

CITY OF DEKALB DEKALB COUNTY, ILLINOIS

CONSTRUCTION PLANS FOR DEKALB TAYLOR MUNICIPAL AIRPORT

REMOVE AND REPLACE A PORTION OF THE AIRCRAFT PARKING APRON

ILLINOIS PROJECT: DKB-4924
 S.B.G. PROJECT: 3-17-SBGP-184

MARCH 3, 2023

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811 Know what's below.
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 J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

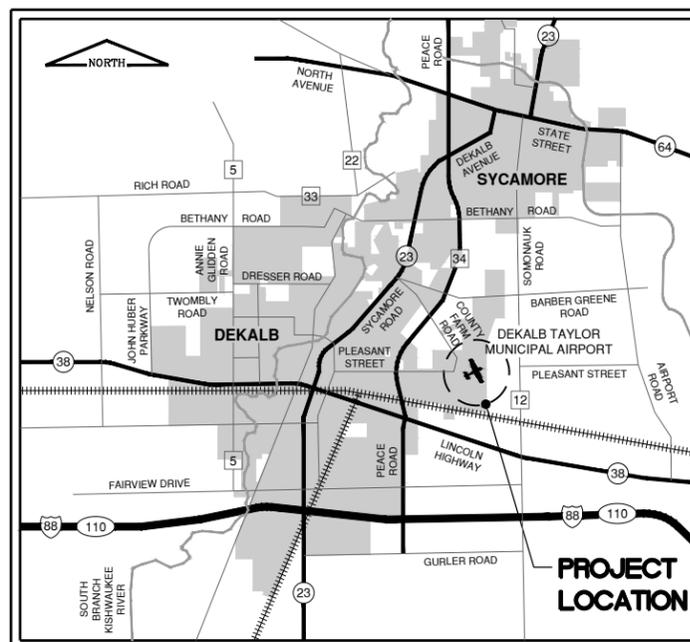
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

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 DEKALB TAYLOR MUNICIPAL AIRPORT

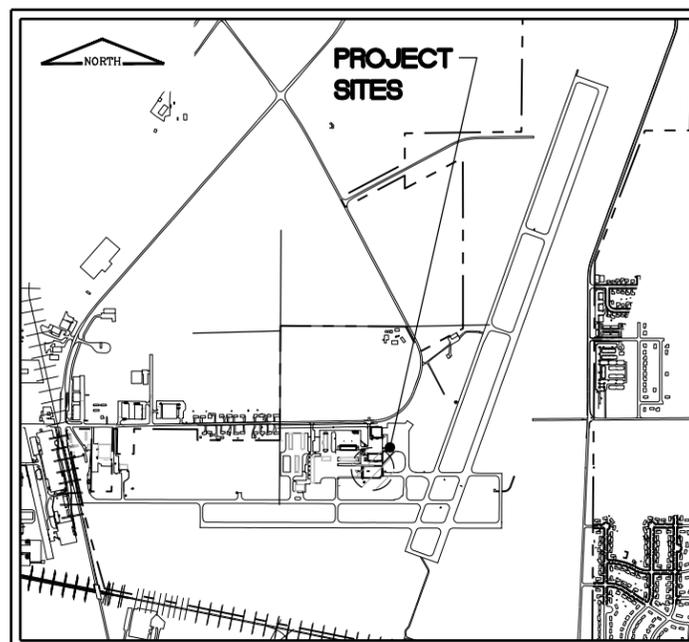
CITY OF DEKALB
 DEKALB TAYLOR MUNICIPAL AIRPORT
 3232 PLEASANT STREET
 DEKALB, ILLINOIS 60115
 Telephone: 815.748.8102

APPROVED BY *Renee Riani*
 RENEE RIANI/AIRPORT MANAGER

DATE MARCH 3, 2023



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E. BEFORE
 EXCAVATING AT 811
 DEKALB TAYLOR MUNICIPAL AIRPORT

TOWNSHIP: 40 NORTH
 RANGE: 5 EAST
 SECTION: 17, 18 AND 19
 COUNTY: DEKALB
 TOWNSHIP: CORTLAND

DESIGN INFORMATION
 DESIGN AIRCRAFT APPROACH CATEGORY D
 DESIGN AIRCRAFT GROUP III (GULFSTREAM V)

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210248-02

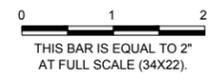


SUBMITTED BY *[Signature]*
 DATE MARCH 3, 2023

IL CONTRACT: **DK063**
IL LETTING ITEM: **01A**
IL PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



SUMMARY OF QUANTITIES				
ITEM	DESCRIPTION	UNIT	QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1.00	
AR150520	MOBILIZATION	LS	1.00	
AR152410	UNCLASSIFIED EXCAVATION	CY	850.00	
AR152480	SHOULDER ADJUSTMENT	SY	600.00	
AR208515	POROUS GRANULAR EMBANKMENT	CY	225.00	
AR209606	CRUSHED AGGREGATE BASE COURSE - 6"	SY	5,350.00	
AR401610	BITUMINOUS SURFACE COURSE	TON	50.00	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	5,300.00	
AR403610	BITUMINOUS BASE COURSE	TON	250.00	
AR501510	10" PCC PAVEMENT	SY	4,950.00	
AR501530	PCC TEST BATCH	EACH	1.00	
AR501900	REMOVE PCC PAVEMENT	SY	35.00	
AR510900	REMOVE TIE DOWN	EACH	7.00	
AR510901	REMOVE TIE DOWN - TYPE A	EACH	5.00	
AR602510	BITUMINOUS PRIME COAT	GAL	110.00	
AR603510	BITUMINOUS TACK COAT	GAL	80.00	
AR620900	PAVEMENT MARKING REMOVAL	SF	1,450.00	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	LF	1,250.00	
AR705610	CONCRETE HEADWALL FOR UNDERDRAIN	EACH	1.00	
AR705640	UNDERDRAIN CLEANOUT	EACH	8.00	
AR705900	REMOVE UNDERDRAIN	LF	100.00	
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	2.00	
AR901510	SEEDING	ACRE	0.15	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	0.15	

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DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON**

QUANTITIES

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DTMA
DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY: CMT-RFD
DRAWN BY: CMT-RFD
CHECKED BY: CMT-RFD
APPROVED BY: DJK
DATE: 3/3/2023
JOB No: 210248-02

FINAL

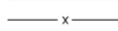
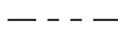
XREF DWG: Detail 2022 Title Block.dwg
21024802 - Detail Block.dwg
21024802 - Detail Airport.dwg
21024802 - Detail utility base.dwg

IMAGE FILES: DTMA Logo-Black.jpg

UPDATE BY: Robert Heinz
LAYOUT: Layout 1

DATE: Friday, March 3, 2023 1:34:54 PM
FILE: K:\Detail\21024802-00\RehabTerminal\Draw\Sheets\21024802 - Site Plan.dwg

LEGEND

-  PROPOSED LIMITS OF PAVEMENT IMPROVEMENTS
-  CONTRACTOR'S STAGING AREA
-  EXISTING BUILDING
-  EXISTING FENCE
-  AIRPORT PROPERTY LINE
-  HORIZONTAL/VERTICAL CONTROL POINT
-  PROJECT REFERENCE POINT
-  LOW PROFILE BARRICADES
-  CONTRACTOR'S HAUL ROUTE
-  FLAGGER/GATE GUARD LOCATION
-  TOFA - TAXIWAY OBJECT FREE AREA
-  CONSTRUCTION SETBACK LINE

HORIZONTAL / VERTICAL CONTROL POINTS (DATUM: NAD83)				
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
CP1	1917359.9910	878328.3570	903.47	NGS "KALBPORT AZ MK" - STEEL ROD IN SLEEVE
CP2	1917400.0500	879876.5500	907.81	NGS "KALBPORT" - STEEL ROD IN SLEEVE
CP3	1917929.0150	881645.7570	908.60	BEACON BASE - SOUTHWEST NUT
CP4	1917750.8370	881610.4240	909.26	CUT "X" SOUTH SIDE WATER VAULT STRUCTURE
CP5	1917548.0990	881631.3340	905.51	CUT "X" NORTH SIDE OF MANHOLE

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	Alignment - taxiway a	STA. 408+00.00	N:1917658.9966 E:880465.0529	STA. 438+84.65	N: 1917652.3771 E: 883549.6974

PROJECT LAYOUT POINTS			
NO.	NORTHING	EASTING	STATION / LOCATION
1	1917658.9966	880465.0529	STA. 408+00.00 CL TAXIWAY A (EXTENDED)
2	1917657.2754	881267.1508	STA. 416+02.10 CL TAXIWAY A STA. 7+09.50 CL TAXIWAY B2
3	1917655.2332	882218.7769	STA. 425+53.73 CL TAXIWAY A STA. 7+09.50 CL TAXIWAY B3
4	1917654.3520	882629.4171	STA. 429+64.37 CL TAXIWAY A STA. 211+43.48 CL TAXIWAY C
5	1917272.7766	881266.1510	STA. 316+01.11 CL TAXIWAY B STA. 3+25.00 CL TAXIWAY B2
6	1917270.7319	882218.9467	STA. 325+53.90 CL TAXIWAY B STA. 3+25.00 CL TAXIWAY B3

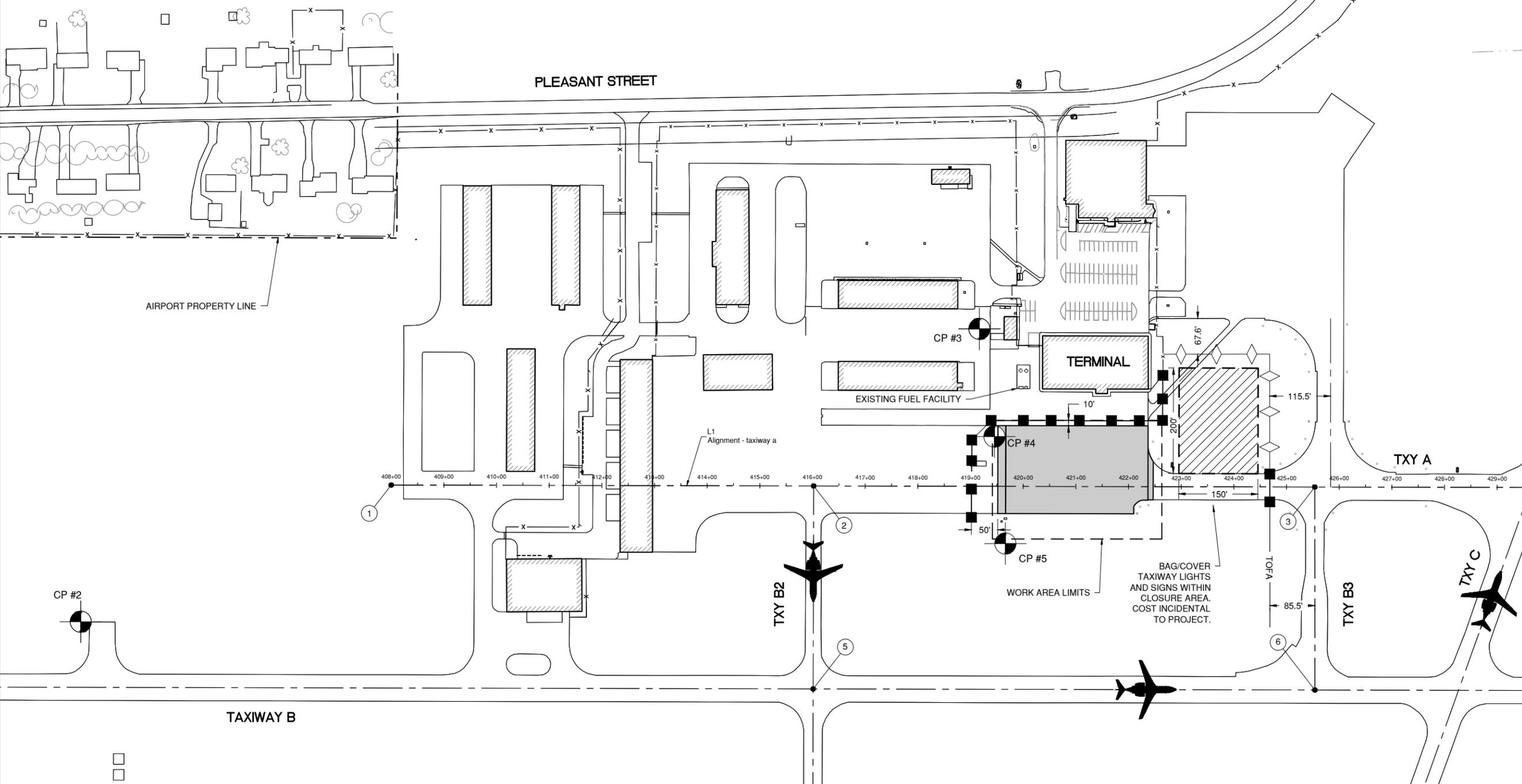


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SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



**DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
 AIRCRAFT PARKING APRON
 SITE PLAN AND CONTROL**

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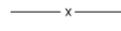
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 DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY:	CMT-RFD
DRAWN BY:	CMT-RFD
CHECKED BY:	CMT-RFD
APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02
FINAL	
SHEET 3 OF 18 SHEETS	

DATE: Friday, March 3, 2023 1:35:13 PM
 FILE: K:\DeKalb\21024802-00 - DeKalb Terminal\Draw\Sheets\21024802 - Sequence Of Construction.dwg
 UPDATE BY: Robert Heinz
 LAYOUT: Layout 1
 IMAGE FILES: DTMA Logo-back.jpg
 XREF DWG: DeKalb 2022 Title Block.dwg
 21024802 - Detail Aircraft Base.dwg
 21024802 - Detail Utility Base.dwg

LEGEND

-  PROPOSED LIMITS OF PAVEMENT IMPROVEMENTS
-  CONTRACTOR'S STAGING AREA
-  EXISTING BUILDING
-  EXISTING FENCE
-  AIRPORT PROPERTY LINE
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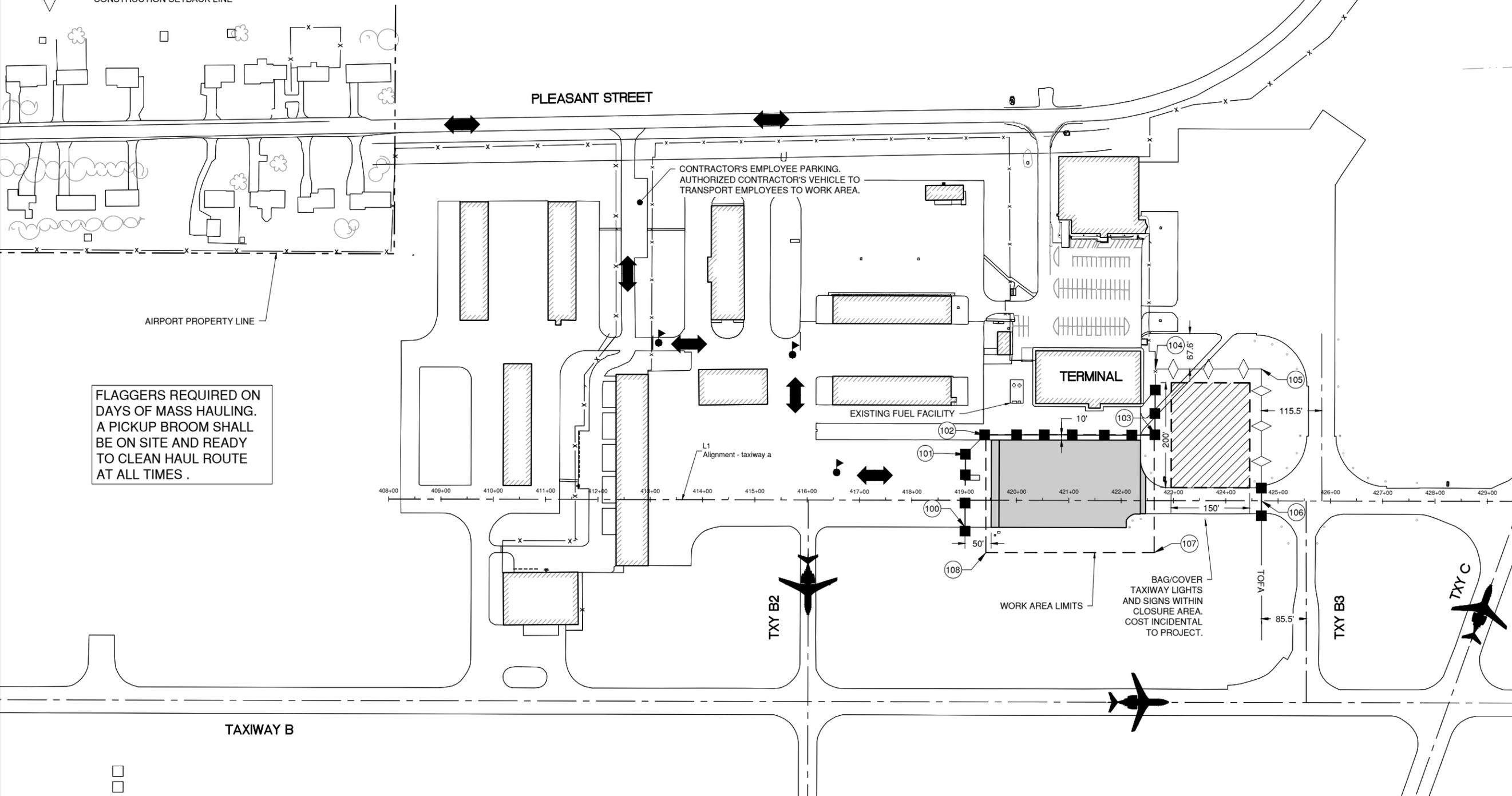
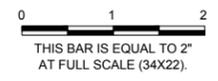
FAA CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	ELEVATION	OBSTRUCTION HEIGHT	ABOVE GROUND ELEVATION
100	N41° 55' 50.94"	W88° 42' 38.51"	908	25	933
101	N41° 55' 52.31"	W88° 42' 38.51"	909	25	934
102	N41° 55' 52.68"	W88° 42' 38.02"	909	25	934
103	N41° 55' 52.71"	W88° 42' 33.72"	908	25	933
104	N41° 55' 53.93"	W88° 42' 33.71"	907	25	932
105	N41° 55' 53.94"	W88° 42' 31.03"	904	25	929
106	N41° 55' 51.46"	W88° 42' 31.02"	905	25	930
107	N41° 55' 50.47"	W88° 42' 33.72"	902	25	927
108	N41° 55' 50.46"	W88° 42' 37.98"	906	25	931



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DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON
CONSTRUCTION SAFETY PHASING PLAN (CSPP)

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 DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY: CMT-RFD
 DRAWN BY: CMT-RFD
 CHECKED BY: CMT-RFD
 APPROVED BY: DJK
 DATE: 3/3/2023
 JOB No: 210248-02

FINAL

SHEET 4 OF 18 SHEETS

DATE: Friday, March 3, 2023 1:35:18 PM
FILE: K:\Detailing\210248-00 - Rehabilitation\Draw\Sheets\210248-02 - Sequence Notes - 1.dwg
UPDATE BY: Robert Heinz
LAYOUT: IN-NEW SHEET TEMPLATE
MANAGE FILES: D:\MVA\cbr-backlog
XREF DWG: Detailing 2022 Title Block.dwg

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 47 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN SHEETS.

WORK AREAS AND DESCRIPTIONS

MOBILIZATION

COMPLETE MOBILIZATION DURING THE MOBILIZATION TIME PERIOD.

WORK AREAS

WORK AREA GENERALLY INCLUDES PAVEMENT REMOVALS, UNDERGROUND DRAIN REMOVALS, UNCLASSIFIED EXCAVATION, UNDERDRAIN IMPROVEMENTS, CONCRETE PAVING, ASPHALT PAVING, AND LANDSCAPING.

RESTRICTIONS

GENERAL (ALL WORK AREAS)

ALL WORK AREAS ARE LOCATED INSIDE THE AOA SECURITY FENCE, AND ARE SUBJECT TO THE LIMITATIONS DESCRIBED IN SPECIFICATIONS, PART 1 - GENERAL PROVISIONS.

WORK CONDUCTED UTILIZING EXTENDED CLOSURES OF AIRFIELD PAVEMENTS AS INDICATED IN THE PLANS WILL ALLOW THE CONTRACTOR TO CONDUCT WORK 24 HOURS A DAY, 7 DAYS A WEEK OR UNLESS SPECIFICALLY NOTED FOR THE RESTRICTIONS IN EACH WORK AREA. THE CLOSURES WILL ALLOW THE CONTRACTOR TO COMPLETE ALL WORK INSIDE THE RESPECTIVE TOFA, OFZ OR RSA, AND TO A DISTANCE OUTSIDE THESE AREAS AS NOT TO RE-ENCROACH THE TOFA, OFZ OR RSA WITH MANPOWER OR EQUIPMENT ONCE THE CLOSURE PERIOD IS CONCLUDED.

SIMULTANEOUS CLOSURES OF AIRFIELD PAVEMENTS TO COMPLETE WORK IN MORE THAN ONE WORK AREA AT A TIME WILL NOT BE ALLOWED EXCEPT AS SPECIFICALLY NOTED.

THE APRON MUST BE OPEN FOR THE AIRVENTURE OSHKOSH FLY-IN SCHEDULED IN JULY. THE TIME PERIOD OF **JULY 4 - JULY 31, 2024** SHALL HAVE ALL PAVEMENT AREAS AVAILABLE FOR USE FOR AIR TRAFFIC. THIS EVENT MUST BE ACCOUNTED FOR IN THE CONSTRUCTION SCHEDULE.

WORK AREA 1A

AN EXTENDED CLOSURE OF A PORTION OF THE APRON AND TAXIWAY A IS REQUIRED TO COMPLETE WORK. TAXIWAY A WILL BE CLOSED WEST OF TAXIWAY B3. ALL LOCATIONS WITHIN THE TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE TAXIWAYS AT END OF THE CLOSURE PERIOD.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
- EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
- EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
- PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
- IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIRPORT UNICOM (122.70 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
- THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.

- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. **EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.**
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

IL CONTRACT: **DK063**
IL LETTING ITEM: **01A**
IL PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON**

CONSTRUCTION SAFETY PHASING NOTES - 1

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DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY:	CMT-RFD
DRAWN BY:	CMT-RFD
CHECKED BY:	CMT-RFD
APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02

FINAL

SHEET 5 OF 18 SHEETS

DATE: Friday, March 3, 2023 1:35:24 PM
 FILE: K:\Detail\210248-00_RehabTempApron\Draw\Sheet210248-02_Sequence Notes - 2.dwg
 UPDATE BY: Robert Heinz
 LAYOUT: INEW SHEET TEMPLATE1
 IMAGE FILES: DTMA Logo Block.dwg
 XREF: DWG: Detail 2022 Title Block.dwg

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

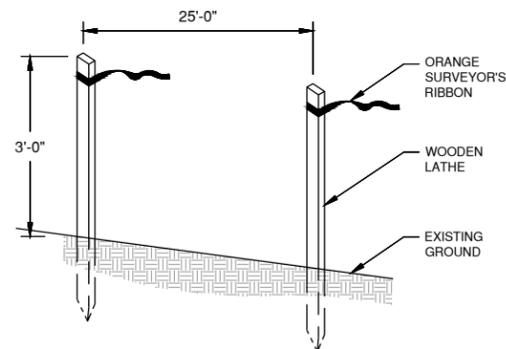
1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
2. LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

1. ALL WORK REQUIRED INSIDE OF THE RUNWAY SAFETY AREA WILL REQUIRE THE RUNWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

19. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.



