

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	01
		ILLINOIS	CONTRACT NO. 61L65	

06-13-2025 LETTING ITEM 012

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

## PLANS FOR PROPOSED FEDERAL AID HIGHWAY FAU 2770 (SHERMER ROAD)

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR LIST OF STATE STANDARDS, SEE SHEET NO. 2

FROM FAU ROUTE 2766 (HARLEM AVE)  
TO FAU ROUTE 1312 IL 58 (GOLF RD)

RESURFACING

SECTION: 24-00114-00-RS

PROJECT: 38DJ (646)

VILLAGE OF MORTON GROVE  
COOK COUNTY

JOB: C-91-147-25



LOCATION OF SECTION INDICATED THIS - [Symbol]

### AGENCY RESPONSIBLE FOR LETTING

APPROVED: Chris Tomich 03/04 2025  
VILLAGE OF MORTON GROVE, VILLAGE ENGINEER

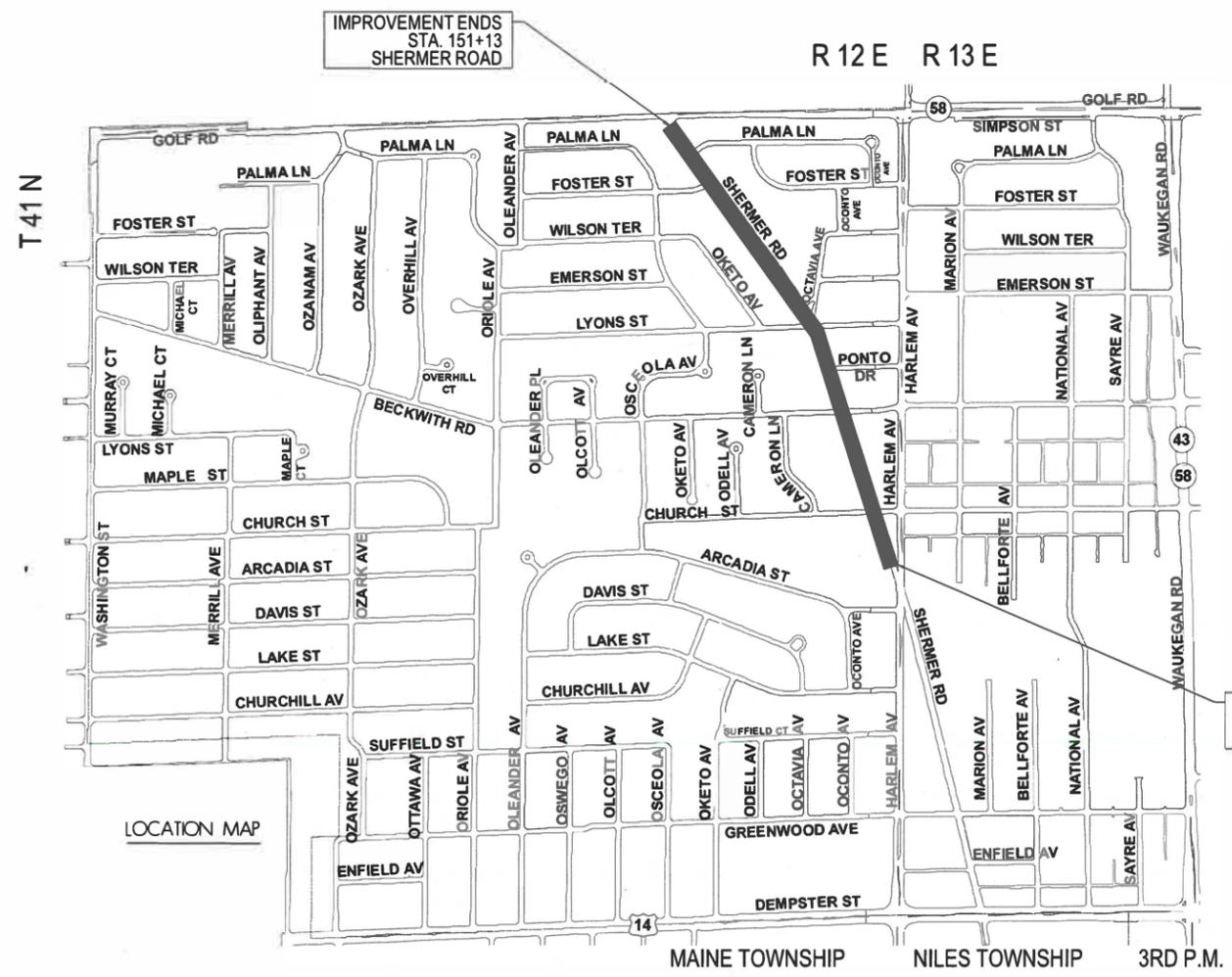
PASSED: April 9 2025  
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID  
BASED ON LIMITED  
REVIEW: April 9 2025  
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

REGISTERED P.E., STATE OF ILLINOIS

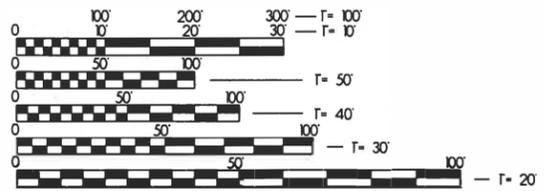
EXPIRATION DATE



#### TRAFFIC DATA - SHERMER RD

ADT (2022) = 6,400  
POSTED SPEED 30 MPH  
DESIGN SPEED 35 MPH

FUNCTIONAL CLASSIFICATION:  
MAJOR COLLECTOR (URBAN)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: CHRIS COMIN  
PROJECT MANAGER:

CONTRACT NO. 61L65

GROSS LENGTH = 3,201 FT. = 0.61 MILE  
NET LENGTH = 3,201 FT. = 0.61 MILE

IMPROVEMENT BEGINS  
STA. 119+12  
SHERMER ROAD

PLANS PREPARED BY THE  
VILLAGE OF MORTON GROVE  
ENGINEERING DIVISION



FEDERAL AID PROGRAM ENGINEER, CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

GENERAL NOTES

A. GENERAL

1. EXISTING UTILITIES

WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION AND ELEVATIONS OF UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO LOCATION AND ELEVATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDERS. THE ENGINEER AND THE OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION AND ELEVATION OF UTILITY FACILITIES, NOR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT IS EACH CONTRACTOR'S AND SUBCONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND DETERMINE THE ACTUAL LOCATION AND ELEVATION OF ALL UTILITIES. EACH CONTRACTOR AND SUBCONTRACTOR SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION AND ELEVATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

2. EXISTING ELEVATIONS AND LOCATIONS

EVERY CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY THE ELEVATIONS AND LOCATIONS OF ALL EXISTING INFORMATION AS SHOWN ON THE PLANS AND NOTIFY THE ENGINEER OF ALL DISCREPANCIES PRIOR TO THE COMMENCEMENT OF THE WORK.

3. PAVEMENT DISTURBED DURING CONSTRUCTION

ANY EXISTING ROADWAY PAVEMENT OR SHOULDER DISTURBED DURING CONSTRUCTION SHALL BE RESTORED IN ACCORDANCE WITH I.D.O.T. AND VILLAGE OF MORTON GROVE STANDARDS.

4. FINAL FRAME ADJUSTMENTS

THE PAVING CONTRACTOR SHALL MAKE ALL NECESSARY FINAL ADJUSTMENTS TO EXISTING AND PROPOSED FRAMES, GRATES, LIDS AND STRUCTURES TO MEET FINAL FINISHED GRADES.

5. RECORD DRAWINGS

EACH CONTRACTOR AND SUBCONTRACTOR SHALL MAINTAIN AND KEEP UP TO DATE A SET OF "RECORD DRAWINGS" SHOWING ALL CHANGES FROM THE ORIGINAL PLANS. ALL CONTRACTORS AND SUBCONTRACTORS SHALL DELIVER THE "RECORD DRAWINGS" TO THE ENGINEER AT THE CONCLUSION OF THE PROJECT. THE ENGINEER WILL TRANSFER THE INFORMATION TO THE ORIGINAL PLANS.

6. CONTACT

THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

B. PAVING

1. THE PAVING CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PROPER DRAINAGE ON THE PROPOSED PAVEMENT. WHERE THE CONTRACTOR IS IN DOUBT ABOUT THE PROPER DIRECTION OF DRAINAGE, HE SHALL REQUEST ASSISTANCE FROM THE ENGINEER.
2. BASE COURSE SHALL NOT BE PLACED ADJACENT TO NEWLY PLACED CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
3. PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.
4. THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED WITH TRACKED EQUIPMENT.

C. RESTORATION AND TREE PRESERVATION

1. ALL PARKWAY TREES SHALL BE PRESERVED. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DAMAGE TO SUCH TREES ~~AND SHALL PROVIDE TEMPORARY FENCES AND OTHER MEASURES AS NECESSARY OR AS DIRECTED BY THE ENGINEER.~~
2. CONTRACTOR SHALL TAKE EXTRA PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
3. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRAING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.

STAGING AND TRAFFIC CONTROL GENERAL NOTES

1. STAGING WILL BE DONE SEQUENTIALLY AS SHOWN IN THE STAGING AND TRAFFIC SEQUENCE TO MAINTAIN TRAFFIC THROUGHOUT THE AREA.
2. WORK WILL BE DONE ALONG ONE SIDE OF THE ROADWAY AT A TIME.
3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE OF TRAFFIC CONTROL SWITCH BETWEEN STAGES. THE CONTRACTOR SHALL ALSO CONFIRM THE PLANNED STAGE CHANGE WITH THE ENGINEER 24 HOURS PRIOR.
4. PRIOR TO REMOVING TRAFFIC CONTROL AND OPENING CLOSED PAVEMENT AREAS TO TRAFFIC, THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION PAY ITEMS.
5. THE CONTRACTOR SHALL GAP CONSTRUCT THE PAVEMENT AND ENTRANCES TO MAINTAIN ACCESS.
6. PAVEMENT, CURB, SIDEWALK AND DRIVEWAYS TO BE REMOVED MUST BE COMPLETELY REMOVED FROM THE SITE BY THE END OF THE DAY. NO REMOVAL ITEMS MAY BE STORED ON SITE.
7. DRIVEWAY ENTRANCE SIGNING (TC-26) WILL BE INSTALLED AT EACH COMMERCIAL ENTRANCE ALONG SHERMER ROAD WITHIN THE PROJECT LIMITS.
8. WORKER SIGNS (W21-1) SHALL BE REMOVED OR COVERED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.

STAGING AND TRAFFIC SEQUENCE OF CONSTRUCTION

STAGE 1

CONSTRUCTION

1. REMOVE EXISTING CURB AND GUTTER AND SIDEWALK ALONG THE EAST SIDE OF SHERMER ROAD.
2. DRAINAGE STRUCTURES WITHIN CURB REMOVAL LIMITS TO BE ADJUSTED.
3. FRAME AND POUR NEW CURB AND GUTTER AND SIDEWALK ALONG THE EAST SIDE OF SHERMER ROAD.

TRAFFIC CONTROL

1. MAINTAIN TRAFFIC UTILIZING STANDARDS 701427, 701501, 701701 AND 701801.
2. WORK AT ENTRANCES / ALLEYS WILL BE GAPPED TO ALLOW ACCESS FOR RESIDENTS.

STAGE 2

CONSTRUCTION

1. REMOVE EXISTING CURB AND GUTTER AND SIDEWALK ALONG THE WEST SIDE OF SHERMER ROAD.
2. DRAINAGE STRUCTURES WITHIN CURB REMOVAL LIMITS TO BE ADJUSTED.
3. FRAME AND POUR NEW CURB AND GUTTER AND SIDEWALK ALONG THE WEST SIDE OF SHERMER ROAD.

TRAFFIC CONTROL

1. MAINTAIN TRAFFIC UTILIZING STANDARDS 701427, 701501, 701701 AND 701801.
2. WORK AT ENTRANCES / ALLEYS WILL BE GAPPED TO ALLOW ACCESS FOR RESIDENTS.

STAGE 3

CONSTRUCTION

1. STRUCTURES WITHIN MAINLINE SHERMER ROAD WILL BE LOWERED PER IDOT D1 DETAIL BD-08.
2. MILL SHERMER ROAD.
3. APPLY TEMPORARY PAVEMENT MARKINGS.

