

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0365	2024-1031-RS	KANE	35	1
		ILLINOIS	CONTRACT NO. 62X69	

D-91-060-25



FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF NORTH AURORA AND THE CITY OF AURORA

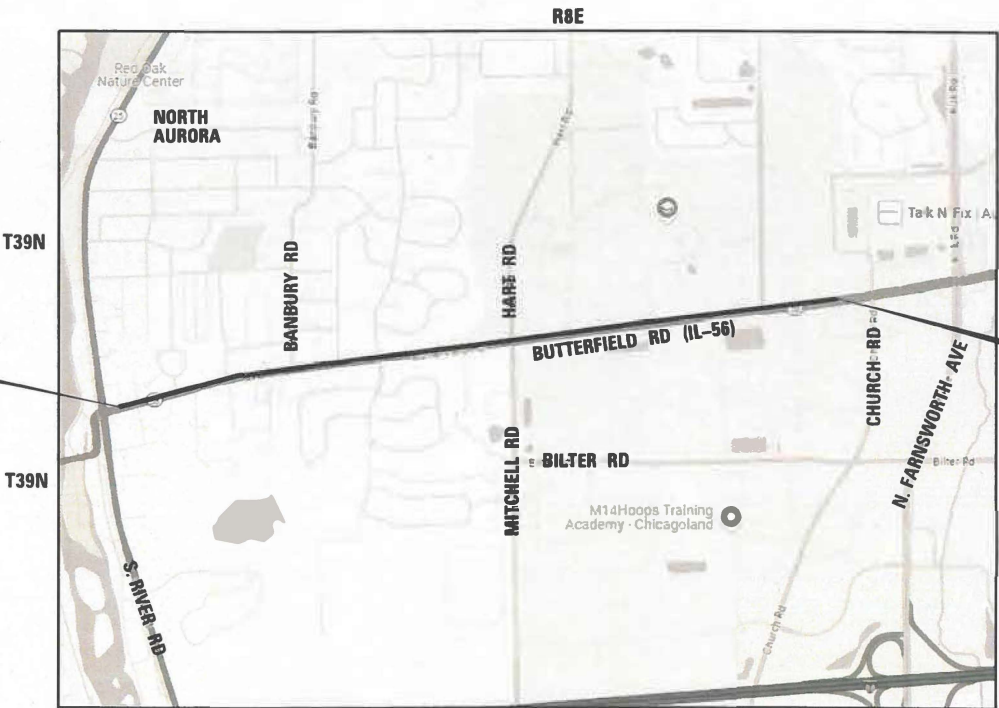
PROPOSED HIGHWAY PLANS

FAP ROUTE 0365: IL 56 (BUTTERFIELD ROAD)
EAST OF ILL 25 (RIVER ROAD) TO WEST OF CHURCH ROAD
SECTION: 2024-1031-RS
SMART & DESIGNED OVERLAY, ADA IMPROVEMENTS,
SHOULDER WIDENING
KANE COUNTY
C-91-080-25

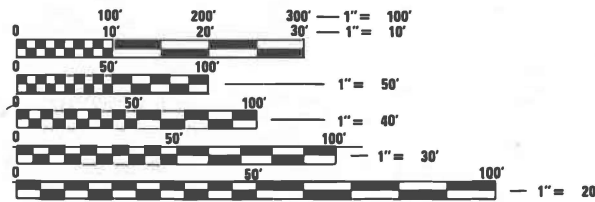
TRAFFIC DATA

2023 ADT (IL-25 TO HART RD) = 11,900 VPD
2023 ADT (HART RD TO WEST OF CHURCH RD) = 15,300 VPD
POSTED SPEED LIMIT = 35 MPH - 50 MPH
OTHER PRINCIPAL ARTERIAL

PROJECT BEGINS
STA 13 + 99



PROJECT ENDS
STA 108 + 27



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62X69

GROSS LENGTH = 9,428 FT. = 1.79 MILE
NET LENGTH = 9,428 FT. = 1.79 MILE



Alexander Carl Lane
ALEXANDER CARL LANE, P.E.
IL LIC. NO. 062-063261
EXP: 11/30/2025
DATE: 03/05/2025

THIS SEAL AND SIGNATURE
PERTAINS TO SHEETS 1 TO 10

INFRASTRUCTURE
ENGINEERING | INCORPORATED
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P 312.425.9560 | F 312.425.9564 | www.infrastructure-eng.com
CONTACT: ALEXANDER LANE (312) 477-0620

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 18th 2025 IR

REGIONAL ENGINEER
May 9 2025
ENGINEER OF DESIGN AND ENVIRONMENT

DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION
May 9 2025

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>TITLE</u>
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
7-10	ROADWAY AND PAVEMENT MARKING PLANS
11-17	TS-05: STANDARD TRAFFIC SIGNAL DESIGN DETAILS
18	TS-07: DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
19	DETECTOR LOOP REPLACEMENT PLAN - IL 56 (BUTTERFIELD ROAD) AT HART ROAD & MITCHELL ROAD
20-21	TRAFFIC SIGNALS - IL 56 (BUTTERFIELD ROAD) AT RADDANT ROAD
22	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
23	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
24	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
25	BD-32: BUTT JOINT AND HMA TAPER DETAILS
26	PD-01: PROJECT DETAILS FOR SINGLE PREPENDICULAR CURB RAMPS
27	PD-02: PROJECT DETAILS FOR SINGLE PREPENDICULAR CURB RAMPS
28	PD-04: PROJECT DETAILS FOR SINGLE PREPENDICULAR CURB RAMPS
29	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
30	TC-11: TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
31	TC-13: TYPICAL PAVEMENT MARKINGS
32	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
33	TC-16: SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
34	TC-22: ARTERIAL ROAD INFORMATION SIGN
35	TC-26: DRIVEWAY ENTRANCE SIGNING

HIGHWAY STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-12	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
701101-05	OFF-RD OPERATIONS, MULTILANE 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15 FT (4.5M) AWAY
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

1. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT PERMISSION FROM THE DEPARTMENT.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
3. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
5. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
6. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
7. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
9. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER.
10. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
11. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
12. THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, VIA EMAIL AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PACEMENT MARKINGS.
13. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXCAT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
14. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITAION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED IN THE CONTRACT SPECIFICATIONS.
15. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
16. THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.
17. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
18. WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. DROP-OFFS GREATER THAN (OR GREATER THAN OR EQUAL TO) 12" WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE TRAVEL LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN 12". THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.
19. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
20. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.

 <div>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.8500 F 312.425.9594 www.infrastructure-eng.com</div>	USER NAME = ALane	DESIGNED - HA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - HA	REVISED -			365	2024-1031-RS	KANE	35	2
		CHECKED - ACL	REVISED -			CONTRACT NO. 62X69				
	PLOT DATE = 4/15/2025	DATE = 03/07/2025	REVISED -			SCALE: NTS	SHEET 1 OF 1 SHEETS	STA. TO STA.		

				CONSTRUCTION CODE		
				0005 ROADWAY		0021 TRAFFIC SIGNAL
				100% STATE	100% STATE	100% STATE
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY			
20101400	NITROGEN FERTILIZER NUTRIENT	POUND	3	3		
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3	3		
20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	3	3		
20200100	EARTH EXCAVATION	CU YD	4,890	4,890		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	880	880		
21101615	TOP SOIL FURNISH AND PLACE, 4"	SQ YD	7	7		
25000115	SEEDING, CLASS 1B	ACRE	0.1	0.1		
25200110	SODDING, SALT TOLERANT	SQ YD	7	7		
25200200	SUPPLEMENTAL WATERING	UNIT	2	2		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	2	2		
28000510	INLET FILTERS	EACH	2	2		
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	295	295		
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	8,789	8,789		
35101400	AGGREGATE BASE COURSE, TYPE B	TON	32	32		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	28,240	28,240		
40600370	LONGITUDINAL JOINT SEALANT	FOOT	28,284	28,284		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	76	76		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	536	536		
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	2,824	2,824		
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	4,915	4,915		
42001300	PROTECTIVE COAT	SQ YD	260	260		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1,997	1,997		
42400800	DETECTABLE WARNINGS	SQ FT	130	130		

* = SPECIALTY ITEM

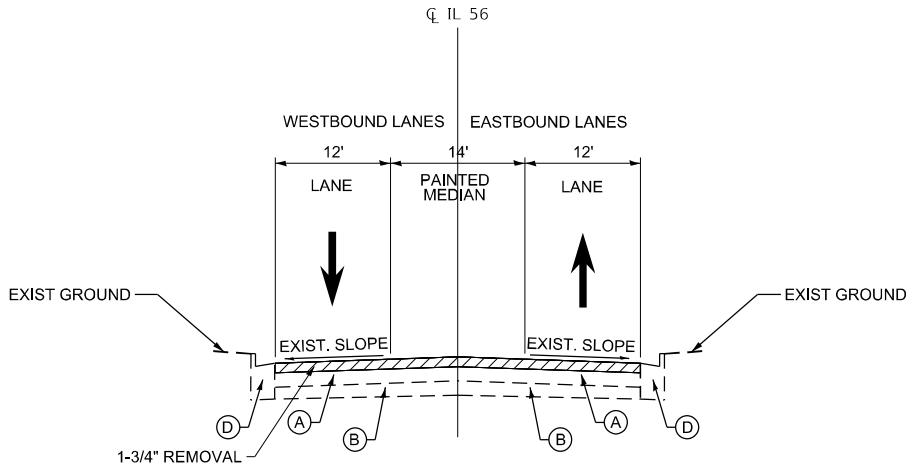
				CONSTRUCTION CODE		
				0005 ROADWAY		0021 TRAFFIC SIGNAL
				100% STATE	100% STATE	100% STATE
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY			
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	24,946	24,946		
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	25,208	25,208		
44000600	SIDEWALK REMOVAL	SQ FT	1,986	1,986		
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	7	7		
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	97	97		
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	58	58		
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	39	39		
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	7	7		
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	96	96		
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	58	58		
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	39	39		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1,465	1,465		
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	8,789	8,789		
56109210	WATER VALVES TO BE ADJUSTED	EACH	1	1		
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1		
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1		
67100100	MOBILIZATION	L SUM	1	1		
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	4,890	4,890		
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	7	7		
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1		
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	14	14		
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		

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<div></div> <div>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9560 F 312.425.9560 www.infrastructure-eng.com</div>	USER NAME = HAlsayed	DESIGNED - HA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - HA	REVISED -						365	2024-1031-RS	KANE	35	3
		CHECKED - ACL	REVISED -						CONTRACT NO. 62X69				
	PLOT DATE = 3/22/2025	DATE - 03/07/2025	REVISED -		SCALE: NTS		SHEET 1 OF 2 SHEETS	STA. TO STA.		ILLINOIS FED. AID PROJECT			

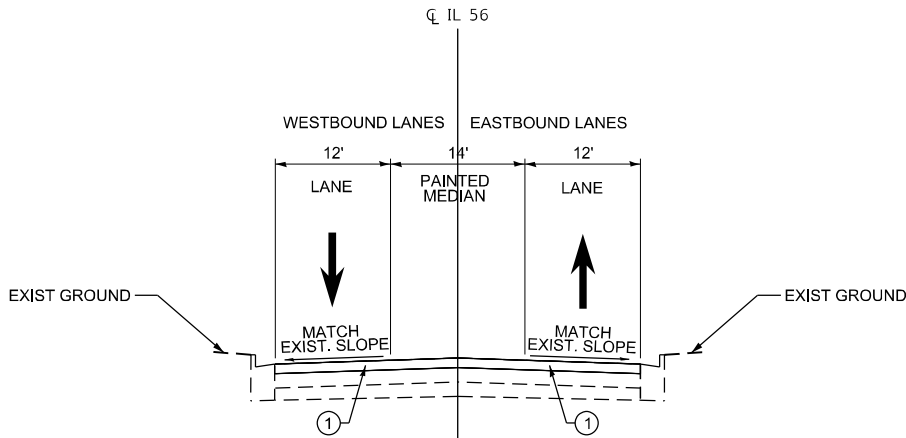
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				0005 ROADWAY		0021 TRAFFIC SIGNAL
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	100% STATE	100% STATE	100% STATE
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1		
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	13,715	13,715		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ. FT.	4,572	4,572		
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ. FT.	1,190	1,190		
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	45,488	45,488		
70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	5,519	5,519		
70300261	TEMPORARY PAVEMENT MARKING - LINE 12" - PAINT	FOOT	733	733		
70300281	TEMPORARY PAVEMENT MARKING - LINE 24" - PAINT	FOOT	346	346		
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ. FT.	1,190	1,190		
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	45,488	45,488		
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5,519	5,519		
78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	470	470		
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	733	733		
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	346	346		
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	723	723		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	723	723		
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2			2
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	261			261
88600100	DETECTOR LOOP, TYPE I	FOOT	1,326			1,326
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1			1

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EXISTING TYPICAL CROSS SECTION

SMART OVERLAY
STA. 13+99 TO STA. 16+65



PROPOSED TYPICAL CROSS SECTION

SMART OVERLAY
STA. 13+99 TO STA. 16+65

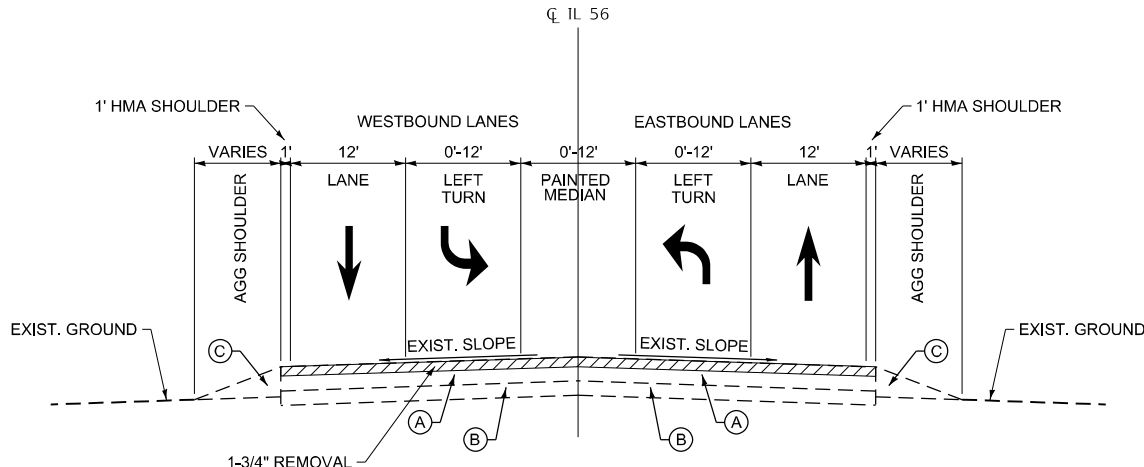
LEGEND

- (A) EXISTING HOT-MIX ASPHALT, +/- 11-1/2"
- (B) EXISTING PCC PAVEMENT (7")
- (C) EXISTING AGGREGATE SHOULDER
- (D) EXISTING PCC CURB AND GUTTER
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1-3/4"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- (3) HOT-MIX ASPHALT SHOULDER, 8"
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (5) AGGREGATE WEDGE SHOULDER, TYPE B

 HOT-MIX ASPHALT REMOVAL

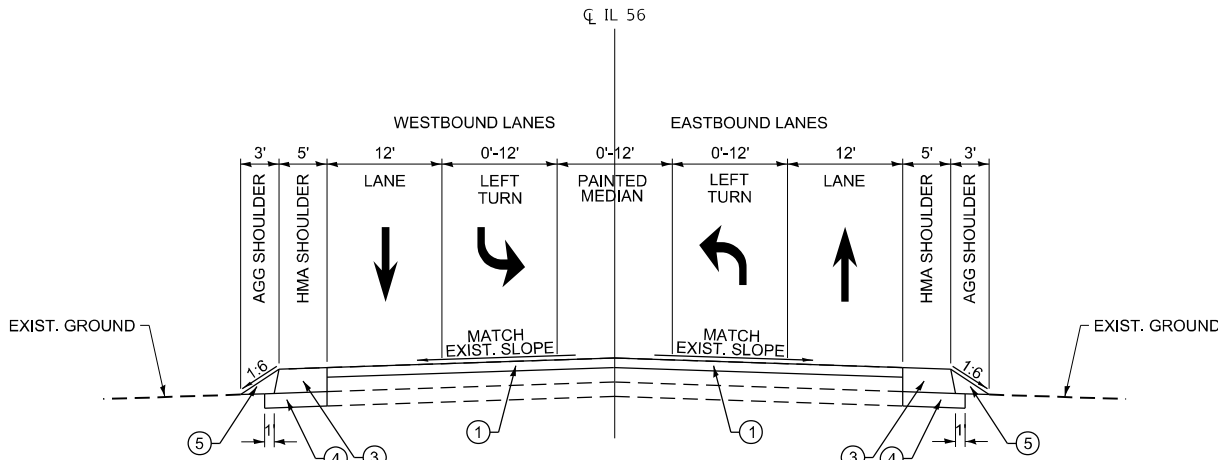
NOTES

- THE CONTRACTOR SHALL MILL FIRST THEN PATCH
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE. STA 13+99 TO STA 65+17
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HMA BINDER COURSE IL-9.5 N70. STA 65+17 TO STA 108+27



EXISTING TYPICAL CROSS SECTION

SMART OVERLAY
STA 16+65 TO STA 43+52



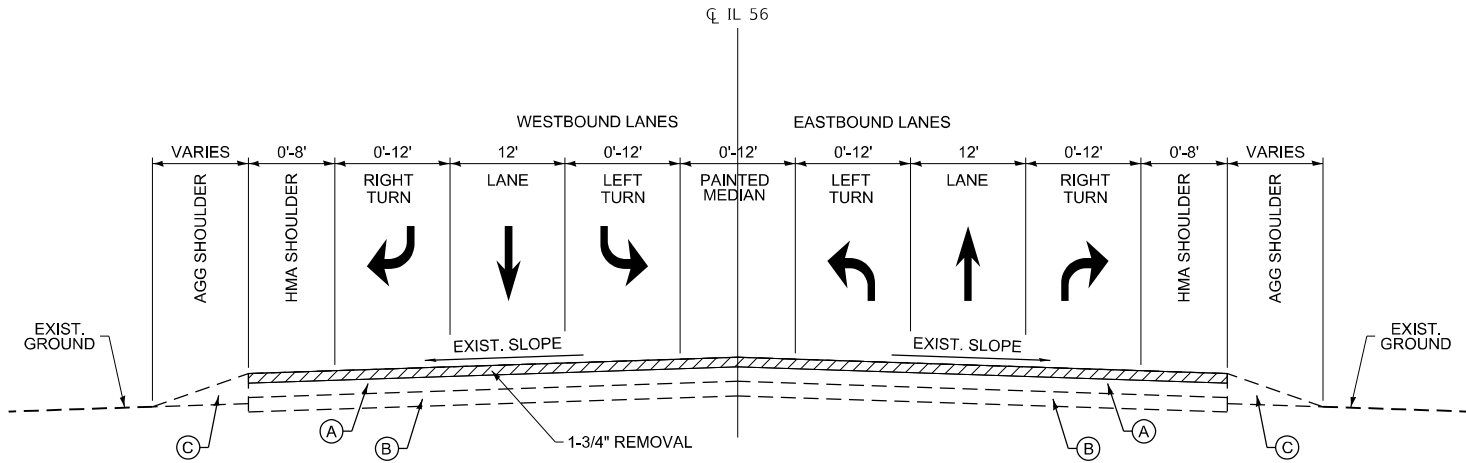
PROPOSED TYPICAL CROSS SECTION

SMART OVERLAY
STA 16+65 TO STA 43+52

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
SMART OVERLAY - STA 13+99 TO STA 65+17		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1-3/4"	4.0% @ 70 GYR	QCP
DESIGNED OVERLAY - STA 65+17 TO STA 108+27		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1-3/4"	4.0% @ 70 GYR	QCP
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"	4.0% @ 70 GYR	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4.0% @ 70 GYR	QC/QA
HMA SHOULDER 8"		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1-3/4"	4.0% @ 70 GYR	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70, 6-1/4"	4.0% @ 70 GYR	QC/QA
TEMPORARY RAMP, SPECIAL		
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, VARIABLE DEPTH	4.0% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

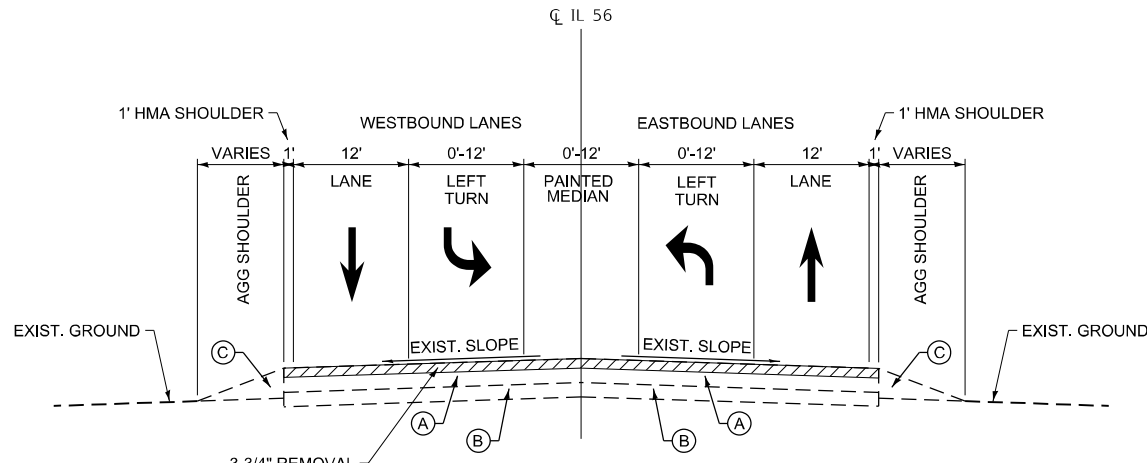
MIXTURE REQUIREMENT NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.



