

CONSTRUCTION PLANS - FOR BID

REHABILITATE RUNWAY 12/30 PAVEMENT

DECATUR PARK DISTRICT
DECATUR AIRPORT (DEC)
DECATUR, MACON COUNTY, ILLINOIS

IDA PROJECT NO. DEC-5216
A.I.P. PROJECT NO. 3-17-0033-TBD
APRIL 18, 2025

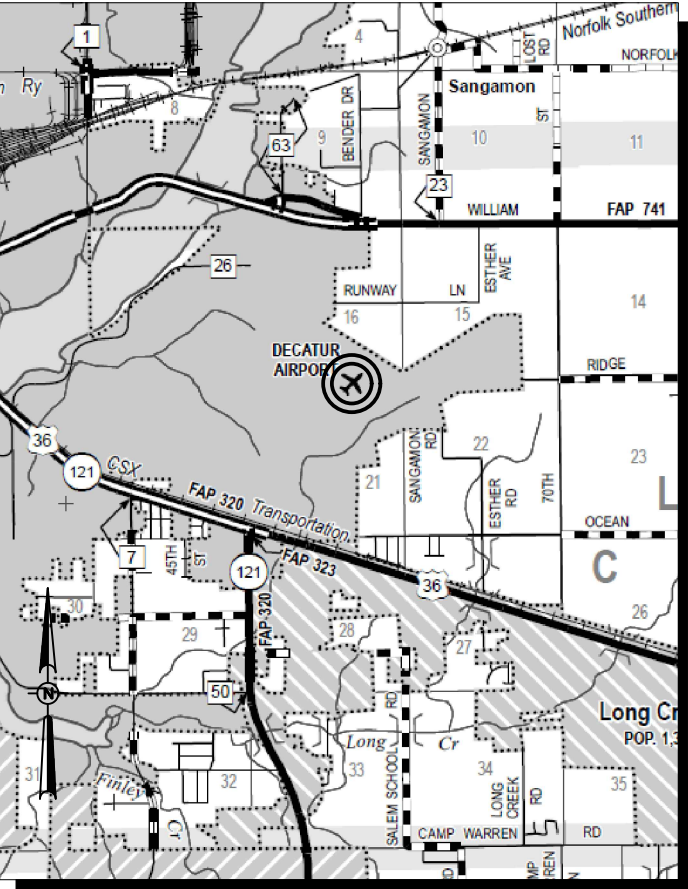
SCOPE OF WORK

THIS PROJECT INVOLVES MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE INNER 100 FEET OF A SECTION OF RUNWAY 12-30. THE BASE BID ALSO INCLUDES BITUMINOUS CRACK SEALING, FULL-DEPTH PATCHING, GROOVING, AND MARKING. OPTIONAL ALTERNATE A COVERS MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE REMAINING WIDTH OF THE SPECIFIED RUNWAY SECTION, WHILE OPTIONAL ALTERNATE B INVOLVES SEAL COATING THE REMAINING WIDTH.

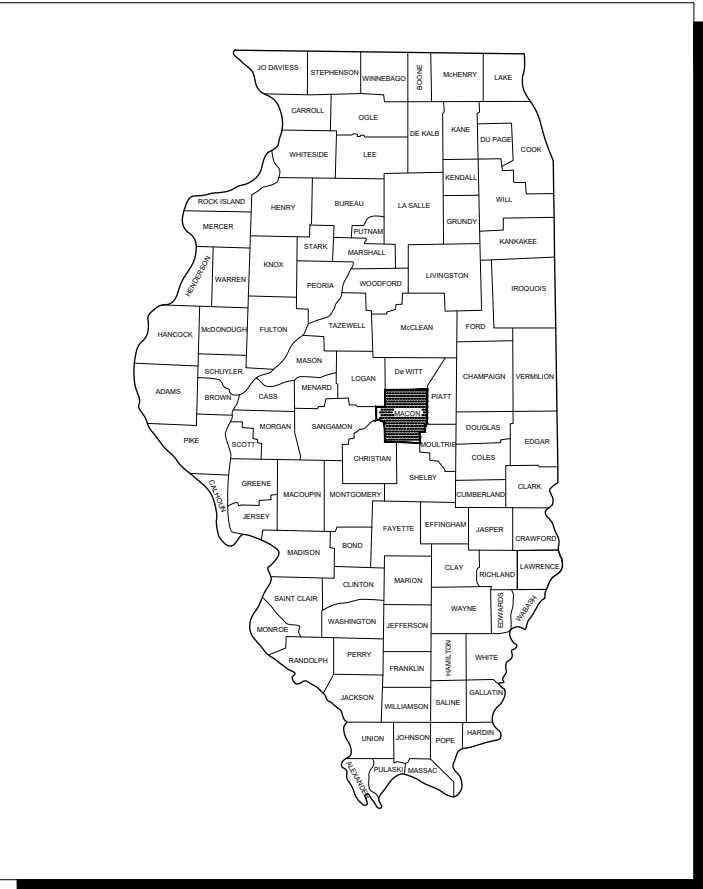
NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

VICINITY MAP



LOCATION MAP



No.	Issue/Description	Sheets Changed	Date	By



HANSON PROFESSIONAL SERVICES INC.
1525 South Sixth Street
Springfield, Illinois 62703-2886
Telephone: 217.788.2450
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Michael Dudas
Michael Dudas, P.E.
Project Engineer

Lic. Exp. 11/30/2025

04/18/2025

Date



DECATUR PARK DISTRICT
DECATUR AIRPORT
910 South Airport Road
Decatur, Illinois 62521
Telephone: 217.428.2423

Tim Wright
Tim Wright
Airport Director

04/18/2025

Date

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SUMMARY OF QUANTITIES - BASE BID				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AW150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AW150520	MOBILIZATION	L SUM	1	
AW201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	10,000	
AW401010	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	L SUM	1	
AW401614	BIT. SURF. CSE.-METHOD II, SUPERPAVE	TON	8,306	
AW401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AW401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	36,112	
AW401650	BITUMINOUS PAVEMENT MILLING	SQ YD	36,112	
AW401916	REM & REP BIT PAVEMENT-TYPE B	SQ YD	18	
AW603510	BITUMINOUS TACK COAT	GALLON	6,139	
AW620520	PAVEMENT MARKING-WATERBORNE	SQ FT	25,788	
AW620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	5,572	
AW620590	TEMPORARY MARKING	SQ FT	25,788	

SUMMARY OF QUANTITIES - OPTIONAL ALTERNATE A				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AX401614	BIT. SURF. CSE.-METHOD II, SUPERPAVE	TON	4,153	
AX401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	10,834	
AX401650	BITUMINOUS PAVEMENT MILLING	SQ YD	18,056	
AX603510	BITUMINOUS TACK COAT	GALLON	3,070	