TRAFFIC CONTROL

1. MAINTAIN TRAFFIC UTILIZING STANDARDS 701311, 701427, 701501, 701701 AND 701801.

STAGE 4

CONSTRUCTION

1. PAVEMENT PATCHING
2. STRUCTURES WITHIN MAINLINE SHERMER ROAD TO BE ADJUSTED.

TRAFFIC CONTROL

1. MAINTAIN TRAFFIC UTILIZING STANDARDS 701311, 701427, 701501, 701701 AND 701801.

STAGE 5

CONSTRUCTION

1. PAVE BINDER COURSE.
2. PAVE SURFACE COURSE.
3. ESTABLISH PERMENANT PAVEMENT MARKINGS.

TRAFFIC CONTROL

1. MAINTAIN TRAFFIC UTILIZING STANDARDS 701301, 701311, 701427, 701501, 701701 AND 701801.

INDEX OF SHEETS

1	COVER SHEET
2	GENERAL NOTES, STAGING AND TRAFFIC CONTROL AND SEQUENCE NOTES, INDEX OF SHEETS, IDOT STANDARD DRAWINGS AND DISTRICT 1 DETAILS
3-4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
6-8	EXISTING ROADWAY & PROPOSED IMPROVEMENT PLAN
9-10	CURB & GUTTER DETAILS
11-13	PAVEMENT MARKING PLANS
14	DRIVEWAY DETAILS (BD-01)
15	DRIVEWAY DETAILS (BD-02)
16	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
17	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
18	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS (BD-24)
19	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
20	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
21	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
22	DISTRICT ONE TYPICAL PAVEMENT MARKINGS DETAILS (TC-13)
23	SHORT TERM PAVEMENT MARKING LETTERS & SYMBOLS (TC-16)
24	ARTERIAL ROAD INFORMATION SIGN (TC-22)

LIST OF IDOT STANDARD DRAWINGS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-12	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-06	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-05	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-06	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-07	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
604001-05	FRAMES AND LIDS TYPE 1
604051-04	FRAMES AND GRATE TYPE 11
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATIONS 2L 2W 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE MULTILANE INTERMITTENT OR MOVING OPERATIONS 40MPH OR LESS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

LIST OF IDOT DISTRICT 1 DETAILS

BD-01	DRIVEWAY DETAILS
BD-02	DRIVEWAY DETAILS
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS
BD-32	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS DETAILS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS & SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN

MODEL & MODEL NAMES  
FILE NUMBER & FILES



USER NAME = \$USERS	DESIGNED - JG/MG/CRT	REVISED - _____
DRAWN - JG	REVISIONS - _____	
PLOT SCALE = \$SCALES	CHECKED - CRT	REVISED - _____
PLOT DATE = \$DATES	DATE - 04-02-2025	REVISED - _____

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
INDEX OF SHEETS, GENERAL NOTES, STANDARD DRAWINGS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	02
			CONTRACT NO. 61L65	
		ILLINOIS	FED. AID PROJECT	

SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
	20101100	TREE TRUNK PROTECTION	EACH	15	15	
	20200100	EARTH EXCAVATION	CU YD	10	10	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	157	157	
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1186	1186	
	25200110	SODDING, SALT TOLERANT	SQ YD	1186	1186	
	25200200	SUPPLEMENTAL WATERING	UNIT	65	65	
	28000510	INLET FILTERS	EACH	24	24	
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	40	40	
	31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	722	722	
	31101600	SUBBASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	60	60	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	7265	7265	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	9603	9603	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	30	30	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	217	217	
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	660	660	
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1337	1337	
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	50	50	

\* SPECIALTY ITEM

SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2092	2092	
	42400800	DETECTABLE WARNINGS	SQ FT	260	260	
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	15283	15283	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	60	60	
	44000600	SIDEWALK REMOVAL	SQ FT	2278	2278	
	44200942	CLASS B PATCHES, TYPE III, 8 INCH	SQ YD	18	18	
	44200944	CLASS B PATCHES, TYPE IV, 8 INCH	SQ YD	484	484	
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	4	4	
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	40	40	
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	94	94	
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	772	772	
	60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2	2	
	60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1	
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	4	4	
	60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	1	1	
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	32		32
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	3		3

\* SPECIALTY ITEM

MODEL: S:\MODEL\MANES  
FILE NAME: ST11E13



USER NAME = SUSERS	DESIGNED - IG/MG/CRT	REVISED - _____
PLOT SCALE = SCALES	DRAWN - JG	REVISED - _____
PLOT DATE = SDATES	CHECKED - CRT	REVISED - _____
	DATE - 04-02-2025	REVISED - _____

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2770	SECTION 24-00114-00-RS	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 03
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				

SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1		1
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1		1
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	4		4
	67100100	MOBILIZATION	L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	18100	18100	
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	6050	6050	
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	330	330	
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	21645	21645	
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	3498	3498	
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12" - PAINT	FOOT	2250	2250	
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24" - PAINT	FOOT	789	789	
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	110	110	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7215	7215	
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1166	1166	

\* SPECIALTY ITEM

SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	750	750	
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	263	263	
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	78	78	
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	19	19	
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	13238	13238	
	Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	18	18	
	Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	3	3	
	X4060280	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	10	10	
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	400	400	
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	1900	1900	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	15	15	
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	

\* SPECIALTY ITEM

MODEL: SMODEL.MAKES  
FILE NAME: SFILES



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DESIGNED - JG/MG/CRT  
DRAWN - JG  
CHECKED - CRT  
PLOT DATE = SDATES  
DATE - 04-02-2025

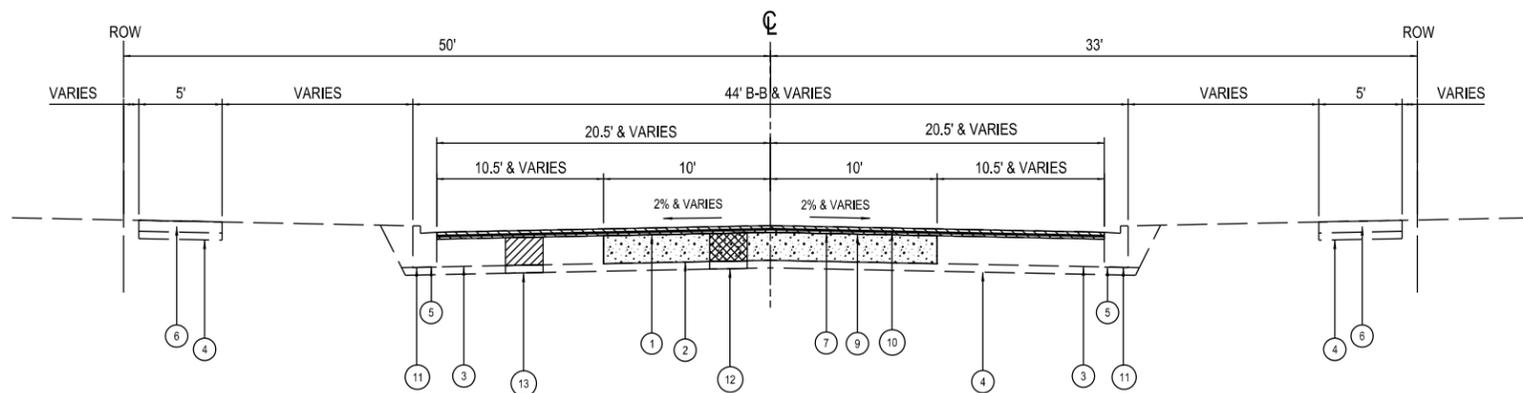
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

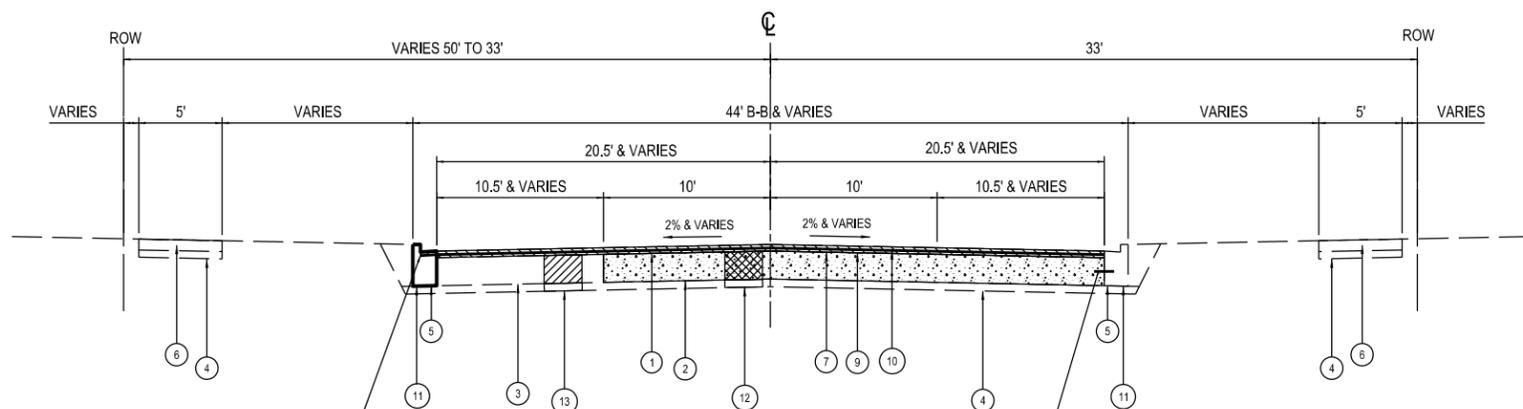
SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	04
CONTRACT NO. 61165				
ILLINOIS FED. AID PROJECT				



EXISTING AND PROPOSED  
 FAU 2770 (SHERMER RD) - STA. 119+12 TO STA. 129+70  
 FAU 2766 (HARLEM AVE) TO FAU 1313 (BECKWITH RD)



EXISTING AND PROPOSED  
 FAU 2770 (SHERMER RD) - STA. 129+70 TO STA. 151+13  
 FAU 1313 (BECKWITH RD) TO FAU 1312 (GOLF RD)

EXISTING OVERLAPPED CURB & GUTTER ON THE LEFT SIDE BETWEEN STA 133+81 AND 150+62 IS TO BE REMOVED & REPLACED WITH STANDARD PROFILE CURB & GUTTER AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ADJACENT ROAD CROSS SLOPE WILL BE ADJUSTED ACCORDINGLY.

EXISTING TIE BAR TO REMAIN IN PLACE  
 (SEE CURB & GUTTER REMOVAL & REPLACEMENT DETAIL).

LEGEND (EXISTING)

- ① HOT-MIX ASPHALT SURFACE, 4 1/4"-9".
- ② PORTLAND CEMENT CONCRETE BASE COURSE, 6"-7 3/4".
- ③ HOT-MIX ASPHALT MATERIAL BASE COURSE, 6"-7 3/4".
- ④ SUBBASE GRANULAR MATERIAL, TYPE B, 4".
- ⑤ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (TYP).
- ⑥ PORTLAND CEMENT CONCRETE SIDEWALK, 5".
- ⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/4".
- ⑧ NOT USED

LEGEND (PROPOSED)

- ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4".
- ⑩ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 1 1/2".
- ⑪ INTERMITTENT COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT (MATCH EXISTING).
- ⑫ CLASS B PATCHES, 8", REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER).
- ⑬ CLASS D PATCHES, 8", REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER).

MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
<b>PAVEMENT RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL 9.5, MIX "D", N50; 1-1/2"	4% @ 50 GYR	LR 1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4"	3.5% @ 50 GYR	LR 1030-2
<b>PATCHING</b>		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 GYR	LR 1030-2
<b>HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL 9.5, MIX "D", N50 3" (IN 2 LIFTS)	4% @ 50 GYR	LR 1030-2
QMP DESIGNATION: LOCAL QUALITY ASSURANCE / QUALITY MANAGEMENT QC/QA PER LR 1030-2		

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
2. THE "AC" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.
3. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE P HMA BC IL-4.75 N50.

MODEL: S:\MODELS\MAMES  
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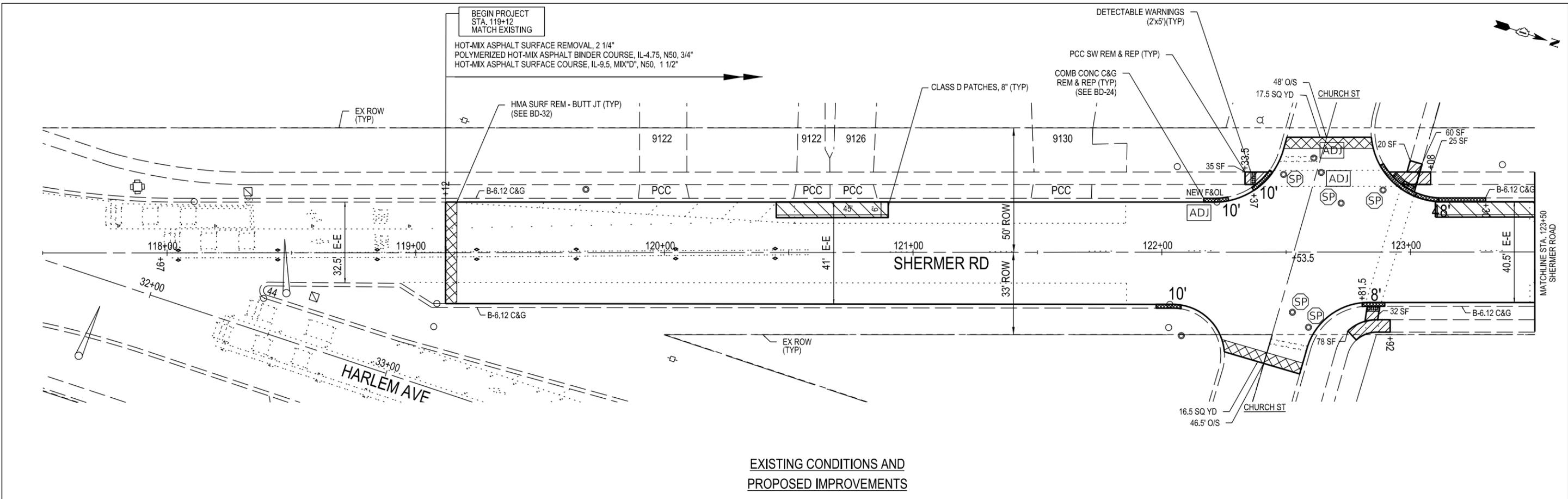
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

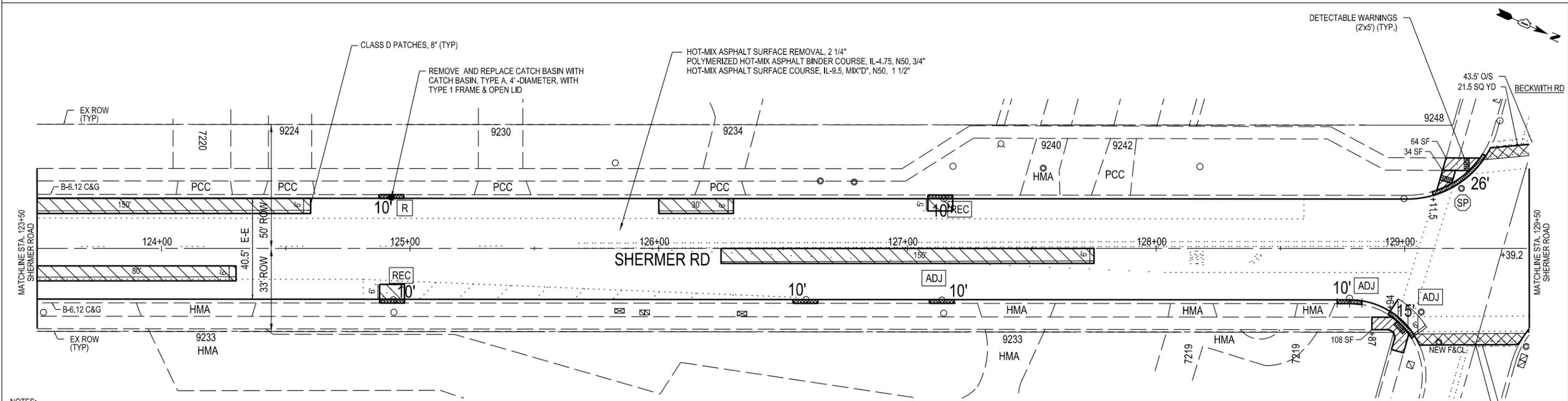
SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
 TYPICAL CROSS SECTIONS

SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	05
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61L65	



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS

- NOTES:
1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
  2. CURB & GUTTER REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  3. CLASS B AND CLASS D PATCHES WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  4. P.C.C. SIDEWALK REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  5. STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED, OR FRAMES AND LIDS TO BE REPLACED, WILL BE LOCATED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.