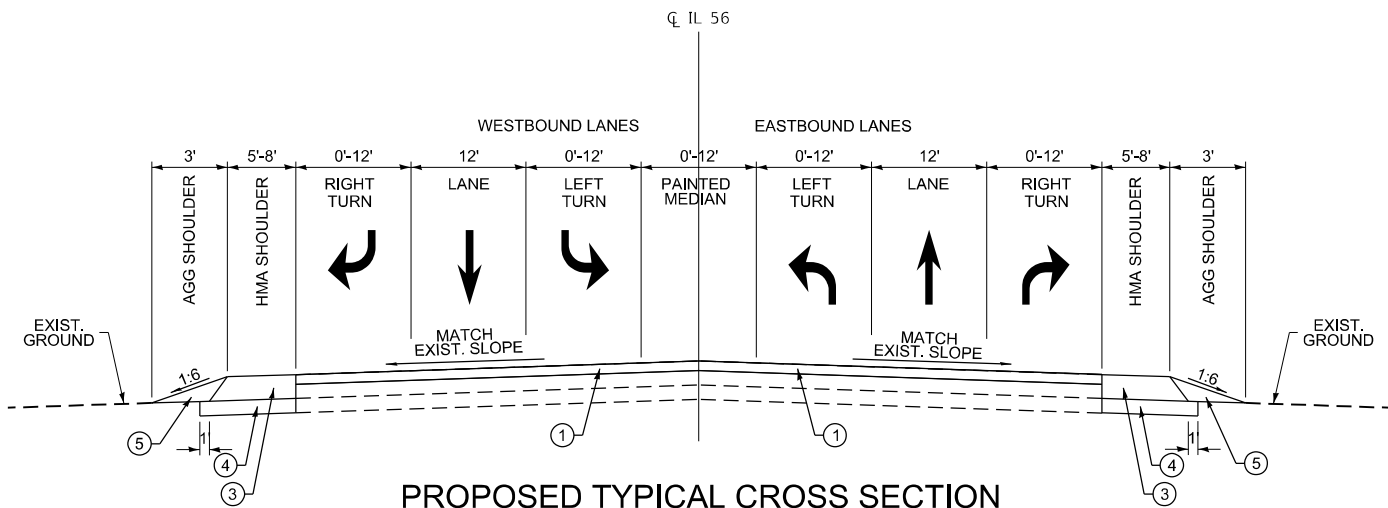
EXISTING TYPICAL CROSS SECTION

SMART OVERLAY
STA. 43+52 TO STA. 65+17



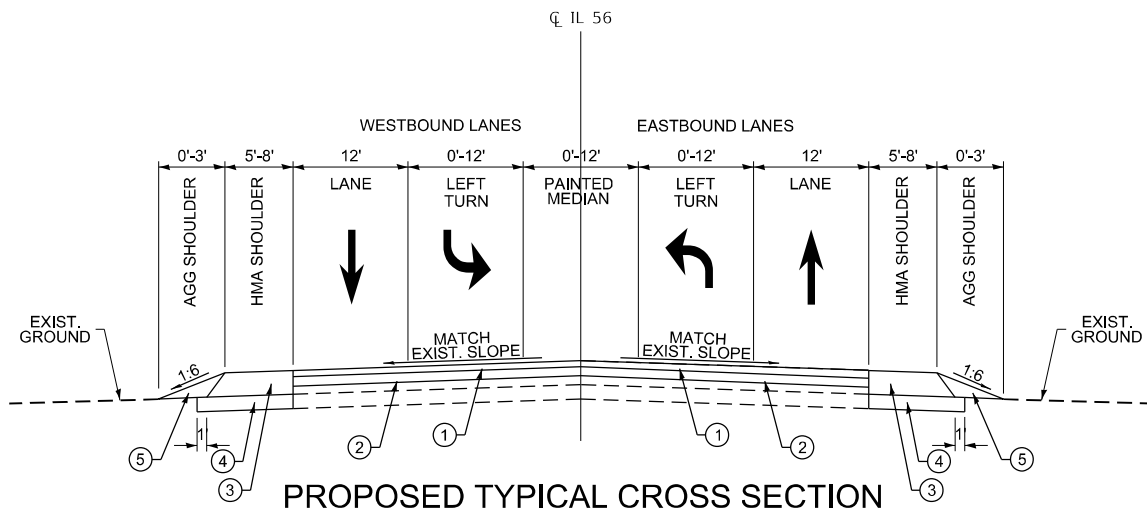
EXISTING TYPICAL CROSS SECTION

DESIGNED OVERLAY
STA 65+17 TO STA 108+27



PROPOSED TYPICAL CROSS SECTION

SMART OVERLAY
STA. 43+52 TO STA. 65+17



PROPOSED TYPICAL CROSS SECTION

DESIGNED OVERLAY
STA 65+17 TO STA 108+27

LEGEND

- (A) EXISTING HOT-MIX ASPHALT, +/- 11-1/2"
- (B) EXISTING PCC PAVEMENT (7")
- (C) EXISTING AGGREGATE SHOULDER
- (D) EXISTING PCC CURB AND GUTTER
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1-3/4"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- (3) HOT-MIX ASPHALT SHOULDER, 8"
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (5) AGGREGATE WEDGE SHOULDER, TYPE B



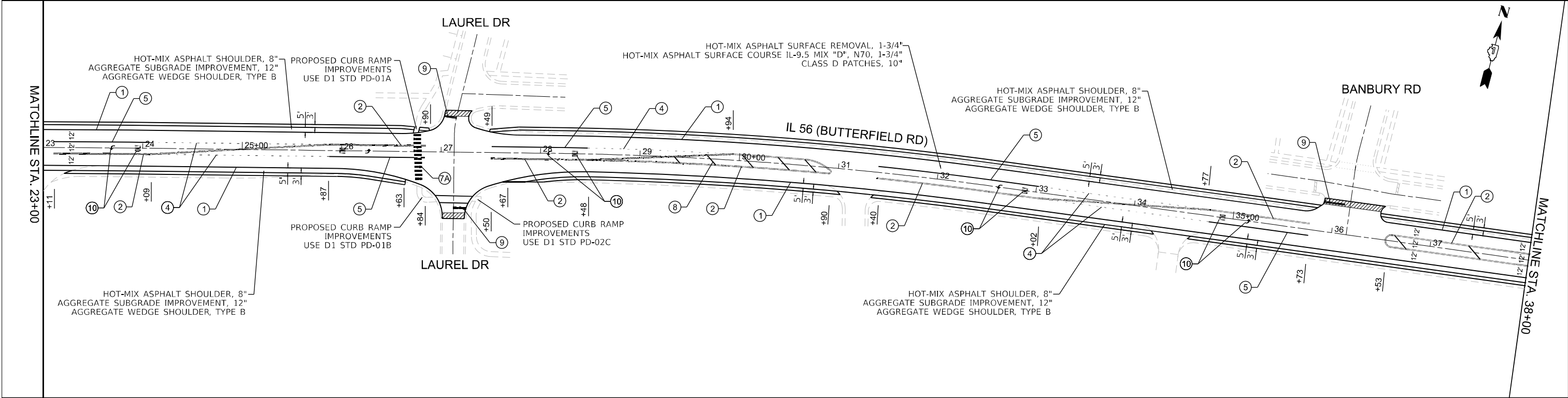
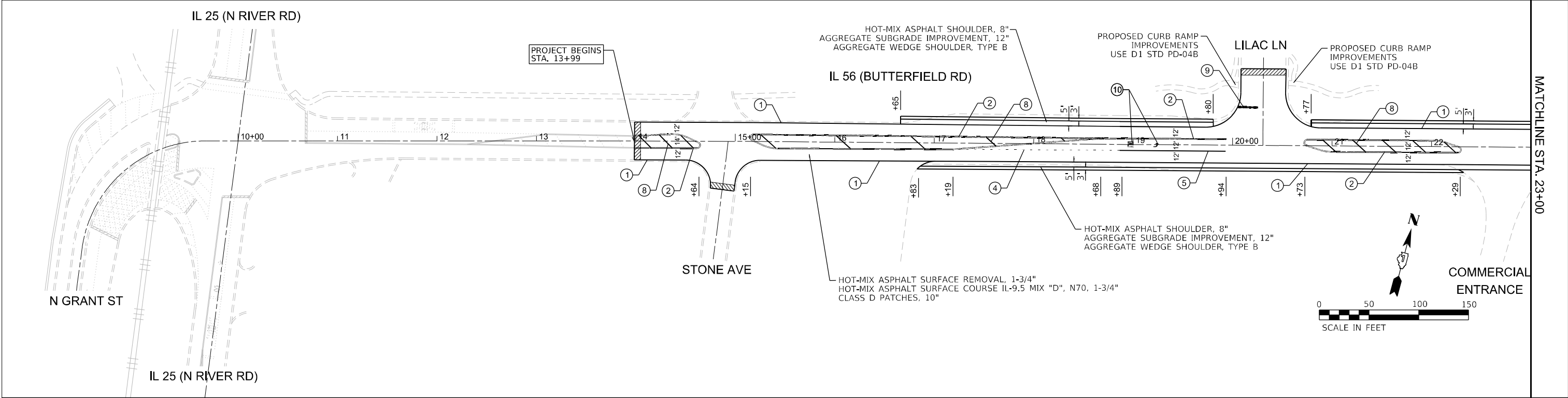
HOT-MIX ASPHALT REMOVAL

NOTES

1. THE CONTRACTOR SHALL MILL FIRST THEN PATCH
2. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE. STA 13+99 TO STA 65+17
3. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HMA BINDER COURSE IL-9.5 N70. STA 65+17 TO STA 108+27

MODEL: Default
FILE NAME: P:\P-22\22-4675-00 DOT Various Phase 2 (PTB 2016-02)\WO 28 62X69\DNVCADD_Sheets\62X69-SHT-T-TYPICAL-02.dgn

 INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9599 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - HA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD			F.A.P. RTE. 365	SECTION 2024-1031-RS	COUNTY KANE	TOTAL SHEETS 35	SHEET NO. 6
		DRAWN - HA	REVISED -					CONTRACT NO. 62X69				
		CHECKED - ACL	REVISED -									
	PLOT DATE = 4/15/2025	DATE = 03/07/2025	REVISED -									
					SCALE: NTS	SHEET 2 OF 2 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① LINE 4", SOLID, WHITE - EDGE LINE (TYP) | ⑧ LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 75' C-C (5 MINIMUM) (TYP) |
| ② LINE 4", DOUBLE SOLID YELLOW, 2 @ 11" C-C - (TYP) | ⑧A LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 150' C-C (5 MINIMUM) (TYP) |
| ③ LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP) | ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP) |
| ④ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑩ LETTERS & SYMBOLS, SOLID WHITE (TYP) |
| ⑤ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | |
| ⑥ LINE 8", SOLID, WHITE - GORE LINE (TYP) | |
| ⑦ LINE 12", SOLID, WHITE - MEDIAN DIAGONALS (TYP) | |
| ⑦A LINE 12", SOLID, WHITE - CROSSWALK (TYP) | |


LEGEND

-  HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

- LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT.
- ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

MODEL: Default
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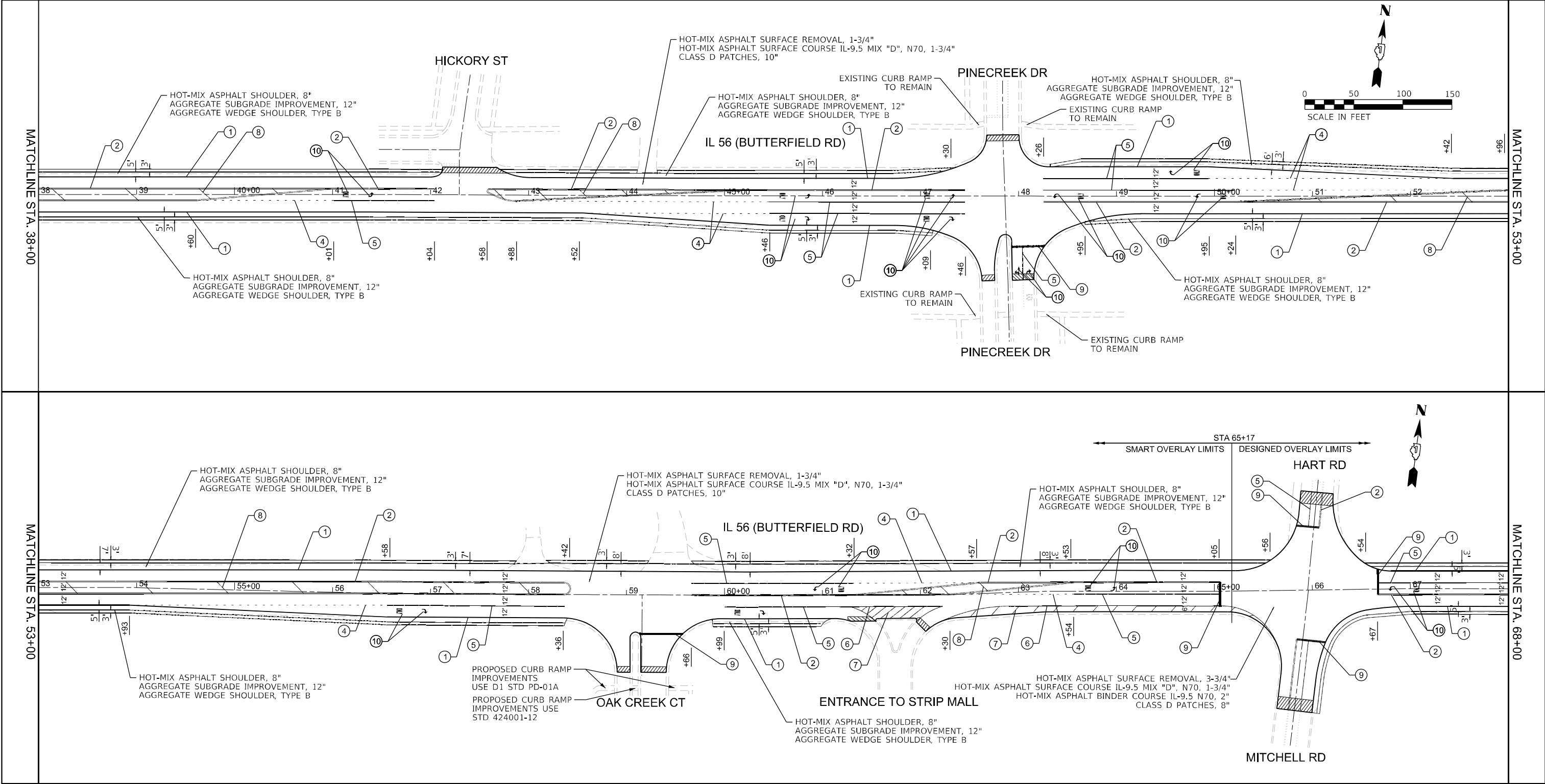
 INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9568 F 312.425.9568 www.infrastructure-eng.com	USER NAME = HAsayed	DESIGNED - HA	REVISED -
		DRAWN - HA	REVISED -
		CHECKED - ACL	REVISED -
	PLOT DATE = 3/7/2025	DATE - 03/07/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD

SCALE: 1"=50' SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	7
CONTRACT NO. 62X69				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① LINE 4", SOLID, WHITE - EDGE LINE (TYP) | ⑧ LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 75' C-C (5 MINIMUM) (TYP) |
| ② LINE 4", DOUBLE SOLID YELLOW, 2 @ 11" C-C - (TYP) | ⑧A LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 150' C-C (5 MINIMUM) (TYP) |
| ③ LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP) | ⑨ LINE 24", SOLID, WHITE - STOP LINES (TYP) |
| ④ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑩ LETTERS & SYMBOLS, SOLID WHITE (TYP) |
| ⑤ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | |
| ⑥ LINE 8", SOLID, WHITE - GORE LINE (TYP) | |
| ⑦ LINE 12", SOLID, WHITE - MEDIAN DIAGONALS (TYP) | |
| ⑦A LINE 12", SOLID, WHITE - CROSSWALK (TYP) | |

