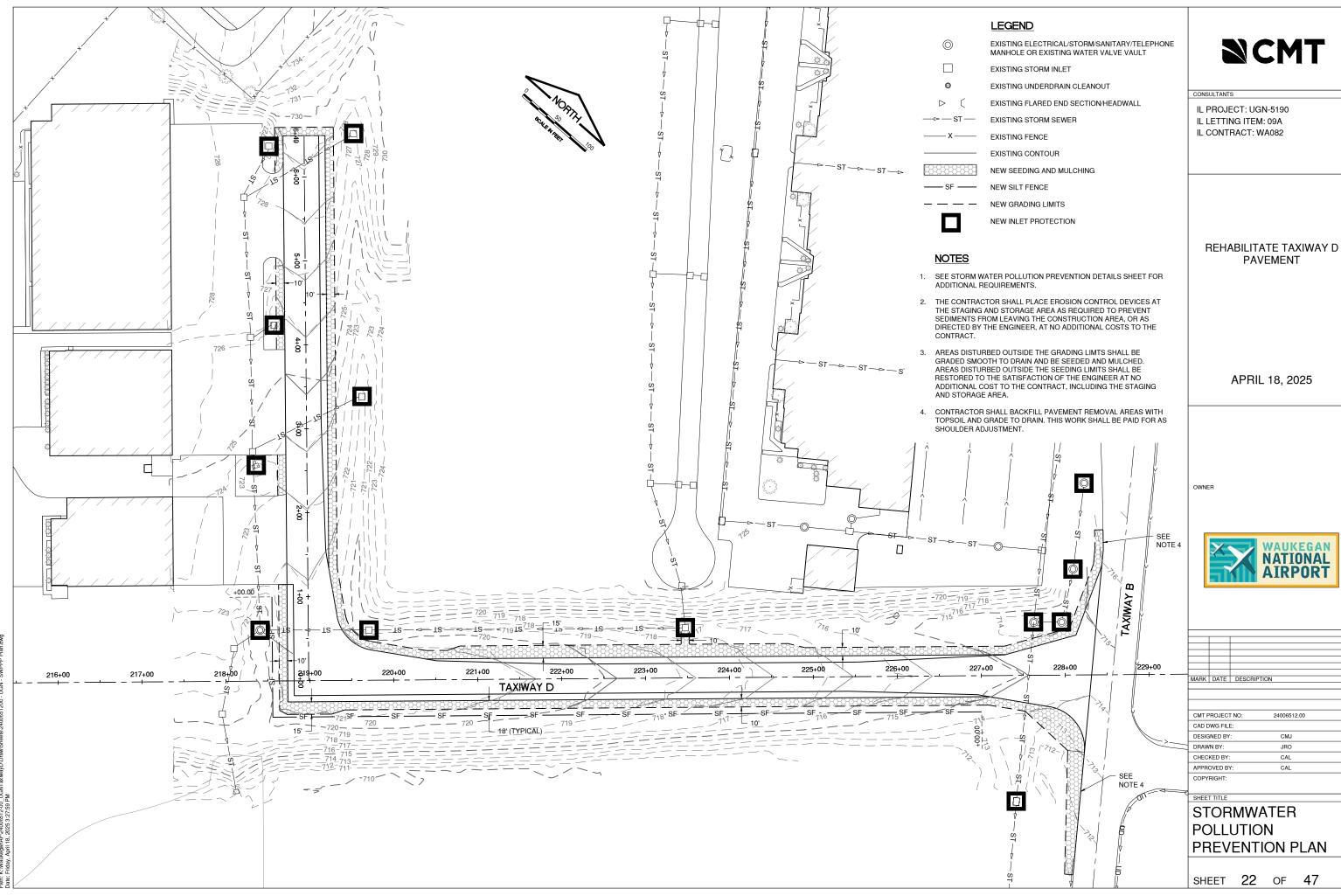


CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082



Txy D.dwg



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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EBOSION CONTROL SYSTEMS. WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING PAVEMENT AND DRAINAGE IMPROVEMENTS AT WAUKEGAN NATIONAL AIRPORT

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING

- 1. INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES
- 2. REMOVE EXISTING PAVEMENT.
- 3. EXCAVATION FOR NEW PAVEMENT
- 4. CONSTRUCT TEMPORARY CONCRETE WASHOUT.
- 5. INSTALL NEW TAXIWAY PAVEMENT
- 6. INSTALL NEW UNDERDRAIN AND ELECTRICAL ITEMS
- 7. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
- 8. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING

AREA OF CONSTRUCTION SITE AND RUNOFF COEFFICIENTS:

THE TOTAL AREA OF THE SITE ESTIMATED TO BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 2.0 ACRES. COMPOSITE RUNOFF COEFFICIENT PRIOR TO CONSTRUCTION = 0.67. COMPOSITE RUNOFF COEFFICIENT AFTER CONSTRUCTION = 0.67. THE TOTAL AREA OF CONSTRUCTION SITE IS ESTIMATED TO BE 5.2 ACRES.

EXISTING SOILS WITHIN PROJECT BOUNDARIES:

232 A - ASHKUM SILTY CLAY LOAM, 0 - 2% SLOPES, K = 0.20 298 A - BECHER SILT LOAM, 0 - 2% SLOPES, K = 0.37 298 A - BECHER SILT LOAM, 0 - 2% SLOPES, K = 0.37 298 B - BECHER SILT LOAM, 2 - 4% SLOPES, K = 0.37 530 B - OZAUKEE SILT LOAM, 2 - 4% SLOPES, K = 0.43 530 C2 - OZAUKEE SILT LOAM, 4 - 6% SLOPES, ERODED, K = 0.43

531 B - MARKHAM SILT LOAM, 2 - 4% SLOPES, K = 0.37

MS4 PERMITTEES IN THE AREA OF THIS PROJECT:

CITY OF WAUKEGAN WAUKEGAN TOWNSHIP LAKE COUNTY

- OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:
- 1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- 2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED LILINOIS ENVIRONMENTAL PROTECTION AGENCY. PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE

THE CONSTRUCTION LOCATED IN DES PLAINES RIVER WATERSHED. THE SITE DRAINS THROUGH A SERIES OF OVERLAND FLOW ROUTES/STORM SEWER INTO THE SUBURBAN COUNTRY CLUB TRIBUTARY THAT OUTLET INTO ULTIMATELY THE DES PLAINES RIVER.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORABILY OR PERMANENTLY CEASED IN A DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS VIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BI PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT. STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) DAYS

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIEF

- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES

SITE INSPECTIONS MUST INCLUDE DISTURBED AREAS OF THE CONSTRUCTION SITE AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE. AREAS INACCESSIBLE DURING INSPECTIONS DUE TO FLOODING OR OTHER UNSAFE CONDITIONS MUST BE INSPECTED WITHIN 72 HOURS OF BECOMING ACCESSIBLE

THE RESIDENT ENGINEER SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE THAT HAVE NOT BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY THAT IS 0.50 INCHES OR GREATER, INSPECTIONS MAY BE REDUCED TO ONCE PER MONTH WHEN CONSTRUCTION ACTIVITIES HAVE CEASED DUE TO FROZEN CONDITIONS. WEEKLY INSPECTIONS MUST RECOMMENCE WHEN CONSTRUCTION ACTIVITIES ARE CONDUCTED, IF THERE IS A 0.50 INCHES OR GREATER RAIN EVENT, OR A DISCHARGE DUE TO SNOWMELT OCCURS.

DISTURBED AREAS, AREAS USED FOR THE STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION AND ALL AREAS UISTONDED AREAS, AREAS OSED FOR THE STONAGE OF MATERIALS THAT ARE CAPOSED TO FREUTHTATION AND ALL AREAS WHERE STORMWATER TYPICALLY FLOWS WITHIN THE SITE MUST BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. ALL LOCATIONS WHERE STABILIZATION MEASURES HAVE BEEN IMPLEMENTED MUST BE OBSERVED TO ENSURE THAT THEY ARE STILL STABILIZED, WHERE DISCHARGE LOCATION OR POINTS ARE ACCESSIBLE, THEY MUST BE INSPECTED TO ASCERTAIN WHETHER EBOSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE MUST BE PECTED FOR EVIDENCE OF OFFSITE SEDIMENT TRACKING

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. <u>THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.</u>

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGEF NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE

THE CONTRACTOR SHALL PROVIDE THE RESIDENT ENGINEER WITH A SPILL RESPONSE PLAN, WHICH SHALL INCLUDE CLEANUP PROCEDURES, CONTAINMENT EFFORTS, AND PROPER EVACUATION PROCEDURES IF APPLICABLE. THE CONTRACTOR SHALL ALSO PROVIDE PROVISIONS FOR REPORTING WHEN THEY RELEASE SPILL EQUAL TO OR EXCEEDING REPORTABLE QUANTITIES.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

AINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE OR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AFRONALITICS MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE

DOCUMENTATION

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA 2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLITION PREVENTION PLAN EBOSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN. ALL INSPECTION REPORTS MUST BE RETAINED AT THE CONSTRUCTION SITE.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT

THE RESIDENT ENGINEER AND/OR CONTRACTOR MUST NOTIFY THE APPROPRIATE AGENCY FIELD OPERATIONS SECTION OFFICE BY EMAIL AT: EPA.SWNONCOMP@ILLINOIS.GOV. TELEPHONE, OR FAX (SEE ATTACHMENT A OF THE GENERAL NPDES STORMWATER PERMIT FOR CONSTRUCTION ACTIVITIES) WITHIN 24 HOURS OF ANY ION FOR ANY VIOLATION OF THE SWPPP OBSERVED DURING ANY INSPECTION. CONDUCTED, OR FOR VIOLATIONS OF ANY CONDITION OF THIS PERMIT. THE RESIDENT ENGINEER AND/OR CONTRACTOR MUST COMPLETE AND SUBMIT WITHIN 5 DAYS AN ION REPORT. CORRECTIVE ACTIONS MUST BE UNDERTAKEN IMMEDIATELY TO ADDRESS THE IDENTIFIED NON-COMPLIANCE ISSUE(S).

AFTER THE INITIAL CONTACT HAS BEEN MADE WITH THE APPROPRIATE AGENCY FIELD OPERATIONS SECTION OFFICE, ALL REPORTS OF AFTEN THE INTUG CONTACT MAS BEEN MADE WITH THE AFFORMATE AGENCY FIELD OF PARTONS SECTION OTHER, ALL REPORTS OF NONCOMPLIANCE MUST BE MAILED TO THE AGENCY AT THE NEW ADDRESS, NOT THE ONE IN THE PERMIT. THIS ADDRESS IS AS FOLLOWS: ILLINOIS EPA/2520 W. ILES AVE./P.O. BOX 19276/SPRINGFIELD, IL 62794-9276. PLEASE NOTE THAT IF THESE ARE DELIVERED VIA FEDEX OR UPS, THESE CARRIERS CANNOT DELIVER TO OUR P.O. BOX AND THIS NUMBER MUST BE EXCLUDED FROM THE MAILING ADDRESS.

AFTER PROJECT FINAL ACCEPTANCE. THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS'