LEGEND

SP	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
ADJ	DRAINAGE STRUCTURE TO BE ADJUSTED
REC	DRAINAGE STRUCTURE TO BE RECONSTRUCTED



MODEL: SMOBELNAMRES  
FILE NAME: ST1E15

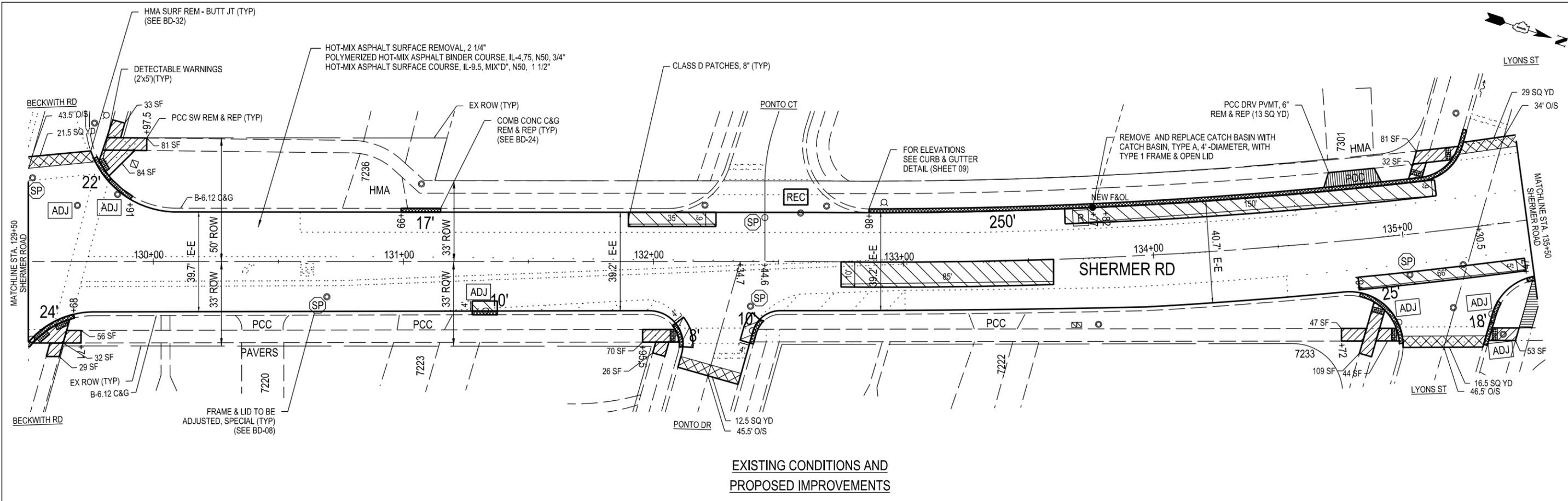


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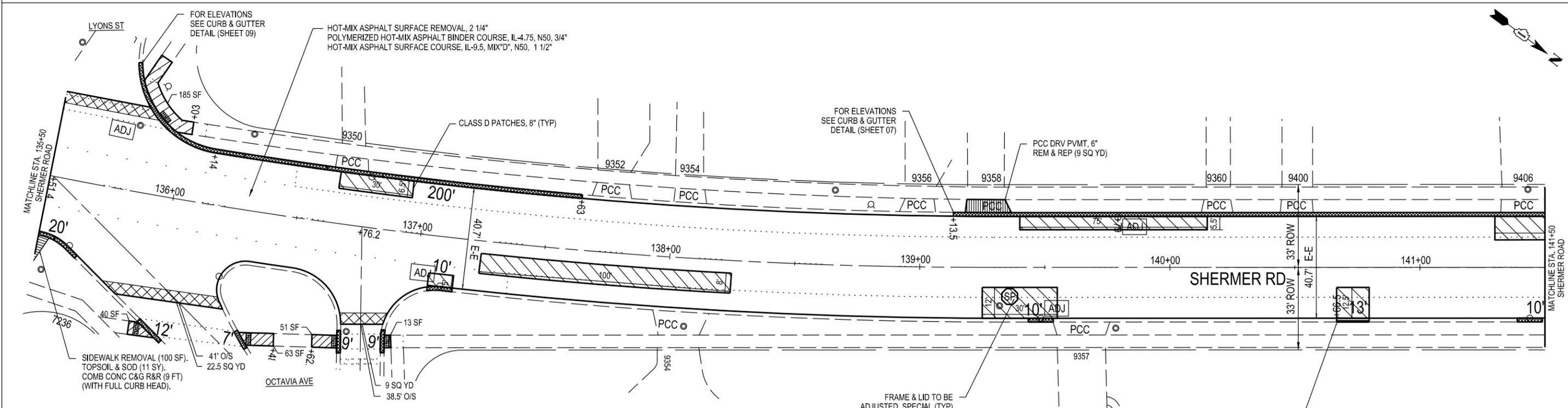
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
ROADWAY PLANS  
SCALE: 1"=20' SHEET 05 OF 09 SHEETS STA. 119+12 TO STA. 129+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	06
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS

- NOTES:
1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
  2. CURB & GUTTER REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  3. CLASS B AND CLASS D PATCHES WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  4. P.C.C. SIDEWALK REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  5. STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED, OR FRAMES AND LIDS TO BE REPLACED, WILL BE LOCATED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.

LEGEND

	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	DRAINAGE STRUCTURE TO BE ADJUSTED
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED



MODEL: SMOBELNAMES  
FILE: ROADP: STILES

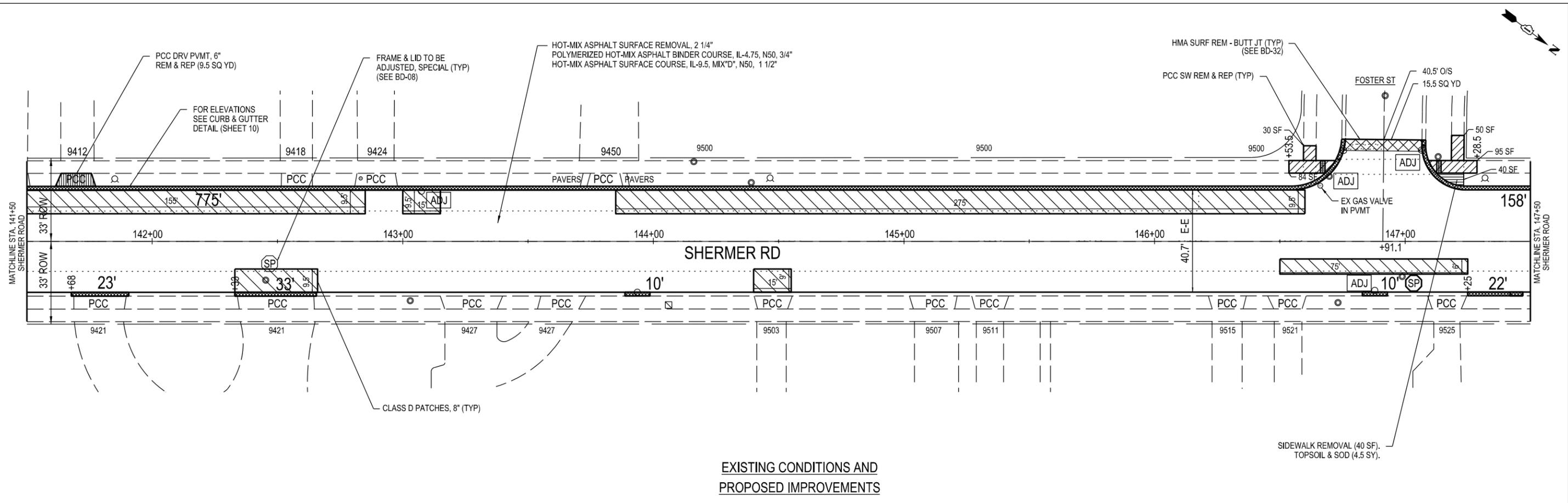


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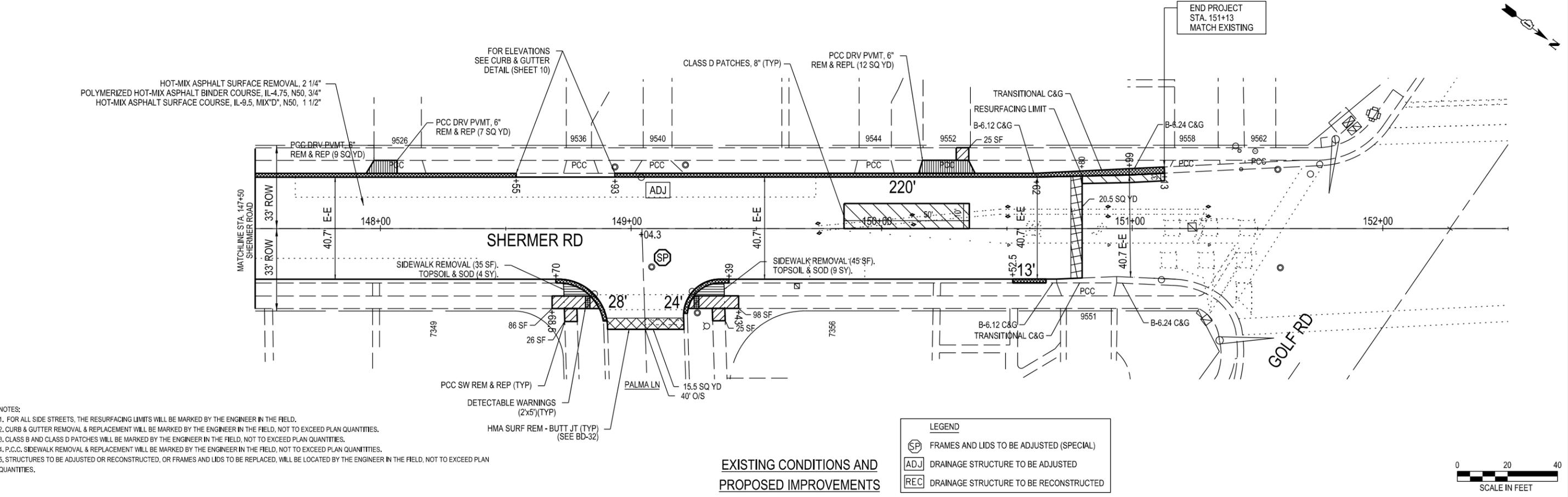
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
ROADWAY PLANS  
SCALE: 1"=20' SHEET X OF SHEETS STA. 129+50 TO STA. 141+50

F.A.U. RTE. 2770	SECTION 24-00114-00-R5	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 07
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS

- NOTES:
1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
  2. CURB & GUTTER REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  3. CLASS B AND CLASS D PATCHES WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  4. P.C.C. SIDEWALK REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
  5. STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED, OR FRAMES AND LIDS TO BE REPLACED, WILL BE LOCATED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.

LEGEND	
	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	DRAINAGE STRUCTURE TO BE ADJUSTED
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED



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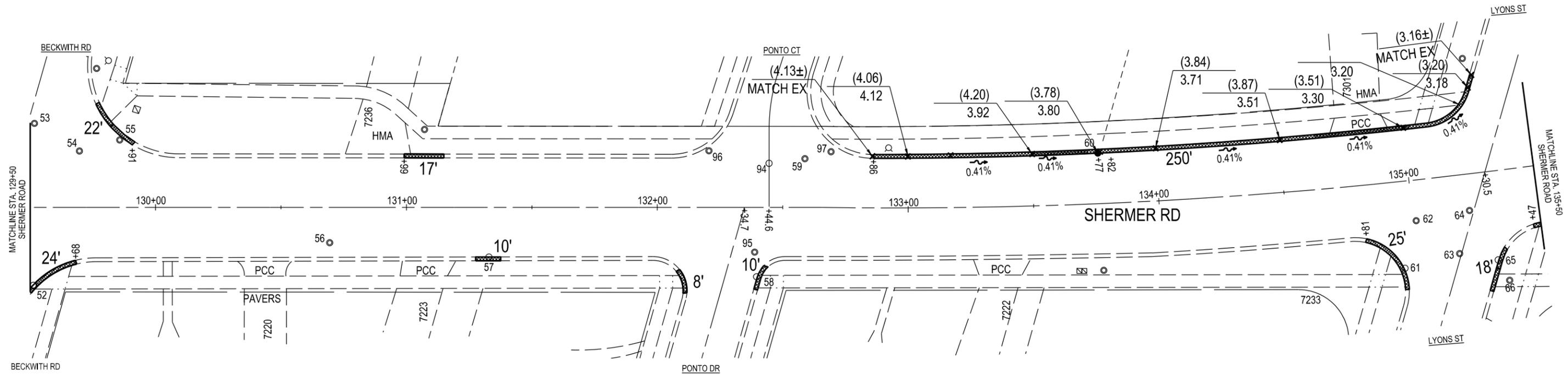


