

DIXON MUNICIPAL AIRPORT CHARLES R. WALGREEN FIELD DIXON, ILLINOIS

CONSTRUCTION PLANS FOR DIXON MUNICIPAL AIRPORT

REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

ILLINOIS PROJECT: C73-4978

MARCH 3, 2023



D Kyle Peabody

LICENSE EXPIRATION
 DATE: 11/30/2023
 DATE SIGNED: 03/09/2023

DESIGN INFORMATION

APPROACH CATEGORY B
 DESIGN GROUP I

DIXON MUNICIPAL AIRPORT

TOWNSHIP: 21 NORTH	DIXON TOWNSHIP
RANGE: 9 EAST	(SECTION: 3)
LEE COUNTY	OPPOSITE LINCOLN HIGHWAY 38 (FRANKLIN GROVE ROAD)

UNICOM RADIO FREQUENCY - 123.05

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JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS. OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

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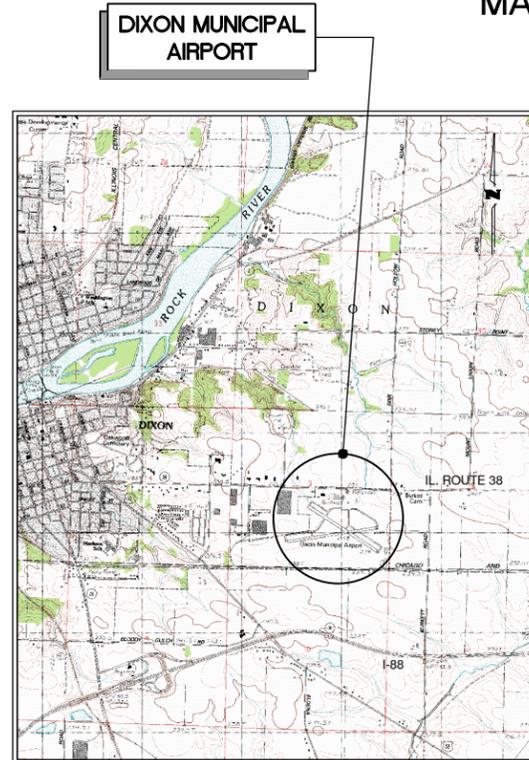
SUBMITTED BY *D Kyle Peabody*
 D. KYLE PEABODY, P.E.

DATE 3/9/2023

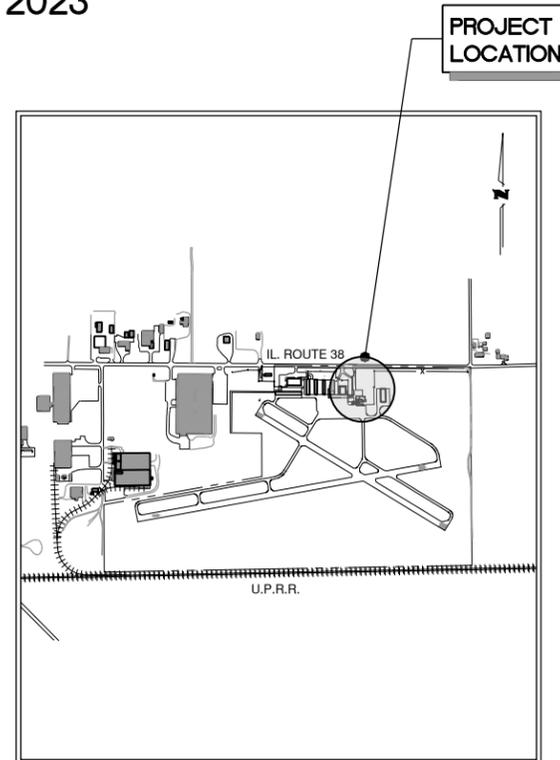
DIXON MUNICIPAL AIRPORT
 CHARLES R. WALGREEN FIELD
 DIXON, ILLINOIS

PUBLIC WORKS
 DIRECTOR/ASSISTANT
 CITY MANAGER *Matt Heckman*
 MATT HECKMAN

DATE 03/09/2023



LOCATION MAP



SITE PLAN

INDEX TO SHEETS

1. COVER SHEET
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4. SEQUENCE OF CONSTRUCTION
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7. GEOMETRIC LAYOUT AND PAVEMENT MARKING PLAN
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9. TYPICAL SECTIONS
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11. STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS
12. DRAINAGE NOTES AND DETAILS
13. MISCELLANEOUS DETAILS
14. GEOTECHNICAL ENGINEERING INFORMATION

SUMMARY OF QUANTITIES

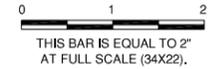
BASE BID				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	195	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	570	
AR156520	INLET PROTECTION	EACH	2	
AR156531	EROSION CONTROL BLANKET	SQ YD	970	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	195	
AR208910	REMOVE & REPLACE AGGREGATE BASE	SQ YD	570	
AR209650	AGGREGATE BASE PREPARATION	SQ YD	2,250	
AR401610	BITUMINOUS SURFACE COURSE	TON	200	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	2,250	
AR403610	BITUMINOUS BASE COURSE	TON	335	
AR501605	5" PCC SIDEWALK	SQ FT	2,785	
AR501690	PCC SIDEWALK REMOVAL	SQ FT	2,570	
AR602510	BITUMINOUS PRIME COAT	GALLON	175	
AR603510	BITUMINOUS TACK COAT	GALLON	350	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	1,040	
AR754410	COMB CONCRETE CURB & GUTTER	FOOT	720	
AR754904	REMOVE COMB CURB & GUTTER	FOOT	570	
AR770945	ADJUST SANITARY MANHOLE	EACH	1	
AR800079	DETECTABLE WARNING SURFACE	SQ FT	45	
AR901510	SEEDING	ACRE	0.2	
AR910200	ROADWAY SIGN	EACH	1	
AR910915	REMOVE ROADWAY SIGN	EACH	1	
ADDITIVE ALTERNATE NO. 1 - PCC REMOVAL AND REPLACEMENT				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS156520	INLET PROTECTION	EACH	2	
AS156531	EROSION CONTROL BLANKET	SQ YD	20	
AS501508	8" PCC PAVEMENT	SQ YD	36	
AS501900	REMOVE PCC PAVEMENT	SQ YD	36	
AS209650	AGGREGATE BASE PREPARATION	SQ YD	35	
AS701512	12" RCP, CLASS IV	FOOT	28	
AS701900	REMOVE PIPE	FOOT	28	
AS751411	INLET-TYPE A	EACH	1	
AS751415	INLET-SPECIAL	EACH	1	
AS751900	REMOVE INLET	EACH	2	
AS754410	COMB CONCRETE CURB & GUTTER	FOOT	40	
AS754904	REMOVE COMB CURB & GUTTER	FOOT	40	

IL CONTRACT: **D1034**
IL LETTING ITEM: **10A**
IL PROJECT: **C73-4978**
S.B.G. PROJECT: **N/A**

SURVEY BOOK # ---

REVISIONS

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

INDEX TO SHEETS/SUMMARY OF QUANTITIES

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 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 03/03/2023
 JOB No: 22004581-00

FINAL

BMP/Stormwater
BMP/Stormwater

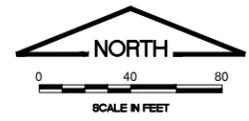
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LAYOUT: Layout

DATE: Friday, March 10, 2023 11:49:30 AM
FILE: K:\Drawings\2023\2023-03-10_RehabilitateAirportAccessRoad\Drawings\SitePlan.dwg

ALIGNMENT DATA					
SEGMENT	ALIGNMENT	STATION	NORTHING/ EASTING	STATION	NORTHING/ EASTING
BASELINE A	ENTRANCE ROAD	4+00	1883724.954 2493410.728	8+00	1884124.854 2493401.780

HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
CP #1	CUT "X" IN WALK	1883851.287	2493373.897
CP #2	IRON PIN	1883532.901	2493539.988
CP #3	CUT "X" FRAME DRAINAGE STRUCTURE	1883793.238	2492849.806