NPDES PERMIT # ____

DATE ISSUED

DATE EXPIRED

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL

- ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- 2. NO WORK SHALL BE PERFORMED IN FLOWING WATER, WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE
- 3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- 4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE
- GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME
- 6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- 7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS. THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER LAKE COUNTY, CITY OF WAUKEGAN, WAUKEGAN NATIONAL AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR. THE CONTRACTOR SHALL ALSO ENSURE AND DEMONSTRATE COMPLIANCE WITH LAKE COUNTY, IDOT, CITY OF WAUKEGAN'S LOCAL WASTE DISPOSAL, SANITARY SEWER OR SEPTIC SYSTEM REGULATIONS.
- 9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY AT A MINIMUM OF 50 FEET FROM RECEIVING WATERS, CONSTRUCTED OR NATURAL SITE DRAINAGE FEATURES, AND STORM DRAIN INLETS.
- 10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- 11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- APPROVED MEANS
- 13. INLET PROTECTION MEASURES MUST BE INSTALLED PRIOR TO STORM WATER RUNOFF FROM THE SITE ENTERING ANY STORM DRAIN INLET THAT CARRIES STORMWATER FLOW FROM THE SITE TO A WATER OF THE U.S., PROVIDED THERE IS THE AUTHORITY TO DO SO, SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS, CLEAN, OR REMOVE AND REPLACE, THE PROTECTION MEASURES AS SEDIMENT ACCUMULATES, THE FILTER BECOMES CLOGGED, AND/OR PERFORMANCE IS COMPROMISED, WHERE THERE IS EVIDENCE OF SEDIMENT ACCUMULATION ADJACENT TO THE INLET PROTECTION MEASURE, THE DEPOSITED SEDIMENT MUST BE REMOVED BY THE FOLLOWING BUSINESS DAY
- 14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL
- THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER, ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EBOSION CONTROL MEASUBES INCLUDING TEMPOBABY SEEDING MULCHING AND/OB EBOSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
- 16. STABILIZATION FEFORTS MUST BE INITIATED WITHIN 1 WORKING DAY OF THE CESSATION OF CONSTRUCTION ACTIVITIES IF THE PORTION UTIL NOT BE DISTURBED FOR AT LEAST 14 CALENDAR DAYS, STABILIZATION OF CONSTINUTION ACTIVITIES IF THE PORTION WILL NOT BE DISTURBED FOR AT LEAST 14 CALENDAR DAYS, STABILIZATION EFFORTS MUST BE COMPLETED WITHIN 14 CALENDAR DAYS, EXCEPTIONS TO THESE REQUIREMENTS ARE PROVIDED IN THE PERMIT. A RECORD OF THE DATES WHEN MAJOR GRADING ACTIVITIES OCCUR, WHEN CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE, AND WHEN STABILIZATION MEASURES ARE INITIATED MUST BE INCLUDED IN THE SWPPP.
- 17. THE CONTRACTOR SHALL MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING. WHEFI WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
- 18. THE CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF FUEL, OIL, HYDRAULIC FLUIDS, OTHER PETROLEUM PRODUCTS, AND THE CONTINUE OF THE INFORMATION OF THE ADDRESS OF CONTAINMENT AREAS. ANY CHEMICAL CONTAINED WITH A STORAGE OF 55 GALLONS OR MORE MUST BE STORED A MINIMUM OF 50 FEET FROM RECEIVING WATERS, CONSTRUCTED OR NATURAL SITE DRAINAGE FEATURES, AND STORM DRAIN INLETS. IF INFEASIBLE DUE TO SITE CONSTRAINTS, STORE CONTAINERS AS FAR AWAY AS THE SITE PERMITS AND DOCUMENT IN THE SWPPP THE SPECIFIC REASONS WHY THE 50-FOOT SETBACK IS INFEASIBLE AND HOW THE CONTAINERS WILL BE STORED.
- 19. NO SOLID MATERIALS, INCLUDING BUILDING MATERIALS, WILL BE DISCHARGED TO WATERS OF THE UNITED STATES, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
- 20. COBRECTIVE ACTIONS MUST BE TAKEN TO ADDRESS ANY OF THE FOLLOWING CONDITIONS IDENTIFIED AT THE SITE: A STORMWATER CONTROL NEEDS REPAIR OR REPLACEMENT; A STORMWATER CONTROL NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT WAS NEVER INSTALLED OR WAS INSTALLED INCORRECTLY; DISCHARGES ARE CAUSING AN EXCEEDANCE OF APPLICABLE WATER QUALITY STANDARDS; OR A PROHIBITED DISCHARGE HAS OCCURRED. CORRECTIVE ACTIONS MUST BE COMPLETED AS SOON AS POSSIBLE AND DOCUMENTED WITHIN 7 DAYS IN AN INSPECTION REPORT OR REPORT OF NONCOMPLIANCE, IF IT IS INFEASIBLE TO COMPLETE THE INSTALLATION OR REPAIR WITHIN 7 CALENDAR DAYS, IT MUST BE DOCUMENTED IN THE RECORDS WHY IT IS INFEASIBLE TO COMPLETE THE INSTALLATION OR REPAIR WITHIN THE 7-DAY TIMEFRAME AND DOCUMENT THE SCHEDULE FOR INSTALLING THE STORMWATER CONTROL(S) AND MAKING IT OPERATIONAL AS SOON AS FEASIBLE AFTER THE 7-DAY TIMEFRAME. IF MAINTENANCE IS REQUIRED FOR THE SAME STORMWATER CONTROL AT THE SAME LOCATION THREE OR MORE TIMES. THE CONTROL MUST BE REPAIRED IN A MANNER THAT PREVENTS CONTINUED FAILURE TO THE EXTENT FEASIBLE, AND THE CONDITION AND HOW IT WAS BEPAIDED MUST BE RECORDED ALTERNATIVELY. IT MUST BE ADDRESSED AS A ROUTINE MAINTENANCE FIX.

BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10	SSUED BY
PROJECT INFORMATION: ROUTE: WAUKEGAN NATIONAL AIRPORT SECTION: 32 COUNTY: LAKE CONT	ED: <u>RE</u> ECT NUMB RACT NUM
I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10 INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDI) THAT AL
SIGNATURE: DA	TE:
PRINTED NAME: TIT	LE:
NAME OF FIRM:	
STREET ADDRESS:	
CITY, STATE, ZIP:	
PHONE NUMBER:	
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETE OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMP	

5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG

12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN

CONTRACTOR CERTIFICATION STATEMENT THO OF THE PROJECT DESCRIBED THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

HABILITATE TAXIWAY D

ER: UGN-5190 IBER: 3-17-SBGP-TBD

MS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION UTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH AS PART OF THIS CERTIFICATION.

CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO THIS IS A CONTRACT REQUIREMENT



CONSULTANTS

IL PROJECT: UGN-5190 IL LETTING ITEM: 09A IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

OWNER



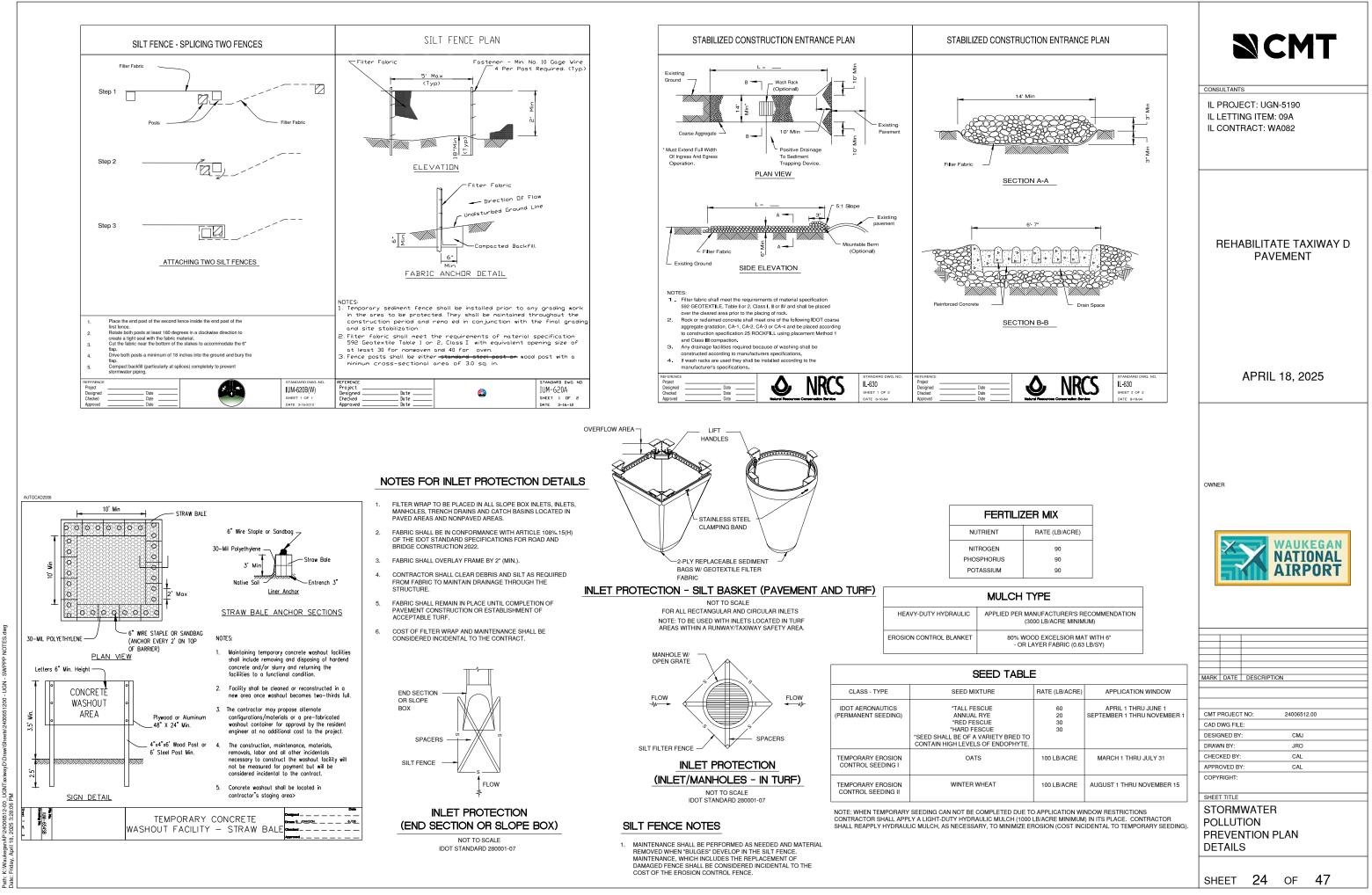
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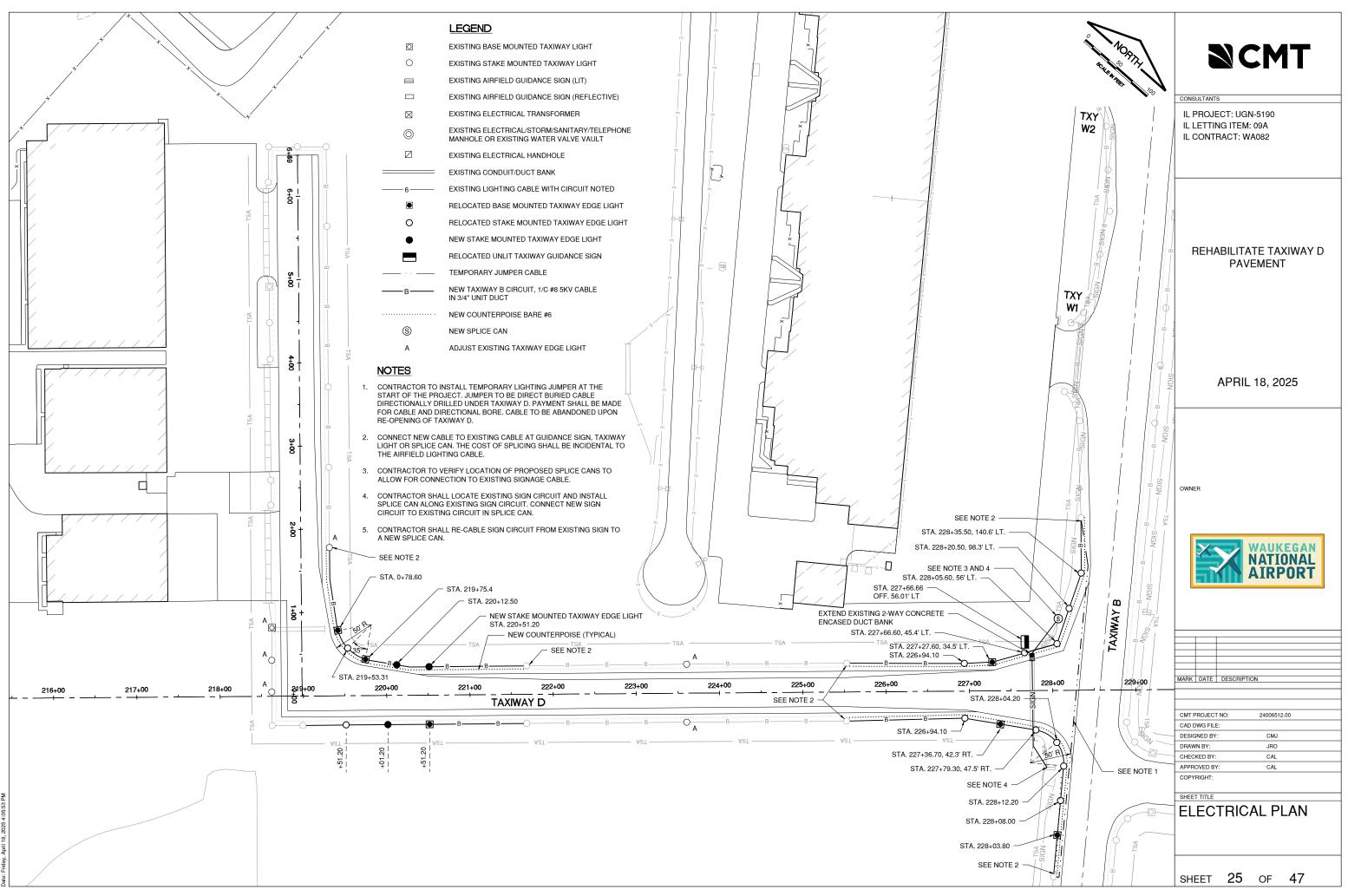
CMT PROJECT NO:	24006512.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	CAL
COPYRIGHT:	