SUMMARY OF QUANTITIES - OPTIONAL ALTERNATE B				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AY401915	REM & REP BIT PAVEMENT-TYPE A	SQ YD	67	
AY625510	TAR EMULSION SEAL COAT	SQ YD	18,056	

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES AND AGENCIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL STOP WORK AND IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

INDEX OF SHEETS	
SHEET NO.	SHEET TITLE
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	ALIGNMENT DATA SHEET
4	CONSTRUCTION SAFETY AND PHASING PLAN
5	CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 1
6	CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 2
7	CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 3
8	PAVEMENT PATCHING DETAILS
9	TYPICAL SECTION - RUNWAY 12-30
10	PAVEMENT PREPARATION PLAN - SHEET 1
11	PAVEMENT PREPARATION PLAN - SHEET 2
12	PAVEMENT PREPARATION PLAN - SHEET 3
13	PROPOSED PLAN AND PROFILE - SHEET 1
14	PROPOSED PLAN AND PROFILE - SHEET 2
15	PROPOSED PLAN AND PROFILE - SHEET 3
16	PROPOSED MARKING AND GROOVING PLAN - SHEET 1
17	PROPOSED MARKING AND GROOVING PLAN - SHEET 2
18	PROPOSED MARKING AND GROOVING PLAN - SHEET 3

GENERAL NOTES

- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE, AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.
- CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS. STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS ARE TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT - THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
- MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED, AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- CERTIFIED PAYROLLS - THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.
- STANDARDS AND CODES - STANDARDS AND CODES REFERENCED HERIN SHALL BE UNDERSTOOD TO BE REFERRING TO THE CURRENT EDITION.

J.U.L.I.E. INFORMATION

COUNTY: _____ MACON
CITY: _____ DECATUR
TOWNSHIP: _____ LONG CREEK
SECTION NO.: _____ 15, 16, 21, AND 22
ADDRESS: _____ DECATUR AIRPORT
910 SOUTH AIRPORT ROAD
DECATUR, IL 62524



Offices Nationwide
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1525 S. 6th Street
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Illinois Licensed
Professional Service Corporation
#184-001084



Decatur Park District
Decatur Airport
910 South Airport Road
Decatur, IL 62521



Michael J. Dudas

DATE LICENSE
SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216
FAA #: 3-17-0033-TBD

NO.	DATE	DESCRIPTION	
		DES	DWN REV

ISSUE: JUNE 13, 2025

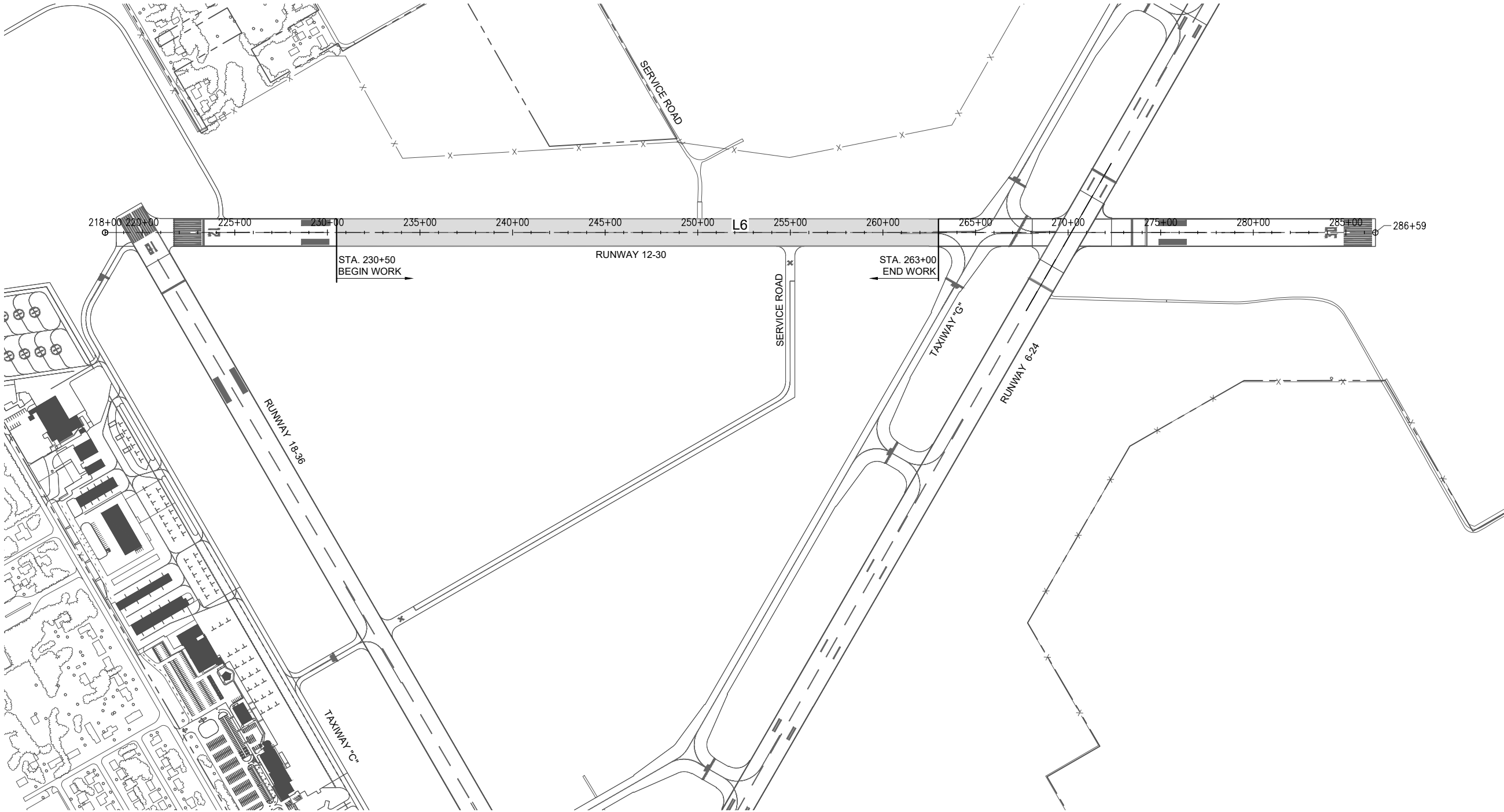
PROJECT NO: 20A0079
CAD FILE: G-002-FLP.DWG
DESIGN BY: HLE 12/30/2024
DRAWN BY: HLE 12/30/2024
REVIEWED BY: MJD 1/28/2025