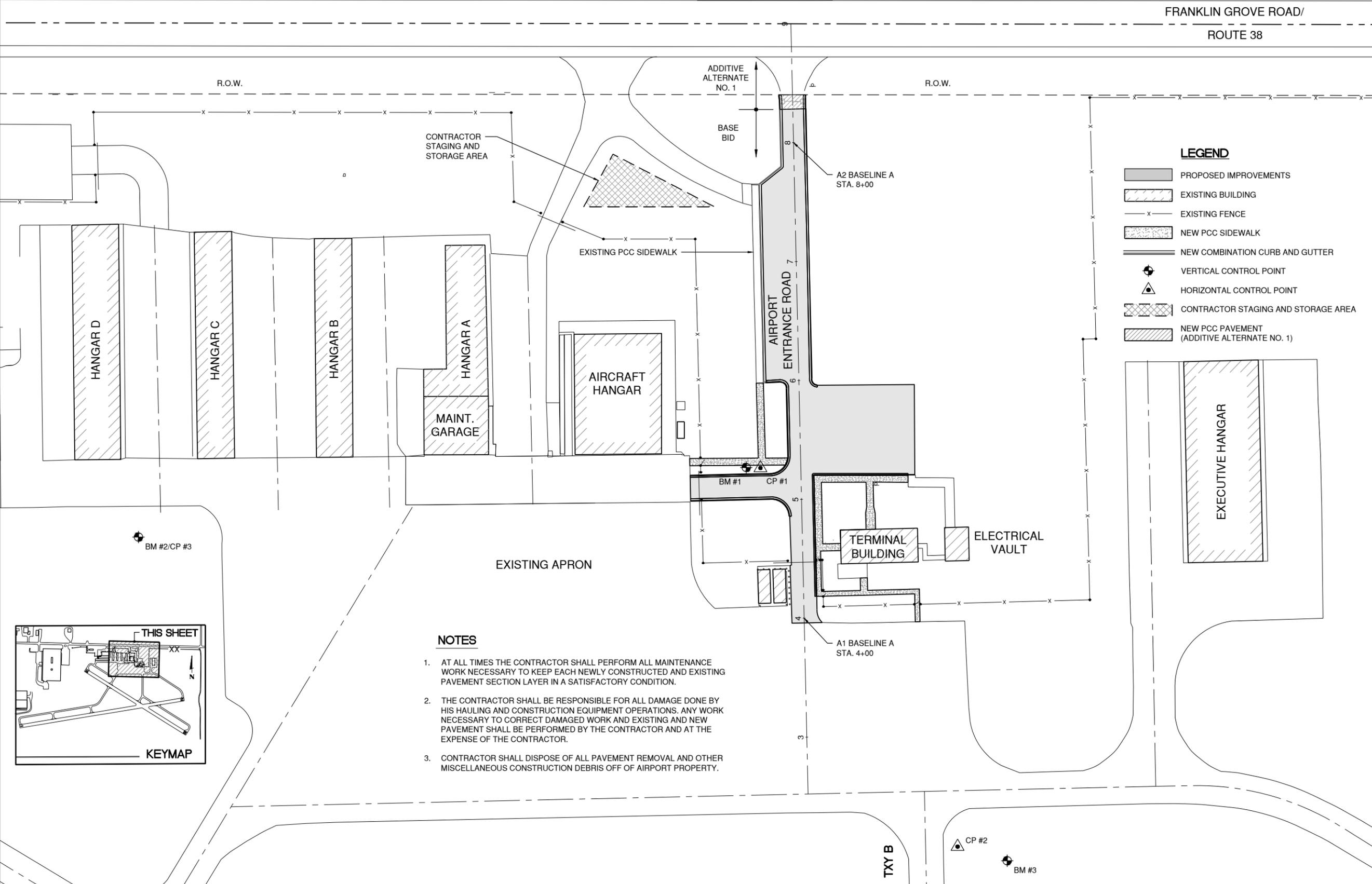
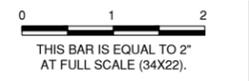
VERTICAL CONTROL				
BENCHMARK	DESCRIPTION	NORTHING	EASTING	ELEVATION
BM #1	ARROW BOLT HYDRANT OUTSIDE GATE 4	1883851.699	2493361.928	784.28
BM #2	CUT "X" FRAME DRAINAGE STRUCTURE	1883793.238	2492849.806	774.99
BM #3	ELECTRICAL HANDHOLE TAXIWAY C AND APRON	1883521.018	2493581.842	781.11



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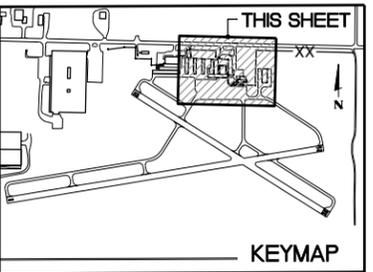
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- LEGEND**
- PROPOSED IMPROVEMENTS
 - EXISTING BUILDING
 - EXISTING FENCE
 - NEW PCC SIDEWALK
 - NEW COMBINATION CURB AND GUTTER
 - VERTICAL CONTROL POINT
 - HORIZONTAL CONTROL POINT
 - CONTRACTOR STAGING AND STORAGE AREA
 - NEW PCC PAVEMENT (ADDITIVE ALTERNATE NO. 1)

- NOTES**
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED AND EXISTING PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT OPERATIONS. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING AND NEW PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
 - CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.



DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING
SITE PLAN AND PROJECT CONTROL PLAN

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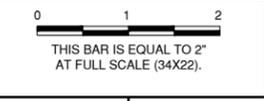
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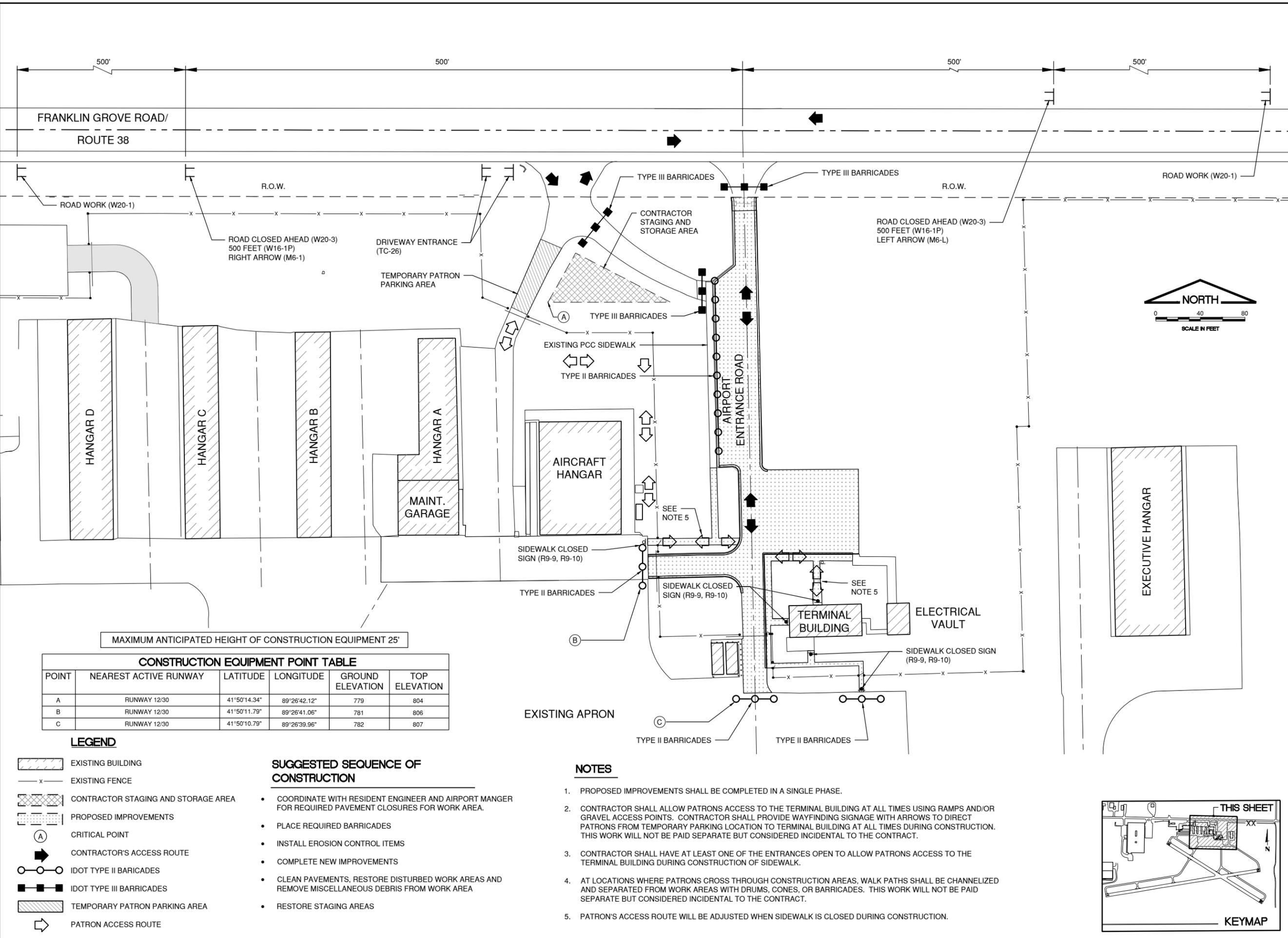
**DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING
 SEQUENCE OF CONSTRUCTION**

