DE085 TOTAL SHEETS = 18

CONSTRUCTION PLANS - FOR BID

REHABILITATE RUNWAY 12/30 PAVEMENT

DECATUR PARK DISTRICT
DECATUR AIRPORT (DEC)
DECATUR, MACON COUNTY, ILLINOIS

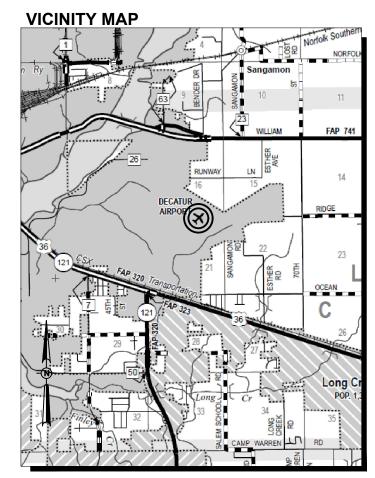
IDA PROJECT NO. DEC-5216 A.I.P. PROJECT NO. 3-17-0033-TBD APRIL 18, 2025

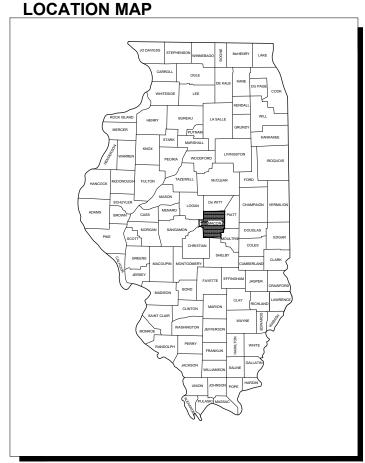
SCOPE OF WORK

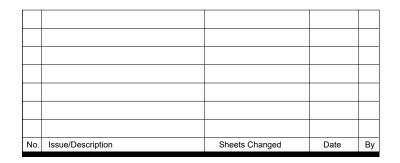
THIS PROJECT INVOLVES MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE INNER 100 FEET OF A SECTION OF RUNWAY 12-30. THE BASE BID ALSO INCLUDES BITUMINOUS CRACK SEALING, FULL-DEPTH PATCHING, GROOVING, AND MARKING. OPTIONAL ALTERNATE A COVERS MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE REMAINING WIDTH OF THE SPECIFIED RUNWAY SECTION, WHILE OPTIONAL ALTERNATE B INVOLVES SEAL COATING THE REMAINING WIDTH.

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.











SUMMARY OF QUANTITIES - BASE BID					
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY	
AW150510	ENGINEER'S FIELD OFFICE	L SUM	1		
AW150520	MOBILIZATION	L SUM	1		
AW201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	10,000		
AW401010	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	L SUM	1		
AW401614	BIT. SURF. CSEMETHOD II, SUPERPAVE	TON	8,306		
AW401630	BITUMINOUS SURFACE TEST SECTION	EACH	1		
AW401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	36,112		
AW401650	BITUMINOUS PAVEMENT MILLING	SQ YD	36,112		
AW401916	REM & REP BIT PAVEMENT-TYPE B	SQ YD	18		
AW603510	BITUMINOUS TACK COAT	GALLON	6,139		
AW620520	PAVEMENT MARKING-WATERBORNE	SQ FT	25,788		
AW620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	5,572		
AW620590	TEMPORARY MARKING	SQ FT	25,788		

SUMMARY OF QUANTITIES - OPTIONAL ALTERNATE A							
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY			
AX401614	BIT. SURF. CSEMETHOD II, SUPERPAVE	TON	4,153				
AX401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	10,834				
AX401650	BITUMINOUS PAVEMENT MILLING	SQ YD	18,056				
AX603510	BITUMINOUS TACK COAT	GALLON	3,070				

SUMMARY OF QUANTITIES - OPTIONAL ALTERNATE B					
ITEM NO.	ITEM NO. DESCRIPTION		TOTAL QUANTITY	AS-BUILT QUANTITY	
AY401915	REM & REP BIT PAVEMENT-TYPE A	SQ YD	67		
AY625510	TAR EMULSION SEAL COAT	SQ YD	18,056		

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES AND AGENCIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL STOP WORK AND IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER. CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND

INDEX OF SHEETS SHEET SHEET TITLE NO. COVER SHEET SUMMARY OF QUANTITIES AND INDEX TO SHEETS ALIGNMENT DATA SHEET CONSTRUCTION SAFETY AND PHASING PLAN CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 1 CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 2 CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 3 PAVEMENT PATCHING DETAILS 8 9 TYPICAL SECTION - RUNWAY 12-30 PAVEMENT PREPARATION PLAN - SHEET 1 10 PAVEMENT PREPARATION PLAN - SHEET 2 11 PAVEMENT PREPARATION PLAN - SHEET 3 12 PROPOSED PLAN AND PROFILE - SHEET 1 13 14 PROPOSED PLAN AND PROFILE - SHEET 2 15 PROPOSED PLAN AND PROFILE - SHEET 3 16 PROPOSED MARKING AND GROOVING PLAN - SHEET 1

PROPOSED MARKING AND GROOVING PLAN - SHEET 2

PROPOSED MARKING AND GROOVING PLAN - SHEET 3

17

18

GENERAL NOTES

- 1. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- 3. ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- 4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- 5. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE, AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRTICAL, DRAINAGE AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.
- 6. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS ARE TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
- 7. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- 8. THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.
- 9. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL
- 11. THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- 12. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- 13. NPDES PERMIT THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
- 14. MATERIAL CERTIFICATIONS MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED, AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- 15. CERTIFIED PAYROLLS THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.
- 16. STANDARDS AND CODES STANDARDS AND CODES REFERENCED HERIN SHALL BE UNDERSTOOD TO BE REFERRING TO THE CURRENT EDITION.

J.U.L.I.E. INFORMATION

COUNTY:	MACON
CITY:	 DECATUR
TOWNSHIP:	 LONG CREEK
SECTION NO.:	 15, 16, 21, AND 22
ADDRESS:	DECATUR AIRPORT
	910 SOUTH AIRPORT ROAD
	DECATUR II 62524

HANSON

Offices Nationwide www.hanson-inc.com

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62703 phone: 217-788-2450 fax: 217-788-2503

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Decatur Park District Decatur Airport 910 South Airport Road Decatur, IL 62521



Michael Unit | Signed: 04/18/2025 | EXPIRES: 11/30/2025

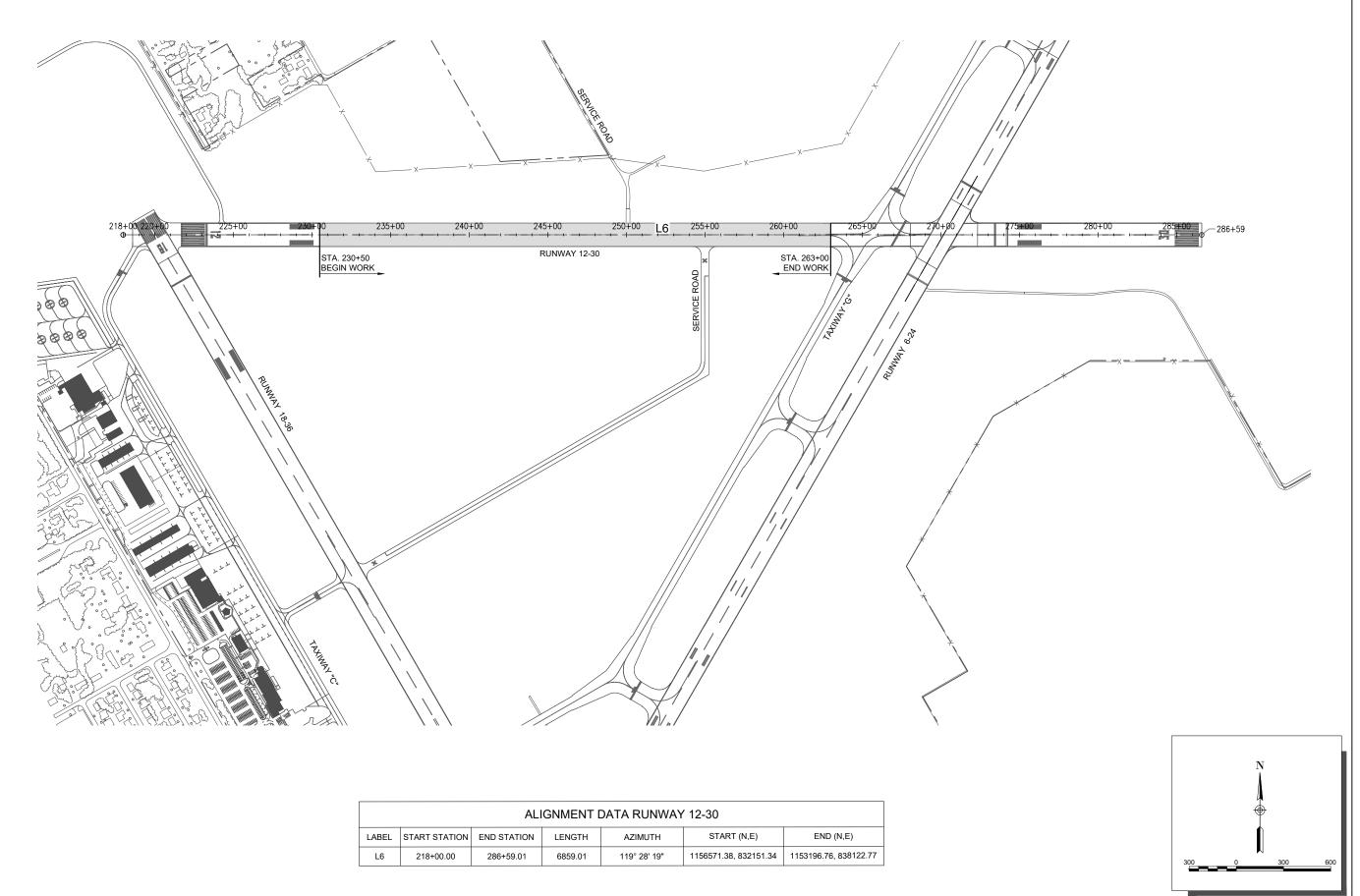
IDA #: DEC-5216 FAA #: 3-17-0033-TBD

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SUMMARY OF QUANTITIES AND INDEX TO SHEETS

REVIEWED BY: MJD 1/28/2025

SHEET TITLE



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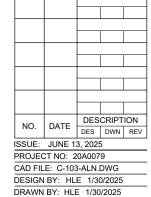


Decatur Park District Decatur Airport 910 South Airport Road Decatur, IL 62521



DATE LICENSE SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216 FAA #: 3-17-0033-TBD

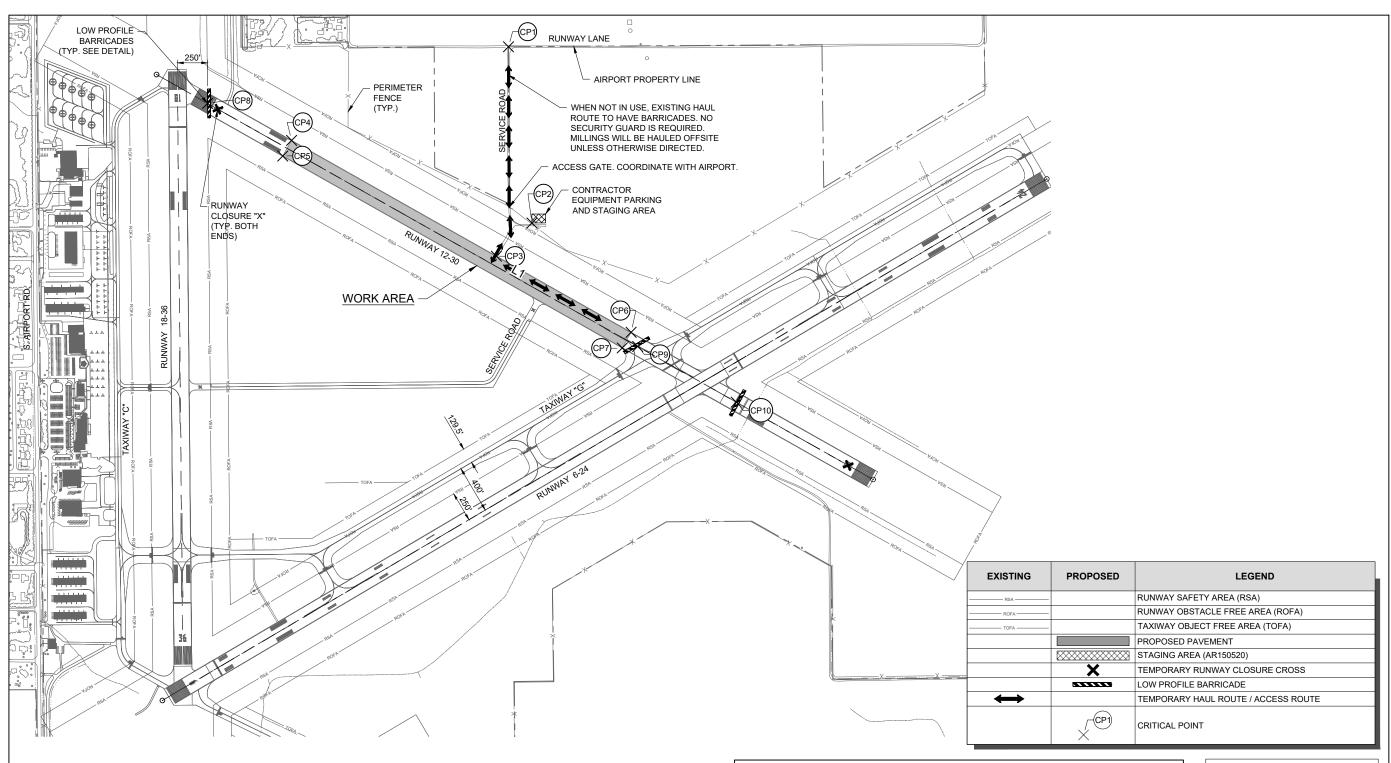


REVIEWED BY: MJD 1/30/2025

SHEET TITLE

ALIGNMENT DATA SHEET

FOR BID



GENERAL NOTES:

WORK TO BE COMPLETED: HAUL ROUTES/STAGING AREAS CONSTRUCTION; MILLING AND OVERLAY OF THE INNER 100 FEET (4-INCH DEPTH) OF RUNWAY 12-30, INCLUDING BITUMINOUS CRACK SEALING, ONE FULL-DEPTH PATCH, GROOVING, AND MARKING.

OPTIONAL ALTERNATE A: MILLING AND OVERLAY (4-INCH DEPTH) OF THE OUTER 25 FEET ON EACH SIDE OF RUNWAY 12-30 AND GROOVING.

OPTIONAL ALTERNATE B: SEAL COATING OF THE OUTER 25 FEET ON EACH SIDE OF RUNWAY 12-30 AND PARTIAL-DEPTH PATCHING.

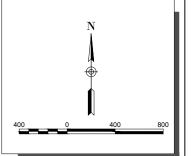
AIRFIELD CLOSURES AND CHANGES:

- RUNWAY 18/36 AND RUNWAY 6/24 WILL REMAIN OPEN.
- RUNWAY 12/30 WILL BE CLOSED UNTIL ALL WORK HAS BEEN COMPLETED.

OPERATIONAL SAFETY NOTES:

- FLAGPERSONS AND OR ESCORT WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- WHEN ACCESSING THE CONSTRUCTION SITE, THE CONTRACTOR SHALL MAINTAIN AIRFIELD SECURITY DURING ALL PHASES OF CONSTRUCTION.
- 3. ALL NOTAMS TO BE ISSUED SHALL BE ISSUED BY AIRPORT REPRESENTATIVES.
- 4. AIRPORT AND CONTRACTOR SHALL COORDINATE THE LOCATION OF UNDERGROUND UTILITIES WITHIN WORK AREAS PRIOR TO THE START

CRITICAL POINTS						
POINT#	DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEV.	EQUIP. HEIGHT	TOTAL ELEV.
1	HAUL ROUTE	N039° 50' 31.25"	W088° 51' 52.37"	679'	25'	704'
2	STAGING AREA	N039° 50' 16.76"	W088° 51' 49.76"	677'	25'	702'
3	HAUL ROUTE	N039° 50' 14.00"	W088° 51' 53.53"	678'	25'	703'
4	PROJECT LIMITS	N039° 50' 23.43"	W088° 52' 15.48"	679'	25'	704'
5	PROJECT LIMITS	N039° 50' 22.14"	W088° 52' 16.42"	679'	25'	704'
6	PROJECT LIMITS	N039° 50' 07.80"	W088° 51' 39.09"	677'	25'	702'
7	PROJECT LIMITS	N039° 50' 06.51"	W088° 51' 40.02"	677'	25'	702'
8	LOW PROFILE BARRICADES	N039° 50' 26.44"	W088° 52' 24.48"	679'	3'	682'
9	LOW PROFILE BARRICADES	N039° 50' 06.64"	W088° 51' 38.38"	678'	3'	681'
10	LOW PROFILE BARRICADES	N039° 50' 02.10"	W088° 51' 27.81"	677'	3'	680'



FOR BID

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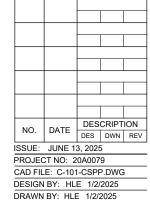


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SIGNED: 04/18/2025 EXPIRES: 11/30/2025

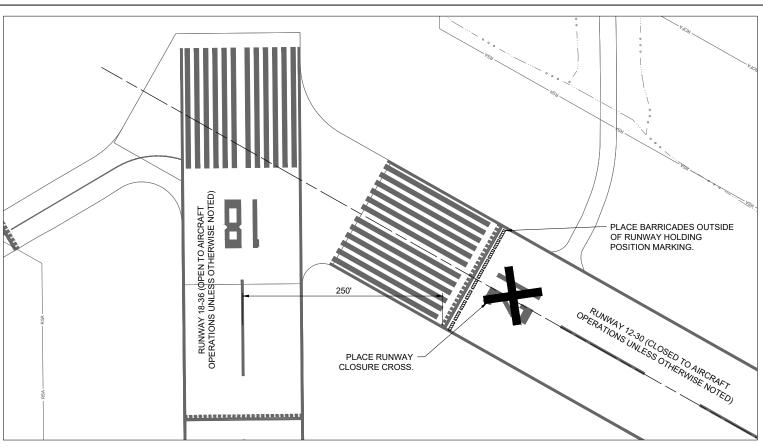
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REVIEWED BY: MJD 1/28/2025

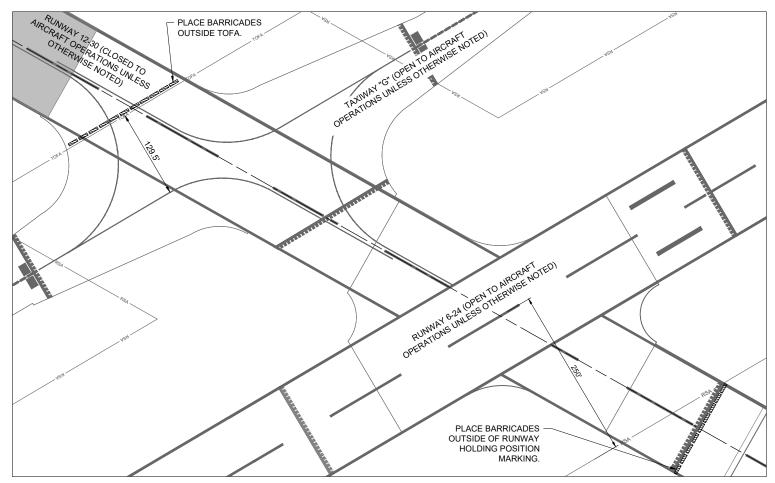
SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN



PLACEMENT OF BARRICADES AROUND WORK AREAS

NOT TO SCALE



PLACEMENT OF BARRICADES AROUND WORK AREAS

NOT TO SCALE

SAFETY NOTES

- THE FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS
 PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS
 SHEET.
- ALL PROVISIONS OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- 3. THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- 4. NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- 5. CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW. EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A
 CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING,
 MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- . NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY. REFER TO THE 7460 DETERMINATION FOR SPECIFIC INFORMATION ON ALLOWABLE WORKING HEIGHTS.
- 8. CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL
 BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED
 WITH STEEL PLATES IN ALL OTHER AREAS.
- 10. OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- 11. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- 12. NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- 13. SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- 14. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- 15. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- 16. CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- 17. CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- 18. THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- 19. CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- 20. CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- 21. CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- 22. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- 23. NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE (150' FOR RUNWAY) 5-23) OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.

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Illinois Licensed Professional Service Corporation #184-001084



Decatur Park District Decatur Airport 910 South Airport Road Decatur, IL 62521



Michael Wall

SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216 FAA #: 3-17-0033-TBD

NO.	DATE	DESCRIPTION		
NO.	DATE	DES	DWN	REV
ISSUE:	JUNE 1	3, 2025	5	
PROJECT NO: 20A0079				
CAD FILE: C-101-CSPP.DWG				
DESIGN	BY: HL	E 1/2/	2025	
DRAWN	BY: HLE	= 1/2/	2025	

REVIEWED BY: MJD 1/28/2025

SHEET TITLE

CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 1

FOR BID

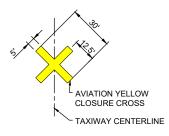


LIGHTED RUNWAY CLOSURE MARKER

NOT TO SCALE

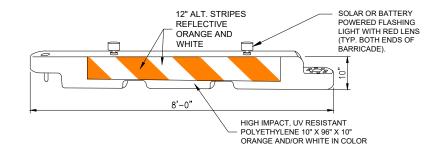
NOTES:

- THE AIRPORT HAS TWO LIGHTED RUNWAY CLOSURE MARKERS AVAILABLE FOR USE ON THIS PROJECT. THE COST OF PLACING, OPERATING, MAINTAINING, AND REMOVING THE LIGHTED RUNWAY CLOSURE MARKERS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL RETURN THE AIRPORT-OWNED LIGHTED RUNWAY CLOSURE MARKERS IN THE SAME OR BETTER CONDITION THAN AT THE START OF
- 2. THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE LIGHTED CROSSES AND MAKE PROMPT REPAIRS AS NECESSARY
- 3. THE CONTRACTOR SHALL BE ON-CALL FOR 24-HOUR EMERGENCY MAINTENANCE WHEN LIGHTED CROSSES ARE BEING USED
- 4. THE LIGHTED MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERALS AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER.
- 5. LIGHTED MARKERS SHALL BE SECURED FROM WIND EFFECTS BY THE CONTRACTOR AS RECOMMENDED BY THE MANUFACTURER.
- 6. THE LIGHTED MARKERS SHALL BE IN PLACE AND OPERATING WHENEVER THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED.



TAXIWAY CLOSURE CROSS MARKER DETAIL

NOT TO SCALE

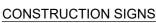


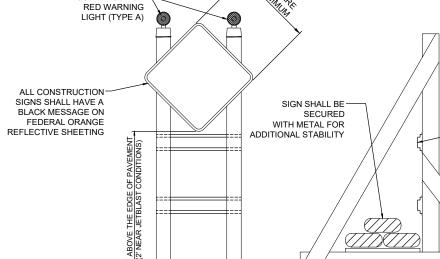
LOW-PROFILE BARRICADE DETAIL

DETAIL ABOVE REPRESENTS ONE OPTION FOR LOW-PROFILE BARRICADES. OTHER OPTIONS MAY BE UTILIZED AS LONG AS THEY MEET THE REQUIREMENTS OF THE PROJECT, INCLUDING BARRICADE NOTE 1.









SIGNAGE NOTES

FRONT VIEW

CONSTRUCTION

ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN

SIDE VIEW

- 2. UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW
- 4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- 5. COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE

BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- 2. BARRICADES SHALL BE "LOW-PROFILE" WITH A MAXIMUM HEIGHT OF 18" ABOVE GROUND, EXCLUSIVE OF ASSOCIATED WARNING LIGHTS AND FLAGS.
- 3. BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT, WITH GAPS BETWEEN BARRICADES NOT TO EXCEED 4' WIDE. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS
- 4. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- 6. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND
- 7. THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- 8. COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE PAID FOR UNDER ITEM AR150530 - TRAFFIC MAINTENANCE.