LEGEND

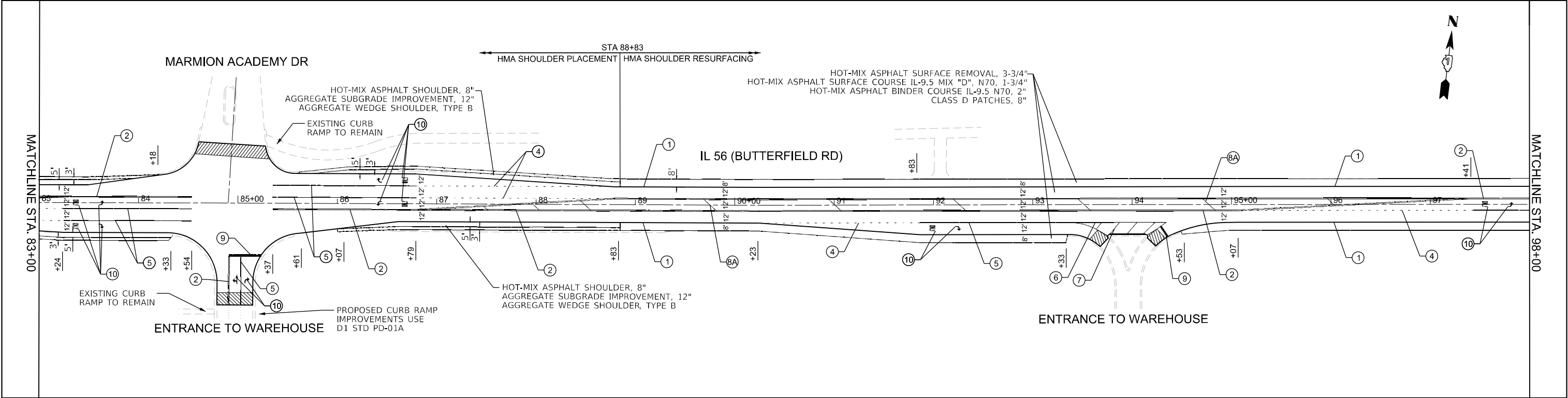
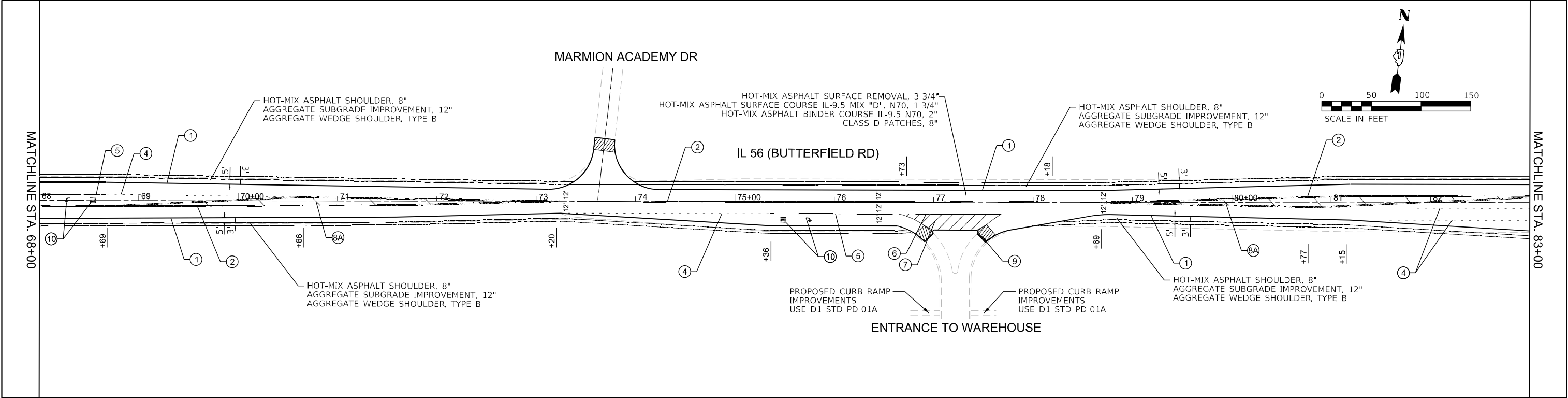
- | | |
|--|--------------------------------------|
| | HMA SURFACE REMOVAL BUTT JOINT, 4.5' |
|--|--------------------------------------|

NOTES:

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MODEL: Default
FILE NAME: C:\bms\elldms\62X69-SHT-PLAN-02.dgn

<div><div>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com</div></div>	USER NAME = HAsayed	DESIGNED - HA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - HA	REVISED -		365	2024-1031-RS	KANE	35	8				
		CHECKED - ACL	REVISED -		CONTRACT NO. 62X69								
	PLOT DATE = 3/22/2025	DATE - 03/07/2025	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE: 1"=50'	SHEET 2 OF 4 SHEETS	STA.	TO STA.					



PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① LINE 4", SOLID, WHITE - EDGE LINE (TYP) | ⑧ LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 75' C-C (5 MINIMUM) (TYP) |
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| ⑤ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | |
| ⑥ LINE 8", SOLID, WHITE - GORE LINE (TYP) | |
| ⑦ LINE 12", SOLID, WHITE - MEDIAN DIAGONALS (TYP) | |
| ⑦A LINE 12", SOLID, WHITE - CROSSWALK (TYP) | |

LEGEND

-  HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

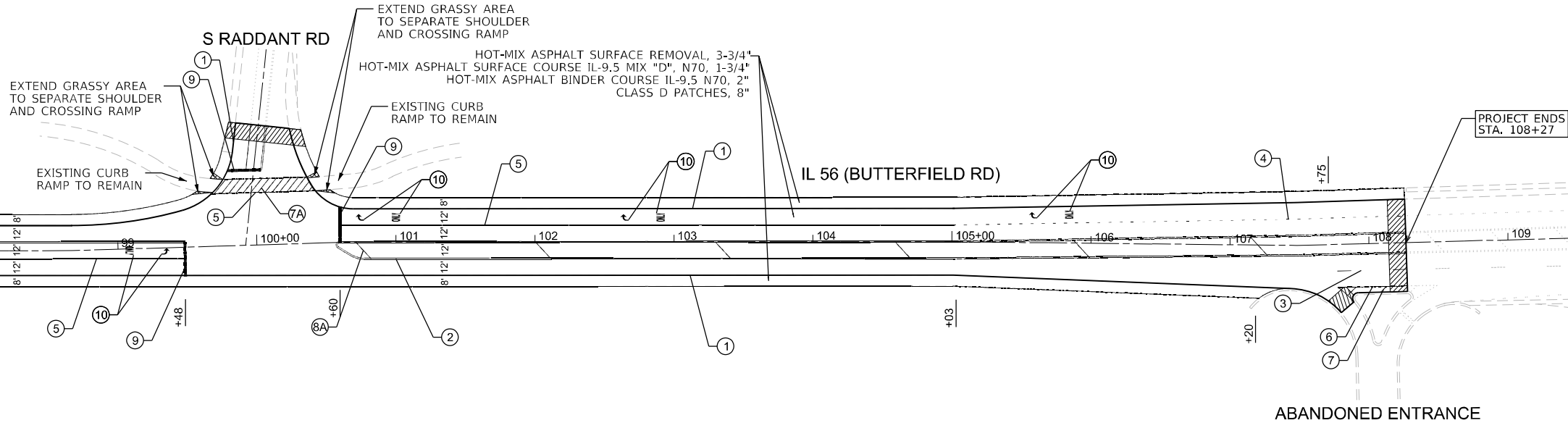
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MODEL: Default
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 <div>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9568 F 312.425.9568 www.infrastructure-eng.com</div>	USER NAME = HAsayed	DESIGNED - HA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - HA	REVISED -		365	2024-1031-RS	KANE	35	9				
		CHECKED - ACL	REVISED -		CONTRACT NO. 62X69								
	PLOT DATE = 3/22/2025	DATE - 03/07/2025	REVISED -		SCALE: 1"=50'	SHEET 3 OF 4 SHEETS	STA.	TO STA.					
					ILLINOIS FED. AID PROJECT								

MODEL: Default
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MATCHLINE STA. 98+00



PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① LINE 4", SOLID, WHITE - EDGE LINE (TYP) | ⑧ LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 75' C-C (5 MINIMUM) (TYP) |
| ② LINE 4", DOUBLE SOLID YELLOW, 2 @ 11" C-C - (TYP) | ⑧A LINE 12", SOLID, YELLOW - DIAGONAL @ 45 DEG. @ 150' C-C (5 MINIMUM) (TYP) |
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| ④ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP) | ⑩ LETTERS & SYMBOLS, SOLID WHITE (TYP) |
| ⑤ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP) | |
| ⑥ LINE 8", SOLID, WHITE - GORE LINE (TYP) | |
| ⑦ LINE 12", SOLID, WHITE - MEDIAN DIAGONALS (TYP) | |
| ⑦A LINE 12", SOLID, WHITE - CROSSWALK (TYP) | |

LEGEND

-  HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

- LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
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**INFRASTRUCTURE
ENGINEERING** INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9568 | F 312.425.9568 | www.infrastructure-eng.com

USER NAME	= HAsayed
PLOT DATE	= 3/22/2025

DESIGNED -	HA
DRAWN -	HA
CHECKED -	ACL
DATE -	03/07/2025

REVISED -	
REVISED -	
REVISED -	
REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
IL 56 (BUTTERFIELD ROAD) - RIVER ROAD TO CHURCH ROAD**

SCALE: 1"=50'	SHEET 4 OF 4 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	10
CONTRACT NO. 62X69				
ILLINOIS		FED. AID PROJECT		

(NOT TO SCALE)

MODEL: Default
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EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

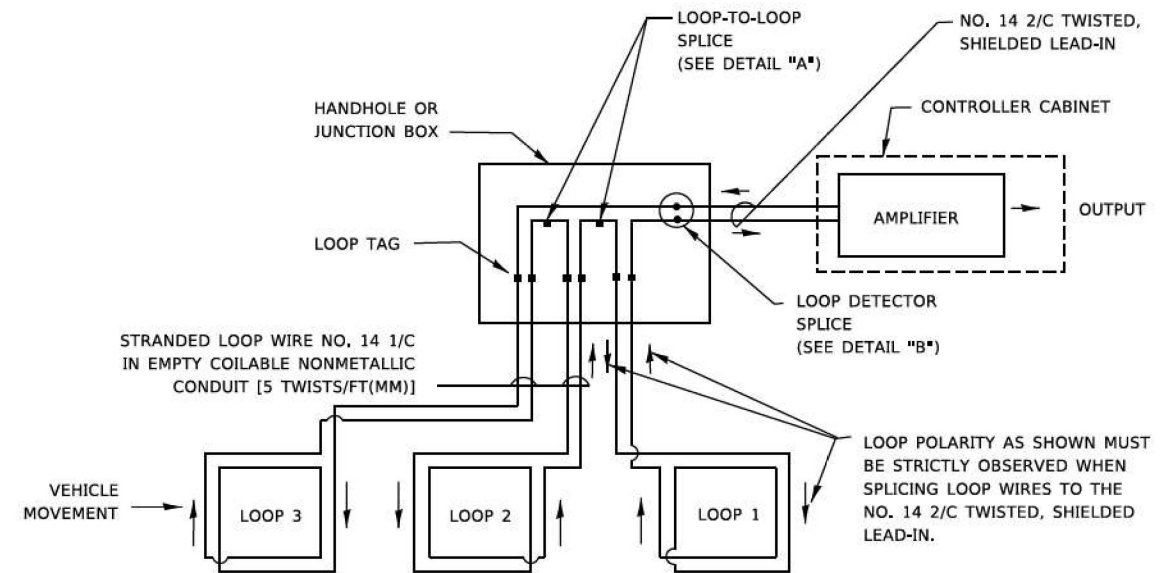
EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

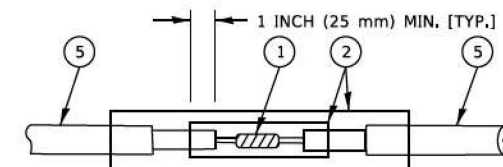
IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

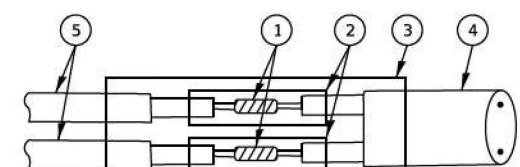
PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



- LOOPS SHALL BE SPLICED IN SERIES.
SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

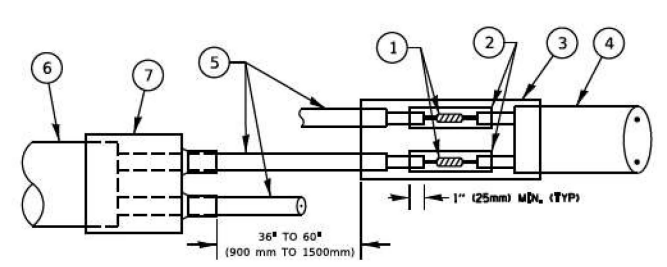


DETAIL "A"
LOOP-TO-LOOP SPLICE



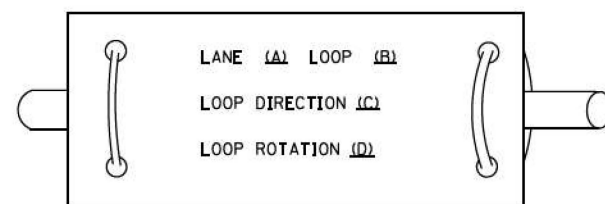
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

- | | |
|---|--|
| <p>① WESTERN UNION SPlice SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.</p> <p>② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.</p> <p>③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.</p> <p>④ NO. 14 2/C TWISTED, SHIELDED CABLE.</p> | <p>⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP</p> <p>⑥ XL POLYOLEFIN 2 CONDUCTOR</p> <p>⑦ BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL</p> |
|---|--|

USER NAME	■ footer]	DESIGNED	-	REVISED	-	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	-	REVISED	-						365	2024-1031-RS	KANE	35	12
PLOT SCALE	■ 50,000 ' / In.	CHECKED	-	REVISED	-		TS-05				CONTRACT NO. 62X69				
PLOT DATE	■ 3/4/2019	DATE	-	REVISED	-						SCALE: NONE	SHEET 2	OF 7	SHEETS	STA.

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND
PEDESTRIAN PUSHBUTTON DETECTORS.



1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

3.5 FT (1.1m)
PEDESTRIAN

7 FT (2.1m)
EQUESTRIAN

8 FT (2.4 m) MIN.
TO 10' (3.0 m) MAX.

SEE TABLE I

SEE NOTE I

SIDEWALK

BACK OF CURB, BACK OF SHOULDER
OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

The diagram illustrates the recommended locations for pedestrian pushbuttons at a building corner. The corner is defined by a 10 FT (3.0 m) MIN. radius. The recommended pushbutton locations are indicated by hatched areas. Dimensions are provided in feet and meters (MAX. for maximum, MIN. for minimum). The diagram shows a corner with a 10 FT (3.0 m) MIN. radius. The recommended pushbutton locations are indicated by hatched areas. Dimensions are provided in feet and meters (MAX. for maximum, MIN. for minimum). The diagram shows a corner with a 10 FT (3.0 m) MIN. radius. The recommended pushbutton locations are indicated by hatched areas. Dimensions are provided in feet and meters (MAX. for maximum, MIN. for minimum).

LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT,
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT.	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

USER NAME = footer]	DESIGNED -	REVISED -
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PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

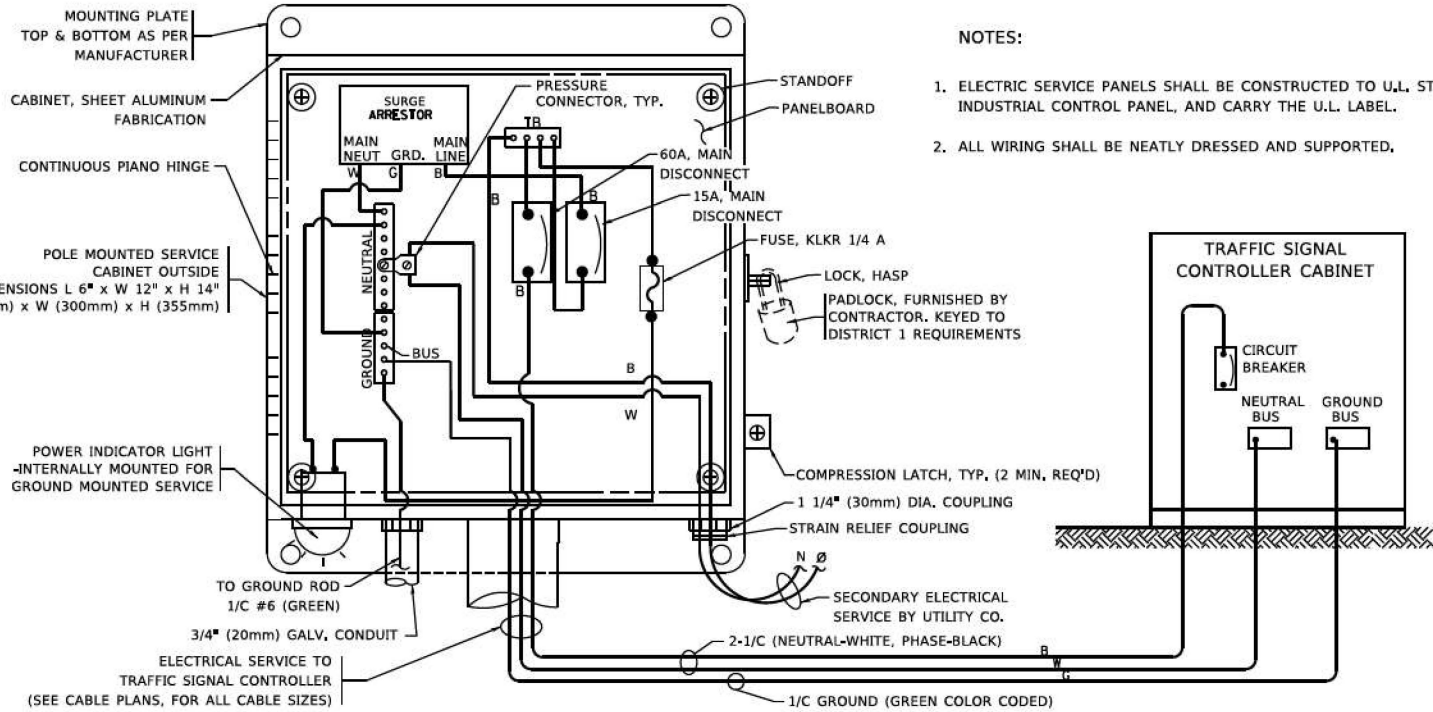
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE

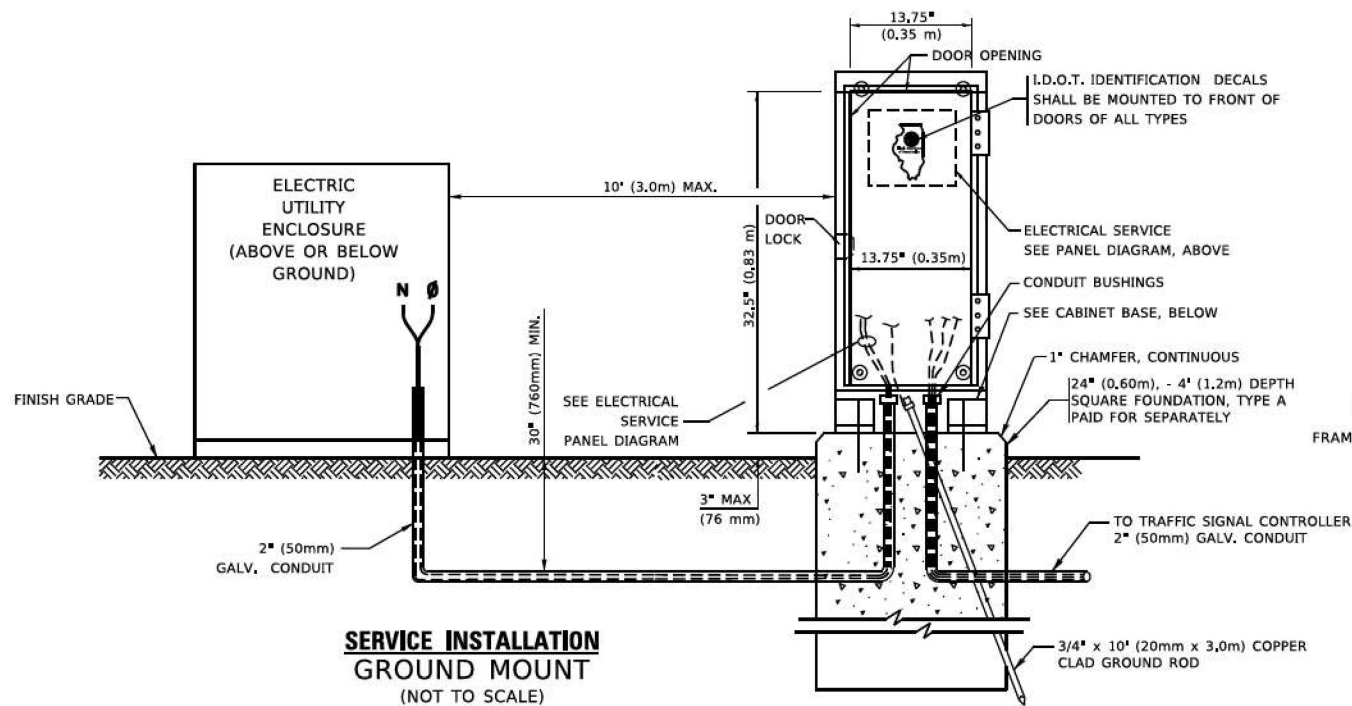
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE	SHEET 3 OF 7 SHEETS	STA.	TO STA.
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F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	13
TS-05		CONTRACT NO. 62X69		
	ILLINOIS	FED. AID PROJECT		