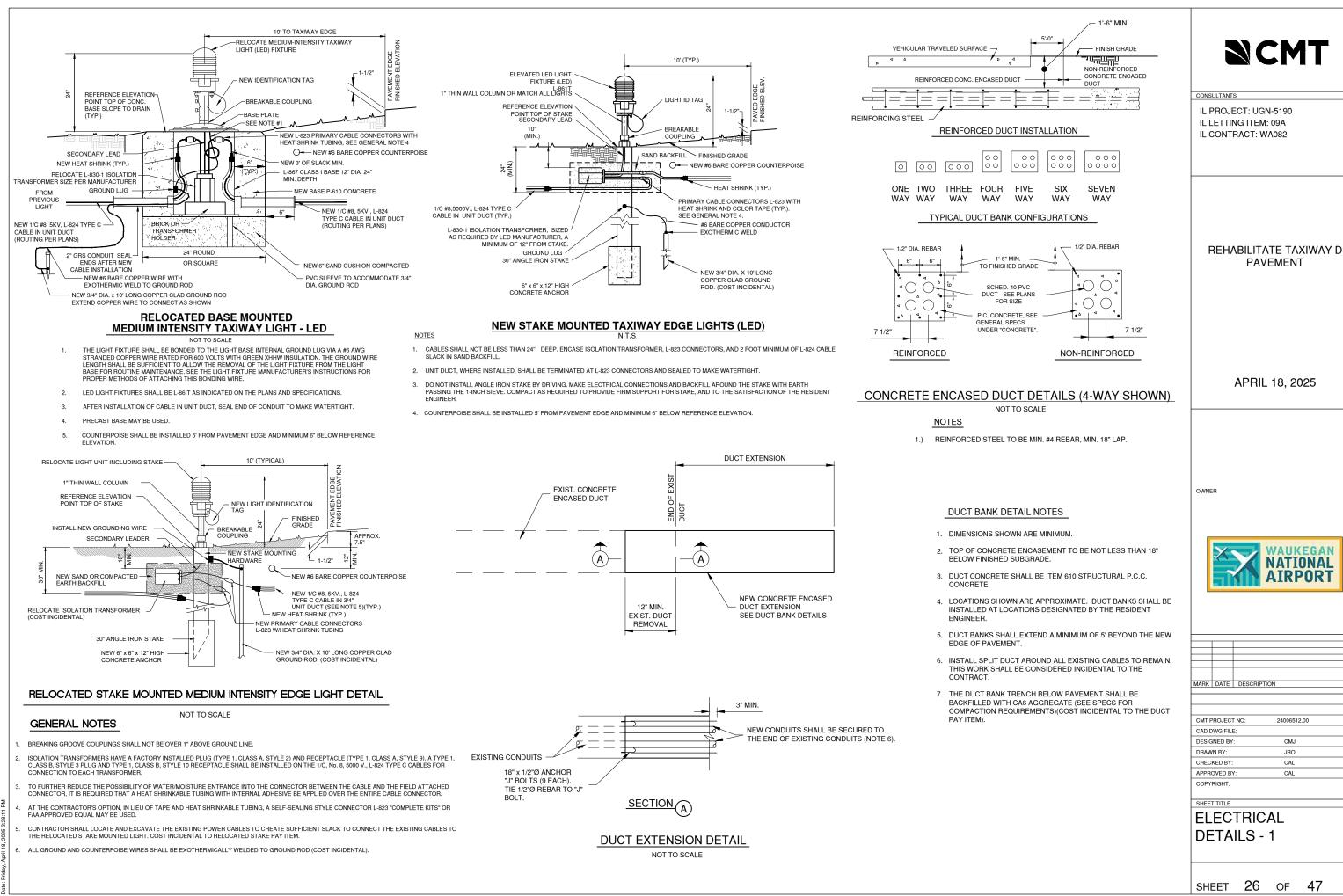
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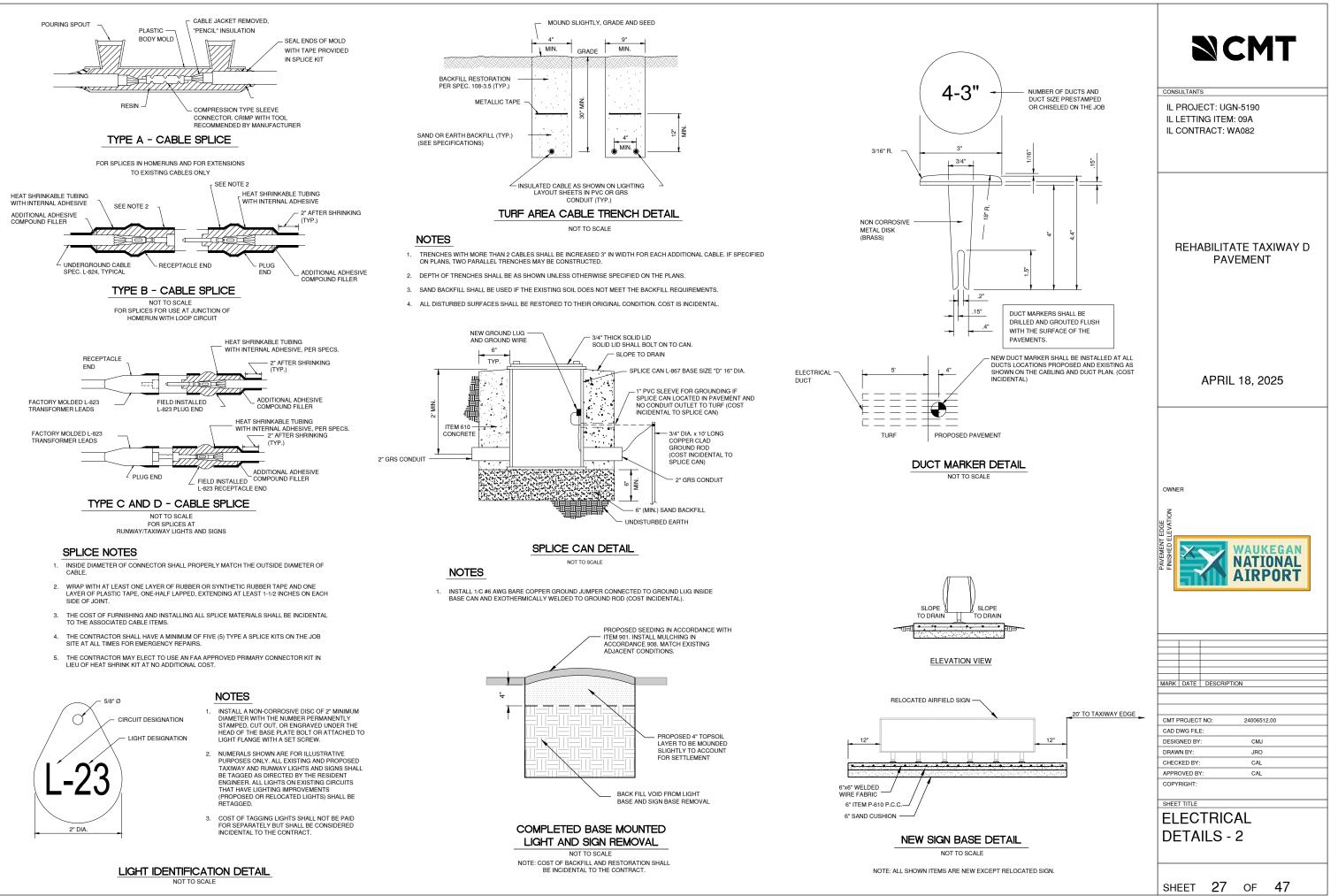
STORMWATER POLLUTION PREVENTION PLAN NOTES

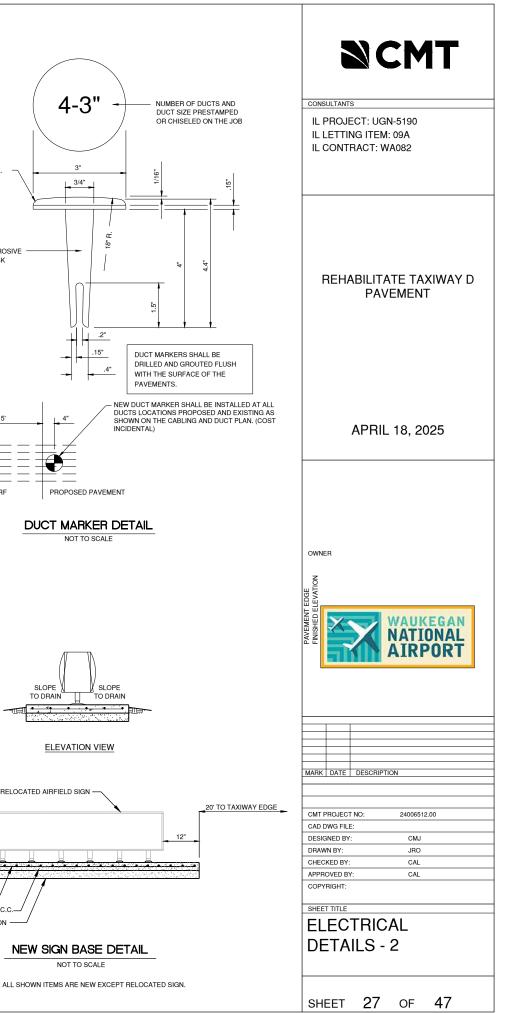
SHEET 23 OF 47

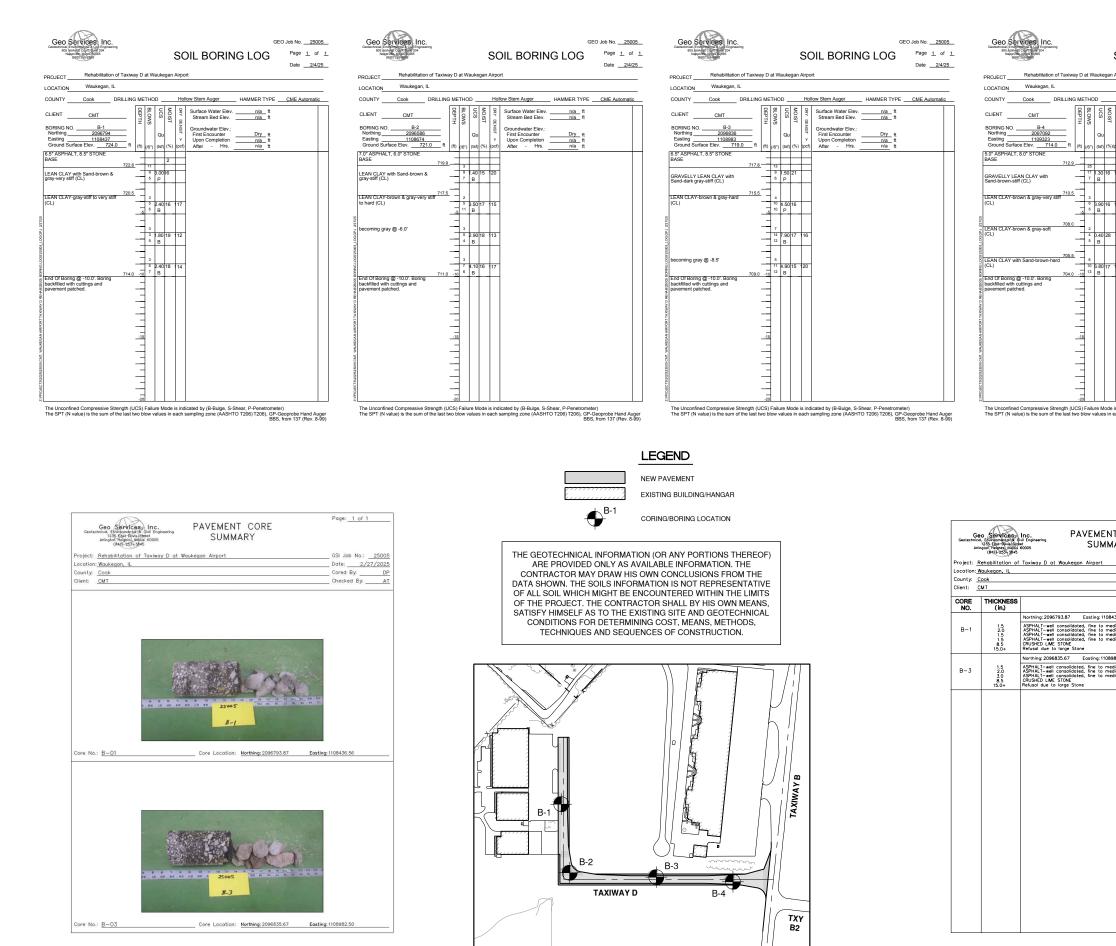












GEO Job No. <u>25005</u> SOIL BORING LOG Page 1 of 1	
Date2/4/25	CONSULTAN
ukegan Airport	IL PRO
D Hollow Stem Auger HAMMER TYPE CME Automatic	IL LETT
CS S Surface Water Elev. <u>n/a</u> ft Stream Bed Elev. <u>n/a</u> ft	IL CONT
Z on the second se	
Qu Y Dry_fit Y Upon Completion ft (st) (%)(%)(%) After - Hrs. ft	
30 16 117	
B	
.9016 117	
B	
140 28 96	
8	REH
.80 17 116	
B	
e Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) liues in each sampling zone (AASHTO 1206) 1206), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)	
MENT CORE	OWNER
JMMARY	
GSI Job No.: _25005	
Date: 2/27/2025 Cored By: DP Checked By: AT	
Checked By: <u>AT</u>	X
DESCRIPTION g: 1108436.56	
to medjum oggregate. Surface mix. to medjum oggregate. Surface mix. to medjum oggregate. Surface mix. to medjum cosre oggregate. Binder mix.	
g: 1108982.50 to medjum aggregațe. Surface mix.	
to medium aggregate. Surface mix. to medium aggregate. Surface mix. to medium coarse aggregate. Binder mix.	
	MARK DATE
	CMT PROJEC
	CAD DWG FI
	DESIGNED B DRAWN BY:
	CHECKED B
	COPYRIGHT
	SHEET TITLE
	GEO
	LAYC
	INFO



