

FOR INDEX OF SHEETS AND LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DUPAGE	30	1
		ILLINOIS	CONTRACT NO. 61F55	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
**PLANS FOR PROPOSED  
 FEDERAL AID HIGHWAY**  
 MUN ROUTE 5041 (FOREST AVENUE)  
 JOLIET STREET TO IL ROUTE 59 (NELTNOR BOULEVARD)  
 RESURFACING, SIDEWALKS AND LIGHTING  
 SECTION: 18-00081-00-RS  
 PROJECT: XZ9U(998)  
 CITY OF WEST CHICAGO  
 DUPAGE COUNTY  
 JOB NO: C-91-218-18



LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

APPROVED: DECEMBER 13 2018  
 CITY OF WEST CHICAGO, DIRECTOR OF PUBLIC WORKS

PASSED: 3/14/2019  
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: MARCH 14 2019  
 ANTHONY J. WOODS, REGISTERED ENGINEER

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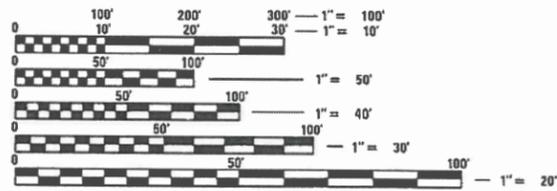
PROFESSIONAL ENGINEER'S CERTIFICATION  
 I HEREBY CERTIFY THAT THIS SUBMISSION WAS PREPARED UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.  
 DATED THIS 14th DAY OF December, 2018

KEVIN VANDEWOESTYNE  
 ILLINOIS REG. PROF. ENGINEER NO. 062-61311 EXPIRATION DATE 11-30-2019

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAMBURG, IL

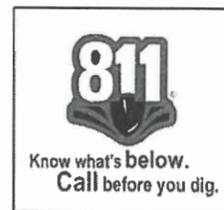
**FOREST AVENUE**

DESIGN DESIGNATION: MINOR COLLECTOR  
 SPEED LIMIT = 20 MPH POSTED  
 TRAFFIC = 1,500 ADT (2016)



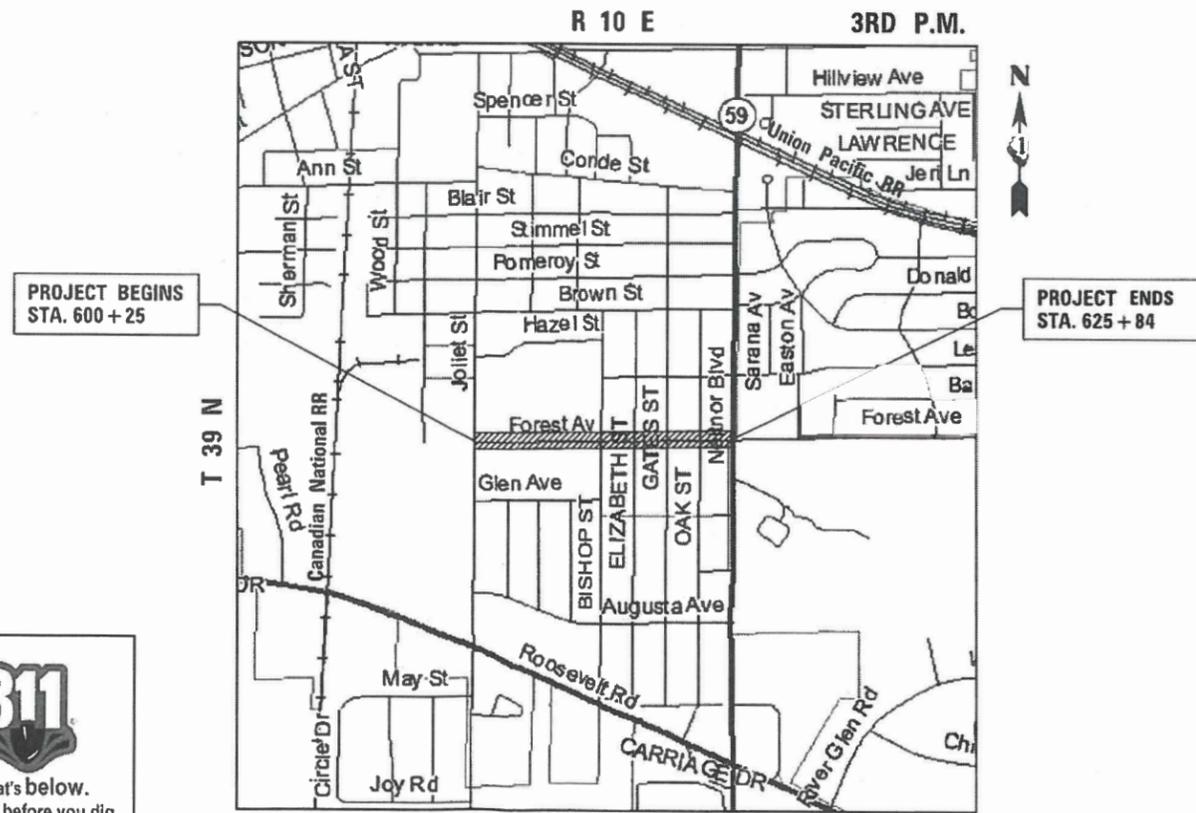
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811



PROJECT MANAGER: KEVIN VANDEWOESTYNE

CONTRACT NO. 61F55



MILTON TOWNSHIP  
**LOCATION MAP**  
 N.T.S.

FOREST AVENUE GROSS LENGTH = 2559 FT. = 0.48 MILES  
 FOREST AVENUE NET LENGTH = 2559 FT. = 0.48 MILES



**INDEX OF DRAWINGS**

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- 14-16 LIGHTING PLANS
- 17-19 SIDEWALK PLANS
- 20 CONSTRUCTION DETAILS
- 21-27 IDOT DISTRICT ONE DETAILS
- 28-30 CROSS SECTIONS

**IDOT DISTRICT ONE DETAILS:**

- BD-08 DETAILS FOR FRAME AND LIDS ADJUSTMENTS WITH MILLING
- BD-32 BUTT JOINT AND HMA TAPER DETAILS
- BE-300 LIGHT POLE FOUNDATION 30' TO 35' M.H. 11-1/2" BOLT CIRCLE
- BE-702 MISC. ELECTRICAL DETAILS SHEET A
- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-22 ARTERIAL ROAD INFORMATION SIGN

**LIST OF STATE STANDARDS:**

- 000001-07 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 424001-11 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
- 442201-03 CLASS C AND D PATCHES
- 630001-12 STEEL PLATE BEAM GUARDRAIL
- 701006-05 OFF RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701301-04 LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W MOVING OPERATIONS- DAY ONLY
- 701501-06 URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
- 701801-06 SIDEWALK CORNER OF CROSSWALK CLOSURE
- 701901-08 TRAFFIC CONTROL DEVICES

GENERAL NOTES:

1. ALL IMPROVEMENTS SHALL BE IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, LATEST EDITION. ANY REFERENCE TO STANDARDS IN THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE LATEST STANDARDS OF IDOT.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER.
4. THE CONTRACTOR SHALL COOPERATE WITH THE CITY OF WEST CHICAGO IF ANY MUNICIPAL, UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.
5. QUANTITIES FOR MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS HAVE BEEN PROVIDED. AFTER THE HMA SURFACE REMOVAL OPERATIONS ARE COMPLETE ALL OPEN CRACKS AND OPEN EXPANSION JOINTS HAVING A WIDTH OF 1/2 IN. OR MORE SHALL BE CLEANED AND FILLED ACCORDING TO ARTICLE 406. THE ACTUAL NEED FOR THESE ITEMS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. IF CRACK FILLING IS NOT REQUIRED THE QUANTITIES WILL BE DEDUCTED PER THE TERMS OF ARTICLE 104.02.
6. QUANTITIES FOR CLASS D PATCHES HAVE BEEN PROVIDED IN THE CONTRACT BASED ON FIELD OBSERVATION OF EXISTING OBSERVATIONS OF EXISTING CONDITIONS. LOCATIONS SHOWN IN THE PLANS MAY DIFFER AND WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF PAVEMENT PATCHING IS NOT REQUIRED THE QUANTITY WILL BE DEDUCTED PER THE TERMS OF ARTICLE 104.02.
7. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AT ALL TIMES DURING THE COURSE OF CONSTRUCTION AND SHALL PREVENT STORM WATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.

8. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE BUTT JOINT AND BITUMINOUS TAPER DETAILS SHEET INCLUDED IN THE PLANS (BD-32).
9. ALL PAVEMENTS, CURB AND GUTTER, SIDEWALKS, DRIVEWAYS TO BE REMOVED SHALL BE SAWCUT PRIOR TO REMOVAL TO PREVENT DAMAGE TO ITEMS TO REMAIN.
10. THE CONTRACTOR SHALL SET AND CHECK ALL CURB FORMS AND STRING LINES PRIOR TO PLACING CONCRETE TO ENSURE POSITIVE DRAINAGE ALONG THE ROADWAY. IMPROPERLY DRAINING CURB SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
11. PARKWAY RESTORATION SHALL INCLUDE REPLACEMENT OF DAMAGED AND DESTROYED LANDSCAPE IN KIND OF THE EXISTING TURF ADJACENT TO THE WORKING LIMITS AND WHERE THE CONTRACTOR'S EQUIPMENT HAS DESTROYED OR DAMAGED THE TURF. THE CONTRACTOR SHALL PREPARE THE GROUND ACCORDING TO SECTION 211 OF THE STANDARD SPECIFICATIONS. SALT TOLERANT SEED AND FERTILIZER SHALL BE PLACED ACCORDING TO SECTION 250 OF THE STANDARD SPECIFICATIONS.
12. ALL STREETS AND COMMERCIAL AND PARKING LOT ENTRANCES SHALL REMAIN OPEN DURING CONSTRUCTION.
13. PAVEMENT PATCHING SHALL BE SCHEDULED IMMEDIATELY FOLLOWING PAVEMENT REMOVAL TO REDUCE DEGRADATION OF THE EXISTING BASE.
14. THE MAXIMUM ALLOWABLE LANE DROP DIFFERENTIAL WILL BE 1-1/2"
15. THE CONTRACTOR SHALL PLACE FINAL THERMOPLASTIC PAVEMENT MARKINGS A MAXIMUM OF THREE (3) DAYS AFTER PLACEMENT OF THE FINAL BITUMINOUS SURFACE COURSE.
16. PRIOR TO HMA RESURFACE REMOVAL, ALL OPEN DRAINAGE UTILITY STRUCTURES SHALL BE PROTECTED WITH FILTER FABRIC TO PREVENT ROADWAY DEBRIS FROM ENTERING UNDERGROUND UTILITIES. IF THE ENGINEER FINDS EVIDENCE OF CONSTRUCTION DEBRIS IN THE UTILITY STRUCTURES AFTER THE HMA SURFACE REMOVAL AND / OR AFTER THE BINDER COURSE AND SURFACE COURSE ARE COMPLETED, THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR REMOVING DEBRIS.
17. WORK SHALL BE CAREFULLY PLANNED BY THE CONTRACTOR TO REDUCE DISRUPTION TO RESIDENTS, THE BUSINESSES AND THE PUBLIC SEEKING TO ACCESS THE BUSINESSES. AT LEAST ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES.
18. DURING AND AFTER CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL ON CITY ROADWAYS AS A RESULT OF CONTRACTOR OPERATIONS, INCLUDING BUT NOT LIMITED TO HMA SURFACE REMOVAL, BINDER COURSE AND SURFACE COURSE INSTALLATION, SHALL BE REMOVED AND DEPOSITED OFF SITE BY THE CLOSE OF EACH BUSINESS DAY. THIS APPLIES TO EXCESSIVE PRIMER LEFT ON CITY ROADWAYS.
19. THE THICKNESS OF HMA MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESSES PERMITTED.
20. THE CONTRACTOR SHALL FURNISH WHITE, PINK, OR PURPLE MARKING PAINT IN AEROSOL CANS, FOR USE BY THE ENGINEER. THE CONTRACTOR AND SUBCONTRACTORS SHALL ONLY USE THESE SAME COLORS FOR THEIR OWN MARKINGS, THEREFORE, NOT USING J.U.L.I.E. UTILITY COLORS.
21. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, AT LEAST 24 HOURS IN ADVANCE IF ANY RESIDENTS OR BUSINESSES WILL HAVE NO OR LIMITED DRIVEWAY ACCESS DUE TO WORK PERFORMED BY THE CONTRACTOR.
22. CONTRACTOR IS TO PLAN HIS WORK SO THAT AT THE END OF EACH WORK DAY, THERE WILL BE NO OPEN HOLES IN THE PAVEMENT OR SIDEWALK AND THAT ALL BARRICADES WILL BE REMOVED FROM THE ROADWAY DURING NONWORKING HOURS, EXCEPT WHERE REQUIRED FOR PUBLIC SAFETY OR CURING OF CONCRETE.
23. ALL PROPOSED CONCRETE CURB AND GUTTER TYPES SHALL BE CONTINUOUSLY REINFORCED WITH TWO (2) NO. 4 REINFORCEMENT BARS.
24. AFTER SIDEWALK AND CURB AND GUTTER REPLACEMENT AND RESTORATION ACTIVITIES HAVE BEEN COMPLETED, THE CONTRACTOR SHALL REMOVE ALL LOOSE AND DEFECTIVE MATERIAL FROM THE SIDEWALK PAVEMENT, CURB AND GUTTER, AND PUBLIC RIGHT-OF-WAY TO THE SATISFACTION OF THE ENGINEER. THIS SHALL INCLUDE, BUT IS NOT LIMITED TO, BARRICADES, FORMS, GRAVEL, EXCESS TOP SOIL, EXCESS CONCRETE, ETC. AUXILIARY EQUIPMENT, SUCH AS BROOMS, SWEEPERS, SCRAPERS, ETC. SHALL BE PROVIDED AS NECESSARY TO PERFORM WORK.



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PLOT DATE = 12/12/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	2
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	FOREST AVE. CONSTR CODE 00005 STU 50/50	FOREST AVE. CONSTR CODE 00021 STU 50/50
Δ	*	20101200	TREE ROOT PRUNING	EACH	10	10	
	*	20101700	SUPPLEMENTAL WATERING	UNIT	20	20	
		20200100	EARTH EXCAVATION	CU YD	95	95	
	CS	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	25	25	
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	371	371	
		25200110	SODDING, SALT TOLERANT	SQ YD	371	371	
		30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SQ YD	50	50	
	CS	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	7,881	7881	
	*	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	6	6	
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	157	157	
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,471	1471	
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	981	981	
	*	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	16	16	
		44000163	HOT-MIX ASPHALT REMOVAL, 3-1/2"	SQ YD	11,675	11675	
	*	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	117	117	
	*	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	117	117	
	*	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	117	117	
	*	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	233	233	
		60250200	CATCH BASINS TO BE ADJUSTED	EACH	6	6	
		60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	8	8	
		67100100	MOBILIZATION	L SUM	1	1	
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
		70107025	CHANGEABLE MESSAGE SIGN	CAL DAY	56	56	
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,396		1396
		70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	465		465
Δ		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	342		342
Δ		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	20		20
Δ		80400100	ELECTRIC SERVICE INSTALLATION	EACH	1		1
Δ		81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2-1/2" DIA.	FOOT	556		556
Δ		81603178	UNIT DUCT, 600V, 6-1C NO. 4, 1/C NO. 6 GROUND, (XLP-TYPE USE), 2" DIA. POLYETHYLENE	FOOT	1,457		1457
Δ		83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	117		117
	CS	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1	
	CS	X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	274		274
		X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	189	189	
	CS	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	8,608		8608
	CS	X4404700	SIDEWALK REMOVAL (SPECIAL)	SQ FT	5,178		5178

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	FOREST AVE. CONSTR CODE 00005 STU 50/50	FOREST AVE. CONSTR CODE 00021 STU 50/50
*	CS	Z0013300	CONCRETE REMOVAL (SPECIAL)	SQ YD	44		44
*	CS	X6061005	CONCRETE CURB, TYPE B (SPECIAL)	FOOT	632	632	
Δ		X8250505	LIGHTING CONTROLLER, SPECIAL	EACH	1		1
*	CS	Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	44	44	
*	CS	Z0004544	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	59	59	
	CS	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	639	639	
		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	
Δ	CS	X0004618	SINGLE STREET LIGHT AND POLE	EACH	13		13

CS = CONTRACT SPECIAL PROVISION (SEE SECTION III)  
\* = NOMINAL QUANTITY PROVIDED. THE ACTUAL NEED WILL BE DETERMINED IN THE FIELD AT TIME OF CONSTRUCTION. QUANTITIES WILL BE DEDUCTED IF THE WORK IS NOT DEEMED NECESSARY.