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SHEET 4 OF 14 SHEETS



MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT 25'

CONSTRUCTION EQUIPMENT POINT TABLE					
POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
A	RUNWAY 12/30	41°50'14.34"	89°26'42.12"	779	804
B	RUNWAY 12/30	41°50'11.79"	89°26'41.06"	781	806
C	RUNWAY 12/30	41°50'10.79"	89°26'39.96"	782	807

LEGEND

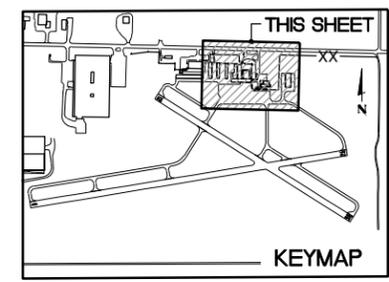
- EXISTING BUILDING
- EXISTING FENCE
- CONTRACTOR STAGING AND STORAGE AREA
- PROPOSED IMPROVEMENTS
- CRITICAL POINT
- CONTRACTOR'S ACCESS ROUTE
- IDOT TYPE II BARRICADES
- IDOT TYPE III BARRICADES
- TEMPORARY PATRON PARKING AREA
- PATRON ACCESS ROUTE

SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
- PLACE REQUIRED BARRICADES
- INSTALL EROSION CONTROL ITEMS
- COMPLETE NEW IMPROVEMENTS
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA
- RESTORE STAGING AREAS

NOTES

1. PROPOSED IMPROVEMENTS SHALL BE COMPLETED IN A SINGLE PHASE.
2. CONTRACTOR SHALL ALLOW PATRONS ACCESS TO THE TERMINAL BUILDING AT ALL TIMES USING RAMPS AND/OR GRAVEL ACCESS POINTS. CONTRACTOR SHALL PROVIDE WAYFINDING SIGNAGE WITH ARROWS TO DIRECT PATRONS FROM TEMPORARY PARKING LOCATION TO TERMINAL BUILDING AT ALL TIMES DURING CONSTRUCTION. THIS WORK WILL NOT BE PAID SEPARATE BUT CONSIDERED INCIDENTAL TO THE CONTRACT.
3. CONTRACTOR SHALL HAVE AT LEAST ONE OF THE ENTRANCES OPEN TO ALLOW PATRONS ACCESS TO THE TERMINAL BUILDING DURING CONSTRUCTION OF SIDEWALK.
4. AT LOCATIONS WHERE PATRONS CROSS THROUGH CONSTRUCTION AREAS, WALK PATHS SHALL BE CHANNELIZED AND SEPARATED FROM WORK AREAS WITH DRUMS, CONES, OR BARRICADES. THIS WORK WILL NOT BE PAID SEPARATE BUT CONSIDERED INCIDENTAL TO THE CONTRACT.
5. PATRON'S ACCESS ROUTE WILL BE ADJUSTED WHEN SIDEWALK IS CLOSED DURING CONSTRUCTION.



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 UPDATE BY: Jim Chise
 LAYOUT: Layout1

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GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AC 150/5370-2 (LATEST EDITION).
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO AR150520 MOBILIZATION.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, FLAGGERS, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL HAVE FLASHING YELLOW LIGHT(S) AND CONFORM TO IDOT STANDARD 701901-08, TYPE II AND TYPE III. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER, THROUGH THE RESIDENT ENGINEER, (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN EXCAVATOR TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET.
- DIXON MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. THE RESTORATION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDING, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 45' FROM ACTIVE TAXIWAYS AND 125' FROM ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY SAFETY AREAS, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER AND/OR AIRPORT MAINTENANCE. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.

ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY OBSTACLE FREE ZONE (ROFZ) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED UNLESS OTHERWISE SHOWN ON THE PLANS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED. THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON THIS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON THIS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

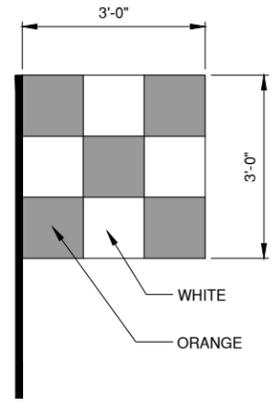
**DESIGN AIRCRAFT APPROACH CATEGORY: B
 DESIGN AIRPORT GROUP: I**
 RUNWAY 12/30 OBSTACLE FREE ZONE TOTAL WIDTH = 250'
 RUNWAY 8/26 OBSTACLE FREE ZONE TOTAL WIDTH= 250'
 TAXIWAY CENTERLINE TO OBJECT SEPARATION = 44.5'
 TAXILANE CENTERLINE TO OBJECT SEPARATION = 39.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

- REHABILITATE TAXIWAYS A, B, C AND D AND T-HANGAR TAXIWAY

CONTRACTOR ACCESS ROUTE NOTES

- ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.

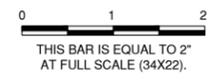


CONSTRUCTION EQUIPMENT AND TRUCK/VEHICLE SIGNAL FLAG
 N.T.S.

IL. CONTRACT: **D1034**
 IL. LETTING ITEM: **10A**
 IL. PROJECT: **C73-4978**
 S.B.G. PROJECT: **N/A**

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**DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING**

**SEQUENCE OF CONSTRUCTION
 GENERAL NOTES AND DETAILS**

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APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	22004581-00

DATE: Friday, March 10, 2023 11:49:50 AM
FILE: K:\Drawings\2023\23-03-00_Road\Drawings\Geometric\Layout.dwg

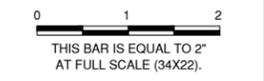
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IL CONTRACT: **DI034**
IL LETTING ITEM: **10A**
IL PROJECT: **C73-4978**
S.B.G. PROJECT: **N/A**