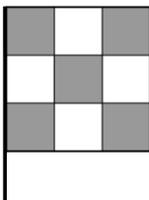
CONSTRUCTION SETBACK LINE DETAIL
NOT TO SCALE

CONSTRUCTION SETBACK NOTES

1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA AND TAXIWAY OBJECT FREE AREA AT THE LIMITS OF PROPOSED IMPROVEMENTS PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

PAYMENT FOR ALL CONSTRUCTION SAFETY MEASURES AND TEMPORARY CONSTRUCTION INCLUDING BUT NOT LIMITED TO TEMPORARY TAXIWAY EDGE LIGHTS AND CABLING, CABLE JUMPERS AND CONNECTIONS, TAXIWAY EDGE SAFETY TREATMENT, COMPACTED MILLINGS WEDGE AND ALL REMOVALS OF TEMPORARY MEASURES AND CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

(ORANGE / WHITE)



CONSTRUCTION EQUIPMENT AND VEHICLE SIGNAL FLAG

N.T.S.

SIGNAL FLAG NOTES

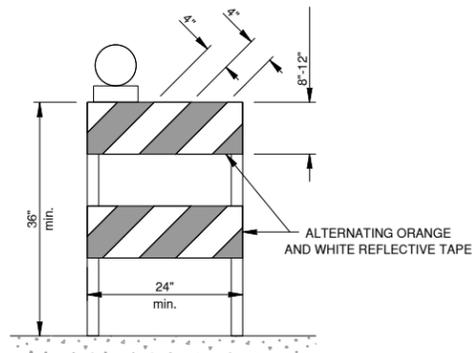
1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.

AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY BUT NOT WITHIN LIMITS OF THIS PROJECT:

- REHABILITATE RUNWAY 9/27

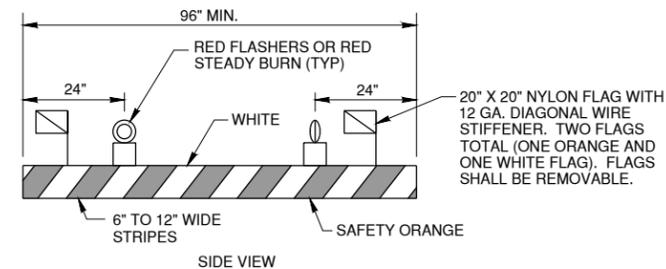


IDOT TYPE II BARRICADE

NOT TO SCALE



PLAN VIEW



SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL. CONTRACT: **DK063**
 IL. LETTING ITEM: **01A**
 IL. PROJECT: **DKB-4924**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS

REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON

CONSTRUCTION SAFETY PHASING NOTES - 2

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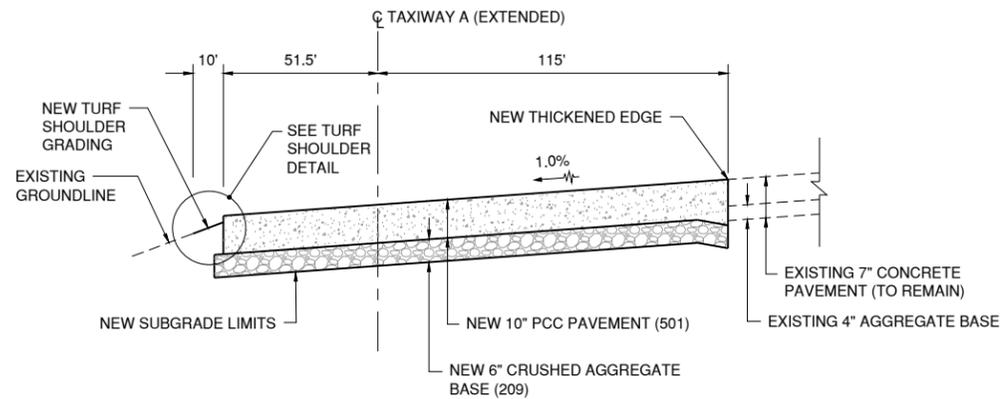
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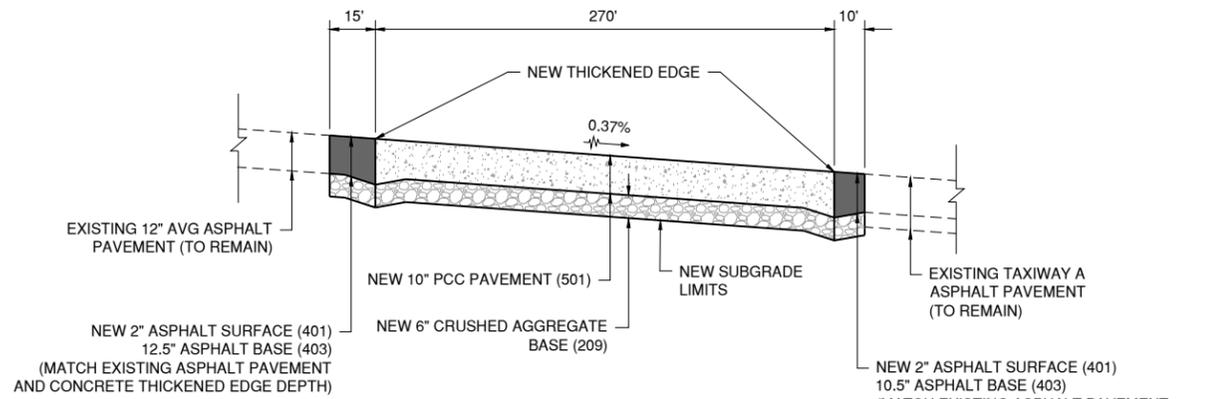
DESIGN BY: CMT-RFD
 DRAWN BY: CMT-RFD
 CHECKED BY: CMT-RFD
 APPROVED BY: DJK
 DATE: 3/3/2023
 JOB No: 210248-02

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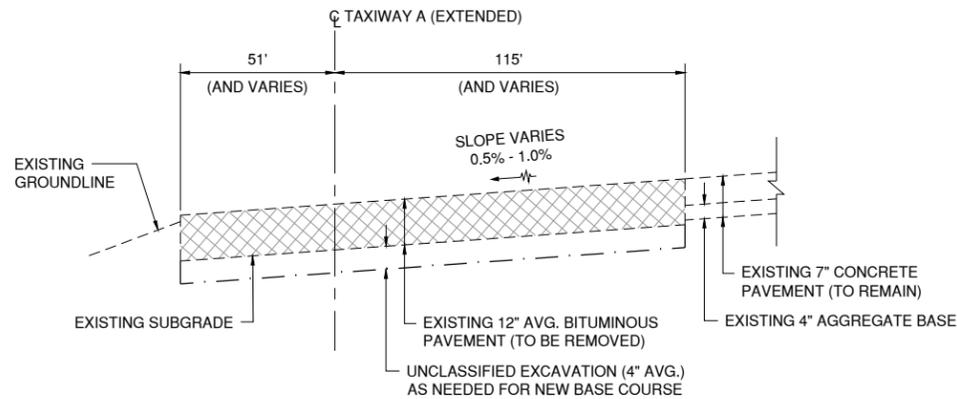
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 DATE: Friday, March 3, 2023 1:35:59 PM
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NEW TERMINAL APRON (SECTION A-A)
NOT TO SCALE



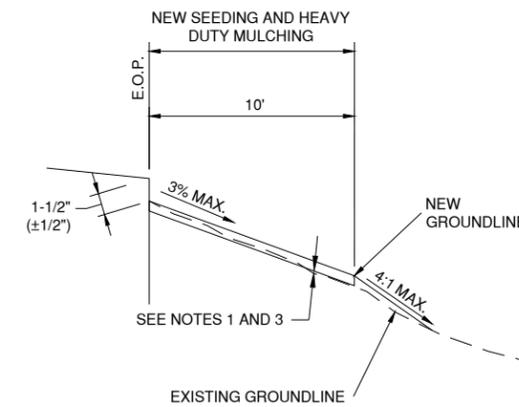
NEW TERMINAL APRON (SECTION B-B)
NOT TO SCALE



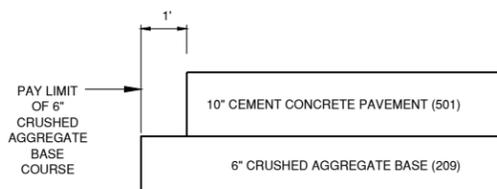
EXISTING TERMINAL APRON DEMOLITION (SECTION A-A)
NOT TO SCALE

TURF SHOULDER NOTES:

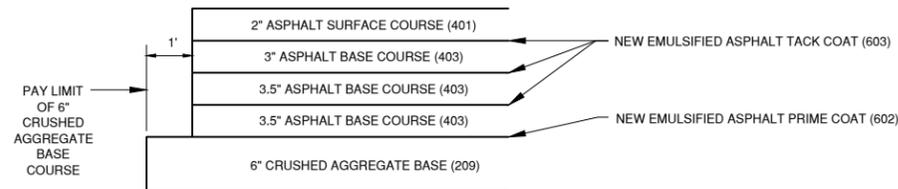
1. TILL AND PULVERIZE EXISTING TURF SHOULDER IN PLACE (COST INCIDENTAL TO SEEDING PAY ITEM).
2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD BY THE RESIDENT ENGINEER AS REQUIRED.
3. TURF SHALL BE SMOOTHLY GRADED TO THE SATISFACTION OF THE RESIDENT ENGINEER BEFORE SEEDING. ADDITIONAL TOPSOIL WILL BE REQUIRED FOR TURF SHOULDER RESTORATION AND SHALL BE INCIDENTAL TO SHOULDER ADJUSTMENT PAY ITEM.



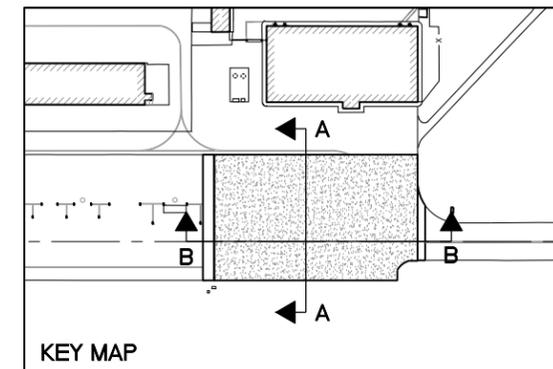
TURF SHOULDER DETAIL
NOT TO SCALE



APRON EDGE DETAIL (TYPICAL)
N.T.S.



ASPHALT PAVEMENT EDGE DETAIL (TYPICAL)
N.T.S.