Δ SPECIALTY ITEMS



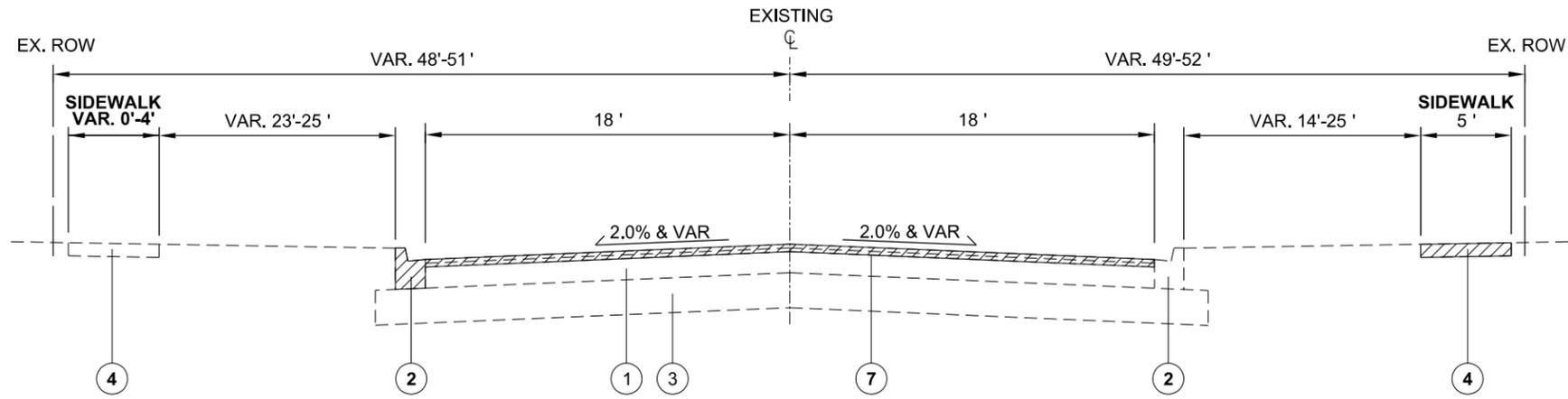
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	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

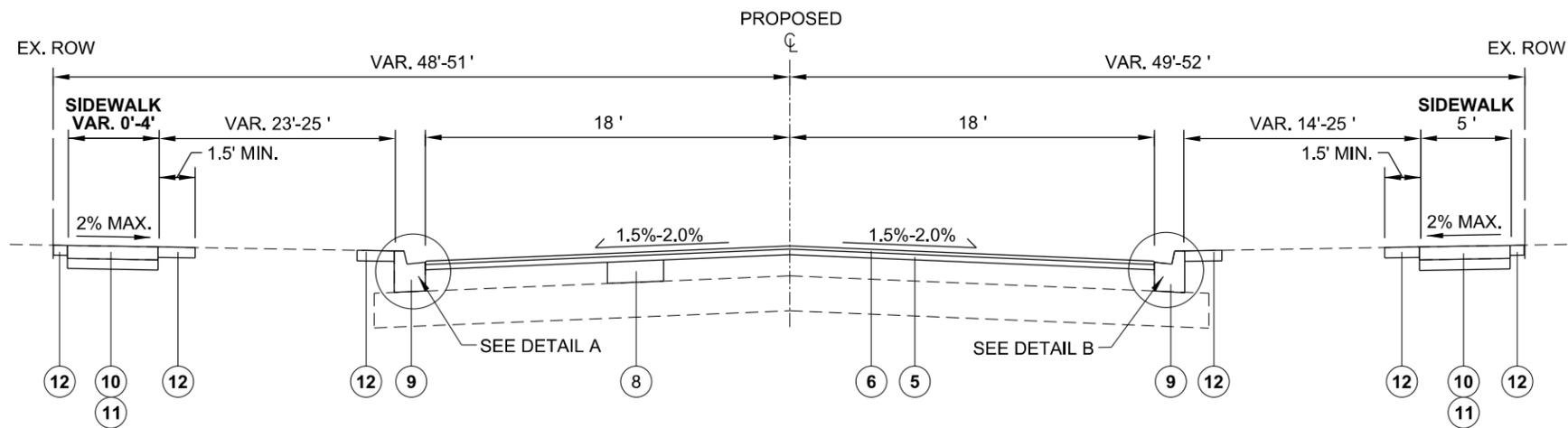
SUMMARY OF QUANTITIES

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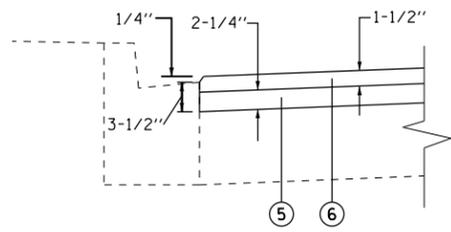
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ILLINOIS FED. AID PROJECT				



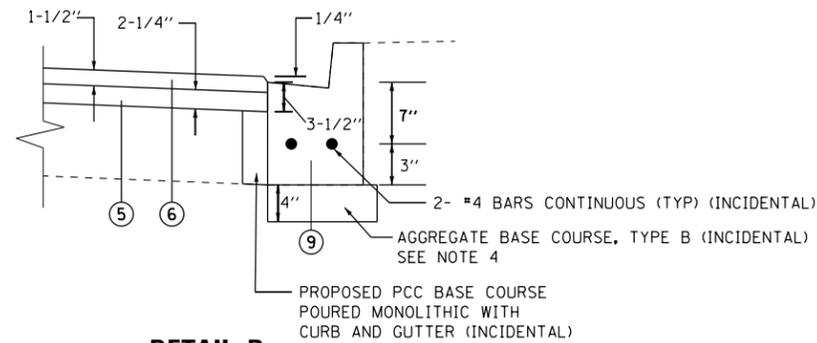
**EXISTING TYPICAL SECTION**  
 FOREST AVENUE (EAST OF JOLIET STREET (FAU 2534) TO WEST OF IL ROUTE 59)  
 STA. 600+25 TO STA. 625+84



**PROPOSED TYPICAL SECTION**  
 FOREST AVENUE (EAST OF JOLIET STREET (FAU 2534) TO WEST OF IL ROUTE 59)  
 STA. 600+25 TO STA. 625+84



**DETAIL A**



**DETAIL B**

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

**LEGEND**

- ① EX. HMA PAVEMENT (VAR. 8.5"-9.5")
- ② EX COMB. CONCRETE CURB AND GUTTER (SEE NOTE 3)
- ③ EX AGGREGATE SUBGRADE (12" TYP)
- ④ EX PCC SIDEWALK
- ⑤ PR HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N 50, 2-1/4"
- ⑥ PR HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1-1/2"
- ⑦ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3-1/2"
- ⑧ PR PAVEMENT PATCH (INTERMITTENT, SEE PLAN SHEETS FOR LOCATIONS)
- ⑨ PR COMB. CONC. CURB AND GUTTER, REMOVAL & REPLACEMENT (SEE NOTE 3 & 4)
- ⑩ PR PCC SIDEWALK (SEE NOTE 5)
- ⑪ PR. AGGREGATE BASE COURSE, TYPE B, 4" (INCLUDED IN THE COST OF PCC SIDEWALK)
- ⑫ PR. TOPSOIL FURNISH AND PLACE AND SODDING SALT TOLERANT

**GENERAL NOTES:**

1. ALL LONGITUDINAL JOINTS BETWEEN LANES MUST BE FINISHED BY THE END OF EACH WORKING DAY TO PREVENT COLD JOINT BONDING BETWEEN SUCCESSIVE DAYS OF PAVING.
2. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS AND BUSINESSES 24 HOURS IN ADVANCE OF OPERATIONS AFFECTING DRIVEWAY INGRESS AND EGRESS.
3. LOCATIONS OF INTERMITTENT CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD DURING CONSTRUCTION BY THE ENGINEER.
4. P.C.C. CURB AND GUTTER SHALL BE CONSTRUCTED ON 4" OF COMPACTED AGGREGATE BASE COURSE, TYPE B, WHERE REQUIRED BY THE ENGINEER. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE BASE COURSE, TYPE B SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB AND GUTTER.
5. EXISTING SIDEWALK LOCATIONS ARE AS FOLLOWS:  
 NORTH SIDE- STA. 600+27 TO STA. 602+56  
 STA. 603+13 TO STA. 605+33  
 STA. 613+05 TO STA. 624+83  
  
 SOUTH SIDE- STA. 600+27 TO STA. 625+84  
  
 PROPOSED SIDEWALK LOCATIONS ARE AS FOLLOWS:  
 NORTH SIDE- STA. 602+56 TO STA. 603+13  
 STA. 605+33 TO STA. 613+05
6. CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N 50 (IL 9.5 mm)	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N 50	4% @ 50 Gyr.
<b>CLASS D PATCHES</b>	
HOT-MIX ASPHALT BINDER, IL-19.0	4% @ 50 Gyr.
<b>HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N 50 (IL 9.5 mm)	4% @ 50 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 12 LBS/ SQ YD/ IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL "PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIALS PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4. THE CONTRACTOR SHALL MILL BEFORE PATCHING.

**LEGEND:**

-  PR. HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINTS (4.5' WIDE)
-  PR. HOT-MIX ASPHALT SURFACE REMOVAL (3.5")  
PR. HOT-MIX BINDER COURSE, N50, IL-19.0 (2.25")  
PR. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (1.5")
-  PR. CLASS D PATCH \*
-  P.C.C. SIDEWALK REMOVAL  
P.C.C. SIDEWALK 5' TO 6' INCH
-  COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT
-  SODDING, SALT TOLERANT \*\*
-  FRAME AND LIDS TO BE ADJUSTED
-  CATCH BASINS TO BE ADJUSTED
-  SANITARY MANHOLE TO BE ADJUSTED

**THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)**

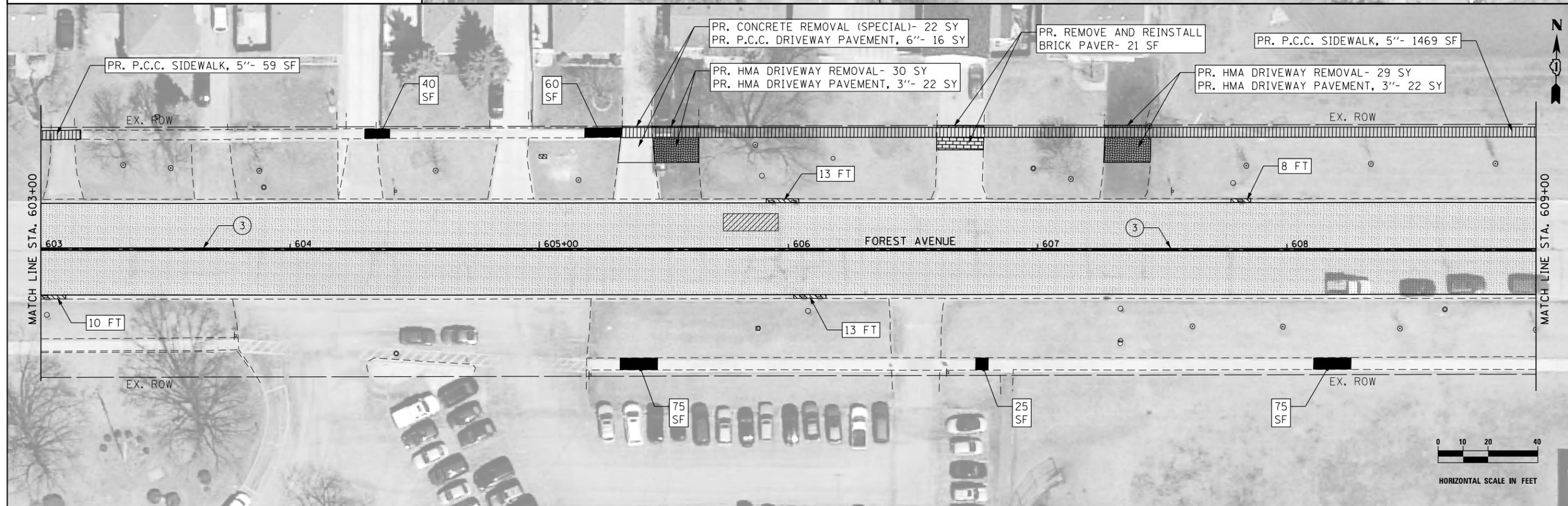
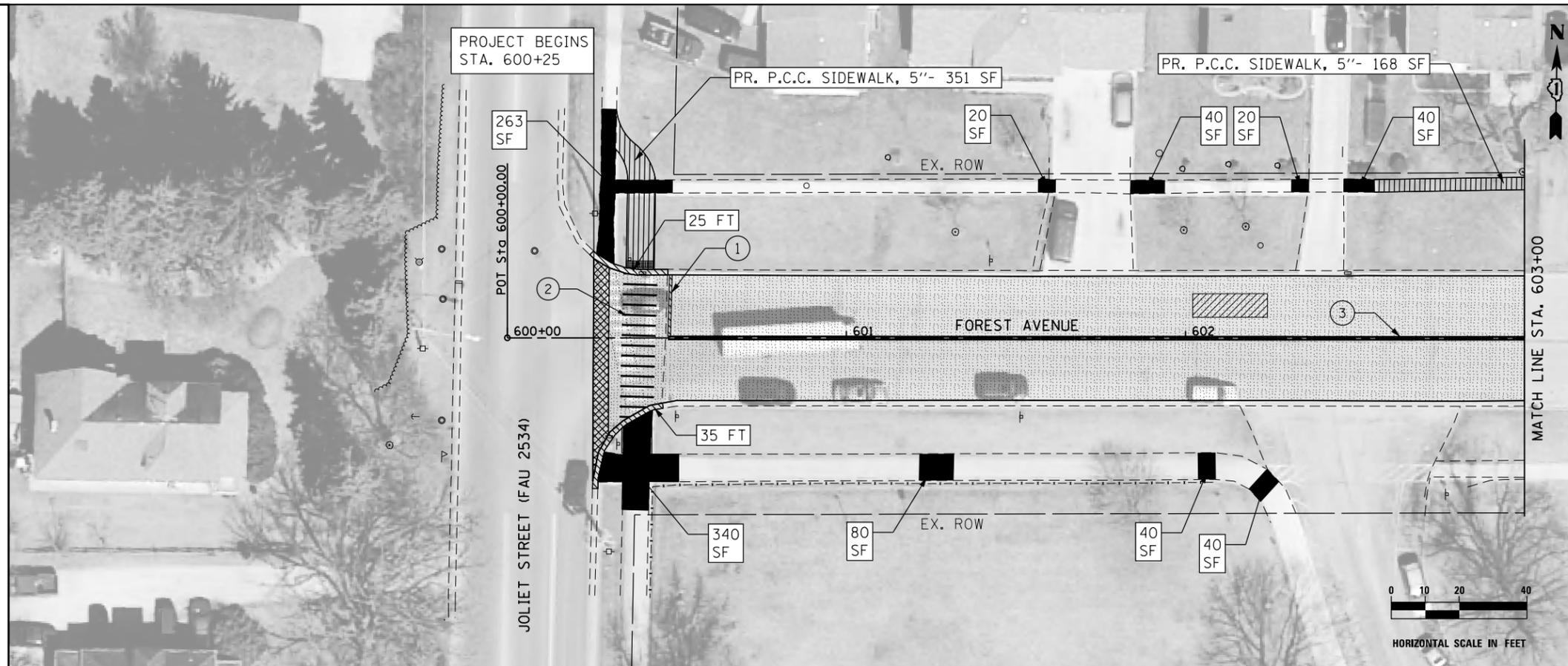
- ① STOP LINE - 1 @ 24", WHITE
- ② SCHOOL CROSSWALK LINE - 12", WHITE (TYP)
- ③ DOUBLE CENTERLINE - 2 @ 4", YELLOW

**XX SF** LIMITS AND QUANTITY OF PR PCC SIDEWALK, 5" REMOVAL AND REPLACEMENT

**XX FT** LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT- XX FT

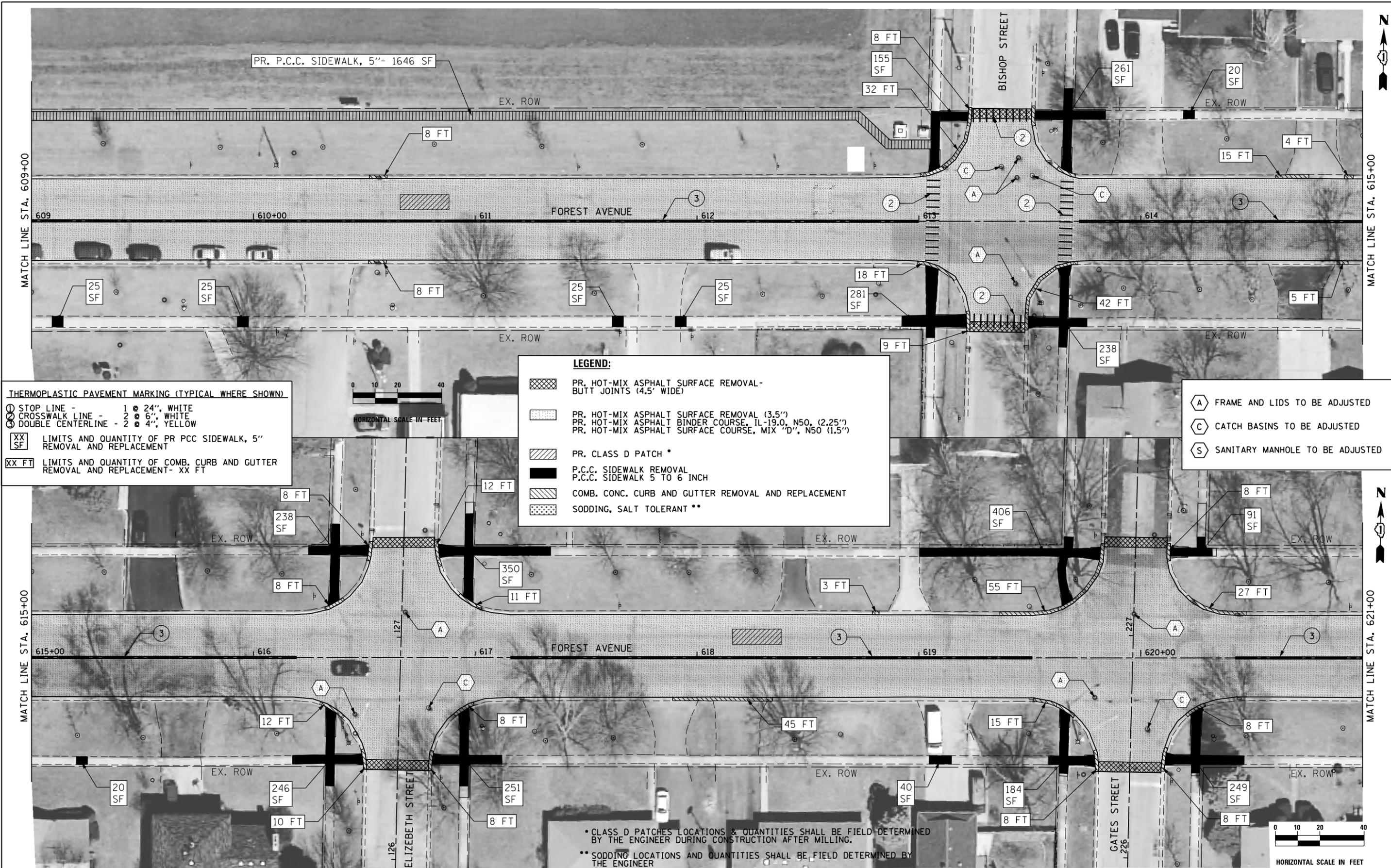
\* CLASS D PATCHES LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.

\*\* SODDING LOCATIONS AND QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
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5041	18-00081-00-RS	DuPAGE	30	5
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**thomas** thomas engineering group, llc  
 762 shoreline drive  
 suite 200  
 aurora, illinois 60504  
 phone: 855-533-1700

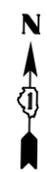
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DRAWN -	REVISOR -	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/12/2018	DATE -	REVISOR -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT			CONTRACT: 61F55	



**LEGEND:**

- PR. HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINTS (4.5' WIDE)
- PR. HOT-MIX ASPHALT SURFACE REMOVAL (3.5")  
PR. HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, (2.25")  
PR. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (1.5")
- PR. CLASS D PATCH \*
- P.C.C. SIDEWALK REMOVAL  
P.C.C. SIDEWALK 5 TO 6 INCH
- COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT
- SODDING, SALT TOLERANT \*\*
- FRAME AND LIDS TO BE ADJUSTED
- CATCH BASINS TO BE ADJUSTED
- SANITARY MANHOLE TO BE ADJUSTED

**THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)**

- ① STOP LINE - 1 @ 24", WHITE
- ② SCHOOL CROSSWALK LINE - 12", WHITE (TYP)
- ③ DOUBLE CENTERLINE - 2 @ 4", YELLOW

LIMITS AND QUANTITY OF PR PCC SIDEWALK, 5" REMOVAL AND REPLACEMENT

LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT - XX FT

\* CLASS D PATCHES LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.