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

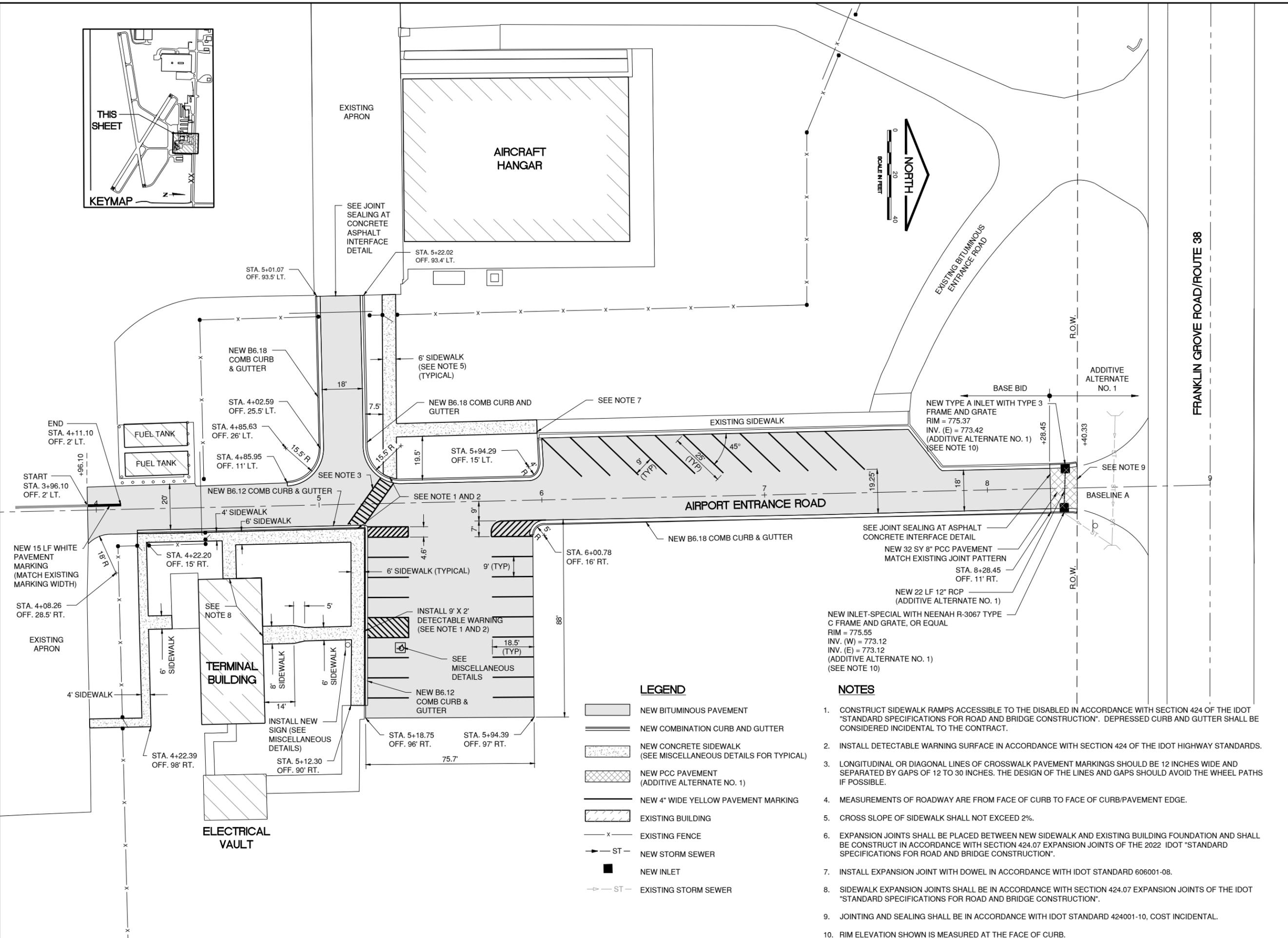
GEOMETRIC LAYOUT AND PAVEMENT MARKING PLAN

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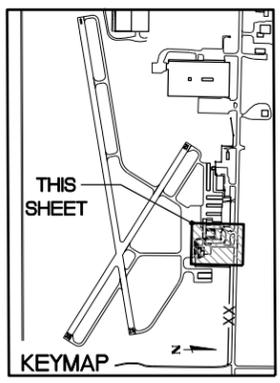
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SHEET 7 OF 14 SHEETS

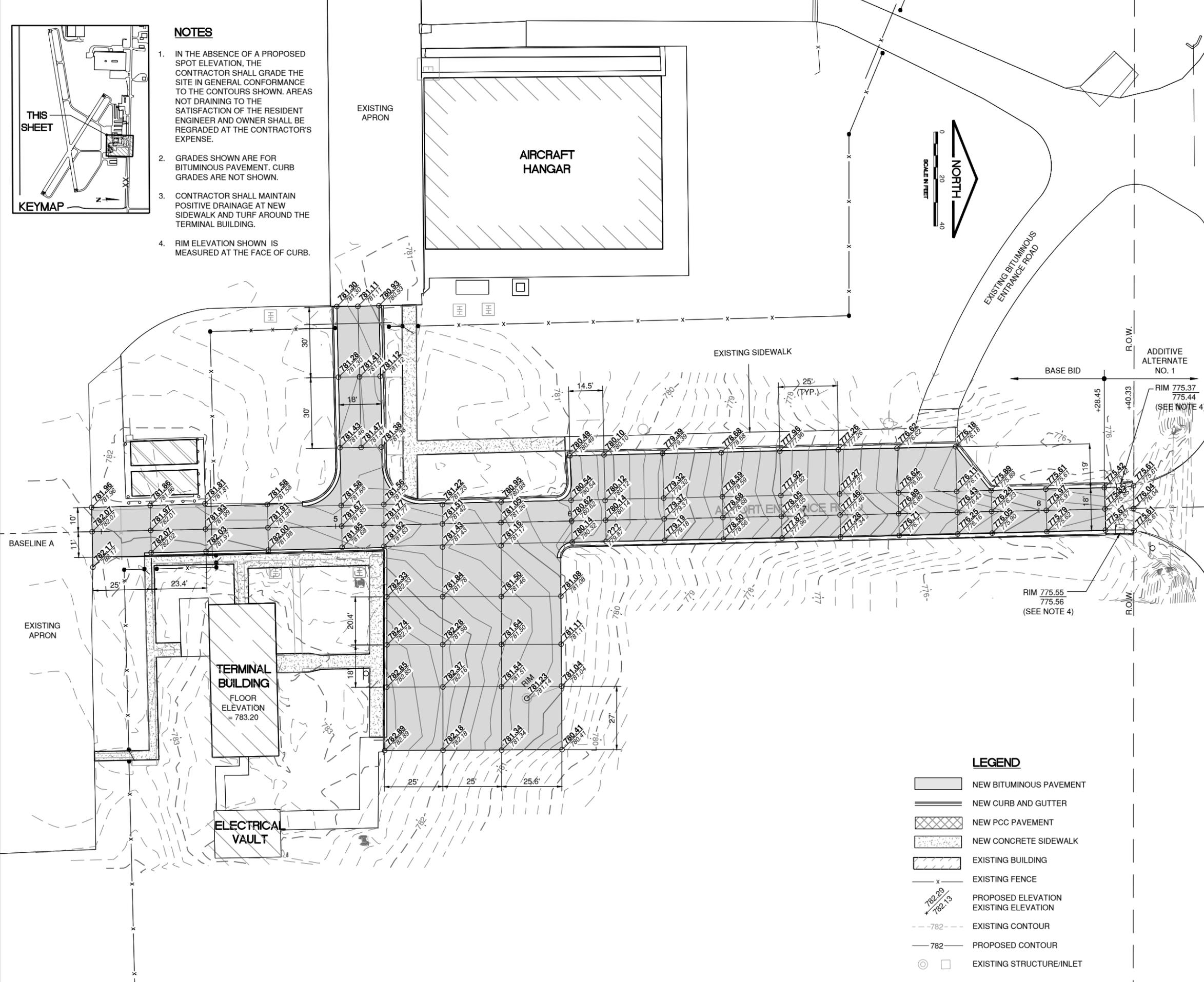


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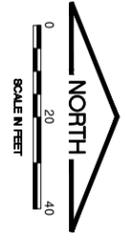
- NOTES**
1. IN THE ABSENCE OF A PROPOSED SPOT ELEVATION, THE CONTRACTOR SHALL GRADE THE SITE IN GENERAL CONFORMANCE TO THE CONTOURS SHOWN. AREAS NOT DRAINING TO THE SATISFACTION OF THE RESIDENT ENGINEER AND OWNER SHALL BE REGRADED AT THE CONTRACTOR'S EXPENSE.
 2. GRADES SHOWN ARE FOR BITUMINOUS PAVEMENT. CURB GRADES ARE NOT SHOWN.
 3. CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AT NEW SIDEWALK AND TURF AROUND THE TERMINAL BUILDING.
 4. RIM ELEVATION SHOWN IS MEASURED AT THE FACE OF CURB.