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Professional Service Corporation #184-001084



Decatur Park District Decatur Airport 910 South Airport Road Decatur, IL 62521

SIGN SHALL BE BRACED

AND CONSTRUCTED

2" x 4" AND 4" x 4" IN

ACCORDANCE WITH

IDOT DETAILS

TOGETHER USING



SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216 FAA #: 3-17-0033-TBD

	NO.	DATE	DESCRIPTION			
	NO. DATE		DES	DWN	REV	
ISSUE: JUNE 13, 2025						
PROJECT NO: 20A0079						
	CAD FILE: C-101-CSPP.DWG					
	DESIGN	BY: HL	E 1/2/	2025		

SHEET TITLE

CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET

DRAWN BY: HLE 1/2/2025

REVIEWED BY: MJD 1/28/2025

THE PURPOSE OF THIS DOCUMENT IS TO PROVIDE INFORMATION CONCERNING PROJECT OPERATIONAL SAFETY AT THE AIRPORT DURING THE PROJECT AND SUPPLEMENTS THE INFORMATION IN THE CONTRACT DOCUMENTS. THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) ADDRESSES ONLY THOSE SAFETY ISSUES PARTICULAR TO AIRPORT OPERATIONAL ENVIRONMENTS. IT IS NOT A COMPREHENSIVE CONSTRUCTION SAFETY DOCUMENT, AND THE CONTRACTOR SHOULD NOT RELY UPON IT AS SUCH. NOTHING CONTAINED IN THIS PLAN WAIVE THE DUTY OF THE CONTRACTOR TO EMPLOY ADEQUATE AND PROVEN SAFETY PRACTICES.

THE AWARDED CONTRACTOR MUST, AFTER REVIEW OF THE CSPP AND PRIOR RECEIVING A NOTICE TO PROCEED, PREPARE AND SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G (OR CURRENT ISSUE). THE DOCUMENT MUST INCLUDE A STATEMENT AFFIRMING THAT THE CONTRACTOR HAS READ AND WILL ABIDE BY THIS CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). IN ADDITION, IT MUST INCLUDE SUPPLEMENTAL INFORMATION THAT WAS INCLUDED BY ADDENDUM DURING THE BIDDING PROCESS, THAT COULD NOT BE INCLUDED PRIOR TO BID AWARD AND/OR THAT IS NEEDED TO CLARIFY OR EMPHASIZE SPECIFIC CONTRACTOR SAFETY MEASURES.

PROJECT DESCRIPTION

THIS PROJECT INVOLVES MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE INNER 100 FEET OF A SECTION OF RUNWAY 12-30. THE BASE BID ALSO INCLUDES BITUMINOUS CRACK SEALING, FULL-DEPTH PATCHING, GROOVING, AND MARKING. ADDITIVE ALTERNATE A COVERS MILLING AND OVERLAYING A 4-INCH LAYER OF BITUMINOUS PAVEMENT ON THE REMAINING WIDTH OF THE SPECIFIED RUNWAY SECTION, WHILE ADDITIVE ALTERNATE B INVOLVES SEAL COATING THE REMAINING WIDTH.

SECTION 1. COORDINATION

- A. PRECONSTRUCTION CONFERENCE: A PRECONSTRUCTION CONFERENCE WILL BE HELD PRIOR TO ISSUING A NOTICE TO PROCEED. AT A MINIMUM, REQUIRED ATTENDEES WILL INCLUDE THE AIRPORT MANAGER, IDOT PERSONNEL, ENGINEER, CONSTRUCTION ADMINISTRATION PERSONNEL, CONSTRUCTION OBSERVATION STAFF, PROJECT SUPERINTENDENT AND FOREMAN FOR THE PRIME CONTRACTOR. THE PRECONSTRUCTION CONFERENCE WILL INCLUDE AN AGENDA ITEM FOR REVIEW OF THE CSPP AND THE CONTRACTORS SPOD AND OTHER REQUIRED PROVISIONS.
- B. CONSTRUCTION PROGRESS MEETINGS: PROGRESS MEETINGS WILL BE HELD ON A WEEKLY OR BI-WEEKLY BASIS
 THROUGHOUT THE DURATION OF THE PROJECT. ADDITIONAL MEETINGS WILL BE HELD WHEN REQUESTED BY THE OWNER/AIRPORT,
 ENGINEER, OR CONTRACTOR. AT A MINIMUM, ATTENDEES WILL INCLUDE THE AIRPORT MANAGER, ENGINEER, CONSTRUCTION
 ADMINISTRATION PERSONNEL. AND PROJECT SUPERINTENDENT FOR THE PRIME CONTRACTOR.
- C. CONTACTS: DURING THE PRECONSTRUCTION CONFERENCE THE OWNER/AIRPORT STAFF, CONTRACTOR, AND ENGINEER SHALL EACH DESIGNATE A REPRESENTATIVE FOR PROJECT SAFETY MATTERS.
- D. SCOPE OR SCHEDULE CHANGES: THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A SCHEDULE DETAILING THE VARIOUS ACTIVITIES NECESSARY TO ACCOMPLISH THE PROJECT. THE CONTRACTOR SHALL SUBMIT AN UPDATED SCHEDULE AND DISCUSS SCHEDULING OF CONSTRUCTION PEACH CONSTRUCTION AT EACH CONSTRUCTION AT EACH CONSTRUCTION AS SCHEDULE CHANGES WITH THE AIRPORT AND ITS DESIGNATED REPRESENTATIVES. CHANGES IN SCOPE OR SCHEDULE MAY NECESSITATE REVISIONS TO THIS CSPP AND REQUIRE REVIEW AND APPROVAL BY THE OWNER AND THE FAA.

FOLLOWING ARE THE GENERAL SAFETY PLAN OBJECTIVES THAT MUST BE ACHIEVED IN ORDER TO MAXIMIZE BOTH CONTRACTOR AND AIRPORT SAFETY AND TO MINIMIZE TIME AND ECONOMIC LOSS TO THE AVIATION COMMUNITY, THE CONSTRUCTION CONTRACTOR AND OTHERS DIRECTLY AFFECTED BY THE PROJECT.

- (A) MAINTAIN SAFETY OF AIRCRAFT OPERATIONS.
- (B) MINIMIZE AIRCRAFT OPERATION/CONSTRUCTION ACTIVITY CONFLICTS.
- (C) KEEP THE AIRPORT OPERATIONAL FOR ALL USER AIRCRAFT
- (D) MINIMIZE DELAYS TO AIRCRAFT OPERATIONS
- (E) MINIMIZE DELAYS TO CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHOULD KEEP THESE OBJECTIVES IN MIND WHEN FORMULATING HIS PROJECT WORK SCHEDULES AND OPERATIONAL ACTIVITIES.

SECTION 2. PHASING

THIS PROJECT OCCURS WITHIN THE AIRPORT OPERATIONS AREA (AOA) OF THE AIRPORT, AND PRIMARILY INSIDE OF AIRCRAFT MOVEMENT AREAS. THERE IS ONE PHASE SHOWN IN THE PROJECT CONSTRUCTION PLANS. THIS SINGLE PHASE SHALL INCLUDE ALL WORK WITHIN THE RUNWAY SAFETY OBJECT AREA (ROFA). THIS SINGLE PHASE INCLUDES REMARKING OF THE PORTION OF THE RUNWAY REHABILITATED AFTER THE 30 DAY (MINIMUM) CURING PERIOD.

THE CONTRACTOR SHALL SUBMIT A PROJECT CONSTRUCTION SCHEDULE AND PHASING PLAN FOR THE WORK A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE PRECONSTRUCTION CONFERENCE. THE SCHEDULE SHALL INCLUDE PHASING OF THE PROPOSED OBSTRUCTION REMOVAL WITH INSTALLATION/REMOVAL OF SAFETY DEVICES AND MAINTENANCE OF TRAFFIC ITEMS. THE CONTRACTOR'S PHASING PLAN WILL BE REVIEWED AT THE PRECONSTRUCTION CONFERENCE AND AT EACH REGULAR CONSTRUCTION PROGRESS MEETING.

$\underline{\text{SECTION 3. AREAS AND OPERATIONS AFFECTED BY CONSTRUCTION}}$

THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) SHEETS INCLUDED IN THIS SECTION AND AS PART OF THE CONSTRUCTION PLANS FOR THE PROJECT DEPICT THE AREAS THAT WILL BE AFFECTED BY THE CONSTRUCTION ACTIVITIES.

AT NO TIME MAY THE CONTRACTOR WORK INSIDE THE AIRCRAFT OPERATIONS AREA (AOA) WHILE IT IS ACTIVE. ANY WORK DONE INSIDE THIS AREA WILL REQUIRE TEMPORARY CLOSURE OF THE RUNWAY, THE AOA IS GOVERNED BY THE RUNWAY OBSTACLE FREE ZONE (OFZ) TO A WIDTH OF 200' FROM THE RUNWAY CENTERLINE, AND THE THRESHOLD SITING SURFACE (TSS) STARTING AT EACH RUNWAY END AND RISING AT A SLOPE OF 20:1.

IN AREAS WHERE IT IS NECESSARY TO MOVE EQUIPMENT OR PERSONNEL THROUGH THE ACTIVE AGA FOR SITE ACCESS, THE CONTRACTOR SHALL PROVIDE AN ESCORT IN TWO-WAY RADIO CONTACT WITH THE AIRPORT GROUND UNICOM (121.75 MHZ) AND/OR TOWER UNICOM (118.9 MHZ) UNLESS OTHERWISE INSTRUCTED TO USE A DIFFERENT FREQUENCY.

ALL EQUIPMENT MUST BE LOWERED WHEN NOT IN USE OR IN TRANSIT AND MAY NOT BE LEFT WITHIN 200' OF THE RUNWAY CENTERLINE, EXTENDED.

THE CONTRACTOR SHALL NOT ENTER ANY AIRPORT AREAS OUTSIDE OF THE DESIGNATED WORK AREAS.

SECTION 4. NAVAID PROTECTION

THE PROJECT IS LOCATED IN THE AREA OF THE RUNWAY NAVIGATIONAL AIDS (NAVAIDS). THE PROJECT IS IN THE VICINITY OF AIRFIELD LIGHTING CIRCUITS AND EQUIPMENT. THE CONTRACTOR SHALL TAKE SPECIAL CARE TO AVOID DAMAGING THESE FACILITIES AND SHALL PROMPTLY REPORT ANY DAMAGE TO THE CONSTRUCTION OSERVATION STAFF AND THE AIRPORT MANAGER. THE CONTRACTOR SHALL PROMPTLY REPAIR ANY DAMAGE CAUSED TO THESE FACILITIES. COORDINATE WITH THE AIRPORT MANAGER TO TAKE THE RUNWAY NAVAIDS AND LIGHTING CIRCUITS OUT OF SERVICE WHEN THE RUNWAY IS CLOSED DUTO CONSTRUCTION. ALL NAVAID CRITICAL AREAS NEAR THE SITE SHALL BE APPROPRIATELY MARKED.

SECTION 5. CONTRACTOR ACCESS

MAY MAY

- A. LOCATION OF STOCKPILED CONSTRUCTION MATERIALS: MILLINGS WILL BE HAULED OFFSITE UNLESS OTHERWISE DIRECTED. IF MILLINGS ARE TO BE STOCKPILED INSTEAD, THE CONTRACTOR IS LIMITED TO THE PLACEMENT OF STOCKPILED MATERIALS AND SHALL COORDINATE WITH THE ENGINEER AND AIRPORT. CONTRACTOR SHALL MANAGE STOCKPILES AND MAINTAIN POSITIVE DRAINAGE SO THEY DO NOT BECOME WILDLIFE ATTRACTIONS OR CREATE FOREIGN OBJECT DEBRIS (FOD.)
- B. VEHICLE AND PEDESTRIAN OPERATIONS:
- 1. PEDESTRIAN AND GROUND VEHICLES TRAINING: THE CONTRACTOR SHOULD COORDINATE WITH THE AIRPORT MANAGER TO OBTAIN NECESSARY TRAINING REQUIRED TO OPERATE VEHICLES AND PEDESTRIANS WITHIN THE AOA, COMMUNICATION WITH ATCT. AND SAFE AND ORDERLY ACCESS TO WORK AREA.
- 2. CONTRACTOR STAGING AREA: THE OWNER HAS DESIGNATED MATERIALS STORAGE AND EQUIPMENT STAGING AREAS ON THE AIRPORT SITE AS INDICATED ON THE PLANS FOR THE CONTRACTORS' UTILIZATION DURING CONSTRUCTION WORK ACTIVITIES. THE CONTRACTOR SHALL USE THIS AREA FOR TEMPORARY STORAGE OF MATERIALS AND SUPPLIES, THE OVERNIGHT PARKING, SERVICING, FUELING AND REPAIR OF EQUIPMENT, FIELD OFFICES, SANITARY FACILITIES, EMPLOYEE PARKING AND OTHER PROJECT WORK ACTIVITIES. NO OTHER AREA OF THE AIRPORT SHALL BE USED FOR SUCH CONTRACTOR PURPOSES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COSTS OF ELECTRICAL, TELEPHONE, AND OTHER SERVICES TO THESE STAGING AREAS (IF NEEDED), AS WELL AS ANY LOCALLY REQUIRED BUILDING CONSTRUCTION OR TEMPORARY USE PERMITS. SINCE ON-SITE WATER OR SEWER UTILITIES ARE NOT AVAILABLE, THE CONTRACTOR SHALL PROVIDE SUITABLE QUANTITY OF POTABLE DRINKING WATER AND TEMPORARY SANITARY-LATRINE UNITS TO ACCOMMODATE THE NEEDS OF CONTRACTOR'S PERSONNEL, VISITORS, AND OTHER PROJECT PARTIES WITHIN THE STAGING AREA.