\*\* SODDING LOCATIONS AND QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER



HORIZONTAL SCALE IN FEET



USER NAME = DonN	DESIGNED -	REVISED -
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PLOT DATE = 12/12/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	7
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ILLINOIS FED. AID PROJECT				

**LIGHTING GENERAL NOTES:**

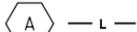
1. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS, INCLUDING ALL ABOVE AND BELOW GRADE APPURTENANCES. THE CONTRACTOR SHALL REPAIR ALL DAMAGE TO THE SATISFACTION OF THE OWNERS REPRESENTATIVE.
2. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE FOLLOWING SPECIFICATIONS, WHICH ARE HEREBY MADE A PART HEREOF:
  - A. "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS", AS PREPARED BY IDOT, LATEST EDITION.
  - B. "THE NATIONAL ELECTRICAL CODE", LATEST EDITION.
  - C. CITY OF WEST CHICAGO "CODE OF ORDINANCES".
3. THE CONTRACT DRAWINGS AND SPECIFICATIONS ARE NOT INTENDED TO SHOW EVERY AND ALL DETAILS OF WORK TO BE PERFORMED OR EQUIPMENT TO BE SUPPLIED. THE INTENT OF THE CONTRACT DRAWINGS AND SPECIFICATIONS IS TO ILLUSTRATE THE CONCEPTUAL DESIGN AND LAYOUT. THE CONTRACTOR SHALL BE KNOWLEDGEABLE AND REGULARLY ENGAGE IN THE TYPE OF WORK DESCRIBED BY THESE CONTRACT DRAWINGS AND SPECIFICATIONS AND SHALL BE RESPONSIBLE FOR UNDERSTANDING THEIR INTENT. ANY WORK TO BE PERFORMED OR ITEM OF EQUIPMENT TO BE SUPPLIED WHICH IS NOT SPECIFICALLY CALLED FOR BY THESE CONTRACT DRAWINGS AND SPECIFICATIONS, BUT WHICH IS NECESSARY TO PROVIDE A COMPLETE AND SUCCESSFUL WORKING SYSTEM SHALL BE INCLUDED IN THE CONTRACTORS SCOPE OF WORK AT NO ADDITIONAL COST TO THE OWNER.
4. THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OR EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT NO ADDITIONAL COST TO THE OWNER. THE COST ASSOCIATED FOR THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
5. BEFORE INSTALLING LIGHT STANDARDS NEAR OVERHEAD OR UNDERGROUND UTILITIES THE CONTRACTOR SHALL CALL ALL AFFECTED UTILITY COMPANIES FOR LOCATION APPROVAL.
6. CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND UTILITIES BEFORE TRENCHING OR AUGERING.
7. CONTRACTOR SHALL SUBMIT SPECIFICATIONS, DRAWINGS AND CATALOG CUTS FOR ALL MATERIAL TO THE RESIDENT ENGINEER FOR REVIEW BEFORE ORDERING ANY MATERIAL FOR THE PROJECT.
8. LOCATION OF SPARE RACEWAYS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR BEFORE EXISTING FOUNDATIONS ARE EXCAVATED.
9. LUMINAIRES SHALL HAVE A TIGHT FIT ON LIGHT POLES TO THE ENGINEER'S SATISFACTION. THIS WORK SHALL INCLUDE FIELD ADJUSTING THE LUMINAIRE, WHICH WILL BE INCLUDED IN THE COST OF THE LUMINAIRE PAY ITEM.
10. ALL POLE HANDHOLE DOORS SHALL FACE AWAY FROM THE DIRECTION OF TRAFFIC.
11. THE CONTRACTOR SHALL COORDINATE PLACEMENT OF ALL UNDERGROUND DUCT AND CONDUITS PRIOR TO CONSTRUCTION OF PROPOSED PAVEMENT, DRIVEWAYS AND SIDEWALKS. IF CONDUIT OR UNIT DUCT IS PUSHED DUE TO SEQUENCING, THERE WILL BE NO ADDITIONAL COMPENSATION.
12. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT STANDARD IS ERECTED.
13. MULTI-UNIT DUCT RUNS SHALL BE INSTALLED IN A COMMON TRENCH AND BACKFILLED. TRENCH AND BACKFILL WILL NOT BE PAID FOR EACH UNIT DUCT OR CONDUIT, BUT WILL BE PAID FOR THE LENGTH OF THE COMMON TRENCH ONLY.
14. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES. THE RESIDENT ENGINEER SHALL EXAMINE THE PROPOSED LOCATIONS AND SHALL CONFIRM THEM BEFORE THE LIGHT POLE FOUNDATIONS ARE INSTALLED.
15. ALL LIGHT POLE FOUNDATIONS SHALL BE INSPECTED BY THE RESIDENT ENGINEER PRIOR TO POURING CONCRETE.
16. NO LIGHT POLE SHALL BE ERECTED UNTIL THE RESPECTIVE FOUNDATION HAS CURED, AND IS APPROVED BY THE RESIDENT ENGINEER.

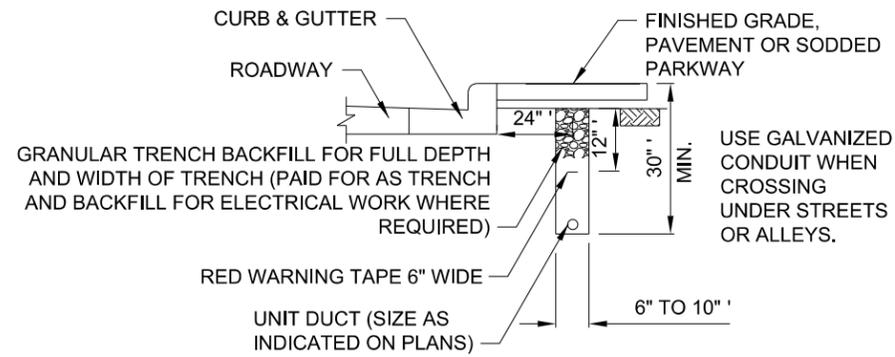
**LIGHTING CONTROLLER NOTES:**

1. ONCE INSTALLATION IS COMPLETE IT IS THE RESPONSIBILITY OF THE ELECTRICAL CONTRACTOR TO CALL THE BUILDING MAINTENANCE DEPARTMENT AT (630)293-2252 AND SCHEDULE AN INSPECTION FOR THE ELECTRICAL PANEL. CONTRACTOR WILL RECEIVE A DATE OF INSPECTION.
2. THE CONTRACTOR SHALL CHECK THE PANEL OR CALL THE BUILDING MAINTENANCE DEPARTMENT AT (630) 293-2252 THE DAY AFTER THE INSPECTION FOR THE RESULTS OF THE INSOECTION. A YELLOW TAG MEANS THAT IT HAS PASSED INSPECTION. A RED TAG MEANS THAT IT HAS FAILED AND THERE IS AN INPECTION SHEET IN THE ELECTRICAL METER PANEL STATING WHAT NEEDS TO BE CORRECTED. IF THE INSTALLATION IS YELLOW TAGGED THE RESIDENT ENGINEER WILL CALL COMED AND REQUEST ELECTRICAL SERVICE BE CONNECTED AND METER INSTALLED.
3. IF THE INSTALLATION HAS BEEN RE TAGGED, THE CONTRACTOR SHALL FIX PROBLEMS THE BUILDING MAINTENANCE SUPERVISOR HAS NOTED.
4. THE ELECTRICAL CONTRACTOR SHALL CALL THE BUILDING MAINTENEACE DEPARTMENT FOR A SECOND INSPECTION. IF CABINET PASSES THIS TIME IT WILL BE YELLOW TAGGED AND THE RESIDENT ENGINEER WILL CALL COMEND AND REQUEST ELECTRICAL SERVICE TO BE CONNECTED AND METER INSTALLED.
5. THE STREET LIGHTING SYSTEM WILL THE BE TEST BURNED FOR 7 CONSECUTIVE DAYS AND NIGHTS.

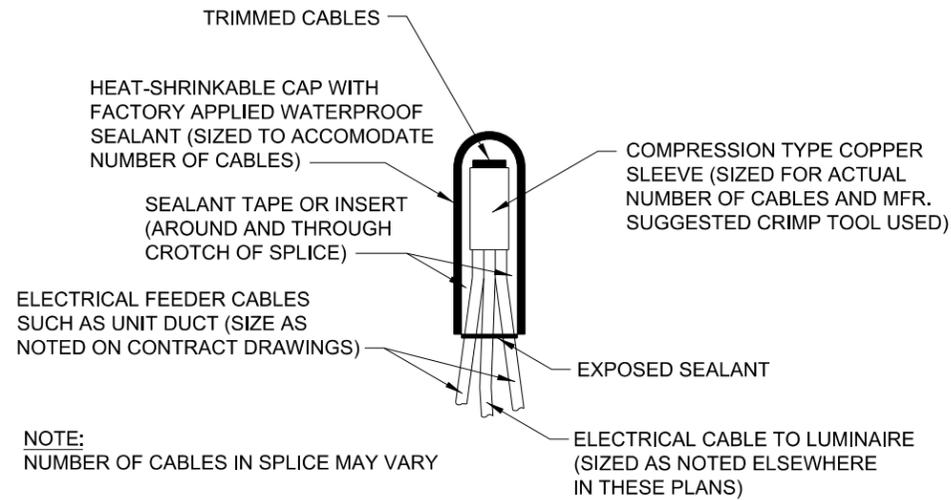
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2-1/2" DIA.	FOOT	556
81603178	UNIT DUCT, 600V, 6-1C NO. 4, 1/C NO. 6 GROUND, (XLP-TYPE USE), 2" DIA. POLYETHYLENE	FOOT	1,457
83600210	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	117
X8250505	LIGHTING CONTROLLER, SPECIAL	EACH	1
XX004618	SINGLE STREET LIGHT AND POLE	EACH	13

**LEGEND:**

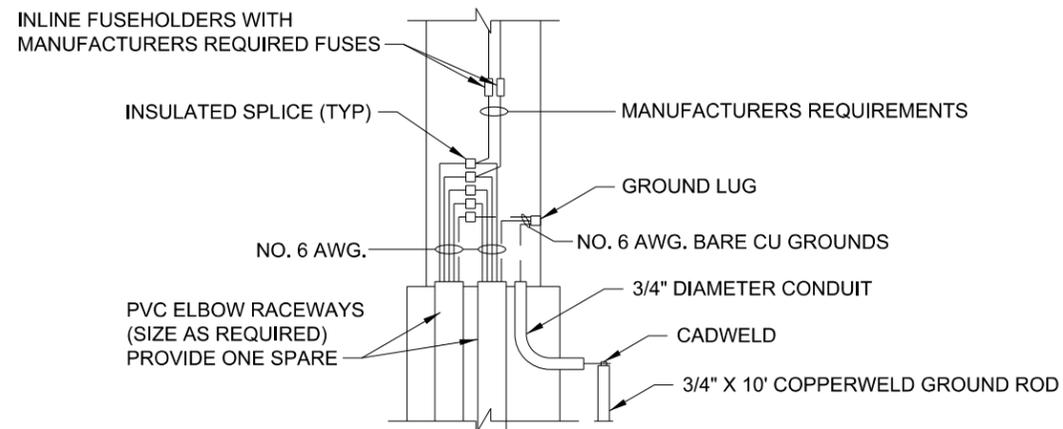
-  SINGLE STREET LIGHT AND POLE WITH 10' MAST ARM (28' POLE HEIGHT)
-  PROPOSED LIGHTING CONTROLLER
-  UNIT DUCT, 600V, 4-1C NO. 6, 1/C NO. 6, GROUND (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE
-  CONDUIT PUSHED OR TRENCHED 2-1/2" DIA. RIGID GALVANIZED STEEL CONDUIT



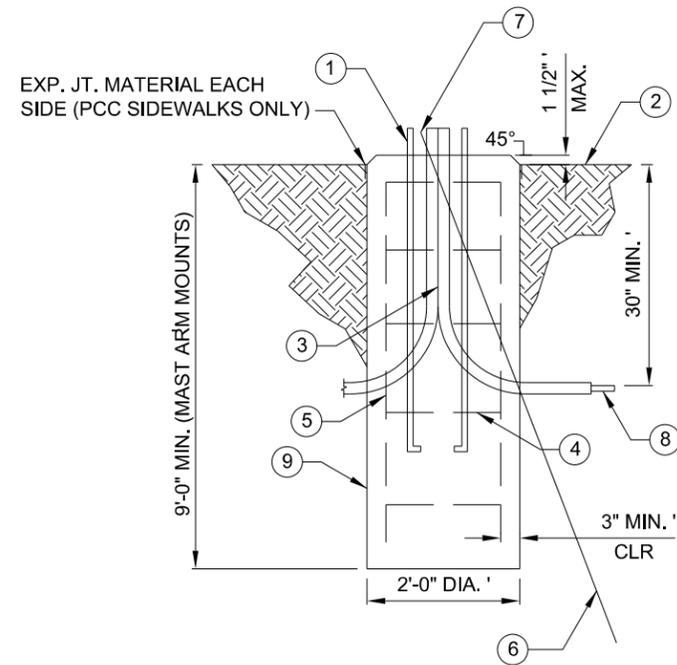
**CONDUIT TRENCH DETAIL**  
NOT TO SCALE



**SPlicing ELECTRIC Cables  
BASIC MATERIALS AND METHODS**  
NOT TO SCALE



**STREET LIGHT POLE BASE WIRING DIAGRAM**  
NOT TO SCALE



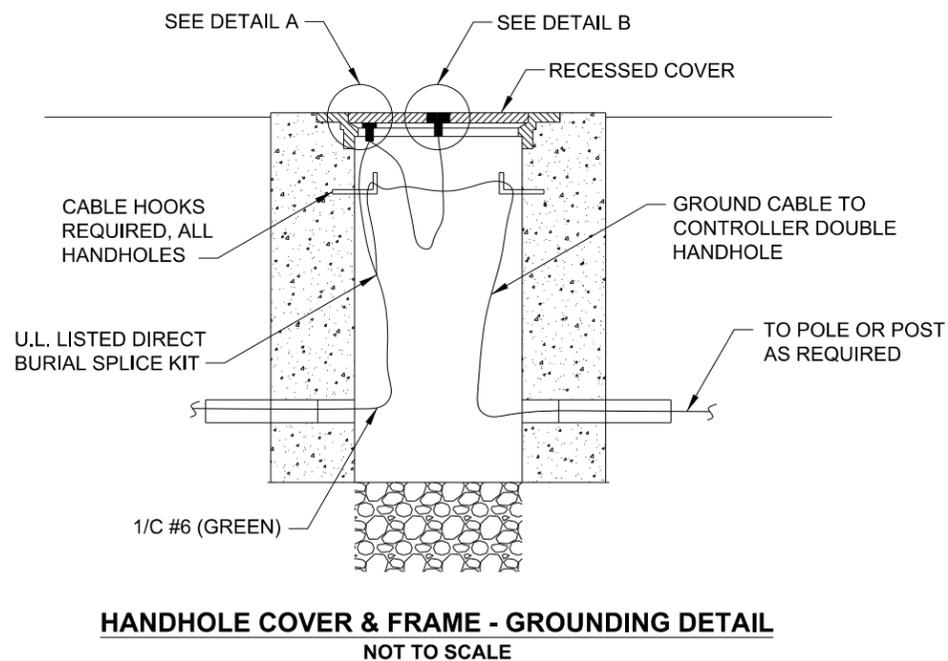
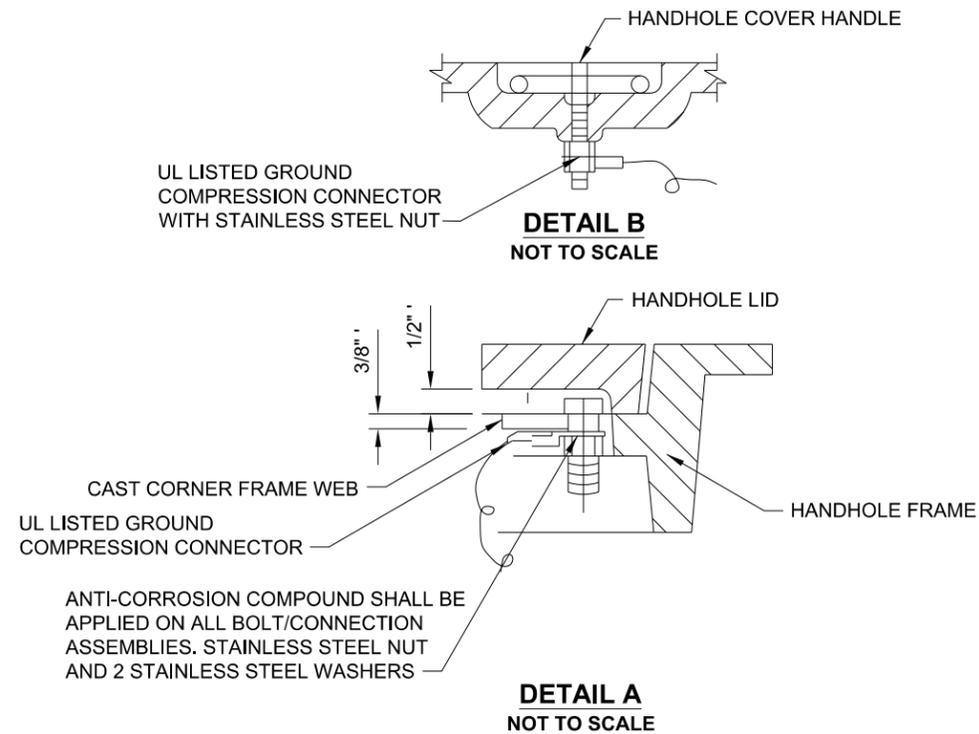
**NOTES:**

1. CONCRETE CLASS SI. SHALL BE SUPPLIED AND PLACED IN ACCORDANCE WITH SECTION 503 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. REINFORCING BARS SHALL MEET THE REQUIREMENTS OF SECTION 508 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
3. EXCAVATION HOLE FOR THE FOUNDATION SHALL BE AUGURED.
4. MINIMUM UNCONFINED COMPRESSIVE STRENGTH OF SOIL AT FOUNDATION BASE SHALL BE 2000 PSI.

**TYPICAL LIGHT POLE FOUNDATION DETAIL**  
NOT TO SCALE

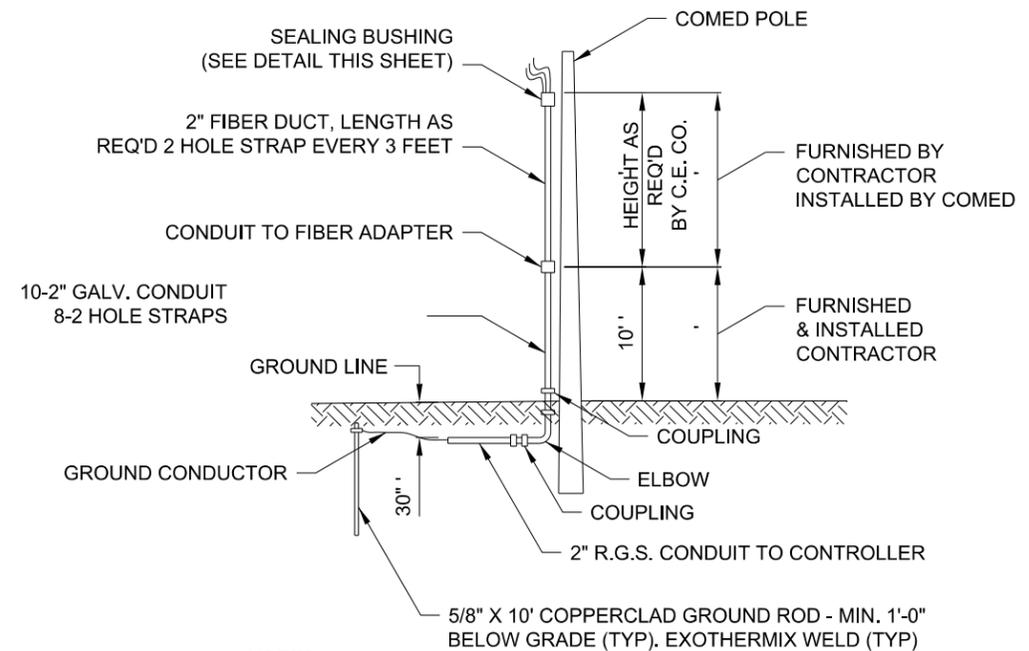
**LEGEND:**

- 1 ANCHOR BOLTS (INSTALLED PER MANUFACTURER'S RECOMMENDATIONS)
- 2 FINISHED GRADE
- 3 PVC ELBOW RACEWAYS (NUMBER AND SIZE AS REQUIRED) PROVIDE ONE SPARE
- 4 #4 TIE HOOPS @ 12" CENTERS
- 5 (8)#6 VERTICAL REINFORCING BARS EQUALLY SPACED
- 6 COPPERWELD GROUND ROD 3/4" DIA. - 10' LONG
- 7 EXOTHERMIC WELD CONNECTIONS AND #6 BARE COPPER WIRE
- 8 UNIT DUCT (SIZE AS INDICATED ON PLANS)
- 9 CONCRETE BASE



NOTE:

ALL CONDUCTORS IN THE UNIT DUCT RUNS SHALL HAVE INDIVIDUALLY COLOR-CODED INSULATION THROUGHOUT THE ENTIRE LENGTH OF THE CONDUCTOR. THE COLOR IDENTIFICATION FOR THE SYSTEM GROUND SHALL BE GREEN. THE PHASE CONDUCTORS SHALL BE IN ACCORDANCE WITH STANDARD IDOT PRACTICES FOR WIRING MULTICONDUCTOR CIRCUITS IN SINGLE UNIT DUCT RUNS.