LEGEND

	NEW BITUMINOUS PAVEMENT
	NEW CURB AND GUTTER
	NEW PCC PAVEMENT
	NEW CONCRETE SIDEWALK
	EXISTING BUILDING
	EXISTING FENCE
	PROPOSED ELEVATION
	EXISTING ELEVATION
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STRUCTURE/INLET

FRANKLIN GROVE ROAD/ROUTE 38



IL CONTRACT: **D1034**
 IL LETTING ITEM: **10A**
 IL PROJECT: **C73-4978**
 S.B.G. PROJECT: **N/A**

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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 REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

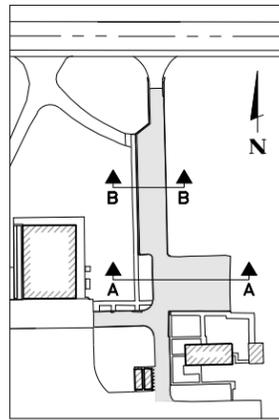
STAKING PLAN

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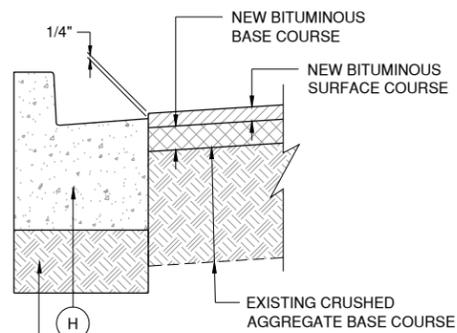
SHEET 8 OF 14 SHEETS



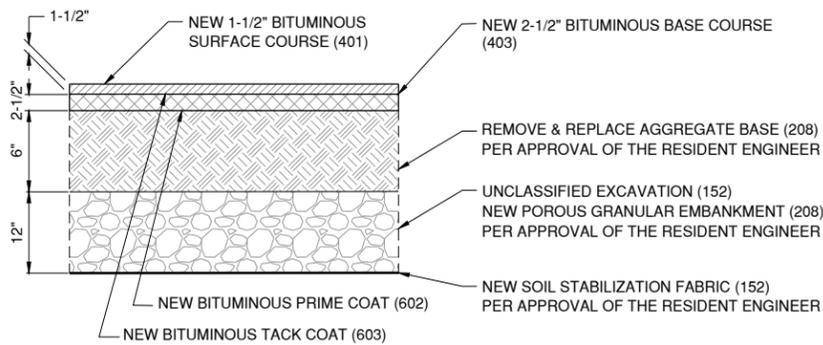
- LEGEND**
- (A) EXISTING 2" TO 5.5" BITUMINOUS PAVEMENT
 - (B) EXISTING 10" TO 13" CRUSHED AGGREGATE BASE COURSE
 - (C) EXISTING COMBINATION CURB AND GUTTER
 - (D) PROPOSED BITUMINOUS REHABILITATION
REMOVE EXISTING 2" TO 5.5" BITUMINOUS PAVEMENT (401900)
NEW 1-1/2" BITUMINOUS SURFACE COURSE (401)
NEW 2-1/2" BITUMINOUS BASE COURSE (403)
AGGREGATE BASE PREPARATION (209) (SEE NOTE 1)
 - (E) NEW BITUMINOUS TACK COAT (603)
 - (F) NEW BITUMINOUS PRIME COAT (602)
 - (G) EXISTING CRUSHED AGGREGATE BASE COURSE (VARIED THICKNESS)
 - (H) NEW COMBINATION CURB AND GUTTER B6.18 (754) IDOT STANDARD 606001-08 /
REMOVE COMBINATION CURB AND GUTTER (754)
 - (I) EXISTING 4" AGGREGATE BASE TO BE REMOVED AND REPLACED
(INCIDENTAL TO 754)

NOTES

- THE CONTRACTOR SHALL PLACE NEW CRUSHED AGGREGATE FILL AS NEEDED, REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE TO GRADE TO 4" SECTION FOR PROPOSED BITUMINOUS PAVEMENT STRUCTURE PER SPECIFICATION 209650, AGGREGATE BASE SEPARATION.
- FOR ADDITIVE ALTERNATE NO. 1, THE CONTRACTOR SHALL PLACE NEW CRUSHED AGGREGATE FILL AS NEEDED, REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE TO GRADE TO 8" SECTION FOR PROPOSED PCC PAVEMENT STRUCTURE PER SPECIFICATION 209650, AGGREGATE BASE SEPARATION.



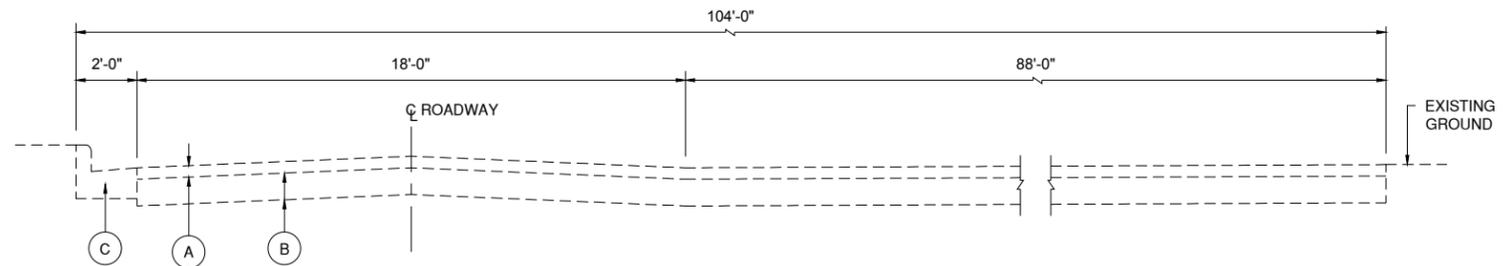
EDGE DETAIL
NOT TO SCALE



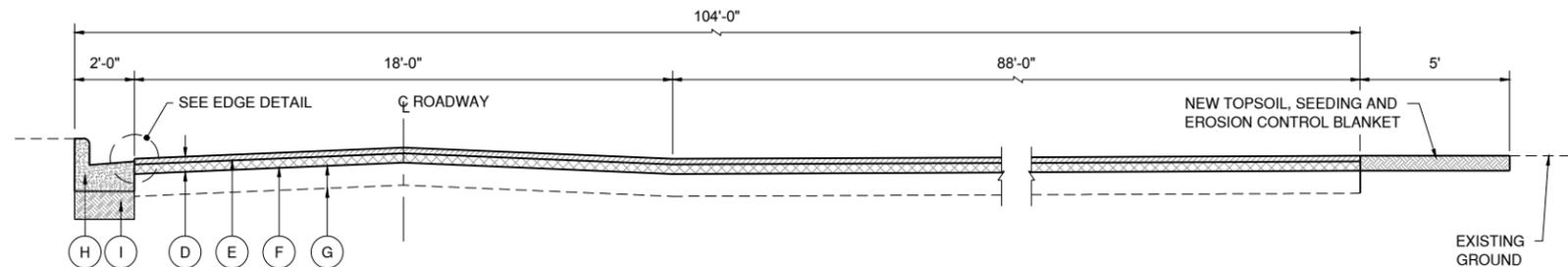
STABILIZATION TYPICAL SECTION
NOT TO SCALE

NOTES

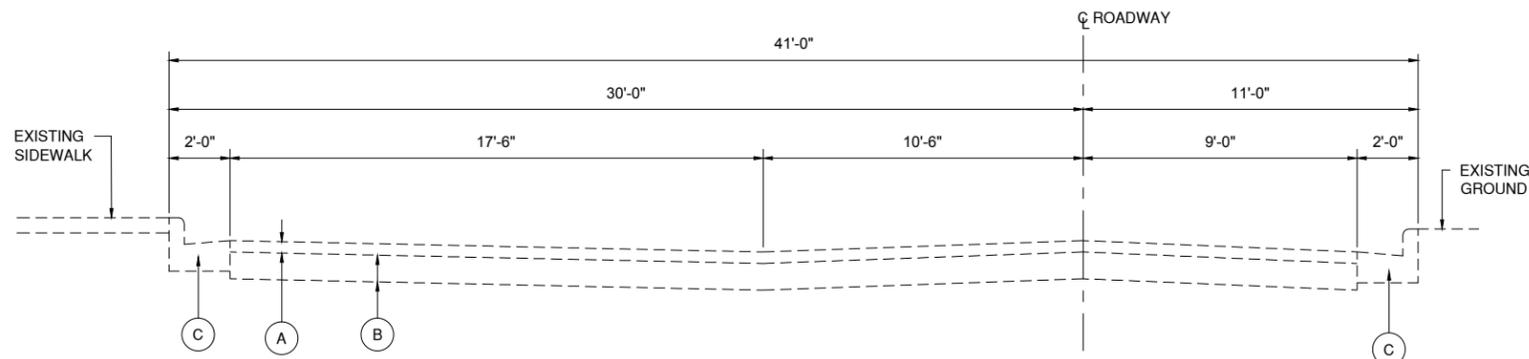
- DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- LOCATIONS FOR FURNISHING AND PLACING NEW AGGREGATE BASE AND POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION.



