

129

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	1
CONTRACT NO. 62G51				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 311: US 34 OGDEN AVENUE
AT ST. JOSEPH CREEK (0.3 MI. E. OF IL 53)
SECTION: 10Y-BJR
FEDERAL PROJECT NUMBER: NHPP-3DRN(626)
BRIDGE DECK OVERLAY AND BRIDGE REPAIRS
DUPAGE COUNTY
C-91-231-18

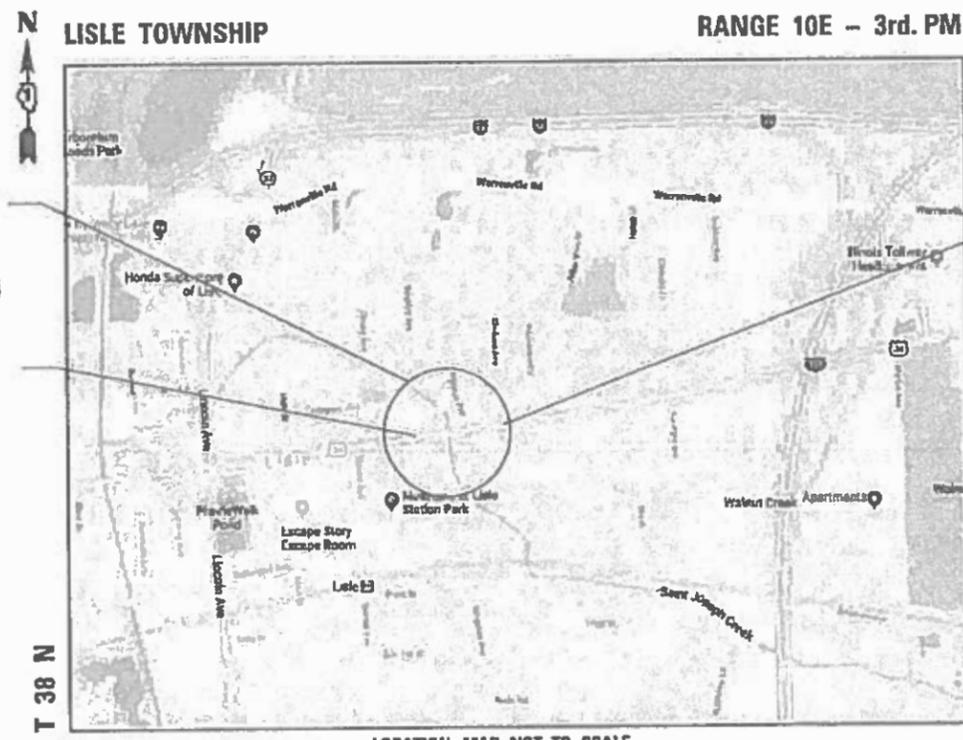
FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION:
OTHER PRINCIPAL ARTERIAL
FAP ROUTE 311: US 34 OGDEN AVENUE
2017 ADT=28,100
SPEED LIMIT 40 MPH

IMPROVEMENT LOCATED IN
THE VILLAGE OF LISLE



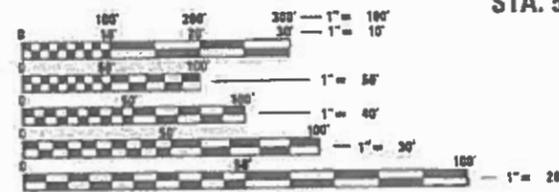
LOCATION OF SECTION INDICATED THUS: -



IMPROVEMENT LOCATION
US 34 (OGDEN AVENUE)
AT ST. JOSEPH CREEK
STRUCTURE NO: 022-0034

IMPROVEMENT BEGINS
STA. 5324 + 53

IMPROVEMENT ENDS
STA. 5348 + 82



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

GROSS LENGTH = 2429.0 FT. = 0.46 MILE
NET LENGTH = 2429.0 FT. = 0.46 MILE

LOCATION MAP NOT TO SCALE

STATE OF ILLINOIS
LICENSED PROFESSIONAL ENGINEER
JAY HOMEDI
062.046191

DATE SIGNED: 01/24/19
EXP. DATE: 11/30/19
SHEETS: 1-22, 28-34

STATE OF ILLINOIS
BRIDGE DESIGN LICENSED STRUCTURAL ENGINEER

DATE SIGNED: 01/24/19
EXP. DATE: 11/30/20
SHEETS: 23-27

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED January 29, 2019
Anthony D. D'Amico, Regional Engineer
March 29, 2019, E.A. Elk, Engineer of Design and Environment
March 29, 2019, Daniel P. Chirba, Director of Highways Project Implementation

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PROJECT MANAGER: MR. FAWAD AQUEEL, PE, PTOE (847) 705-4247

CONTRACT NO. 62G51

Accurate GROUP, INC.
WWW.ACCGI.COM
101 SCHELTER RD., SUITE B-200
LINCOLNSHIRE, ILLINOIS 60069
T (847) 613-1100 F (847) 613-110
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 104.0020 3

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HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600 MM) FROM PAVEMENT EDGE
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS- DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTORS MOUNTING DETAILS
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION

COMMITMENTS

NONE

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- MEADE ELECTRIC COMPANY, THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR, LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES MAY BE AT THE CONTRACTOR'S EXPENSE.
- IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, USGS, THE VILLAGE OF LISLE, OTHER LOCAL GOVERNMENT AGENCIES, AND IDOT.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- PRIOR TO BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THE MARKINGS CAN BE RE-ESTABLISHED. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, ARTERIAL TRAFFIC FIELD ENGINEER, AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A USACE PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO SECURE AND COMPLY WITH A USACE PERMIT FOR CONTRACTOR'S ACTIVITIES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- ANY SIGNAGE, PAVEMENT MARKINGS AND REFLECTORS DAMAGED DURING CONSTRUCTION OUTSIDE THE REMOVAL LINES SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
- THE CENTERLINE IS FOR INFORMATION ONLY.

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USER NAME = JENT	DESIGNED - LRC	REVISED -
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PLOT DATE = 2/26/2019	DATE - 12/07/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE HIGHWAY STANDARDS
GENERAL NOTES AND COMMITMENTS
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	2
CONTRACT NO.				62G51
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONSTR. CODE	80% FED 20% STATE BRIDGE 0059 S.N. 022-0034
28000510	INLET FILTERS	EACH	17		17
50102400	CONCRETE REMOVAL	CU YD	1.8		1.8
50300255	CONCRETE SUPERSTRUCTURE	CU YD	1.8		1.8
50300260	BRIDGE DECK GROOVING	SQ YD	591		591
50300300	PROTECTIVE COAT	SQ YD	670		670
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6		6
67100100	MOBILIZATION	L SUM	1		1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56		56
70300900	PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS	SQ FT	147		147
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	16156		16156
70300906	PAVEMENT MARKING TAPE, TYPE IV 6"	FOOT	254		254
70300924	PAVEMENT MARKING TAPE, TYPE IV 24"	FOOT	114		114
70400100	TEMPORARY CONCRETE BARRIER	FOOT	250		250
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	187.5		187.5

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONSTR. CODE	80% FED 20% STATE BRIDGE 0059 S.N. 022-0034
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2		2
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2		2
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	197		197
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4768		4768
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	905		905
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	165		165
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	63		63
* 78004110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE C - LINE 4"	FOOT	60		60
* 78004130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE C - LINE 6"	FOOT	30		30
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	432		432
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	32		32
* 78100300	REPLACEMENT REFLECTOR	EACH	212		212
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	30		30
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	8		8

* SPECIALTY ITEM

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USER NAME = johnn	DESIGNED - LRC	REVISED -
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PLOT DATE = 1/23/2019		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	3
CONTRACT NO. 62G51			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONSTR. CODE	
				80% FED	20% STATE
				BRIDGE	
				0059	
				S.N. 022-0034	
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1	1	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	120	120	
X0326806	WASHOUT BASIN	L SUM	1	1	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2761	2761	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	5659	5659	
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	45	45	
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	212	212	
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	60	60	
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	30	30	
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	20	20	
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	612	612	
Z0012146	BRIDGE DECK SCARIFICATION 2 3/4"	SQ YD	612	612	

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONSTR. CODE	
				80% FED	20% STATE
				BRIDGE	
				0059	
				S.N. 022-0034	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	492	492	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	44	44	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	27	27	
Z0021904	SILICONE JOINT SEALER, 1"	FOOT	240	240	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4	
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1	

* SPECIALTY ITEM

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USER NAME = john	DESIGNED - LRC	REVISED -
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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	4
CONTRACT NO.			62G51	
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1. THE PERMANENT TRAFFIC CONTROL DEPICTED HERE IN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS AS SHOWN IN THE INDEX OF SHEETS AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL) UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS OR SPECIAL PROVISIONS.
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
3. ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
5. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER 7 DAYS OF SERVICE SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR THE INITIAL PLACEMENT AND A ONE-TIME REPLACEMENT HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO 7 DAYS OF SERVICE OR REPLACEMENT AFTER THE INITIAL REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING LABOR, SIGNS AND TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC UNLESS NOTED OTHERWISE IN THE SPECIAL PROVISIONS.
7. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3" x 6" DELINEATOR INSTALLED. COST OF THE DELINEATOR IS INCLUDED IN TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
8. WORK ZONE SPEED LIMIT SHALL BE 35 MPH ON US 34 (OGDEN AVE.).
9. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
10. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
11. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
12. THE CONTRACTOR SHALL PLACE ONE (1) CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH APPROPRIATE INFORMATION SHALL BE PLACED TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAY, "CHANGEABLE MESSAGE SIGN".
13. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE 12' THRU-LANE IN EACH DIRECTION FOR TWO-WAY TRAFFIC FLOW UNLESS OTHERWISE SPECIFIED.
14. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
15. PLACE TEMPORARY DRIVEWAY ENTRANCE SIGNING IN ACCORDANCE WITH IDOT DISTRICT ONE HIGHWAY STANDARD TC-26 - DRIVEWAY ENTRANCE SIGNING.
16. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN.
17. TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATORS SHALL BE PLACED AS INDICATED IN THE PLANS. FURNISHING, INSTALLING AND RELOCATING TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATORS SHALL BE IN ACCORDANCE WITH IDOT SPECIAL PROVISIONS, IDOT HIGHWAY STANDARDS, STANDARD SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
18. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION.
19. TEMPORARY CONCRETE BARRIER WALL SHALL BE CONTINUOUSLY PINNED TO THE PAVEMENT IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION WHERE A 3.5 FOOT CLEAR ZONE FREE FROM DROP-OFFS, FIXED OBJECTS, OR OTHER OBSTACLES CANNOT BE PROVIDED BEHIND THE WALL.
20. THE CONTRACTOR SHALL MAINTAIN THE EXISTING SIDEWALK ON AT LEAST ONE SIDE OF THE ROAD AT ALL TIMES. ANY SIDEWALKS OR PATHS THAT NEED TO BE CLOSED SHALL BE CLOSED IN ACCORDANCE WITH HIGHWAY STANDARD 701801.

SEQUENCE OF CONSTRUCTION

THE FOLLOWING SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC IS SUGGESTED. VARIATIONS MAY BE MADE WITH THE APPROVAL OF THE ENGINEER.

FOR EACH STAGE OF CONSTRUCTION, PROVIDE TRAFFIC CONTROL AS SHOWN ON THE SUGGESTED MAINTENANCE OF TRAFFIC PLANS. COORDINATE INSTALLATION OF TEMPORARY PAVEMENT MARKINGS AND OTHER TRAFFIC CONTROL DEVICES WITH THE EXISTING TRAFFIC PATTERNS AT THE ENDS OF THE PROJECT.

PRE-STAGE I

1. INSTALL EROSION CONTROL DEVICES AS PER EROSION CONTROL PLANS.

STAGE I

1. INSTALL STAGE I TRAFFIC CONTROL ON US 34 (OGDEN AVE) AND TEMPORARY PAVEMENT MARKINGS INCLUDING WIDTH RESTRICTION SIGNAGE ON IL 53 (LINCOLN AVE) AND I 355 EXIT RAMP TO US 34 (OGDEN AVE) AS PER MAINTENANCE OF TRAFFIC PLANS.
2. SHIFT TRAFFIC TO THE NORTH SIDE OF THE EXISTING ROADWAY UTILIZING THE WESTBOUND LANES ON US 34 (OGDEN AVE) (MINIMUM ONE 12' THRU LANE IN EACH DIRECTION).
3. PERFORM THE BRIDGE DECK SCARIFICATION, BRIDGE DECK AND APPROACH SLAB REPAIRS, CLEAN AND RESEAL THE PAVEMENT RELIEF JOINT AND PERFORM CONCRETE LATEX OVERLAY OF THE EASTBOUND LANES.

STAGE II

1. REMOVE STAGE I TRAFFIC CONTROL AND PLACE STAGE II TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS PER MAINTENANCE OF TRAFFIC PLANS.
2. SHIFT TRAFFIC TO THE SOUTH ON THE EASTBOUND LANES OF US ROUTE 34 (MINIMUM ONE 12' THRU LANE IN EACH DIRECTION).
3. PERFORM THE BRIDGE DECK SCARIFICATION, BRIDGE DECK AND APPROACH SLAB REPAIRS, CLEAN AND RESEAL THE PAVEMENT RELIEF JOINT AND PERFORM CONCRETE LATEX OVERLAY OF THE WESTBOUND LANES.