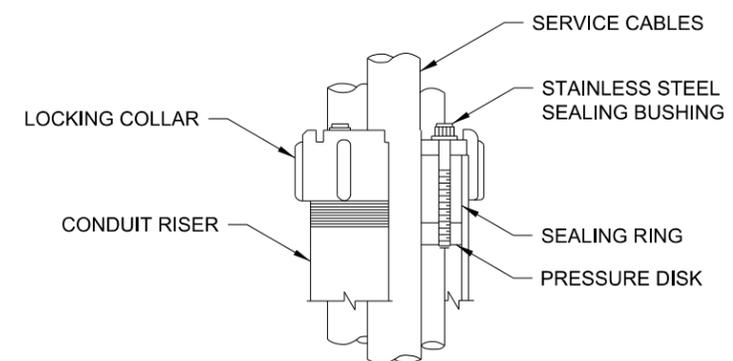
NOTES:

ALL ITEMS SHOWN ABOVE INCLUDING 2" R.G.S. CONDUIT WITH 3 1/0 AWG XLP-TYPE USE CABLES SHALL BE INCLUDED IN COST OF ELECTRIC SERVICE INSTALLATION.

SEE COMED BOOK OF INFORMATION AND REQUIREMENTS FOR THE SUPPLY OF ELECTRICAL SERVICE PAGE NO. 90591.

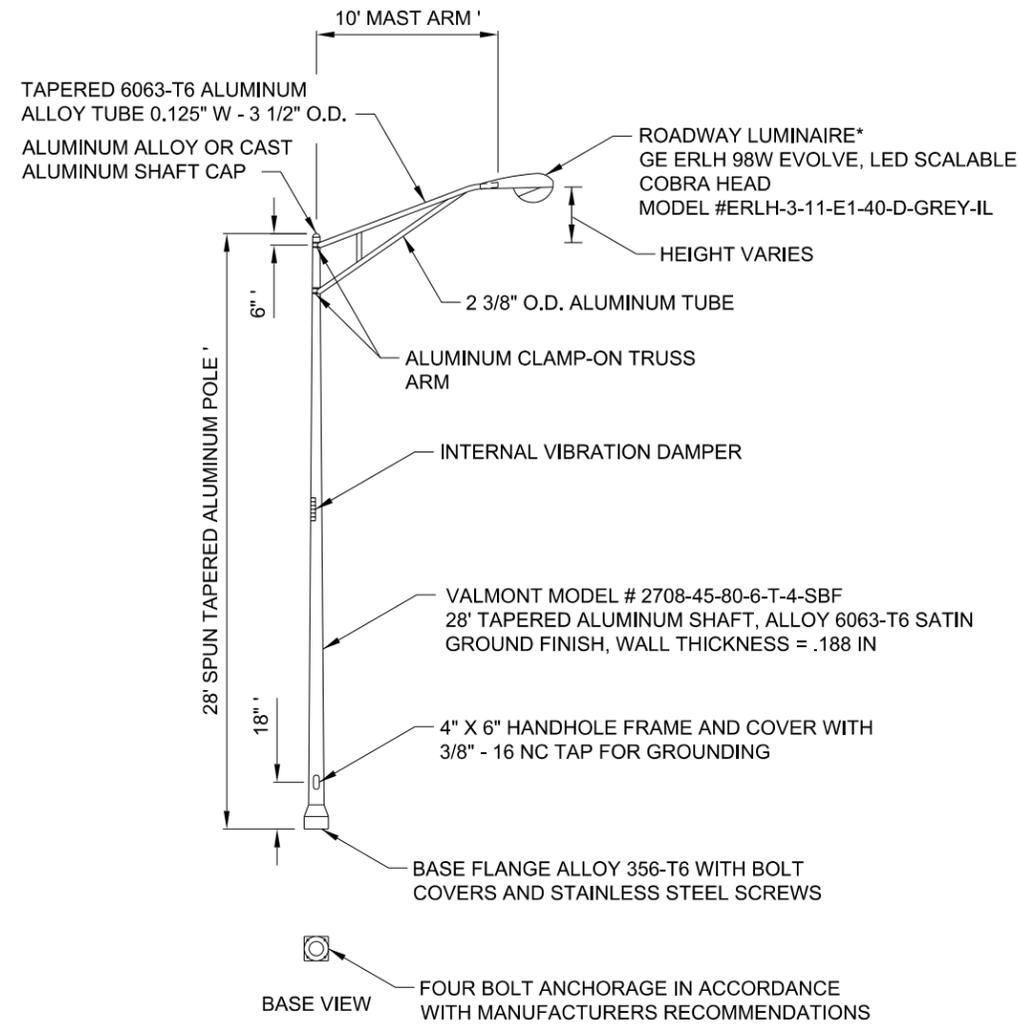
GROUND ROD, GROUND CLAMPS AND GROUND WIRE TO BE IN ACCORDANCE WITH COMED REQUIREMENTS.

**COMED OVERHEAD CONNECTION POLE**  
NOT TO SCALE



USER NAME = DonN	DESIGNED -	REVISED
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS		30	10
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				



**NOTES:**

- ALL ITEMS SHOWN ABOVE INCLUDING LIGHT POLE, LUMINAIRE/LAMP, MAST ARM, BASE FLANGE, AND INTERIOR WIRING AND FUSING SHALL BE INCLUDED IN THE COST OF "SINGLE STREET LIGHT AND POLE".
- LIGHT POLES SHALL MEET WIND LOADING AND VIBRATION REQUIREMENTS ACCORDING TO THE LATEST AASHTO STANDARDS AND ARTICLE 1069.01 IN STANDARD SPECIFICATIONS. ALL LUMINAIRES SHALL HAVE A MULTI-TAP BALLAST WIRED FOR 240 VOLT OPERATION.

**SINGLE STREET LIGHT AND POLE**  
NOT TO SCALE

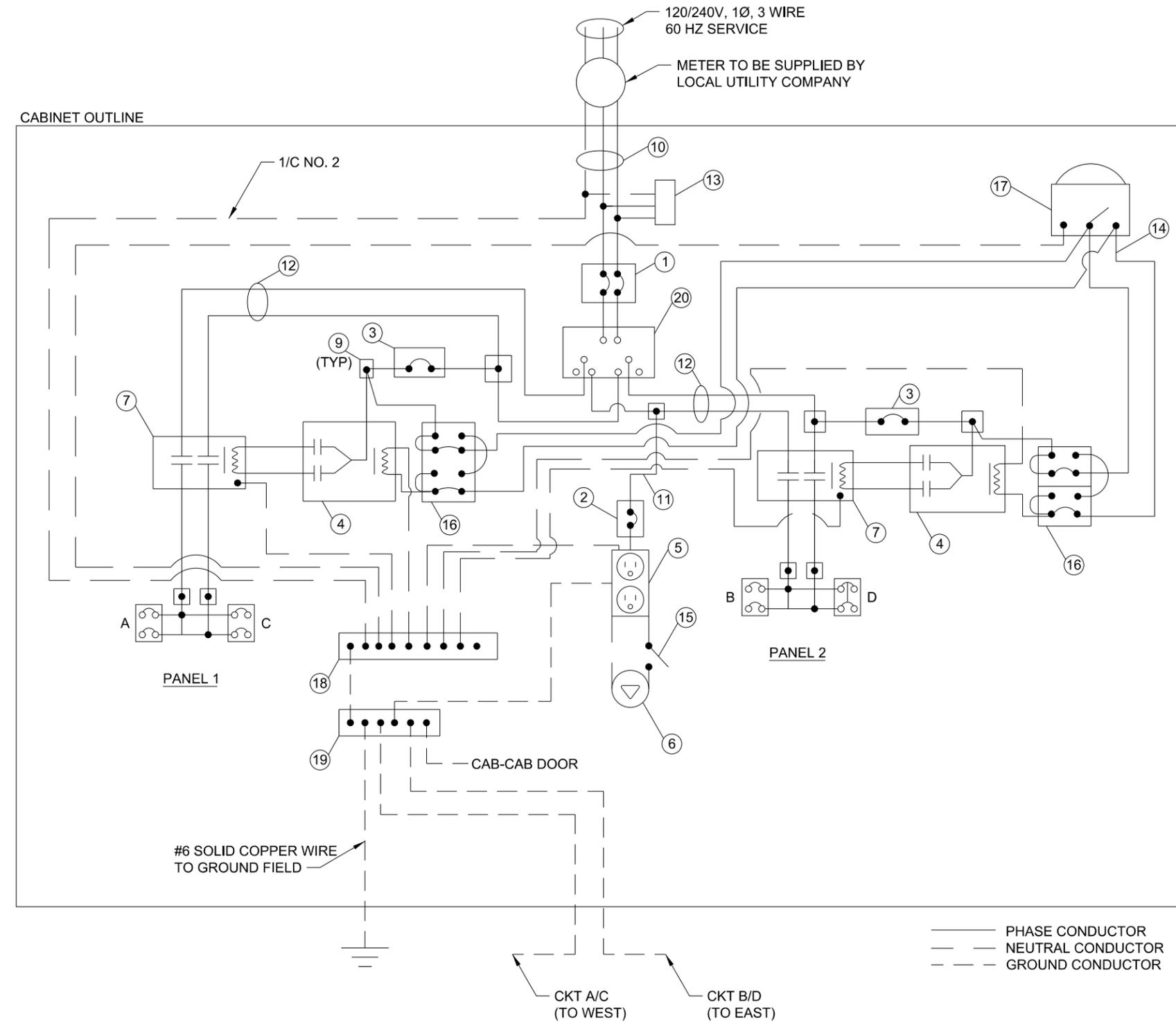
**Ordering Number Logic**  
Evolve™ LED Streetlight (ERLH)



**ERLH 3 11 E1 40 D GRAY IL-**

PROD. ID	VOLTAGE	LUMEN OUTPUT	DISTRIBUTION	CCT	CONTROLS	COLOR	OPTIONS
E = Evolve	0 = 120-277*	10	A1 = Extra Narrow Asymmetric	30 = 3000K	A = ANSI C136 41 7-pin receptacle with Shorting Cap	GRAY = Gray	A = 4 Bolt Slipfitter †
R = Roadway	1 = 120	11	B1 = Narrow Asymmetric (Medium)	40 = 4000K	D = ANSI C136 41 7-pin receptacle with non-Dimming PE Control.*	BLCK = Black	F = Fusing
L = Local	2 = 208	13	C1 = Asymmetric (Short)		E = ANSI C136 41 7-pin Receptacle with non-Dimming PE Control.*	DKBZ = Dark Bronze	G = Internal Bubble Level
H = High Output	3 = 240	14	D1 = Asymmetric Forward		* PE Control Only available for 120-277V or 480V Discrete. Not available for 347-480V or 347V Discrete.		I = IP66 Optical
	4 = 277	15	E1 = Asymmetric (Medium)		NOTE: Dimming controls wired for 0-10V standard unless DALI option "U" requested.		L = Tool-Less Entry
	5 = 480		F1 = Asymmetric (Wide)				R = Optional Secondary Enhanced Surge Protection (10kV/5kA)
	D = 347		G1 = Asymmetric (Extra Wide)				U = Universal DALI Programmable +^
	H = 347-480*						X = Single Package #
							Y = Coastal Finish *
							XXX = Special Options

PRODUCT ID	LUMEN OUTPUT	DISTRIBUTION	TYPICAL INITIAL LUMENS		TYPICAL SYSTEM WATTAGE	BUG RATING		IES FILE NUMBER	
			4000K	3000K		4000K	3000K	4000K	3000K
ERLH	10	A1	9500	9100	90	B3-U0-G2	B3-U0-G2	ERLH_10A140	ERLH_10A130
ERLH	10	B1	9800	9500	90	B3-U0-G1	B2-U0-G1	ERLH_10B140	ERLH_10B130
ERLH	10	C1	10000	9600	90	B2-U0-G1	B2-U0-G1	ERLH_10C140	ERLH_10C130
ERLH	10	D1	9800	9500	90	B2-U0-G2	B2-U0-G2	ERLH_10D140	ERLH_10D130
ERLH	10	E1	10000	9600	90	B2-U0-G2	B2-U0-G2	ERLH_10E140	ERLH_10E130
ERLH	10	F1	10000	9600	90	B2-U0-G2	B2-U0-G2	ERLH_10F140	ERLH_10F130
ERLH	10	G1	10000	9600	90	B2-U0-G2	B2-U0-G2	ERLH_10G140	ERLH_10G130
ERLH	11	A1	10900	10500	108	B3-U0-G2	B3-U0-G2	ERLH_11A140	ERLH_11A130
ERLH	11	B1	11200	10800	108	B3-U0-G2	B3-U0-G1	ERLH_11B140	ERLH_11B130
ERLH	11	C1	11500	11100	108	B3-U0-G2	B3-U0-G2	ERLH_11C140	ERLH_11C130
ERLH	11	D1	11200	10800	108	B2-U0-G2	B2-U0-G2	ERLH_11D140	ERLH_11D130
ERLH	11	E1	11500	11100	108	B3-U0-G2	B3-U0-G2	ERLH_11E140	ERLH_11E130
ERLH	11	F1	11500	11100	108	B3-U0-G2	B3-U0-G2	ERLH_11F140	ERLH_11F130
ERLH	11	G1	11500	11100	108	B3-U0-G2	B3-U0-G2	ERLH_11G140	ERLH_11G130
ERLH	13	A1	12300	11900	125	B3-U0-G2	B3-U0-G2	ERLH_13A140	ERLH_13A130
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ERLH	13	G1	13000	12500	125	B3-U0-G2	B3-U0-G2	ERLH_13G140	ERLH_13G130
ERLH	14	A1	13300	12800	139	B3-U0-G3	B3-U0-G3	ERLH_14A140	ERLH_14A130
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ERLH	14	E1	14000	13500	139	B3-U0-G2	B3-U0-G2	ERLH_14E140	ERLH_14E130
ERLH	14	F1	14000	13500	139	B3-U0-G2	B3-U0-G2	ERLH_14F140	ERLH_14F130
ERLH	14	G1	14000	13500	139	B3-U0-G2	B3-U0-G2	ERLH_14G140	ERLH_14G130
ERLH	15	A1	14200	13700	161	B3-U0-G3	B3-U0-G3	ERLH_15A140	ERLH_15A130
ERLH	15	B1	14700	14200	161	B3-U0-G2	B3-U0-G2	ERLH_15B140	ERLH_15B130
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ERLH	15	D1	14700	14200	161	B3-U0-G2	B3-U0-G2	ERLH_15D140	ERLH_15D130
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ERLH	15	F1	15000	14500	161	B3-U0-G2	B3-U0-G2	ERLH_15F140	ERLH_15F130
ERLH	15	G1	15000	14500	161	B3-U0-G2	B3-U0-G2	ERLH_15G140	ERLH_15G130



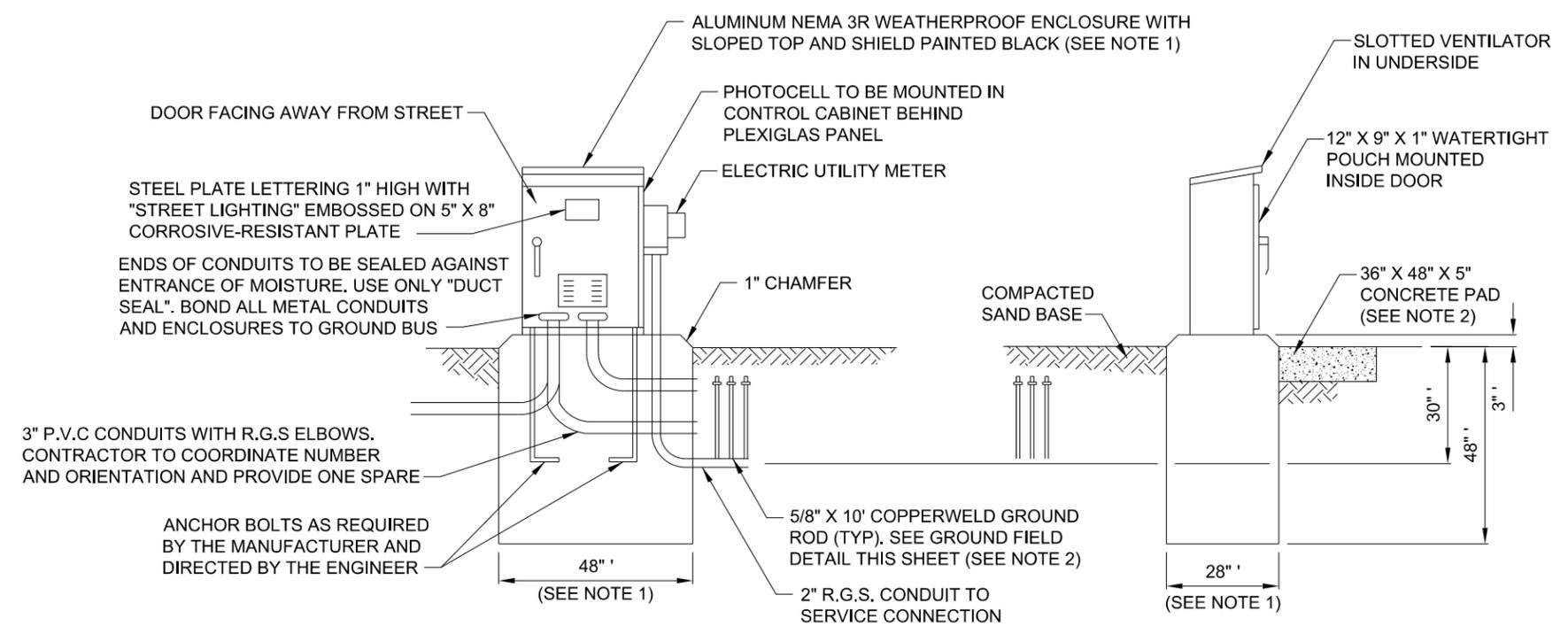
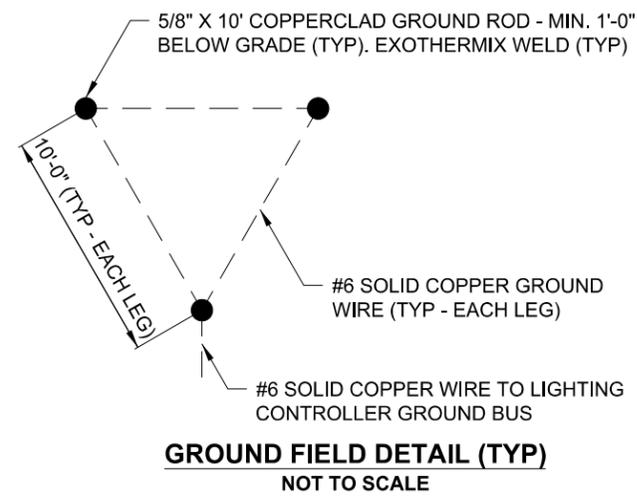
**TYPICAL LIGHTING CONTROLLER WIRING DETAIL\***  
NOT TO SCALE