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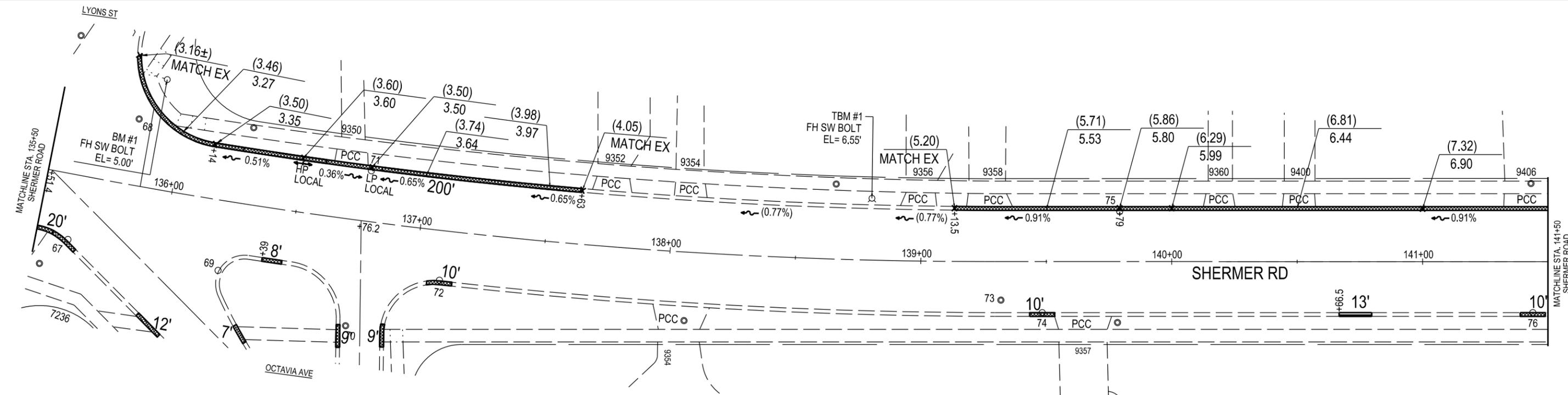
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
ROADWAY PLANS  
SCALE: 1"=20' SHEET X OF SHEETS STA. 141+50 TO STA. 151+77

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	08
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



- NOTES:
- ELEVATIONS SHOWN FOR CURB & GUTTER ARE FLOW LINE UNLESS OTHERWISE NOTED.
  - EXISTING ELEVATIONS AND SLOPES ARE IN PARENTHESIS. PROPOSED ELEVATIONS AND SLOPES ARE WITHOUT PARENTHESIS.

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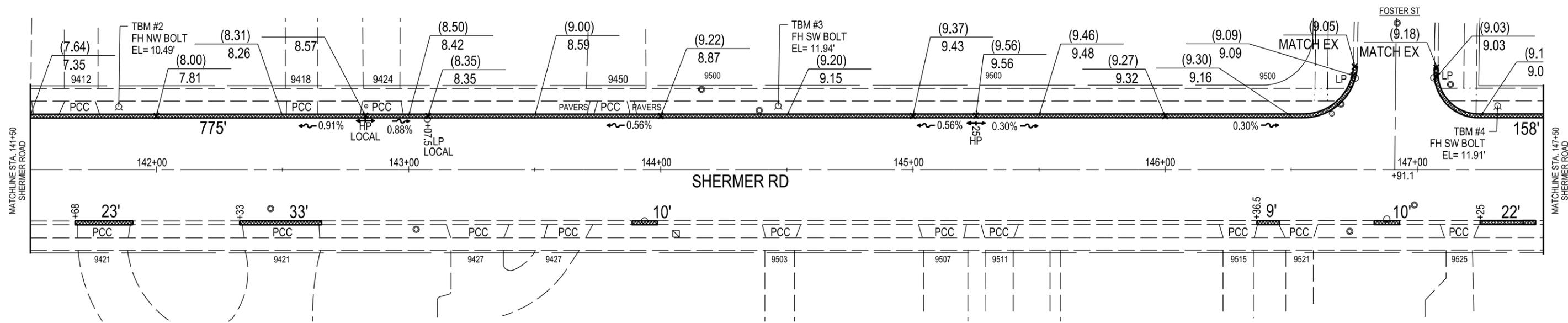


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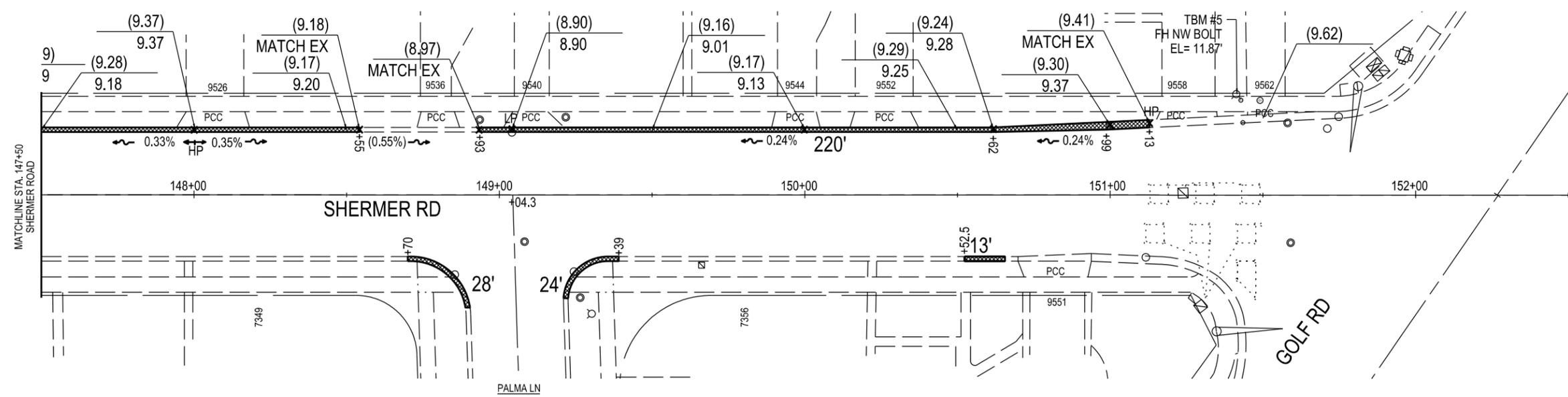
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
CURB & GUTTER DETAIL  
SCALE: 1"=20' SHEET X OF SHEETS STA. 129+50 TO STA. 141+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	09
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND PROPOSED IMPROVEMENTS

- NOTES:
- ELEVATIONS SHOWN FOR CURB & GUTTER ARE FLOW LINE UNLESS OTHERWISE NOTED.
  - EXISTING ELEVATIONS AND SLOPES ARE IN PARENTHESIS. PROPOSED ELEVATIONS AND SLOPES ARE WITHOUT PARENTHESIS.



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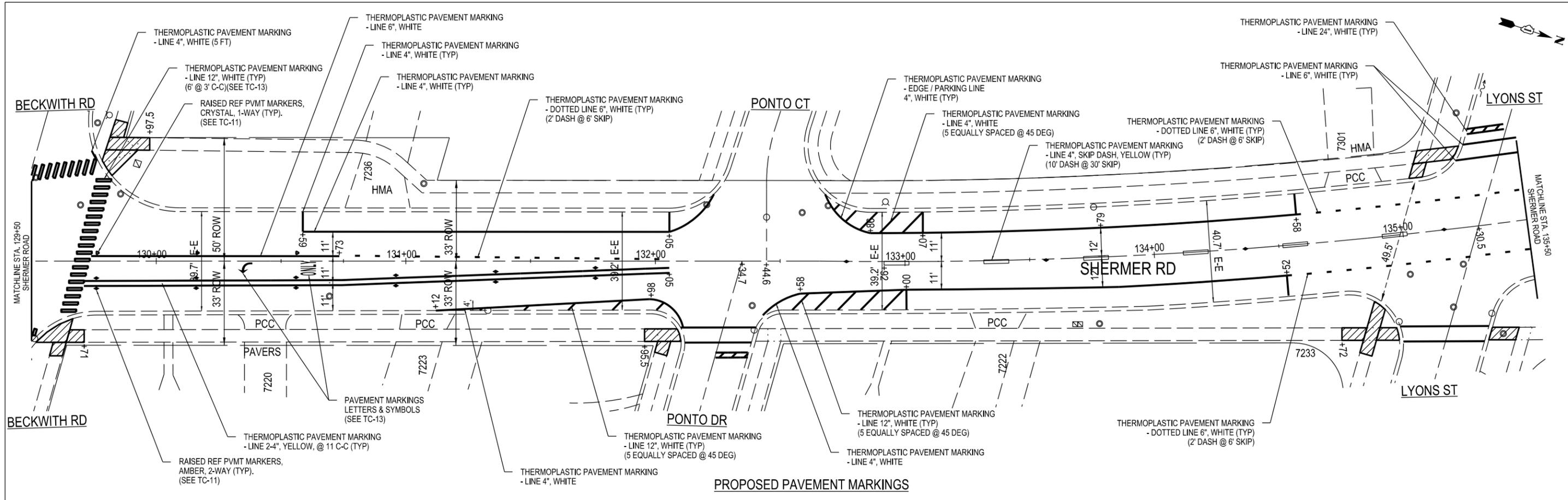
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

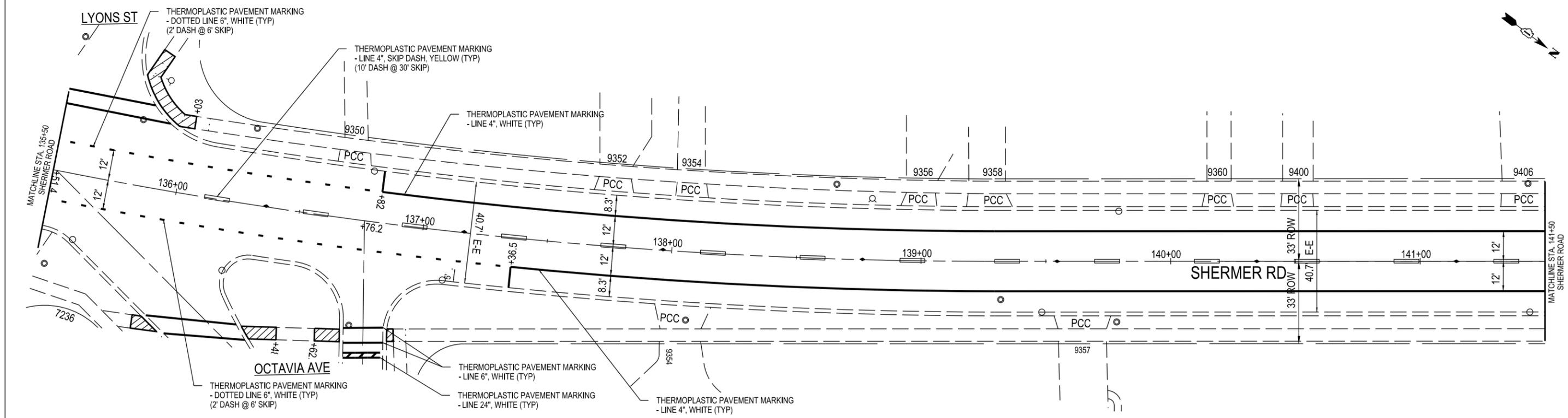
SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
CURB & GUTTER DETAIL  
SCALE: 1"=20' SHEET X OF SHEETS STA. 141+50 TO STA. 151+13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	10
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				





PROPOSED PAVEMENT MARKINGS



PROPOSED PAVEMENT MARKINGS

- NOTES:
1. PAVEMENT MARKINGS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-13) WHEREVER POSSIBLE.
  2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-11) WHEREVER POSSIBLE.



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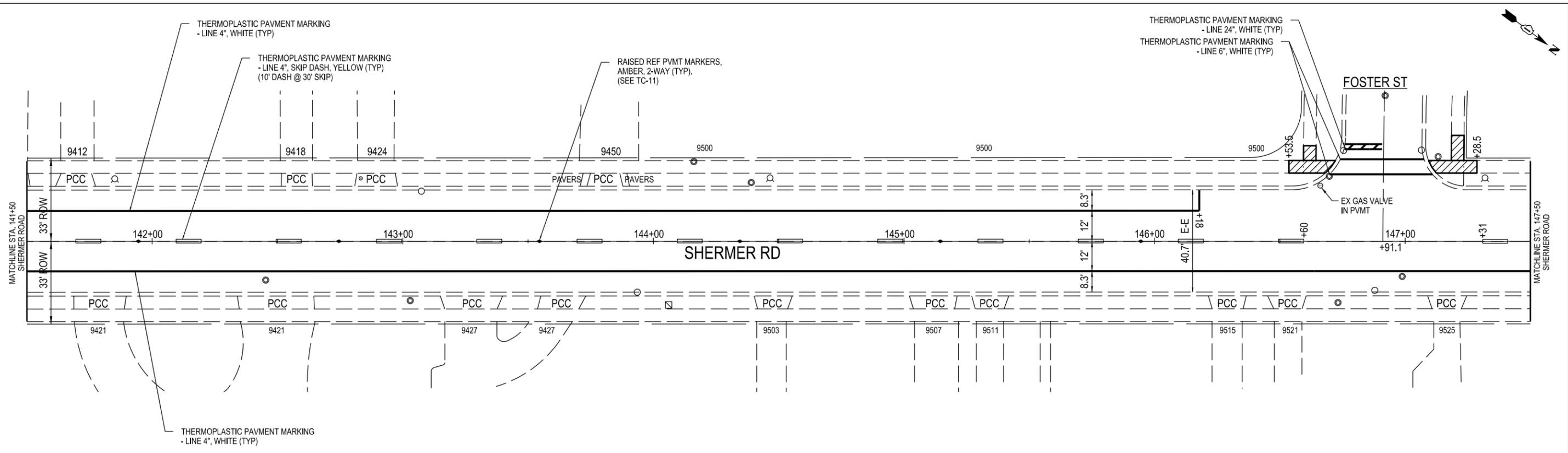