STAGE III

1. REMOVE STAGE II TRAFFIC CONTROL DEVICES AND FULLY REOPEN THE ROAD TO TRAFFIC.
2. INSTALL PERMANENT PAVEMENT MARKING AND REMOVE EROSION CONTROL DEVICES.

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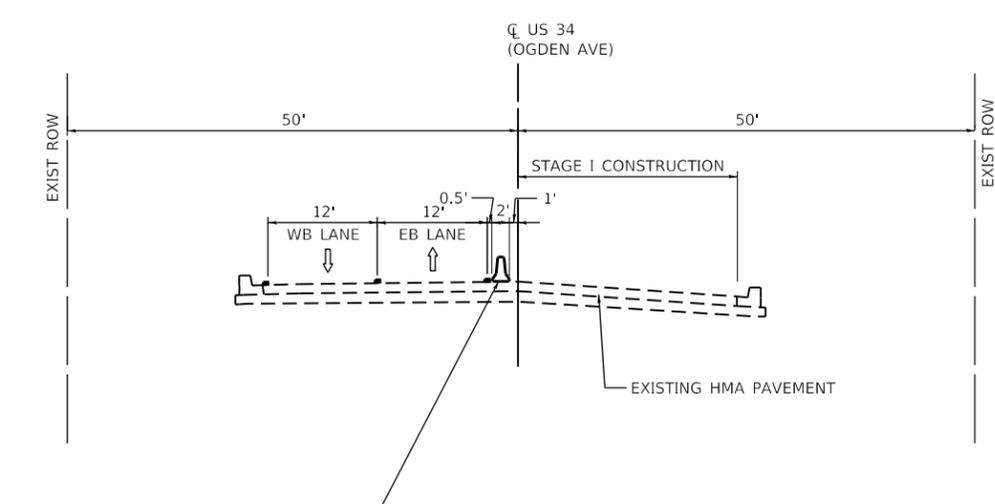
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTAINENCE OF TRAFFIC GENERAL NOTES
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

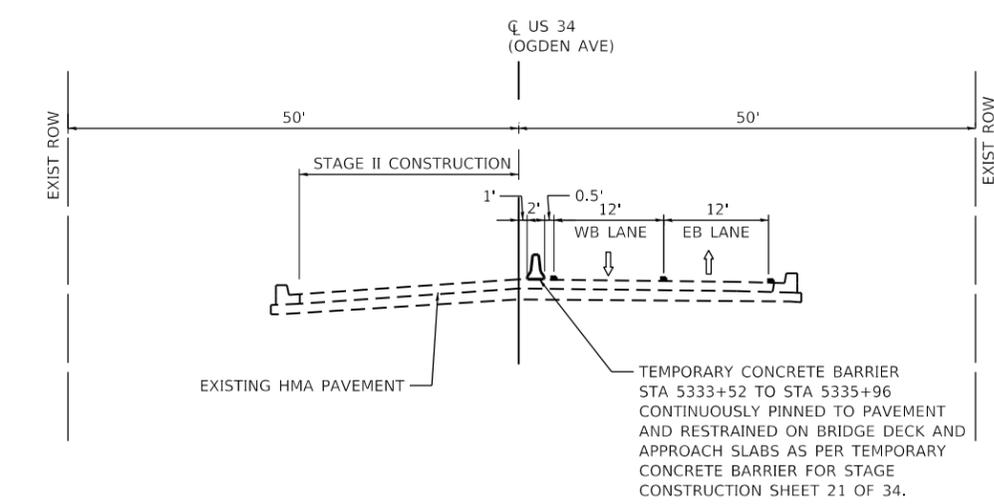
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	5
			CONTRACT NO.	62G51
ILLINOIS FED. AID PROJECT				

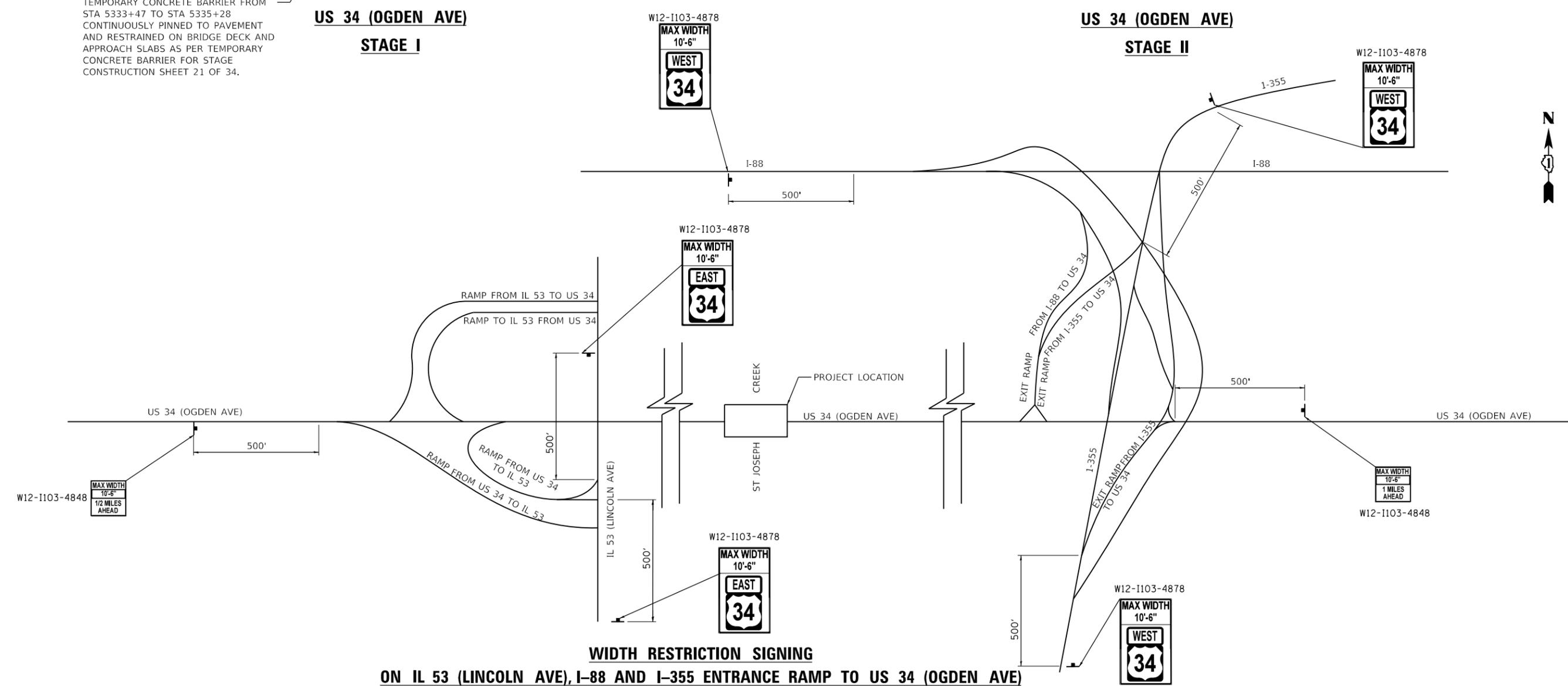


TEMPORARY CONCRETE BARRIER FROM STA 5333+47 TO STA 5335+28 CONTINUOUSLY PINNED TO PAVEMENT AND RESTRAINED ON BRIDGE DECK AND APPROACH SLABS AS PER TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION SHEET 21 OF 34.

**US 34 (OGDEN AVE)
STAGE I**



**US 34 (OGDEN AVE)
STAGE II**



**WIDTH RESTRICTION SIGNING
ON IL 53 (LINCOLN AVE), I-88 AND I-355 ENTRANCE RAMP TO US 34 (OGDEN AVE)**

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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

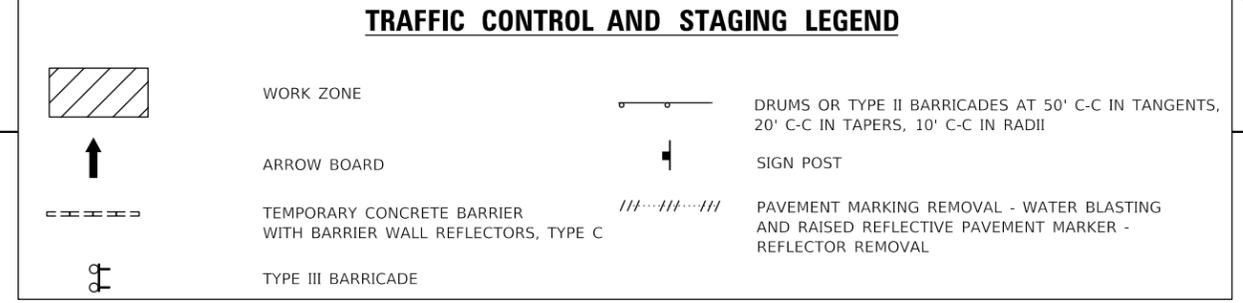
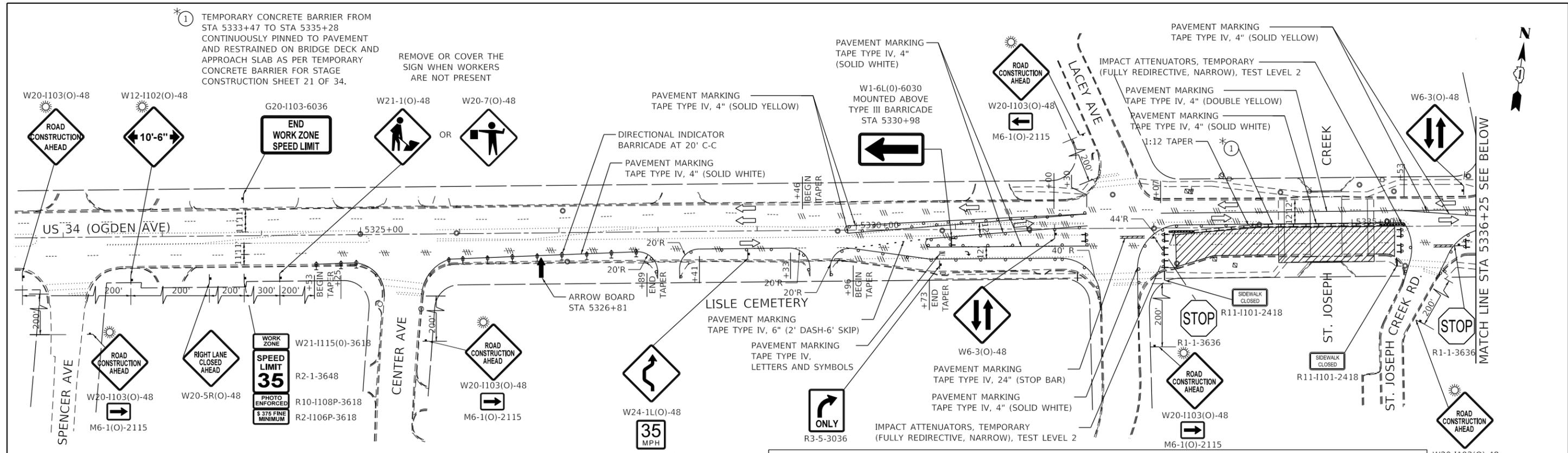
**MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

F.A.P. RTE. 311	SECTION 10Y-BJR	COUNTY DUPAGE	TOTAL SHEETS 34	SHEET NO. 6
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

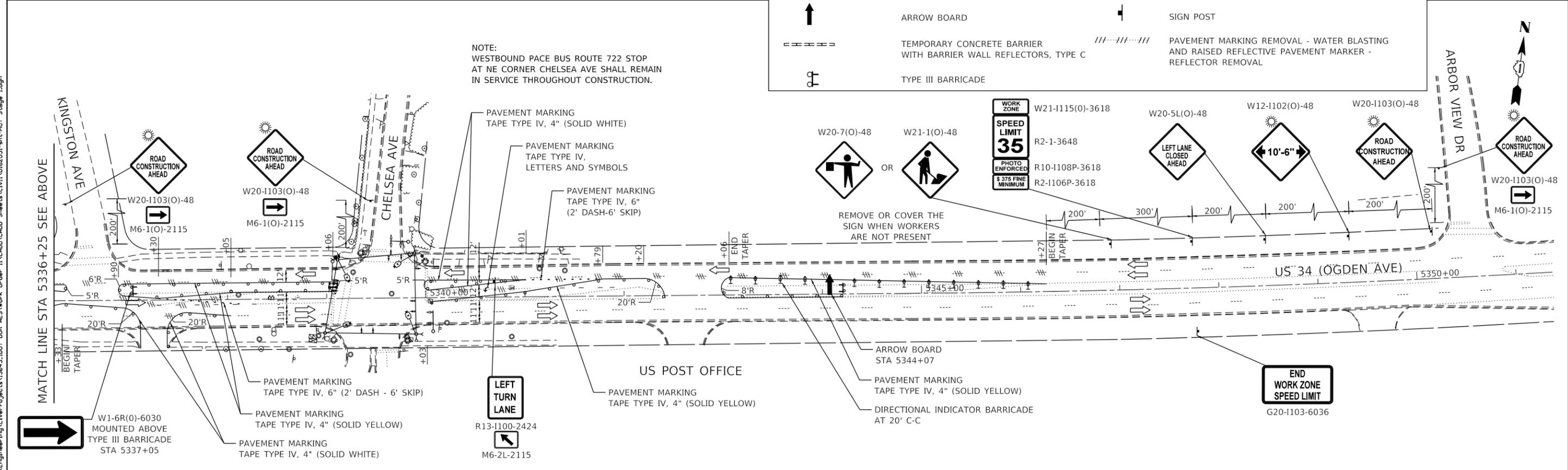
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① TEMPORARY CONCRETE BARRIER FROM STA 5333+47 TO STA 5335+28 CONTINUOUSLY PINNED TO PAVEMENT AND RESTRAINED ON BRIDGE DECK AND APPROACH SLAB AS PER TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION SHEET 21 OF 34.