ITEM	QTY	DESCRIPTION	SPECIFICATION
1	1	MAIN CIRCUIT BREAKER	100 AMPHERE, 2 P, 240V RATING, 22K AIC (NOTE 3)
2	1	LAMPHOLDER CIRCUIT BREAKER	15 AMPHERE, 1 P, 120V RATING, 22K AIC
3	2	PHOTOELECTRIC CONTROL CIRCUIT BREAKER	15 AMPHERE, 1 P, 120V RATING, 22K AIC
4	2	AUXILIARY RELAY	120V OPERATED SPST 60 HZ COIL 600V NORMALLY OPEN CONTACTS
5	1	CABINET RECEPTACLE AND BOX	COMMERCIAL GRADE GFCI 15A/120V, MOUNTED IN A WEATHERPROOF CAST ALUMINUM SIGLE GANG BOX WITH WEATHERPROOF COVER
6	1	CABINET LIGHT AND BOX	120V WEATHERPROOF LAMPHOLDER MOUNTED IN A CAST ALUMINUM J BOX & EXT. GRADE 100W LAMP
7	2	CONTACTOR	100 AMPHERE, 2 POLE, 120V COIL, MECH. HELD
8	4	BRANCH LINE CIRCUIT BREAKERS	4 - 30 AMPHERE, 2P, 240V V RATION, 22K ATC
9		POWER DUSTRIUTION BLOCK	600V, INSULATED, SIZE AS REQUIRED
10		SERVICE CABLES	3 - 600V (XLP-TYPE USED) NO. 2
11		LAMPHOLDER WIRE	2 - 600V XLP NO. 10
12		CONTROL WIRE	2 - 600V XLP NO. 10
13	1	SURGE ARRESTOR	10 K AMPHERE RATING AT 750V, PHASE 1
14		PHOTOELECTRIC CONTROL WIRE	3 - 600V XLP NO. 10
15	1	DOOR SWITCH	20A/120V DOOR MOUNTED SNAP ACTION TYPE PLUNGER SWITCH
16	2	HAND-AUTO-OFF CONTROL SWITCH	20A, 3 POS. IN INDUSTRIAL DUTY ENCLOSURE
17	1	PHOTOCELL	120V, MTD. ON CABINET, DELAY TYPE, SPST-NC
18	1	NEUTRAL BUS	600V, ALUMINUM ALLOY, SIZE AS REQUIRED
19	1	GROUND BUS	600V, ALUMINUM ALLOY, SIZE AS REQUIRED
20	1	SPLICE BLOCK	600V, INSULATED, SIZE AS REQUIRED

**NOTES:**

1. ALL ITEMS LISTED IN LIGHTING CONTROLLER COMPONENT SCHEDULE SHALL BE CONSIDERED INCLUDED IN THE COST OF "LIGHTING CONTROLLER" INCLUDING CABINET AND FOUNDATION.
2. THE LIGHTING CONTROLLER TOGETHER WITH ALL OF ITS COMPONENTS SHALL BE UL LISTED AS AN "ENCLOSED INDUSTRIAL CONTROL PANEL" UNDER UL508A
3. CONNECTION OF SURGE ARRESTOR TO LINE SIDE OF MAIN CIRCUIT BREAKER SHALL NOT BE "DOUBLE LUGGED."

**TYPICAL LIGHTING CONTROLLER COMPONENT SCHEDULE**  
NOT TO SCALE



NOTE:  
 ALL CONDUCTORS IN THE UNIT DUCT RUNS SHALL HAVE INDIVIDUALLY COLOR-CODED INSULATION THROUGHOUT THE ENTIRE LENGTH OF THE CONDUCTOR. THE COLOR IDENTIFICATION FOR THE SYSTEM GROUND SHALL BE GREEN. THE PHASE CONDUCTORS SHALL BE IN ACCORDANCE WITH STANDARD IDOT PRACTICES FOR WIRING MULTICONDUCTOR CIRCUITS IN SINGLE UNIT DUCT RUNS.

**LIGHTING CONTROLLER CABINET & FOUNDATION**  
 NOT TO SCALE

USER NAME = DonN	DESIGNED -	REVISED
DRAWN -	REVISOR -	
PLOT SCALE = 2.0000' / 1" =	CHECKED -	REVISOR -
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5041	18-00081-00-RS	DuPAGE	30	13
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				

PROJECT BEGINS  
STA. 600+25

SEE NOTE 2\*

P1-CKT A  
STA. 600+55

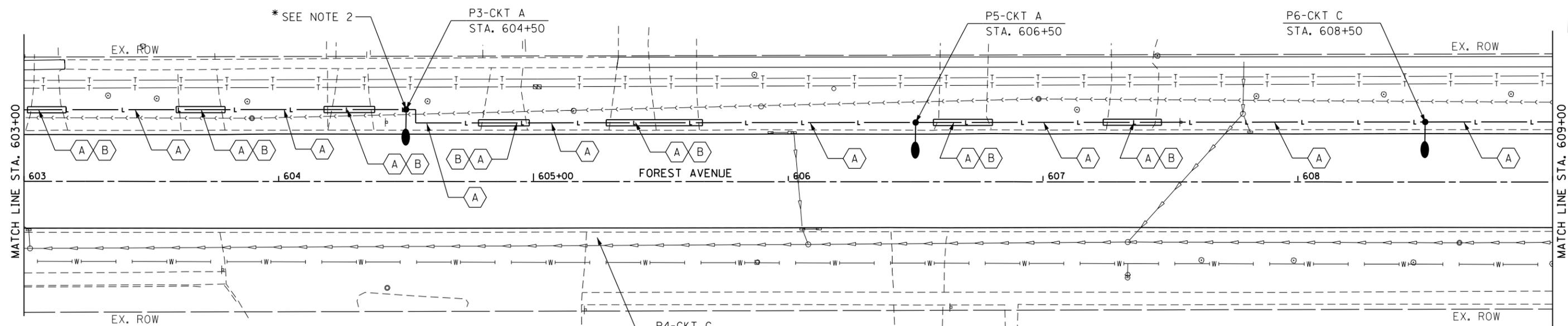
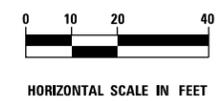
P2-CKT C  
STA. 602+55

SEE NOTE 2\*

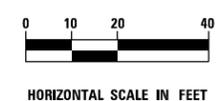
**LEGEND:**

- SINGLE STREET LIGHT AND POLE WITH 10' MAST ARM (28' POLE HEIGHT)  
\* 15' MAST ARM WHERE NOTED
- PROPOSED LIGHTING CONTROLLER
- 
- 

- NOTE:**
- TYPICAL SETBACK SHALL BE 3' FROM FACE OF CURB TO CENTER OF LIGHT POLE
  - 15' MAST ARM WHERE NOTED

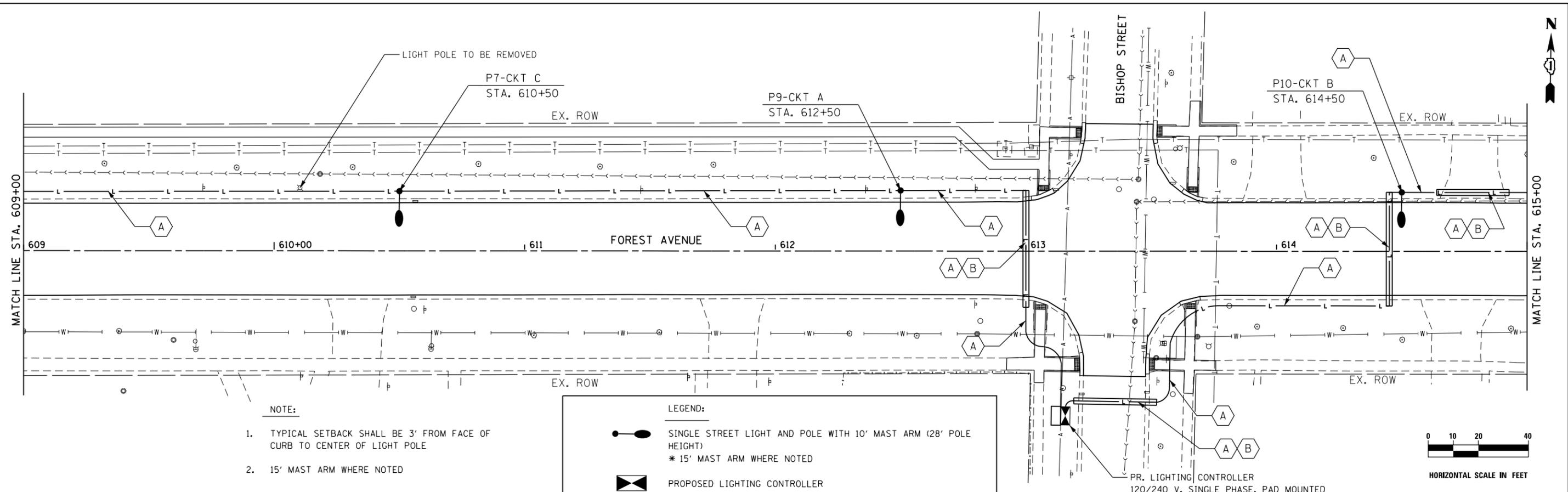


- NOTE:**
- TYPICAL SETBACK SHALL BE 3' FROM FACE OF CURB TO CENTER OF LIGHT POLE
  - 15' MAST ARM WHERE NOTED



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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2018	CHECKED -	REVISED -
	DATE -	REVISED -

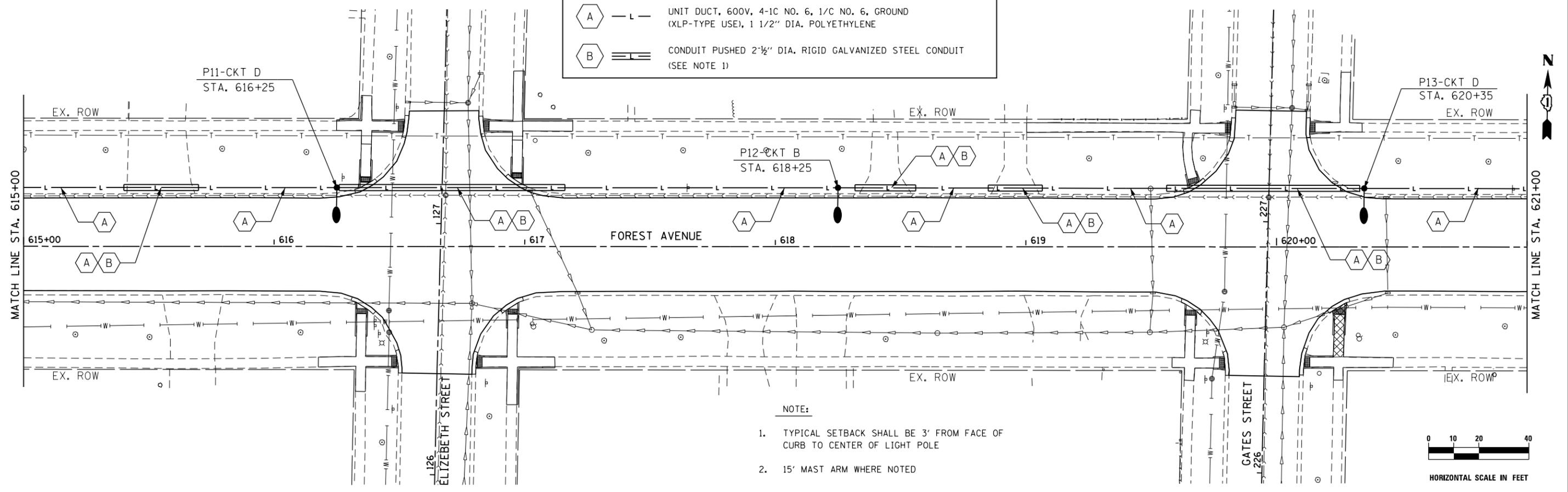
MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	14
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				



- NOTE:**
1. TYPICAL SETBACK SHALL BE 3' FROM FACE OF CURB TO CENTER OF LIGHT POLE
  2. 15' MAST ARM WHERE NOTED

**LEGEND:**

- SINGLE STREET LIGHT AND POLE WITH 10' MAST ARM (28' POLE HEIGHT)  
\* 15' MAST ARM WHERE NOTED
- PROPOSED LIGHTING CONTROLLER
- UNIT DUCT, 600V, 4-1C NO. 6, 1/C NO. 6, GROUND (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE
- CONDUIT PUSHED 2-1/2" DIA. RIGID GALVANIZED STEEL CONDUIT (SEE NOTE 1)

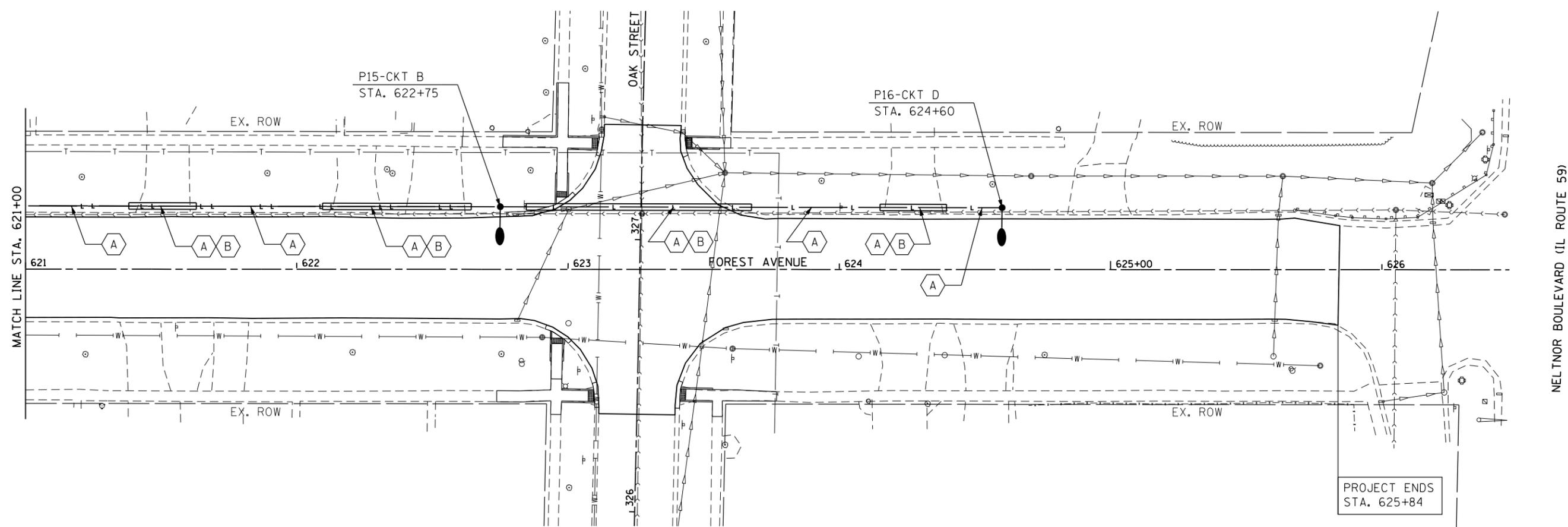


- NOTE:**
1. TYPICAL SETBACK SHALL BE 3' FROM FACE OF CURB TO CENTER OF LIGHT POLE
  2. 15' MAST ARM WHERE NOTED

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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2018	CHECKED -	REVISED -
	DATE -	REVISED -

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				

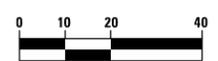


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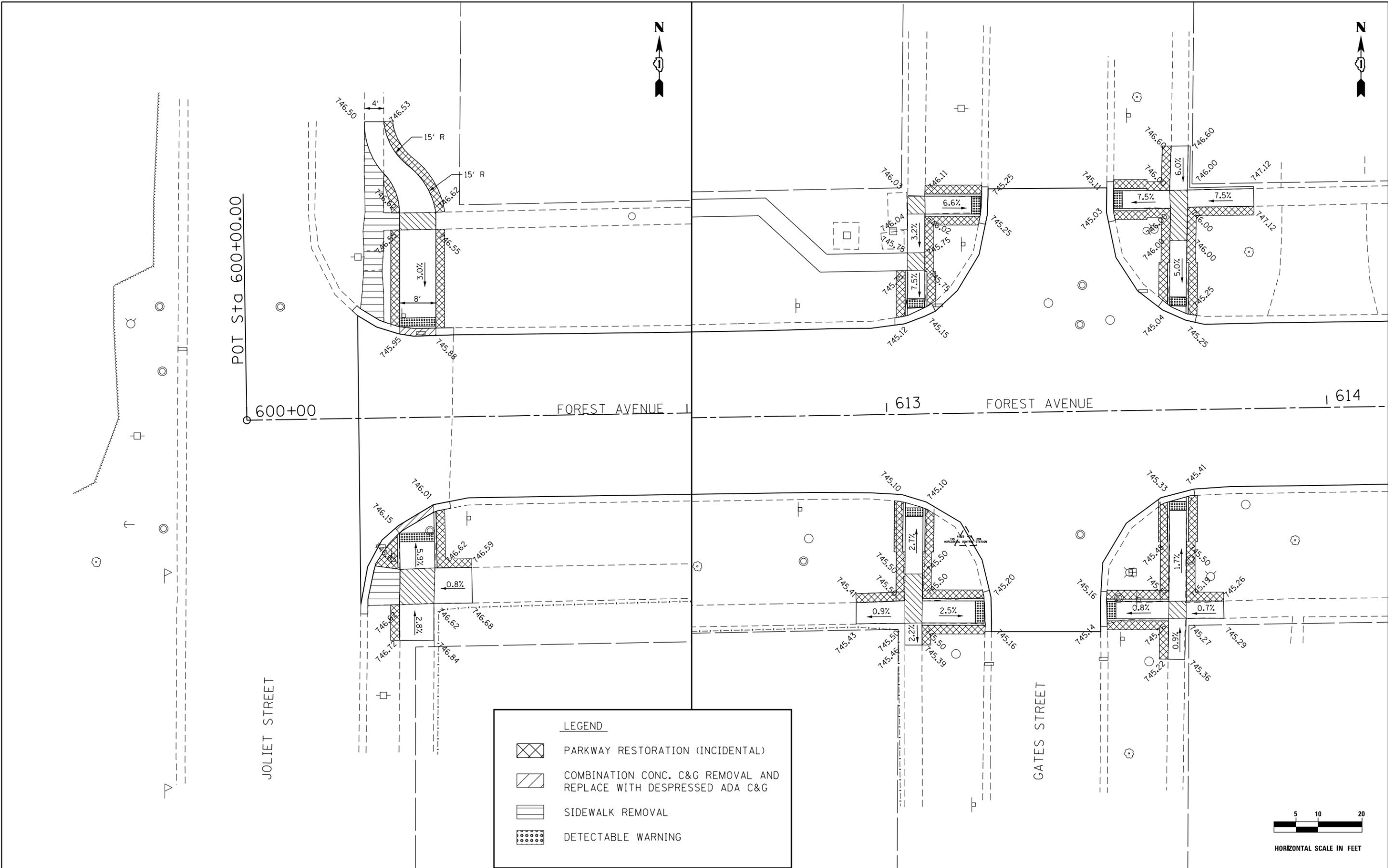
- SINGLE STREET LIGHT AND POLE WITH 10' MAST ARM (28' POLE HEIGHT)  
\* 15' MAST ARM WHERE NOTED
- PROPOSED LIGHTING CONTROLLER
- UNIT DUCT, 600V, 4-1C NO. 6, 1/C NO. 6, GROUND (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE
- CONDUIT PUSHED 2-1/2" DIA. RIGID GALVANIZED STEEL CONDUIT (SEE NOTE 1)