JECT: UGN-5190 TING ITEM: 09A ITRACT: WA082

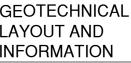
HABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

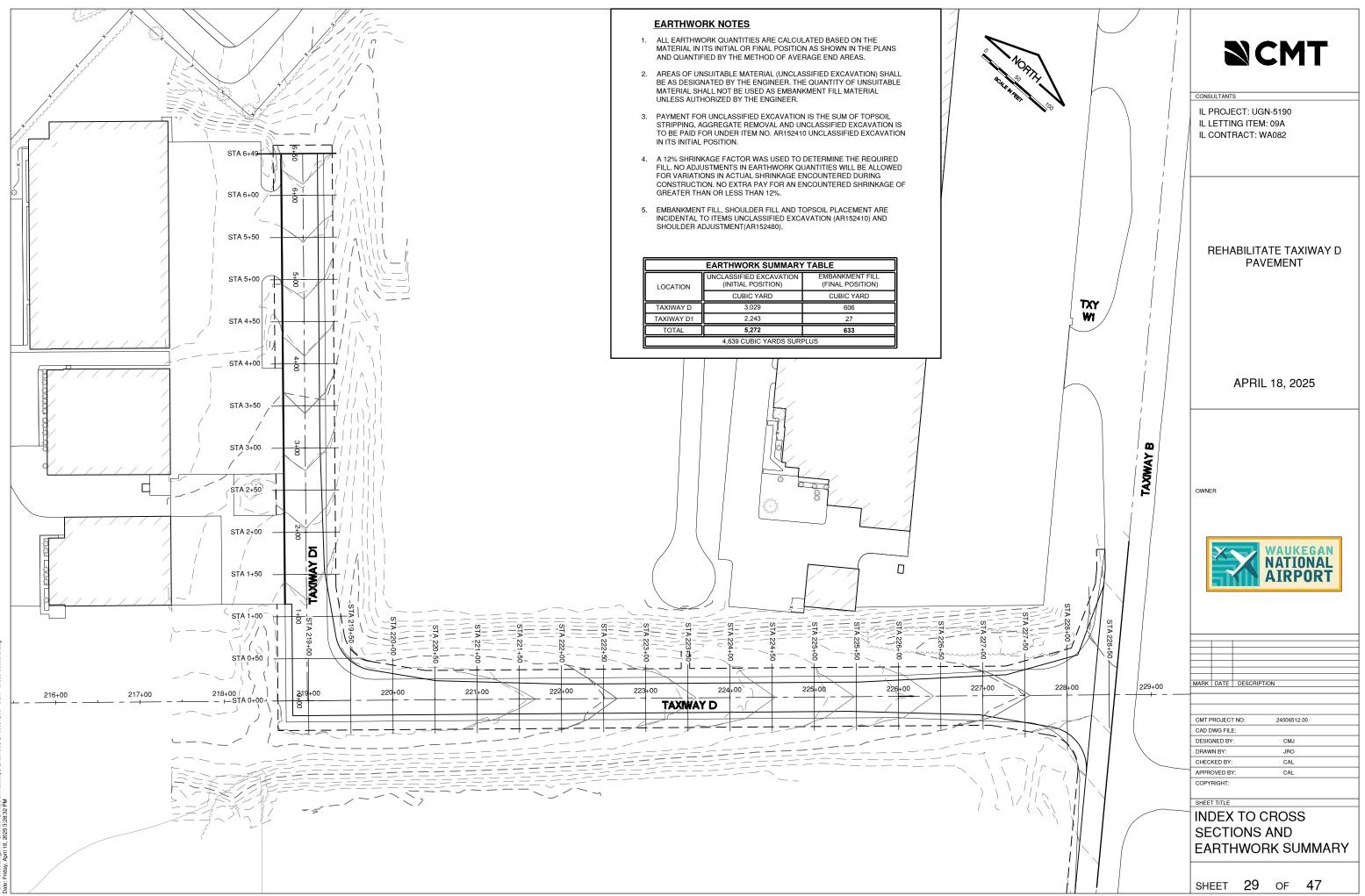


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CMT F	ROJECT	NO:	24006512.00	

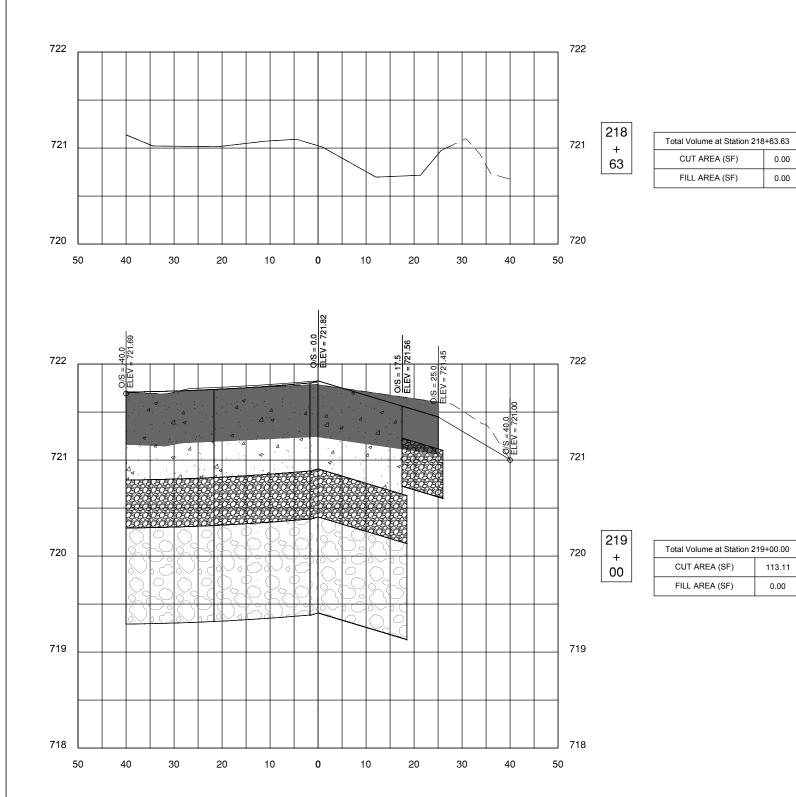
CAD DWG FILE:		
DESIGNED BY:	CMJ	
DRAWN BY:	JRO	
CHECKED BY:	CAL	
APPROVED BY:	CAL	
COPYRIGHT:		



SHEET 28 OF 47



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dwg

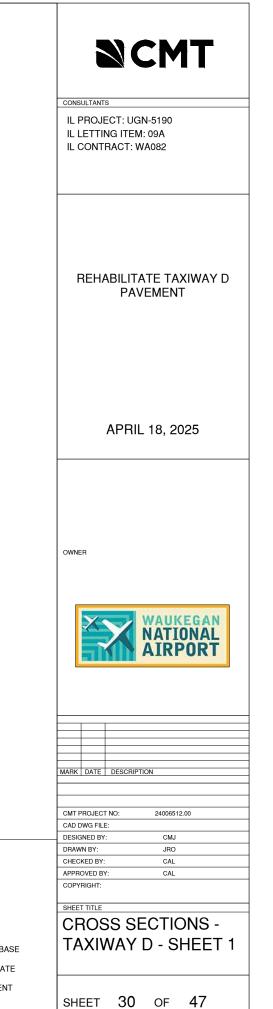
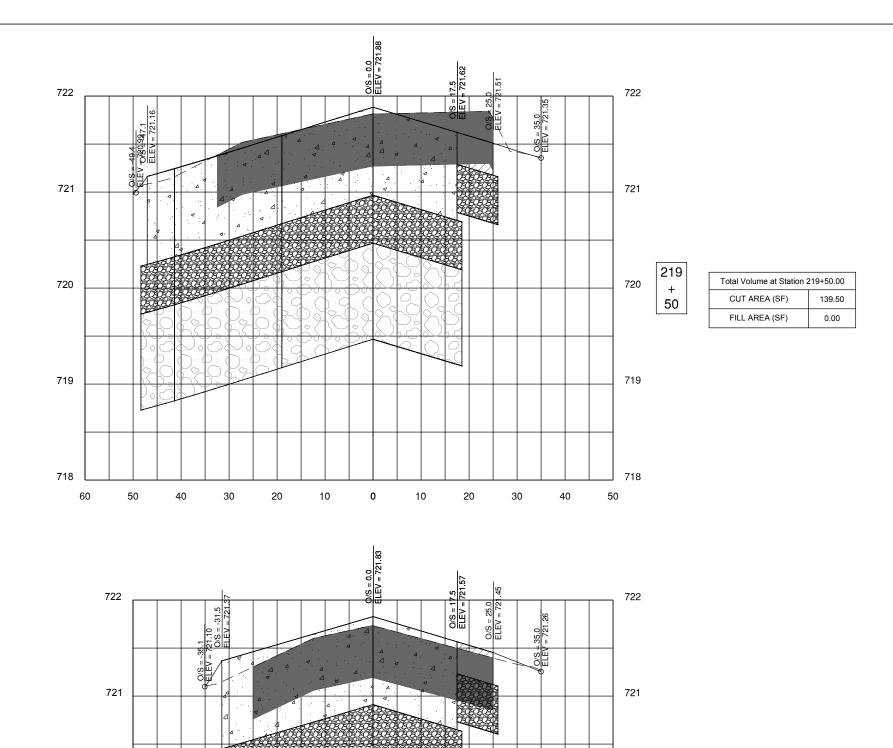


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 Total Volume at Station 220+00.00

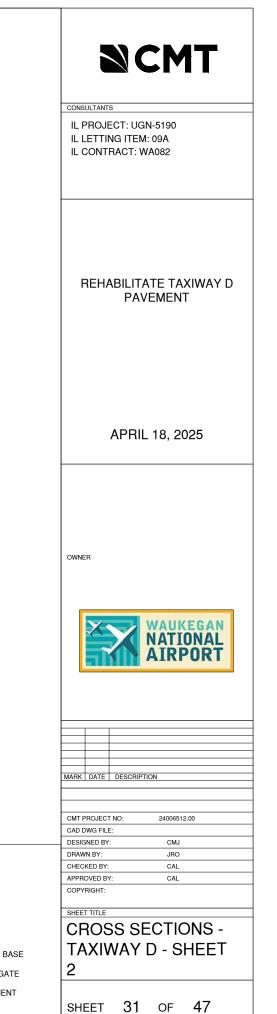
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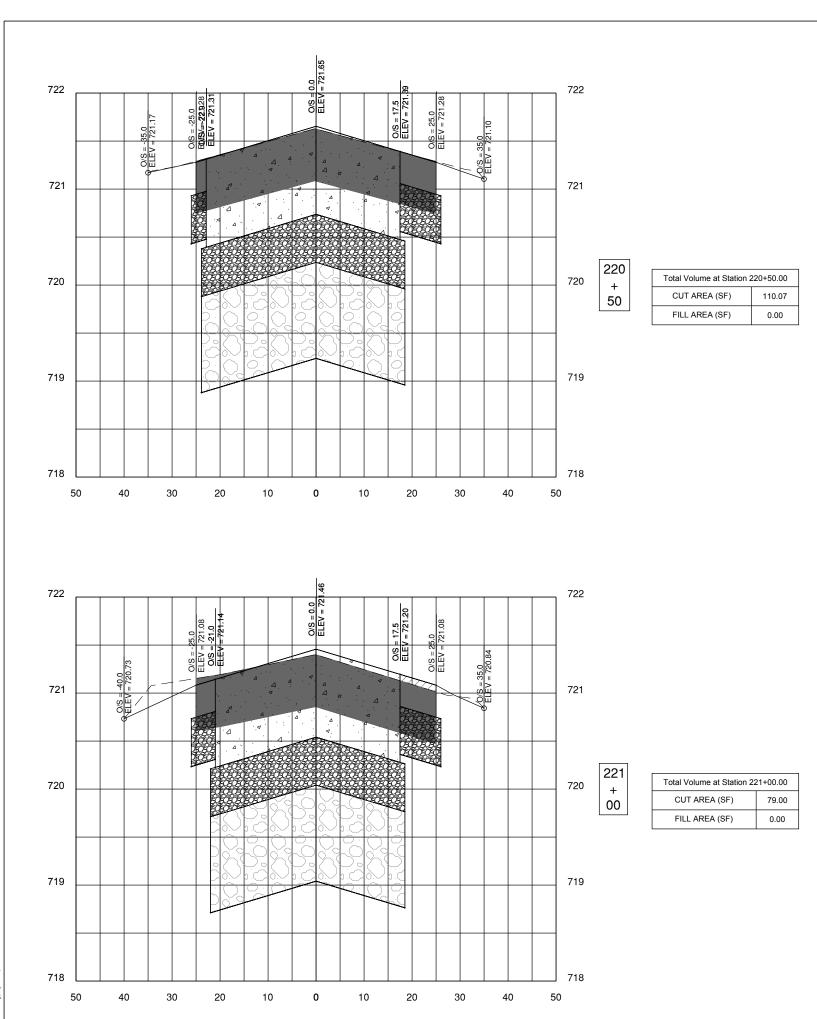
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 FILL AREA (SF)