REMOVE OR COVER THE SIGN WHEN WORKERS ARE NOT PRESENT



NOTE:
WESTBOUND PACE BUS ROUTE 722 STOP AT NE CORNER CHELSEA AVE SHALL REMAIN IN SERVICE THROUGHOUT CONSTRUCTION.



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PLLOT SCALE = 100.0000' / 1\"/>			
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

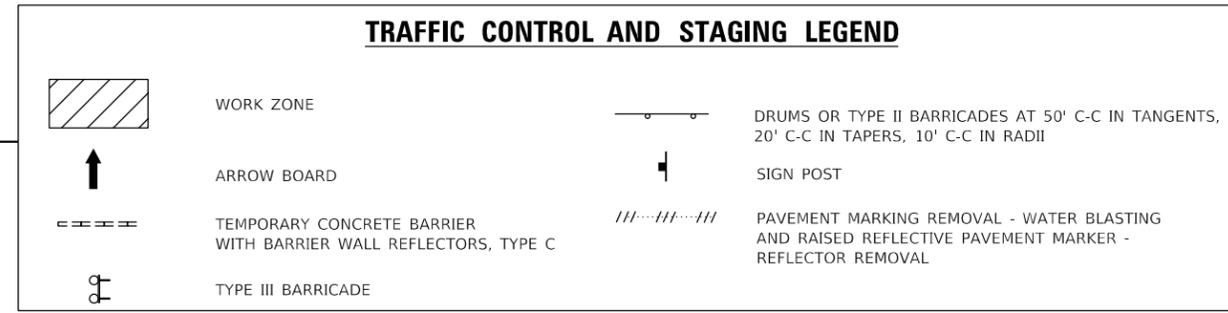
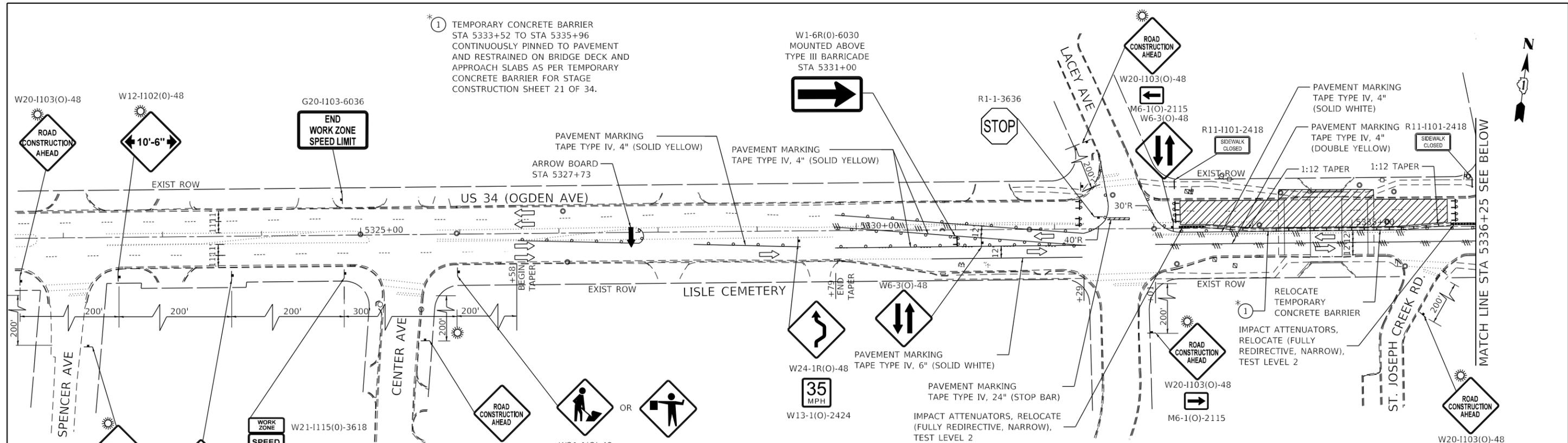
MAINTENANCE OF TRAFFIC STAGE I US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 311	SECTION 10Y-BJR	COUNTY DUPAGE	TOTAL SHEETS 34	SHEET NO. 7
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

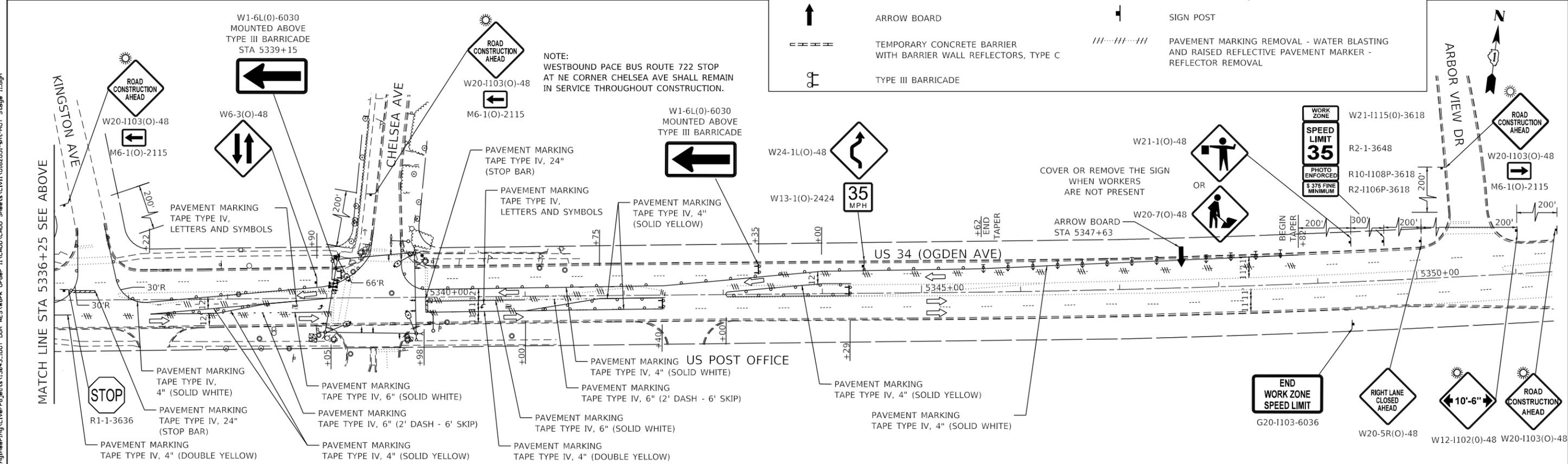
FILE NAME = Q:\Engineering\LiveProjects\13043.IDOT DUR AES\Work D-dur-11\CADD\CADD Sheets\Cv\1\0162051-shr-MOT Stage 1.dgn

* 1 TEMPORARY CONCRETE BARRIER
 STA 5333+52 TO STA 5335+96
 CONTINUOUSLY PINNED TO PAVEMENT
 AND RESTRAINED ON BRIDGE DECK AND
 APPROACH SLABS AS PER TEMPORARY
 CONCRETE BARRIER FOR STAGE
 CONSTRUCTION SHEET 21 OF 34.

W1-6R(O)-6030
 MOUNTED ABOVE
 TYPE III BARRICADE
 STA 5331+00



NOTE:
 WESTBOUND PACE BUS ROUTE 722 STOP
 AT NE CORNER CHELSEA AVE SHALL REMAIN
 IN SERVICE THROUGHOUT CONSTRUCTION.



FILE NAME = Q:\Engineering\Live\Projects\13843.IDOT DUR AES\Work D-der_11\CADD\CADD Sheets\Cv\1\0162051-shr-MOT Stage II.dgn



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	DATE - 12/07/2018	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC STAGE II
 US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 311	SECTION 10Y-BJR	COUNTY DUPAGE	TOTAL SHEETS 34	SHEET NO. 8
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL NOTES:

- ALL CONTROL MEASURES NECESSARY MUST MEET THE MINIMUM REQUIREMENTS AS DESCRIBED IN THE LATEST EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. ADDITIONAL DETAILS AND BMPs ARE ALSO AVAILABLE AND CAN BE UTILIZED AS SHOWN IN THE ILLINOIS URBAN MANUAL, REVISED TO LATEST VERSION AS AMENDED. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE: (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).
- ALL THE SOIL EROSION AND SEDIMENT CONTROL FEATURES MUST BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. SOIL DISTURBANCE MUST BE PHASED OR ENACTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES MUST CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND/OR PERMANENT MEASURES.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND THE INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER AND WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
- ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITIES.
- DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION ACTIVITY, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN ONE (1) CALENDAR DAY.
- THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREA AS THE PROJECT PROGRESSES AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE EARTHEN MATERIAL TO THE SATISFACTION OF THE ENGINEER OR AUTHORIZED IDOT PERSONNEL.
- THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION. THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES TO BE RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS THROUGHOUT THE PROJECT.
- THE CONTRACTOR'S REPRESENTATIVE HAS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES AND HAVE TAKEN AN ILLINOIS DEPARTMENT OF TRANSPORTATION OR APPROVED EQUAL EROSION AND SEDIMENT CONTROL COURSE. THIS PERSON SHALL HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTION CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN PROVIDED BY THE ENGINEER. THIS INDIVIDUAL AND THE ENGINEER MUST MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN DAYS OF THE FOLLOWING:
 - DISTURBED AREAS OF THE PROJECT SITE THAT HAVE NOT BEEN FULLY STABILIZED.
 - STRUCTURAL CONTROL MEASURES (SUCH AS PERIMETER EROSION BARRIER, ETC.)
 - LOCATIONS WHERE VEHICLES ENTER OR EXIT THE PROJECT SITE.
 - AN ADDITIONAL INSPECTION OF THE ITEMS LISTED ABOVE MUST BE MADE WITHIN 24-HOURS AFTER A 24-HOUR RAINFALL OR EQUIVALENT SNOWFALL EVENT GREATER THAN 0.5-INCH. DURING WINTER MONTHS, ALL MEASURES MUST BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
- ALL THE EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED DURING THE CONSTRUCTION SEASON, AS WELL AS OVER THE WINTER SHUTDOWN PERIOD AND OTHER DAYS WHEN THE PROJECT IS CLOSED DOWN FOR A LONGER DURATION. ANY CONTROL MEASURES FILLED MORE THAN 75% MUST BE CLEANED AND RESET AND THESE SPOILS REMOVED TO AN APPROVED SITE.
- THE CONTRACTOR'S REPRESENTATIVE AND THE ENGINEER MUST KEEP A WRITTEN REPORT SUMMARIZING THE REQUIRED INSPECTIONS. THE REPORTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION. THE REPORT MUST ALSO BE RETAINED FOR THREE YEARS FROM THE DATE THE SITE IS FINALLY STABILIZED.
- ANY SEDIMENT LADEN DEWATERING DISCHARGE MUST BE DIRECTED TO AN APPROVED SEDIMENT TRAPPING CONTROL MEASURE PRIOR TO RELEASE FROM THE PROJECT SITE.
- NO WORK IS ALLOWED BEYOND THE PERMITTED AREA. ANY WORK WITHIN A CREEK OR DITCH CAPABLE OF CONVEYING WATER MUST BE CONDUCTED IN THE DRY. PROVISIONS MUST BE MADE TO BYPASS PUMP OR DEWATER ANY AREAS IN WHICH WORK WILL BE CONDUCTED. IN HIGH FLOW CHANNELS WHERE DEWATERING IS NOT POSSIBLE OR PRACTICAL, SILT FENCE OR SEDIMENT CURTAINS MAY BE INSTALLED PARALLEL TO THE STREAM BANK. IN NO CASE WILL THE CURTAINS BE INSTALLED PERPENDICULAR TO THE FLOW. DEWATERING MUST BE DISCHARGED TO A STABLE, NON-ERODIBLE SURFACE AND IN-STREAM WORK BARRIERS MUST BE COMPOSED OF NON-ERODIBLE MATERIAL.
- THE CONTRACTOR MUST COOPERATE WITH THE ENGINEER AND HIS/HER REPRESENTATIVE WHO WILL MAKE SITE VISITS TO REVIEW THE COMPLIANCE OF THE PLANS IN THE FIELD AND AUDIT IF NECESSARY. THE CONTRACTOR MUST PREPARE THE LOGS AND RECORDS WHEN REQUIRED AND SUBMIT TO IDOT AND/OR APPROPRIATE AGENCIES.
- STABILIZATION MEASURES SHALL BE INITIATED IMMEDIATELY WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN ONE (1) DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF FOURTEEN (14) OR MORE CALENDAR DAYS.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- STABILIZATION MEASURES SHALL BE INITIATED IMMEDIATELY WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN ONE (1) DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF FOURTEEN (14) OR MORE CALENDAR DAYS.
- EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE ENGINEER.