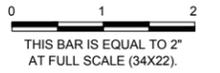


KEY MAP

IL CONTRACT: **DK063**
 IL LETTING ITEM: **01A**
 IL PROJECT: **DKB-4924**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

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NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
 AIRCRAFT PARKING APRON**

TYPICAL SECTIONS

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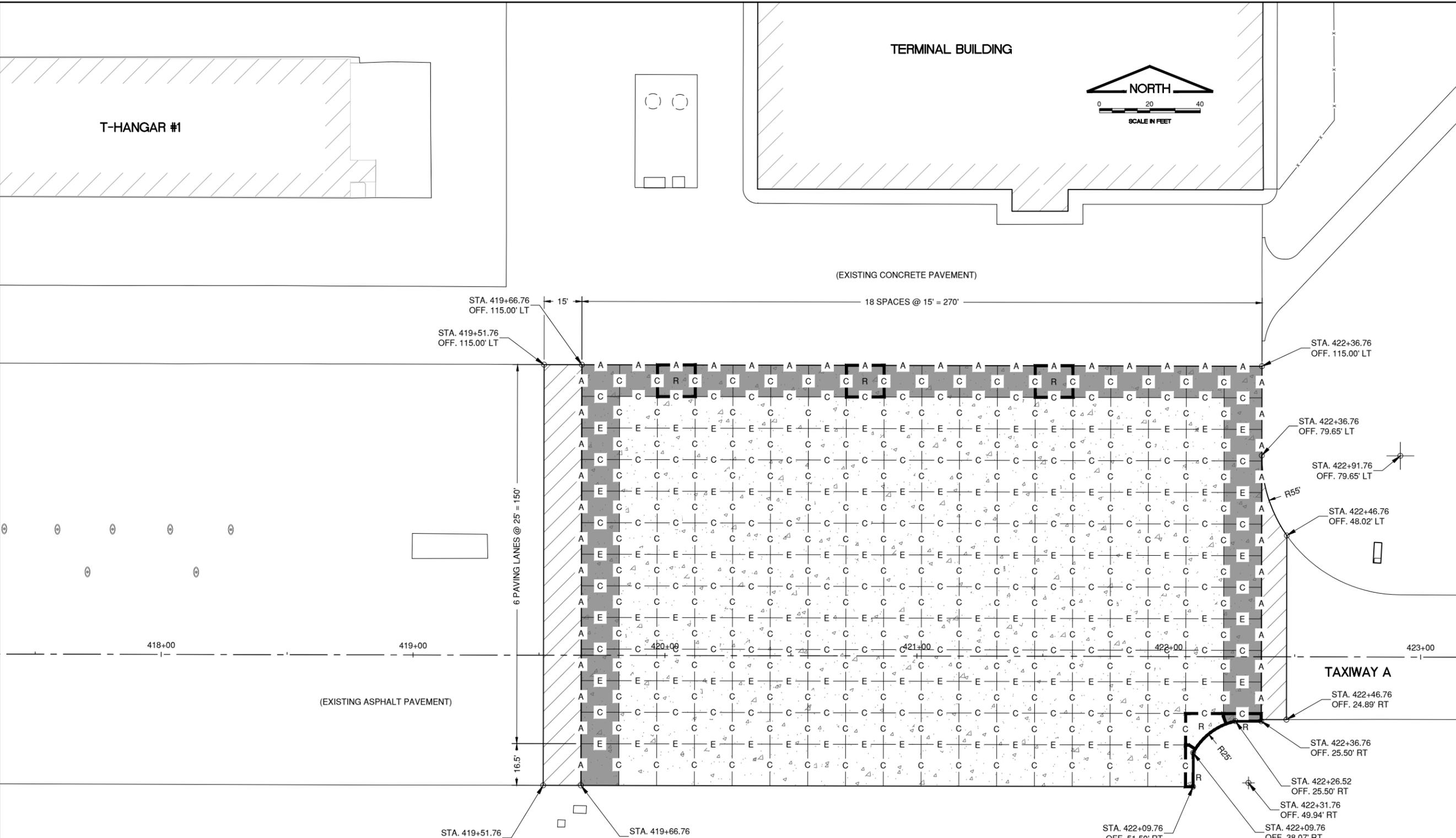
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21024802 - detail utility plan.dwg
21024802 - detail alignments.dwg

IMAGE FILES: DTMA Logo-Block.jpg

UPDATE BY: Robert Heinz
LAYOUT: Layout 1

DATE: Friday, March 3, 2023 1:36:14 PM
FILE: K:\Detail\21024802-00 - Rehabilitation\Draw\Sheets\21024802 - Jointing Plan.dwg



NOTES

- ALL PANELS WITH STRUCTURES SHALL BE REINFORCED. COST INCIDENTAL TO 501. SEE REINFORCED PANEL DETAIL ON PAVING DETAIL SHEET-1. CONTRACTOR SHALL CUT OUT A 2.5" DIAMETER OF WELDED WIRE FABRIC IN PANELS CENTERED ON A UNDERDRAIN CLEANOUT.
- ANY VARIATION TO THIS JOINTING PLAN MUST BE APPROVED BY THE RESIDENT ENGINEER.
- THE BITUMINOUS/CONCRETE PAVEMENT INTERFACE SHALL BE SEALED AS DETAILED ON THE JOINTING DETAIL SHEET. COST OF JOINTING AND SEALING SHALL BE INCIDENTAL TO THE CONTRACT.

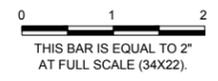
LEGEND

- A— TYPE A THICKENED EDGE JOINT
- C— TYPE C DOWELED CONTRACTION JOINT
- E— TYPE E DOWELED CONSTRUCTION JOINT
- R** NEW REINFORCED PANEL
- NEW THICKENED EDGE
- NEW 10' CONCRETE
- NEW ASPHALT PAVEMENT

IL. CONTRACT: **DK063**
IL. LETTING ITEM: **01A**
IL. PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

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**DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON
PROPOSED GEOMETRY AND JOINTING PLAN**

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APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02

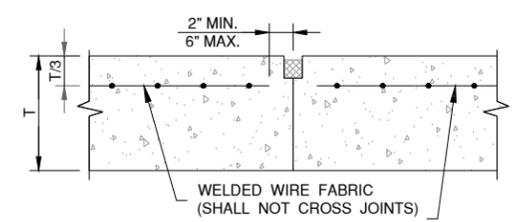
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IMAGE FILES: DTMA Logo-Block.jpg

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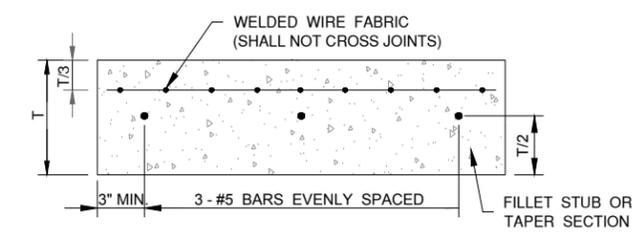
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REINFORCED PANEL DETAIL

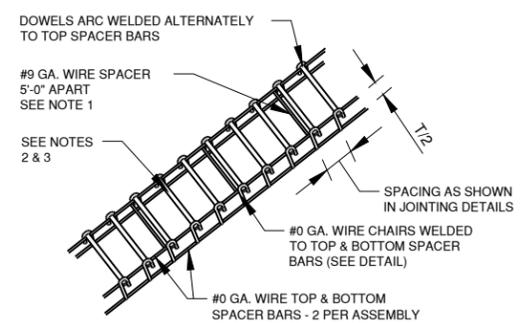
N.T.S.

NOTE: AMOUNT OF STEEL SHALL BE 0.050% OR GREATER



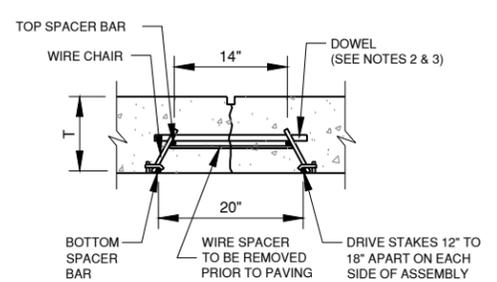
REINFORCED FILLET/TAPER SECTION STEEL DETAIL

N.T.S.



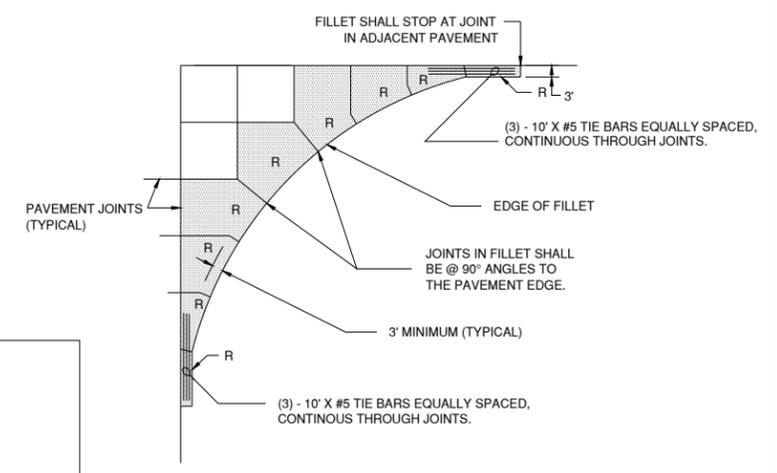
DOWEL BASKET ASSEMBLY DETAIL

N.T.S.



DOWEL BAR INSTALLATION DETAIL

N.T.S.



FILLET DETAIL AND FILLET REINFORCING LAYOUT

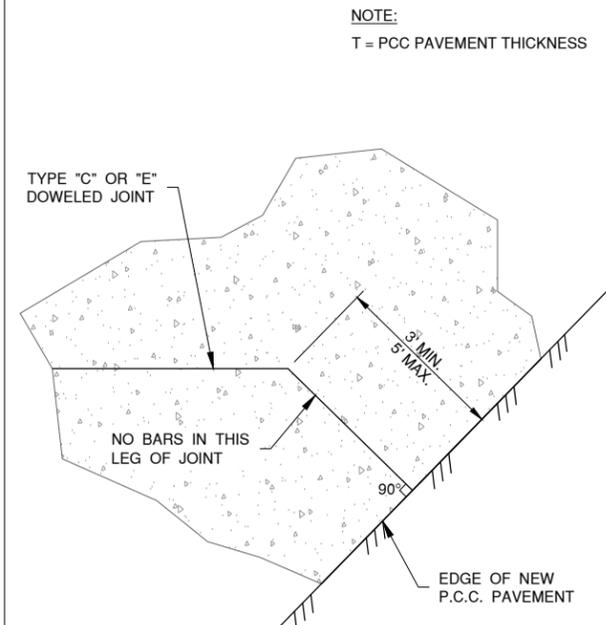
N.T.S.

Ⓡ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH DEFORMED WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

WIRE FABRIC NOTES:

1. PANELS TO BE REINFORCED WITH WIRE FABRIC DENOTED AS "R" ON THE JOINTING PLAN DRAWINGS.
2. WIRE FABRIC SHALL BE PLACED AT THE VERTICAL POSITION OF T/3 AS SHOWN.
3. WHEN A STRUCTURE IS LOCATED WITHIN A PANEL, WIRE FABRIC SHALL BE PLACED TO WITHIN 3" OF THE STRUCTURE.
4. MINIMUM WELDED WIRE FABRIC LAP IS 18 INCHES.
5. ALL WELDED WIRE FABRIC SHALL BE GRADE 60.
6. THE AREA OF WELDED WIRE FABRIC SHALL PROVIDE AT LEAST 0.05% OF REINFORCEMENT AREA TO UNIT CONCRETE AREA, ASSUMING GRADE 60 STEEL.