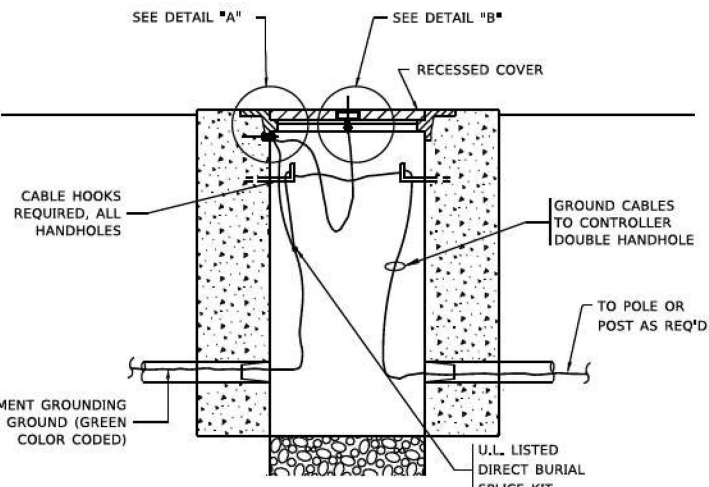
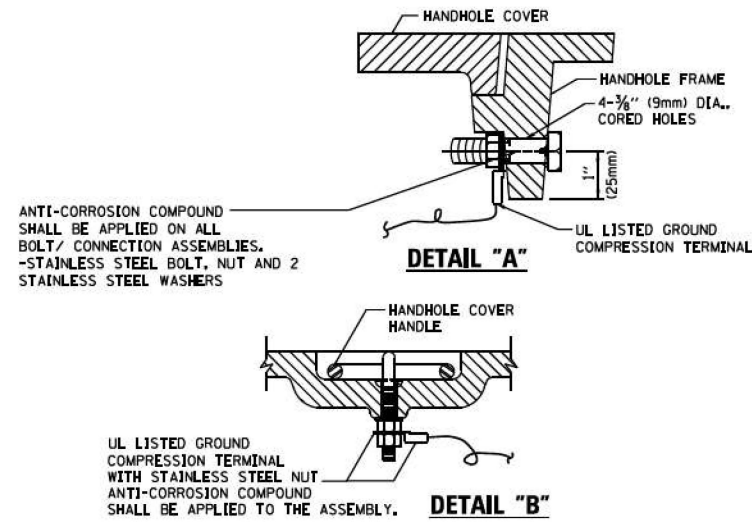
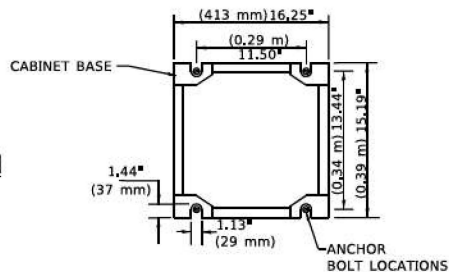


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



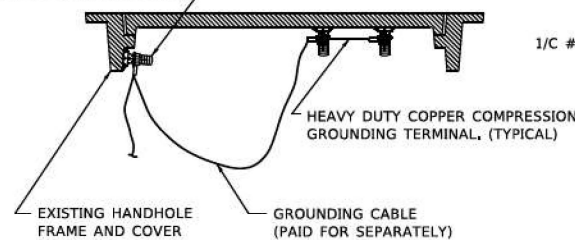
SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET - BASE BOLT PATTERN
(NOT TO SCALE)



HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)

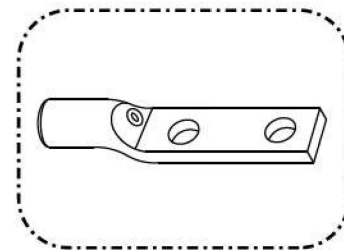
(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL). ANTI-CORROSION COMPOUND SHALL BE APPLIED TO EACH ASSEMBLY.



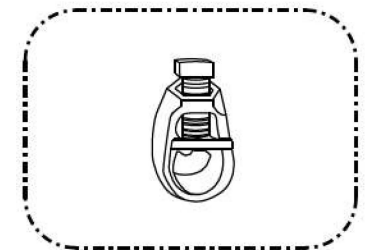
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)

NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



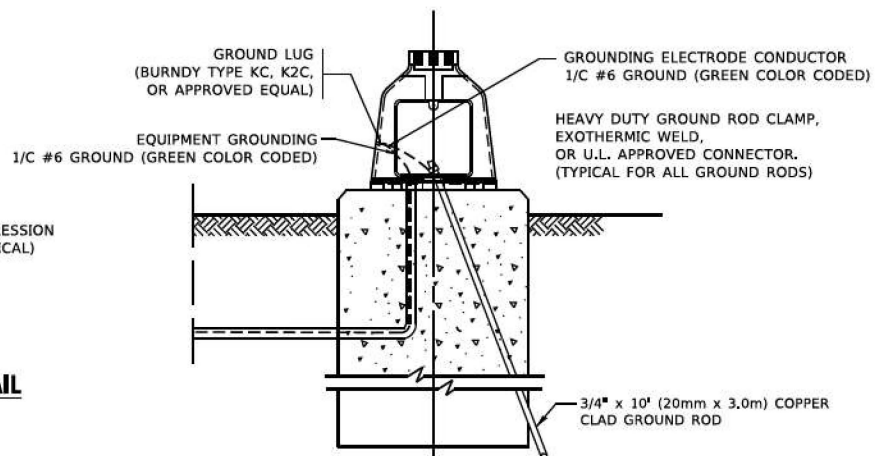
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



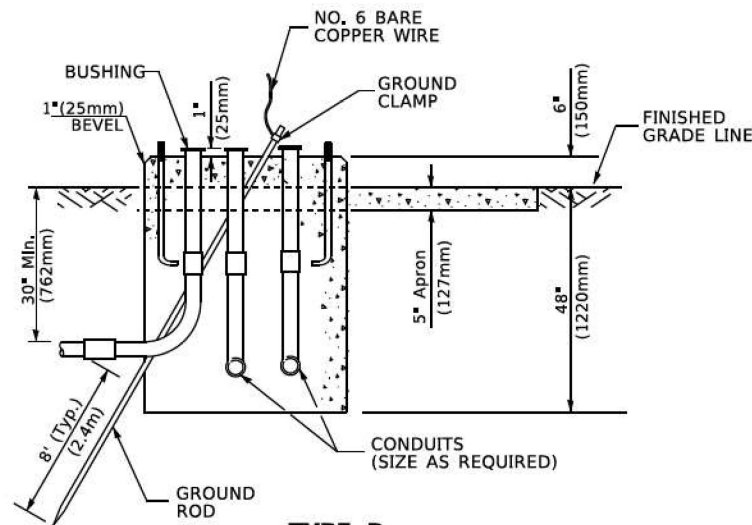
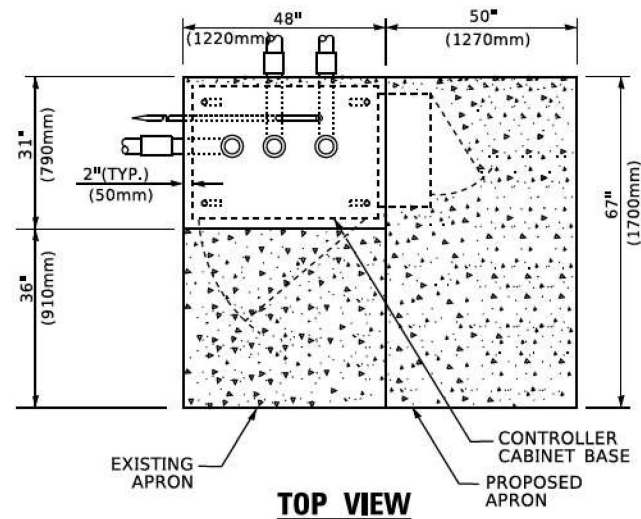
MAST ARM POLE /POST-GROUNDING DETAIL
(NOT TO SCALE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

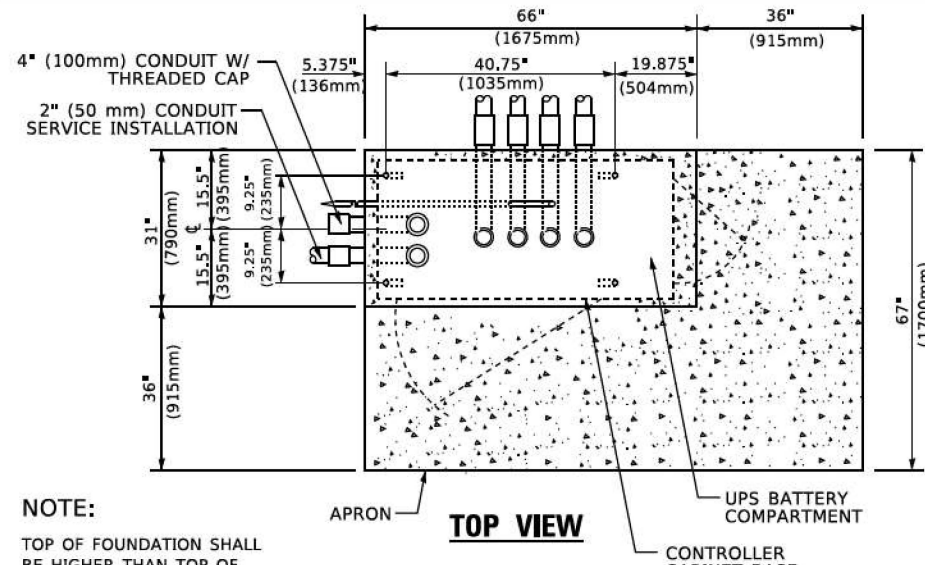
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	14
TS-05		CONTRACT NO. 62X69		
		ILLINOIS	FED. AID PROJECT	

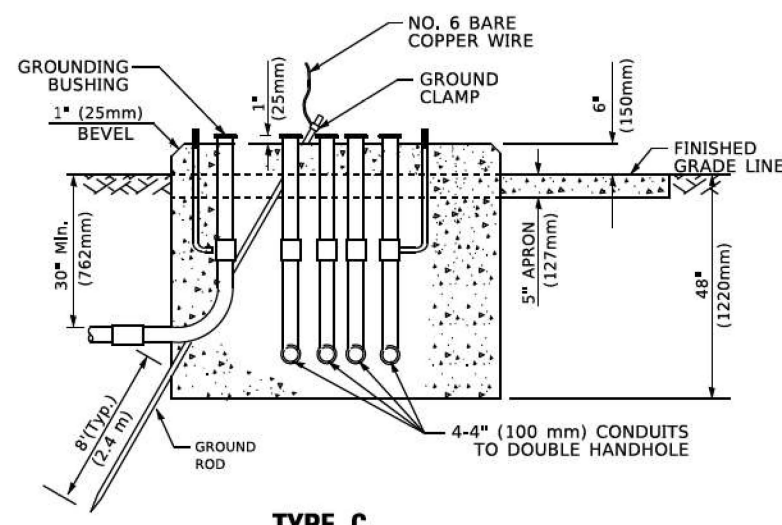


TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET

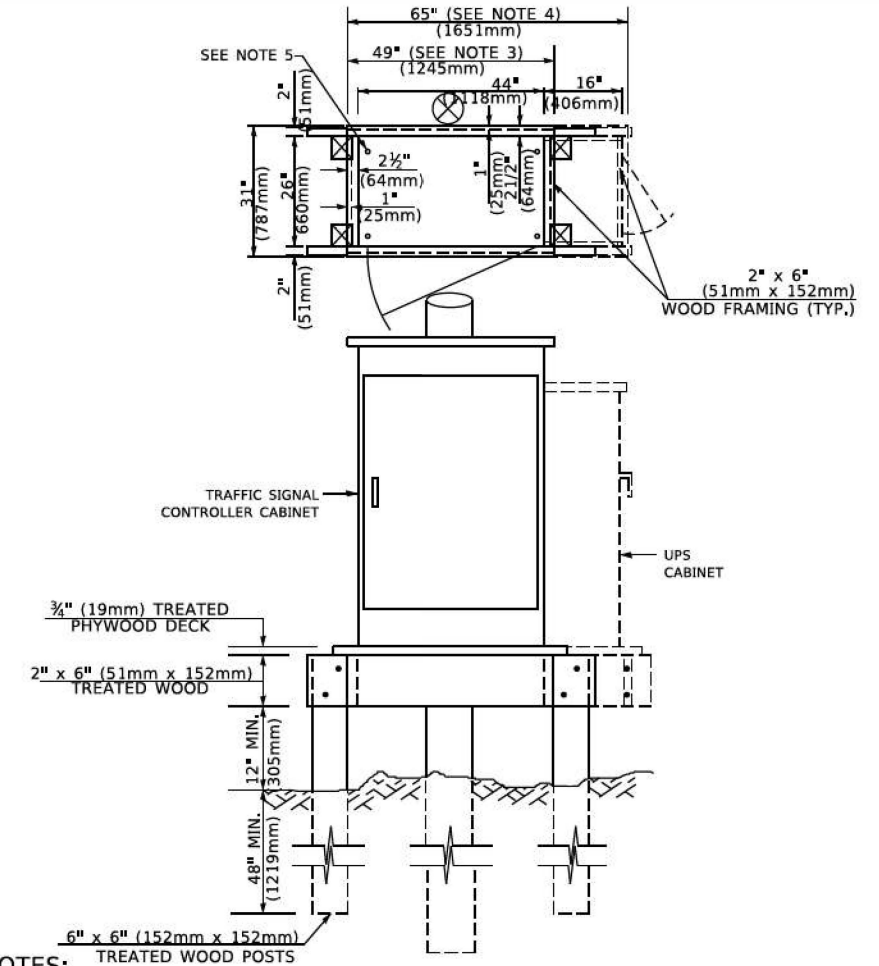


NOTE:

TOP OF FOUNDATION SHALL
BE HIGHER THAN TOP OF
DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD)		
(L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

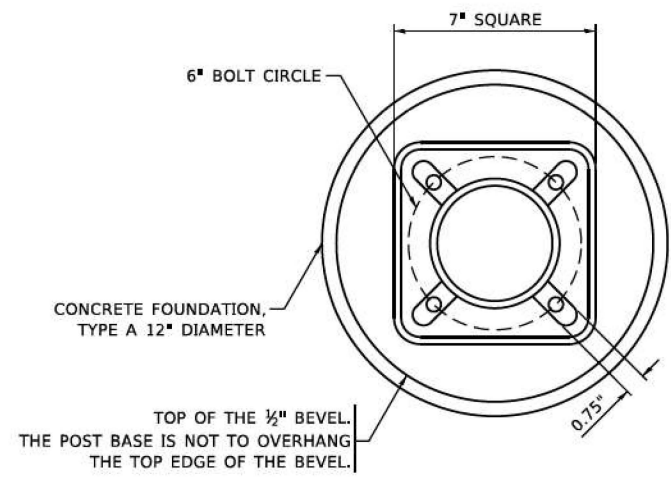
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DRAWN -	REVISED -	
PLOT SCALE - 50,000' / 1"	CHECKED -	REVISED -
PLOT DATE - 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

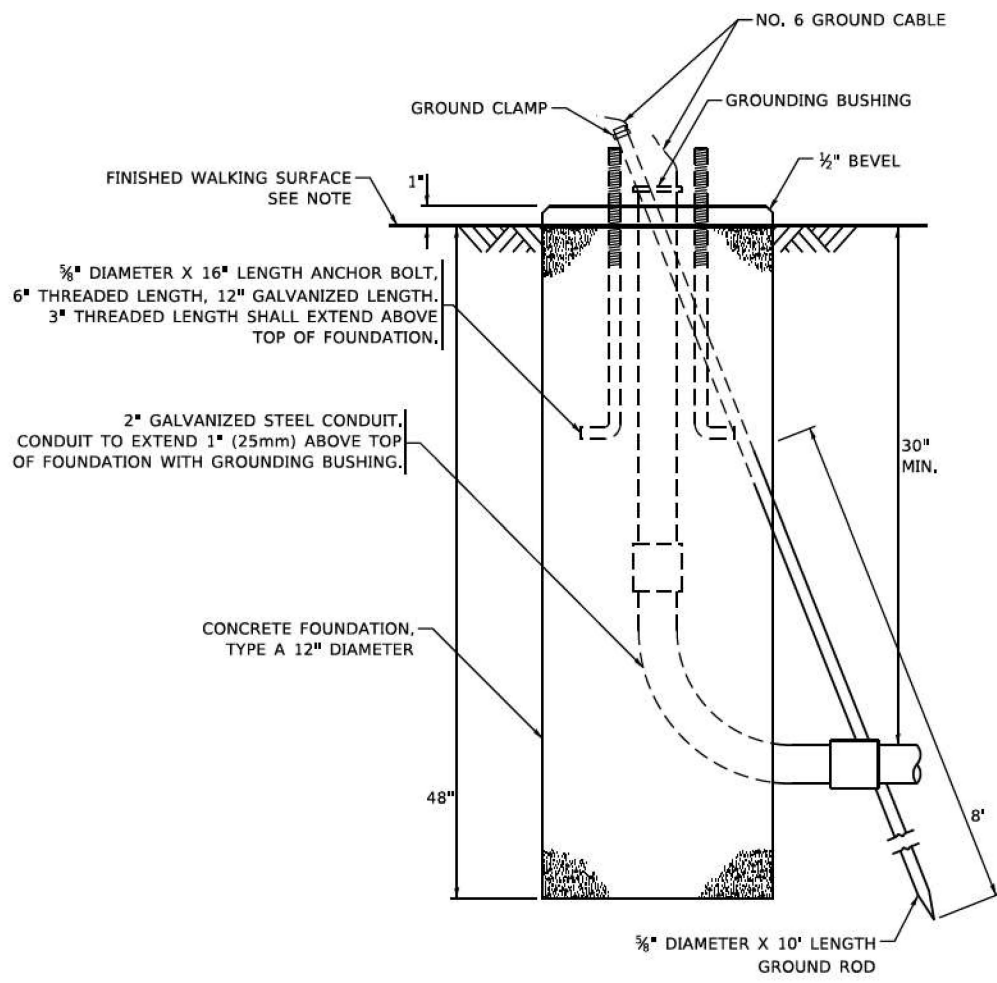
SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	15
TS-05		CONTRACT NO. 62X69		
		ILLINOIS	FED. AID PROJECT	