ALL ON-SITE CONTRACTOR EQUIPMENT SHALL MEET AND BE SAFELY OPERATED IN ACCORDANCE WITH APPLICABLE LOCAL, STATE AND FEDERAL ENVIRONMENTAL REGULATIONS

THE CONTRACTOR SHALL NOT PARK EQUIPMENT NOR STORE SUPPLIES AND MATERIALS IN ANY PORTION OF THE RUNWAY AND TAXIMAY, SAFETY AREAS, OBJECT FREE AREAS OR APPROACH/DEPARTURE SURFACES. WHEN WORK IS RECOURED WITHIN THESE CRITICAL OPERATIONAL AREAS, THE CONTRACTOR'S EQUIPMENT AND VEHICLES, SUPPLIES AND MATERIALS SHALL BE PARKED AND EASILY TRANSPORTABLE SO THAT THEY MAY BE QUICKLY REMOVED TO ACCOMMODATE AIRCRAFT OPERATIONS. SUCH WORK ACTIVITIES SHALL BE UNDER THE DIRECT CONTROL OF RADIO-EQUIPPED MONITORS AND SIGNALMEN, AS OUTLINED IN THIS DOCLIMENT.

3. ACCESS AND HAUL ROADS: THE CONSTRUCTION PLANS DEPICT THE SITE ACCESS AND HAUL ROUTES FROM PUBLIC ROADWAYS AND HAUL ROUTES TO THE RESPECTIVE WORK AREAS. THE CONTRACTOR SHALL NOT DEVIATE FROM THESE HAUL ROUTES, AND SHALL PERFORM SUCH MAINTENANCE WORK, INCLUDING DUST CONTROL FOR UNPAYED FACILITIES, AS NECESSARY TO KEEP THEM IN USABLE CONDITION AT ALL TIMES. ANY/ALL DAMAGE TO EXISTING PAVEMENTS OR TURF AREAS WITHIN THESE DESIGNATED HAUL ROUTES CAUSED BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED TO ORIGINAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR EMPLOYEE PERSONAL VEHICLES MAY NOT BE PARKED OR DRIVEN IN THE ADA. PARKING AREAS FOR CONTRACTOR EMPLOYEES WILL BE IN THE AREAS DESIGNATED ON THE PLANS OR OTHERWISE DESIGNATED BY THE AIRPORT MANAGER. FOLLOWING COMPLETION. HAUL ROUTES SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION AT NO COST TO THE OWNER.

MARKING AND LIGHTING OF VEHICLES: ALL CONTRACTOR VEHICLES AND CONSTRUCTION EQUIPMENT WORKING ON THE AIRPORT AIRSIDE OF THE PROJECT FENCING, BARRICADED OR STAKED LIMITS DURING DAYLIGHT HOURS, SHALL BE EQUIPPED WITH A FLAG ON A STAFF ATTACHED SO THAT THE FLAG IS READILY VISIBLE ATOP THE HIGHEST PORTION OF THE MACHINE. THE FLAG SHALL BE AT LEAST 3-FOOT SQUARE HAVING A CHECKERED PATTERN COMPRISED OF INTERNATIONAL ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE. CONTRACTOR VEHICLES OPERATING INSIDE THE AIRPORT SECURITY FENCE SHALL BE IDENTIFIED WITH COMPANY LOGOS OR INSIGNIAS. ANY AND ALL VEHICLES NOT ROUTINELY OPERATING ON THE AIRPORT SHALL BE ESCORTED BY APPROPRIATELY FLAGGED AND/OR LIGHTED VEHICLES.

- C. VEHICLES AND EQUIPMENT OPERATING AT NIGHT ON THE AIRPORT'S AIRSIDE OF THE PROJECT FENCING, BARRICADED OR STAKED LIMITS, SHALL BE EQUIPPED WITH APPROPRIATELY SIZED, FLASHING, OR STEADY-BURNING YELLOW BEACONS, MOUNTED ON THE UPPERMOST PART OF THE VEHICLE OR MACHINES SO AS TO BE CONSPICUOUS FROM ANY AND ALL DIRECTIONS, INCLUDING THE AIR. MARKING AND LIGHTING OF VEHICLES SHALL BE IN ACCORDANCE WITH FAA AC 150/6310-50 (OR CURRENT ISSUE).
- D. TWO-WAY RADIO COMMUNICATIONS: IN AREAS WHERE IT IS NECESSARY TO MOVE EQUIPMENT OR PERSONNEL THROUGH THE ACTIVE AOA FOR SITE ACCESS, THE CONTRACTOR SHALL PROVIDE AN ESCORT IN TWO-WAY RADIO CONTACT WITH THE AIRPORT GROUND UNICOM (121.75 MHZ) AND/OR TOWER UNICOM (118.9 MHZ) UNLESS OTHERWISE INSTRUCTED TO USE A DIFFERENT FREQUENCY.
- E. AIRPORT SECURITY/PUBLIC PROTECTION: AIRPORT ACCESS AIRSIDE OF THE AIRPORT SECURITY FENCING, WHICH DEFINES THE AIRPORT OPERATIONS AREA (AOA) SHALL BE LIMITED TO APPROPRIATE CONTRACTOR VEHICLES. ACCESS SHALL BE THROUGH THE SECURITY GATE IDENTIFIED ON THE PLANS. SECURITY GATES SHALL REMAIN CLOSED AND LOCKED AT ALL TIMES, EXCEPT WHEN USED FOR ACTIVELY ACCESSING THE PROJECT SITE, AT WHICH TIME THEY SHALL BE SECURED BY DEDICATED CONTRACTOR PERSONNEL ALL PROJECT VISITORS, MATERIALS DELIVERIES AND OTHER PARTIES TRAVELING AIRSIDE OF THE PROJECT FENCED, BARRICADED OR STAKED WORK AREAS SHALL BE ESCORTED BY CONTRACTOR PERSONNEL. NO UNAUTHORIZED PERSONS OR UNESCORTED PERSONNEL SHALL BE ALLOWED TO ENTER THE AIRPORT. ALL PERSONS SHOULD REMAIN OUTSIDE OF THE RSA TO PROTECT PERSONS AND PROPERTY FROM AIRCRAFT BLAST.

SECTION 6. WILDLIFE MANAGEMENT

WILDLIFE, AND ESPECIALLY BIRDS, CAN POSE SERIOUS HAZARDS TO FLIGHT SAFETY. DURING CONSTRUCTION, THE CONTRACTOR SHALL MINIMIZE OR ELIMINATE TO THE EXTENT PRACTICABLE THOSE ACTIVITIES THAT WILL ATTRACT WILDLIFE TO THE AOA. THE FOLLOWING MINIMUM STEPS SHALL BE TAKEN DURING CONSTRUCTION.

A. TRASH: DO NOT LEAVE FOOD, EMPTY FOOD CONTAINERS, OR LITTER ON THE PROJECT SITE. ALSO, DO NOT LEAVE THESE ITEMS IN OPEN VEHICLE AREAS SUCH AS TRUCK BEDS.

- B. STANDING WATER: THE CONTRACTOR SHALL AVOID GENERATING AREAS OF STANDING WATER. AS NECESSARY, THE CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE TO ALL STANDING WATER AS GENERATED BY CONSTRUCTION ACTIVITIES.
- C. TALL GRASS AND SEEDS: THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A UNIFORM STAND OF GRASS ON ALL DISTURBED AREAS RESULTING FROM CONSTRUCTION ACTIVITIES, TO THE SATISFACTION OF THE AIRPORT MANAGER. AIRPORT PERSONNEL ARE RESPONSIBLE FOR MOWING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION LIMITS.
- D. POORLY MAINTAINED FENCING AND GATES: THE CONTRACTOR SHALL ENSURE ACCESS GATES REMAIN SECURELY CLOSED AT ALL TIMES WHEN NOT IN USE.
- E. DISRUPTION OF EXISTING WILDLIFE HABITAT: IF CONSTRUCTION ACTIVITIES DISRUPT WILDLIFE THAT MAY POST A SAFETY RISK TO AIRCRAFT OPERATIONS, THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER.

SECTION 7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

PAVEMENTS WITHIN AND ADJACENT TO THE PROJECT SITE SHALL BE KEPT FREE OF ALL DEBRIS, DIRT, WASTE, ETC., AT ALL TIMES. ACCIDENTAL SPILLS OF DIRT, EXCAVATION, OR OTHER MATERIALS SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR. THE CONTRACTOR WILL BE REQUIRED TO CONTINUOUSLY CLEAR THE PROJECT SITE OF ANY AND ALL DEBRIS CAPABLE OF BEING BLOWN BY WIND ONTO ACTIVE AIRFIELD AREAS.

DUST CONTROL MEASURES DURING GRADING AND HAULING OPERATIONS SHALL BE IMPLEMENTED BY THE CONTRACTOR TO ASSURE THAT AIRCRAFT OPERATIONS, SAFETY AND VISIBILITY ARE NOT IMPAIRED, NOR A NUISANCE RESULT FROM SUCH CONSTRUCTION WORK. IF REQUIRED BY THE AIRPORT, THE CONTRACTOR WILL PROVIDE A WATER TRUCK TO CONTROL DUST. WASTE DISPOSAL AREAS ARE NOT AVAILABLE ON THE AIRPORT SITE; THEREFORE, THE CONTRACTOR SHALL SAFELY REMOVE AND TRANSPORT ALL WASTE MATERIALS TO AN OFF-SITE, APPROVED DISPOSAL SITE OR LANDFILL.

SECTION 8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHOULD BE ADEQUATELY PREPARED TO CONTAIN AND CLEANUP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS FROM VEHICLES OR EQUIPMENT UTILIZED ON THE PROJECT. SPECIAL CARE MUST BE TAKEN WHEN HANDLING OR TRANSPORTING HAZARDOUS MATERIALS ON AIRPORT PROPERTY. SHOULD THE CONTRACTOR ENCOUNTER UNLABELED DRUMS, MATERIALS WITH EVIDENT PETROLEUM CONTAMINATION, OR OTHER POTENTIALLY SIGNIFICANT OR HAZARDOUS MATERIALS HE SHALL IMMEDIATELY TAKE MEASURES TO PROTECT WORKERS AND NEARBY RESIDENTS FROM EXPOSURE. THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER, ENGINEER AND THE APPROPRIATE HAZARDOUS MATERIALS (HAZMAT) RESPONSE TEAM. THE ENGINEER WILL ISSUE INSTRUCTIONS ON PROCEEDING WITH CONSTRUCTION IN UNAFFECTED AREAS OR SUSPENDING ALL CONSTRUCTION AFTER SUCH NOTIFICATION. IF CONTAMINATION IS THE FAULT OF THE CONTRACTOR THE CONTRACTOR SHALL BEAR ALL COSTS ASSOCIATED THEREWITH.

SECTION 9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- A. LIST OF RESPONSIBLE REPRESENTATIVES/POINTS OF CONTACT: A LIST OF DESIGNATED REPRESENTATIVES/POINTS OF CONTACT SHALL BE COMPLETED AN INCLUDED AS PART OF THE CONTRACTOR'S SPCD. AT A MINIMUM, CONTACT INFORMATION SHALL BE INCLUDED FOR THE AIRPORT MANAGER, ENGINEER, CONSTRUCTION ADMINISTRATION/OBSERVATION STAFF, MODOT, CONTRACTOR'S SUPERINTENDENT, CONTRACTOR'S FOREMAN, AND FOREMAN FOR ANY SUBCONTRACTOR'S ERPRORMING WORK ON THE AIRPORT, CONTRACT INFORMATION SHALL INCLUDE PHONE NUMBERS THAT CAN BE REACHED 24 HOURS A DAY.
- B. NOTICES TO AIR MISSIONS (NOTAM): NOTAM'S ARE ISSUED BY THE LOCAL OR NEAREST FAA FLIGHT SERVICE STATION WHEN AIRPORT CONDITIONS EXIST THAT COULD ADVERSELY AFFECT THE SAFETY OF AIRCRAFT OPERATIONS, SUCH AS CONSTRUCTION ACTIVITIES WHICH REQUIRE CLOSURE OF ALL OR PARTS OF AIRPORT FACILITIES, ROUGH PAVEMENT, WEATHER-CAUSED EFFECTS, BIRD HAZARDS, OBSTRUCTIONS, ETC.