SHEET TITLE

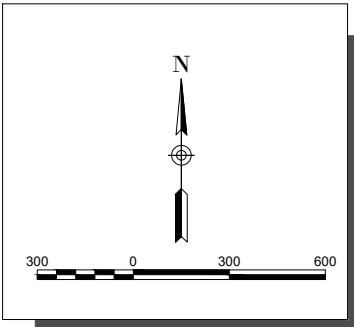
SUMMARY OF
QUANTITIES AND
INDEX TO SHEETS

FOR BID

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ALIGNMENT DATA RUNWAY 12-30						
LABEL	START STATION	END STATION	LENGTH	AZIMUTH	START (N,E)	END (N,E)
L6	218+00.00	286+59.01	6859.01	119° 28' 19"	1156571.38, 832151.34	1153196.76, 838122.77



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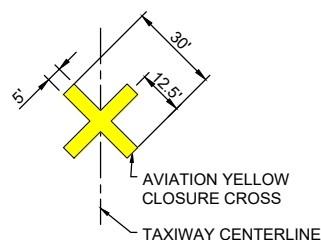
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DESIGN BY: HLE 1/30/2025
DRAWN BY: HLE 1/30/2025
REVIEWED BY: MJD 1/30/2025

SHEET TITLE

ALIGNMENT DATA
SHEET



1. THE AIRPORT HAS TWO LIGHTED RUNWAY CLOSURE MARKERS AVAILABLE FOR USE ON THIS PROJECT. THE COST OF PLACING, OPERATING, MAINTAINING, AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL RETURN THE AIRPORT-OWNED LIGHTED RUNWAY CLOSURE MARKERS IN THE SAME OR BETTER CONDITION THAN AT THE START OF CONSTRUCTION.
2. THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE LIGHTED CROSSES AND MAKE PROMPT REPAIRS AS NECESSARY.
3. THE CONTRACTOR SHALL BE ON-CALL FOR 24-HOUR EMERGENCY MAINTENANCE WHEN LIGHTED CROSSES ARE BEING USED.
4. THE LIGHTED MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERALS AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER.
5. LIGHTED MARKERS SHALL BE SECURED FROM WIND EFFECTS BY THE CONTRACTOR AS RECOMMENDED BY THE MANUFACTURER.
6. THE LIGHTED MARKERS SHALL BE IN PLACE AND OPERATING WHENEVER THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED.



12" ALT. STRIPES
REFLECTIVE
ORANGE AND
WHITE

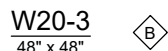
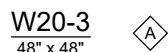
SOLAR OR BATTERY
POWERED FLASHING
LIGHT WITH RED LENS
(TYP. BOTH ENDS OF
BARRICADE).

10"

8'-0"

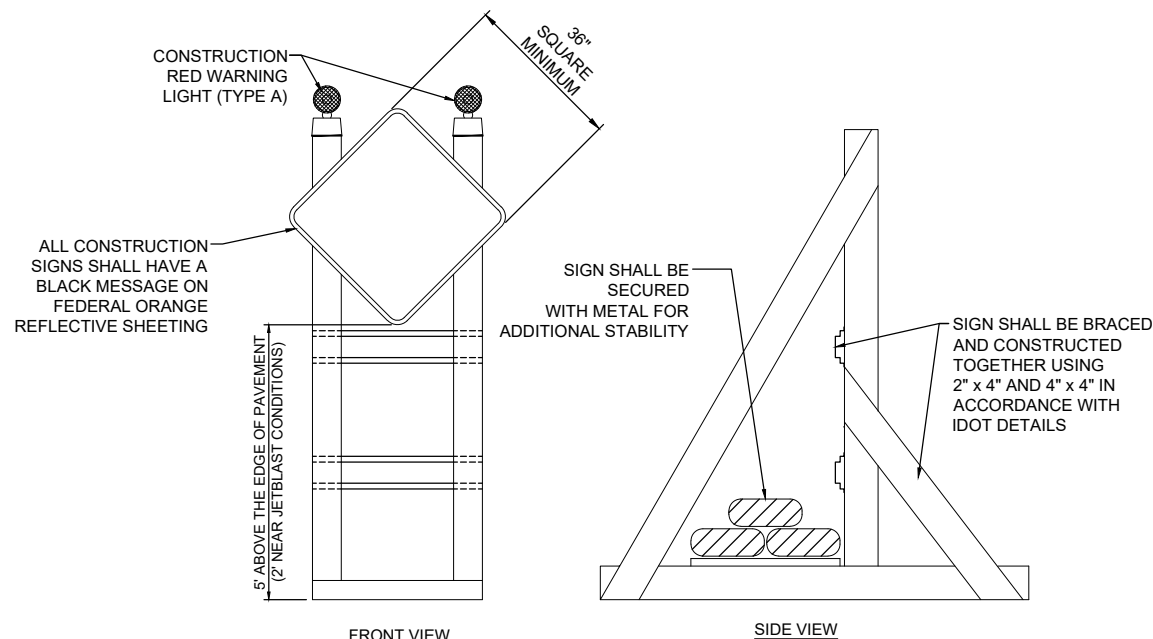
HIGH IMPACT, UV RESISTANT
POLYETHYLENE 10" X 96" X 10"
ORANGE AND/OR WHITE IN COLOR

DETAIL ABOVE REPRESENTS ONE OPTION FOR LOW-PROFILE
BARRICADES. OTHER OPTIONS MAY BE UTILIZED AS LONG AS
THEY MEET THE REQUIREMENTS OF THE PROJECT, INCLUDING
BARRICADE NOTE 1.



BARRICADE NOTES

1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
2. BARRICADES SHALL BE "LOW-PROFILE" WITH A MAXIMUM HEIGHT OF 18" ABOVE GROUND, EXCLUSIVE OF ASSOCIATED WARNING LIGHTS AND FLAGS.
3. BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT, WITH GAPS BETWEEN BARRICADES NOT TO EXCEED 4' WIDE. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
4. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
5. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
6. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
7. THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
8. COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE PAID FOR UNDER ITEM AR150530 - TRAFFIC MAINTENANCE.



1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
2. UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS.
3. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS, THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A).
4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
5. COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.