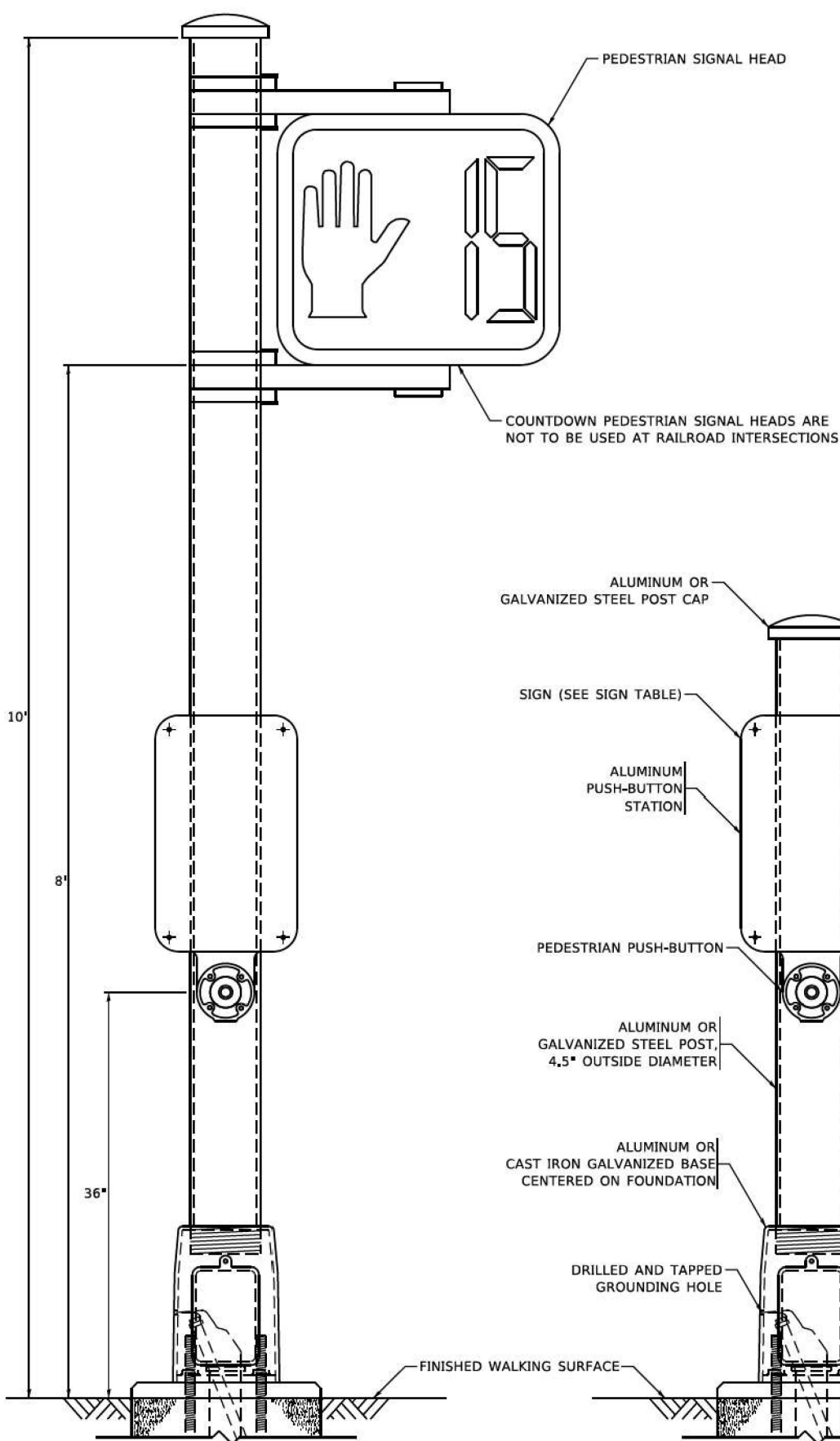


BOLT PATTERN

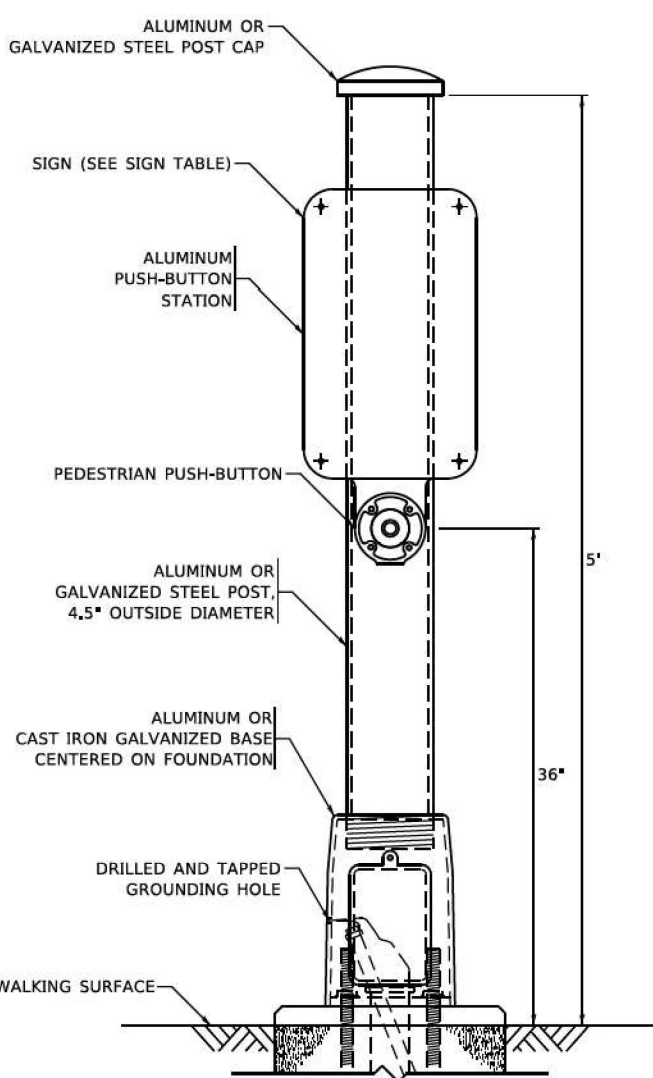
NOTE:
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



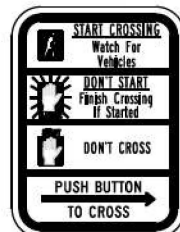
**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



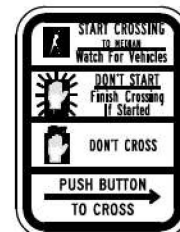
PEDESTRIAN SIGNAL POST, 10 FT.



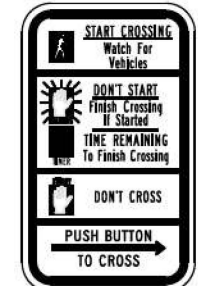
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

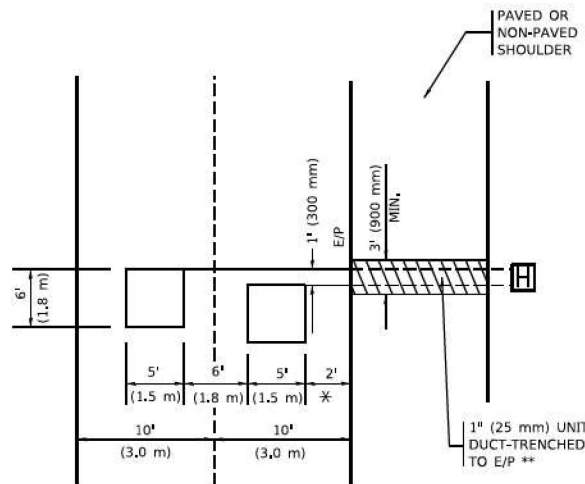
SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

- NOTES:**
1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
 2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
 3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

MODEL: Default
FILE: W:\GIS\Projects\2024\2024-1031-RS\2024-1031-RS-05.dgn
PROJECT: 2024-1031-RS
SHEET: 17 OF 35
DATE: 11/23/2020
DESIGNED: IP
DRAWN: IP
CHECKED: LP
DATE: 10-15-2018

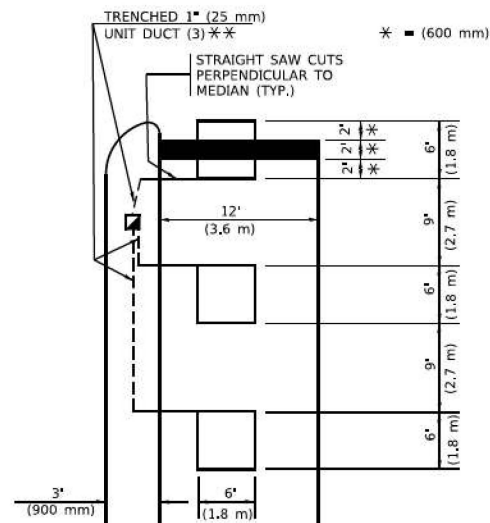
PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.



* = (600 mm)

* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

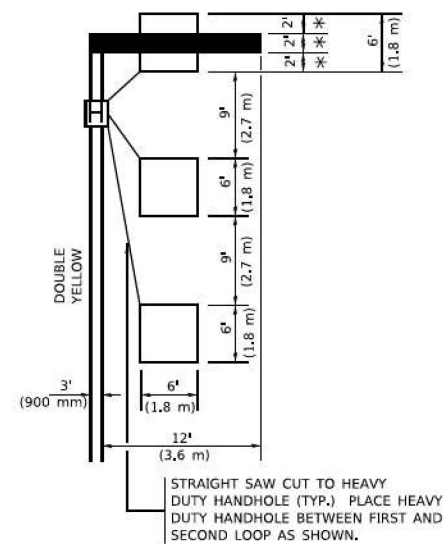
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE, REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

* = (600 mm)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

DETAIL 1
N.T.S.

[illegible]

DETAIL 2
N.T.S.

* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER
FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE
DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS
FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

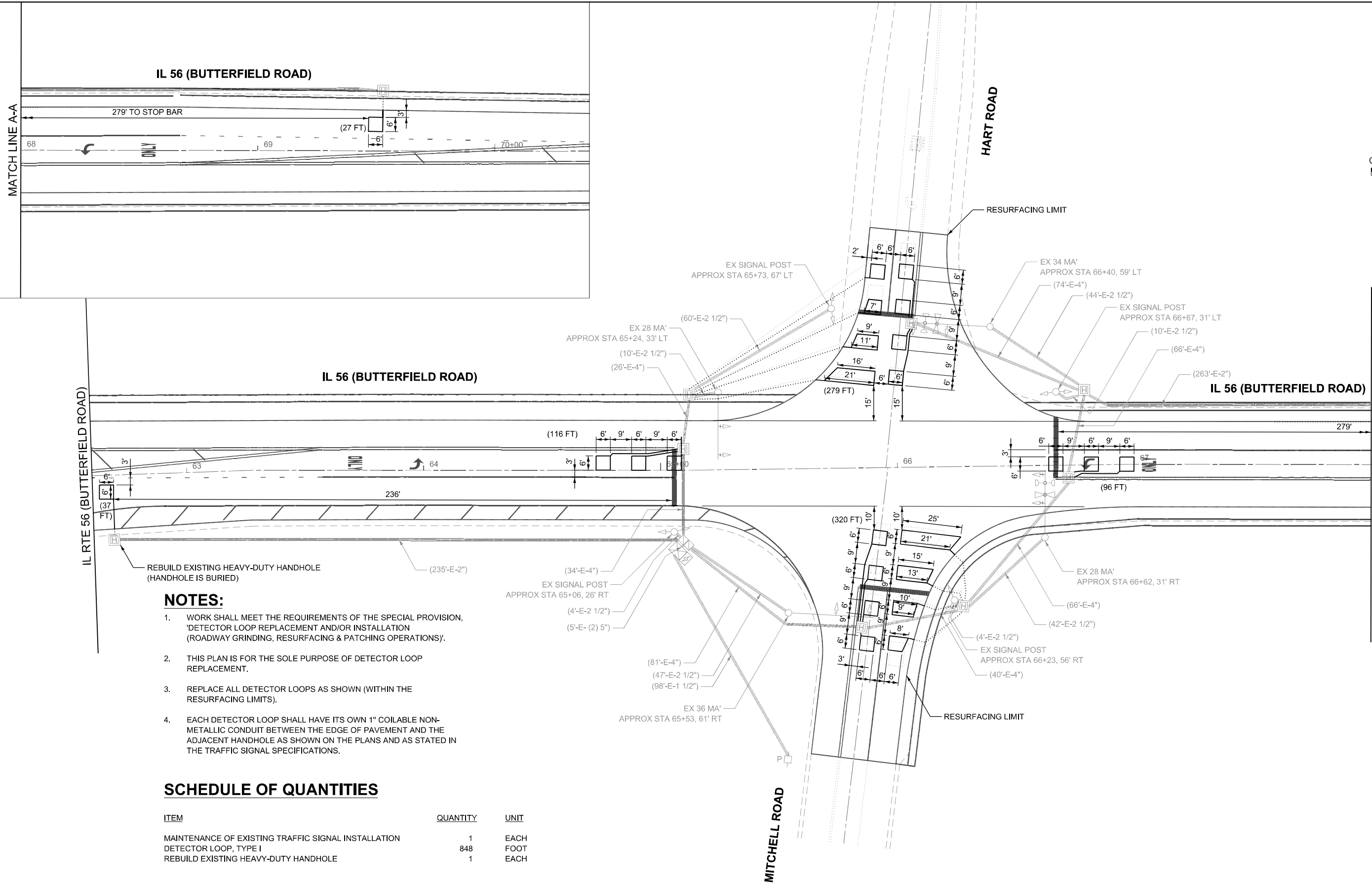
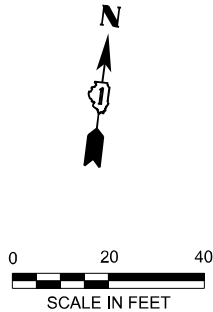
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	DRAWN -	REVISED -
PLOT SCALE ■ 50,0000 * / In.	CHECKED ■ R.K.F.	REVISED -
PLOT DATE ■ 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	18
TS-07		CONTRACT NO. 62X69		
ILLINOIS		FED. AID PROJECT		



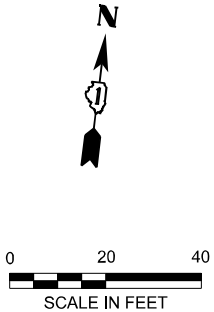
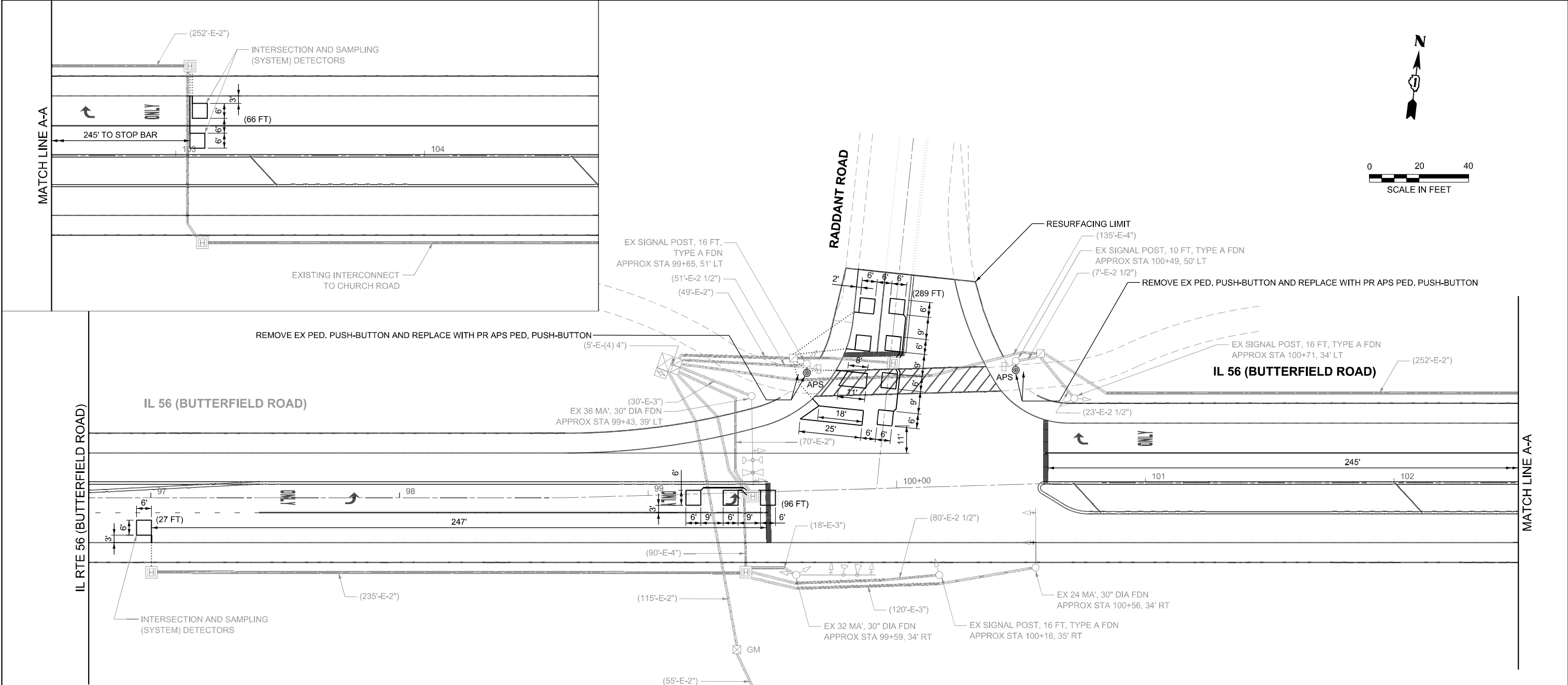
NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, 'DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)'.
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.
3. REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS).
4. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

SCHEDULE OF QUANTITIES

ITEM	QUANTITY	UNIT
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	1	EACH
DETECTOR LOOP, TYPE I	848	FOOT
REBUILD EXISTING HEAVY-DUTY HANDHOLE	1	EACH

MODEL: Default
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REMOVAL AND RELOCATION NOTES:

- THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR OWN EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
- EACH PEDESTRIAN PUSH-BUTTON
- ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
- CONTRACTOR SHALL PERFORM WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE PEDESTRIAN EQUIPMENT IS NOT OPERATIONAL.
- CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE APS EQUIPMENT WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
- APS BUTTONS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
- DETECTOR LOOP WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)".
- REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS).
- EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
- ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.

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INFRASTRUCTURE ENGINEERING INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
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USER NAME	= HAsayed
DESIGNED	- MK
DRAWN	- MK
CHECKED	- ACL
PLOT DATE	= 3/7/2025

REVISED	-
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REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

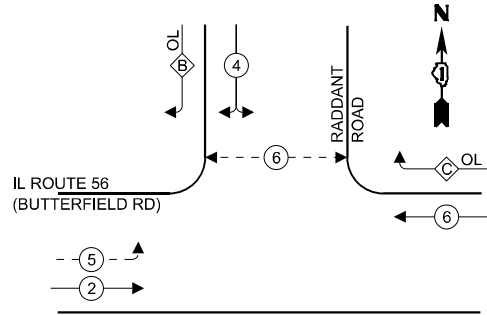
**TRAFFIC SIGNAL MODERNIZATION PLAN
IL 56 (BUTTERFIELD RD) AT RADDANT RD**

SCALE: 1" = 20' SHEET 2 OF 3 SHEETS STA. TO STA.