SOIL PROTECTION SCHEDULE:

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUN.	JUL.	AUG.	SEP.	OCT.	NOV.	DEC.
PERMANENT SEEDING						→				→		
DORMANT SEEDING	→		→							→		→
TEMPORARY SEEDING										→		
EROSION BLANKET / HYDROMULCH											→	

SOIL EROSION AND SEDIMENT CONTROL STRATEGY:

- ERECT EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN ON THE PLANS.
- INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES FOR THE DURATION OF CONSTRUCTION.
- TEMPORARY STABILIZATION OF EACH STAGE SHOULD BE COMPLETED BEFORE WORK BEGINS ON SUBSEQUENT STAGES.
- STABILIZE DISTURBED AREAS WITH TEMPORARY EROSION CONTROL MEASURES. THESE TEMPORARY EROSION CONTROL MEASURES WILL NOT BE PAID FOR SEPARATELY, BUT ARE INCLUDED IN THE COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION UNLESS SPECIFIED OTHERWISE.
- WHEN THE PERMANENT STABILIZATION IS ESTABLISHED, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.

HIGHWAY STANDARD

STD. NO.	TITLE
280001	TEMPORARY EROSION CONTROL SYSTEMS

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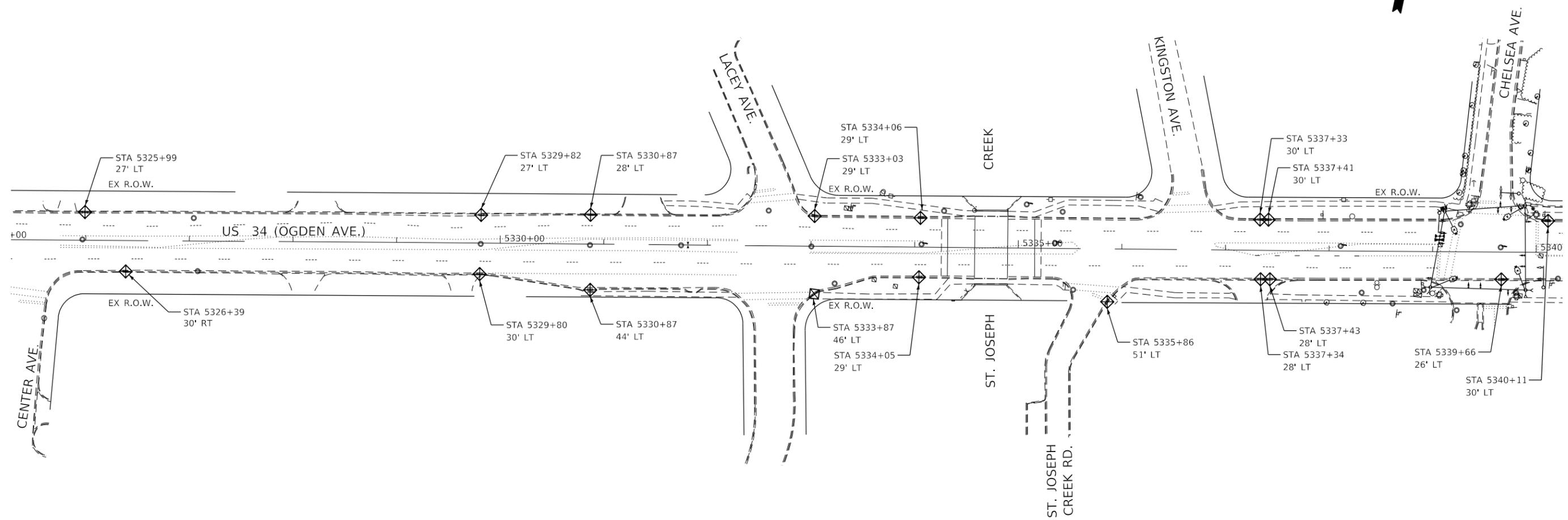
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PLOT SCALE = 2.0000' / in.	CHECKED - LRC	REVISED -
PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION AND SEDIMENT CONTROL NOTES
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	9
CONTRACT NO.			62G51	
ILLINOIS FED. AID PROJECT				



◆ INLET FILTERS

NOTE

1. DIRECT OR INDIRECT PUMPING OF SEDIMENT LADEN WATER INTO ST. JOSEPH CREEK OR STORM WATER FACILITY WITHOUT FILTRATION IS PROHIBITED.

FILE NAME = Q:\Engineering\LiveProjects\13043.IDOT DUR AES\Work Order\11\CADD\CADD Sheets\Cvt1\0162051-shr-Eros Ctr.Ldgn



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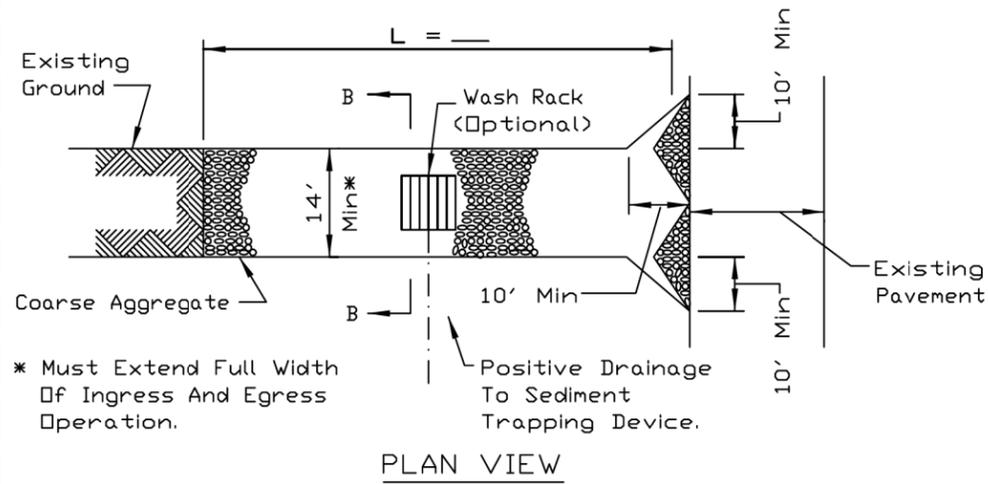
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION AND SEDIMENT CONTROL PLANS
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

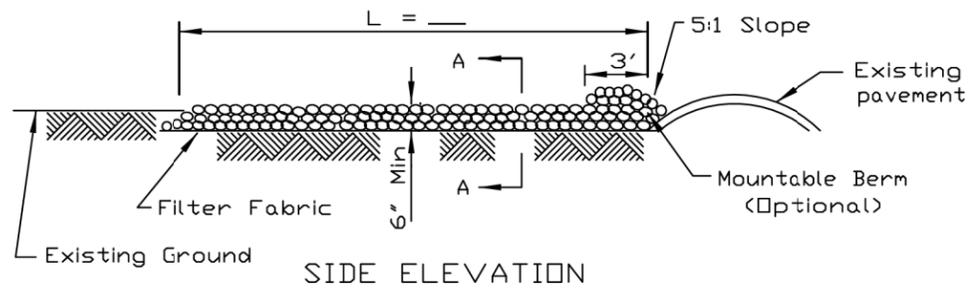
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311	10Y-BJR	DUPAGE	34	10
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

STABILIZED CONSTRUCTION ENTRANCE PLAN



PLAN VIEW



SIDE ELEVATION

NOTES:

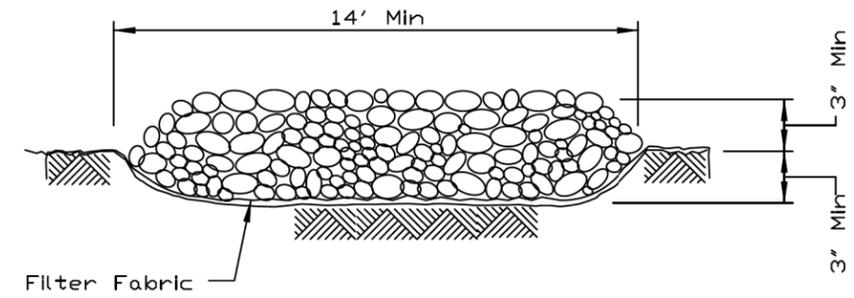
1. Filter fabric shall meet the requirements of material specification 592 GEOTEXTILE, Table I or 2, Class I, II or IV and shall be placed over the cleared area prior to the placing of rock.
2. Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
3. Any drainage facilities required because of washing shall be constructed according to manufacturers specifications.
4. If wash racks are used they shall be installed according to the manufacturer's specifications.

REFERENCE	Project	_____
	Designed	_____ Date _____
	Checked	_____ Date _____
	Approved	_____ Date _____

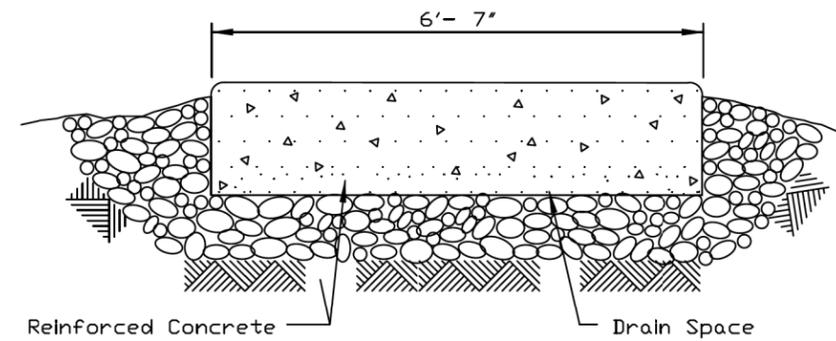


STANDARD DWG. NO.	IL-630
SHEET	1 OF 2
DATE	8-18-94

STABILIZED CONSTRUCTION ENTRANCE PLAN



SECTION A-A



SECTION B-B

REFERENCE	Project	_____
	Designed	_____ Date _____
	Checked	_____ Date _____
	Approved	_____ Date _____



STANDARD DWG. NO.	IL-630
SHEET	2 OF 2
DATE	8-18-94

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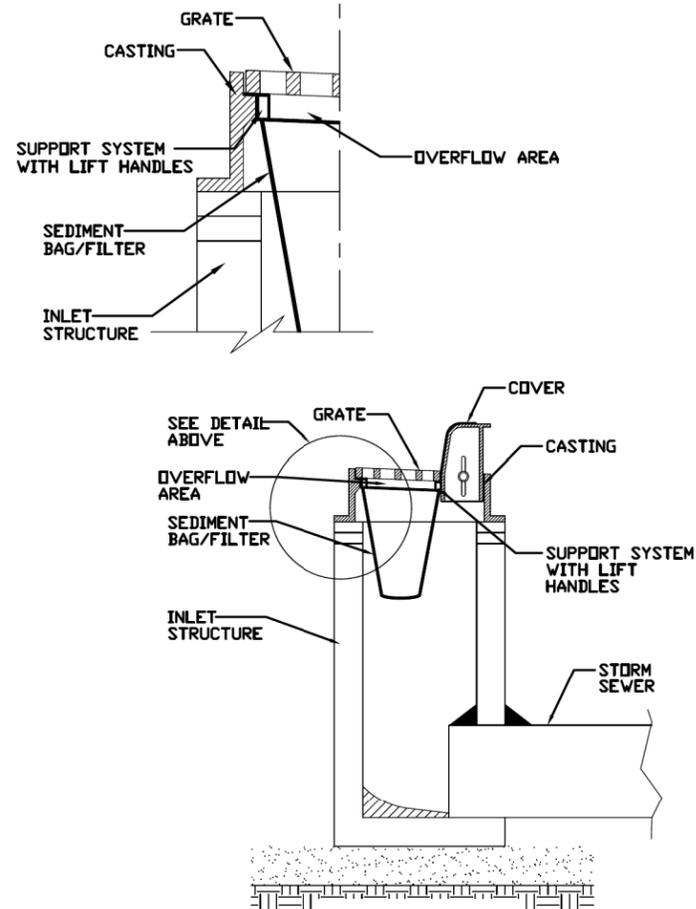
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL DETAILS - I
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