THE AIRPORT MANAGER IS RESPONSIBLE FOR FILING NOTAM'S WITH THE FAA. THE CONTRACTOR SHALL COOPERATE FULLY WITH THE AIRPORT MANAGER, PROVIDING AT LEAST 48 HOUR ADVANCE NOTICE REGARDING ANY PROJECT ACTIVITIES WHICH REQUIRE A NOTAM, FURNISHING PETINENT INFORMATION ON EFFECTIVE DATE, DIMENSIONS AND ELEVATIONS, SKETCHES OR DRAWINGS, REASON/CAUSE OF ACTION, ETC. HE SHALL ALSO ADVISE THE AIRPORT MANAGER WHEN THE AIRPORT CONDITIONS AND/OR SITUATIONS HAVE BEEN IMPROVED TO A POINT WHERE NOTAM'S MAY BE CANCELED. ANY QUESTIONS CONCERNING NOTAM COORDINATION, SCHEDULING OF WORK, SAFETY PROCEDURES, ETC. SHOULD BE RESOLVED WITH THE AIRPORT MANAGER OR ENGINEER PRIOR TO CONSTRUCTION.

- C. EMERGENCY NOTIFICATION PROCEDURES: IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911 AND ALSO NOTIFY THE AIRPORT MANAGER AND THE ENGINEER. THE CONTRACTOR SHALL INCLUDE NON-EMERGENCY CONTACT INFORMATION FOR LOCAL POLICE, FIRE, AND MEDICAL AS PART OF THE POINTS OF CONTACT LIST INCLUDED IN THE SPCD.
- D. COORDINATION WITH ARFF: IN THE EVENT OF FIRE, THE CONTRACTOR SHALL NOTIFY ARFF ON-SITE, THE AIRPORT MANAGER, AND THE ENGINEER.
- E. NOTIFICATION TO THE FAA: THE ENGINEER HAS SUBMITTED ANTICIPATED CONSTRUCTION EQUIPMENT HEIGHTS AND LOCATIONS FOR AIRSPACE REVIEW BY IDOT/FAA. LIMITATIONS ON HEIGHT AND LOCATIONS OF CONSTRUCTION EQUIPMENT ARE DETAILED ON THE CSPP DRAWING SHEET. THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER AND THE ENGINEER IF ANY DEVIATIONS FROM APPROVED AIRSPACE SUBMITTAL ARE REQUIRED. THE CONTRACTOR WILL NOT BE PERMITTED DEVIATE FROM THE APPROVED AIRSPACE SUBMITTAL UNTIL FAA APPROVAL IS RECEIVED.

SECTION 10. INSPECTION REQUIREMENTS

- A. DAILY INSPECTIONS: THE AIRPORT MANAGER AND CONTRACTOR WILL CONDUCT DAILY SAFETY INSPECTIONS TO ENSURE COMPLIANCE WITH THE CSPP. IF SIGNIFICANT SAFETY ISSUES ARE OBSERVED OR REPORTED AT OTHER TIMES BY OR TO THE AIRPORT MANAGER OR ENGINEER, MORE FREQUENT INSPECTIONS MAY BE REQUIRED UNTIL THE ISSUES ARE CORRECTED. THE CONTRACTOR WILL BEAR THE COST OF THE MORE FREQUENT INSPECTIONS UNTIL THE ISSUE IS CORRECTED. A SAMPLE DAILY INSPECTION CHECKLIST IS INCLUDED IN APPENDIX D OF FAA ADVISORY CIRCULAR 150/5370-2G, INCLUDED WITHIN THE PROJECT MANUAL.
- B. FINAL INSPECTION: THE ENGINEER AND AIRPORT MANAGER WILL CONDUCT A FINAL INSPECTION OF THE PROJECT AFTER SUBSTANTIAL COMPLETION IS REACHED. THE FINAL INSPECTION WILL NOTE ANY DEFICIENCIES OR CONCERNS THAT ARE TO BE ADDRESSED PRIOR TO ACCEPTING THE PROJECT AS PHYSICALLY COMPLETE.

SECTION 11. UNDERGROUND UTILITIES

THIS CONTRACT INCLUDES WORK THAT MAY AFFECT EXISTING AIRPORT ELECTRICAL CABLES AND POWER CIRCUITS, AS WELL AS OTHER UNDERGROUND WATER, SEWER, TELEPHONE, GAS, ELECTRICAL AND OTHER PUBLIC UTILITIES AT SEVERAL LOCATIONS ON THE AIRPORT PROPERTY. THE CONTRACTOR SHALL EXERCISE CAUTION AND PROTECT EXISTING UTILITIES TO REMAIN OPERATIONAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH UTILITY OWNERS FOR LOCATING AND MARKING THE EXACT FIELD LOCATIONS, MAINTAINING SUCH MARKING AND PROTECTION OF UTILITIES FOR THE PROJECT DURATION. THE CONTRACTOR SHALL NOTIFY UTILITY OWNERS PRIOR TO REMOVAL OF ANY EXISTING ELECTRICAL, TELEPHONE OR OTHER UTILITY SERVICES. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT MANAGER FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT UTILITIES.

SECTION 12. PENALTIES

THE CONTRACTOR AND SUBCONTRACTORS SHALL COMPLY WITH THE AIRPORT SAFETY PLAN AND THE AIRPORT SECURITY MEASURES AS STATED BY THE AIRPORT MANAGER. NON-COMPLIANCE WITH AIRPORT RULES AND REGULATIONS AND THE CSPP DRAWINGS MAY RESULT IN WORK BEING SUSPENDED UNTIL APPROPRIATE REMEDIES ARE TAKEN TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT MANAGER. ANY COSTS ASSOCIATED WITH NON-COMPLIANCE TO THE CSPP DRAWINGS SHALL SOLELY BE BORNE BY THE CONTRACTOR.

SECTION 13. SPECIAL CONDITIONS

DURING TIMES WHEN THE SAFETY OF FLIGHT/AIRCRAFT OPERATIONS COULD BE IMPAIRED, PARTICULARLY DURING IFR WEATHER OR WHEN EQUIPMENT IS IDLE, OR UPON NOTICE FROM THE AIRPORT MANAGER, ALL CRANE BOOMS, TOWERS AND OTHER MOVABLE APPENDAGES SHALL BE LOWERED TO THE MAXIMUM EXTENT.

SECTION 14. RUNWAY AND TAXIWAY VISUAL AIDS

GENERAL: THE PROJECT WILL IMPACT RUNWAY MARKINGS. THE SECTION OF EXISTING RUNWAY PAVEMENT MARKINGS TO BE REHABILITATED WILL BE REMOVED AND REPLACED TO COMPLY WITH THE FAA STANDARDS. NEW PAVEMENT MARKINGS WILL NOT BE INSTALLED UNTIL THE APPROPRIATE CURE TIME OF BITUMINOUS PAVEMENT TREATMENT WILL ALLOW FOR NEW PAVEMENT MARKINGS TO BE INSTALLED PER THE SPECIFICATIONS. EXISTING RUNWAY LIGHTING, SIGNS AND VISUAL AIDS WILL REMAIN.

SECTION 15. MARKING AND SIGNS FOR ACCESS ROUTES

THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE LAYOUT OF THE AIRFIELD AND THE REQUIRED ROUTES OF ACCESS TO THE STAGING AREA AND VARIOUS PHASES OF WORK. TEMPORARY MOVABLE SIGNS WILL BE REQUIRED ON EACH SIDE OF ANY ACTIVE TAXIWAY THE CONTRACTOR'S ACCESS/HAUL ROUTE CROSSES, IF APPLICABLE. IF THE CONTRACTOR DEEMS NECESSARY, OR AS REQUIRED BY LOCAL STANDARDS, THE CONTRACTOR MAY INSTALL OTHER TEMPORARY SIGNAGE FOR ACCESS ROUTES. FOR MOVABLE STOP SIGNS AND ANY OTHER TEMPORARY SIGNAGE THE CONTRACTOR WISHES TO INSTALL, THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS AND A SIGNAGE PLAN FOR APPROVAL USING PROCEDURES OUTLINED IN THE CONTRACT FOR SHOP DRAWING AND MATERIAL SUBMITTALS.

SECTION 16. HAZARD MARKING AND LIGHTING

PURPOSE: HAZARD MARKING, LIGHTING, AND SIGNING PREVENT PILOTS FROM ENTERING AREAS CLOSED TO AIRCRAFT, AND PREVENT CONSTRUCTION PERSONNEL FROM ENTERING AREAS OPEN TO AIRCRAFT. THE CONTRACTOR SHALL DELINEATE THE WORK LIMITS TO PREVENT PERSONNEL AND EQUIPMENT FROM ENTERING THE AIRFIELD. ADDITIONALLY, OPEN TRENCHES, EXCAVATIONS, OR OTHER HAZARDS SHALL BE APPROPRIATELY MARKED IN THE FIELD TO PREVENT DAMAGE TO PERSONS OR PROPERTY.

EQUIPMENT: LOW-PROFILE BARRICADES, TRAFFIC CONES, OR OTHER OWNER-APPROVED DEVICES SHALL BE USED TO DELINEATE THE PROJECT WORK LIMITS AND THE LIMITS THAT CONTRACTOR PERSONNEL AND EQUIPMENT ARE ALLOWED TO OPERATE WITHIN. LOW-PROFILE BARRICADES SHALL INCLUDE A FLAG AND LIGHT AND MEET THE REQUIREMENTS OF FAA AC 5370-2G (OR CURRENT ISSUE). THE EQUIPMENT SHALL BE SUFFICIENTLY WEIGHTED TO REMAIN IN PLACE WHEN SUBJECTED TO TYPICAL WINDS, PROP WASH, OR JET BLAST.

VEHICLES/EQUIPMENT WHICH OPERATING IN THE AOA SHALL BE MARKED AND LIGHTED IN ACCORDANCE WITH THIS CSPP. THE MAXIMUM EQUIPMENT HEIGHT ALLOWED ON THE AIRPORT SHALL BE AS INDICATED ON THE CSPP DRAWING SHEET. DURING TIMES WHEN THE SAFETY OF FLIGHT/AIRCRAFT OPERATIONS COULD BE IMPAIRED, PARTICULARLY DURING IFR WEATHER OR WHEN EQUIPMENT IS IDLE, ALL CRANE BOOMS, TOWERS AND OTHER MOVABLE APPENDAGES SHALL BE LOWERED TO THE MAXIMUM EXTENT.

SECTION 17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

CONSTRUCTION AREA LIGHTING WILL BE REQUIRED IF CONSTRUCTION ACTIVITIES ARE CONDUCTED DURING NIGHTTIME HOURS. ALL EQUIPMENT, EXCEPT HAUL TRUCKS, REQUIRED BY THE CONTRACTOR FOR THEIR OPERATIONS SHALL BE EQUIPPED WITH ARTIFICIAL ILLUMINATION SUFFICIENT TO SAFELY COMPLETE THE WORK. A LIGHTING PLAN MUST BE SUBMITTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO THE START OF ANY NICHTTIME WORK.

A MINIMUM OF 20 FOOT-CANDLES OF ILLUMINATION SHOULD BE PROVIDED IN THE WORK AREA. AS A PARTIAL FULFILLMENT OF THE REQUIREMENTS, THE CONTRACTOR SHALL FURNISH AND USE, COMPLETE ARTIFICIAL LIGHTING UNITS WITH A MINIMUM CAPACITY OF 3,000 WATT ELECTRIC BEAM LIGHTS, AFFIXED TO ALL EQUIPMENT IN SUCH A WAY TO DIRECT ILLUMINATION ON THE AREA UNDER CONSTRUCTION.

THE AREA LIGHTING SHALL BE AIMED DOWNWARD AND SHALL NOT BE AIMED OR REFLECTED IN SUCH A WAY TO INTERFERE WITH AIRCRAFT OPERATIONS. IF AIMING IS NOT SUFFICIENT TO PREVENT SUCH INTERFERENCE, ADDITIONAL SHIELDING SHALL BE PROVIDED IN ORDER TO MITIGATE THE IMPACTS TO AIRCRAFT OPERATIONS. THE CONTRACTOR SHALL NOT AIM AREA LIGHTING DIRECTLY ONTO PLACES OF RESIDENCE ADJACENT/NEARBY TO THE WORK AREA.