 0.00

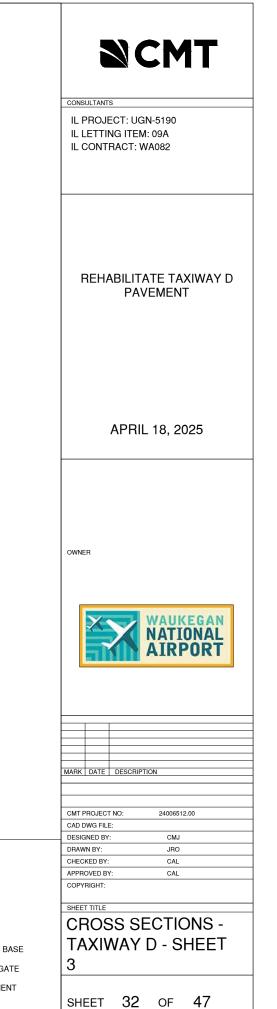
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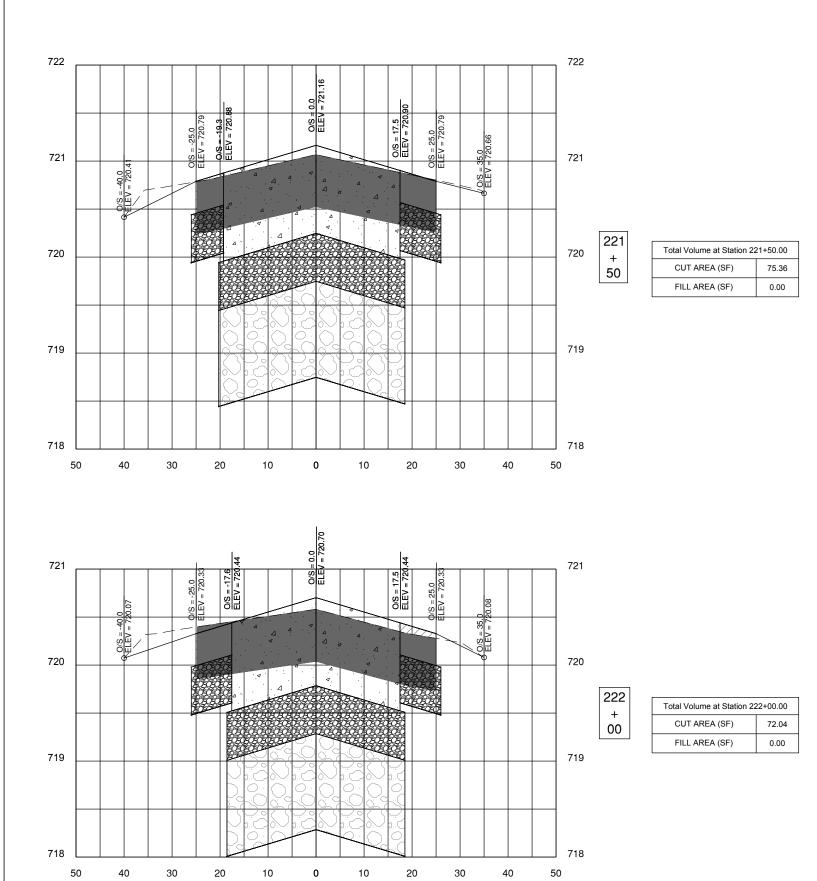


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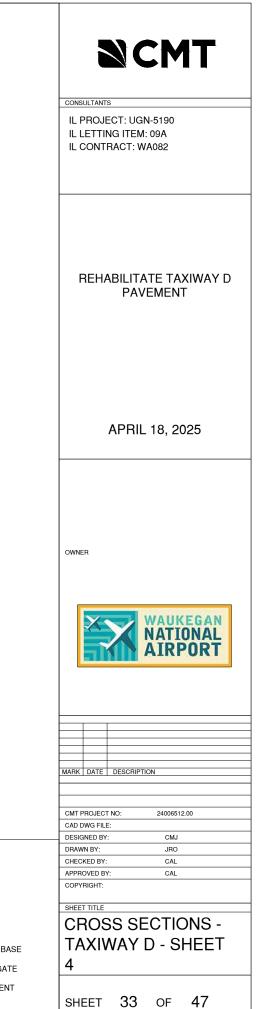
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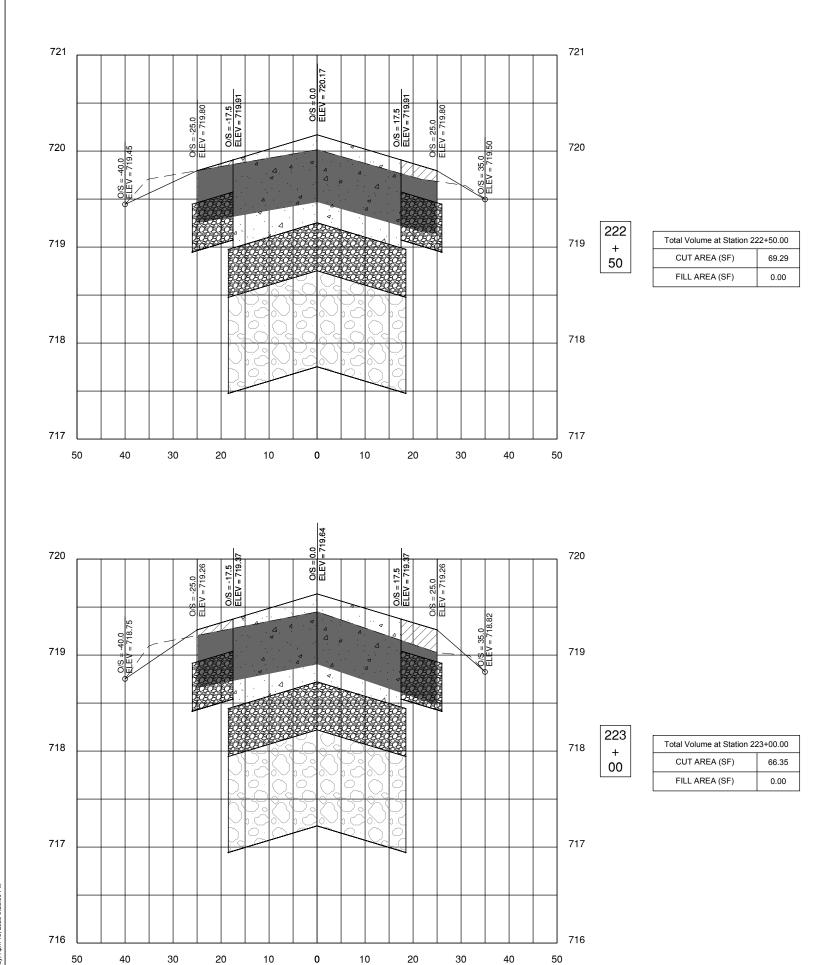
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE
5.93.93	NEW 208 PGE AGGREG
	EXISTING HMA PAVEME (TO BE REMOVED)



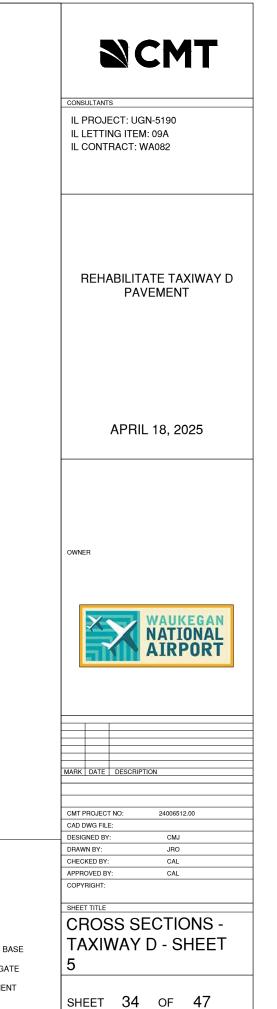




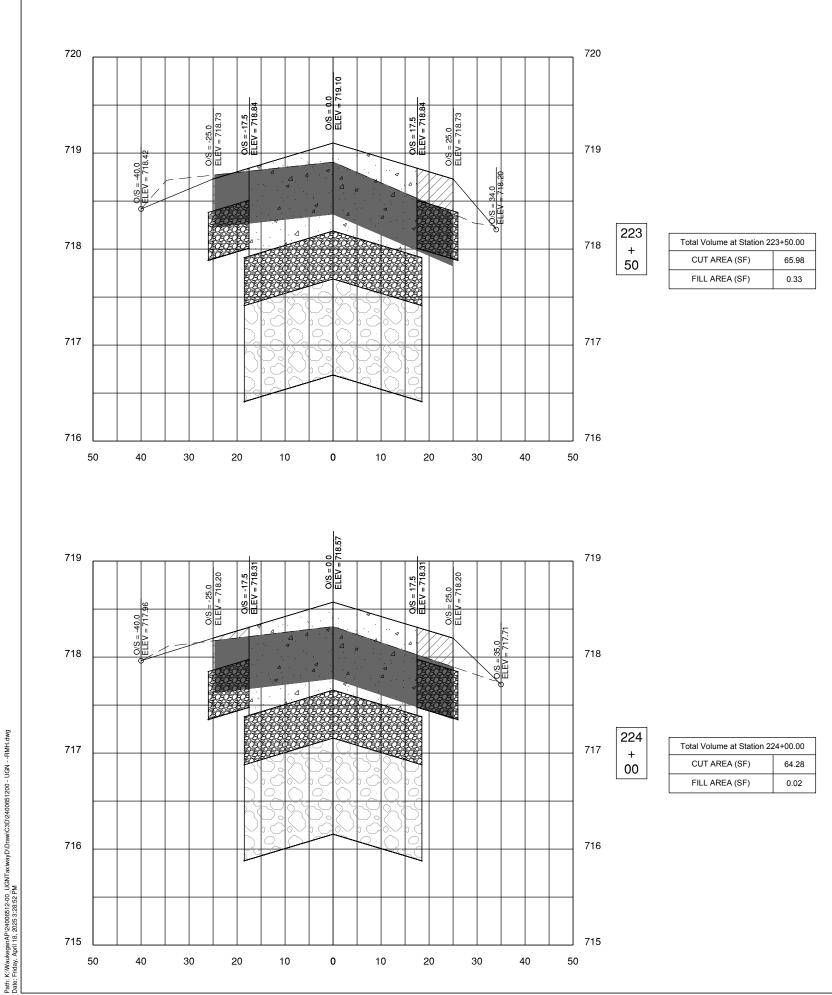
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
a a	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE B
1.60.60.2	NEW 208 PGE AGGREGA
	EXISTING HMA PAVEMEN (TO BE REMOVED)



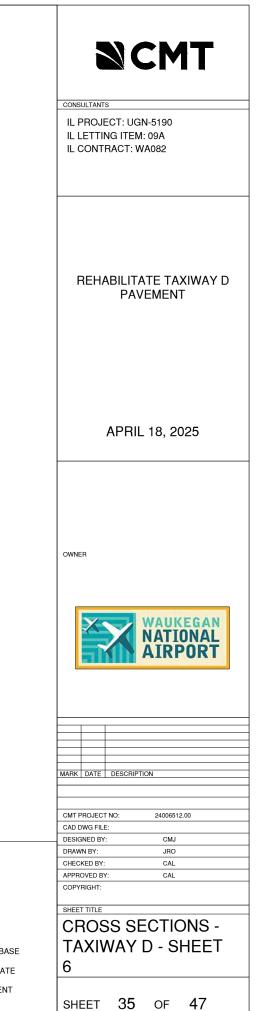
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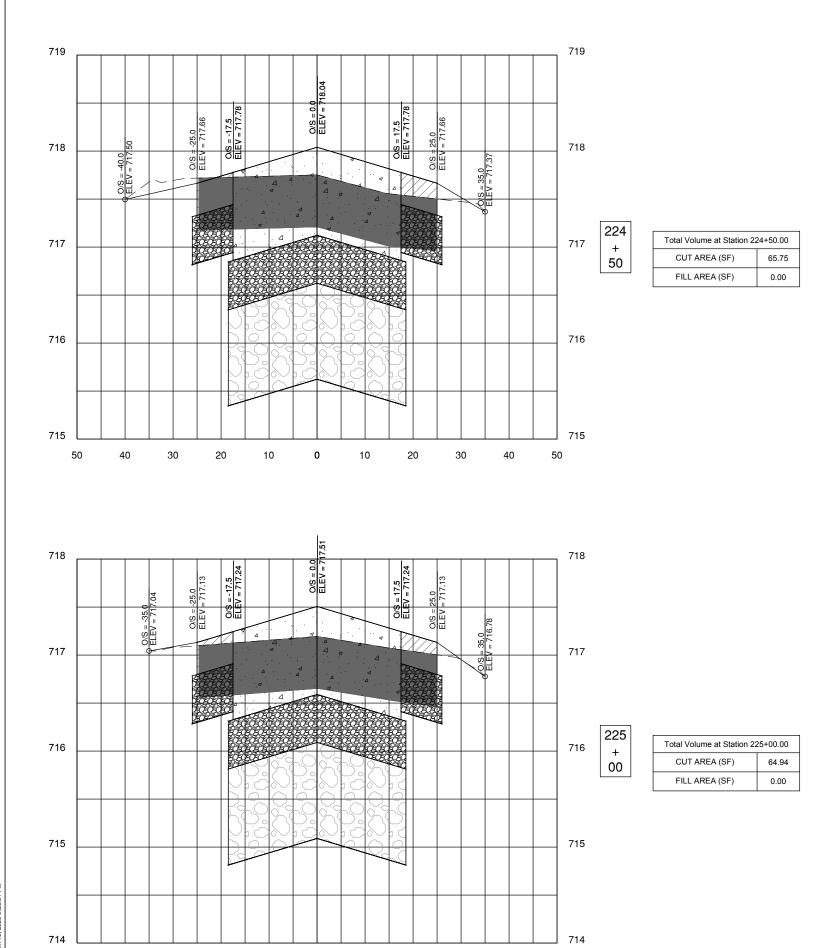
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE E
5.93.93	NEW 208 PGE AGGREG
	EXISTING HMA PAVEME (TO BE REMOVED)