Decatur Park District
Decatur Airport
910 South Airport Road
Decatur, IL 62521



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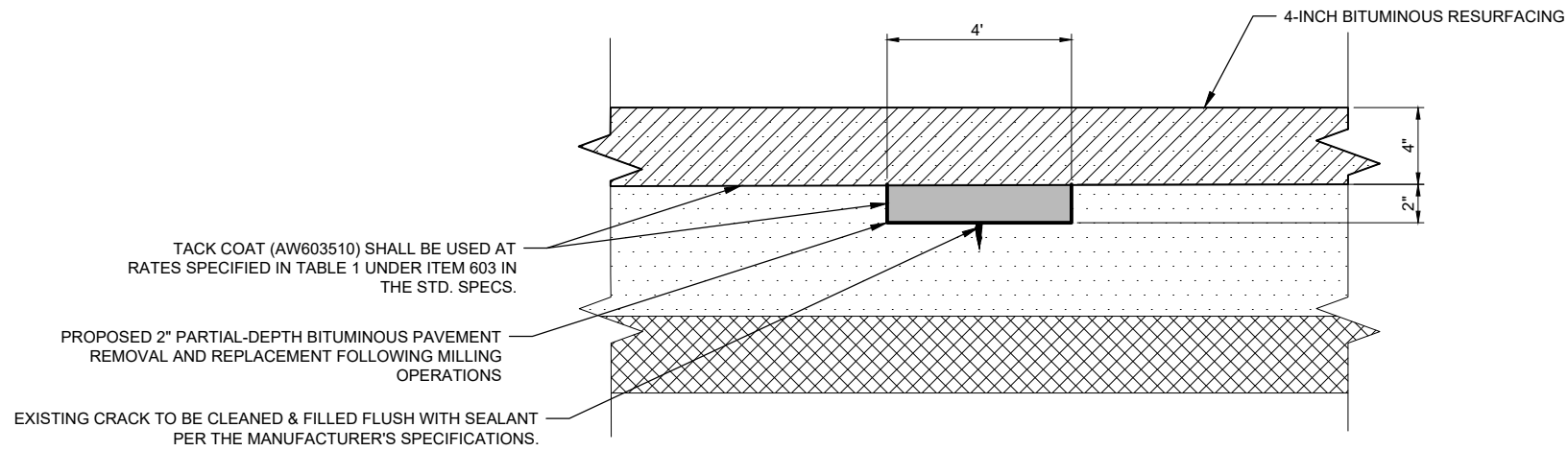
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DESIGN BY: HLE 1/2/2025
DRAWN BY: HLE 1/2/2025
REVIEWED BY: MJD 1/28/2025

SHEET TITLE

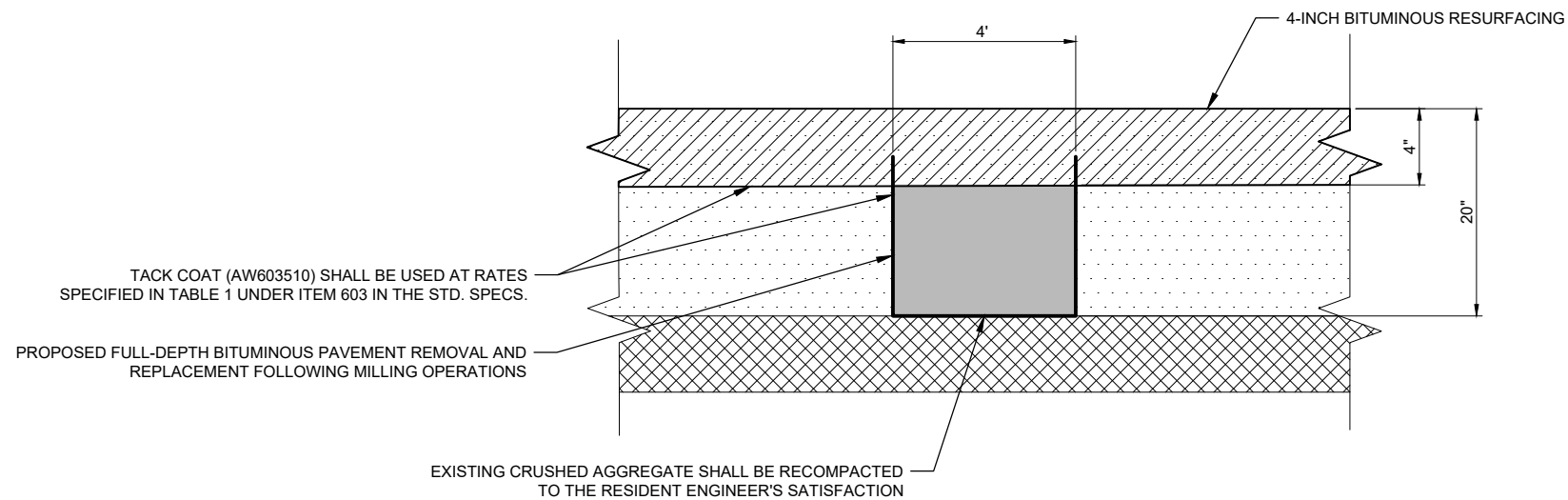
CONSTRUCTION
SAFETY DETAILS
AND NOTES - SHEET
2

FOR BID

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AY401915 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE A (PARTIAL DEPTH)
NOT TO SCALE



AW401916 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE B (FULL DEPTH)
NOT TO SCALE

EXISTING	PROPOSED	LEGEND
		BIT. PAVEMENT
		BIT. PAVEMENT MILLING (VARIABLE)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE



Decatur Park District
Decatur Airport
910 South Airport Road
Decatur, IL 62521



Michael J. Dudas

DATE: 04/18/2025
SIGNED: 04/18/2025
LICENSE: 11/30/2025
EXPIRES: 11/30/2025

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SHEET TITLE

PAVEMENT
PATCHING DETAILS

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BITUMINOUS PAVEMENT MILLING
(AX401650) AND BITUMINOUS
OVERLAY (AX401614) - 4-INCH DEPTH

MATCH
EXISTING
(TYP.)

MATCH EXISTING
1% AND VARIES

MATCH EXISTING
1% AND VARIES

MATCH
EXISTING
(TYP.)

GRADING, SEEDING,
AND MULCHING
INCIDENTAL.

TACK COAT (AW603510) SHALL BE
USED AT RATES SPECIFIED IN
TABLE 1 UNDER ITEM 603 IN THE
STD. SPECS.

BITUMINOUS PAVEMENT MILLING
(AW401650) AND BITUMINOUS
OVERLAY (AW401614) - 4-INCH DEPTH

75' 75'
BASE BID - INNER 100'

EXIST.
12"
BIT.

EXIST.
8"
PCC

TYPICAL SECTION A-A - RUNWAY 12-30 - ALT. A

NOT TO SCALE

EMULSIFIED ASPHALT SEAL
COAT (AY608710)

MATCH
EXISTING
(TYP.)

MATCH EXISTING
1% AND VARIES

MATCH EXISTING
1% AND VARIES

TACK COAT (AW603510) SHALL BE
USED AT RATES SPECIFIED IN
TABLE 1 UNDER ITEM 603 IN THE
STD. SPECS.