SECTION 18. PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS

WHEN ANY AIRCRAFT GROUND OPERATIONS ARE UNDERWAY WITHIN A RUNWAY OR TAXIWAY SYSTEM, CONTRACTOR'S WORK ACTIVITIES, MATERIALS, PERSONNEL, AND EQUIPMENT ARE PROHIBITED WITHIN SUCH AREAS, WHICH ARE DESIGNATED AS THE RUNWAY AND TAXIWAY, SAFETY AREAS, OBJECT FREE AREAS OR APPROACH/DEPARTURE SURFACES. ACTIVE AIRCRAFT ARE ASSUMED TO HAVE THE RIGHT-OF-WAY OVER VEHICLES, PERSONNEL, OR OTHER CONTRACTOR EQUIPMENT. WHEN WORK IS ANTICIPATED WITHIN THE RUNWAY SAFETY AREA, RUNWAY OBSTACLE FREE ZONE, OR TAXIWAY SAFETY AREA, THE CONTRACTOR SHALL CLOSE DOWN THE ASSOCIATED RUNWAY AND/OR TAXIWAY IN ACCORDANCE WITH THE CONSTRUCTION SAFETY PLAN SHEETS IN THE PROJECT CONSTRUCTION PLANS.

PROTECTION ZONES				
ITEM	DIMENSIONS			
RUNWAY SAFETY AREA (RSA)	8,799' X 500'			
RUNWAY OBJECT FREE AREA (ROFA)	8,799' X 800'			
TAXIWAY SAFETY AREA (TSA)	171' WIDE			
TAXIWAY OBJECT FREE AREA (TOFA)	259' WIDE			
RUNWAY OBSTACLE FREE ZONE (ROFZ)	8,799' X 200'			
RUNWAY PROTECTION ZONE (RPZ)	500' X 1,010' X 1,700'			

SECTION 19. OTHER LIMITATIONS ON CONSTRUCTION

PROHIBITIONS: THE MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT WITHIN THE PROJECT LIMITS IS EXPECTED TO BE NO HIGHER THAN 25 FEET AT ANY GIVEN LOCATION. EQUIPMENT EXCEEDING THESE HEIGHTS WILL REQUIRE THAT THE AIRPORT FILE FAA FORM 7460-1, NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION, AND RECEIPT OF FAA APPROVAL. CONTRACTOR SHALL COORDINATE EQUIPMENT HEIGHTS WITH THE AIRPORT PRIOR TO THE START OF CONSTRUCTION TO ALLOW ADEQUATE TIME FOR FAA REVIEW.

SMOKING IS NOT PERMITTED INSIDE THE AOA FENCE.

OPEN FLAME WELDING AND TORCH CUTTING OPERATIONS ARE NOT PERMITTED UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED AND THESE OPERATIONS ARE AUTHORIZED BY THE AIRPORT.

WORK HOURS: WORK WILL NOT BE ALLOWED AT NIGHT EXCEPT AS REQUIRED BY THE CONTRACT DOCUMENTS OR APPROVED BY THE AIRPORT.

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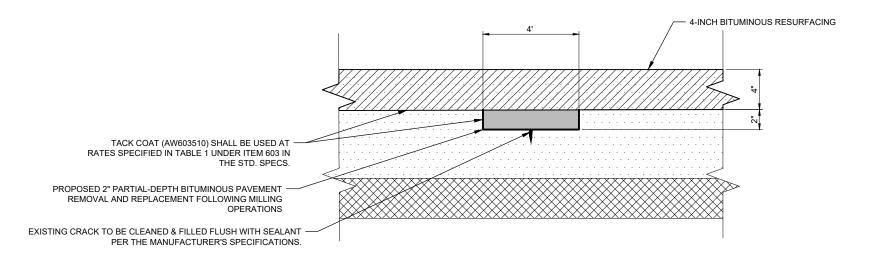
IDA #: DEC-5216 FAA #: 3-17-0033-TBD

NO. DATE DESCRIPTION
DES DWN REV
ISSUE: JUNE 13, 2025
PROJECT NO: 20A0079

CAD FILE: C-101-CSPP.DWG DESIGN BY: HLE 2/3/2025 DRAWN BY: HLE 2/3/2025 REVIEWED BY: MJD 2/4/2025

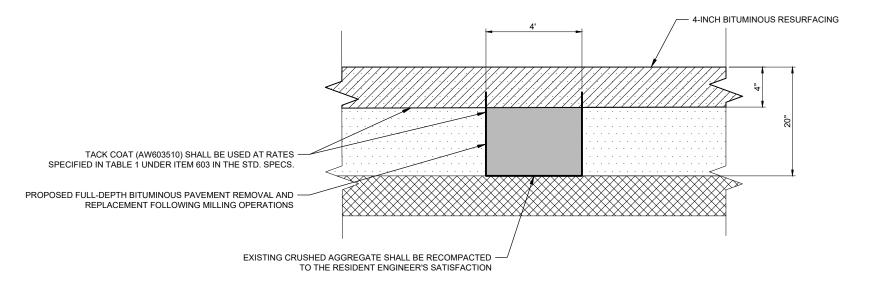
SHEET TITLE

CONSTRUCTION SAFETY DETAILS AND NOTES - SHEET 3



AY401915 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE A (PARTIAL DEPTH)

NOT TO SCALE



AW401916 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE B (FULL DEPTH)

NOT TO SCALE

EXISTING	PROPOSED	LEGEND
		BIT. PAVEMENT
		BIT. PAVEMENT MILLING (VARIABLE)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE

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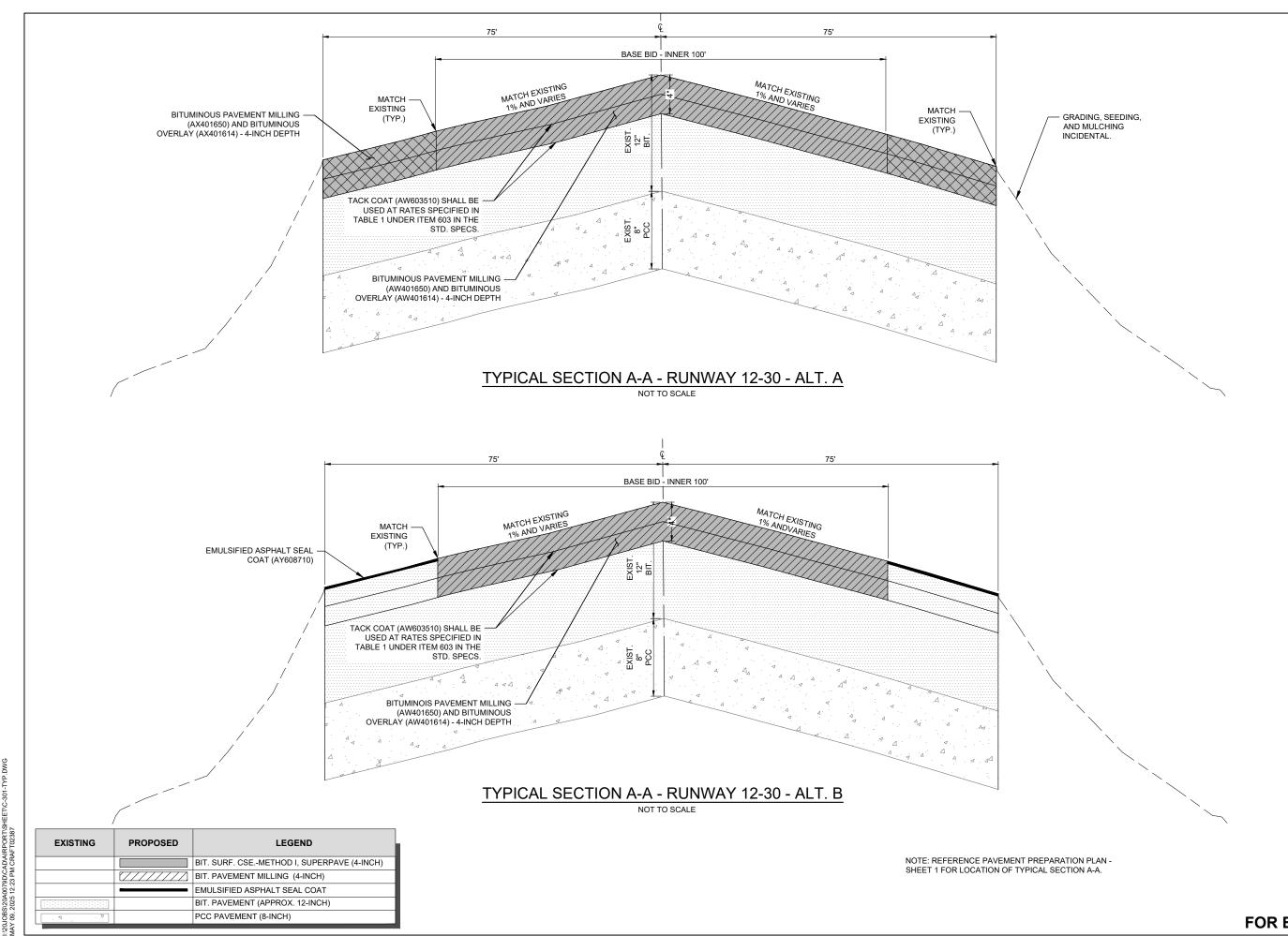
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PROJECT NO: 20A0079						
CAD FILE: C-301-TYP.DWG						
1	DESIGN BY: HLE 1/3/2025					
1	DRAWN BY: HLE 1/3/2025					

PAVEMENT PATCHING DETAILS

SHEET TITLE

REVIEWED BY: MJD 1/28/2025



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CAD FILE: C-301-TYP.DWG					
DESIGN BY: HLE 1/3/2025					
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SHEET TITLE

TYPICAL SECTION -**RUNWAY 12-30**

REVIEWED BY: MJD 1/28/2025

ELECTRICAL NOTES

- THE CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. THE CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.
- WHEN A RUNWAY IS CLOSED THE RESPECTIVE LIGHTING AND NAVAIDS SHALL BE SHUT OFF. WHEN A TAXIWAY IS CLOSED THE RESPECTIVE LIGHTING SYSTEM SHALL BE SHUT OFF.
- 3. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN.

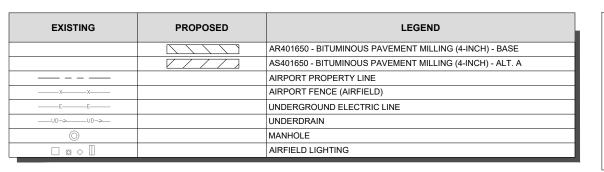
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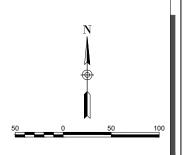
CLEAN & SEAL BITUMINOUS CRACKS

1. A NOMINAL QUANTITY OF AW201661 "CLEAN AND SEAL BITUMINOUS CRACKS" HAS BEEN INCLUDED AND WILL BE LAID OUT BY THE RESIDENT ENGINEER/TECHNICIAN FOLLOWING PAVEMENT MILLING. THE NOMINAL QUANTITY WAS DETERMINED BY A FIELD SURVEY (CONDUCTED ON 01/04/21) AND ASSUMES THAT 50% OF SURVEYED CRACKS WILL BE ELIMINATED AFTER THE COMPLETION OF MILLING. IN ADDITION, THE NOMINAL QUANTITY ASSUMES SURVEYED AREAS REQUIRING A TYPE A, PARTIAL DEPTH REPAIR (AW401915) WILL BECOME CRACKS UNDER AW201661.