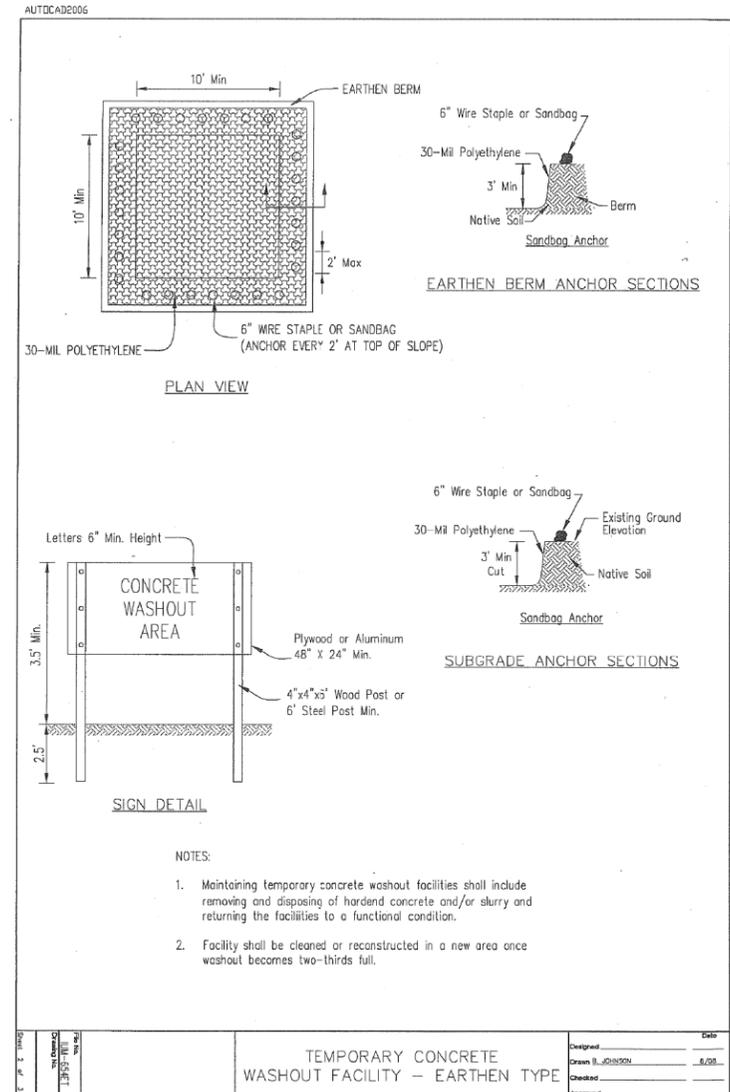
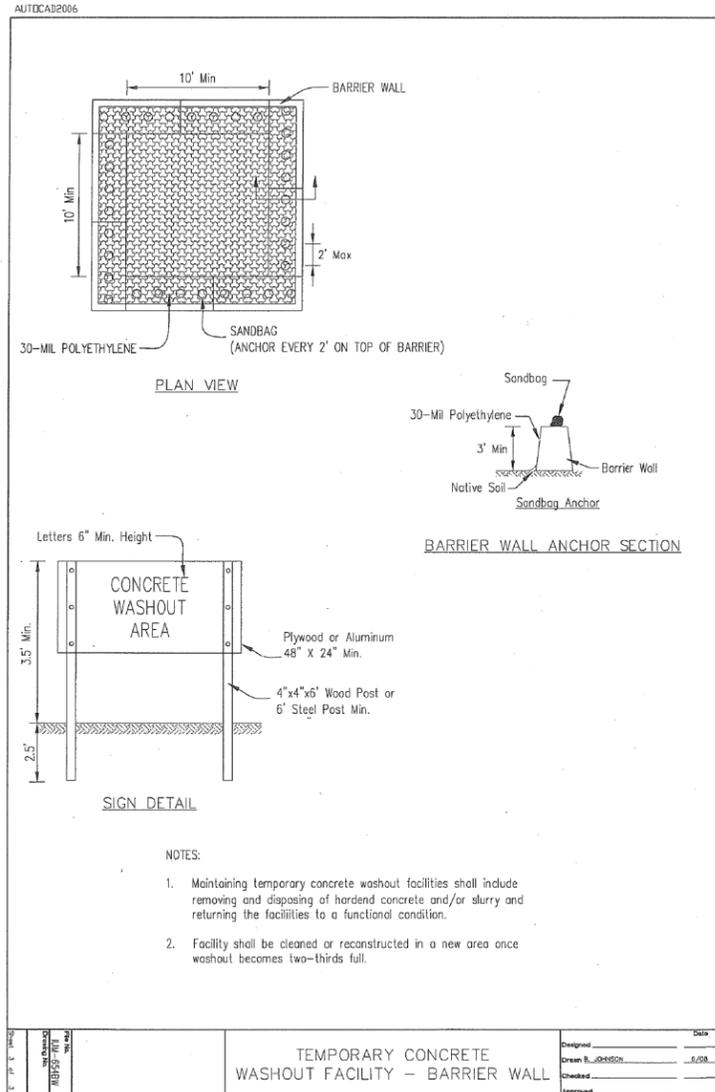
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	11
			CONTRACT NO. 62G51	
ILLINOIS FED. AID PROJECT				

INLET PROTECTION - PAVED AREAS DROP-IN PROTECTION



REFERENCE	STANDARD DWG. NO.
Project _____	IUM-561D
Designed _____ Date _____	SHEET 1 OF 1
Checked _____ Date _____	DATE 01-11-11
Approved _____	



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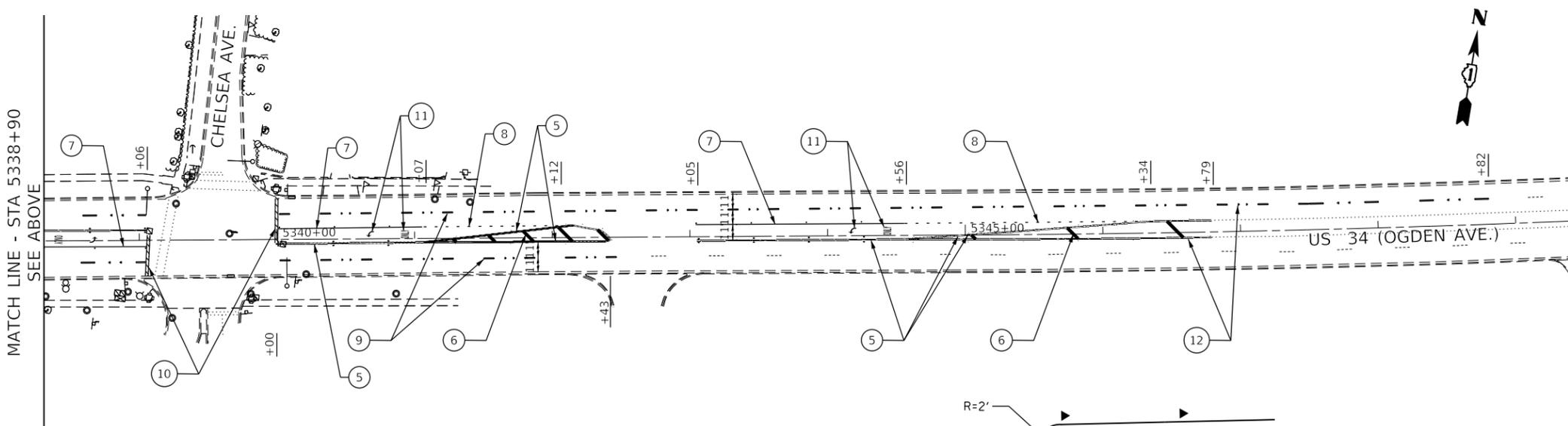
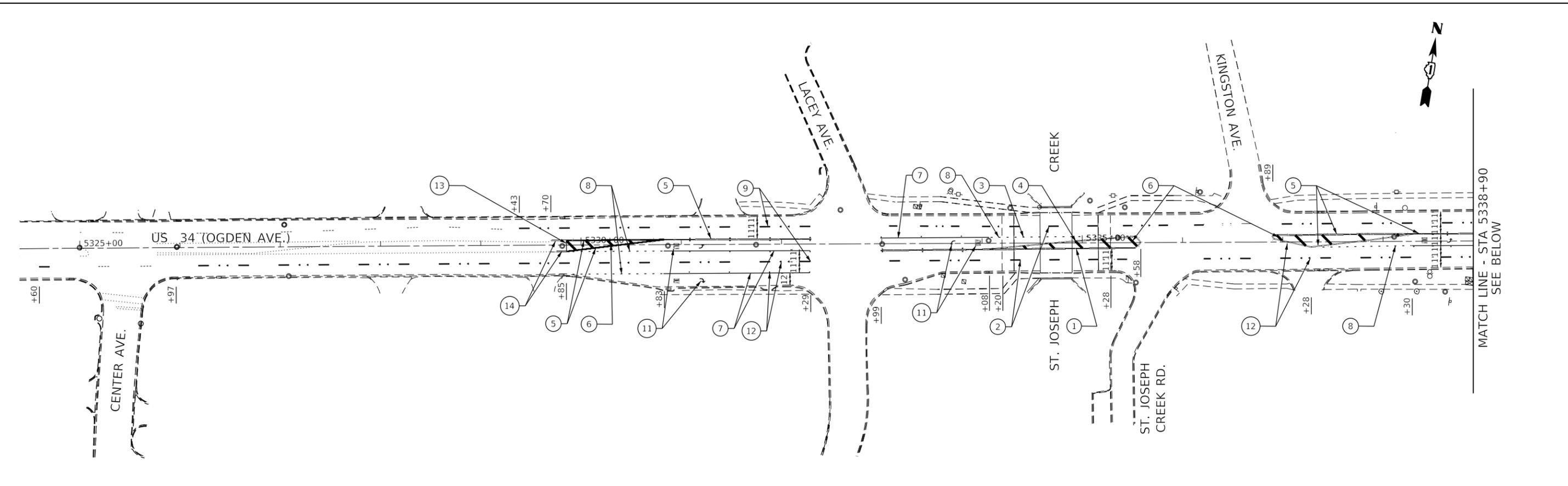
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL DETAILS - II
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	12
CONTRACT NO.				62G51
ILLINOIS FED. AID PROJECT				

FILE NAME = D:\Engineering\LiveProjects\13043.IDOT DUR AES\Work D-rdr 11\CADD\CADD Sheets\Civil\0162051-shr-PMK 1.dgn



PAVEMENT MARKING LEGEND

- 1 MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW) AT 11" C-C
- 2 RECESSED PREFORMED PLASTIC PAVEMENT MARKING TYPE C - LINE 4" (WHITE - 10' LINE, 30' SPACE)
- 3 RECESSED PREFORMED PLASTIC PAVEMENT MARKING TYPE C - LINE 6" (WHITE - 2' LINE, 6' SPACE)
- 4 MODIFIED URETHANE PAVEMENT MARKING - 12" (YELLOW DIAGONAL AT 45°, 75' C-C SPACING) - 5 MINIMUM
- 5 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW) AT 11" C-C
- 6 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL AT 45°, 75' C-C SPACING) - 5 MINIMUM
- 7 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- 8 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE - 2' LINE, 6' SPACE)
- 9 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE - 10' LINE, 30' SPACE)
- 10 THERMOPLASTIC PAVEMENT MARKING - LINE 24" (SOLID WHITE)
- 11 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- 12 REPLACEMENT REFLECTOR
- 13 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID YELLOW)
- 14 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' LINE, 30' SPACE) AT 5 1/2" C-C

NOTES:

1. DO NOT INSTALL RAISED REFLECTIVE PAVEMENT MARKERS FROM STA 5334+20 TO STA 5335+16.
2. INSTALL PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE STANDARD TC-13 - TYPICAL PAVEMENT MARKINGS.
3. THERMOPLASTIC PAVEMENT MARKINGS TO BE PLACED ON ASPHALT PAVEMENT. PREFORMED PLASTIC AND MODIFIED URETHANE PAVEMENT MARKINGS TO BE PLACED ON CONCRETE PAVEMENT.
4. INSTALL RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT ONE STANDARD TC-11 - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT).

TYPICAL PAINTED MEDIAN NOSE MARKING



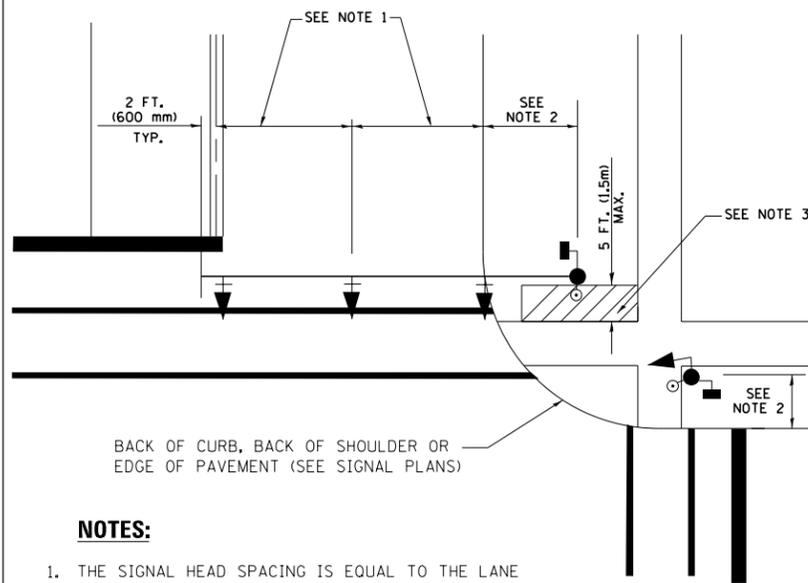
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PLOT DATE = 3/11/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN			
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	13
CONTRACT NO.				62G51
ILLINOIS FED. AID PROJECT				