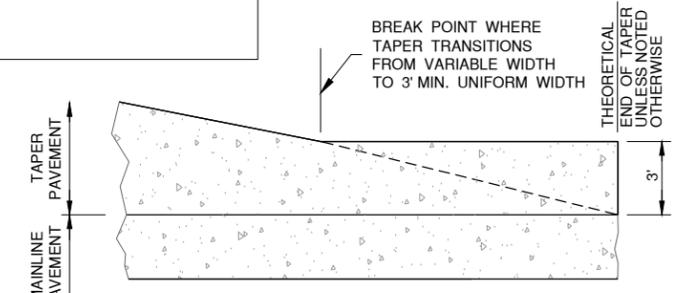
WELDED WIRE FABRIC	
SLAB THICKNESS	SUGGESTED FABRIC SIZE
5" - 8"	6 x 6 - W 4.0 x W 4.0
8" - 16"	6 x 6 - W 5.5 x W 5.5
16" - 18"	6 x 6 - W 5.5 x W 5.5
23" - 24"	6 x 6 - W 7.5 x W 7.5



PAVING DETAIL

N.T.S.

NOTE:
T = PCC PAVEMENT THICKNESS



STUB DETAIL

N.T.S.

TYPICAL AT END OF TAPER

TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

DOWEL BASKET DETAILS

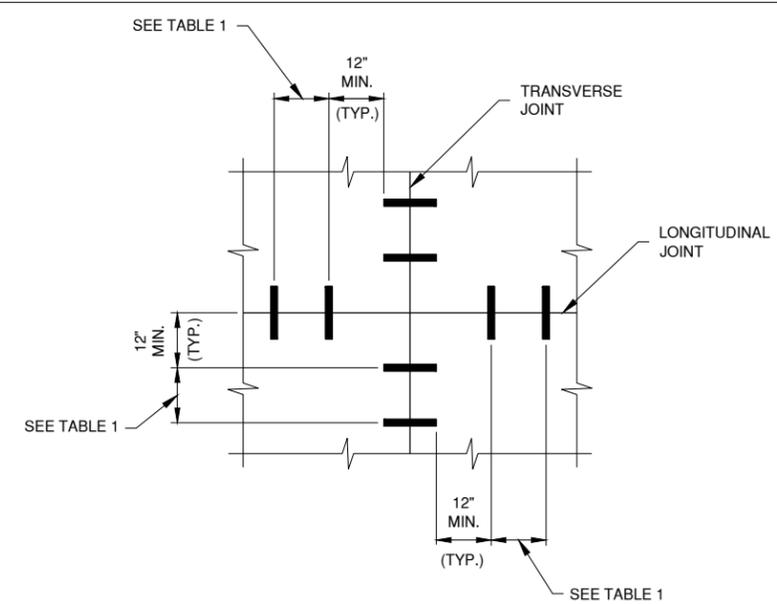
N.T.S.

DOWEL BASKET NOTES

1. #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT.
2. DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 1.
3. DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.
4. T = PCC PAVEMENT THICKNESS

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			PAVEMENT THICKNESS T - INCHES	TIE BAR DETAILS		
	DIA.	LENGTH	SPACING		BAR SIZE	LENGTH	SPACING
5 - 7	3/4"	18"	12" O.C.	6 OR LESS	#4	20"	36" O.C.
7.5 - 12	1"	18"	12" O.C.	GREATER THAN 6	#5	30"	30" O.C.
12.5 - 16	1 1/4"	20"	15" O.C.				
16.5 - 20	1 1/2"	20"	18" O.C.				
20.5 - 24	2"	24"	18" O.C.				

NOTE: ALL DOWELS CENTERED ON JOINT UNLESS OTHERWISE NOTED



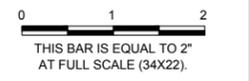
DOWEL PLACEMENT DETAIL

N.T.S.

IL CONTRACT: **DK063**
IL LETTING ITEM: **01A**
IL PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON**

PAVING DETAILS - 1

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DRAWN BY: CMT-RFD
CHECKED BY: CMT-RFD
APPROVED BY: DJK
DATE: 3/3/2023
JOB No: 210248-02

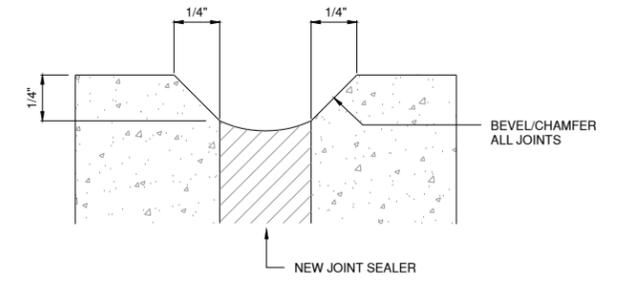
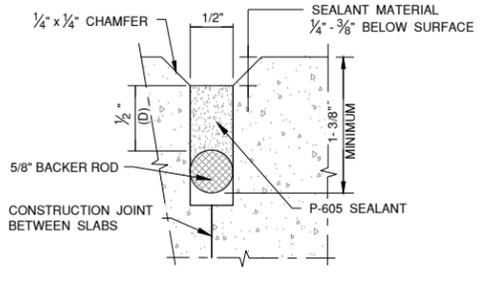
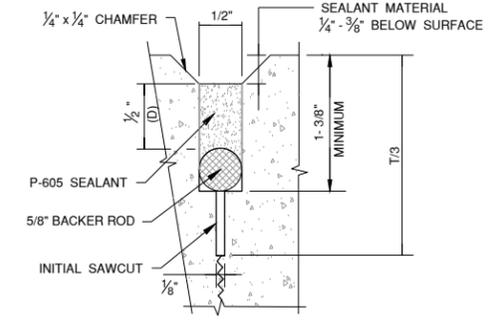
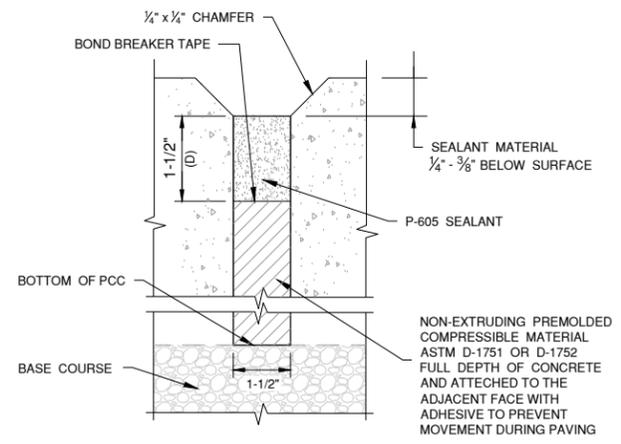
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IMAGE FILES: DTMA Logo-Block.dwg

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LAYOUT: IN NEW SHEET TEMPLATE!

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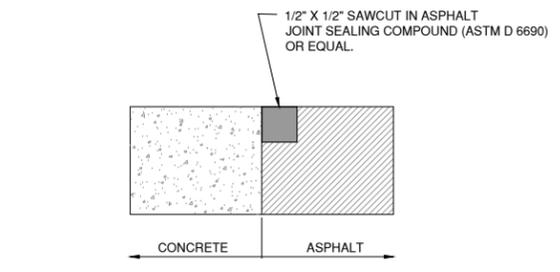
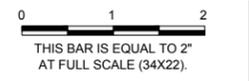


NOTE:
D = DEPTH
T = PCC PAVEMENT THICKNESS

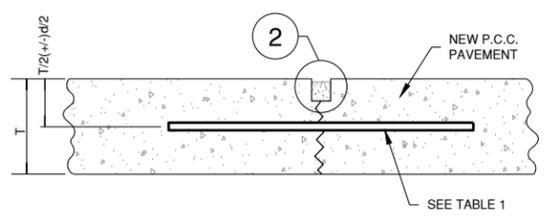
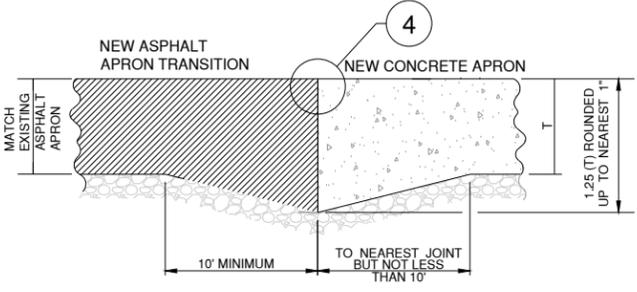
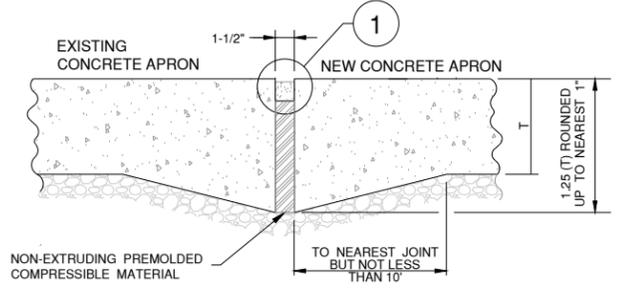
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IL LETTING ITEM: **01A**
IL PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



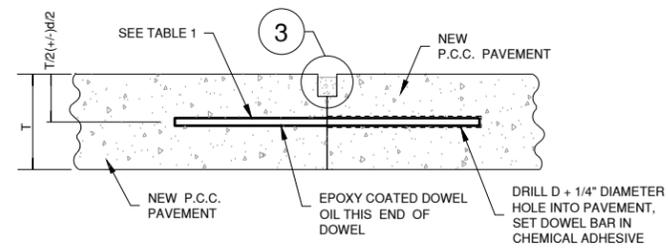
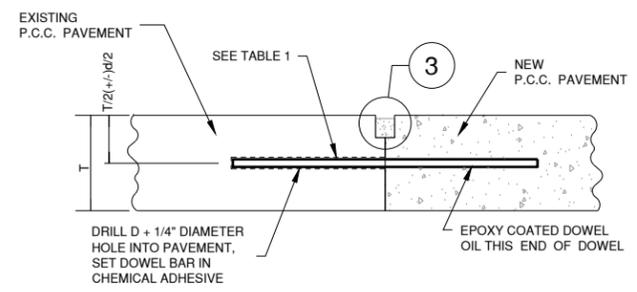
NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION



SYMBOL — A — A — A —

SYMBOL — A — A — A —

SYMBOL — C — C — C —



SYMBOL — E — E — E —

NOTE:
D = DEPTH
T = PCC PAVEMENT THICKNESS

SYMBOL — E — E — E —

JOINT SEALANT NOTES:

1. FACTOR, W/D. FIELD POURED SEALANTS REQUIRE DIFFERENT SEALANT RESERVOIR TO PROVIDE PROPER SHAPE.
2. BACKER ROD AND PREMOLDED COMPRESSIBLE MATERIAL MUST BE COMPATIBLE WITH THE DESIRED SHAPE FACTOR. TYPE OF LIQUID SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE
3. PLACE TOP OF BACKUP MATERIAL AS SHOWN TO CONFORM TO MANUFACTURER'S SHAPE FACTOR. SEALANT DEPTH ("D") IS MEASURED FROM THE HIGHEST SHAPE FACTORS FOR OPTIMUM PERFORMANCE. CONTRACTOR SHALL SUBMIT MFG. RECOMMENDATIONS ON JOINT RESERVOIR WITH SEALANT SUBMITTAL
4. SEALING PROCEDURES SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
5. ROUTING AND SEALING OF ALL BITUMINOUS AND PCC PAVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE PORTLAND CEMENT CONCRETE PAVEMENT PAY ITEM.