BITUMINOUS PAVEMENT MILLING

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- 3. ALL MILLED MATERIAL SHALL BE HAULED OFFSITE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN





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IDA #: DEC-5216 FAA #: 3-17-0033-TBD

	NO.	DATE	DESCRIPTION		
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PROJECT NO: 20A0079					
CAD FILE: C-111-DEM.DWG					

CAD FILE: C-111-DEM.DWG
DESIGN BY: HLE 1/3/2025
DRAWN BY: HLE 1/3/2025
REVIEWED BY: MJD 1/28/2025

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SHEET TITLE

PAVEMENT PREPARATION PLAN -SHEET 1

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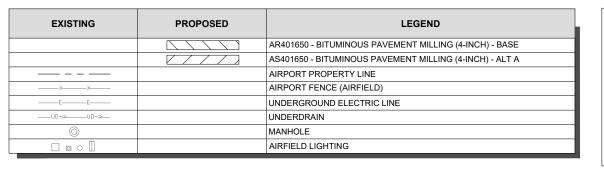
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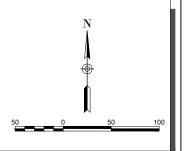
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DESIGN BY: HLF 1/3/2025

DRAWN BY: HLE 1/3/2025

REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PAVEMENT PREPARATION PLAN -SHEET 2

11

ELECTRICAL NOTES

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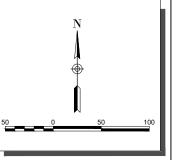
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EXISTING	PROPOSED	LEGEND		
		AR401650 - BITUMINOUS PAVEMENT MILLING (4-INCH) - BASE		
		AS401650 - BITUMINOUS PAVEMENT MILLING (4-INCH) - ALT A		
		AIRPORT PROPERTY LINE		
XX		AIRPORT FENCE (AIRFIELD)		
———E————E———		UNDERGROUND ELECTRIC LINE		
——UD->——UD->—		UNDERDRAIN		
0		MANHOLE		
		AIRFIELD LIGHTING		



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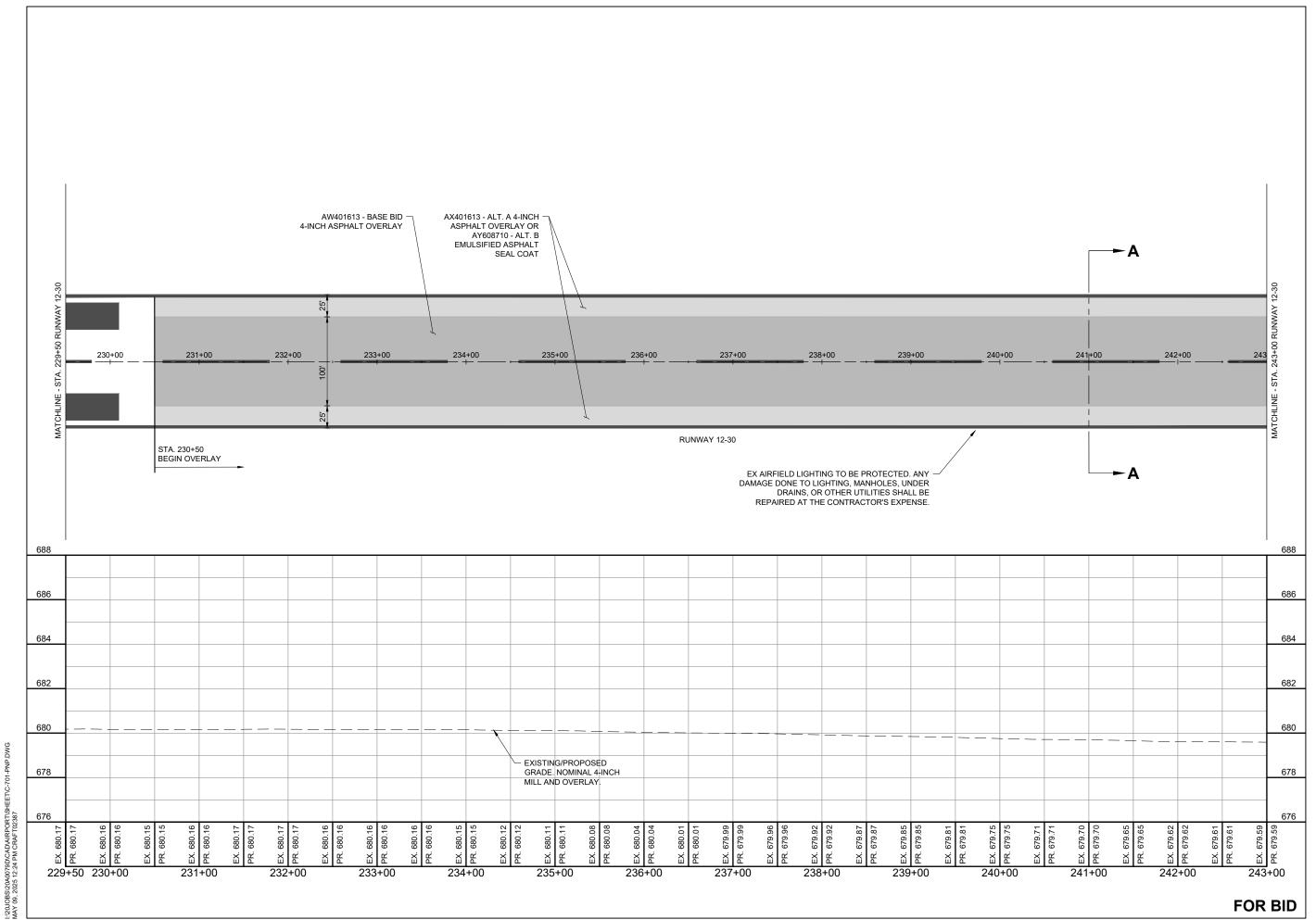
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	ISSUE:	5			
PROJECT NO: 20A0079					
CAD FILE: C-111-DEM.DWG					

DESIGN BY: HLE 1/3/2025 DRAWN BY: HLE 1/3/2025 REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PAVEMENT PREPARATION PLAN -SHEET 3

12



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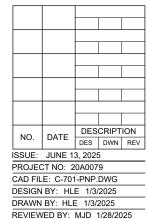
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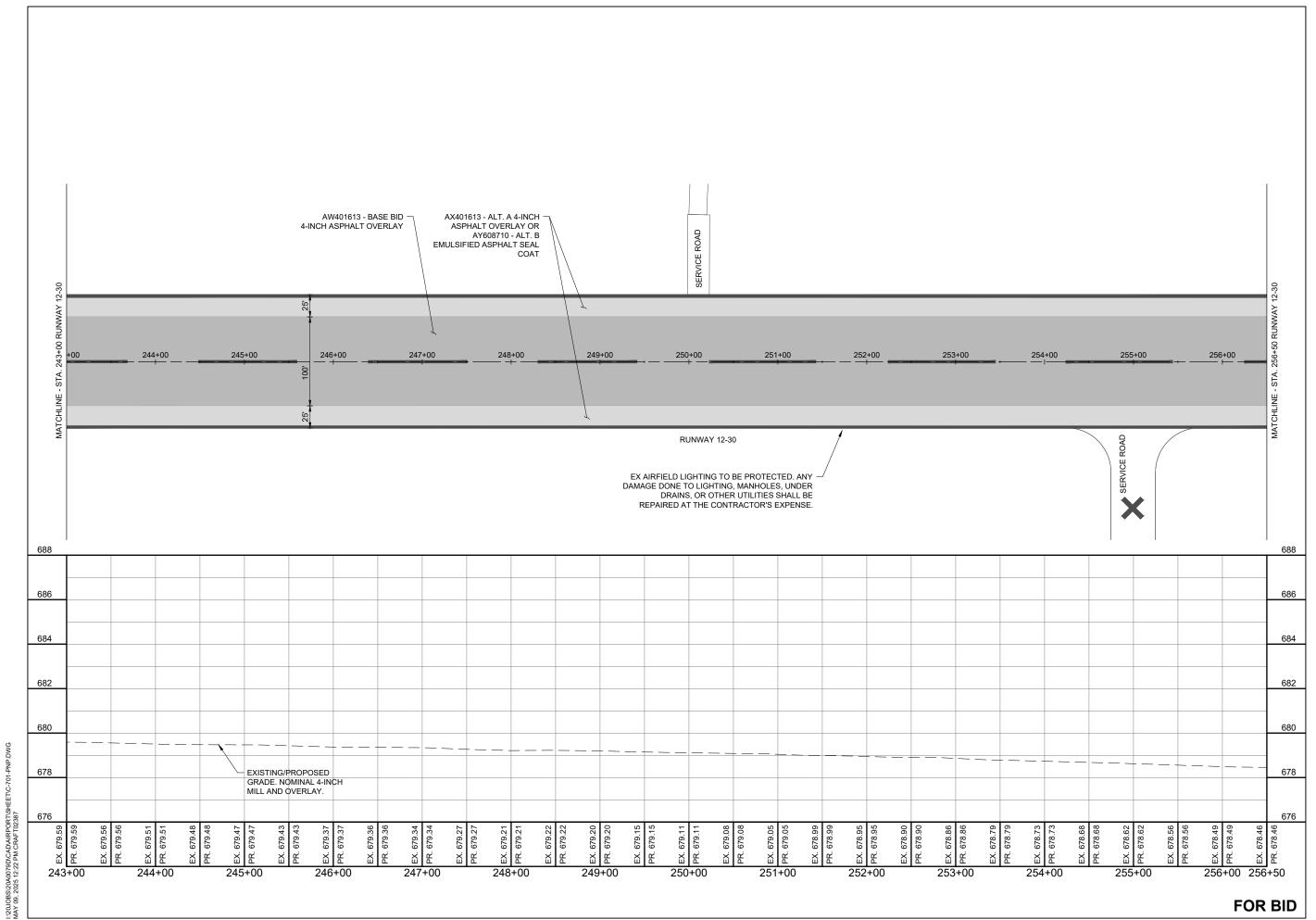
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SHEET TITLE

PROPOSED PLAN AND PROFILE -SHEET 1



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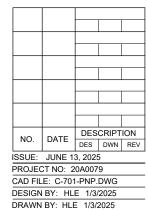


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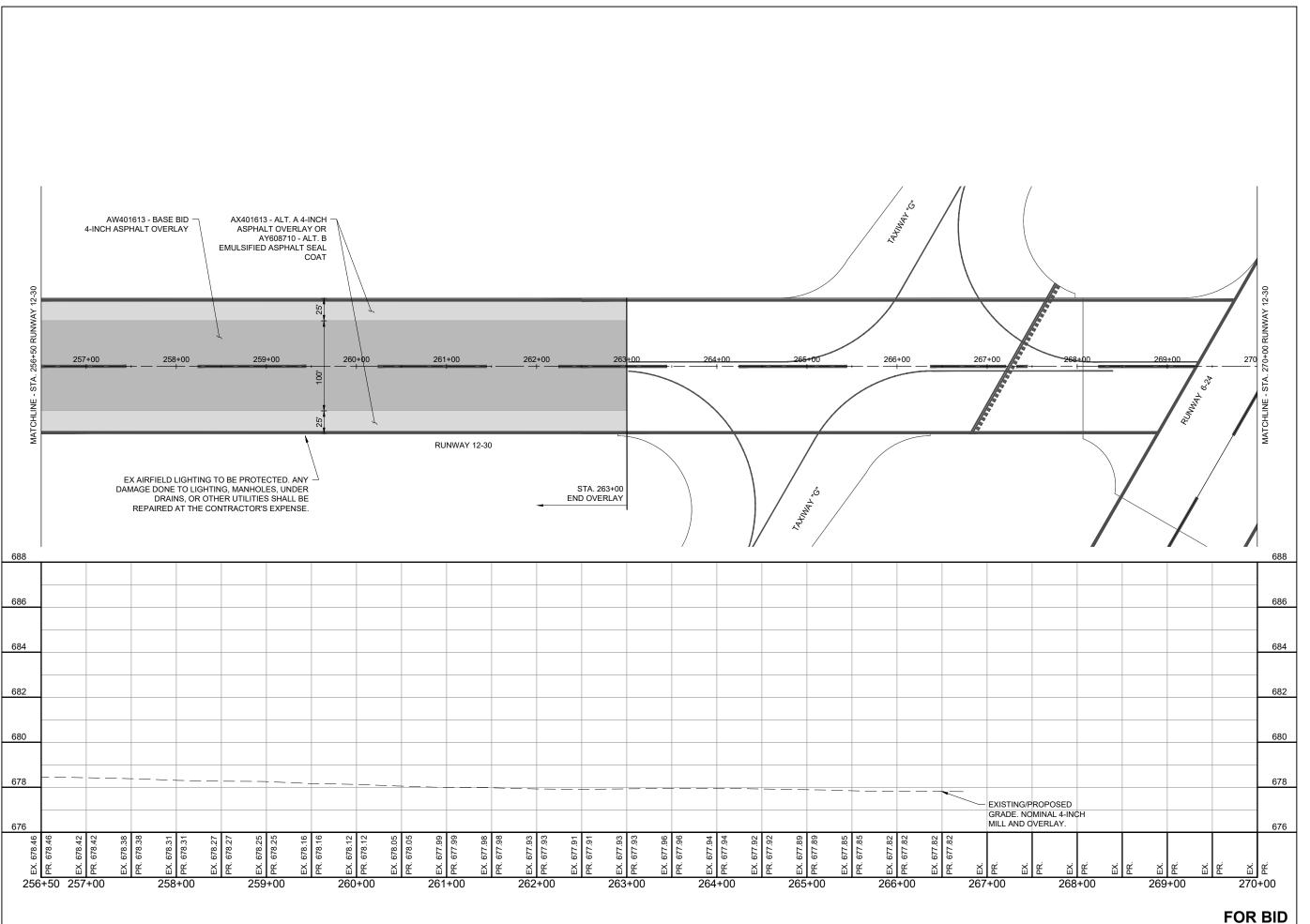
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REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PROPOSED PLAN AND PROFILE -SHEET 2