BITUMINOUS PAVEMENT MILLING
(AW401650) AND BITUMINOUS
OVERLAY (AW401614) - 4-INCH DEPTH

75' 75'
BASE BID - INNER 100'

EXIST.
12"
BIT.

EXIST.
8"
PCC

TYPICAL SECTION A-A - RUNWAY 12-30 - ALT. B

NOT TO SCALE

EXISTING	PROPOSED	LEGEND
		BIT. SURF. CSE.-METHOD I, SUPERPAVE (4-INCH)
		BIT. PAVEMENT MILLING (4-INCH)
		EMULSIFIED ASPHALT SEAL COAT
		BIT. PAVEMENT (APPROX. 12-INCH)
		PCC PAVEMENT (8-INCH)

NOTE: REFERENCE PAVEMENT PREPARATION PLAN -
SHEET 1 FOR LOCATION OF TYPICAL SECTION A-A.

FOR BID



Offices Nationwide
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Illinois Licensed
Professional Service Corporation
#184-001084



Decatur Park District
Decatur Airport
910 South Airport Road
Decatur, IL 62521



Michael J. Dudas

DATE: 04/18/2025
SIGNED: 04/18/2025
LICENSE: 062-069152
EXPIRES: 11/30/2025

IDA #: DEC-5216
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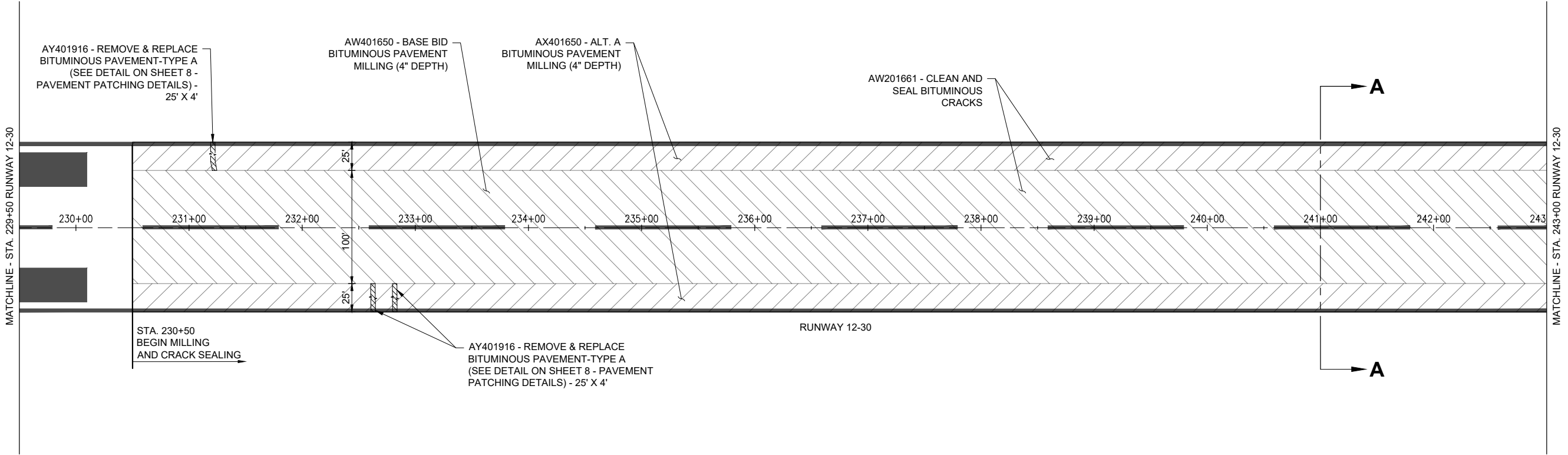
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 13, 2025
PROJECT NO: 20A0079
CAD FILE: C-301-TYP.DWG
DESIGN BY: HLE 1/3/2025
DRAWN BY: HLE 1/3/2025
REVIEWED BY: MJD 1/28/2025

SHEET TITLE

TYPICAL SECTION -
RUNWAY 12-30

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NOTES

CLEAN & SEAL BITUMINOUS CRACKS

1. A NOMINAL QUANTITY OF AW201661 "CLEAN AND SEAL BITUMINOUS CRACKS" HAS BEEN INCLUDED AND WILL BE LAID OUT BY THE RESIDENT ENGINEER/TECHNICIAN FOLLOWING PAVEMENT MILLING. THE NOMINAL QUANTITY WAS DETERMINED BY A FIELD SURVEY (CONDUCTED ON 01/04/21) AND ASSUMES THAT 50% OF SURVEYED CRACKS WILL BE ELIMINATED AFTER THE COMPLETION OF MILLING. IN ADDITION, THE NOMINAL QUANTITY ASSUMES SURVEYED AREAS REQUIRING A TYPE A, PARTIAL DEPTH REPAIR (AW401915) WILL BECOME CRACKS UNDER AW201661.

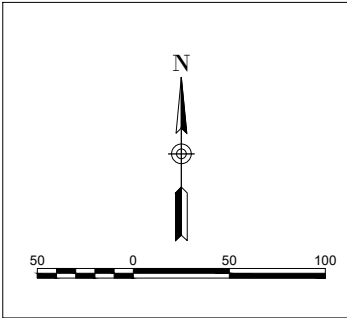
BITUMINOUS PAVEMENT MILLING

2. CONTRACTOR SHALL PROTECT ALL ADJACENT PAVEMENT EDGES DURING MILLING AND PAVING OPERATIONS. ANY DAMAGE DONE TO EXISTING PAVEMENT OR MARKINGS SHALL BE REPAIRED OR REMARKED AT THE CONTRACTOR'S EXPENSE.
3. ALL MILLED MATERIAL SHALL BE HAULED OFFSITE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.

ELECTRICAL NOTES

1. THE CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. THE CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
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		AS401650 - BITUMINOUS PAVEMENT MILLING (4-INCH) - ALT. A
		AIRPORT PROPERTY LINE
		AIRPORT FENCE (AIRFIELD)
		UNDERGROUND ELECTRIC LINE
		UNDERDRAIN
		MANHOLE
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Decatur Park District
Decatur Airport
910 South Airport Road
Decatur, IL 62521