JOINTING NOTES

1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
3. ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
4. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
5. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
6. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
7. COST OF ALL JOINT SAWING, CLEANING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
8. SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
9. EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 1.
10. CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER DETAIL THIS SHEET.
11. CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
12. JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			PAVEMENT THICKNESS T - INCHES	TIE BAR DETAILS		
	DIA.	LENGTH	SPACING		BAR SIZE	LENGTH	SPACING
5 - 7	3/4"	18"	12" O.C.	6 OR LESS	#4	20"	36" O.C.
7.5 - 12	1"	18"	12" O.C.	GREATER THAN 6	#5	30"	30" O.C.
12.5 - 16	1 1/4"	20"	15" O.C.				
16.5 - 20	1 1/2"	20"	18" O.C.				
20.5 - 24	2"	24"	18" O.C.				

NOTE: ALL DOWELS CENTERED ON JOINT UNLESS OTHERWISE NOTED

DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
 AIRCRAFT PARKING APRON
 PAVING DETAILS - 2

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DESIGN BY:	CMT-RFD
DRAWN BY:	CMT-RFD
CHECKED BY:	CMT-RFD
APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02

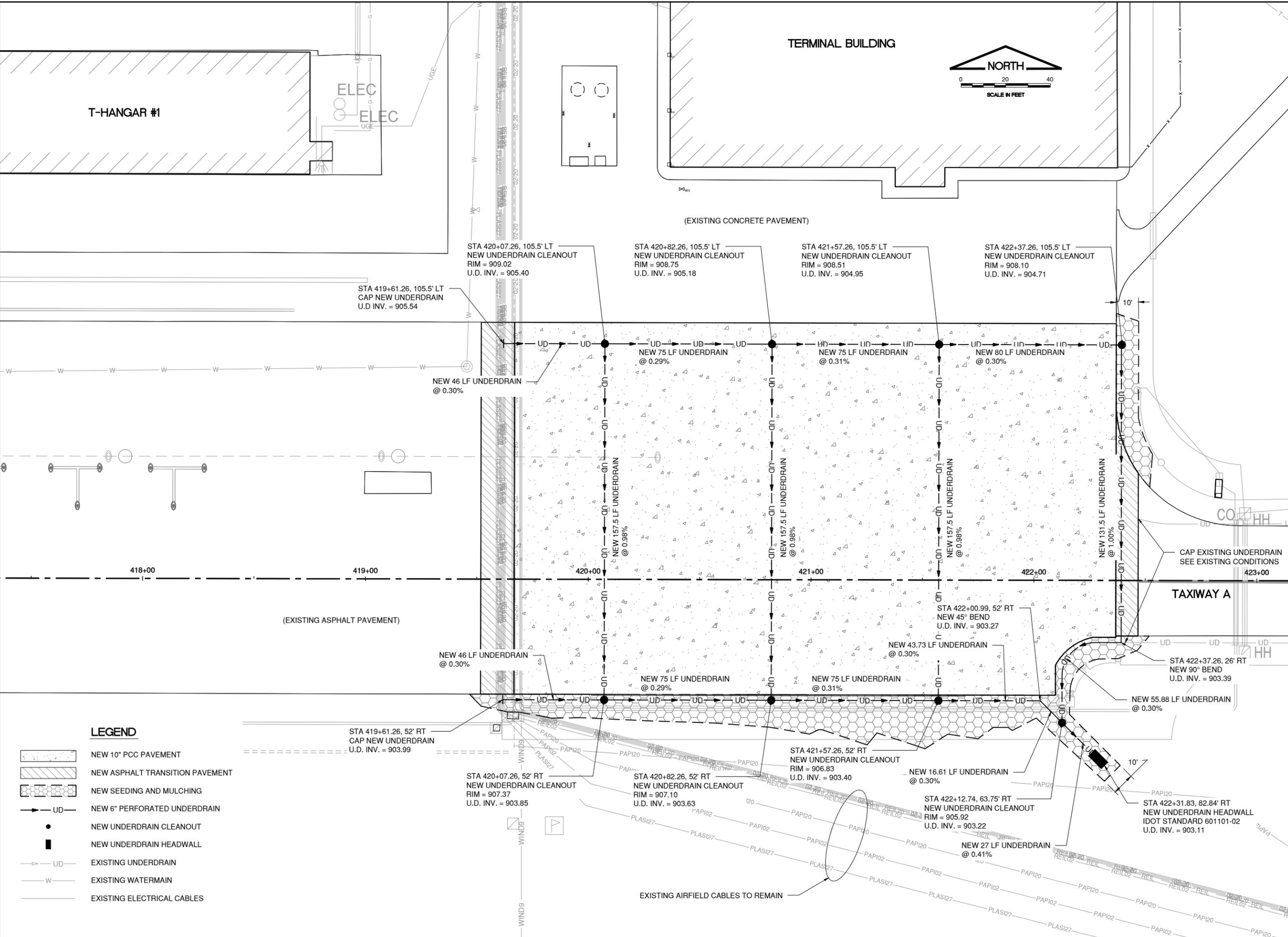
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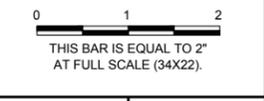
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IL CONTRACT: **DK063**
 IL LETTING ITEM: **01A**
 IL PROJECT: **DKB-4924**
 S.B.G. PROJECT: **3-17-SBGP-184**

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**DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
 AIRCRAFT PARKING APRON
 DRAINAGE AND LANDSCAPING PLAN**

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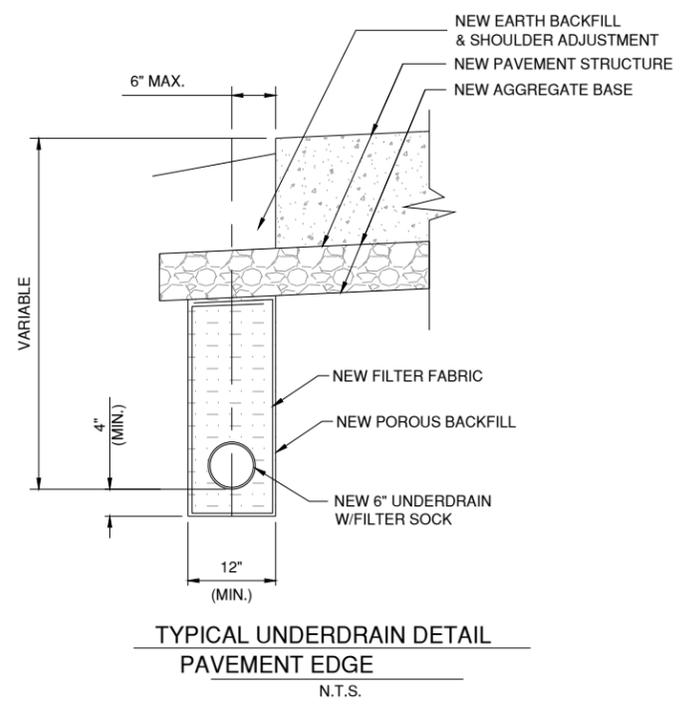
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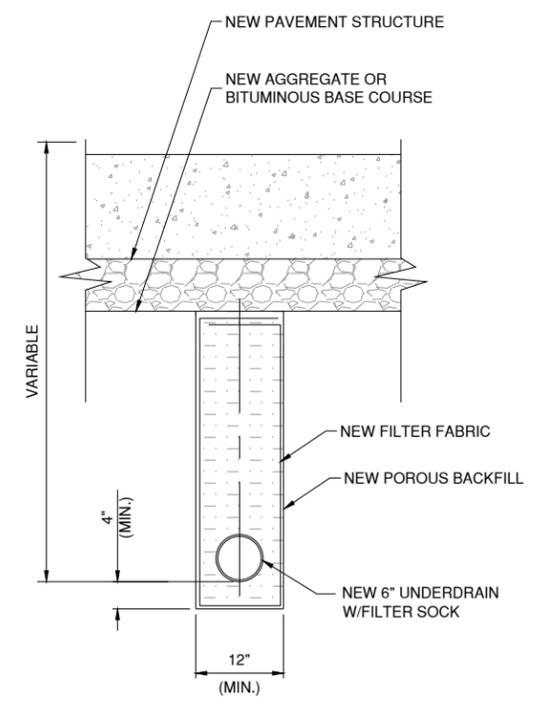
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 CHECKED BY: CMT-RFD
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 DATE: 3/3/2023
 JOB No: 210248-02

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SHEET 13 OF 18 SHEETS



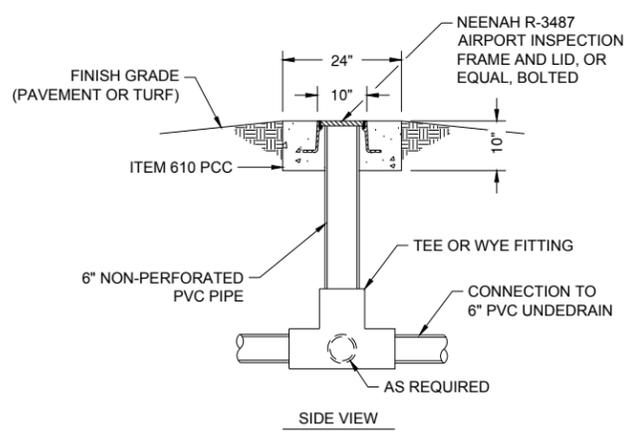
TYPICAL UNDERDRAIN DETAIL
 PAVEMENT EDGE
 N.T.S.



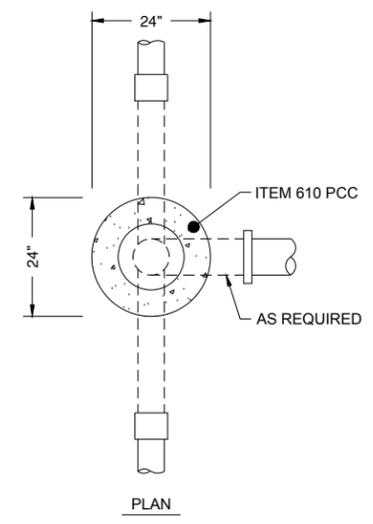
TYPICAL UNDERDRAIN DETAIL
 BELOW PAVEMENT
 N.T.S.

UNDERDRAIN CLEANOUT NOTES

1. CLEANOUTS IN NEW PAVEMENT SHALL BE CAST IN PLACE AT THE TIME OF PAVING.



UNDERDRAIN CLEAN-OUT DETAIL
 NOT TO SCALE

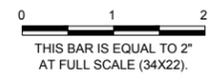


UNDERDRAIN CLEAN-OUT DETAILS
 NOT TO SCALE

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**DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
 AIRCRAFT PARKING APRON
 DRAINAGE DETAILS**