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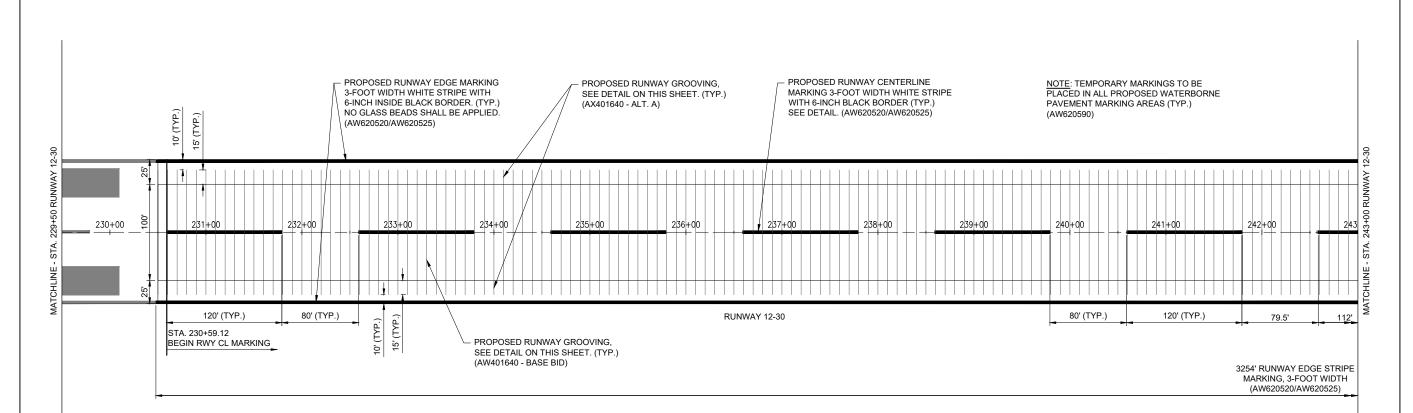
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	NO.	DATE	DES	DWN	REV
ISSUE: JUNE 13, 2025					
PROJECT NO: 20A			0A007	9	
CAD FILE: C-701-PNP.DWG					
	DESIGN BY: HLE 1/3/2025				

SHEET TITLE

PROPOSED PLAN AND PROFILE -SHEET 3

DRAWN BY: HLE 1/3/2025 REVIEWED BY: MJD 1/28/2025

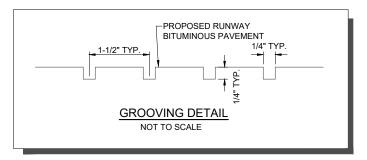


WATERBORNE PAVEMENT MARKING NOTES

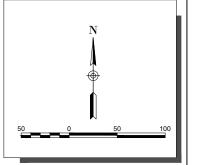
- A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED.
- WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
- GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT. GLASS BEADS SHALL NOT BE APPLIED TO RUNWAY EDGE STRIPES.
- 4. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
- 5. THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

BITUMINOUS GROOVING NOTES

- THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
- 2. THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP AND PLACED ON 1 1/2" CENTERS
- TOLERANCES ON THE GROOVING WILL BE 1 1/2" IN ALIGNMENT FOR 75', ±1/16" IN DEPTH, +1/16" IN WIDTH, -1/8" IN GROOVE SPACING.
- GROOVES SHALL BE SAWED NO LESS THAN 6" AND NO MORE THAN 18" FROM AN IN-PAVEMENT LIGHT.
- ALL SOLID MATERIAL GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND DISPOSED OF AT AN APPROVED LAND FILL OFF THE AIRPORT SITE, AT THE CONTRACTOR'S OWN EXPENSE.
- AT THE END OF EACH WORK DAY THE RUNWAY WILL BE FLUSHED, CLEANED AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.



MARKING SCHEDULE - BASE BID						
AR620520 - PAVEMENT MARKING - WATERBORNE (WHITE)						
RUNWAY 12-30 CENTERLINE STRIPES (120' X 3')	360	13	4680			
RUNWAY 12-30 CENTERLINE STRIPES (112' X 3')	336	4	1344			
RUNWAY 12-30 EDGE STRIPES (3254' X 3')	9762	2	19524			
AR620520 - PAVEMENT MARK	ING - WATERBORN	E (YELLOW)	1			
DECORIDATION	UNIT AREA	NO.	TOTAL AREA			
DESCRIPTION	(S.F.)	REQUIRED	(S.F.)			
12" TAXIWAY CENTERLINES - TWY G	209	1	209			
(SOUTHWEST)						
12" TAXIWAY CENTERLINES - TWY G (NORTHEAST)	31	1	31			
AR620525 PAVEMENT M	ARKING - BLACK B	ORDER	•			
DESCRIPTION	UNIT AREA	NO.	TOTAL AREA			
DESCRIPTION	(S.F.)	REQUIRED	(S.F.)			
TAXIWAY CENTERLINES	242	1	242			
RUNWAY 12-30 CENTERLINE STRIPES (120' X 3')	124	13	1612			
RUNWAY 12-30 CENTERLINE STRIPES (112' X 3')	116	4	464			
RUNWAY 12-30 EDGE STRIPES	1627	2	3254			



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Decatur Park District Decatur Airport 910 South Airport Road Decatur, IL 62521



SIGNED: 04/18/2025 EXPIRES: 11/30/2025

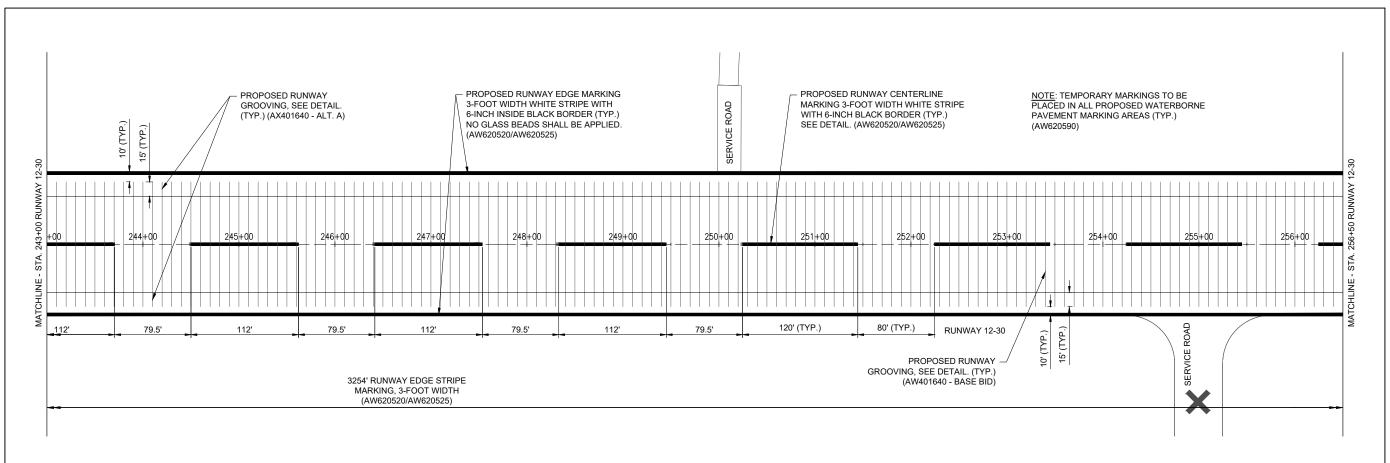
IDA #: DEC-5216 FAA #: 3-17-0033-TBD

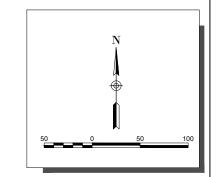
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	NO.	DATE	DES	DWN	REV				
	ISSUE:	JUNE 1	3, 2025	5					
PROJECT NO: 20A0079									
	CAD FILE: C-102-MRK.DWG								
	DESIGN BY: HLE 1/3/2025								
	DRAWN	BY: HLE	1/3/2	DRAWN BY: HLE 1/3/2025					

REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PROPOSED MARKING AND GROOVING PLAN - SHEET 1





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DATE LICENSE SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216 FAA #: 3-17-0033-TBD

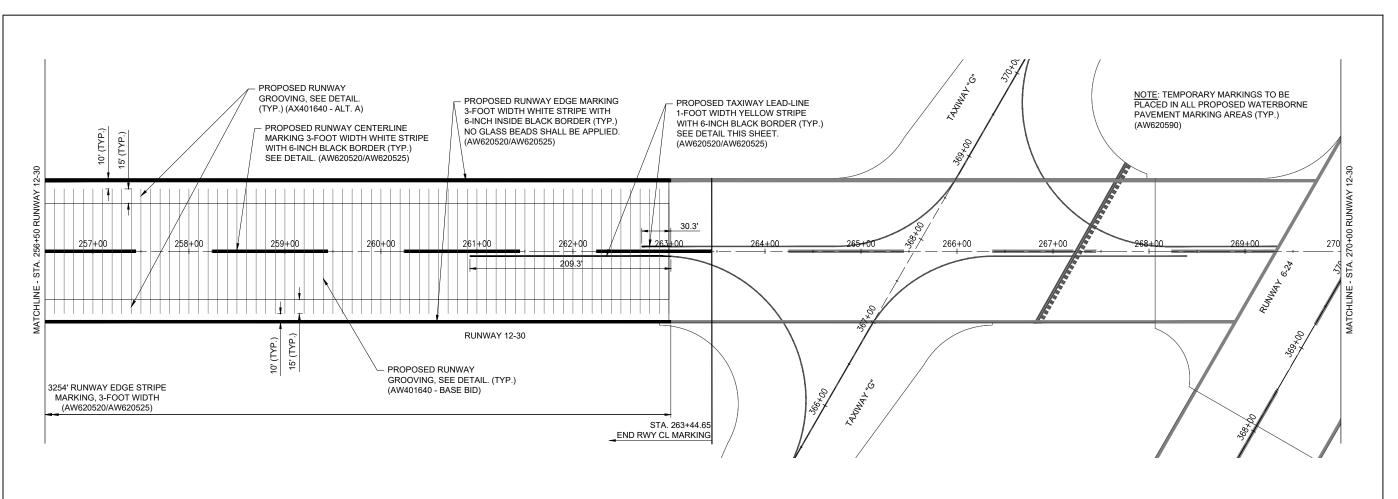
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į	ISSUE: JUNE 1		3, 2025			
j	PROJEC	0A007	9			
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i	DESIGN BY: HLE 1/3/2025					

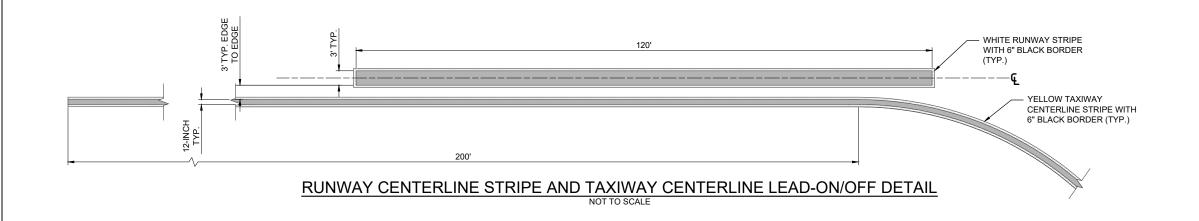
REVIEWED BY: MJD 1/28/2025

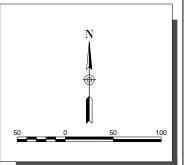
DRAWN BY: HLE 1/3/2025

SHEET TITLE

PROPOSED MARKING AND GROOVING PLAN - SHEET 2







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DATE LICENSE SIGNED: 04/18/2025 EXPIRES: 11/30/2025

IDA #: DEC-5216 FAA #: 3-17-0033-TBD

NO.	DATE	DES	CRIPTION		
NO.	DATE	DES	DWN	REV	
ISSUE:	JUNE 1	3, 2025	5		
PROJEC	CT NO: 2	0A007	9		
CAD FILE: C-102		2-MRK	.DWG		
DESIGN	E 1/3/	2025			
DRAWN	BY: HLE	E 1/3/	2025		

REVIEWED BY: MJD 1/28/2025

SHEET TITLE

PROPOSED MARKING AND GROOVING PLAN - SHEET 3

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