Michael J. Dudas

DATE: 04/18/2025
SIGNED: 04/18/2025
LICENSE: 062-069152
EXPIRES: 11/30/2025

IDA #: DEC-5216
FAA #: 3-17-0033-TBD

NO.	DATE	DESCRIPTION		
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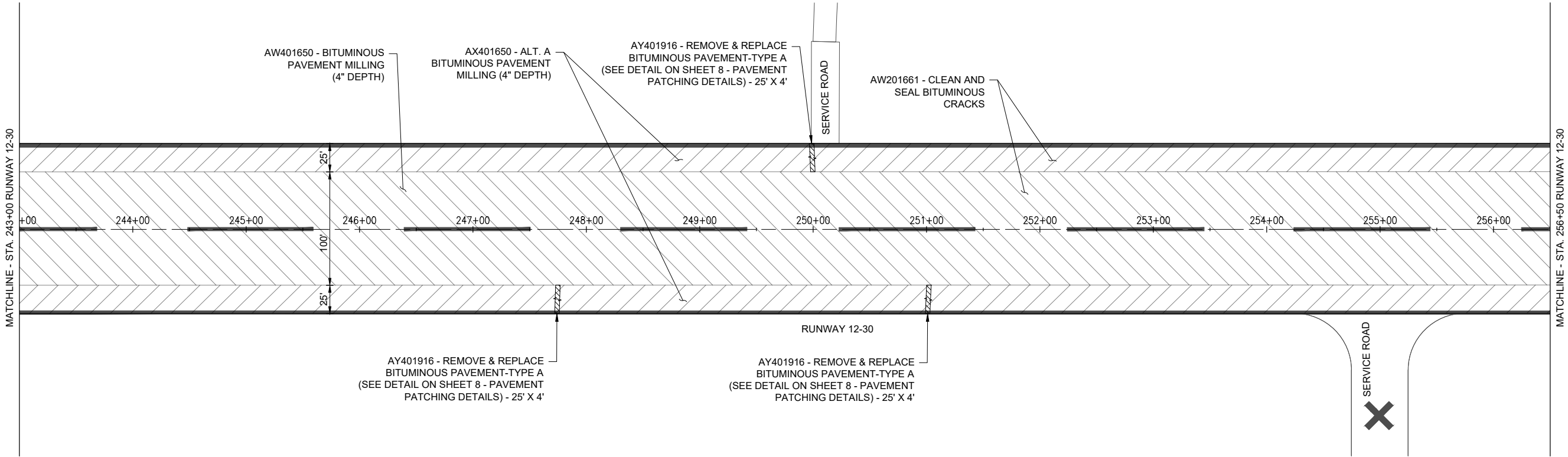
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SHEET TITLE

PAVEMENT
PREPARATION PLAN -
SHEET 1

FOR BID

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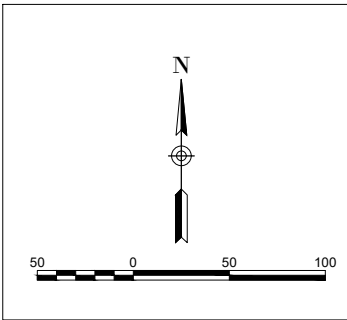
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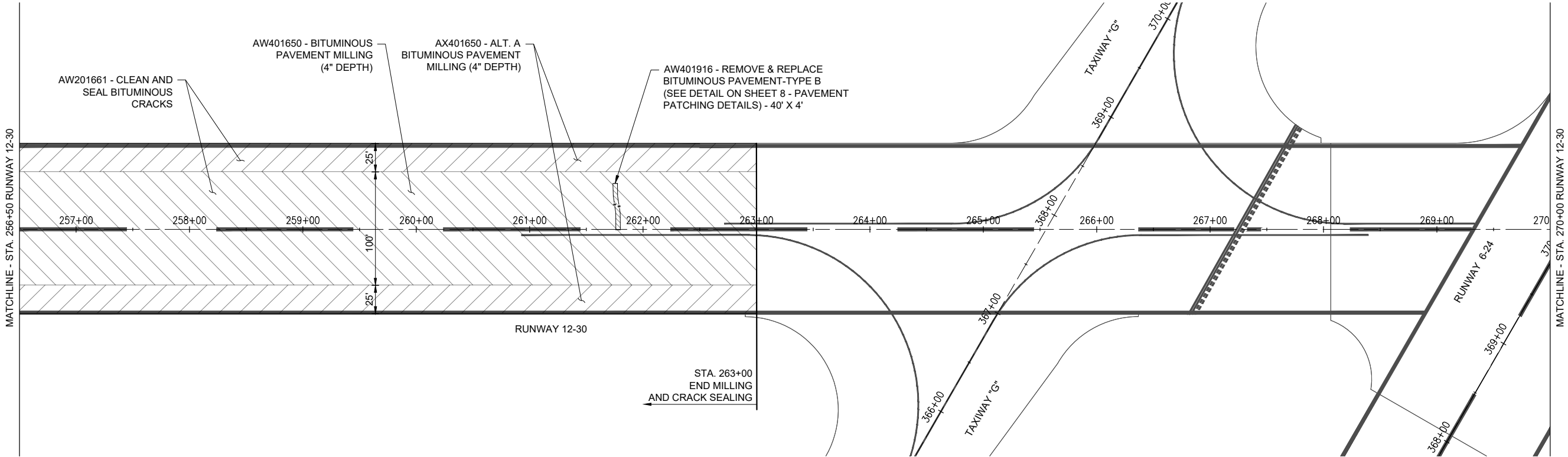
DRAWN BY: HLE 1/3/2025

REVIEWED BY: MJD 1/28/2025

SHEET TITLE

**PAVEMENT
PREPARATION PLAN -
SHEET 2**

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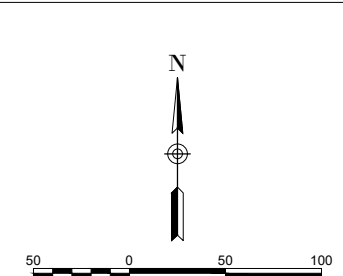
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SHEET TITLE

PAVEMENT
PREPARATION PLAN -
SHEET 3



DATE	LICENSE
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SHEET TITLE

PROPOSED PLAN AND PROFILE - SHEET 1





DATE	LICENSE
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REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PROPOSED PLAN
AND PROFILE -
SHEET 2