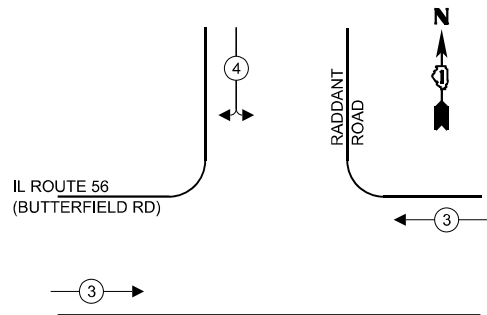
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	20
CONTRACT NO. 62X69				
ILLINOIS FED. AID PROJECT				

**TS 11484
EAGLE 16H**

EXISTING AND PROPOSED
CONTROLLER SEQUENCE



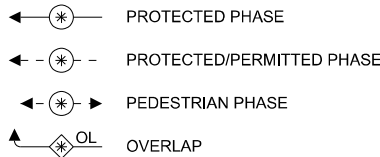
EXISTING AND PROPOSED
EMERGENCY VEHICLE
PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL
SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	8	11	88
4-SECTION	-	14	-
5-SECTION	4	13	52
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	2	15	30
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			345
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	1	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			950

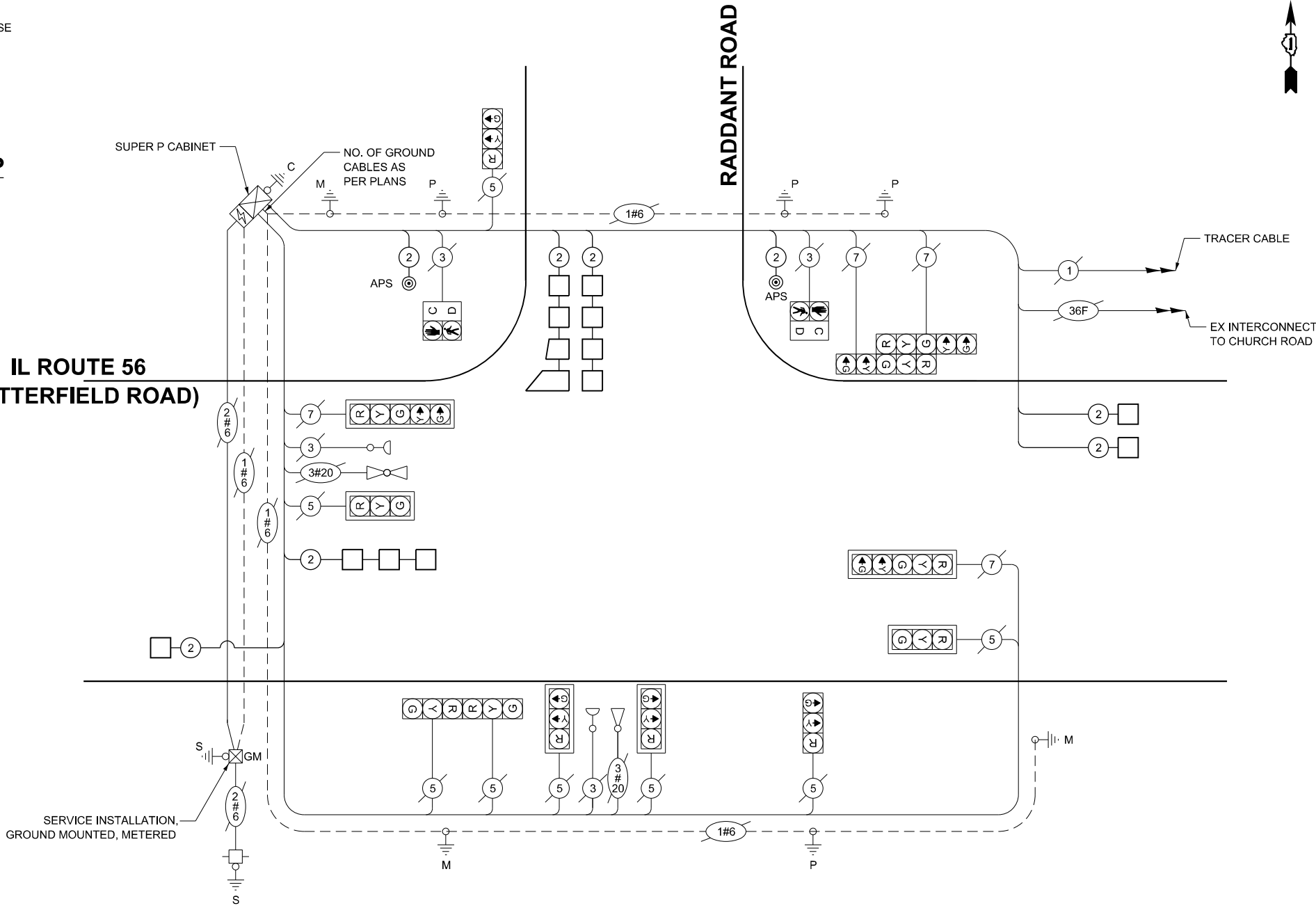
LEGEND



RIGHT TURN OVERLAP
PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4	+ 5
C	= 6	+ 4

IL ROUTE 56
(BUTTERFIELD ROAD)



CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	TOTAL
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	261
DETECTOR LOOP, TYPE I	FOOT	478
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ROD AND CLEAN EXISTING CONDUIT	FOOT	203
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

ENERGY COSTS TO:
CITY OF AURORA

44 E. DOWNER PLACE
AURORA, IL 60505

ENERGY SUPPLY:

CONTACT: CASSIE EVANS

PHONE: (773) 241-0741

COMPANY: COMMONWEALTH EDISON

ACCOUNT NUMBER:

METER NUMBER:

TS 11488
EAGLE 16H

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DIAGRAM, & EMERG. VEH. PREEMPTION
SEQUENCE - IL 56 (BUTTERFIELD RD) AT RADDANT RD

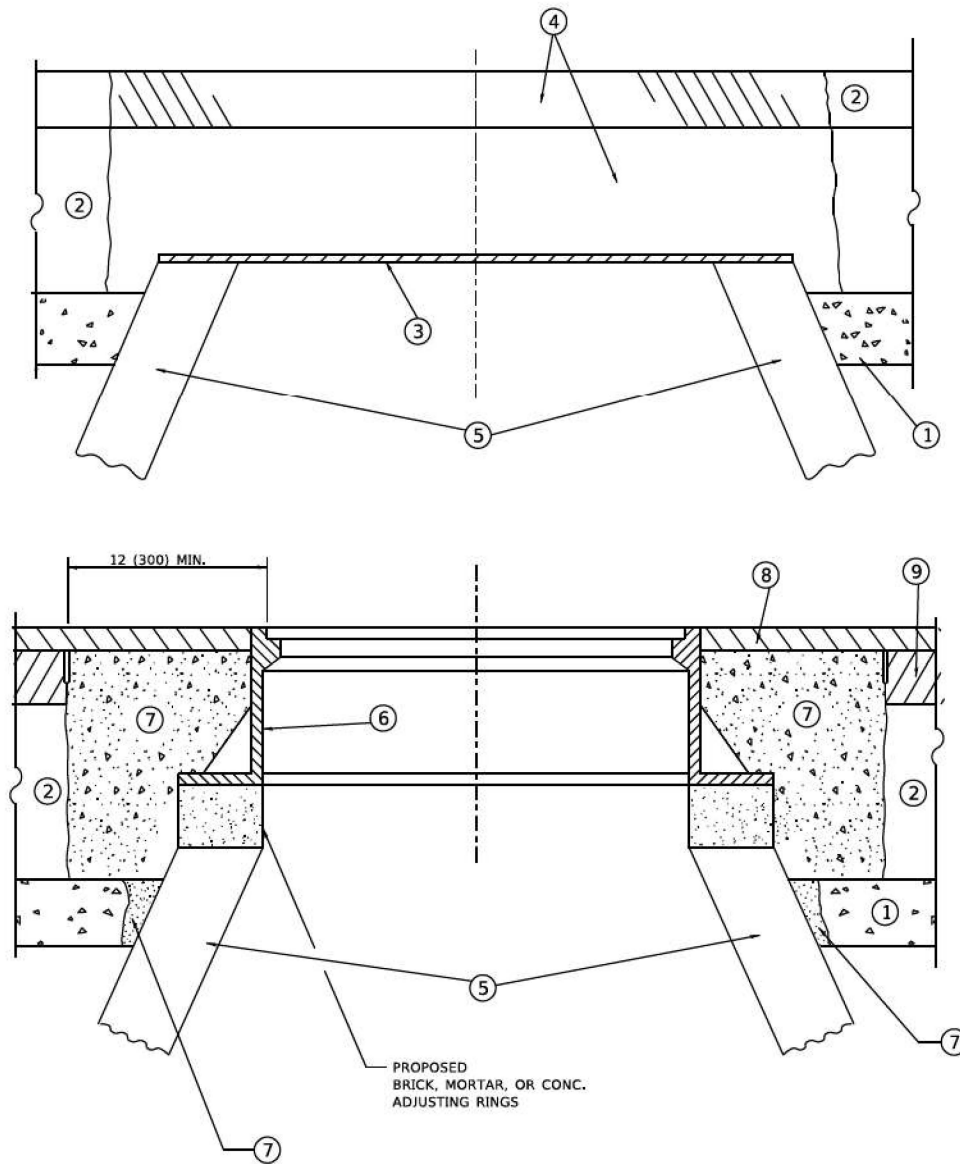
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	21
CONTRACT NO. 62X69				
ILLINOIS FED. AID PROJECT				

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USER NAME	= HAAayed
DESIGNED	- MK
DRAWN	- MK
CHECKED	- ACL
DATE	- 03/07/2025
REVISION	-
REVISION	-
REVISION	-
REVISION	-



DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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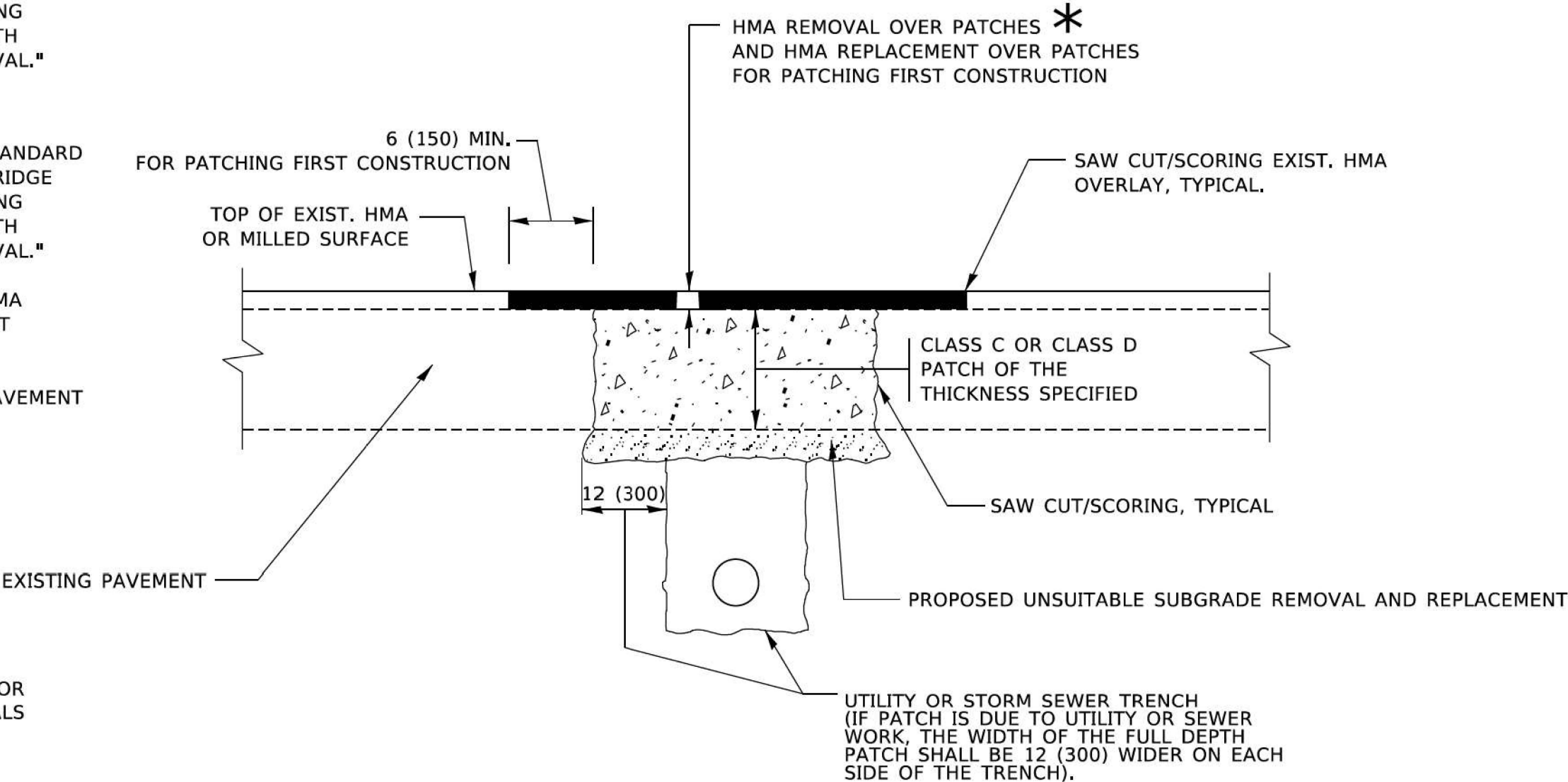
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			DRAWN		REVISED	R. BORO 12-06-11						365	2024-1031-RS	KANE	35	22			
	PLOT SCALE	100,0000' / 1in.	CHECKED		REVISED	K. SMITH 11-18-22						BD600-03 (BD-08)				CONTRACT NO. 62X69			
	PLOT DATE	9/15/2023	DATE	10-25-94	REVISED	K. SMITH 09-15-23						SCALE: NONE				SHEET 1	OF 1 SHEETS	STA.	TO STA.
																ILLINOIS FED. AID PROJECT			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

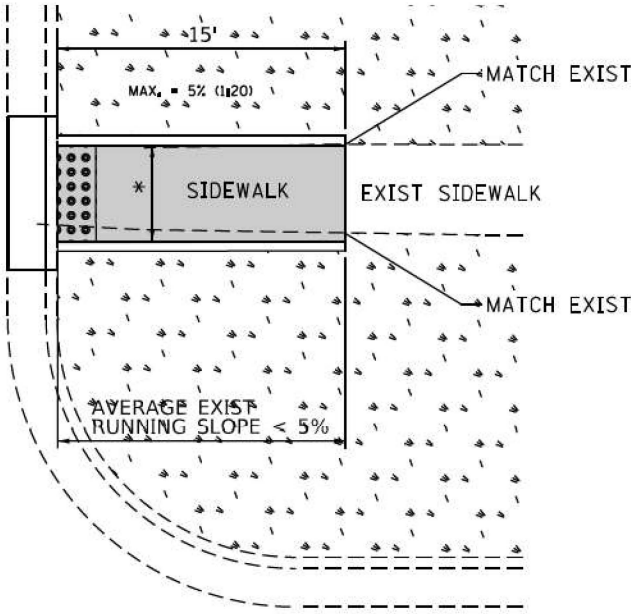
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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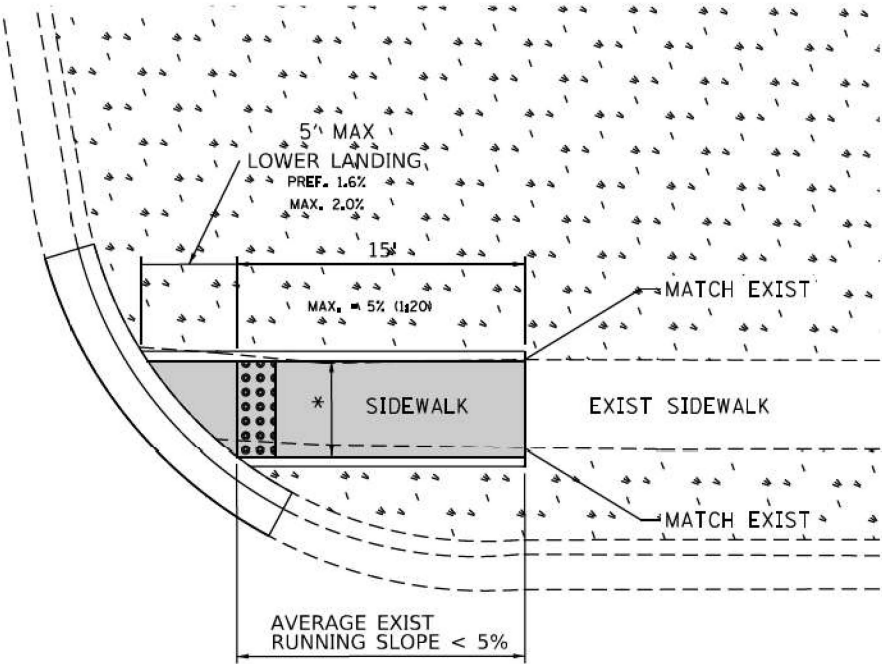
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ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

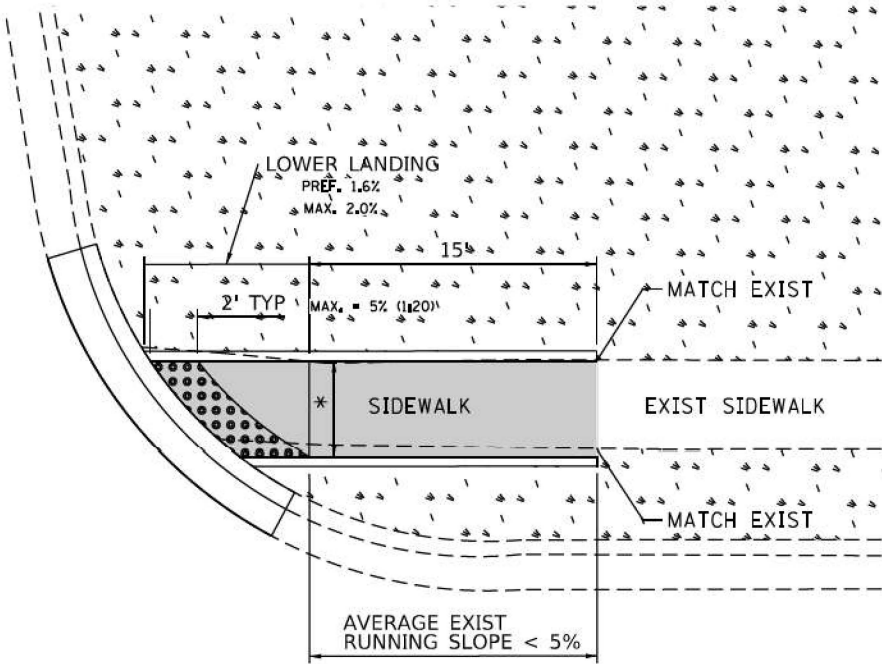
PD-01A



PD-01B



PD-01C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

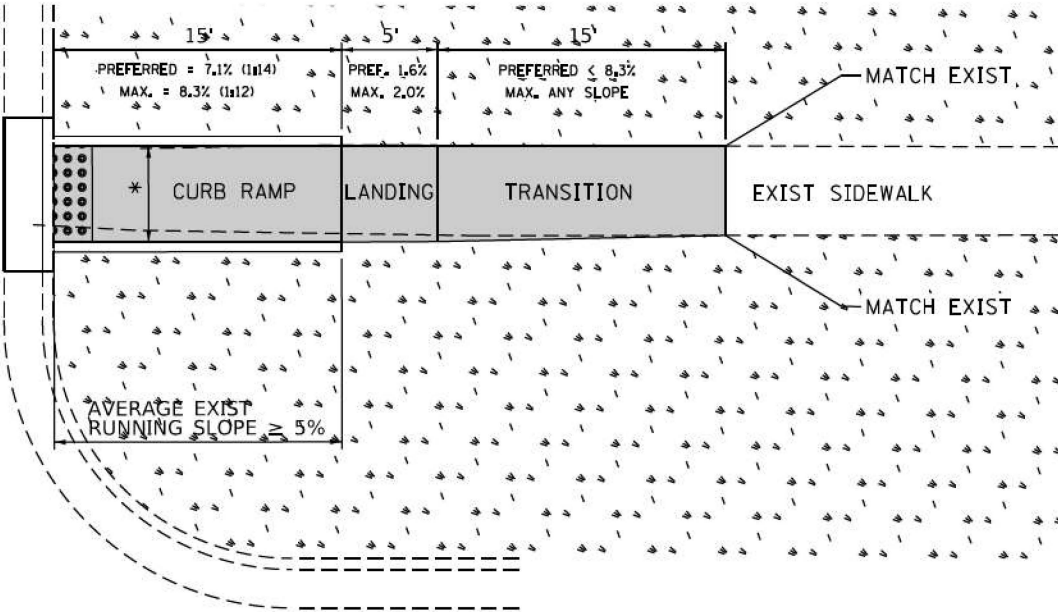
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