- NOTE:**
- TYPICAL SETBACK SHALL BE 3' FROM FACE OF CURB TO CENTER OF LIGHT POLE
  - 15' MAST ARM WHERE NOTED

CIRCUIT ID	135 W ROADWAY LUMINAIRE		TOTAL LOAD (AMPS)
	QTY.	LOAD (AMPS)	
A	4	0.8	4.0
B	3	0.8	3.2
C	4	0.8	3.2
D	3	0.8	3.2



HORIZONTAL SCALE IN FEET



LEGEND

- PARKWAY RESTORATION (INCIDENTAL)
- COMBINATION CONC. C&G REMOVAL AND REPLACE WITH DESPRESSED ADA C&G
- SIDEWALK REMOVAL
- DETECTABLE WARNING

**thomas**  
 thomas engineering group, llc  
 762 shoreline drive  
 suite 200  
 aurora, illinois 60504  
 phone: 855-533-1700  
 service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED -
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	DATE -	REVISED -

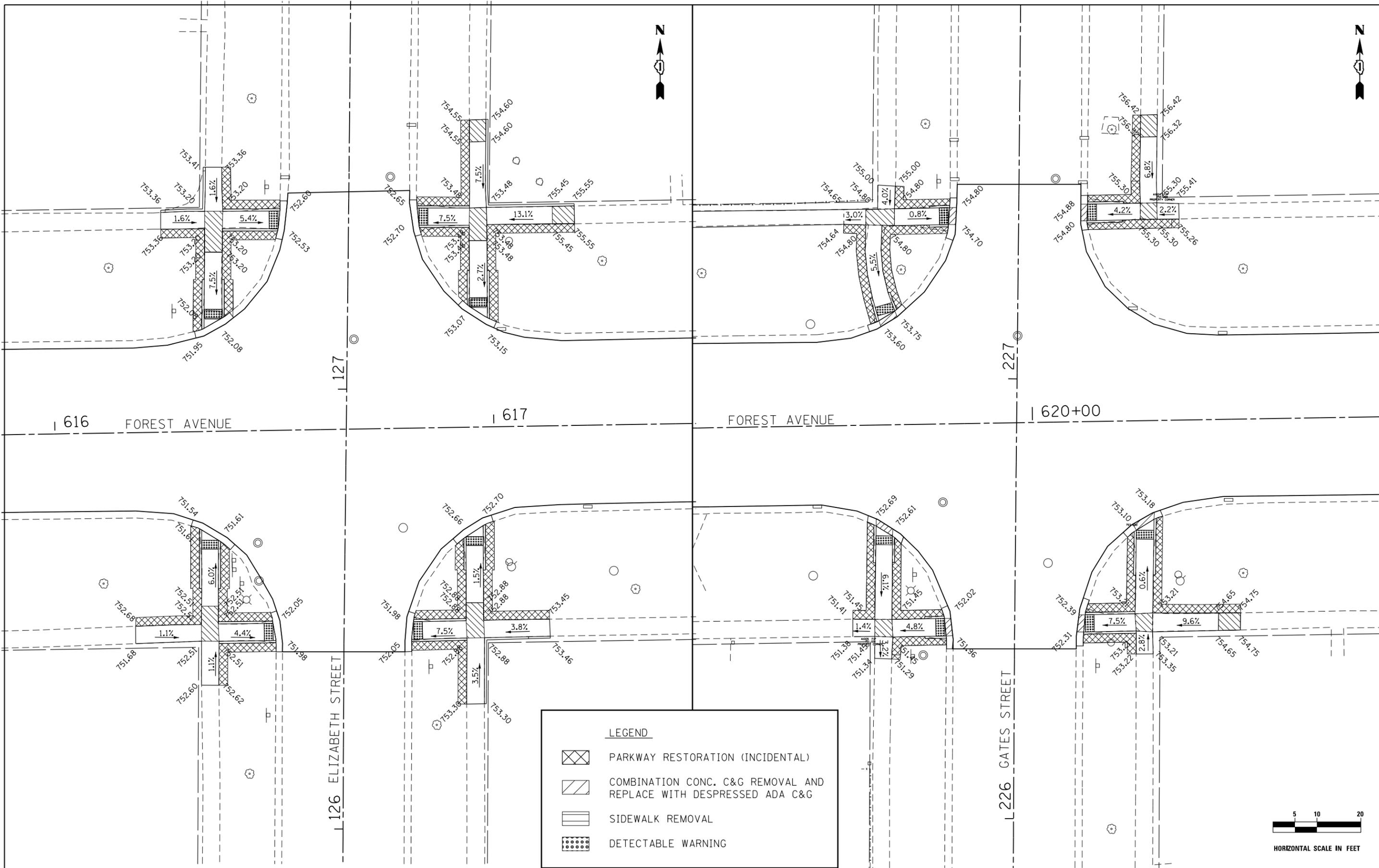
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DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAILS**

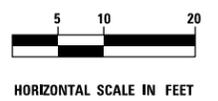
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	17
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- PARKWAY RESTORATION (INCIDENTAL)
- COMBINATION CONC. C&G REMOVAL AND REPLACE WITH DESPRESSED ADA C&G
- SIDEWALK REMOVAL
- DETECTABLE WARNING



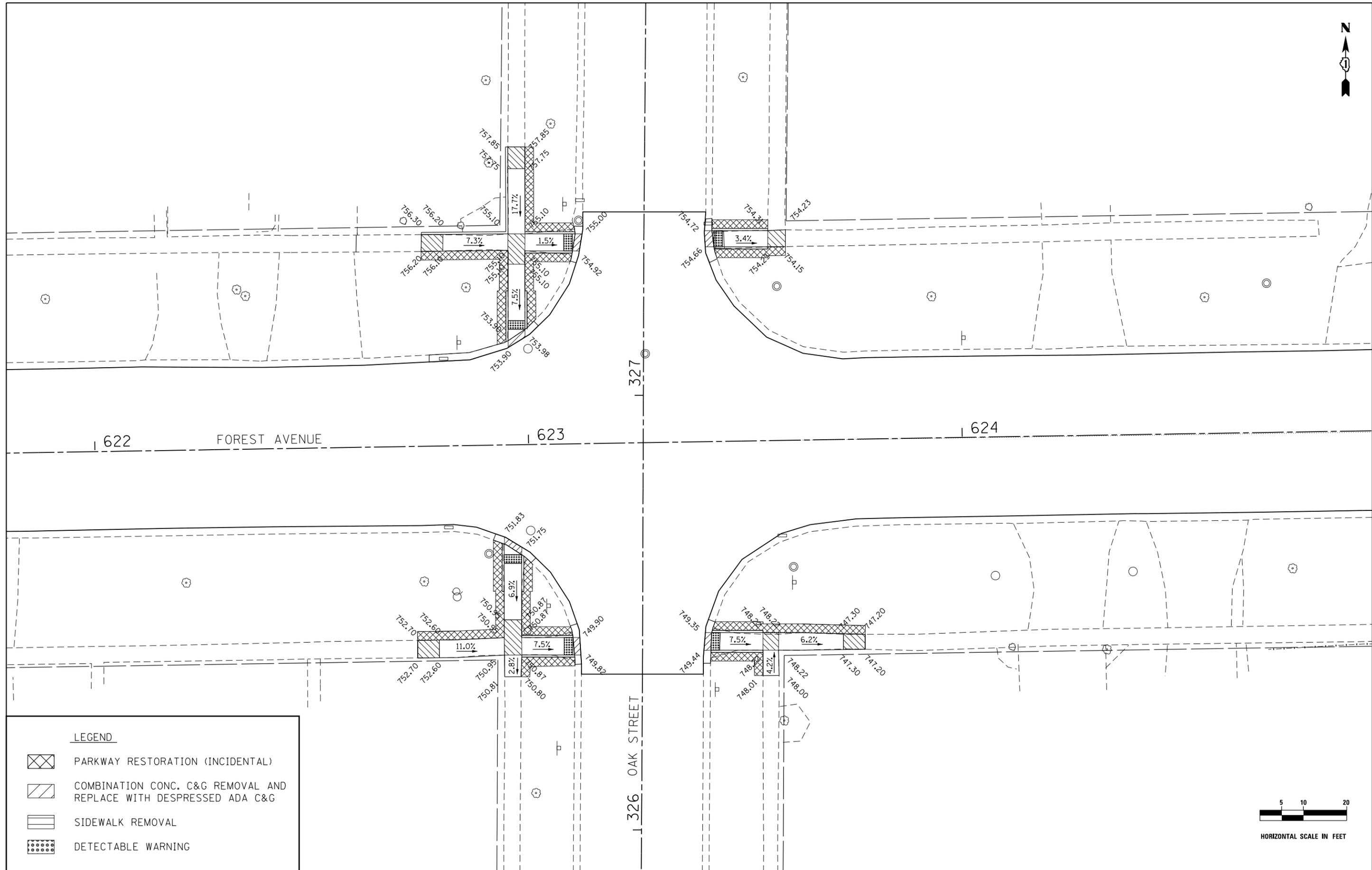
**thomas** thomas engineering group, llc  
 762 shoreline drive  
 suite 200  
 aurora, illinois 60504  
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 service at the highest grade

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DRAWN -	REVISOR -	
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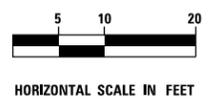
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SIDEWALK DETAILS</b>				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	18
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				



LEGEND	
	PARKWAY RESTORATION (INCIDENTAL)
	COMBINATION CONC. C&G REMOVAL AND REPLACE WITH DESPRESSED ADA C&G
	SIDEWALK REMOVAL
	DETECTABLE WARNING



**thomas**  
 thomas engineering group, llc  
 762 shoreline drive  
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 aurora, illinois 60504  
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 service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED -
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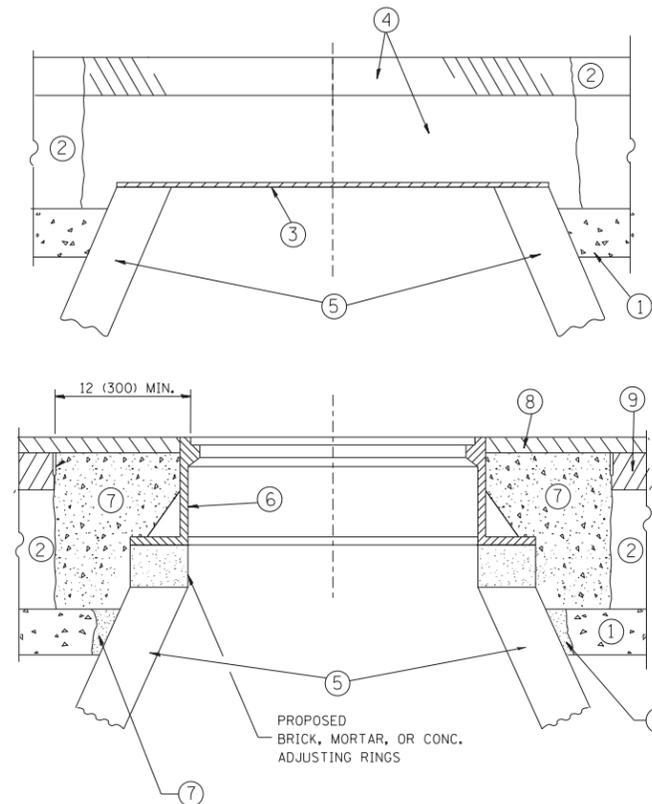
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAILS**

SCALE: SHEET OF SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	19
CONTRACT: 61F55				
ILLINOIS FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

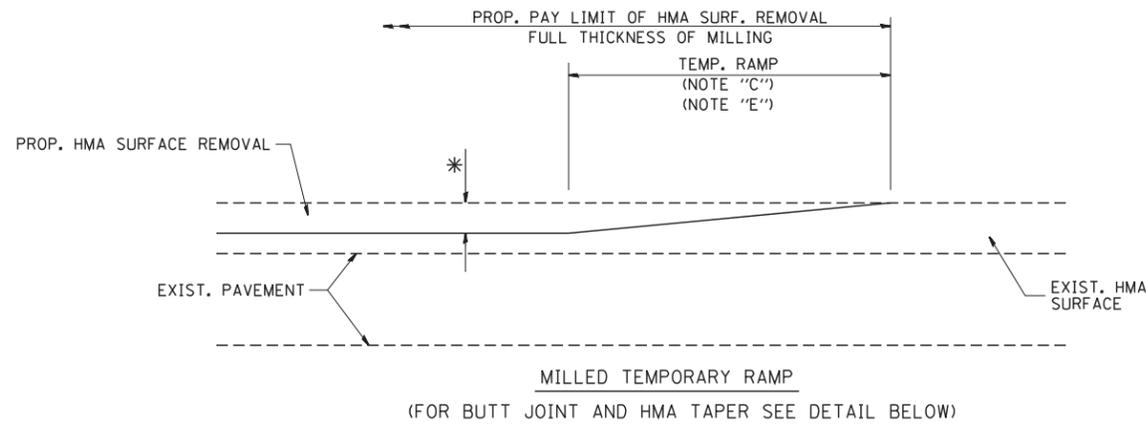
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

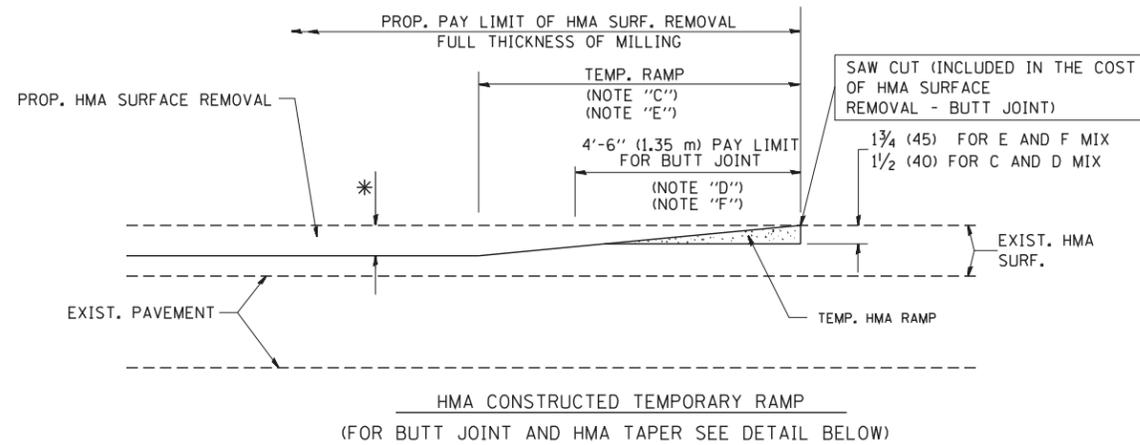
**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

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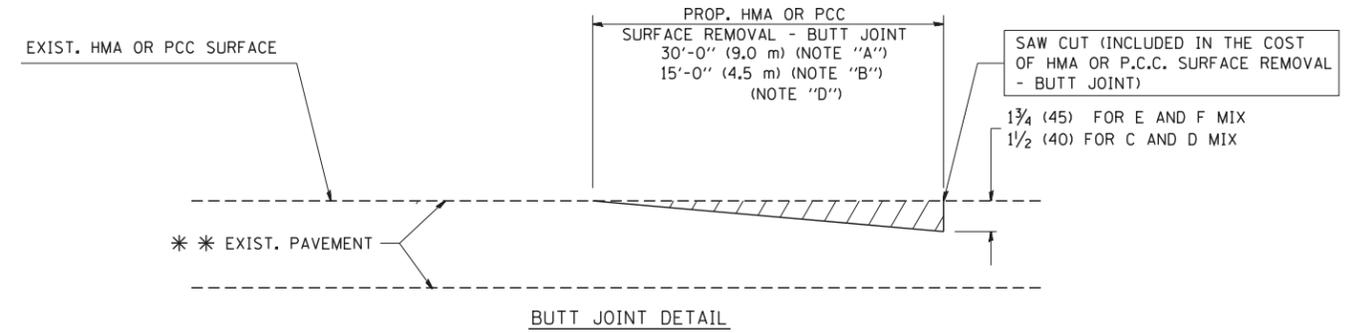