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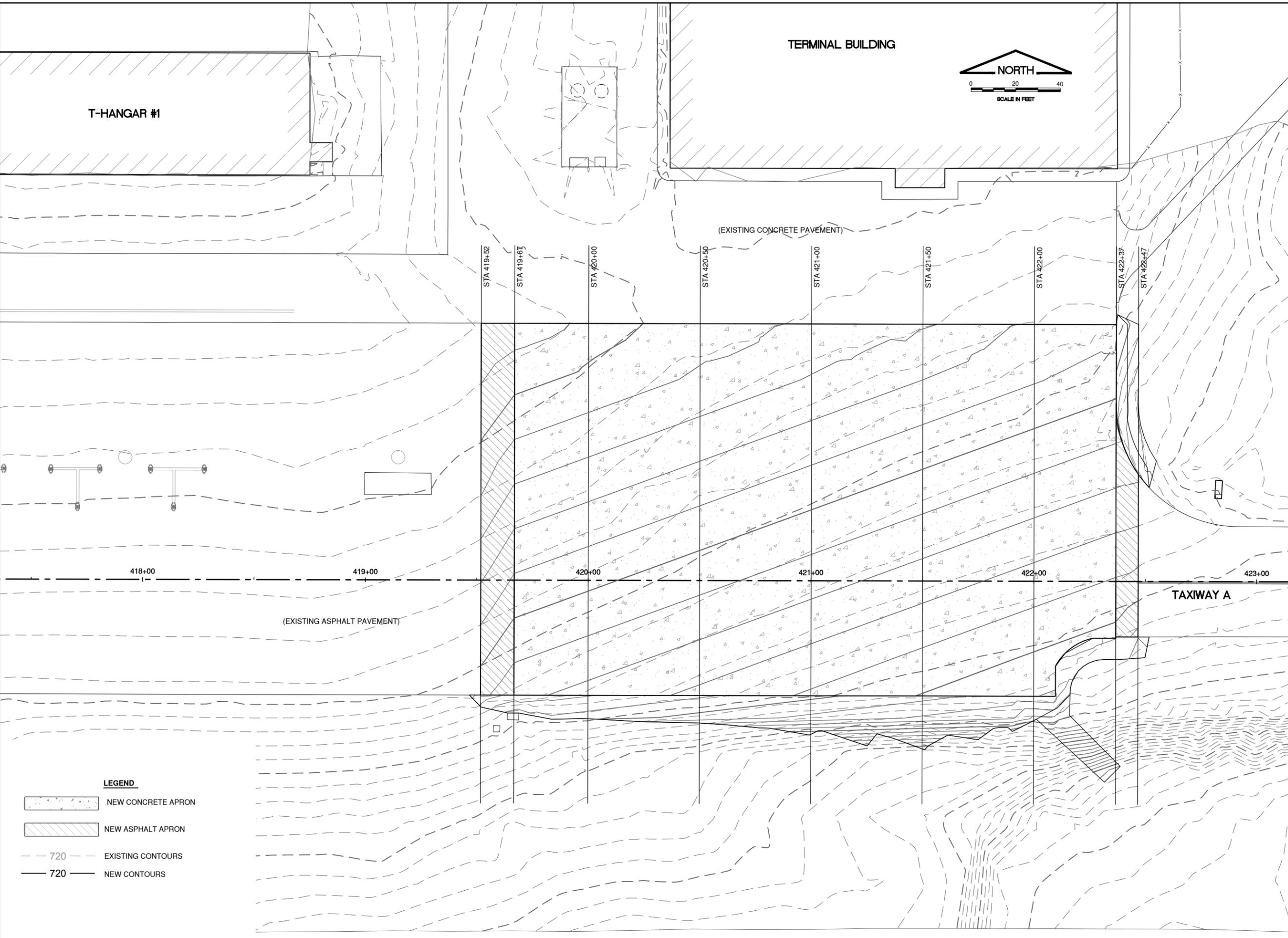
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CHECKED BY:	CMT-RFD
APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02

FINAL

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 UPDATE BY: Robert Heinz
 LAYOUT: Index To Sections
 IMAGE FILES: D:\MVA\cgs-backup
 XREF DWG: 210248-00 - Detail Base.dwg
 210248-00 - Detail Base.dwg
 Detail 2022 Title Block.dwg



LEGEND

	NEW CONCRETE APRON
	NEW ASPHALT APRON
	720 EXISTING CONTOURS
	720 NEW CONTOURS

IL CONTRACT: **DK063**
 IL LETTING ITEM: **01A**
 IL PROJECT: **DKB-4924**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # ----

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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON
INDEX TO SECTIONS

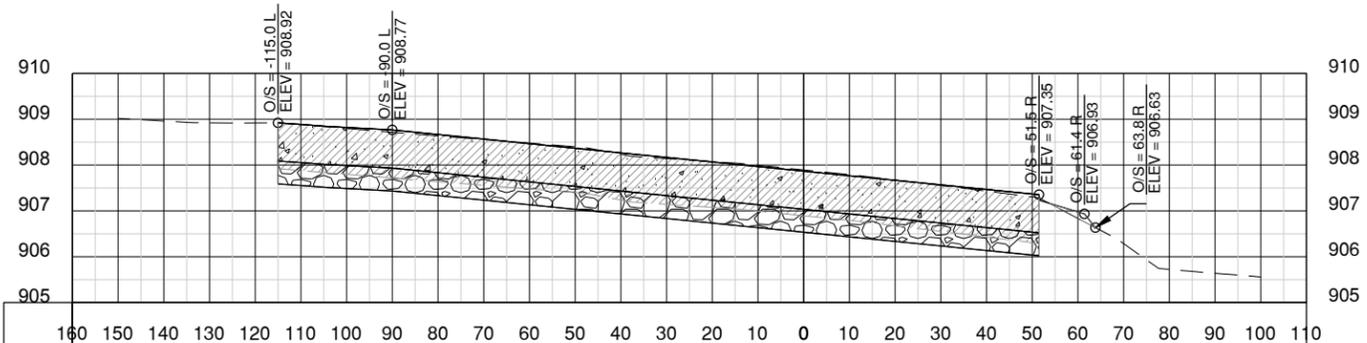

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 CHECKED BY: CMT-RFD
 APPROVED BY: DJK
 DATE: 3/3/2023
 JOB No: 210248-02

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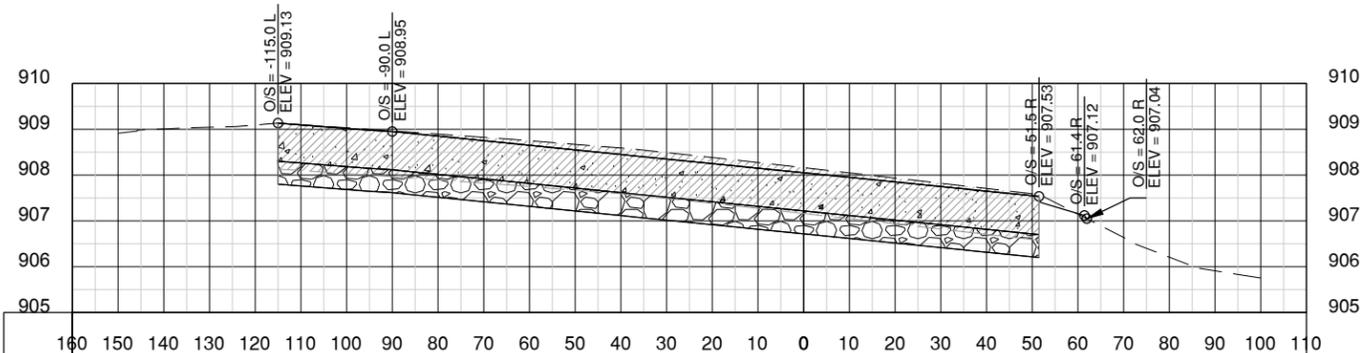
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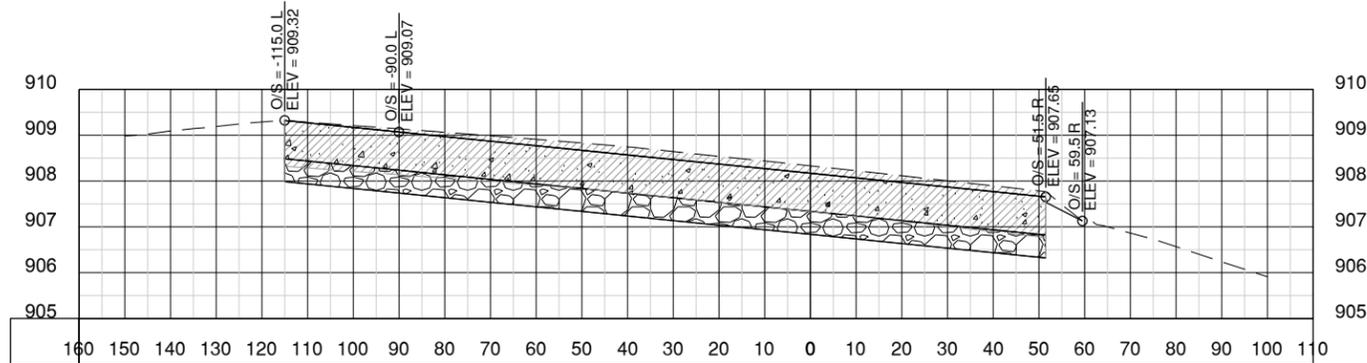
420
+
50

TOTAL VOLUME AT STATION 420+50.00	
UNCLASSIFIED EXCAVATION	54.40 SF
TOPSOIL FILL	0.89 SF
TOPSOIL CUT	0.05 SF



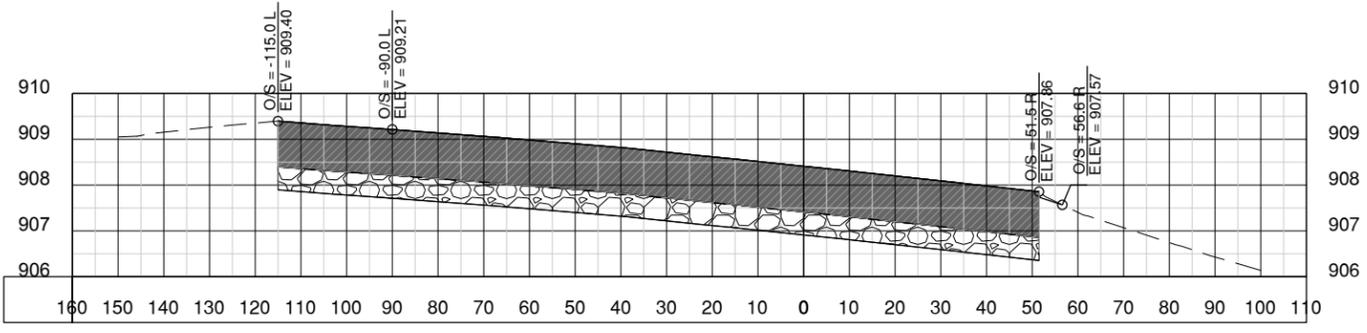
420
+
00

TOTAL VOLUME AT STATION 420+00.00	
UNCLASSIFIED EXCAVATION	69.23 SF
TOPSOIL FILL	0.05 SF
TOPSOIL CUT	0.66 SF



419
+
66

TOTAL VOLUME AT STATION 419+66.76	
UNCLASSIFIED EXCAVATION	76.71 SF
TOPSOIL FILL	0.00 SF
TOPSOIL CUT	0.92 SF



419
+
51

TOTAL VOLUME AT STATION 419+51.76	
UNCLASSIFIED EXCAVATION	83.28 SF
TOPSOIL FILL	0.00 SF
TOPSOIL CUT	0.24 SF

LEGEND

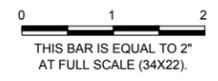
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- NEW 12" AVERAGE PAVEMENT REMOVAL (PCC / BITUMINOUS)
- NEW GROUND LINE
- EXISTING GROUND LINE

NOTES
 TOPSOIL CUT AND FILL IN PROPOSED TURF AREAS ARE NOT INCLUDED IN THE EARTHWORK QUANTITIES AND ARE CONSIDERED INCIDENTAL TO (AR152840) SHOULDER ADJUSTMENT.