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LEGEND ----- EXISTING GROUND PROPOSED GROUND NEW PCC PAVEMENT NEW HMA PAVEMENT NEW 209 AGGREGATE BASE NEW 208 PGE AGGREGATE EXISTING HMA PAVEMENT (TO BE REMOVED)



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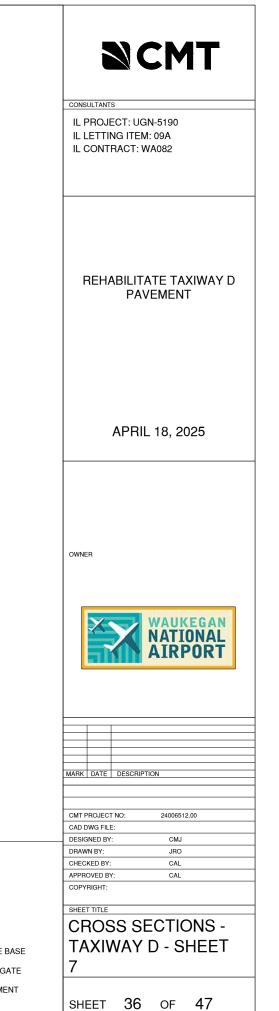
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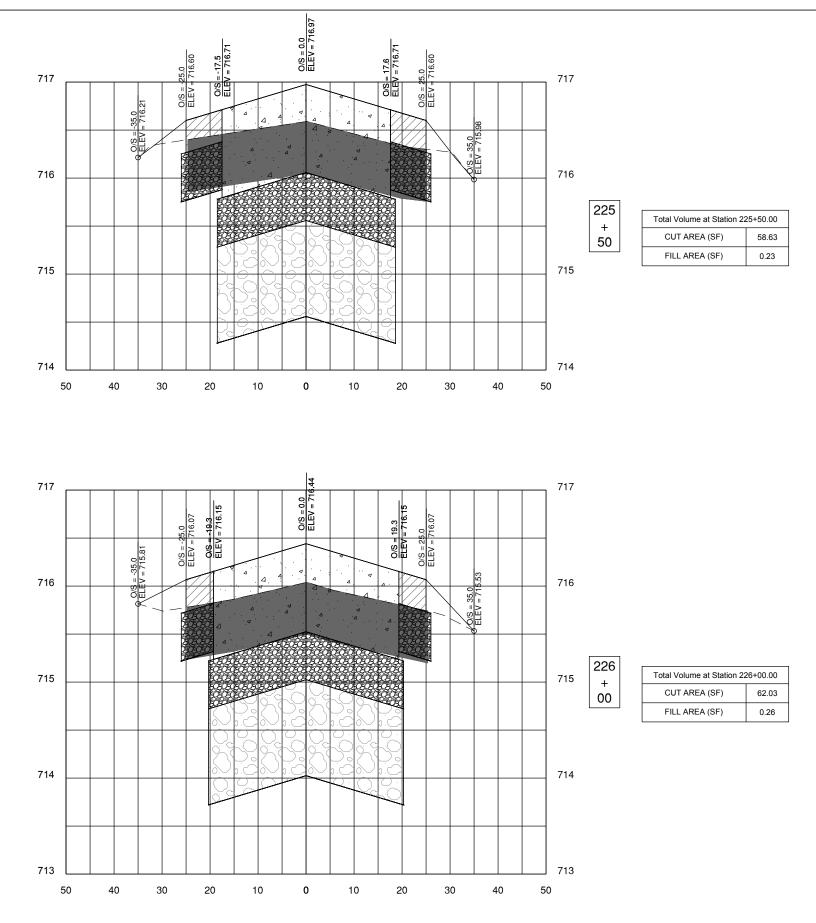
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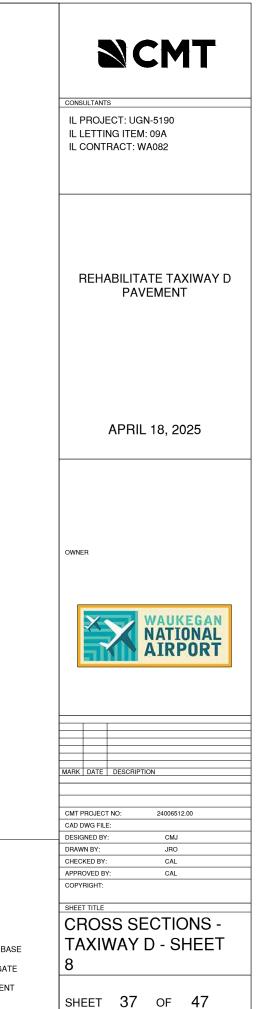
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE
5.93.92	NEW 208 PGE AGGREG
	EXISTING HMA PAVEM



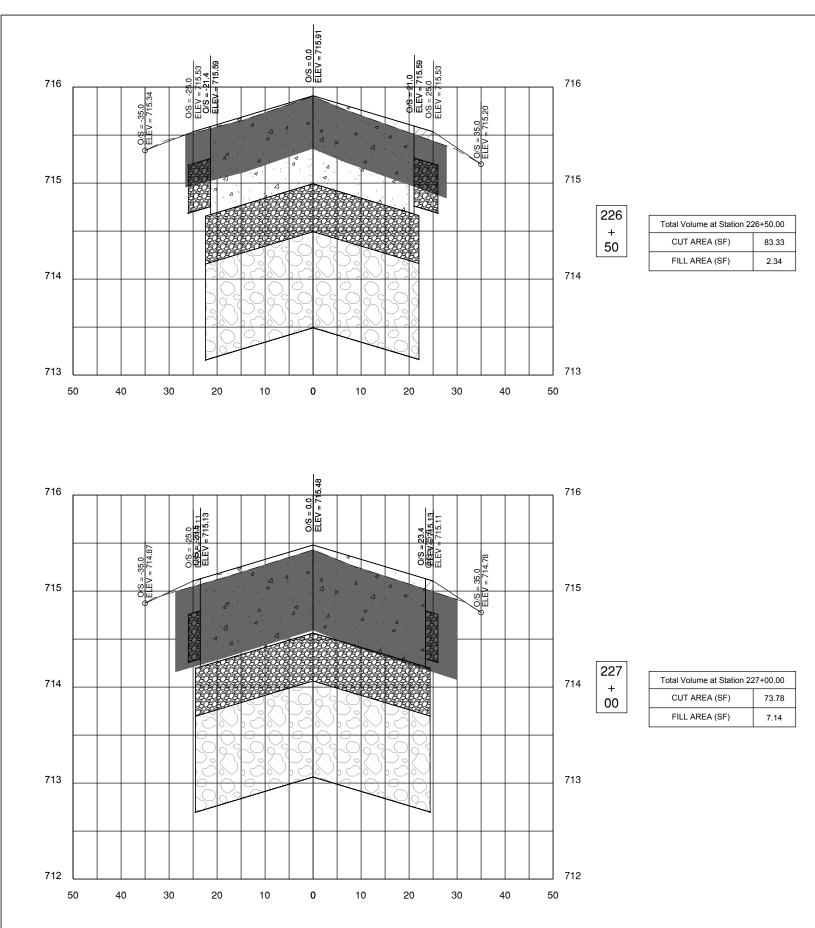
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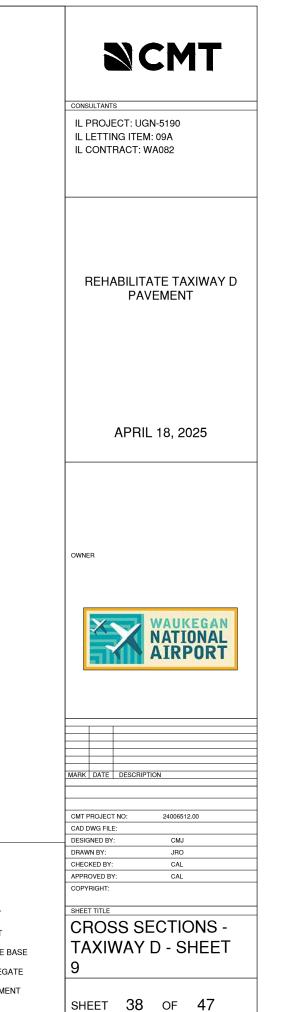
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE B
5:9:30	NEW 208 PGE AGGREGA
	EXISTING HMA PAVEMEN (TO BE REMOVED)



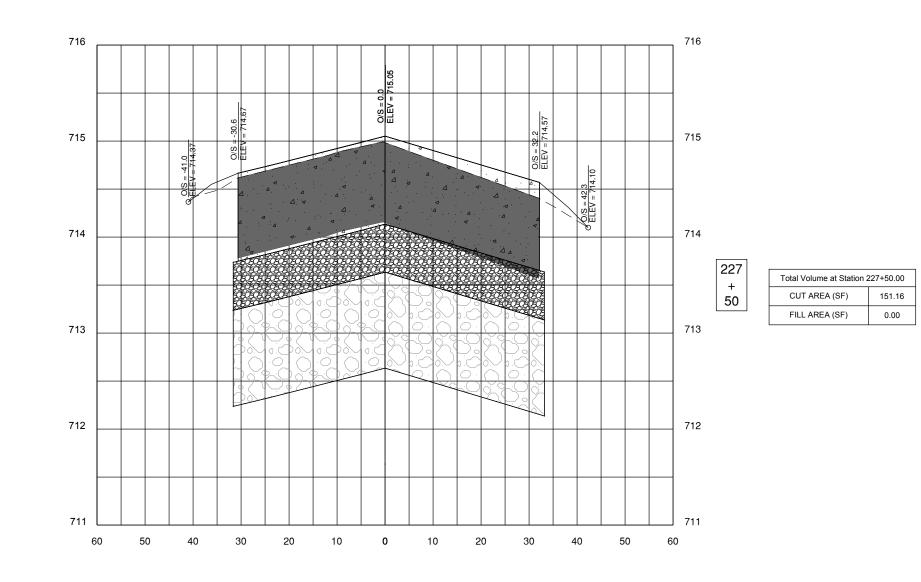
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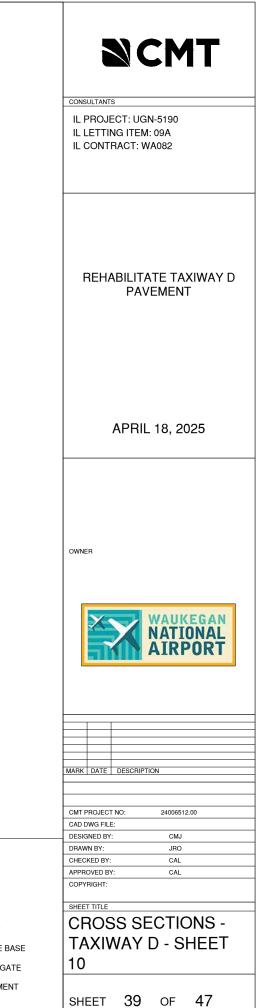
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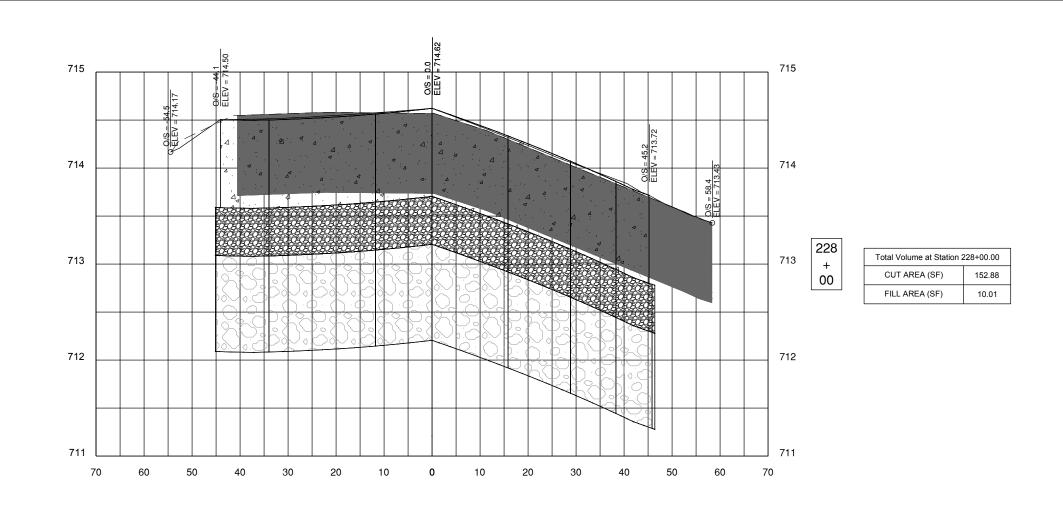