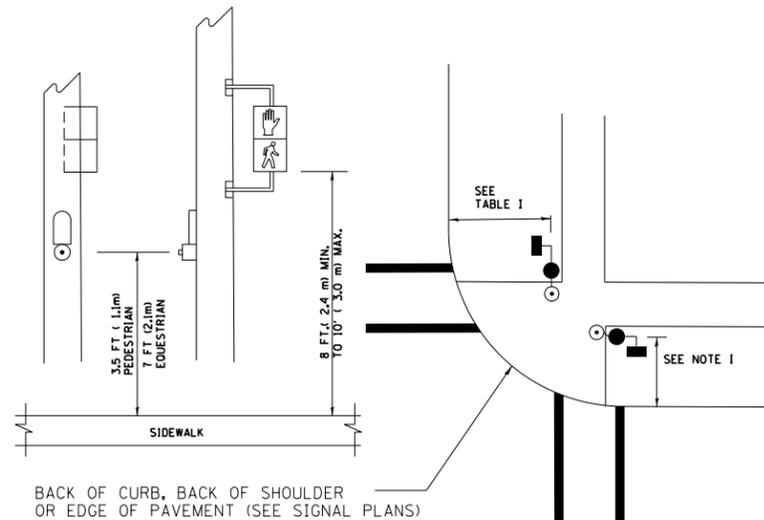
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

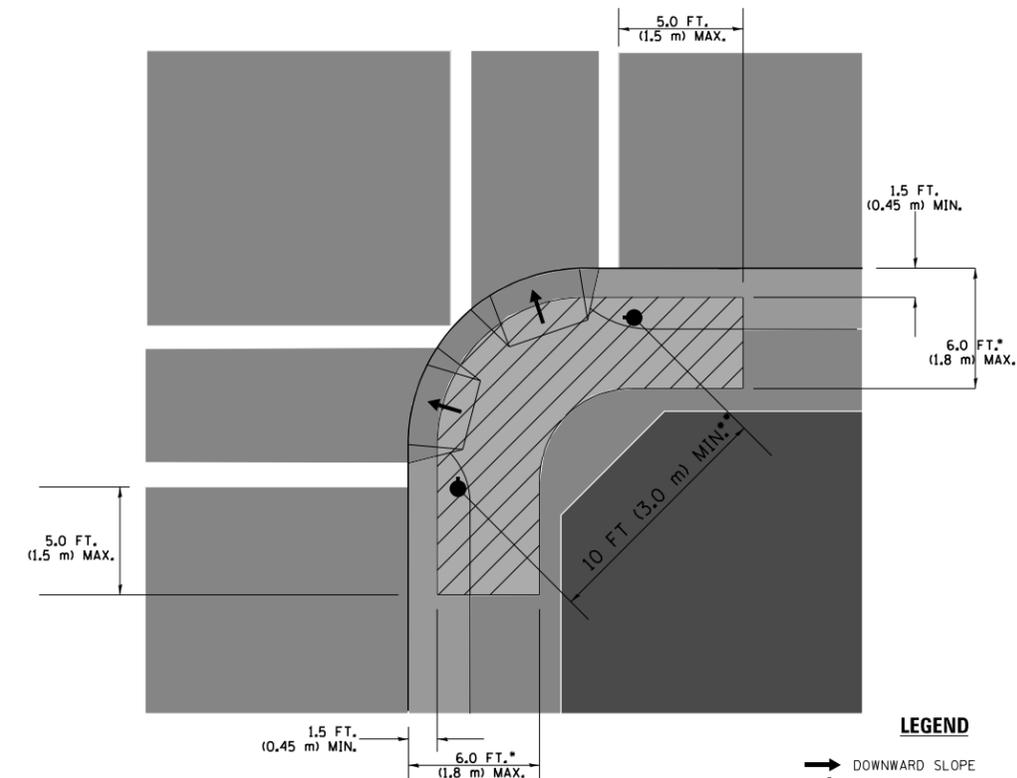
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

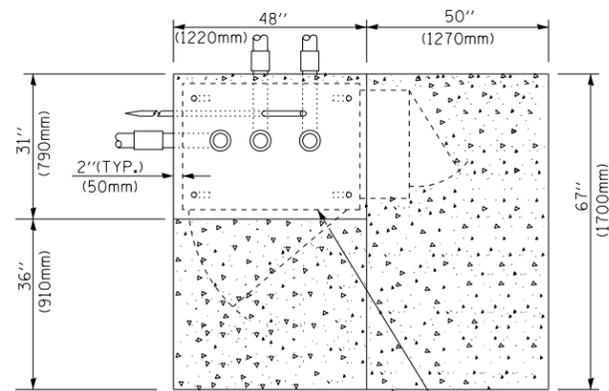
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

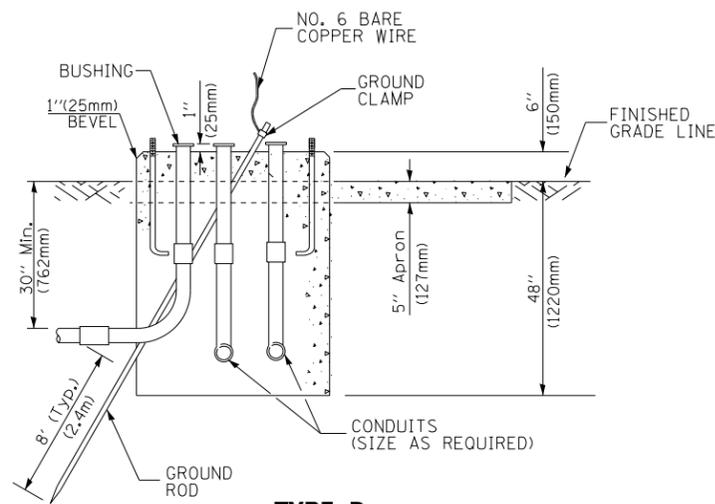
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

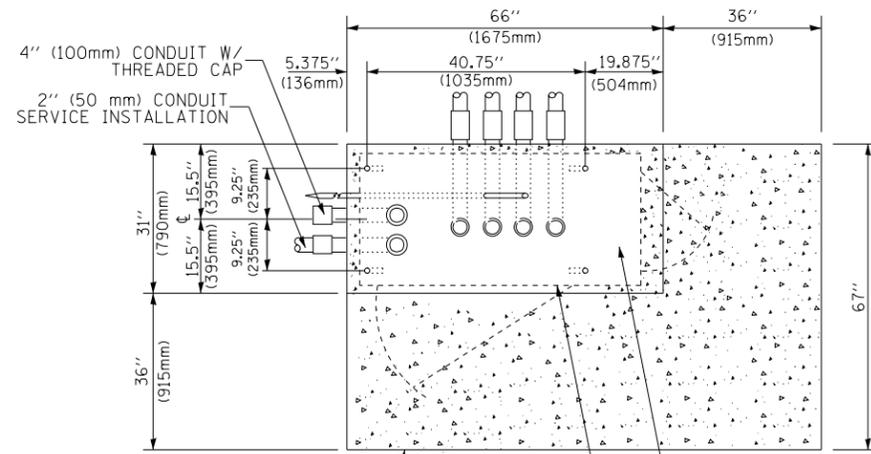
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



TOP VIEW



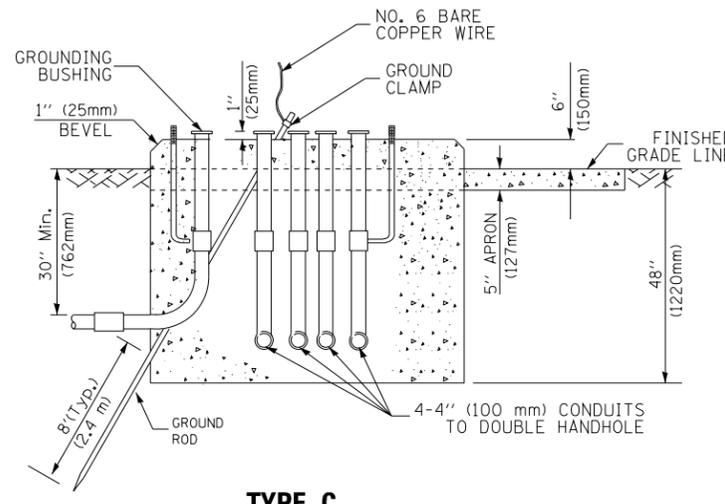
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



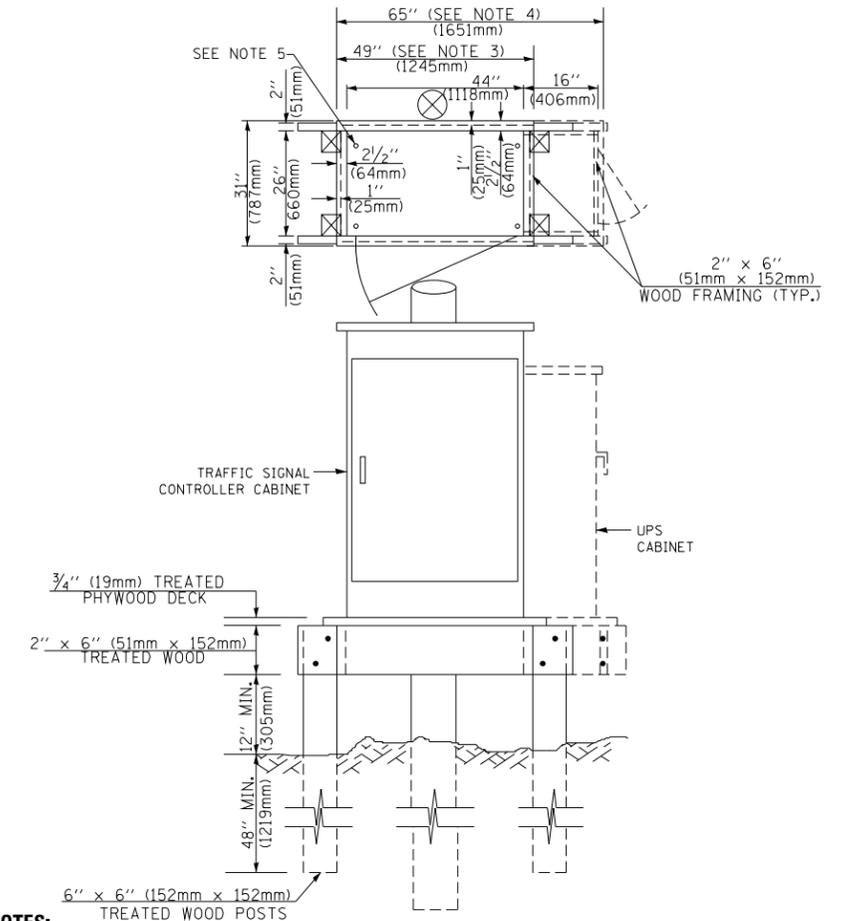
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

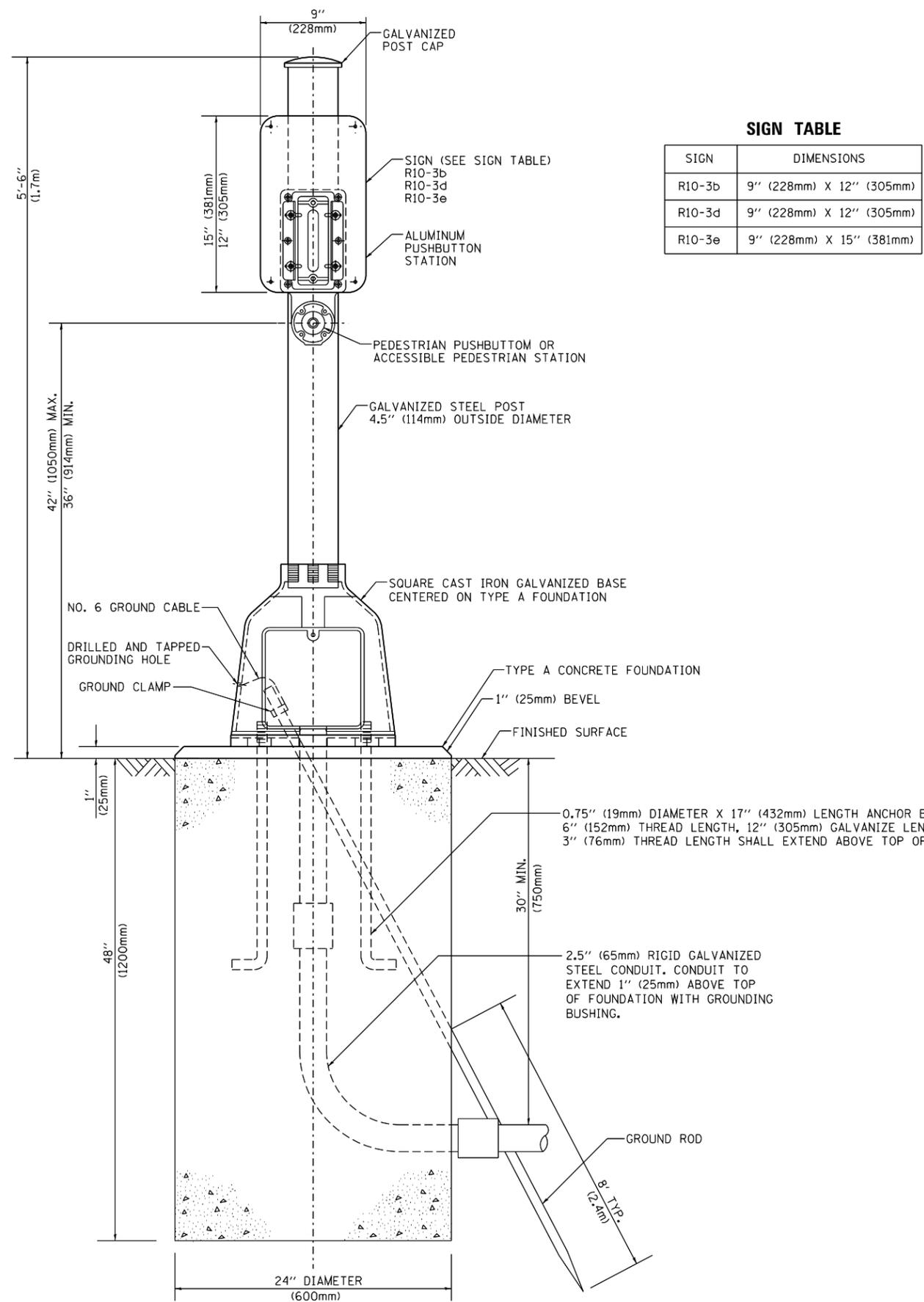
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