IL CONTRACT: **DK063**
 IL LETTING ITEM: **01A**
 IL PROJECT: **DKB-4924**
 S.B.G. PROJECT: **3-17-SBGP-184**

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NUMBER	BY	DATE



DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON
CROSS SECTIONS - 1

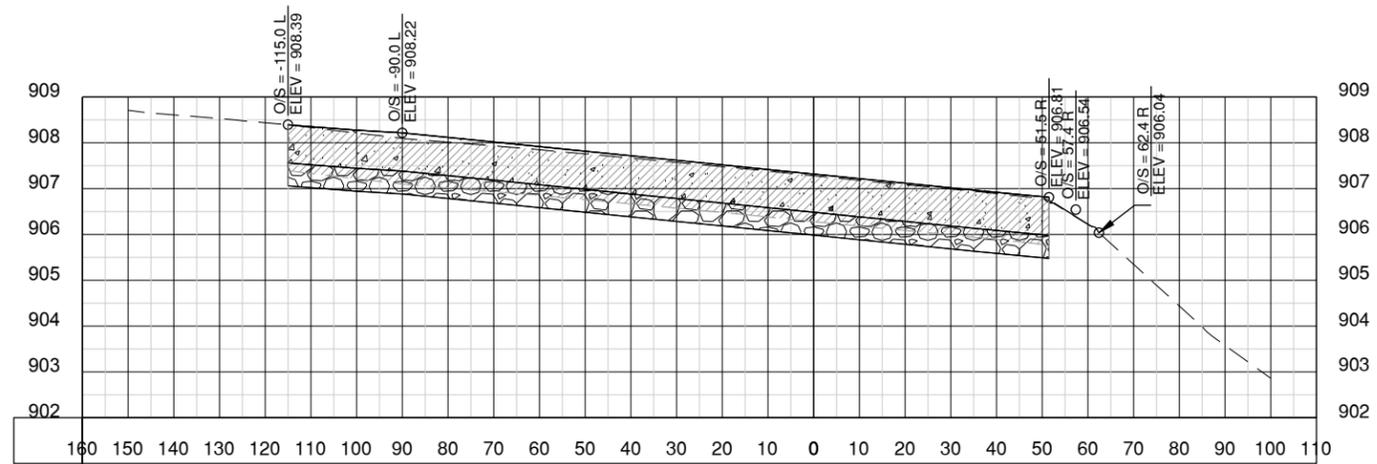
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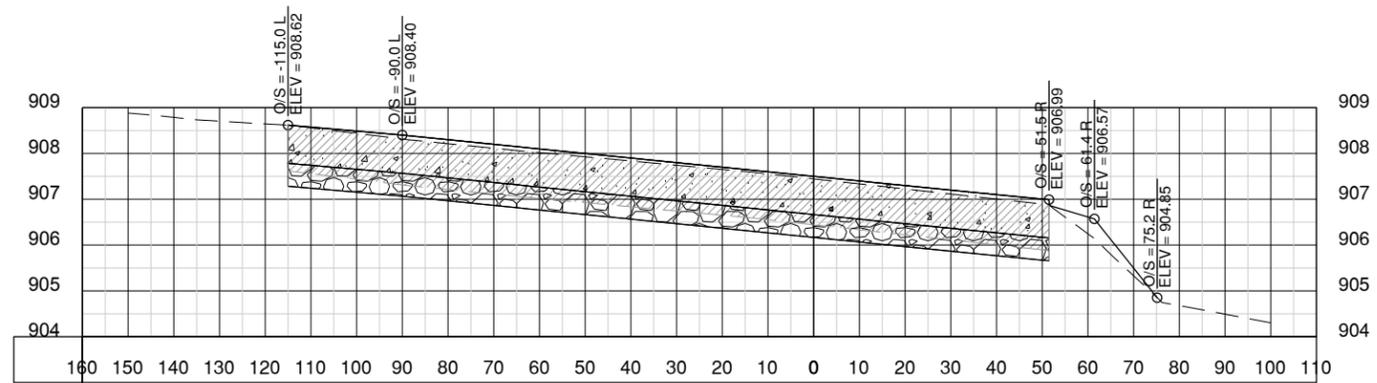
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JOB No:	210248-02

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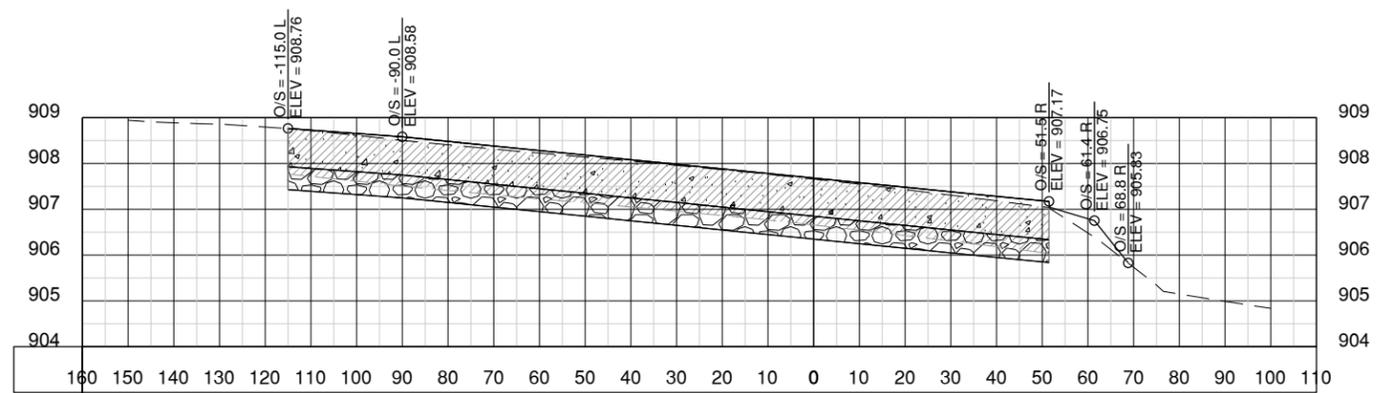
422
+
00

TOTAL VOLUME AT STATION 422+00.00	
UNCLASSIFIED EXCAVATION	46.82 SF
TOPSOIL FILL	0.88 SF
TOPSOIL CUT	0.02 SF



421
+
50

TOTAL VOLUME AT STATION 421+50.00	
UNCLASSIFIED EXCAVATION	44.16 SF
TOPSOIL FILL	5.12 SF
TOPSOIL CUT	0.00 SF



421
+
00

TOTAL VOLUME AT STATION 421+00.00	
UNCLASSIFIED EXCAVATION	47.18 SF
TOPSOIL FILL	3.17 SF
TOPSOIL CUT	0.00 SF

LEGEND

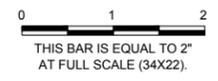
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- NEW 12" AVERAGE PAVEMENT REMOVAL (PCC / BITUMINOUS)
- NEW GROUND LINE
- EXISTING GROUND LINE

NOTES
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IL. CONTRACT: **DK063**
IL. LETTING ITEM: **01A**
IL. PROJECT: **DKB-4924**
S.B.G. PROJECT: **3-17-SBGP-184**

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**DEKALB TAYLOR MUNICIPAL AIRPORT
DEKALB, ILLINOIS
REMOVE AND REPLACE A PORTION OF THE
AIRCRAFT PARKING APRON
CROSS SECTIONS - 2**

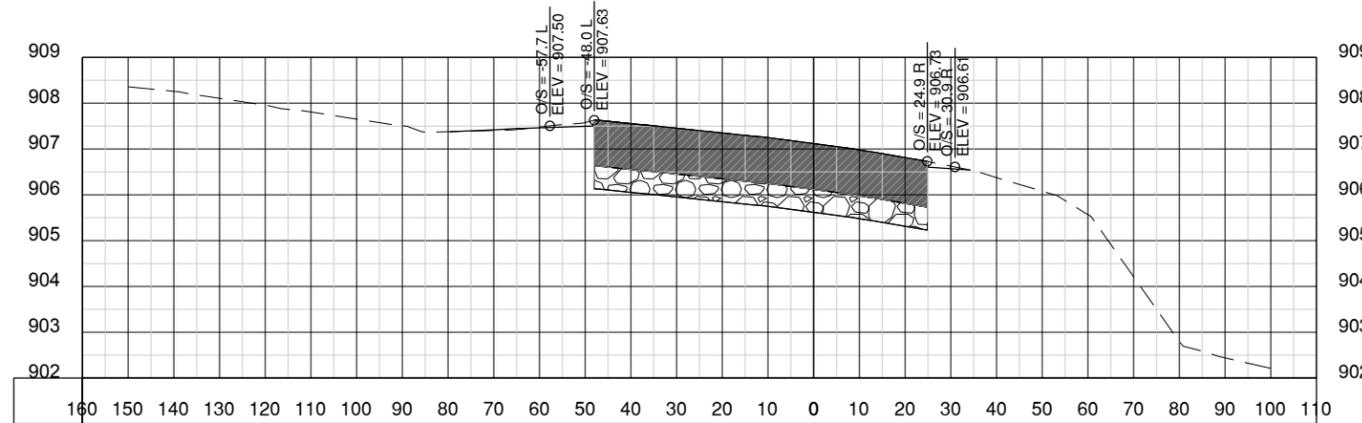
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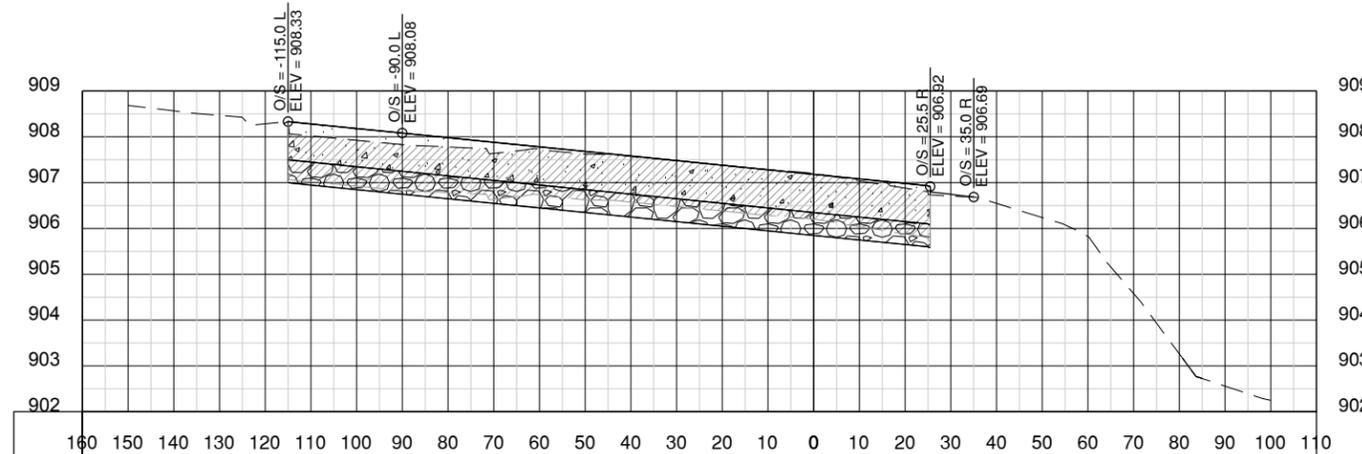
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CHECKED BY:	CMT-RFD
APPROVED BY:	DJK
DATE:	3/3/2023
JOB No:	210248-02

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422
+
46

TOTAL VOLUME AT STATION 422+46.76	
UNCLASSIFIED EXCAVATION	36.41 SF
TOPSOIL FILL	0.00 SF
TOPSOIL CUT	0.57 SF



422
+
36

TOTAL VOLUME AT STATION 422+36.76	
UNCLASSIFIED EXCAVATION	32.61 SF
TOPSOIL FILL	0.33 SF
TOPSOIL CUT	0.00 SF

LEGEND

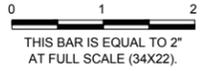
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	NEW 12" AVERAGE PAVEMENT REMOVAL (PCC / BITUMINOUS)
	NEW GROUND LINE
	EXISTING GROUND LINE

NOTES
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**DEKALB TAYLOR MUNICIPAL AIRPORT
 DEKALB, ILLINOIS
 REMOVE AND REPLACE A PORTION OF THE
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CROSS SECTIONS - 3

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