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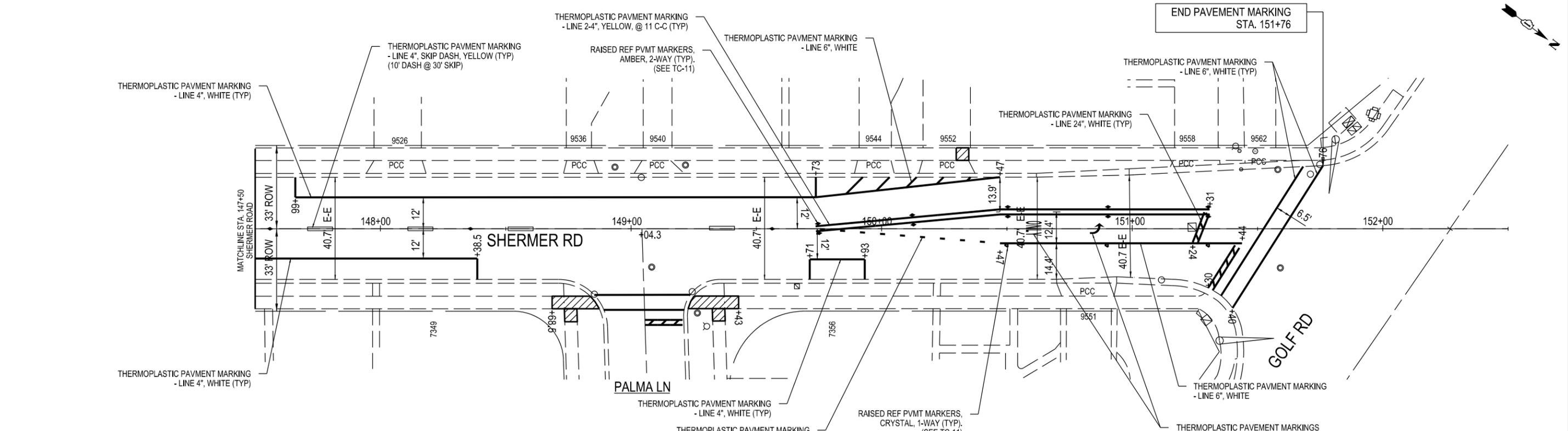
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
PAVEMENT MARKING PLANS  
SCALE: 1"=20' SHEET X OF SHEETS STA. 129+50 TO STA. 141+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	12
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



PROPOSED PAVEMENT MARKINGS



PROPOSED PAVEMENT MARKINGS

- NOTES:
1. PAVEMENT MARKINGS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-13) WHEREVER POSSIBLE.
  2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-11) WHEREVER POSSIBLE.



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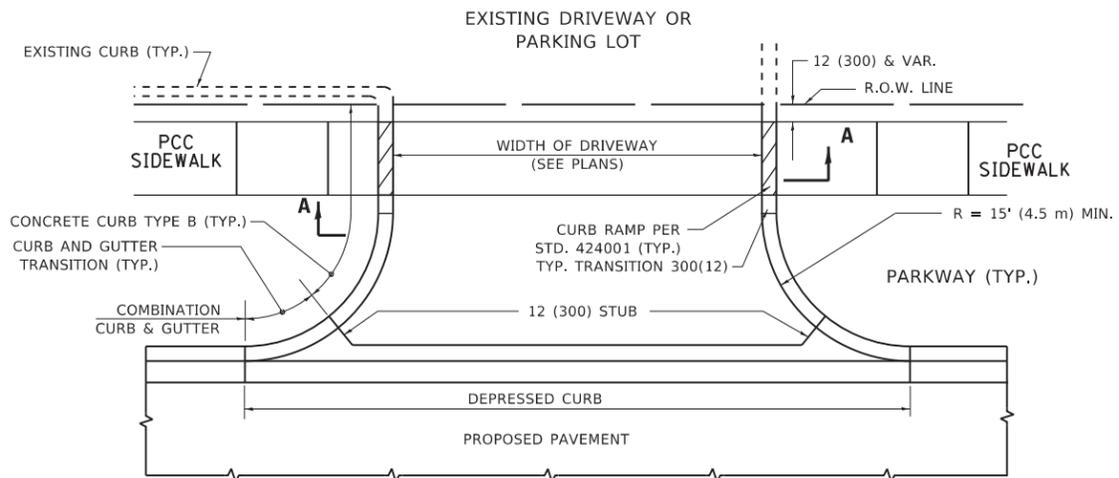
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

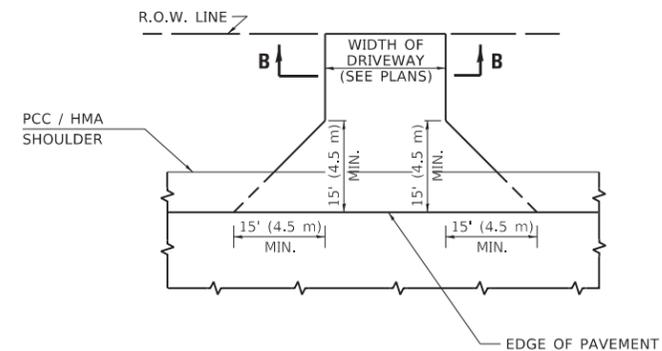
SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
PAVEMENT MARKING PLANS

SCALE: 1"=20' SHEET X OF SHEETS STA. 141+50 TO STA. 151+77

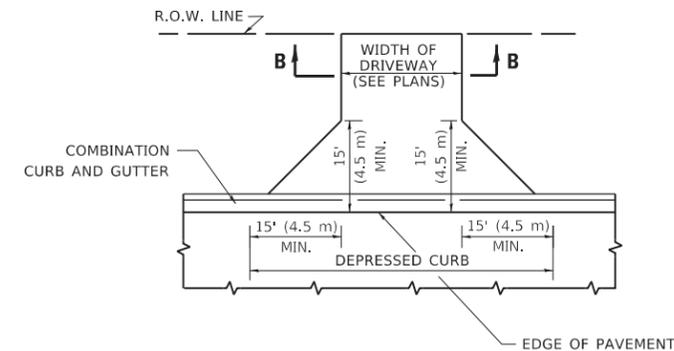
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2770	24-00114-00-R5	COOK	24	13
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



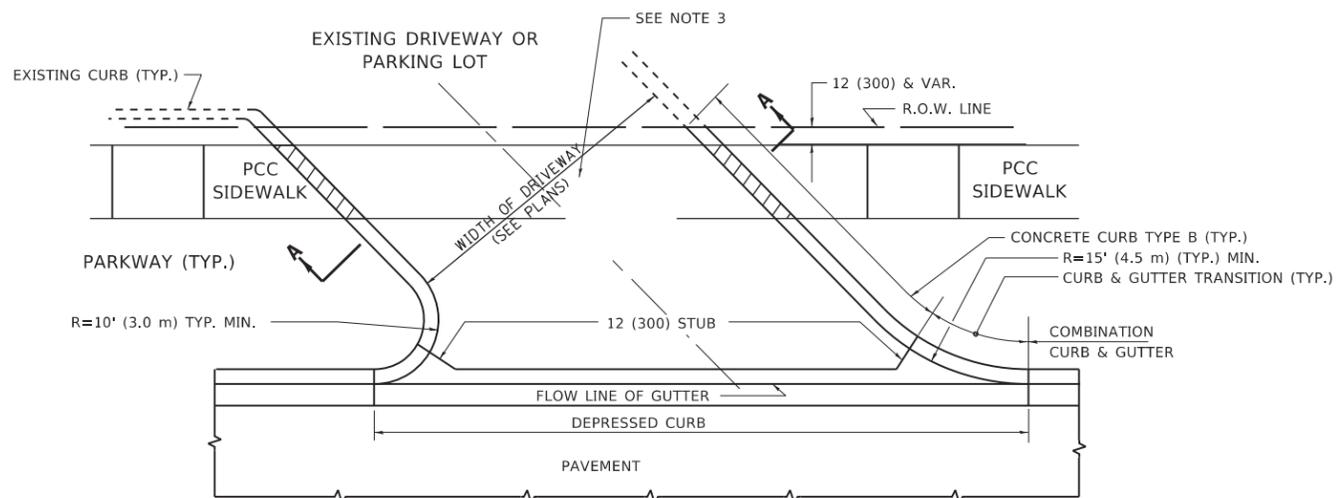
**WITH CONCRETE CURB, TYPE B**



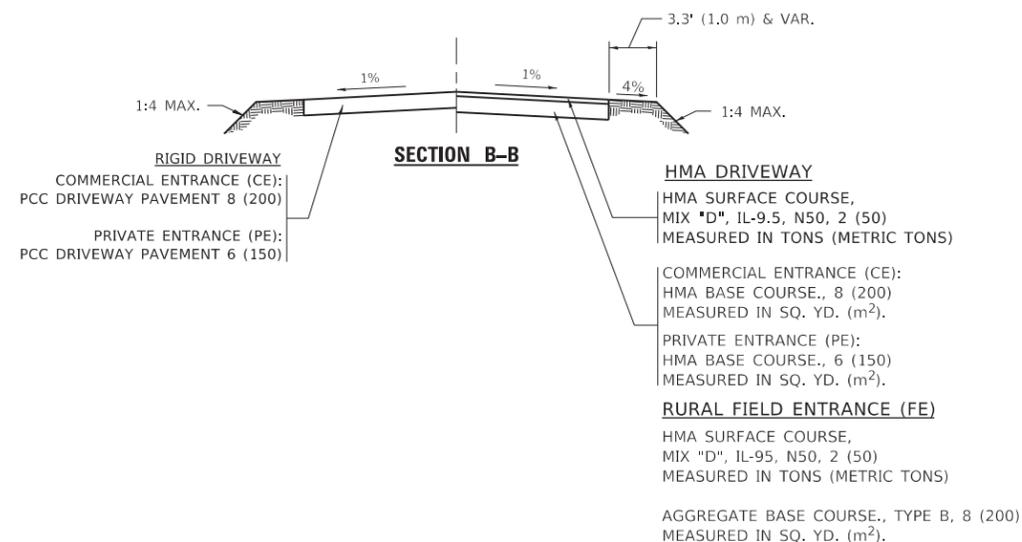
**ADJACENT TO PCC /HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**WITH CONCRETE CURB, TYPE B**



**RIGID DRIVEWAY**  
 COMMERCIAL ENTRANCE (CE):  
 PCC DRIVEWAY PAVEMENT 8 (200)  
 MEASURED IN SQ. YD. (m<sup>2</sup>)  
 PRIVATE ENTRANCE (PE):  
 PCC DRIVEWAY PAVEMENT 6 (150)  
 MEASURED IN SQ. YD. (m<sup>2</sup>)



**SECTION A-A**

**HMA DRIVEWAY**  
 HMA SURFACE COURSE,  
 MIX "D", IL-9.5, N50, 2 (50)  
 MEASURED IN TONS (METRIC TONS)  
 COMMERCIAL ENTRANCE (CE):  
 HMA BASE COURSE, 8 (200)  
 MEASURED IN SQ. YD. (m<sup>2</sup>).  
 PRIVATE ENTRANCE (PE):  
 HMA BASE COURSE, 6 (150)  
 MEASURED IN SQ. YD. (m<sup>2</sup>).

**GENERAL NOTES**

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.
- COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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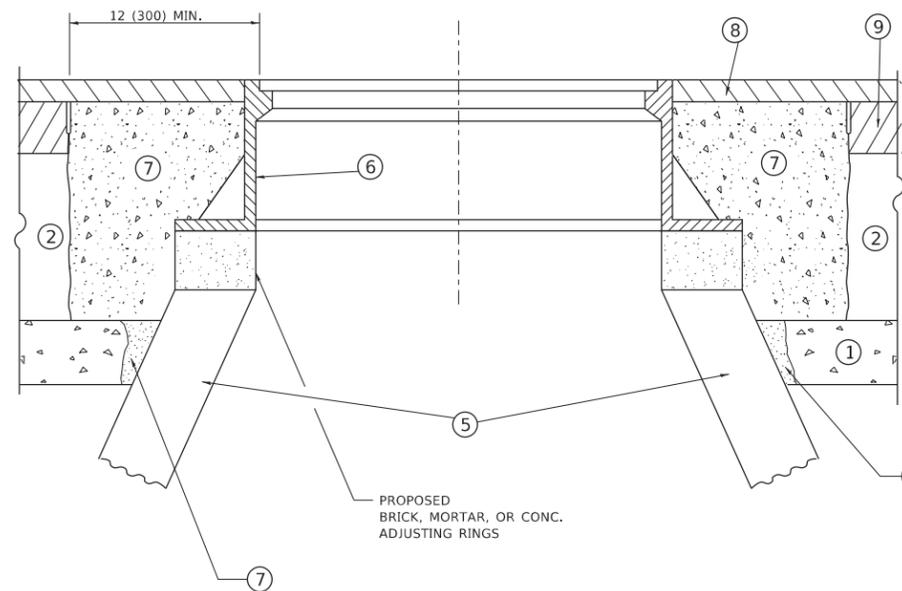
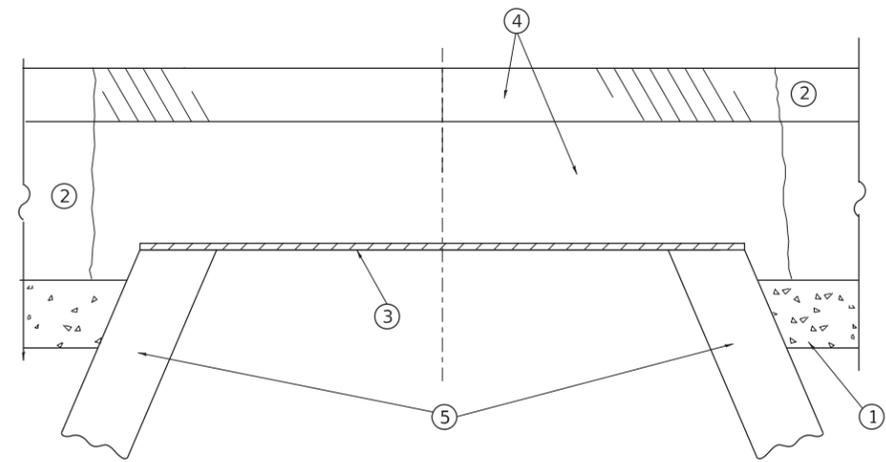
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.  
 AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	14
BD400-01 (BD-01)		CONTRACT NO.	61L65	
ILLINOIS		FED. AID PROJECT		