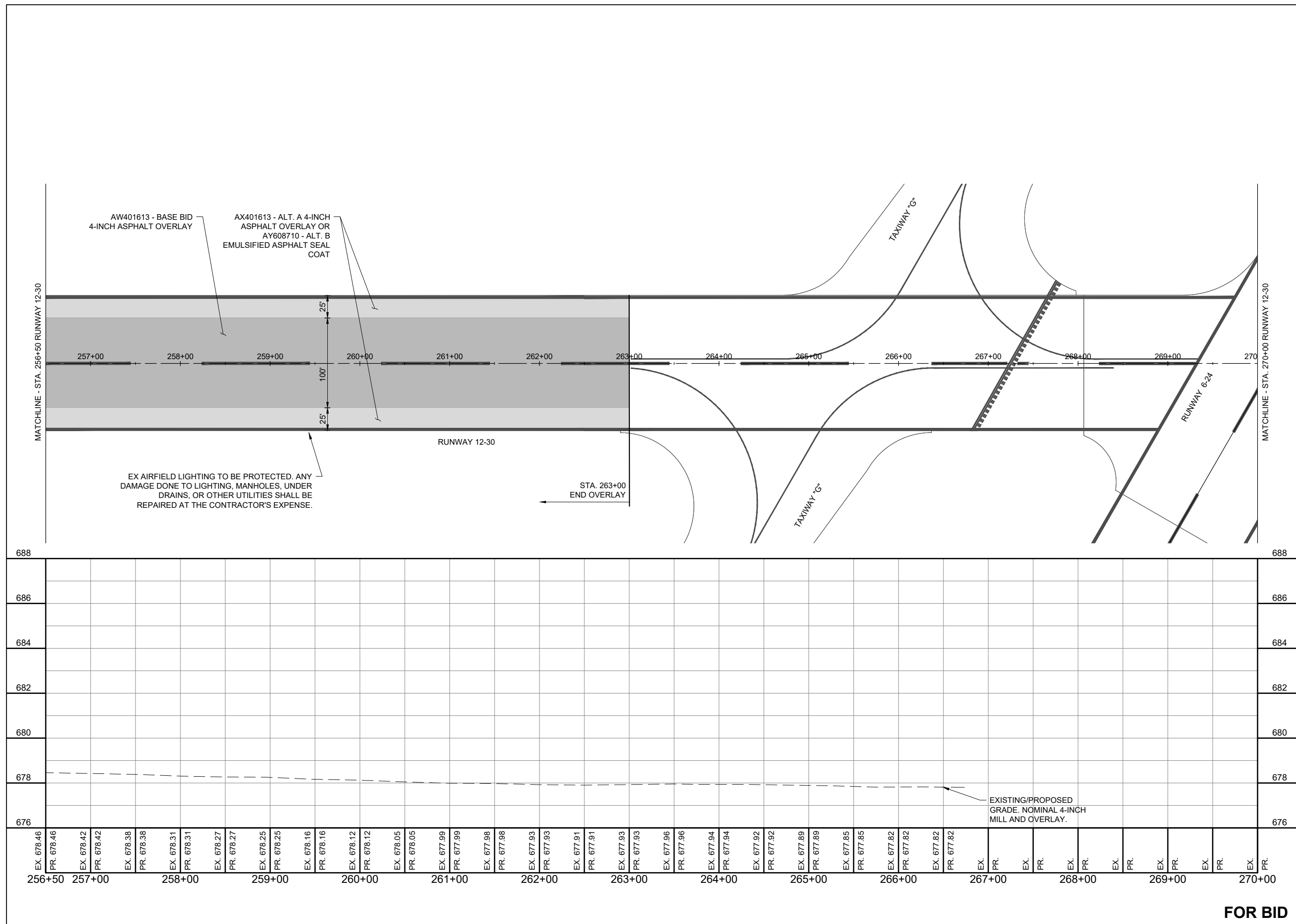


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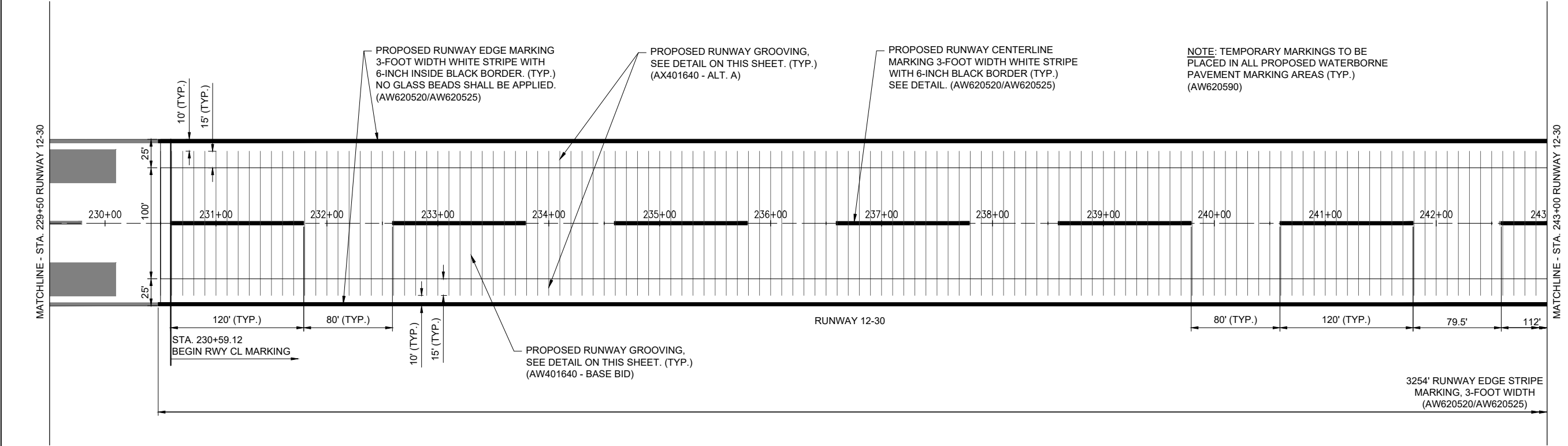
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SHEET TITLE

PROPOSED PLAN AND PROFILE - SHEET 3



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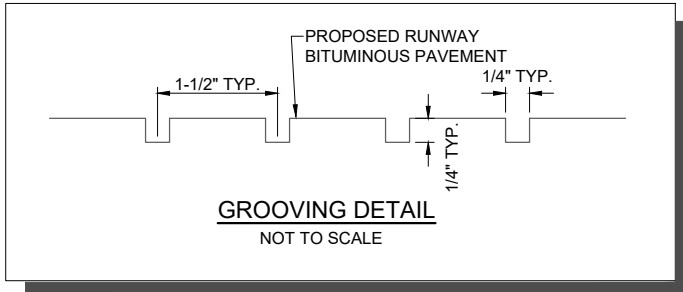


WATERBORNE PAVEMENT MARKING NOTES

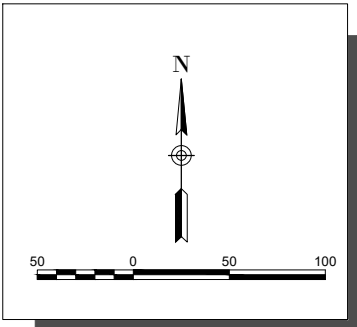
- A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED.
- WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
- GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT. GLASS BEADS SHALL NOT BE APPLIED TO RUNWAY EDGE STRIPES.
- CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

BITUMINOUS GROOVING NOTES

- THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
- THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP AND PLACED ON 1 1/2" CENTERS
- TOLERANCES ON THE GROOVING WILL BE 1 1/2" IN ALIGNMENT FOR 75', ±1/16" IN DEPTH, +1/16" IN WIDTH, -1/8" IN GROOVE SPACING.
- GROOVES SHALL BE SAWED NO LESS THAN 6" AND NO MORE THAN 18" FROM AN IN-PAVEMENT LIGHT.
- ALL SOLID MATERIAL GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND DISPOSED OF AT AN APPROVED LAND FILL OFF THE AIRPORT SITE, AT THE CONTRACTOR'S OWN EXPENSE.
- AT THE END OF EACH WORK DAY THE RUNWAY WILL BE FLUSHED, CLEANED AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.