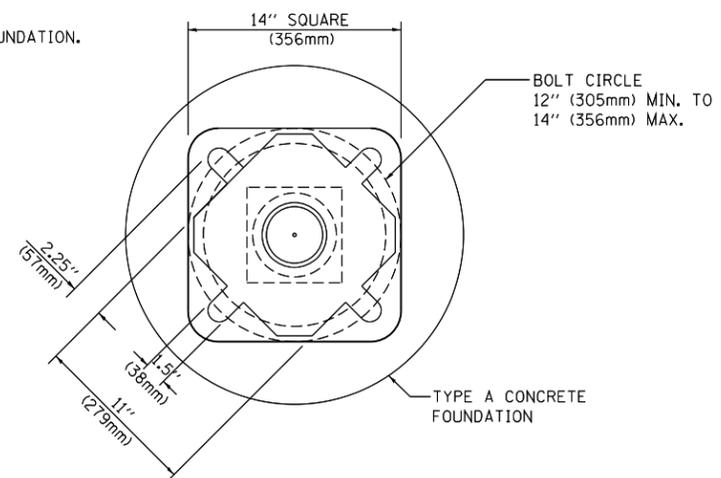
DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 5 OF 7 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	16
TS-05		CONTRACT NO. 62G51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

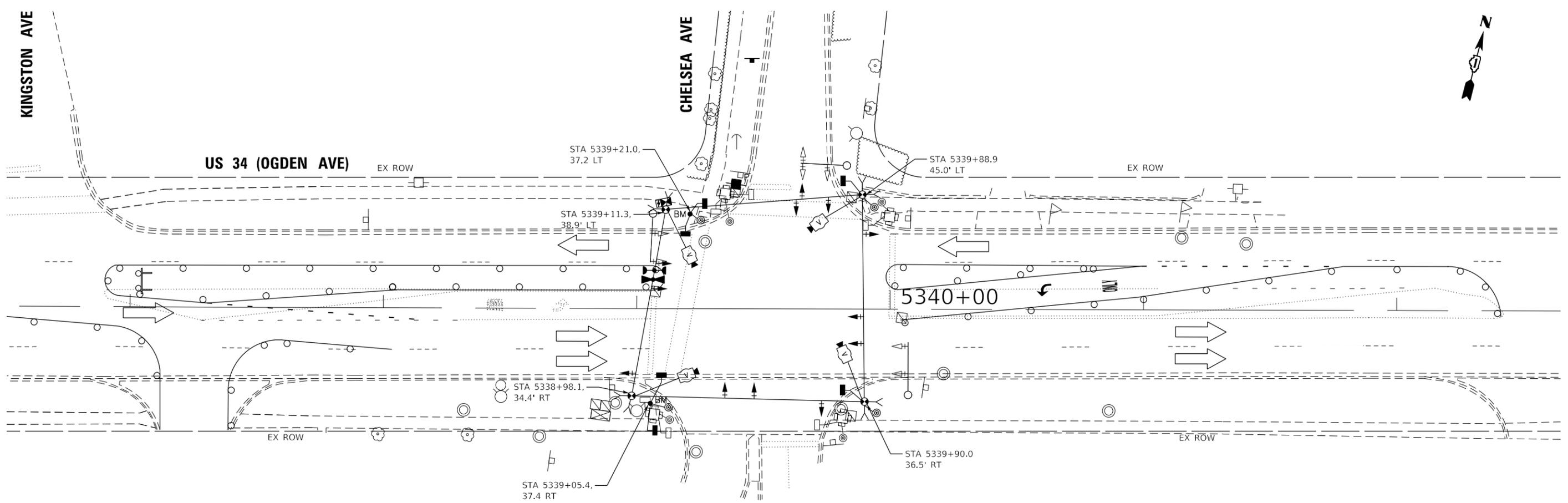
PEDESTRIAN PUSH BUTTON POST, TYPE A

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PLOT SCALE = 50.0000' / in.		CHECKED - DAD	REVISED -
PLOT DATE = 1/13/2014		DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 7 OF 7 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	17
TS-05			CONTRACT NO. 62G51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES

1. DE-ENERGIZE AND BAG EXISTING SIGNAL HEADS
2. USE CLASS 2A SEED MIXTURE AND TOPSOIL AS REQUIRED TO PERMANENTLY RESTORE THE OGDEN AVE / CHELSEA AVE INTERSECTION AFTER THE TEMPORARY TRAFFIC SIGNAL INSTALLATION IS REMOVED. ALL RUTS ARE TO BE REMOVED AND ALL BARE AREAS ARE TO BE SEEDED. THIS RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT IS INCLUDED IN THE COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION.

FILE NAME = Q:\Engineering\LiveProjects\13043.IDOT DUR AES\Work Order\11\CADD\CADD Sheets\Cvt\1\0162051-shr-Temp Trf Sig Chelsea Stage Ldgn

**TS 21700
ECON 48**

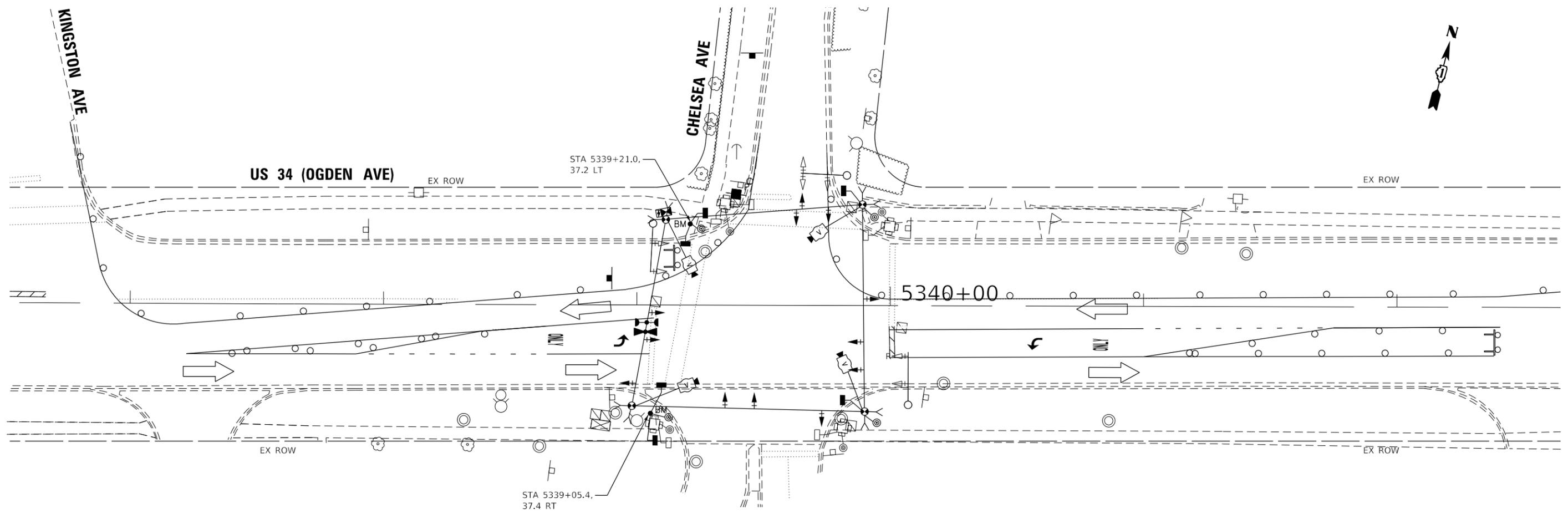


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DRAWN - JN	REVISED -	
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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY TRAFFIC SIGNAL PLAN - STAGE I US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	18
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				



NOTES

1. DE-ENERGIZE AND BAG EXISTING SIGNAL HEADS
2. USE CLASS 2A SEED MIXTURE AND TOPSOIL AS REQUIRED TO PERMANENTLY RESTORE THE OGDEN AVE / CHELSEA AVE INTERSECTION AFTER THE TEMPORARY TRAFFIC SIGNAL INSTALLATION IS REMOVED. ALL RUTS ARE TO BE REMOVED AND ALL BARE AREAS ARE TO BE SEEDED. THIS RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT IS INCLUDED IN THE COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION.

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	DRAWN - JN	REVISED -
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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

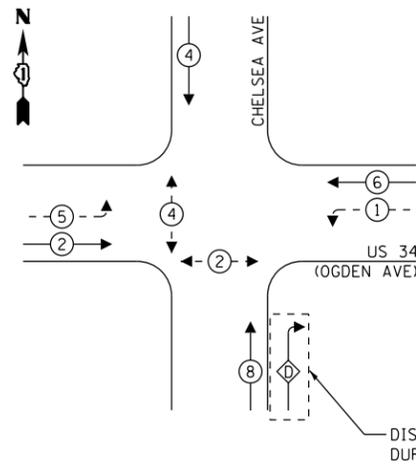
**TEMPORARY TRAFFIC SIGNAL PLAN - STAGE II
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	19
CONTRACT NO.				62G51
ILLINOIS FED. AID PROJECT				

**TS 21700
ECON 48**

TEMPORARY CONTROLLER SEQUENCE



LEGEND:

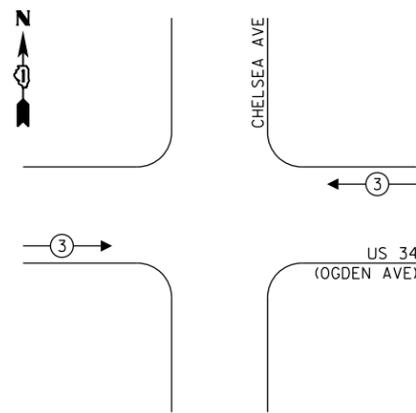
- ← (⊙) ← PROTECTED PHASE
- ← (⊙) ← PROTECTED/PERMITTED PHASE
- ← (⊙) ← PEDESTRIAN PHASE
- ← (⊙) ← OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

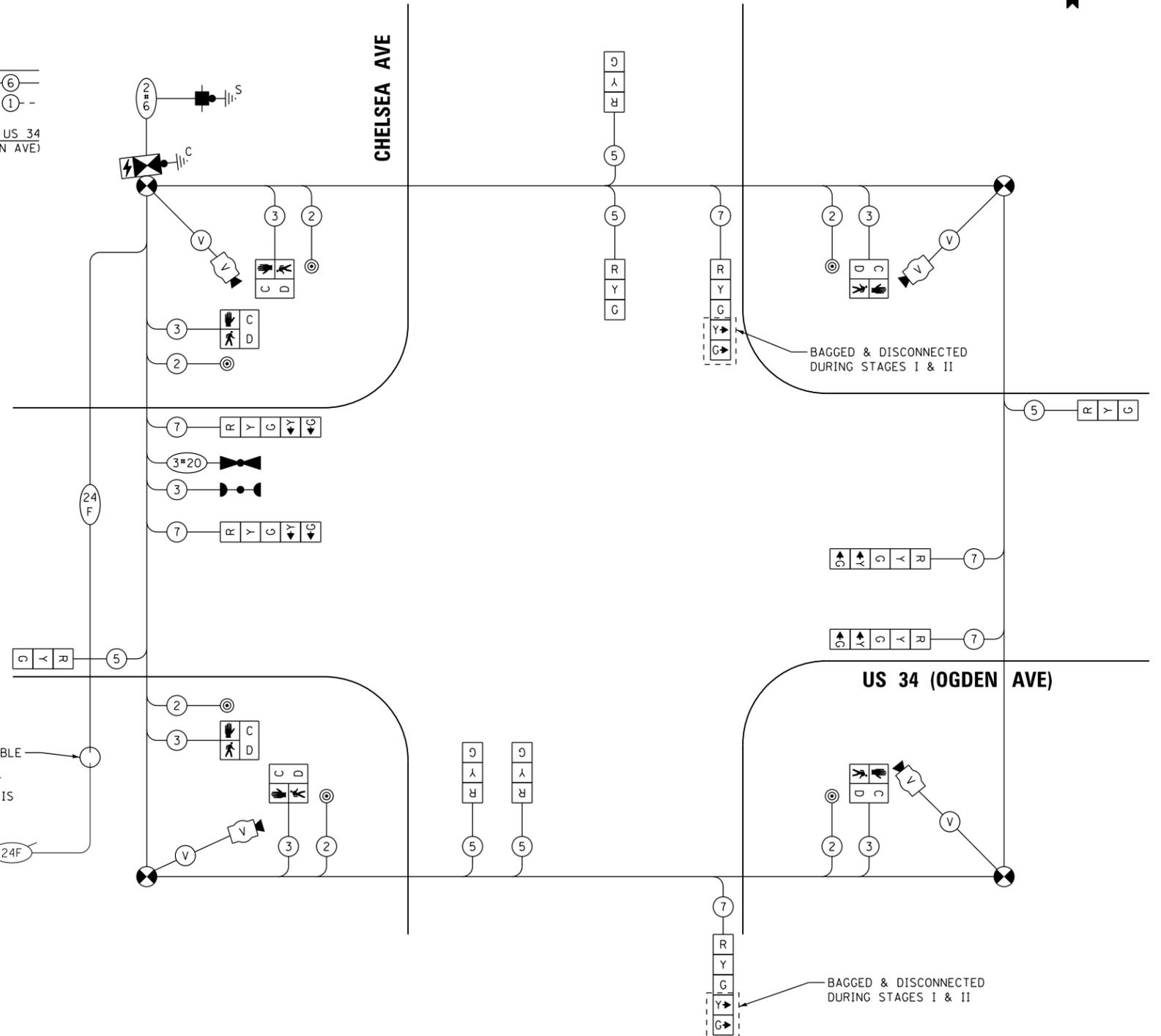
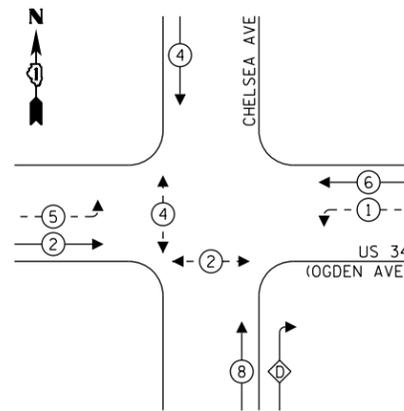
OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
D	= 8	+ 1