**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

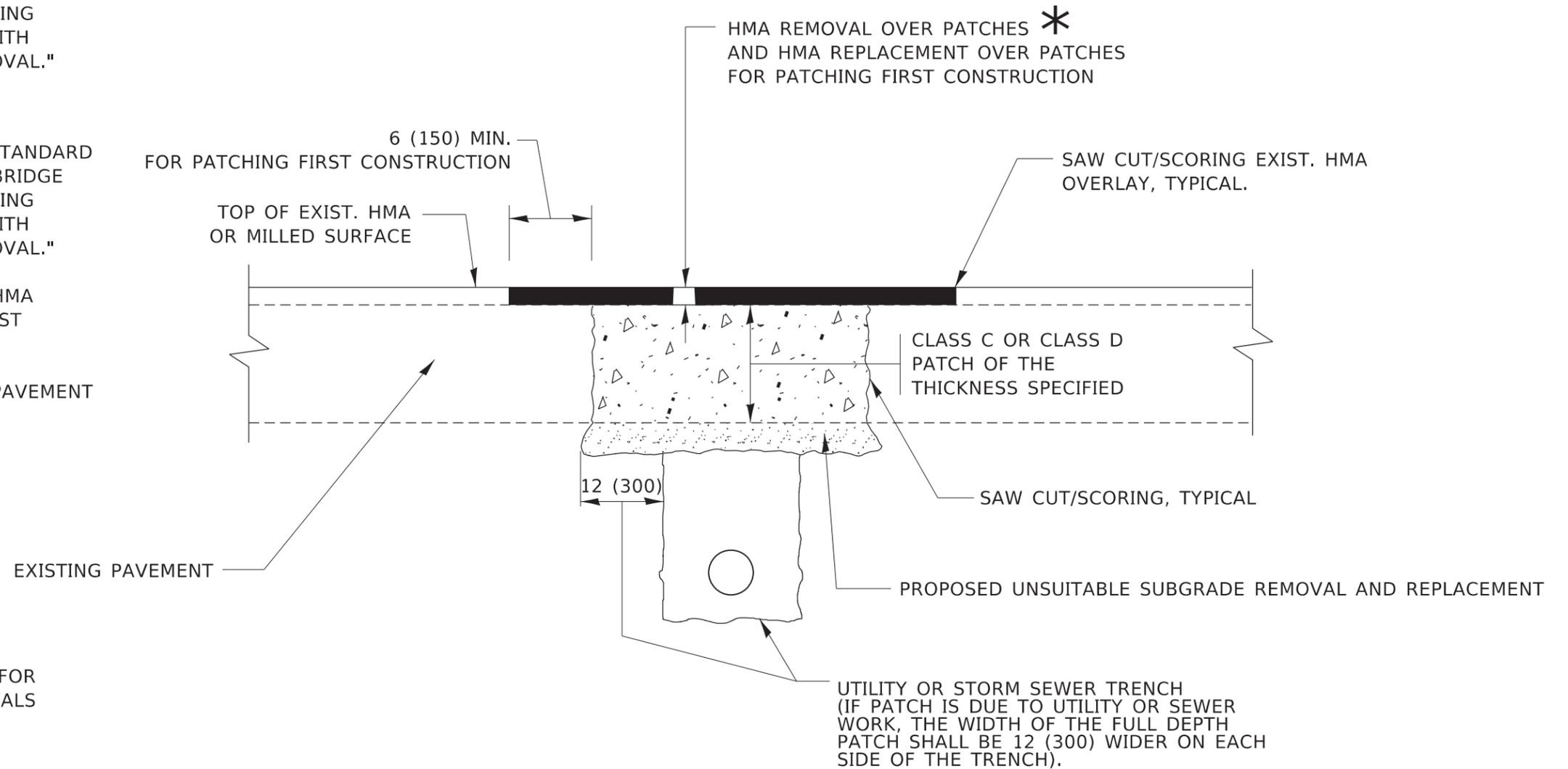
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2770	24-00114-00-RS	COOK	24	16
BD600-03 (BD-08)		CONTRACT NO. 61L65		
ILLINOIS		FED. AID PROJECT		

**METHOD OF MEASUREMENT**

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

**BASIS OF PAYMENT**

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN -	REVISED - R. BORO 09-04-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 11/18/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

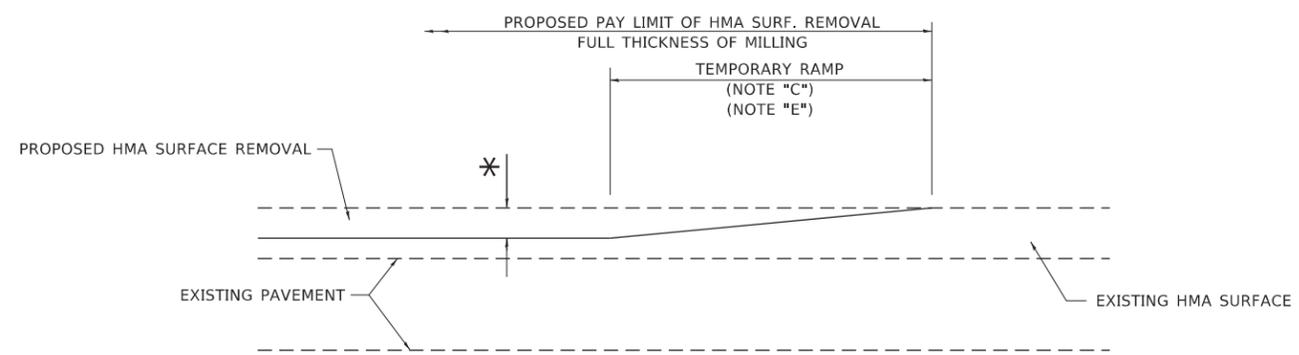
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

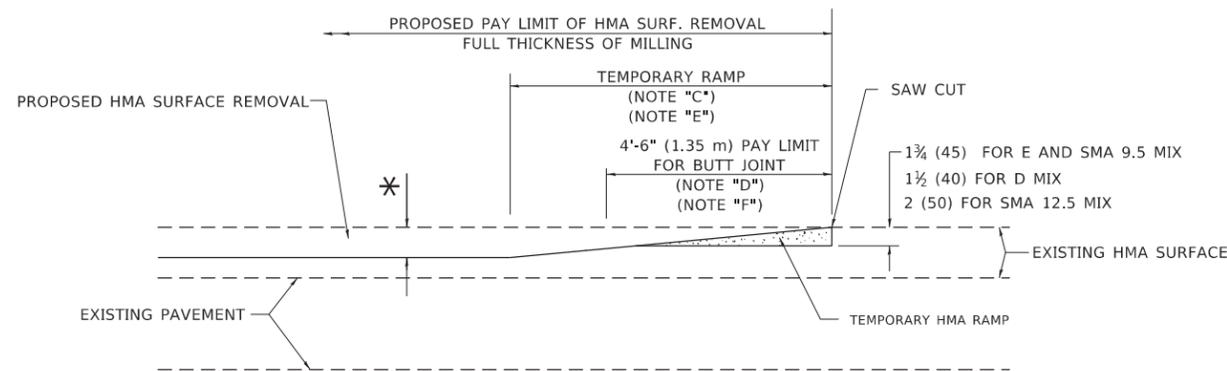
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	17
BD400-04 (BD-22)			CONTRACT NO. 61L65	
ILLINOIS FED. AID PROJECT				





**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

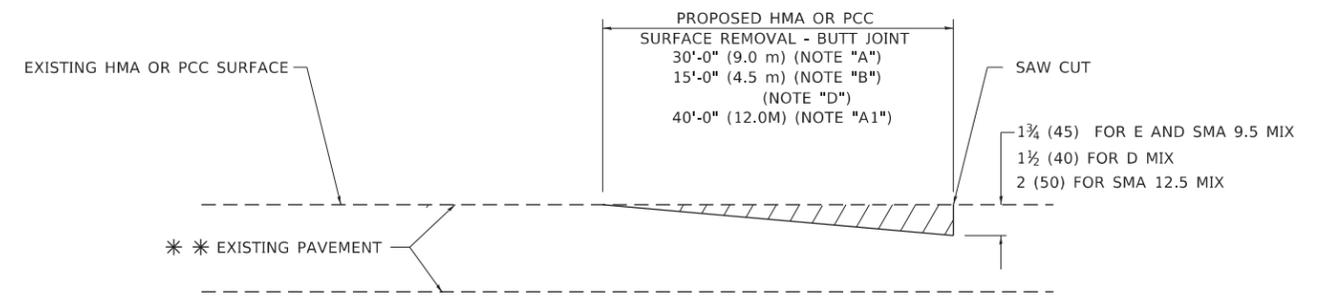
**OPTION 1**



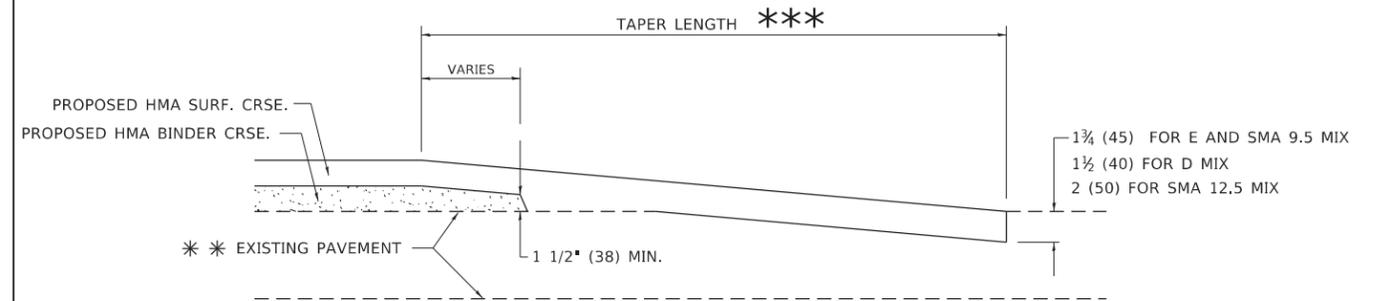
**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

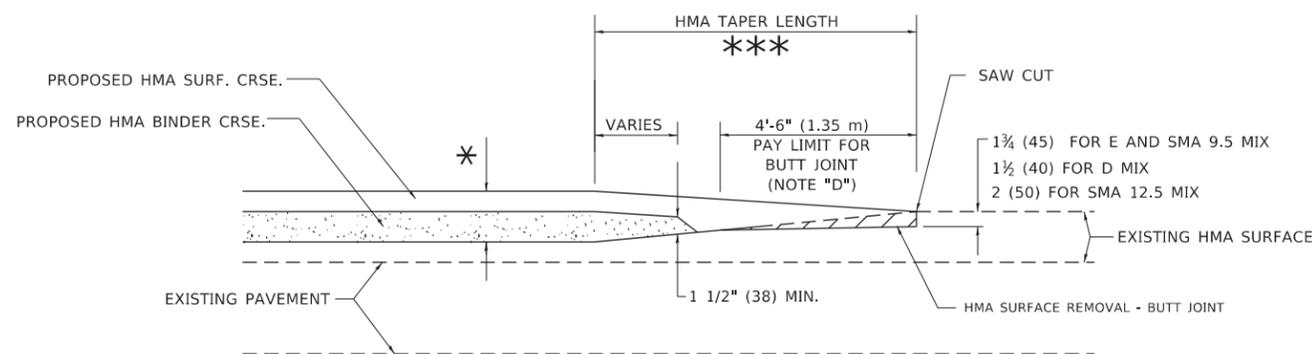
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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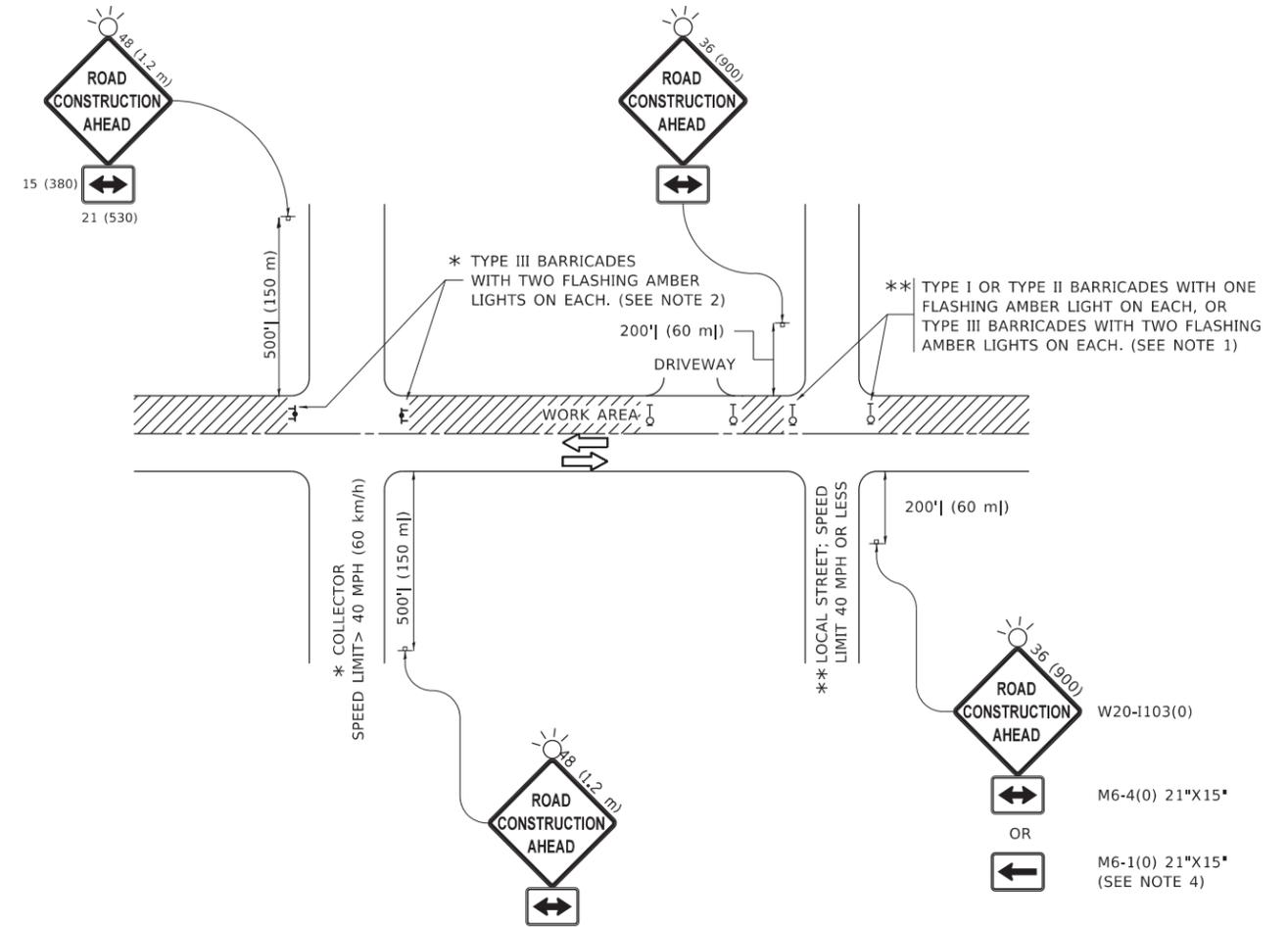
USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	19
BD400-05 BD-32		CONTRACT NO. 61L65		
ILLINOIS FED. AID PROJECT				