MARKING SCHEDULE - BASE BID			
AR620520 - PAVEMENT MARKING - WATERBORNE (WHITE)			
RUNWAY 12-30 CENTERLINE STRIPES (120' X 3')	360	13	4680
RUNWAY 12-30 CENTERLINE STRIPES (112' X 3')	336	4	1344
RUNWAY 12-30 EDGE STRIPES (3254' X 3')	9762	2	19524
AR620520 - PAVEMENT MARKING - WATERBORNE (YELLOW)			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
12" TAXIWAY CENTERLINES - TWY G (SOUTHWEST)	209	1	209
12" TAXIWAY CENTERLINES - TWY G (NORTHEAST)	31	1	31
AR620525 PAVEMENT MARKING - BLACK BORDER			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
TAXIWAY CENTERLINES	242	1	242
RUNWAY 12-30 CENTERLINE STRIPES (120' X 3')	124	13	1612
RUNWAY 12-30 CENTERLINE STRIPES (112' X 3')	116	4	464
RUNWAY 12-30 EDGE STRIPES	1627	2	3254



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SHEET TITLE

PROPOSED MARKING
AND GROOVING PLAN
- SHEET 1



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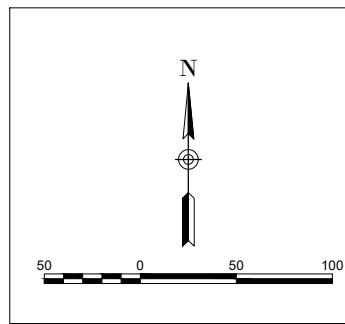
DESIGN BY: HLE 1/3/2025

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SHEET TITLE

PROPOSED MARKING AND GROOVING PLAN - SHEET 2



FOR BID

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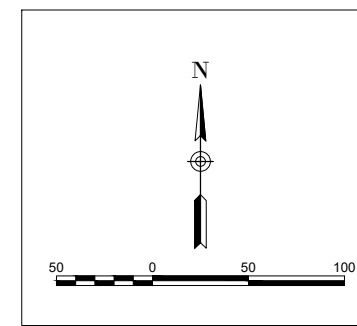
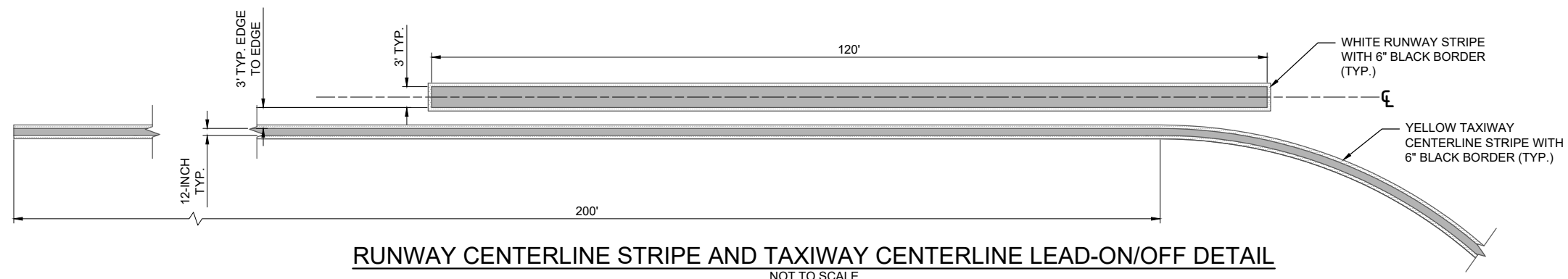
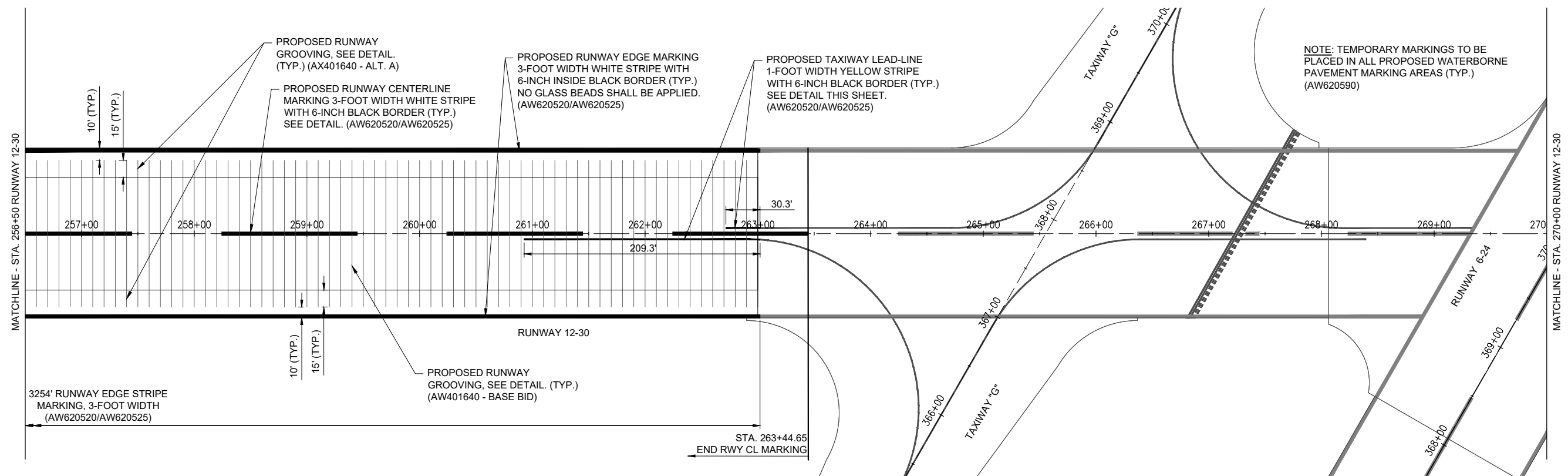
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PROPOSED MARKING
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- SHEET 3



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