**OPTION 1**

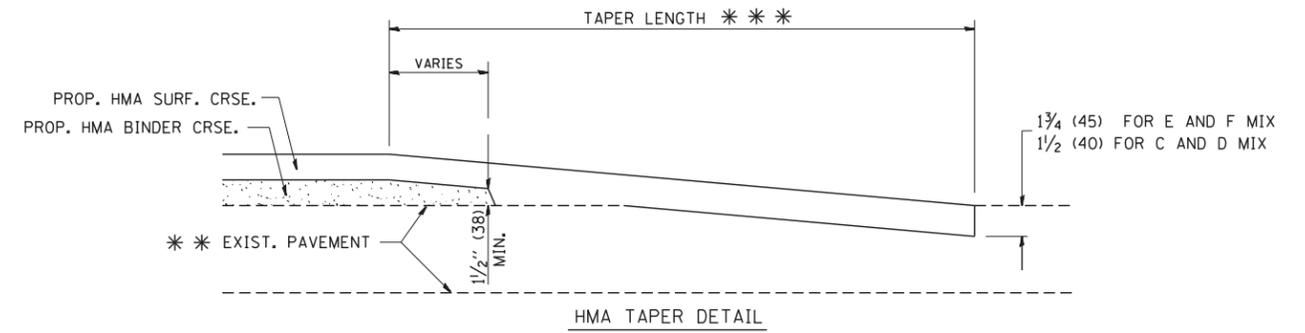


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

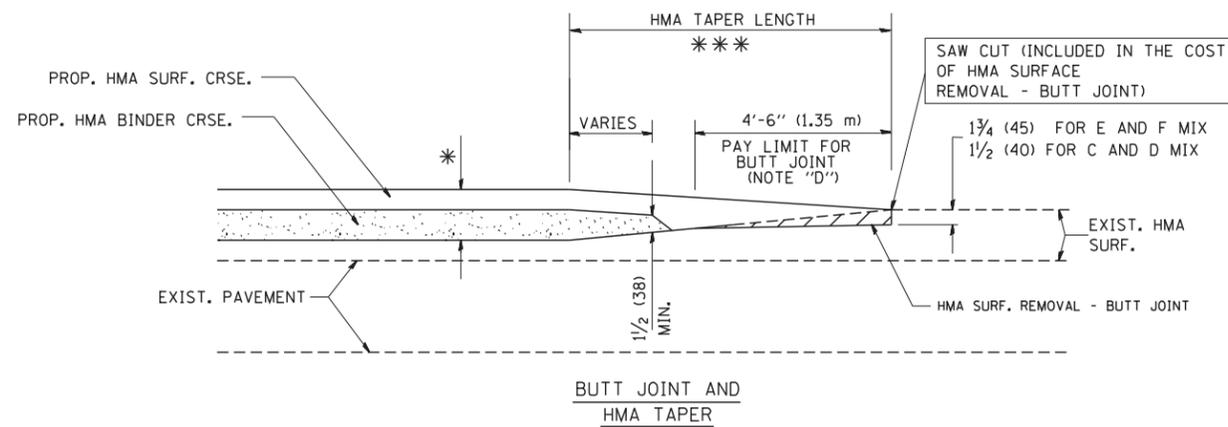
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

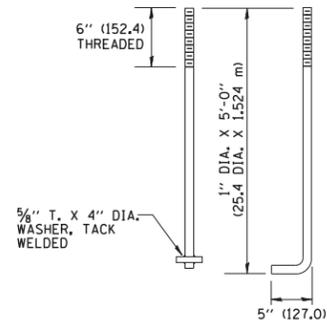
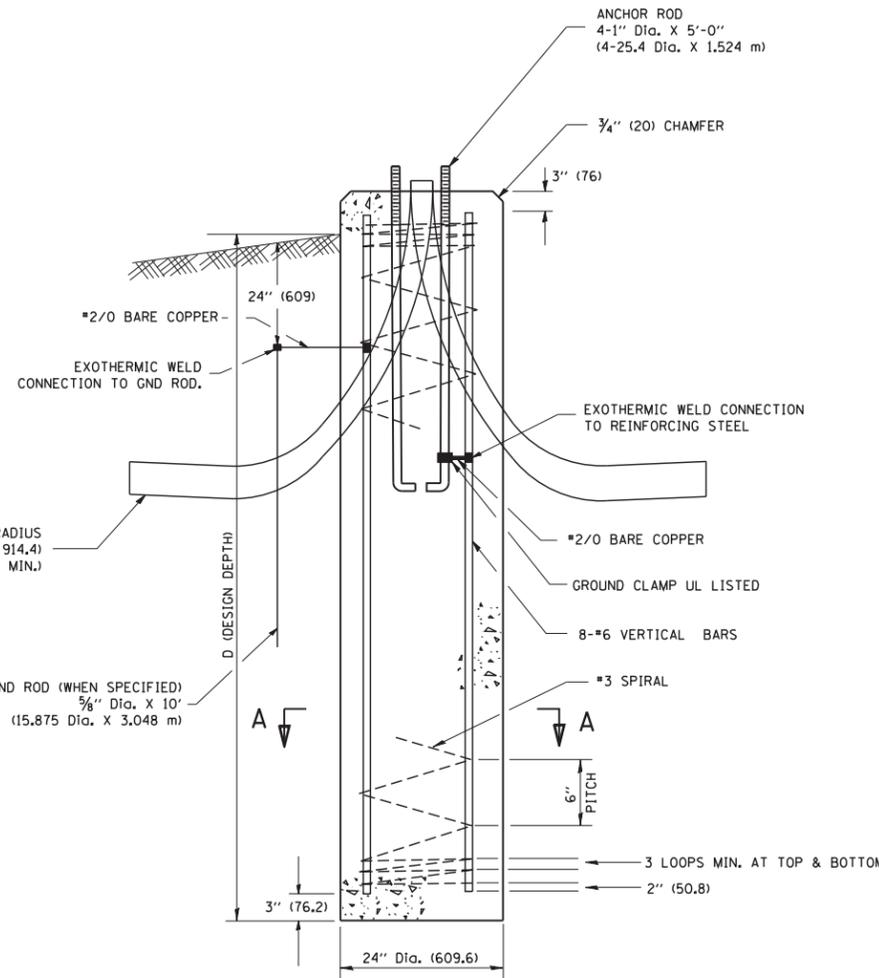
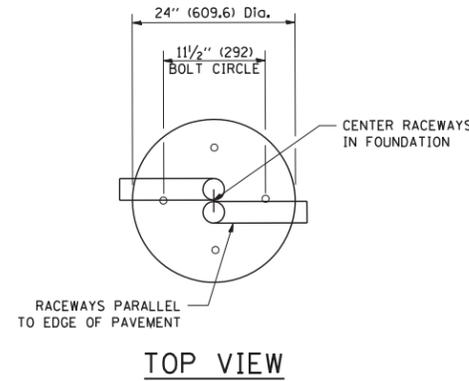
**BUTT JOINT AND  
HMA TAPER DETAILS**

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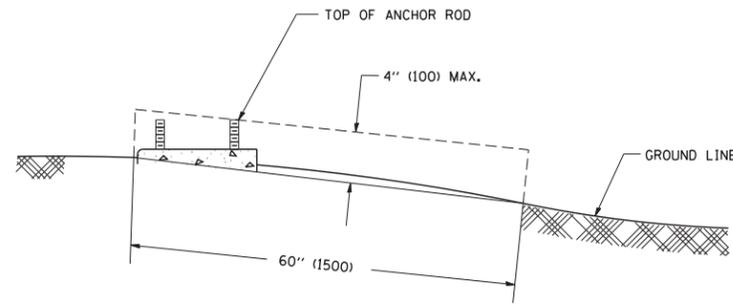
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<b>BD400-05 BD32</b>		CONTRACT NO.	61F55	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LIGHT POLE FOUNDATION DEPTH TABLE**  
**30 FT. (9.144 m) TO 35 FT. (10.668 m) MOUNTING HEIGHT**

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O <sub>u</sub> = 0.375 TON/SQ. FT.	11'-0" (3.35 m)	12'-8" (3.85 m)
MEDIUM CLAY O <sub>u</sub> = 0.75 TON/SQ.FT	9'-0" (2.74 m)	14'-10" (4.52 m)
STIFF CLAY O <sub>u</sub> = 1.50 TON/SO. FT.	7'-6" (2.29 m)	8'-7" (2.61 m)
LOOSE SAND φ = 34°	9'-6" (2.90 m)	10'-7" (3.22 m)
MEDIUM SAND φ = 37.5°	9'-0" (2.74 m)	9'-10" (2.99 m)
DENSE SAND φ = 40°	8'-3" (2.51 m)	9'-7" (2.91 m)

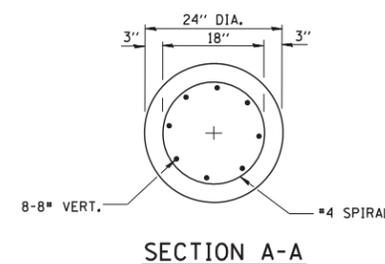


**ANCHOR BOLT DETAIL**



**FOUNDATION EXTENSION DETAIL**

**FOUNDATION DETAIL**



**SECTION A-A**

**NOTES**

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS S1. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.

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 PLOT DATE = 1/4/2008

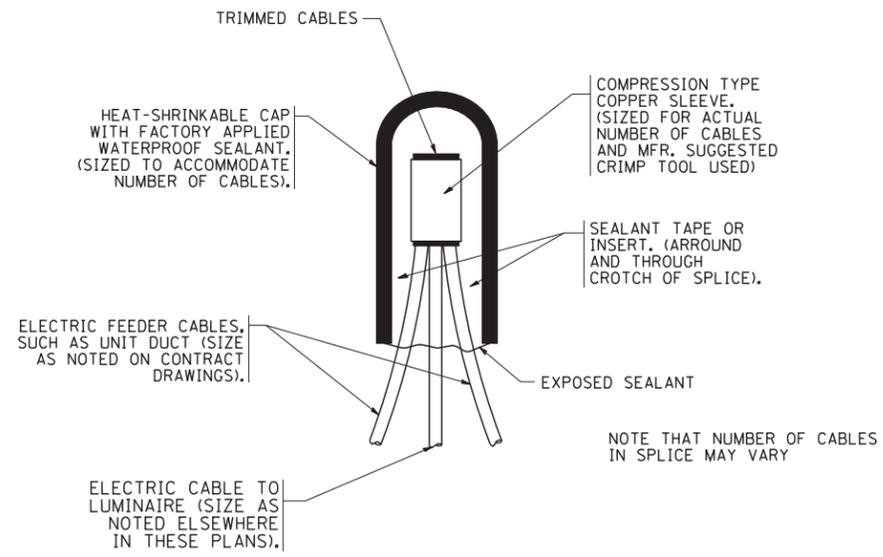
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

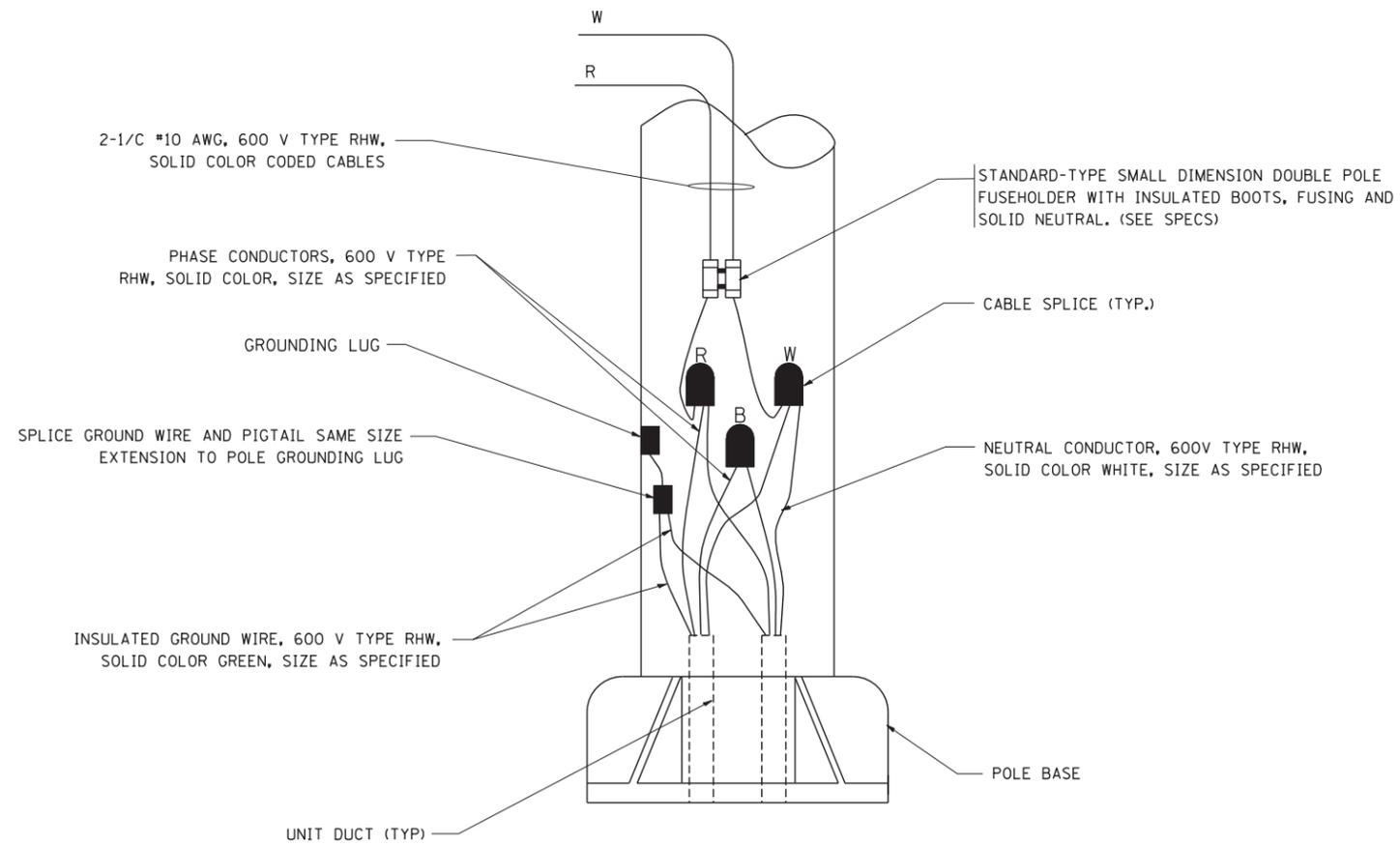
**LIGHT POLE FOUNDATION**  
**30' (9.144 m) TO 35' (10.668 m) M.H. 11 12" (292 mm) BOLT CIRCLE**  
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BE-300			CONTRACT NO. 61F55	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



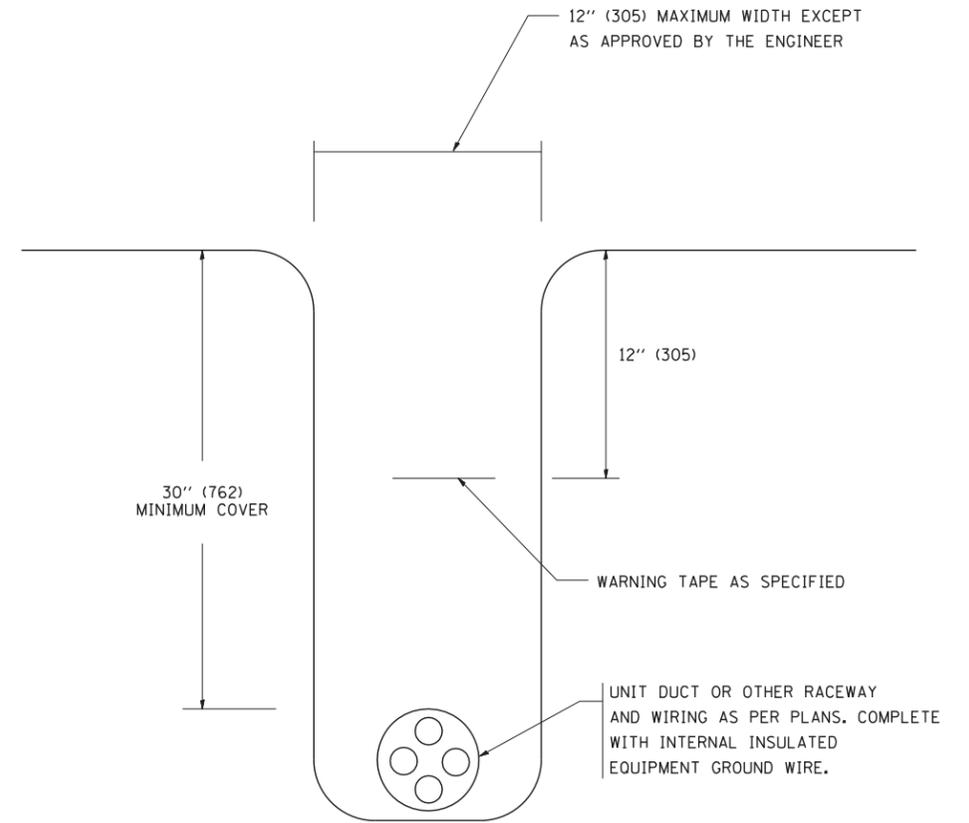
**TYPICAL SPLICE DETAIL**

N.T.S.



**POLE WIRING DETAIL**

N.T.S.



**TYPICAL WIRING IN TRENCH DETAIL**

N.T.S.

FILE NAME =	USER NAME = gaglionobt
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DESIGNED -	REVISD - 08-08-03
DRAWN -	REVISD -
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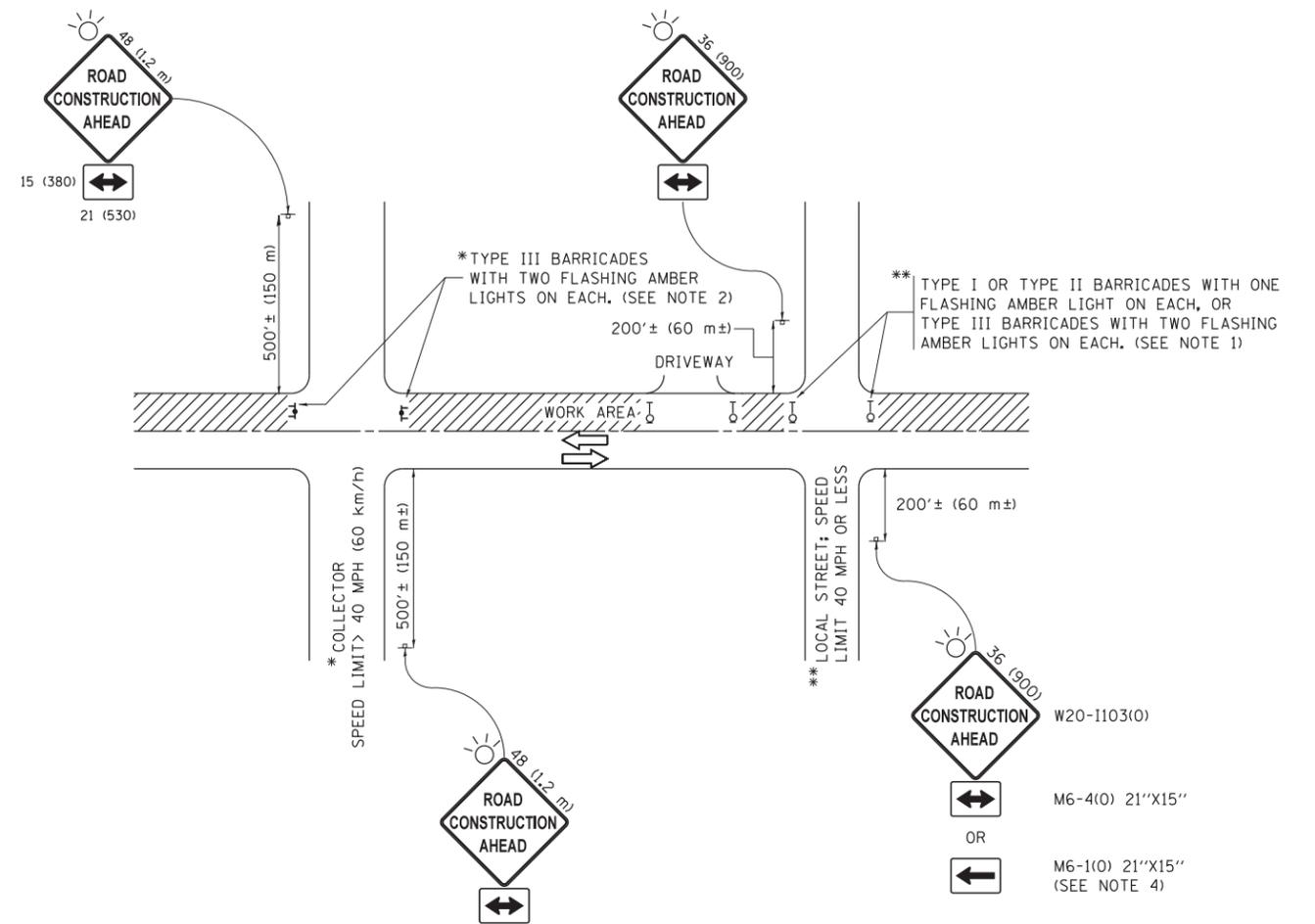
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DATE -	REVISD -