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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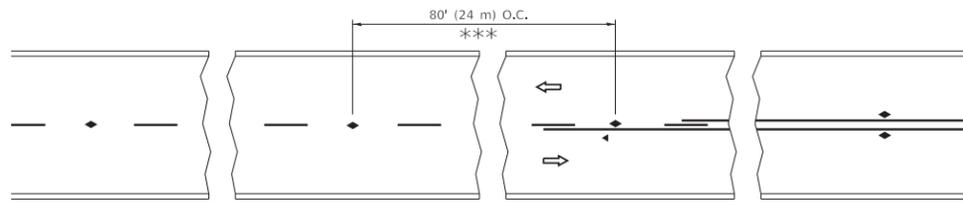
USER NAME = Lawrence,DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00
	DRAWN -	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 5/3/2024	DATE - 06-89	REVISED - D. SENDRAK 05-03-24

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

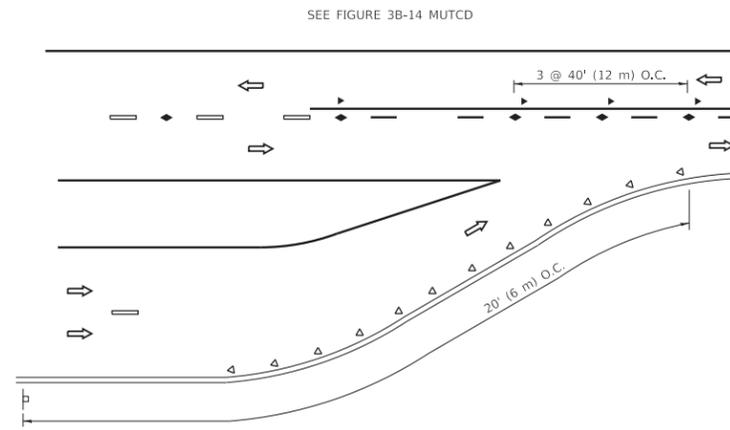
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ILLINOIS FED. AID PROJECT				

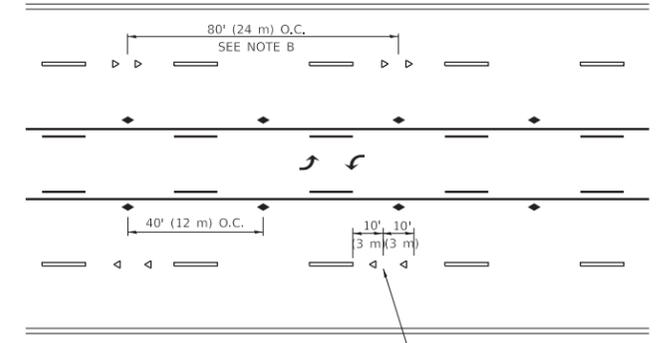


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

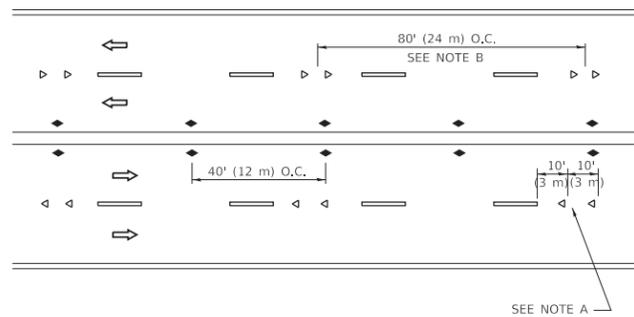
**TWO-LANE/TWO-WAY**



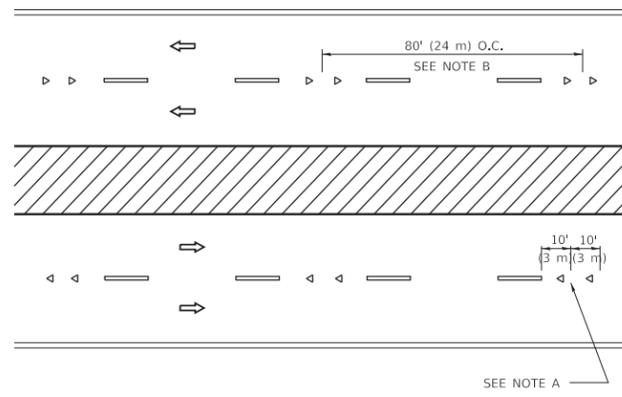
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

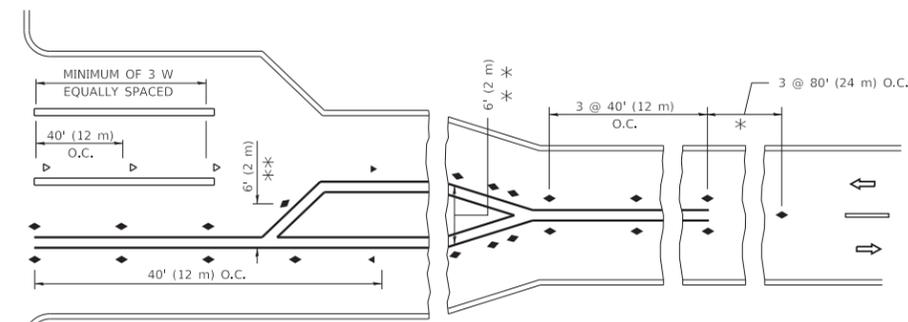
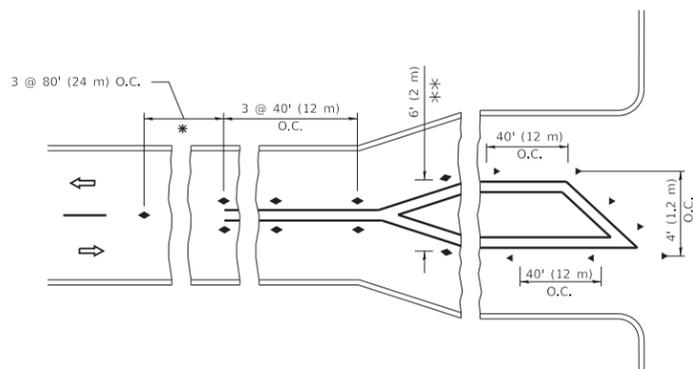
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



**TURN LANES**

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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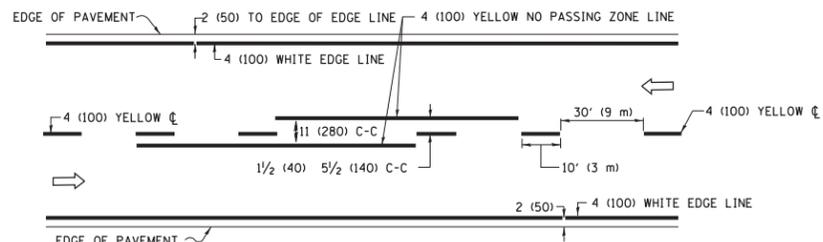
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

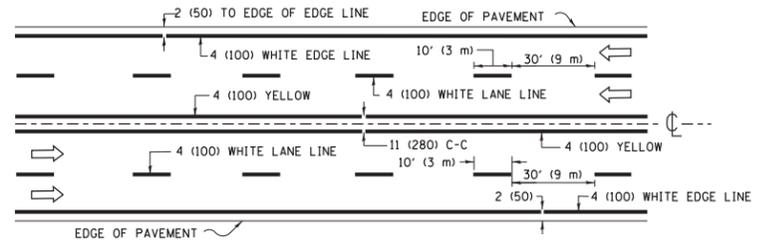
**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

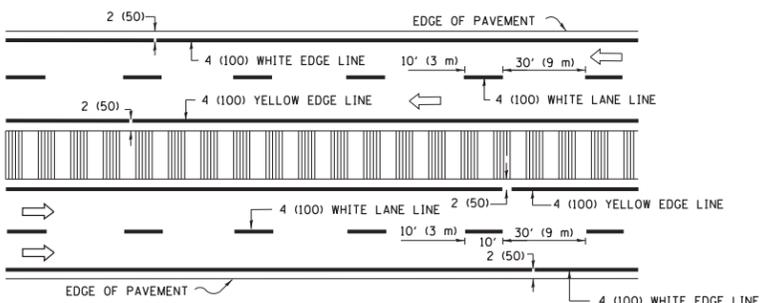
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	21
TC-11			CONTRACT NO. 61L65	
ILLINOIS		FED. AID PROJECT		



**2-LANE ROADWAY**

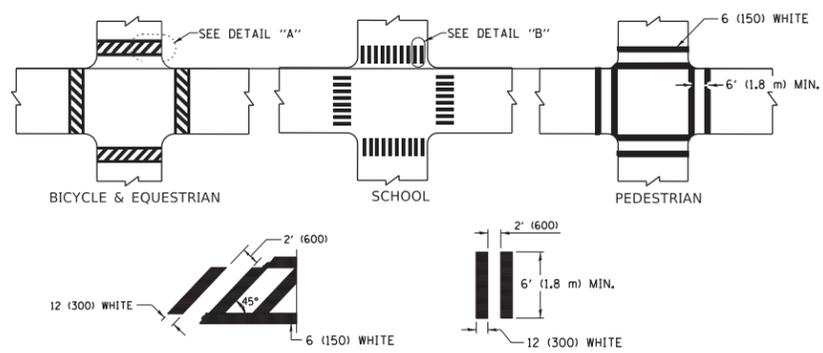


**MULTI-LANE UNDIVIDED**



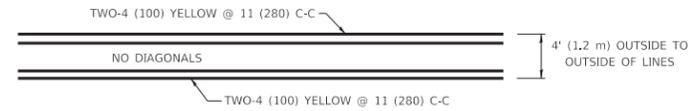
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

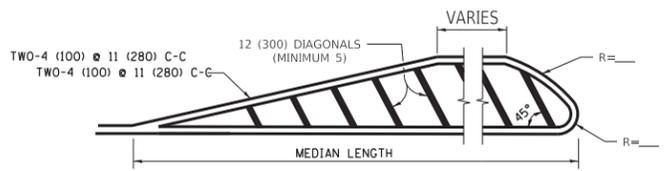


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

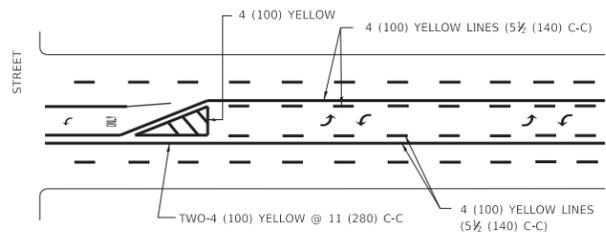


**4' (1.2 m) WIDE MEDIANS ONLY**



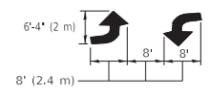
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



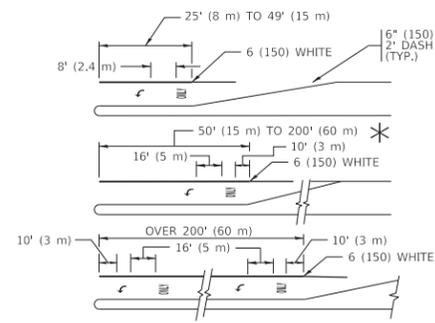
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

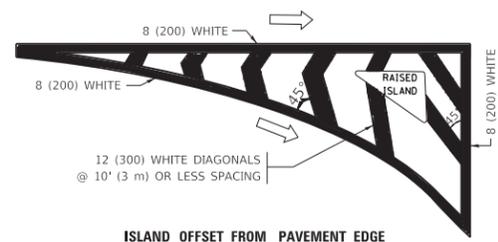
**TYPICAL PAINTED MEDIAN MARKING**



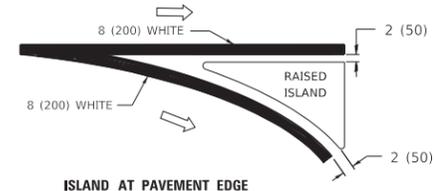
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