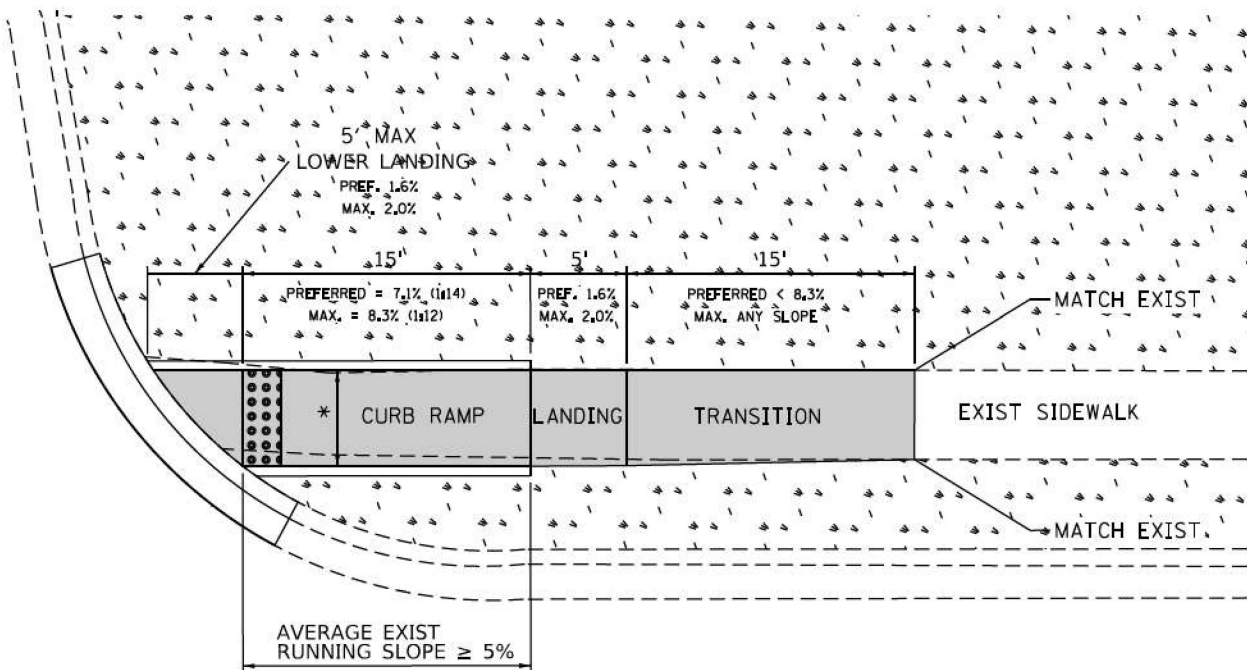
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Se\WP\PLANPREP\SQUAD\1\Des.RL\Typical ADA details\Typical-ADA-shit-plan.dgn	DRAWN - RL	11/12/2019	REVISED -			365	2024-1031-RS	KANE	35	26					
Default	CHECKED -		REVISED -			PD-01			CONTRACT NO. 62X69						
	PLOT DATE = 12/17/2019	DATE -	REVISED -			SCALE:		SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

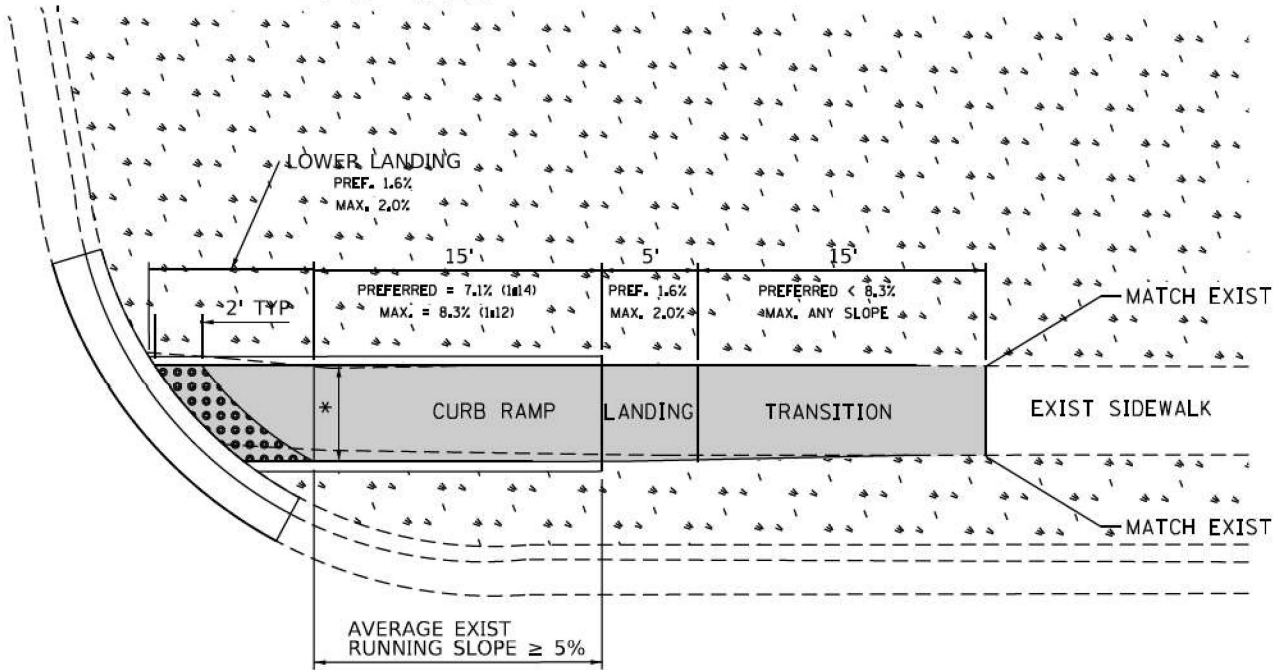
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PD-02B



PD-02C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

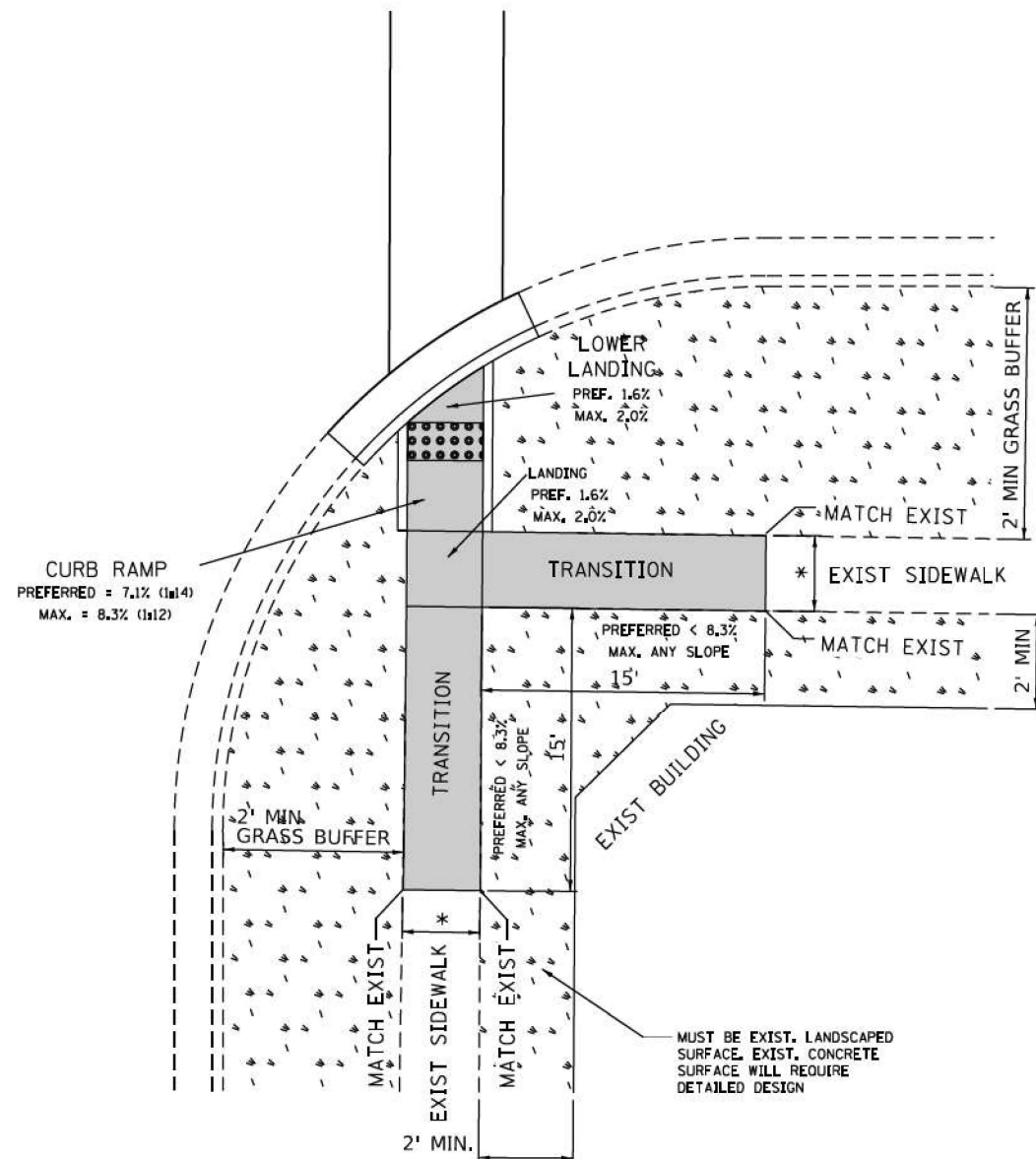
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

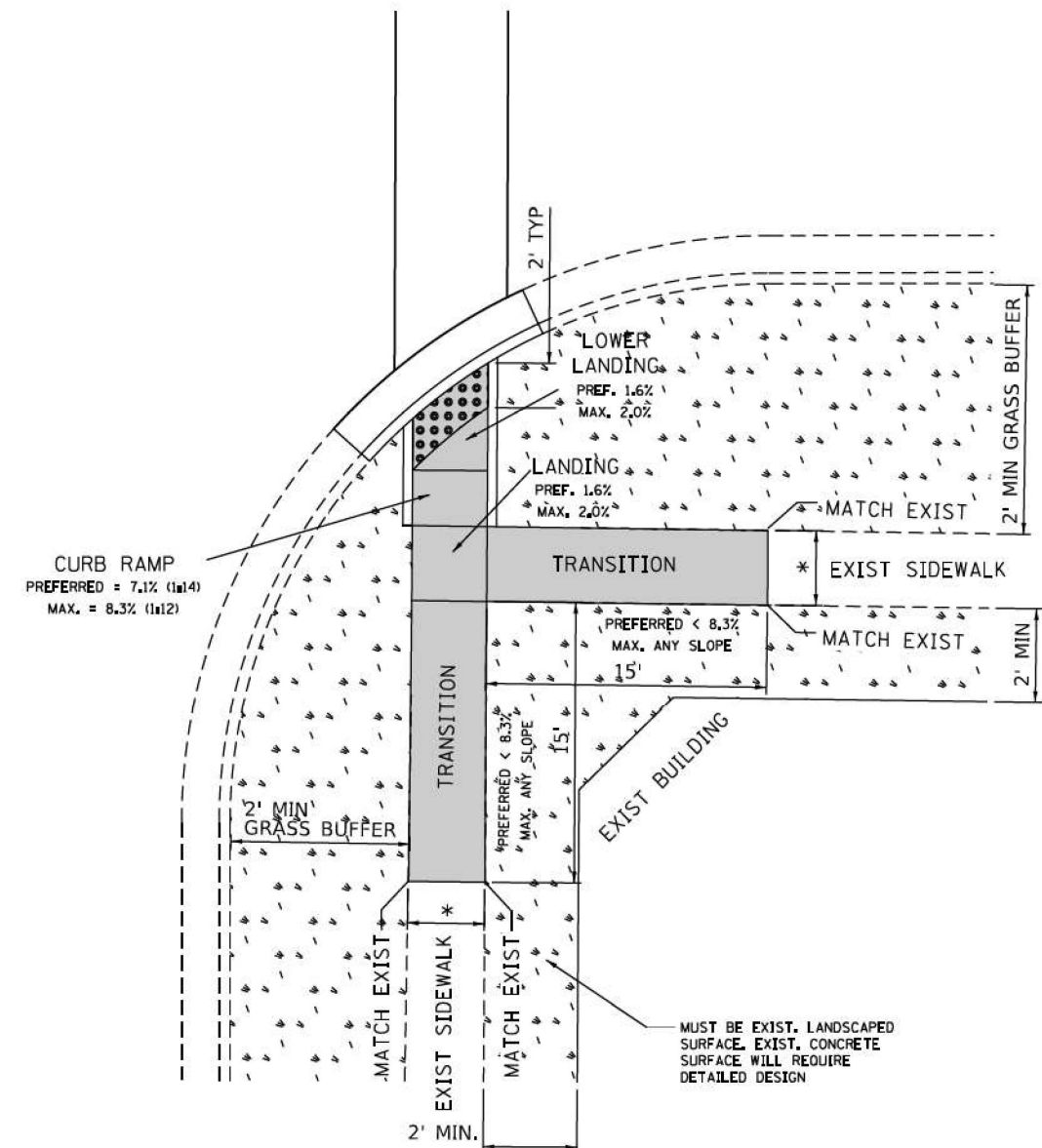
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SA\WP\PLANPREP\SQUAD\INDes.RL\Typical ADA details\Typical-ADA-shr-plerudgn	DRAWN - RL	11/12/2019	REVISED -	365		2024-1031-RS	KANE	35	27						
Default	CHECKED -		REVISED -	PD-02		CONTRACT NO. 62X69									
	PLOT DATE = 12/17/2019	DATE -	REVISED -						ILLINOIS FED. AID PROJECT						
				SCALE:		SHEET	OF	SHEETS	STA.	TO STA.					

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND



PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

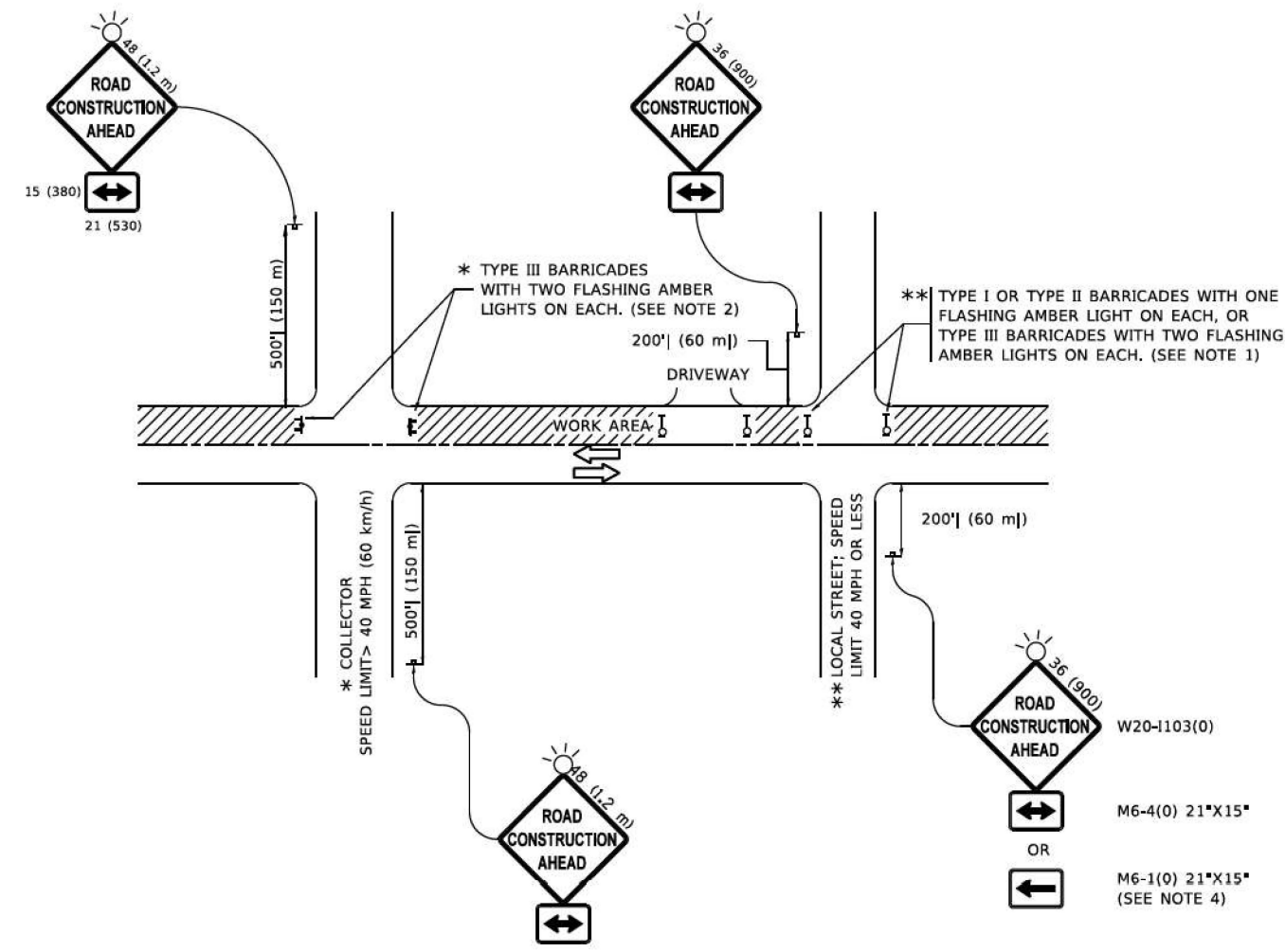
CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN
TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

FILE NAME =	USER NAME = ledezerm	DESIGNED -	--- --/--/----	REVISED -		<div style="text-align: center;"> STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION </div>	PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SA\MP\PLAN\PREP\SQUAD\IN\Des.RL\Typical ADA details\Typical-ADA-shr-tpier.dgn		DRAWN -	RL 11/12/2019	REVISED -			W/TURNING SPACE (PD-04)				365	2024-1031-RS	KANE	35	28
		CHECKED -		REVISED -							PD-04 CONTRACT NO. 62X69				
Default		DATE =	12/17/2019	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT				