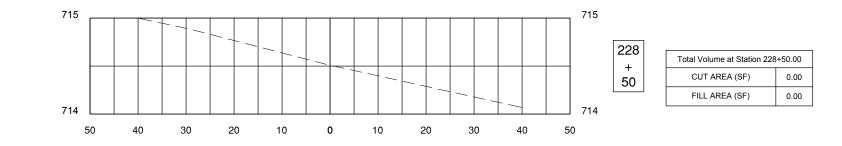
	LEGEND EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE BASE
5.93.93	NEW 208 PGE AGGREGATE
	EXISTING HMA PAVEMENT (TO BE REMOVED)



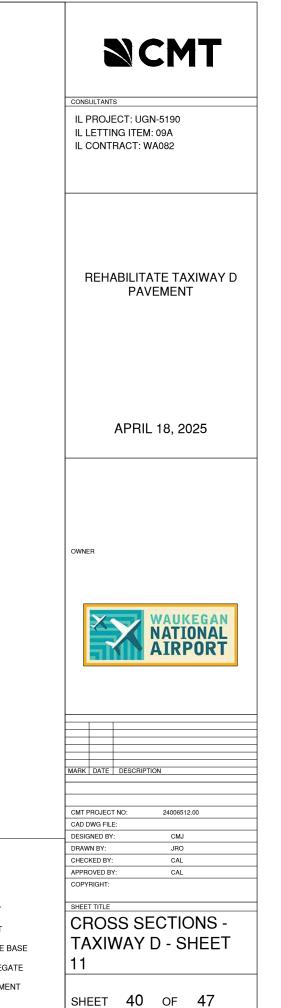
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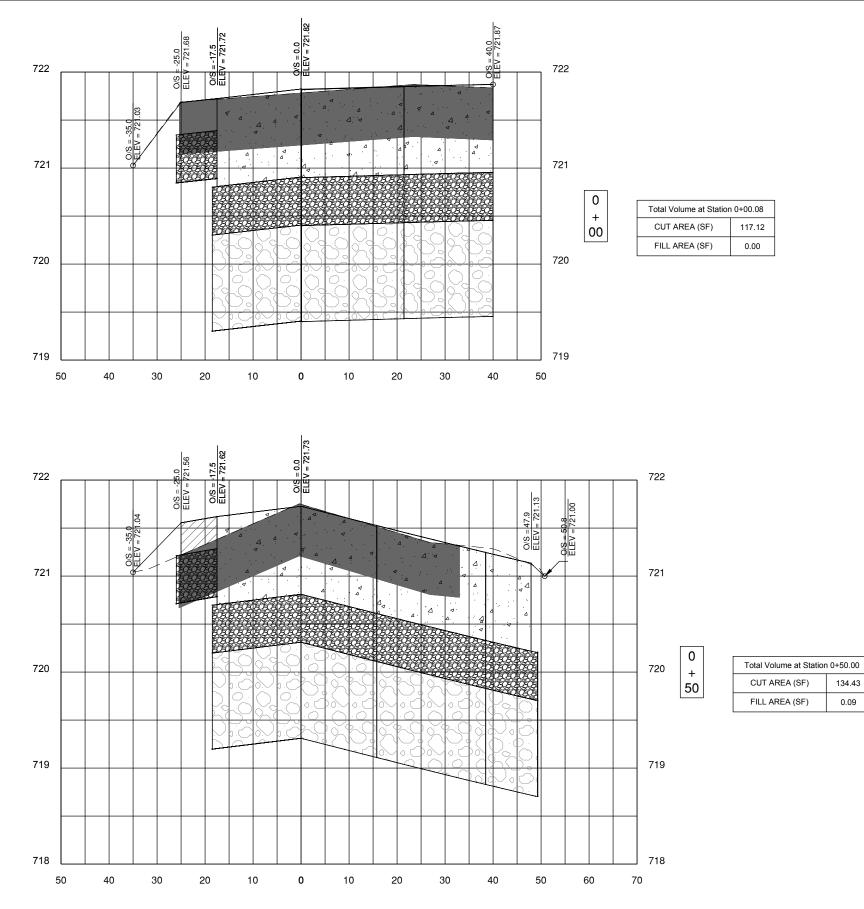




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	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
4 4	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE
1.30.30	NEW 208 PGE AGGREG
	EXISTING HMA PAVEME (TO BE REMOVED)



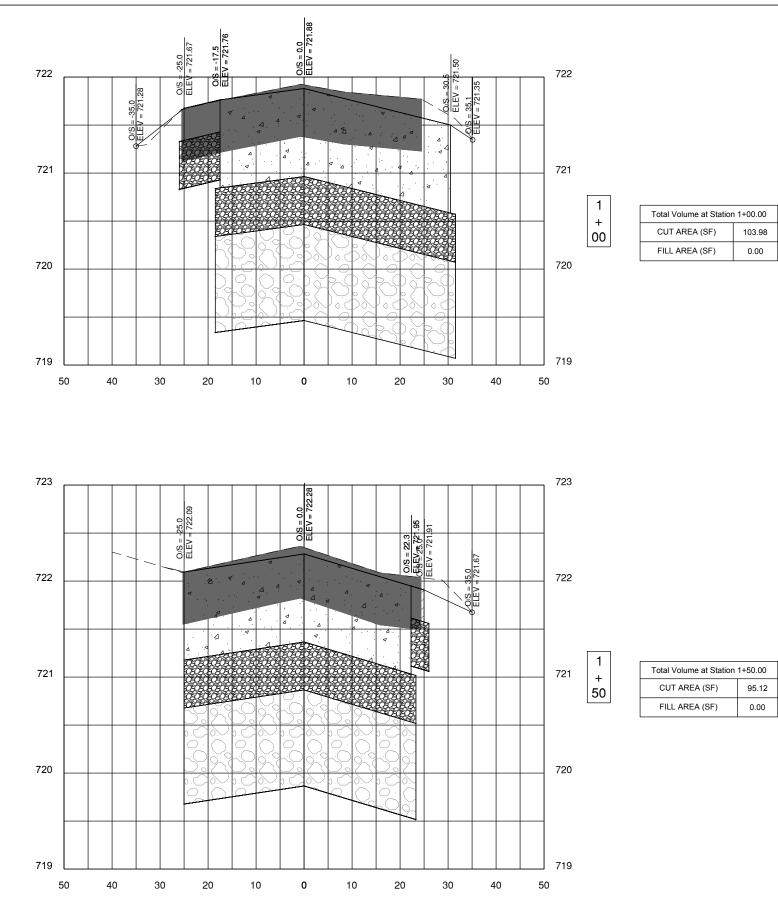


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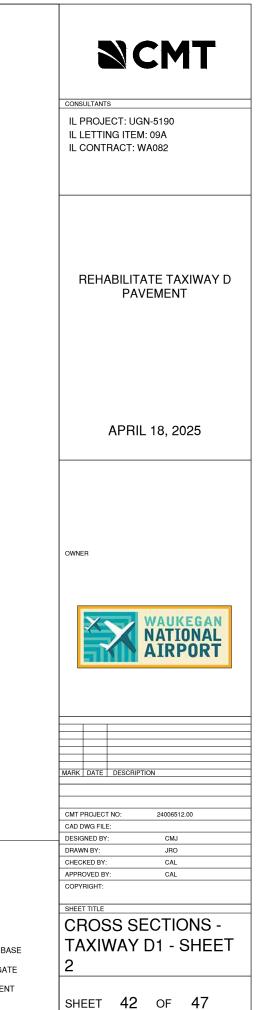
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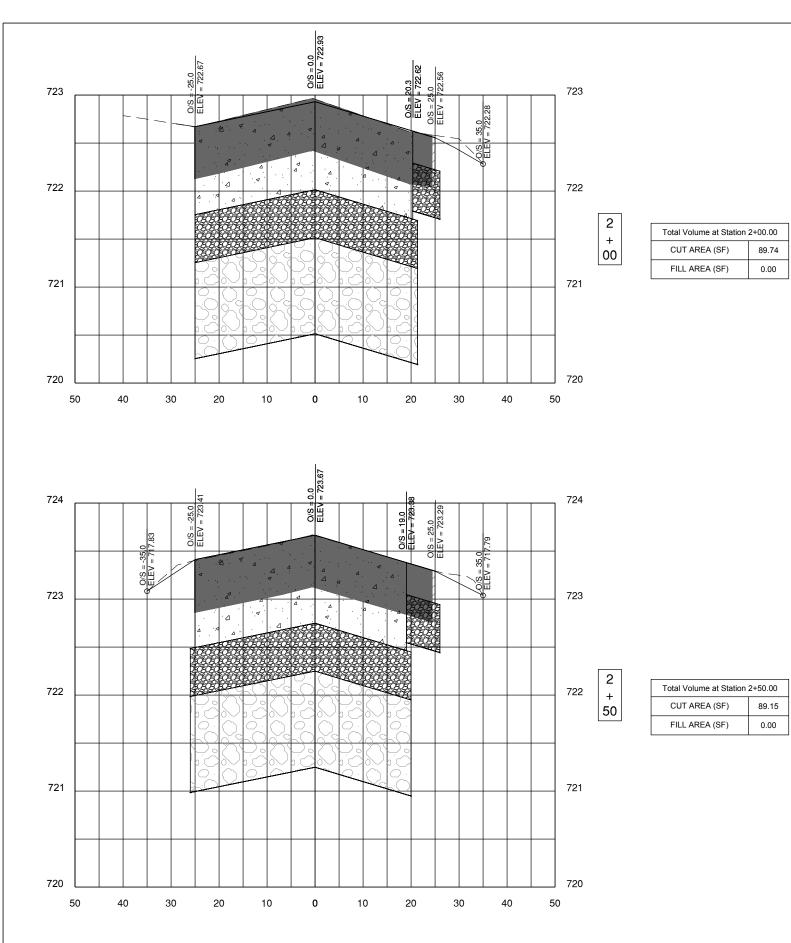
LEGEND ----- EXISTING GROUND PROPOSED GROUND NEW PCC PAVEMENT NEW HMA PAVEMENT NEW 209 AGGREGATE BASE NEW 208 PGE AGGREGATE





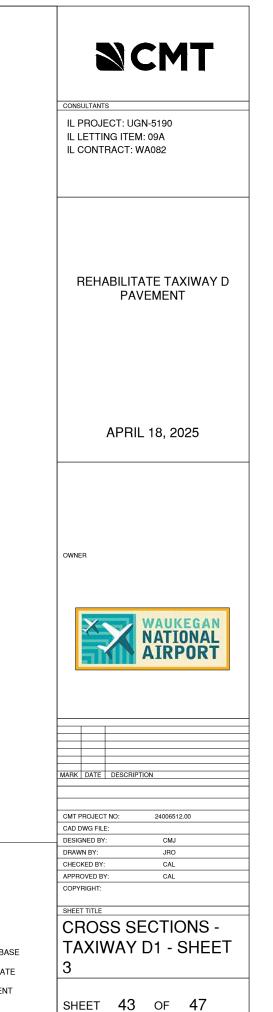


	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE B
1.30.3073	NEW 208 PGE AGGREGA
	EXISTING HMA PAVEMEN (TO BE REMOVED)