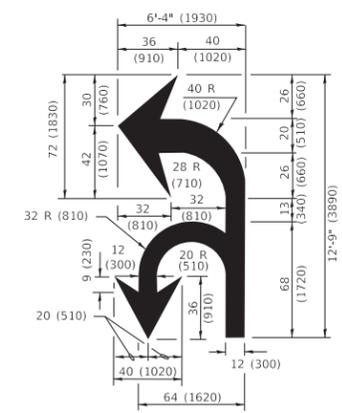


**ISLAND OFFSET FROM PAVEMENT EDGE**

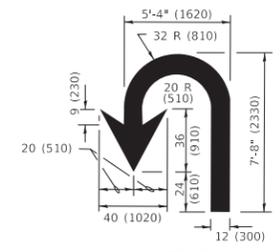


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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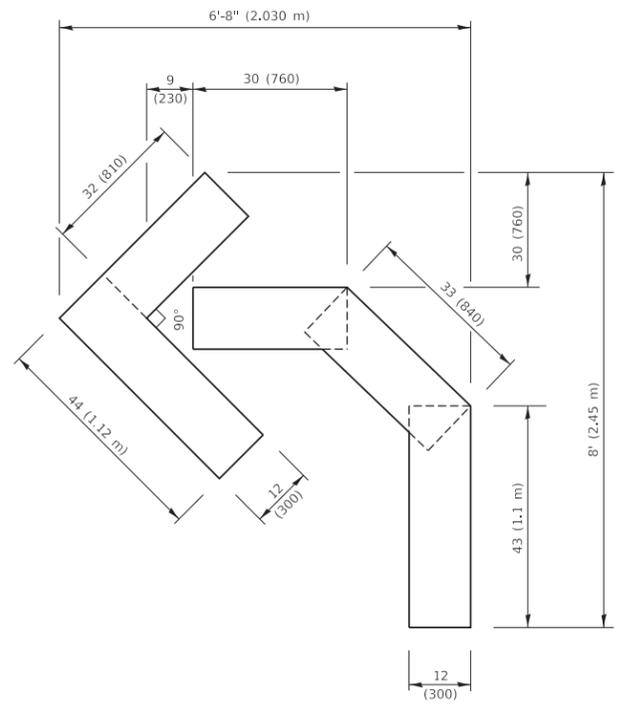
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**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

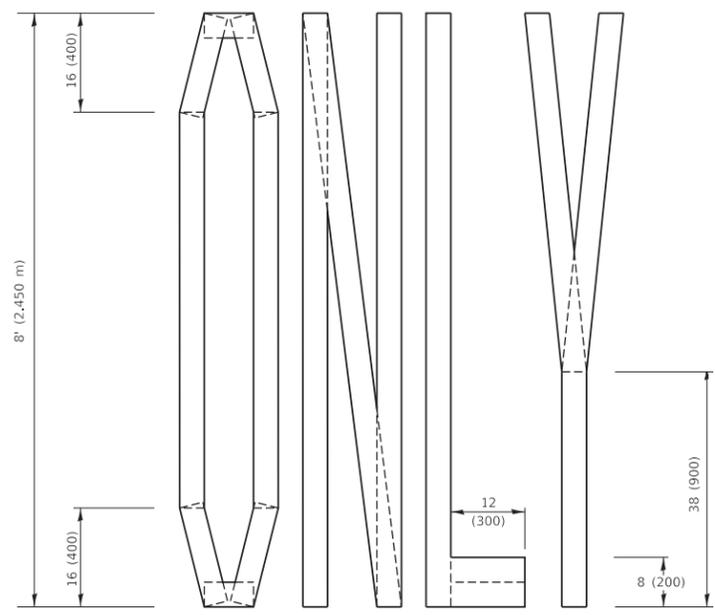
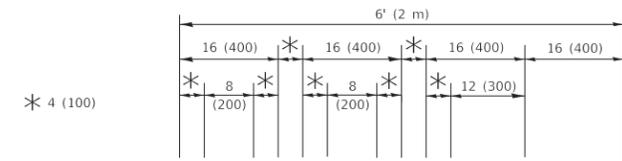
**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

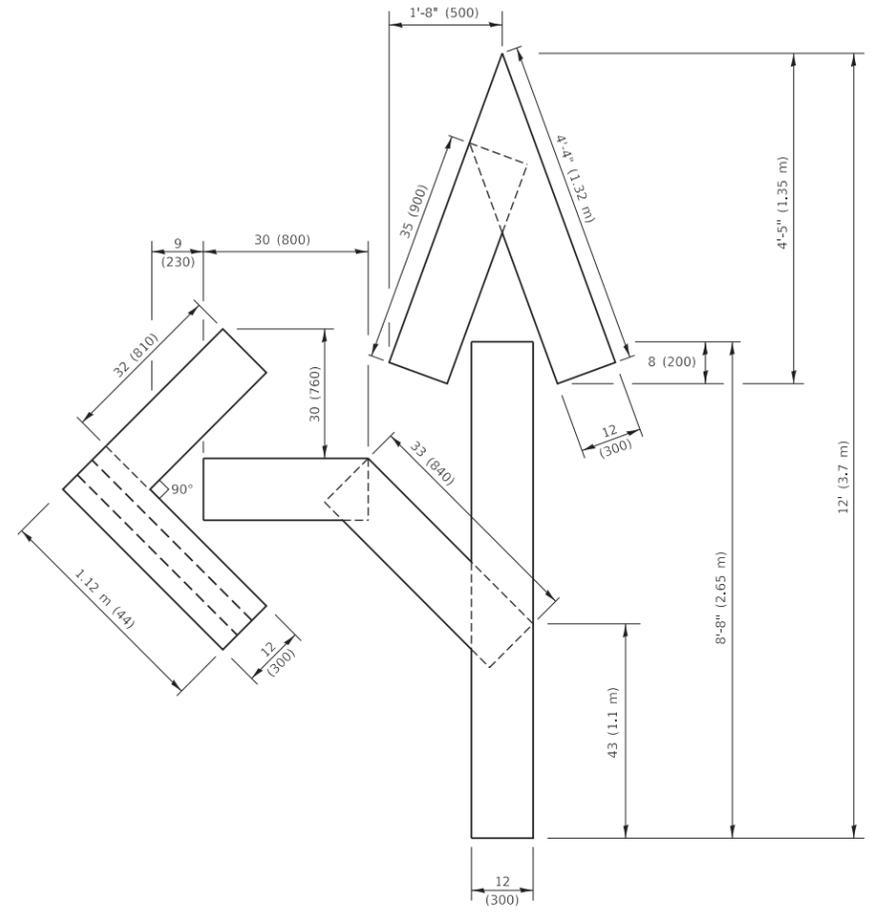
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TC-13		CONTRACT NO. 61L65		
ILLINOIS		FED. AID PROJECT		



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

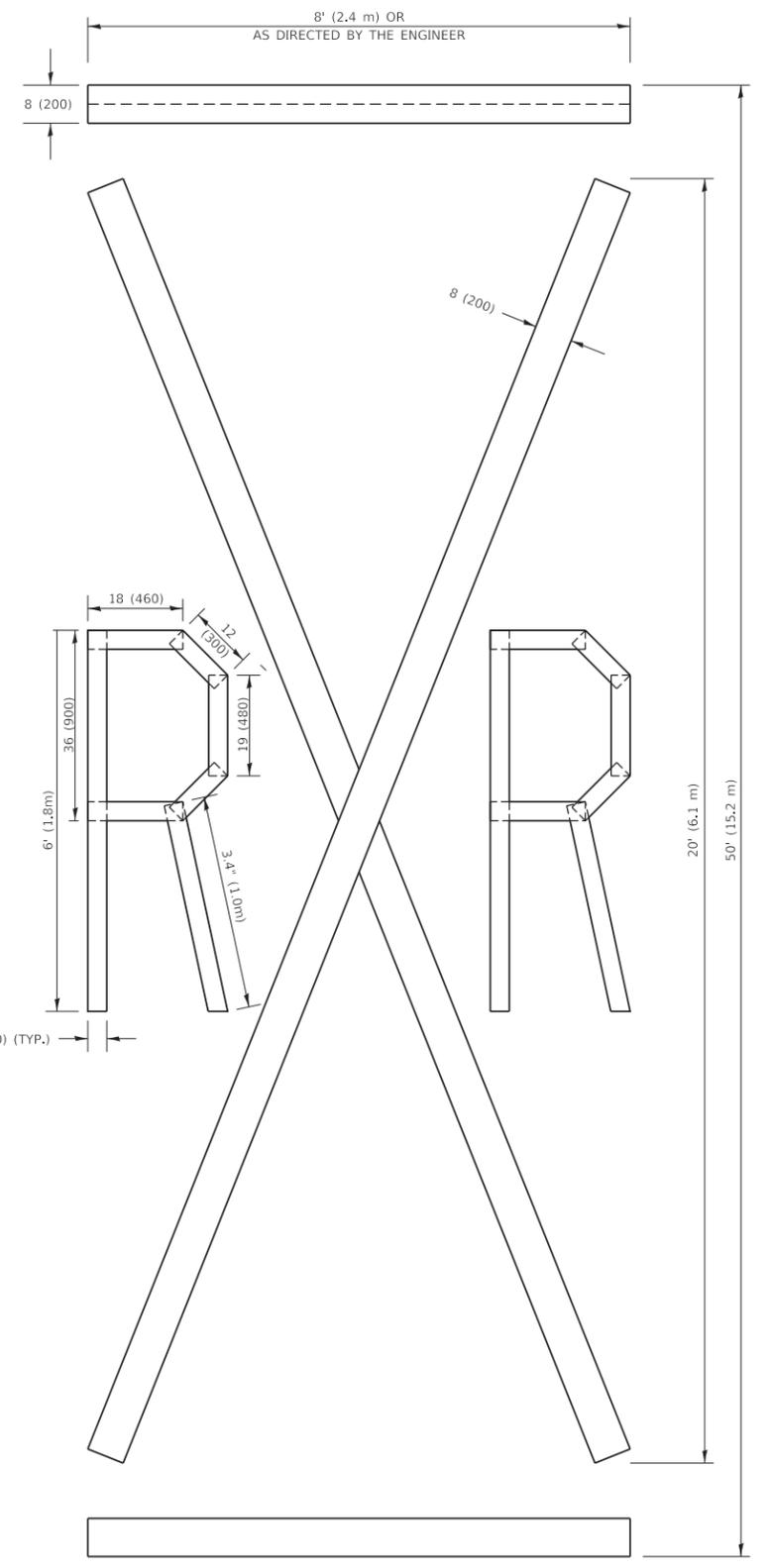


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
 FILE NAME: P:\PROJECTS\2019\10-37-35 AM User\606m\16.dgn  
 PROJECT: 24-00114-00-RS  
 SHEET: 24-00114-00-RS-23

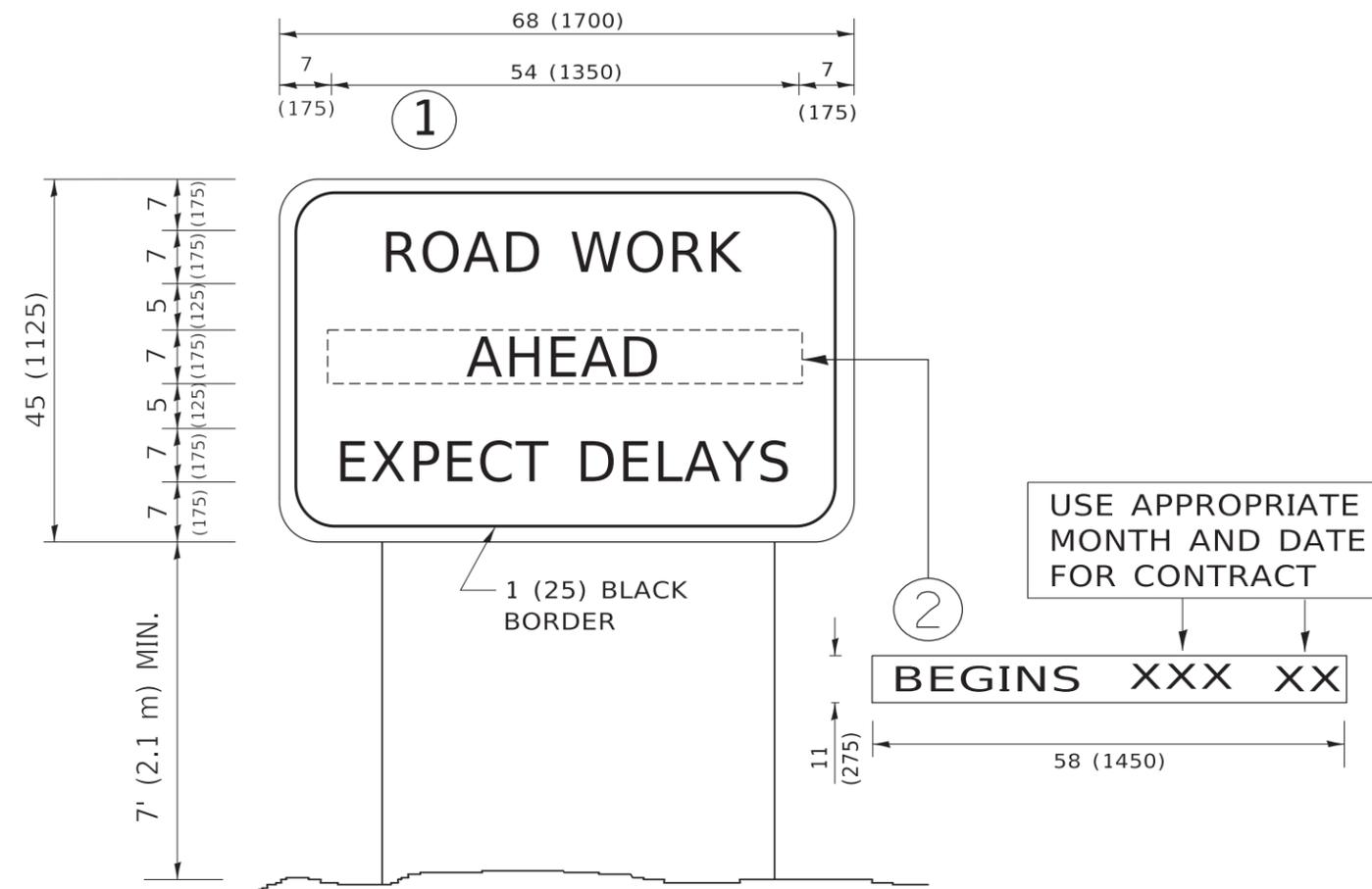
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = 50.0068 ' / in.	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	23
TC-16			CONTRACT NO. 61L65	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: D:\draft  
 FILE NAME: p:\u1108\BEBIDN\TEG\_ILlincois.gov\RWIDOT\Documents\DOT\_Offices\District\_1\Projects\DH5422\24\CADData\CAD\sheet\TC22.dgn

USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	24
TC-22			CONTRACT NO. 61L65	
ILLINOIS FED. AID PROJECT				