DESIGNED -	REVISD - 08-08-03
DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MISC. ELECTRICAL DETAILS SHEET A</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	24
<b>BE-702</b>		CONTRACT NO.	61F55	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

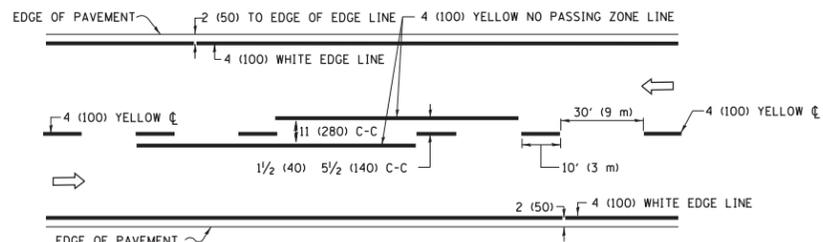
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

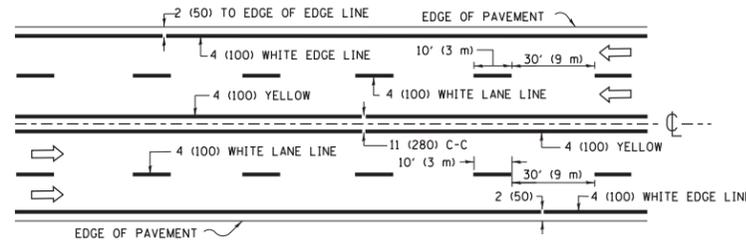
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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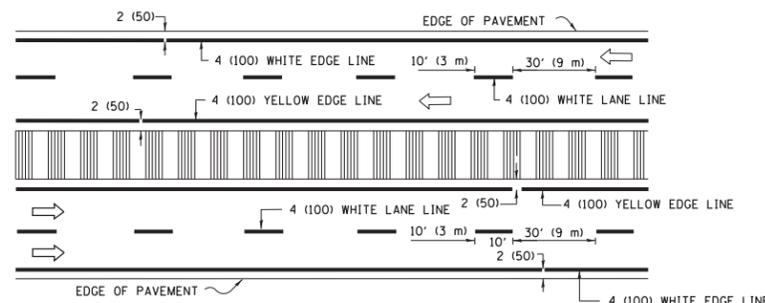
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<b>TC-10</b>			CONTRACT NO.	61F55
ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

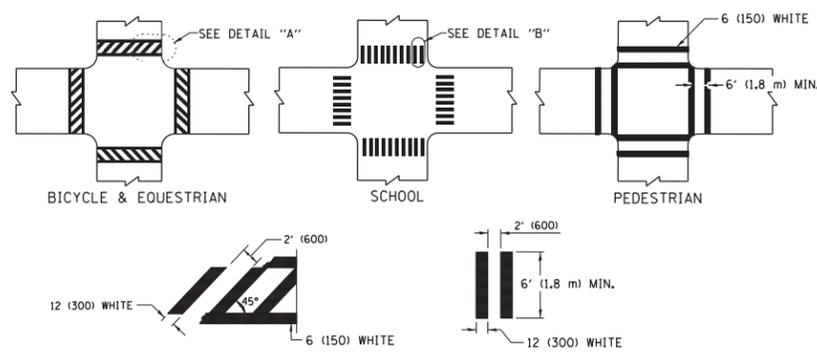


**MULTI-LANE UNDIVIDED**



**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

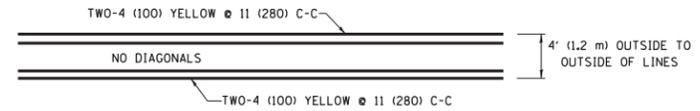


**DETAIL "A"**

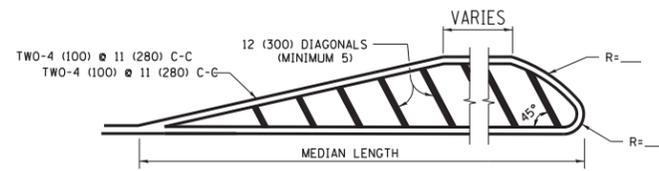
**DETAIL "B"**

**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

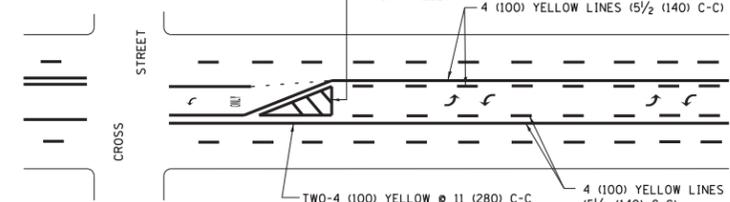


**4' (1.2 m) WIDE MEDIANS ONLY**

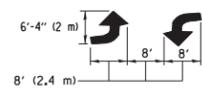


**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

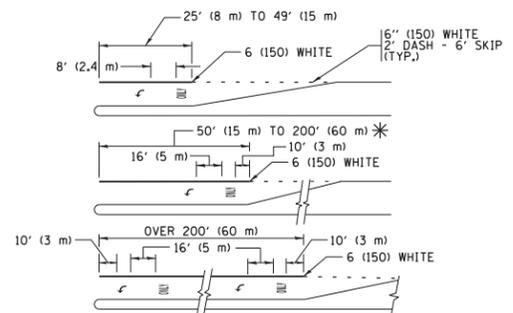


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

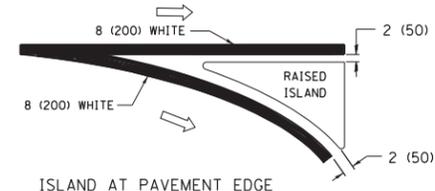
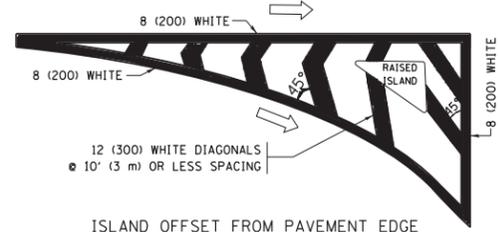


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

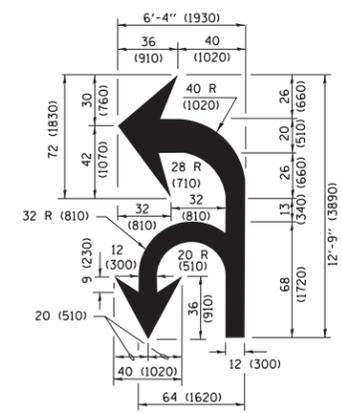
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

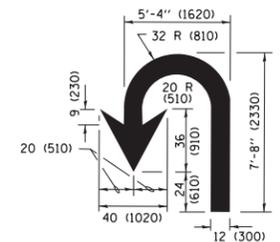
**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

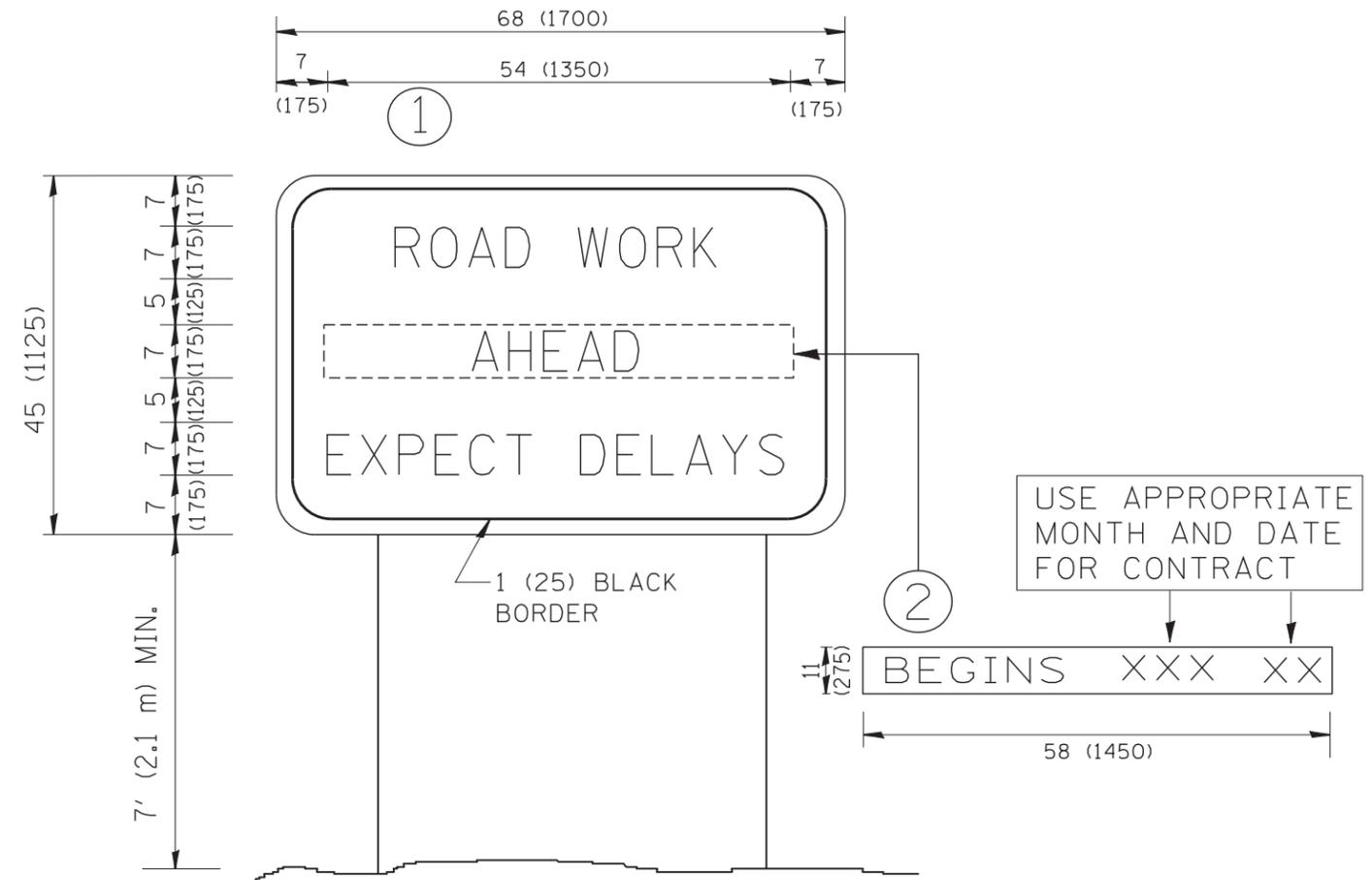
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	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	26
<b>TC-13</b>		CONTRACT NO.	61F55	
ILLINOIS FED. AID PROJECT				



**NOTES:**

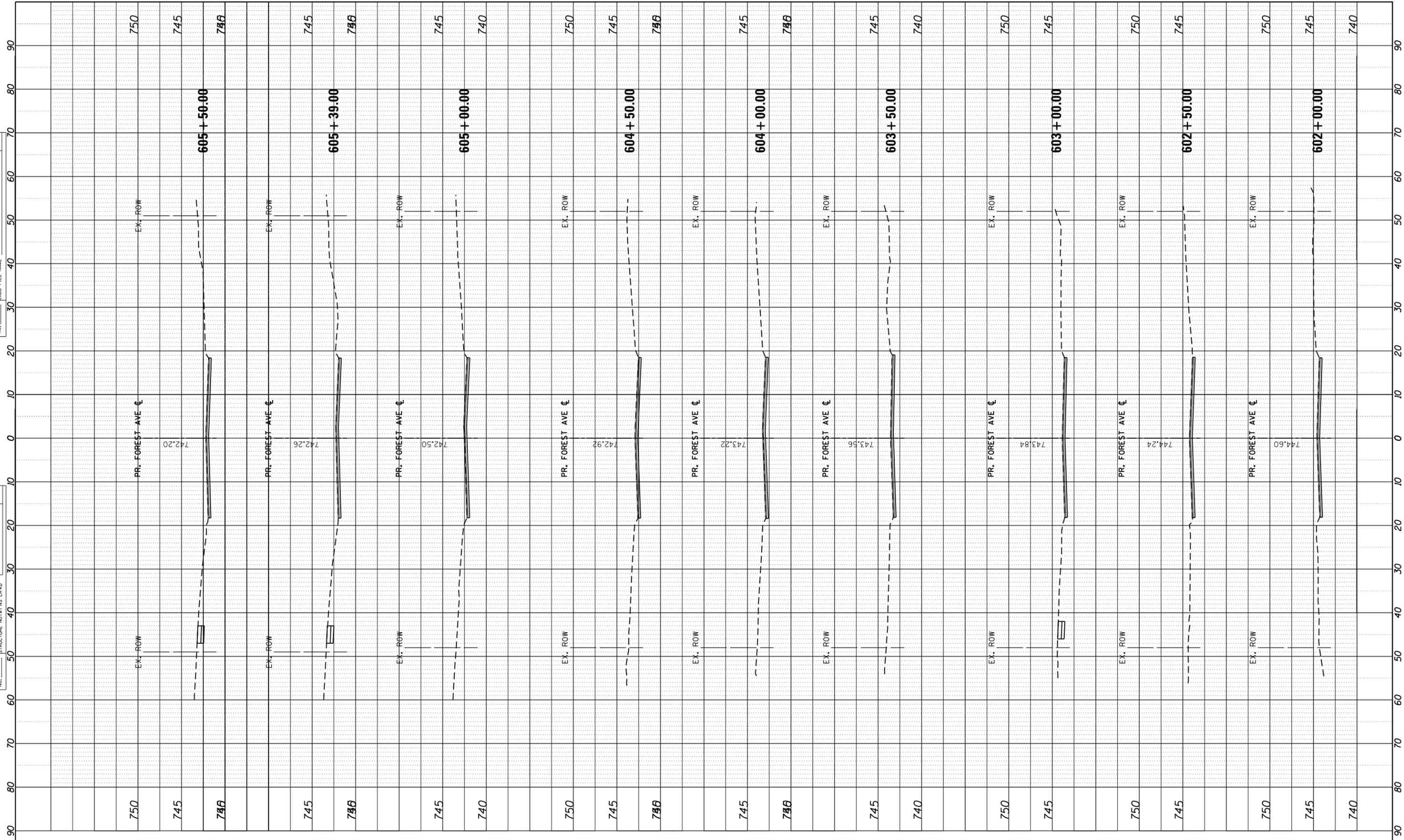
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>			MUN 5041	SECTION 18-00081-00-RS	COUNTY DuPAGE	TOTAL SHEETS 30	SHEET NO. 27
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PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT							

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	ALIGNMENT CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOT AT THIS CROSS SECTION		



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

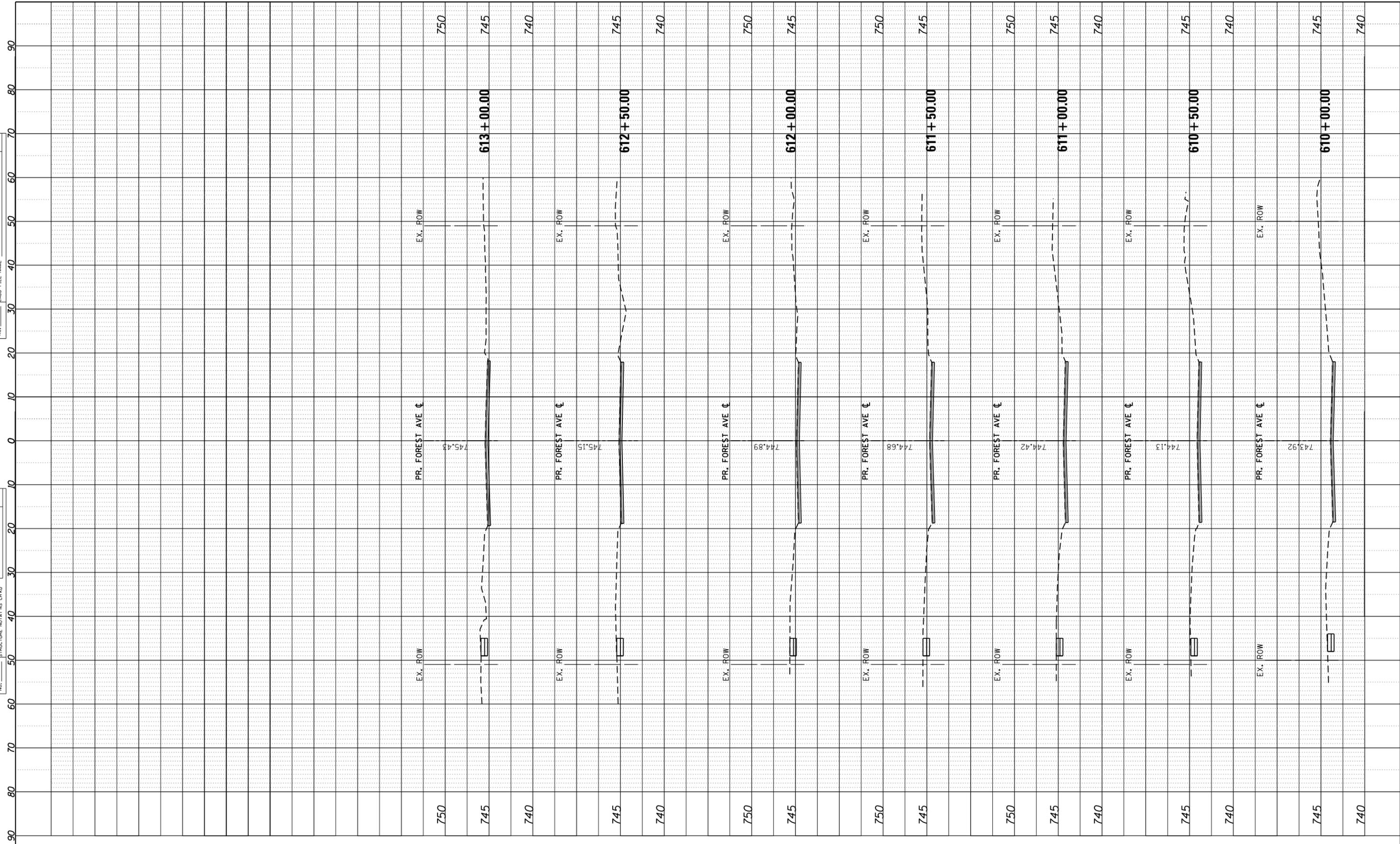
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5041	18-00081-00-RS	DuPAGE	30	28
CONTRACT NO. 61F55			ILLINOIS FED. AID PROJECT	



PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	ALIGNMENT CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE NOT AT THIS CHKD	



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61F55			ILLINOIS FED. AID PROJECT	