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 LEGEND

 EXISTING GROUND

 PROPOSED GROUND

 NEW PCC PAVEMENT

 NEW HMA PAVEMENT

 NEW 209 AGGREGATE BASE

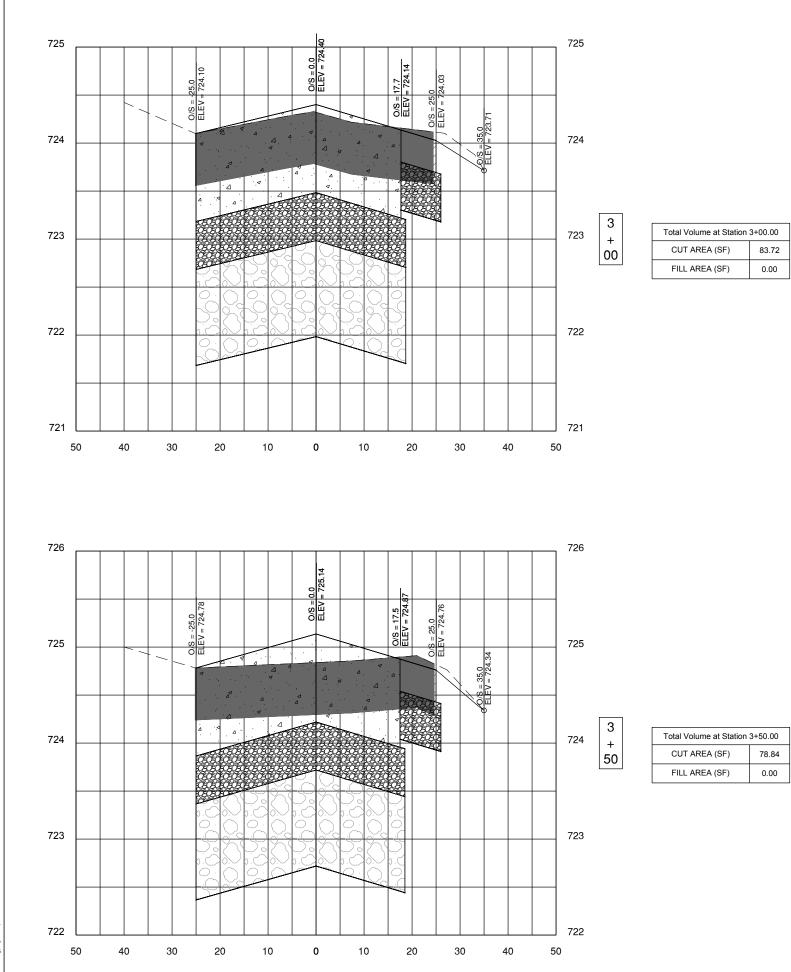
 NEW 208 PGE AGGREGATE

 NEW 208 PGE AGGREGATE

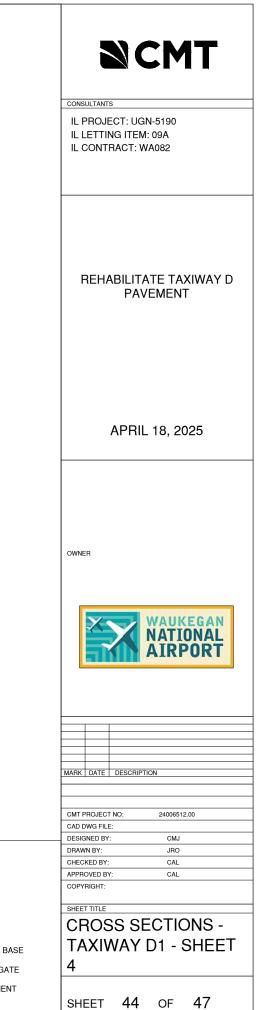
 EXISTING HMA PAVEMENT

 EXISTING HMA PAVEMENT

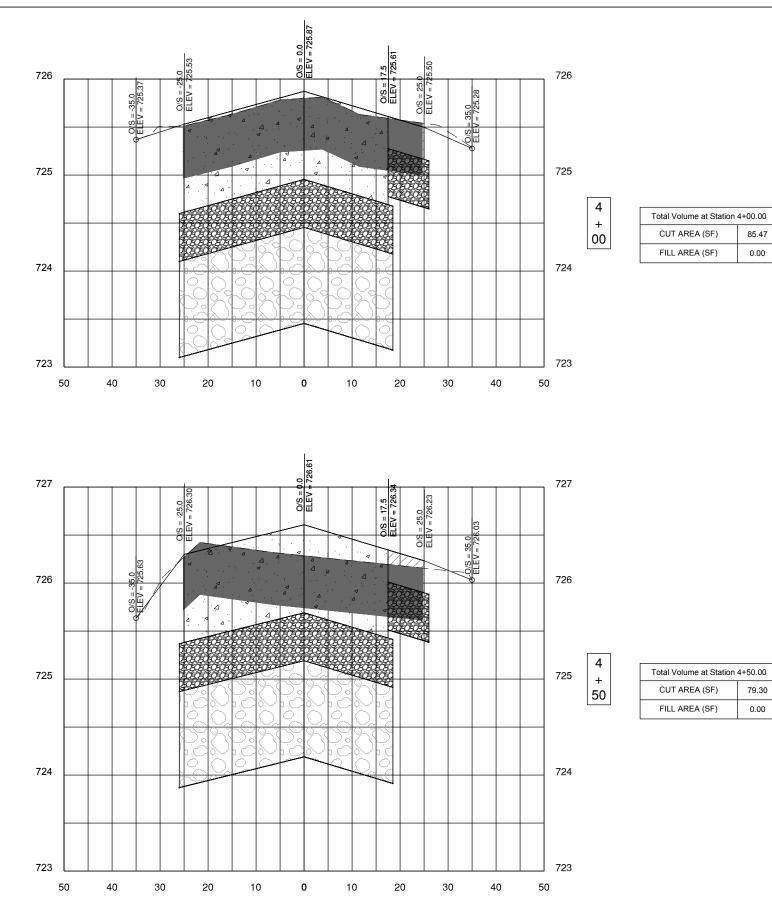
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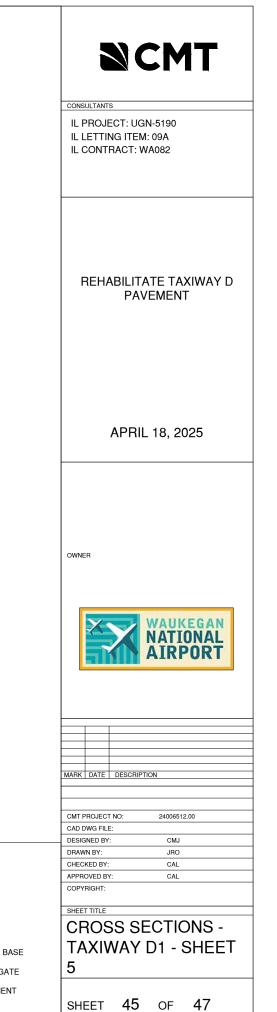
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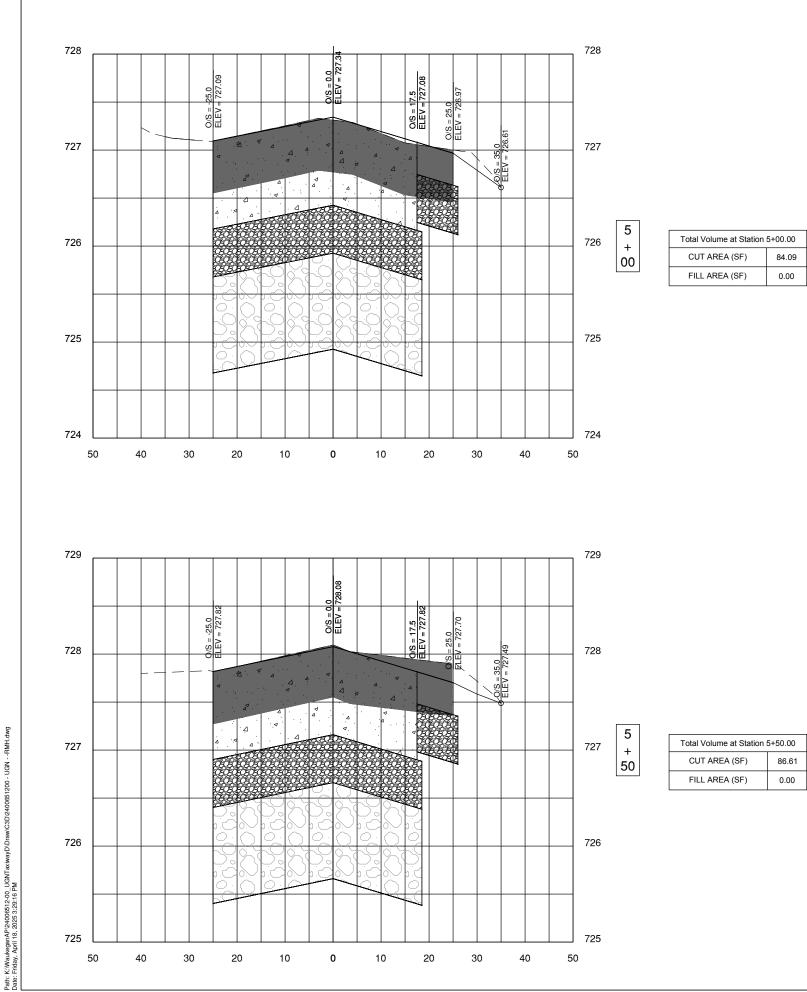
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE E
5.93.93	NEW 208 PGE AGGREG
	EXISTING HMA PAVEME (TO BE REMOVED)



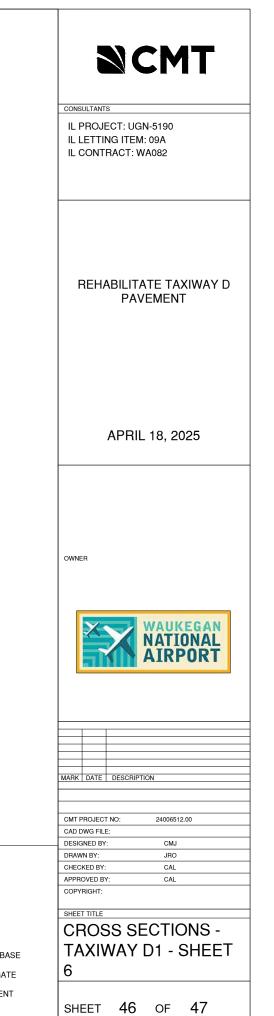




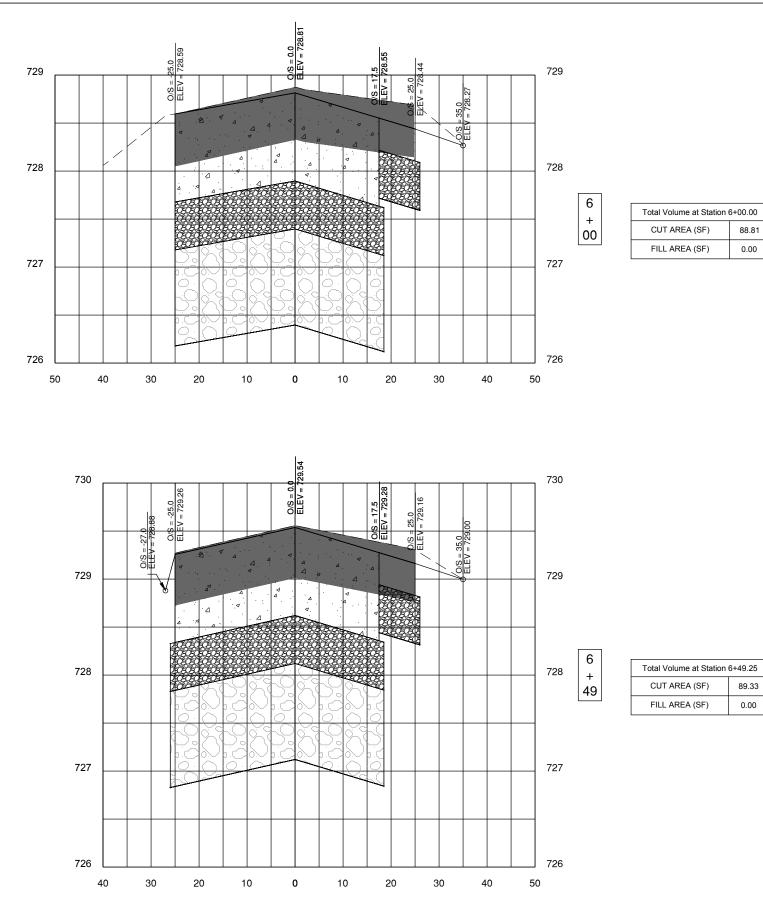
	LEGEND
	EXISTING GROUND
	PROPOSED GROUND
a	NEW PCC PAVEMENT
	NEW HMA PAVEMENT
	NEW 209 AGGREGATE E
5.93.99	NEW 208 PGE AGGREG
	EXISTING HMA PAVEME (TO BE REMOVED)







LEGEND ----- EXISTING GROUND - PROPOSED GROUND NEW PCC PAVEMENT NEW HMA PAVEMENT NEW 209 AGGREGATE BASE NEW 208 PGE AGGREGATE EXISTING HMA PAVEMENT (TO BE REMOVED)

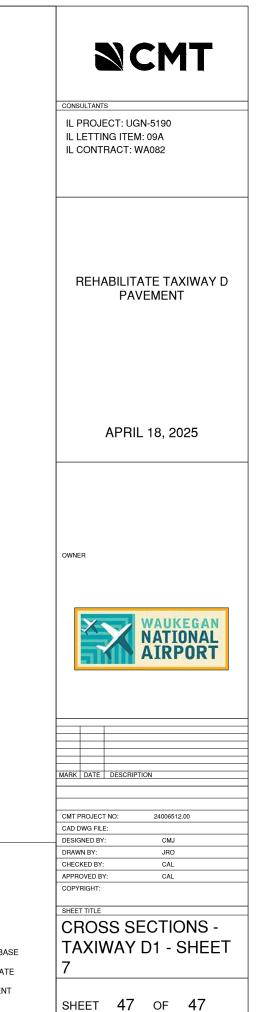


Total Volume at Station	6+49.25
CUT AREA (SF)	89.33
FILL AREA (SF)	0.00

88.81

0.00

2 Mg



LEGEND ----- EXISTING GROUND PROPOSED GROUND NEW PCC PAVEMENT NEW HMA PAVEMENT NEW 209 AGGREGATE BASE NEW 208 PGE AGGREGATE EXISTING HMA PAVEMENT (TO BE REMOVED)