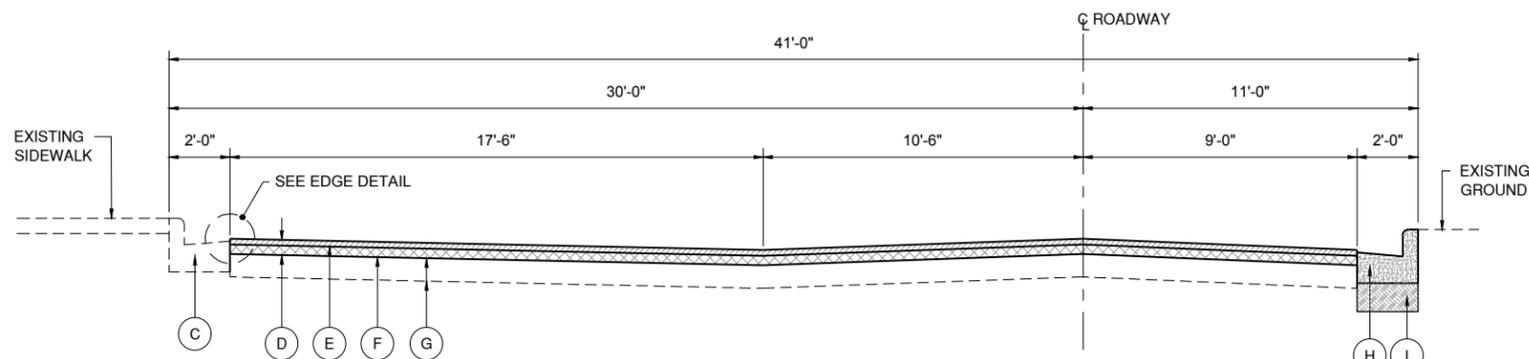
EXISTING TYPICAL SECTION A-A
NOT TO SCALE



NEW TYPICAL SECTION A-A
NOT TO SCALE



EXISTING TYPICAL SECTION B-B
NOT TO SCALE



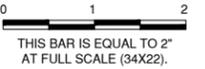
NEW TYPICAL SECTION B-B
NOT TO SCALE

IL CONTRACT: **D1034**
IL LETTING ITEM: **10A**
IL PROJECT: **C73-4978**
S.B.G. PROJECT: **N/A**

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

TYPICAL SECTIONS

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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT DIXON MUNICIPAL AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, REMOVAL AND REPLACEMENT OF CURB AND GUTTER AND SIDEWALK, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS:

- INLET PROTECTION.
- VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.
- TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.
- INSTALLATION OF NEW PAVEMENT MARKING.
- REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

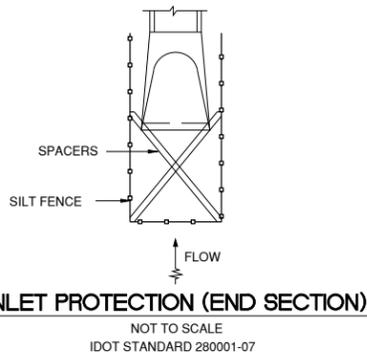
DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.



DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

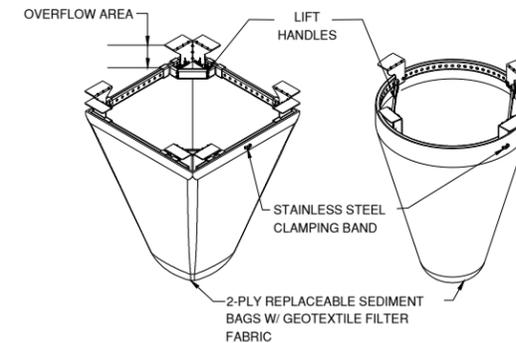
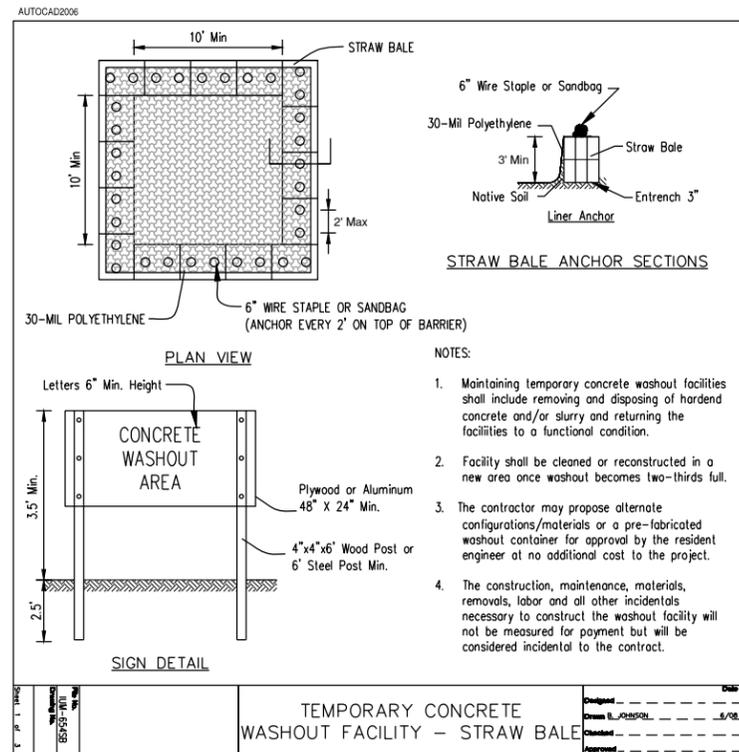
ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.



INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)
NOT TO SCALE
FOR ALL RECTANGULAR AND CIRCULAR INLETS

NOTES FOR INLET PROTECTION DETAILS

1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2022.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
6. COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL CONTRACT: **D1034**

IL LETTING ITEM: **10A**

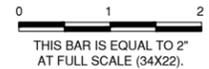
IL PROJECT: **C73-4978**

S.B.G. PROJECT: **N/A**

SURVEY BOOK # ----

REVISIONS

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS

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DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	22004581-00

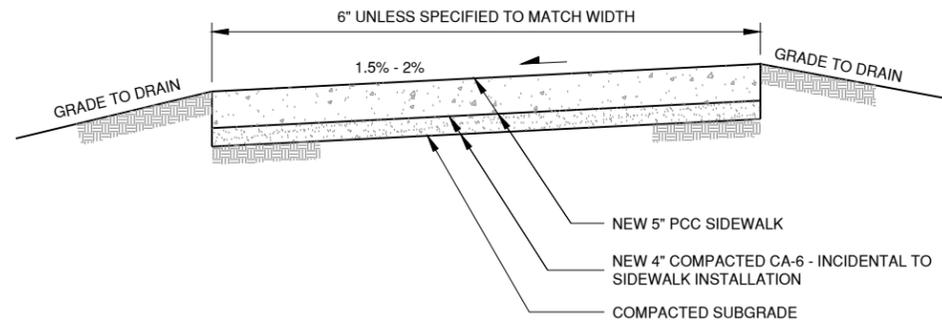
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 LAYOUT: Layout1

6MP/Busby/2/1/2023

UPDATE BY: Jim Chise
LAYOUT: Layout

DATE: Friday, March 10, 2023 11:50:17 AM
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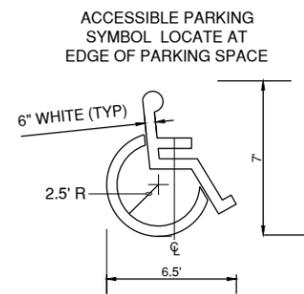


NEW SIDEWALK TYPICAL SECTION

NOT TO SCALE

NOTES

- UNCLASSIFIED EXCAVATION REQUIRED TO INSTALL SIDEWALK SECTIONS AS SHOWN ON THE GEOMETRIC LAYOUT AND MARKING PLAN SHALL BE CONSIDERED INCIDENTAL TO AR501605 5" PCC SIDEWALK.
- MAXIMUM CROSS SLOPE IS 2% MEASURED PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL.