Model: Default
FILE: \\wms-prod\wms\ent\ent\com\p\w\DOT\Documents\DOT_Office\01-24-24\1\Project\01-24-24\1\CAD\DATA\CAD\Sheet10.dgn



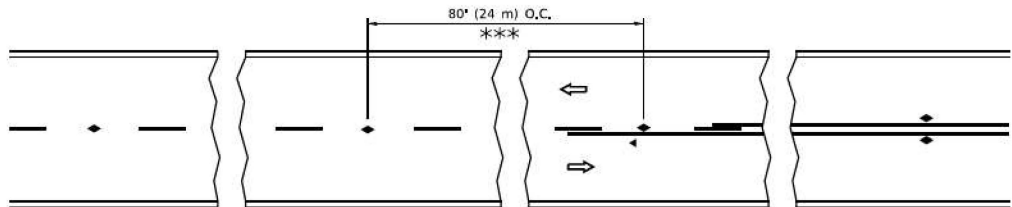
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS, CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

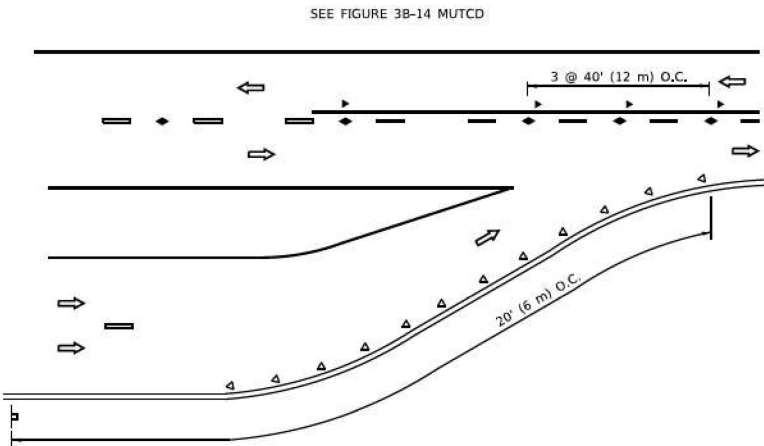
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P. RTE. 365 SECTION 2024-1031-RS COUNTY KANE TOTAL SHEETS 35 SHEET NO. 29			
SCALE: NONE				SHEET 1 OF 1 SHEETS				TC-10 CONTRACT NO. 62X69			
DATE: 06-89				TO STA.				ILLINOIS FED. AID PROJECT			

USER NAME	DESIGNED	REVISED
Lawrence,DeManche	L.H.A.	T. RAMMACHER 01-06-00
	DRAWN	A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED	A. SCHUETZE 09-15-16
PLOT DATE = 3/3/2024	DATE	D. SENDERAK 05-03-24

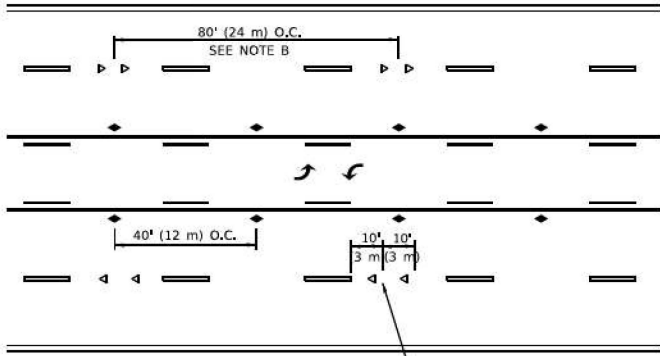


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

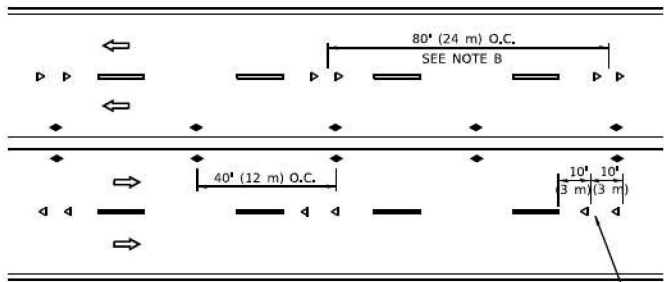
TWO-LANE/TWO-WAY



LANE REDUCTION TRANSITION

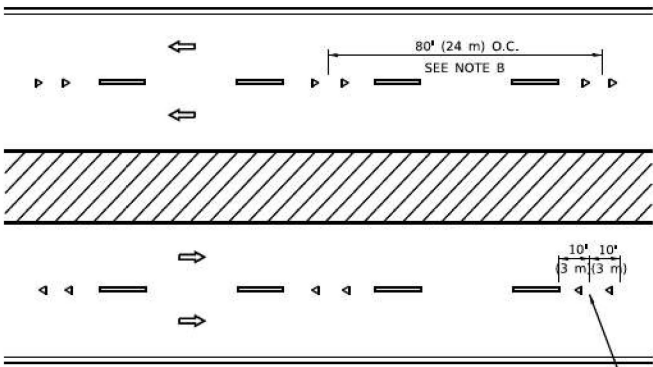


TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

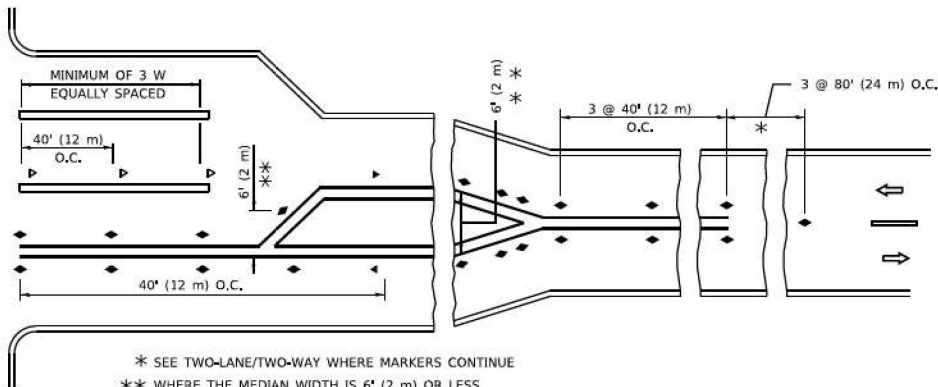
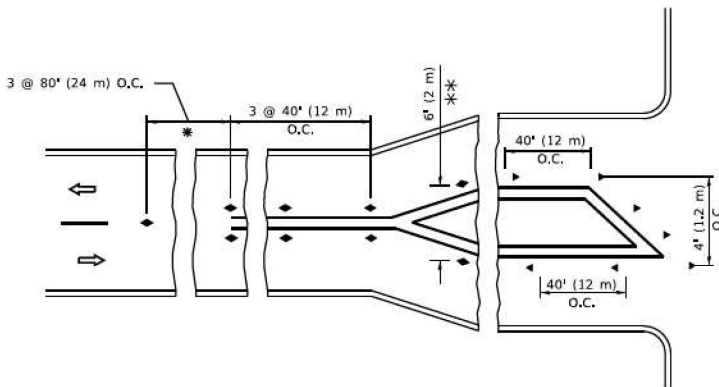
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (m) / millimeters unless otherwise shown.

USER NAME	footen]	DESIGNED	-	REVISED	- T. RAMMACH 03-12-99
		DRAWN	-	REVISED	- T. RAMMACH 01-06-00
PLOT SCALE	= 50,000 ' / in.	CHECKED	-	REVISED	- C. JUCIUS 09-09-09
PLOT DATE	= 3/4/2019	DATE	-	REVISED	- C. JUCIUS 07-01-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	30
TC-11		CONTRACT NO. 62X69		
ILLINOIS		FED. AID PROJECT		

2-LANE ROADWAY

MULTI-LANE UNDIVIDED

MULTI-LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

DETAIL "A"

DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

4' (1.2 m) WIDE MEDIANS ONLY

MEDIANS OVER 4' (1.2 m) WIDE

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

ISLAND OFFSET FROM PAVEMENT EDGE

ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

COMBINATION

LEFT AND U-TURN

U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2,4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 8' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; *RR* IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR *X*	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R* = 3.6 SQ. FT. (0.33 m ²) EACH *X* = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footenl	DESIGNED = EVERS	REVISED = C. JUCIUS 09-09-09
	DRAWN =	REVISED = C. JUCIUS 07-01-13
PLOT SCALE = 50,0000' / in.	CHECKED =	REVISED = C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE = 03-19-90	REVISED = C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE

TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.
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F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	31
TC-13		CONTRACT NO. 62X69		
		ILLINOIS	FED. AID PROJECT	

	WORK AREA
	LANE OPEN TO TRAFFIC
	ARROW BOARD
	TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
	DRUM WITH STEADY BURN LIGHT
	SIGN ASSEMBLY
	TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE
(AS SHOWN IN FIG. 1), USE FIGURE 1.

B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE
OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE
SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF
28 (710) IN HEIGHT.
3. LIGHTS SHALL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY
OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED
THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN
WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN
THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER
THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600)
AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL
FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS
ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH
REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN
TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC
CONTROL STANDARDS OR ITEMS.

LEFT
TURN
LANE

R3-100L
24 x 24 (600 x 600)

M6-2L
21 x 15 (530 x 380)

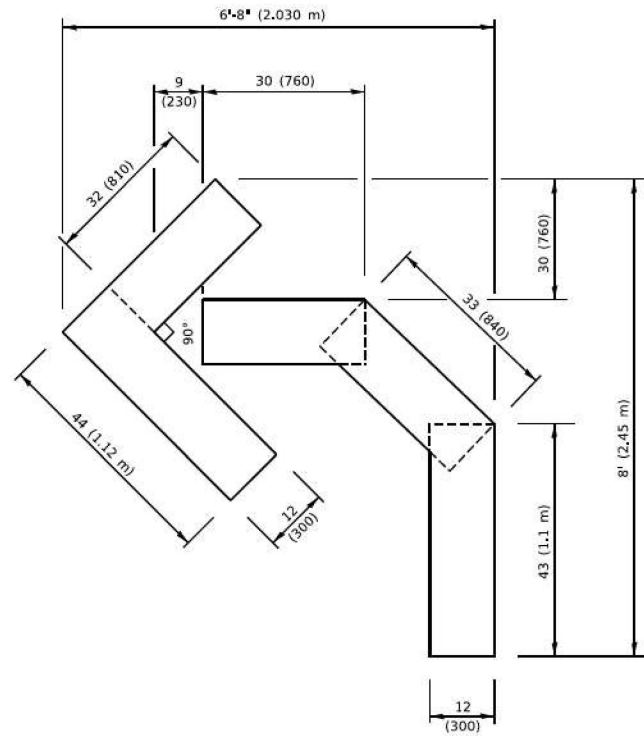
5' (1.5 m) MIN.
(SEE NOTE 7)

STABILIZE SIGN
SUPPORT WITH
SANDBAGS AS
NECESSARY

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

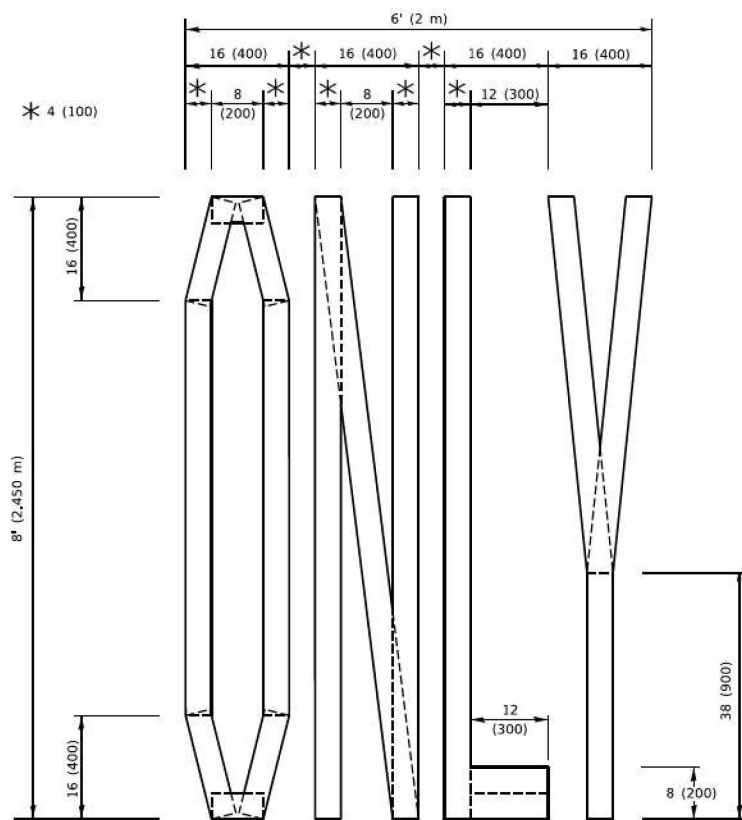
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	32
TC-14		CONTRACT NO. 62X69		
ILLINOIS		FED. AID PROJECT		



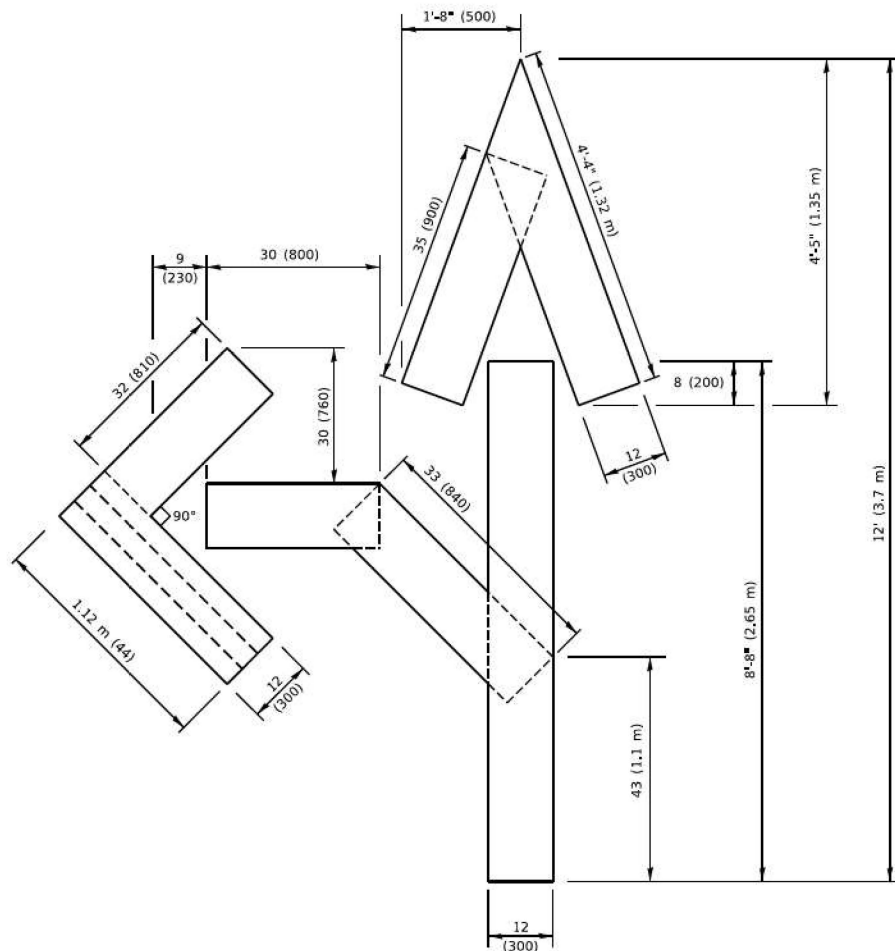
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

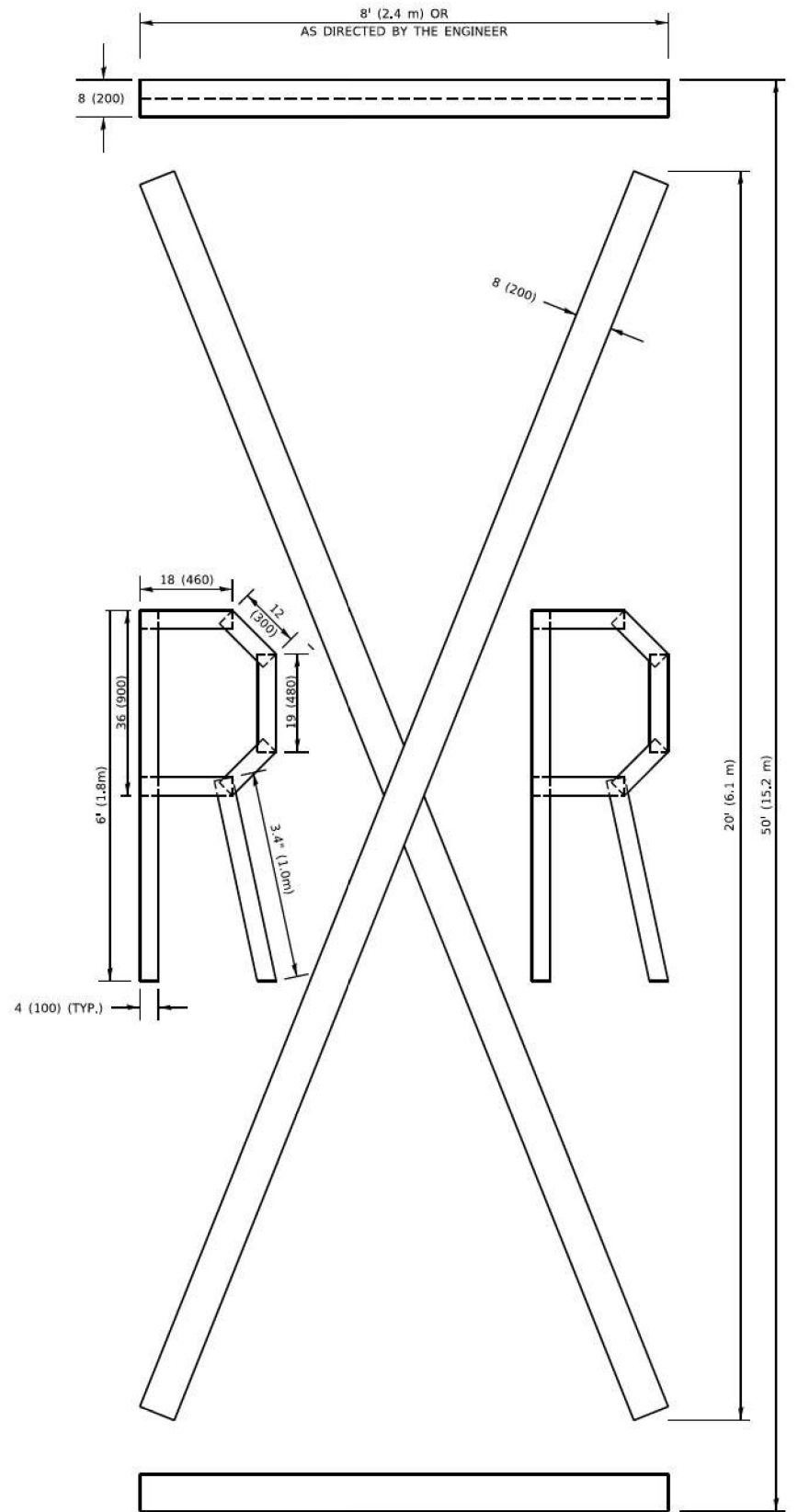


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = footen]	DESIGNED -	REVISED - T. RAMMACH 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50,0068 * / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F. A. P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2024-1031-RS	KANE	35	33
TC-16		CONTRACT NO. 62X69		
		ILLINOIS FED. AID PROJECT		