ACCESSIBILITY PARKING SPACE MARKING DETAIL

NOT TO SCALE

SIDEWALK RAMPS

- SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED SHALL BE PROVIDED AT ALL LOCATIONS WHERE THE PROPOSED SIDEWALK MEETS THE PROPOSED DEPRESSED CURB AND GUTTER AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 424 OF THE STANDARD SPECIFICATIONS AND STANDARD 424001. THIS WORK SHALL BE PAID FOR AS PORTLAND CEMENT CONCRETE SIDEWALK.

THIS SIGN TYPICAL AT ALL ACCESSIBLE PARKING SPACES



R7-8

PENALTY SIGN WITH \$250 FINE LABEL

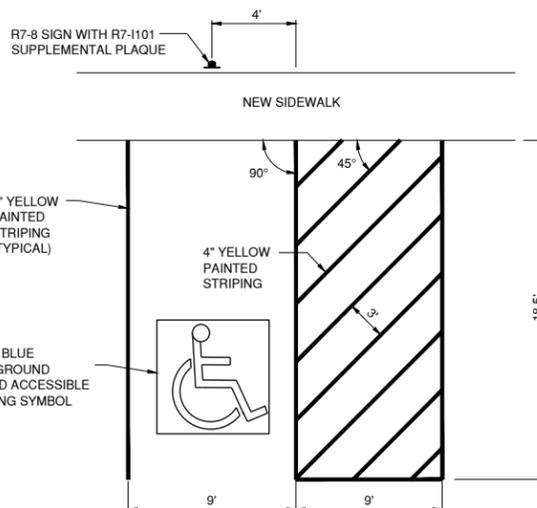


R7-1101

5'-0" MIN. FROM GROUND ELEVATION
TELESCOPING STEEL SIGN SUPPORT

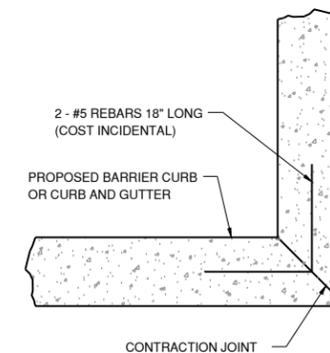
ACCESSIBLE PARKING SIGN

NOT TO SCALE



HANDICAP PARKING STALL STRIPING DETAIL

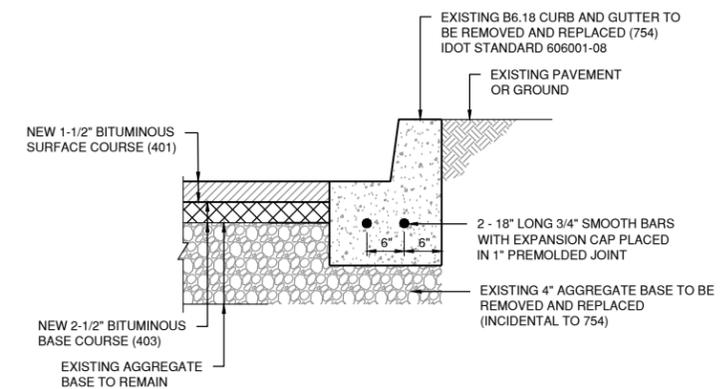
NOT TO SCALE



CORNER REINFORCEMENT DETAIL

NOT TO SCALE

REINFORCEMENTS SHALL BE INSTALLED AT ALL CORNERS.

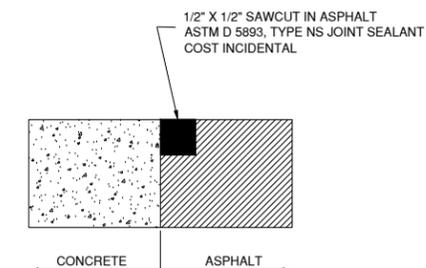


COMBINATION CURB AND GUTTER DETAIL

NOT TO SCALE

BARRIER CURB, DEPRESSED CURB AND CURB AND GUTTER NOTES

- CONTRACTION JOINTS SHALL BE PLACED AT TEN (10) FOOT MINIMUM ON CENTERS AND SHALL BE SAW CUT TO MINIMUM DEPTH OF TWO (2) INCHES FROM FRONT TO BACK AS SOON AS THE CONCRETE IS SUFFICIENTLY CURED TO ALLOW CUTTING. AS A MAXIMUM, THE CURB SHALL BE SAWED WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP1 OR EQUIVALENT.
- EXPANSION JOINTS SHALL BE CONSTRUCTED AT A 50' MAXIMUM SPACING.
- ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FUGITIVE DYE, AS PER TYPE II MEMBRANE CURING.
- CURB AND GUTTER SHALL HAVE A LIGHT BROOM FINISH.
- EXPANSION JOINTS SHALL ALSO BE PLACED AT ALL POINTS OF CURVATURE, AT 5' EACH SIDE OF ALL DRAINAGE STRUCTURES, AT THE END OF THE DAYS POUR, OR AS DIRECTED BY THE ENGINEER.
- TWO (2) NO. 5 REBARS (TEN) 10' LONG SHALL BE PLACED ON EITHER SIDE OF ALL PROPOSED UTILITY TRENCHES.
- THREE (3) NO. 5 REBARS SPACED 5' APART SHALL BE PLACED THE LENGTH OF ALL DEPRESSED CURB.



JOINT SEALING AT CONCRETE ASPHALT INTERFACE

N.T.S.

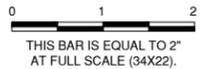
NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION

IL CONTRACT: **D1034**
IL LETTING ITEM: **10A**
IL PROJECT: **C73-4978**
S.B.G. PROJECT: **N/A**

SURVEY BOOK # ----

REVISIONS

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

MISCELLANEOUS DETAILS

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	22004581-00

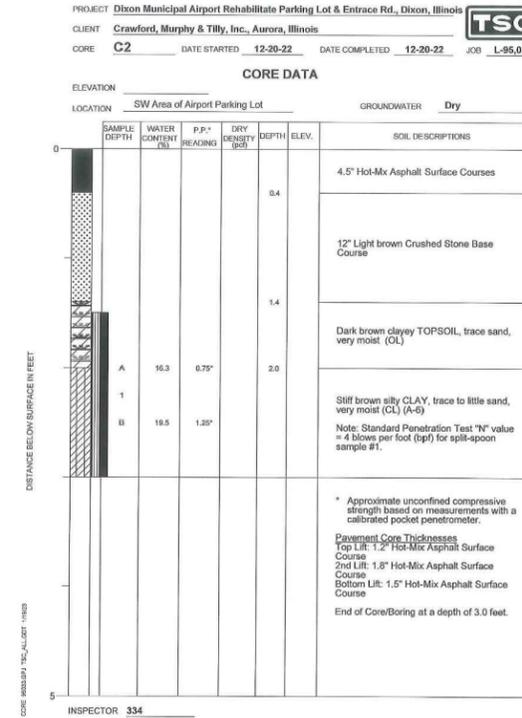
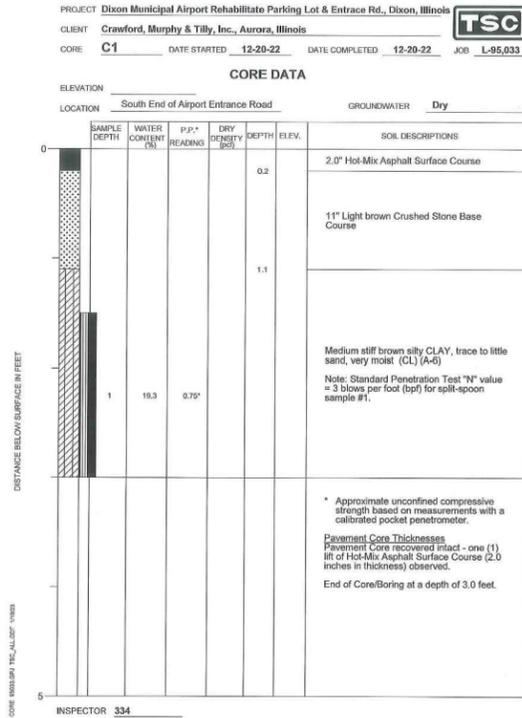
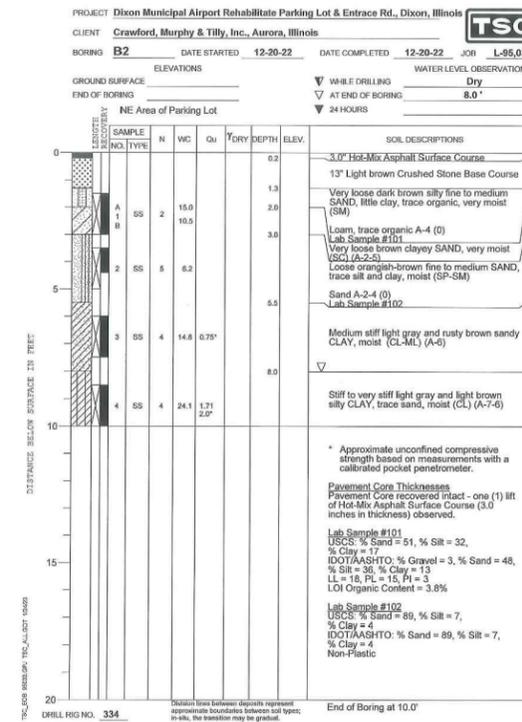
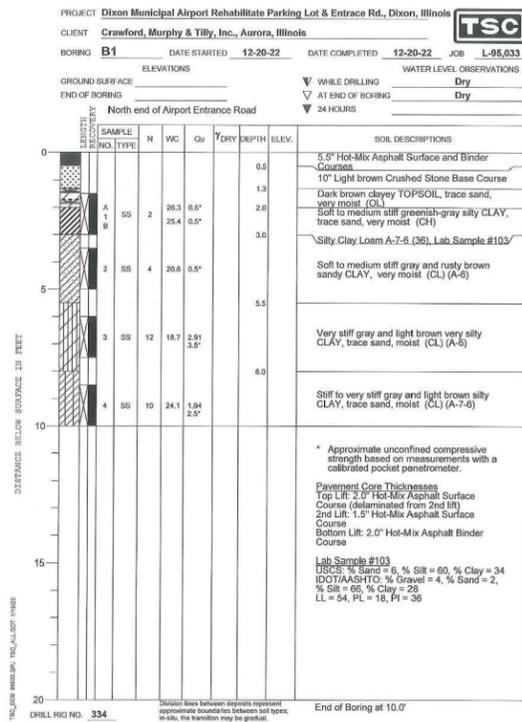
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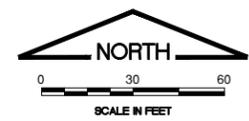
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UPDATE BY: Jim Chise
LAYOUT: Layout

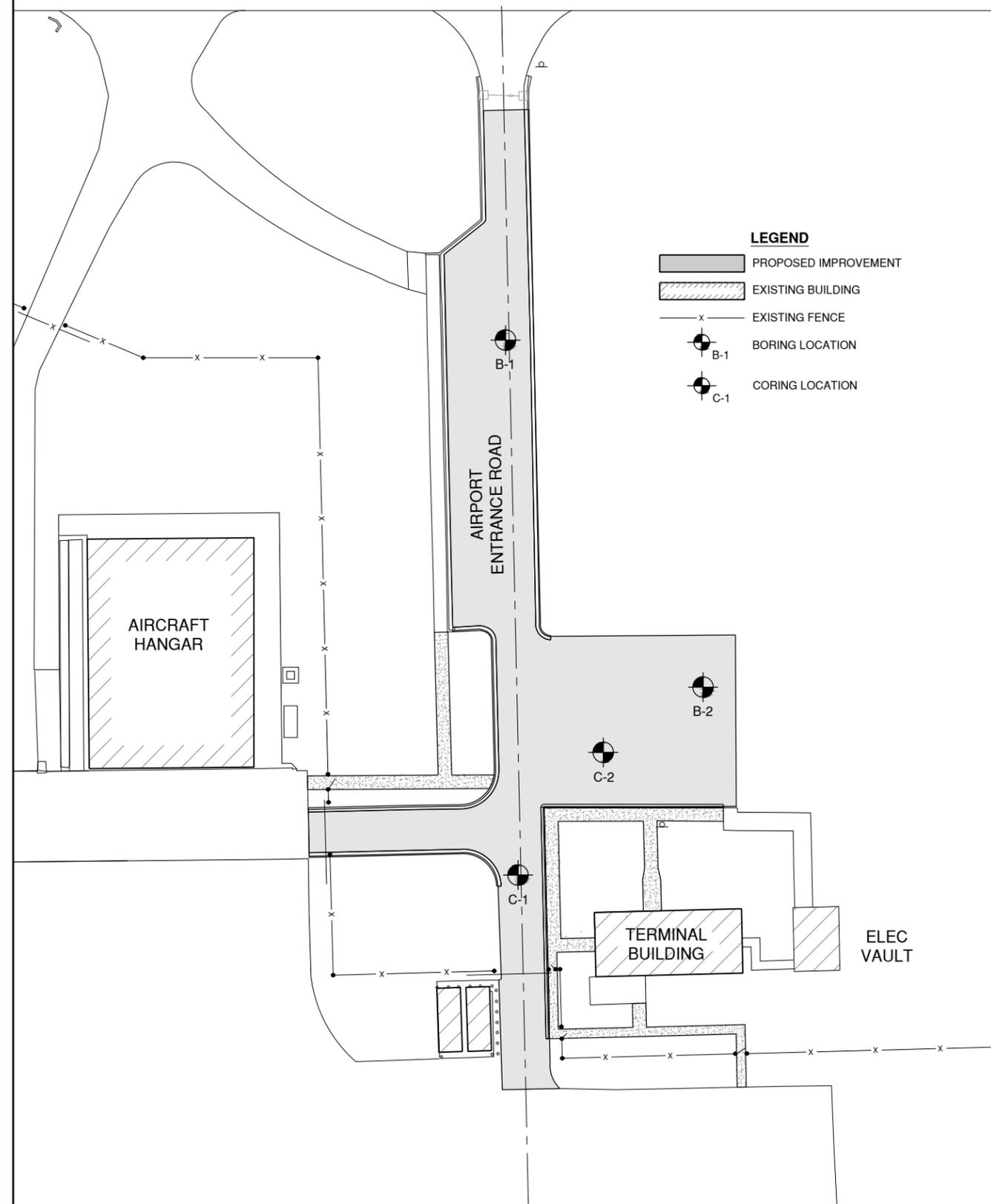
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FRANKLIN GROVE ROAD/
ROUTE 38

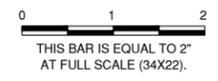


- LEGEND**
- PROPOSED IMPROVEMENT
 - EXISTING BUILDING
 - EXISTING FENCE
 - BORING LOCATION
 - CORING LOCATION

IL CONTRACT: **D1034**
IL LETTING ITEM: **10A**
IL PROJECT: **C73-4978**
S.B.G. PROJECT: **N/A**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS**

REHABILITATE AIRPORT ACCESS ROAD AND AUTO PARKING

GEOTECHNICAL ENGINEERING INFORMATION

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DRAWN BY: JRO
CHECKED BY: DKP
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DATE: 03/03/2023
JOB No: 22004581-00

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SHEET 14 OF 14 SHEETS