DISCONNECTED DURING STAGES I & II

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



EXISTING CONTROLLER SEQUENCE



SPLICE 24 FIBER (12SM, 12MM) CABLE TO EXISTING FIBER CABLE TO MAINTAIN EXISTING INTERCONNECT UNTIL PERMANENT INTERCONNECT IS RE-ESTABLISHED.

TO MAIN ST-LISLE (24F)

TEMPORARY CABLE PLAN
(NOT TO SCALE)

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	12	10	10	12.0
PED. SIGNAL	6	20	100	120.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	1	150	100	150.0
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				549.8

ENERGY COSTS TO:

VILLAGE OF LISLE
228 S MAIN ST
LISLE, IL 60532

ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630) 424-5704
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---



USER NAME = JENT	DESIGNED - JN	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - JN	REVISED -
PLOT DATE = 3/11/2019	CHECKED - TGM	REVISED -
	DATE - 12/07/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN STAGE I & II
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

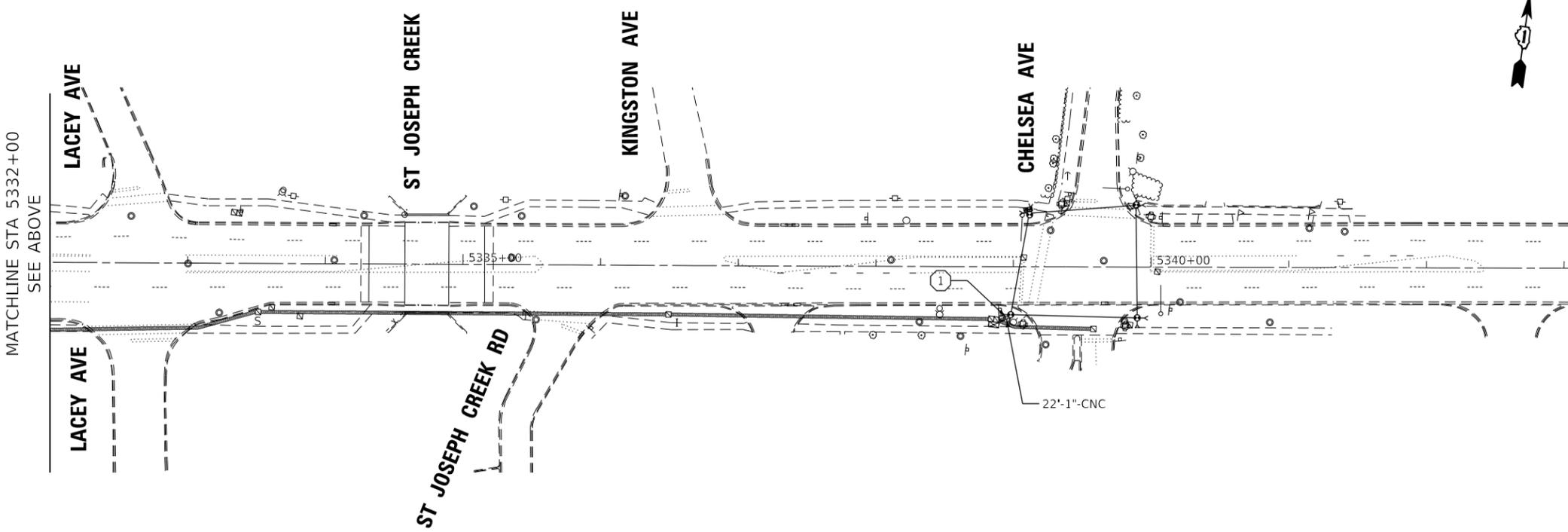
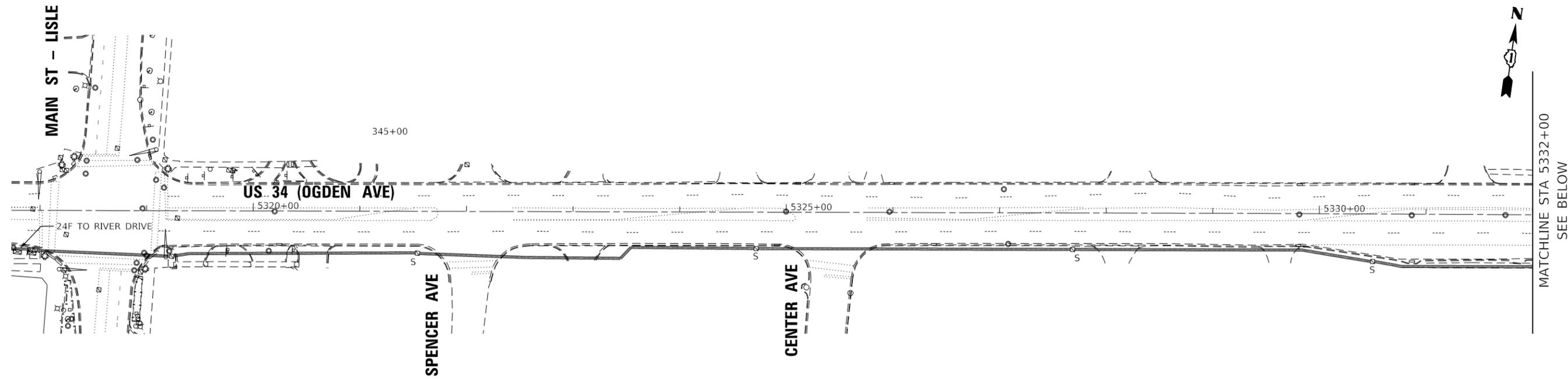
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	20
CONTRACT NO.				62G51
ILLINOIS FED. AID PROJECT				

TS 21700
ECON 48

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FILE NAME = Q:\Engineering\LiveProjects\13043.IDOT DUR AES\Work D-rdr 11\CADD\CADD Sheets\Cvt1\0162051-shr-Temp_Interconnect.dgn



- 1 NOTES
1. THE EXISTING 24 FIBER CABLE SHALL BE DISCONNECTED AT THE CHELSEA AVE CABINET AND PULLED BACK TO THE ADJACENT DOUBLE HANDHOLE.
 2. SPLICE 24 FIBER (12SM, 12MM) CABLE TO THE EXISTING FIBER CABLE IN THE DOUBLE HANDHOLE TO MAINTAIN THE EXISTING INTERCONNECT.
 3. WHEN THE TEMPORARY TRAFFIC SIGNAL IS REMOVED FROM SERVICE, THE 24 FIBER CABLE SHALL BE RECONNECTED IN THE CHELSEA AVE CABINET TO RE-ESTABLISH THE EXISTING INTERCONNECT.



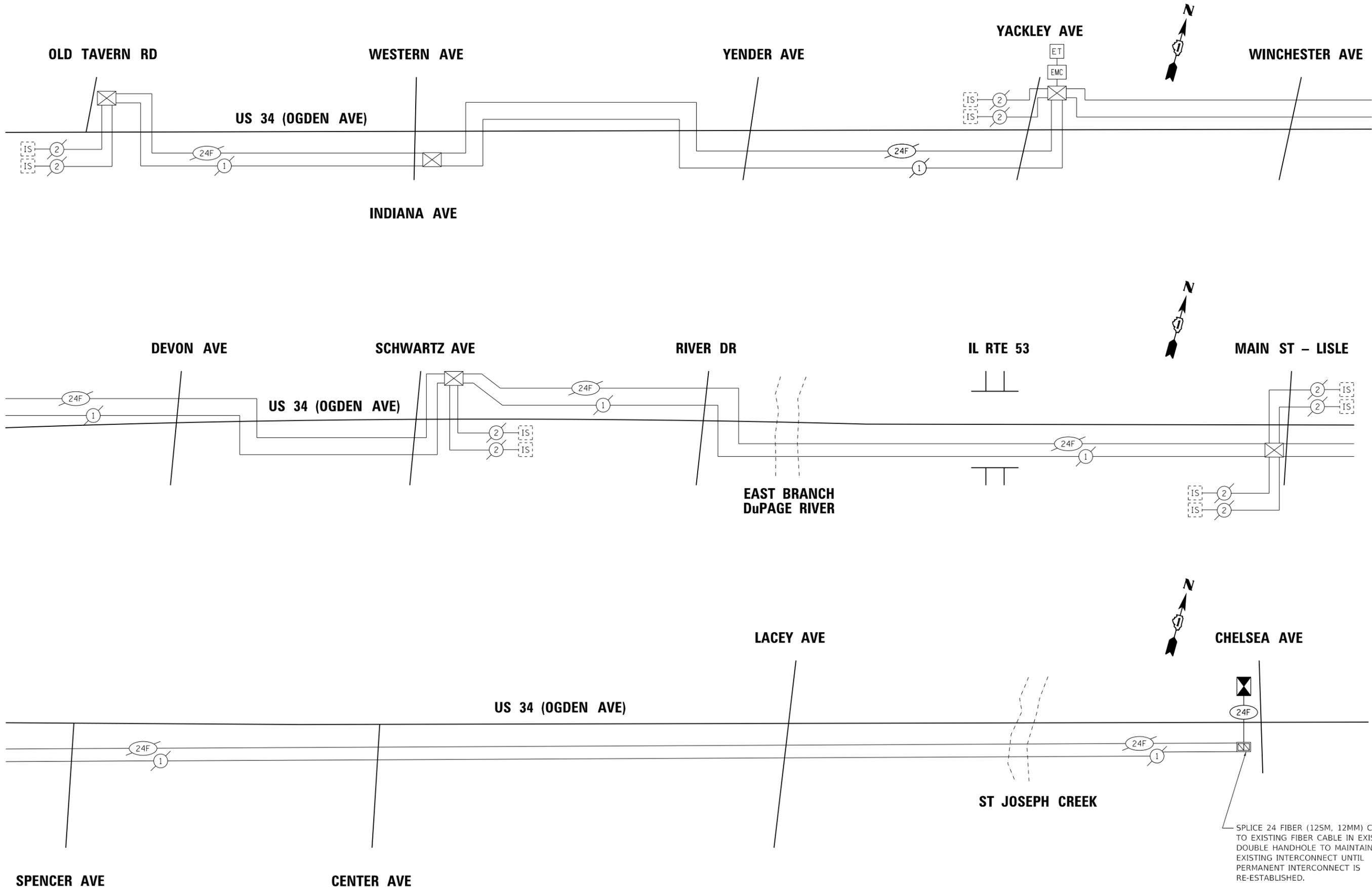
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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLAN
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	21
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				



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PLOT DATE = 1/23/2019	DATE - 12/07/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT SCHEMATIC
US 34 (OGDEN AVENUE) AT ST. JOSEPH CREEK**

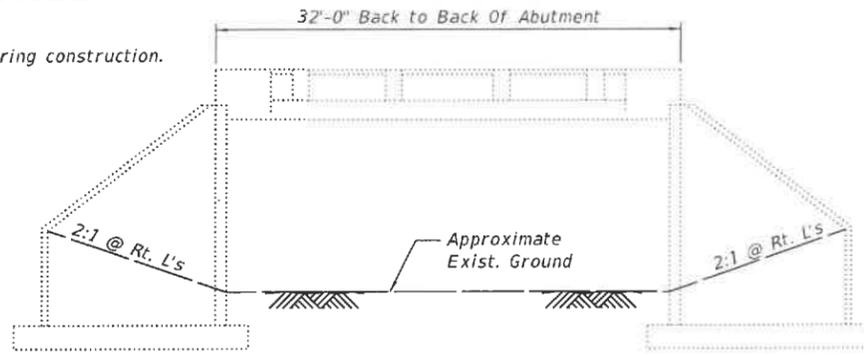
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CONTRACT NO.			62G51	
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

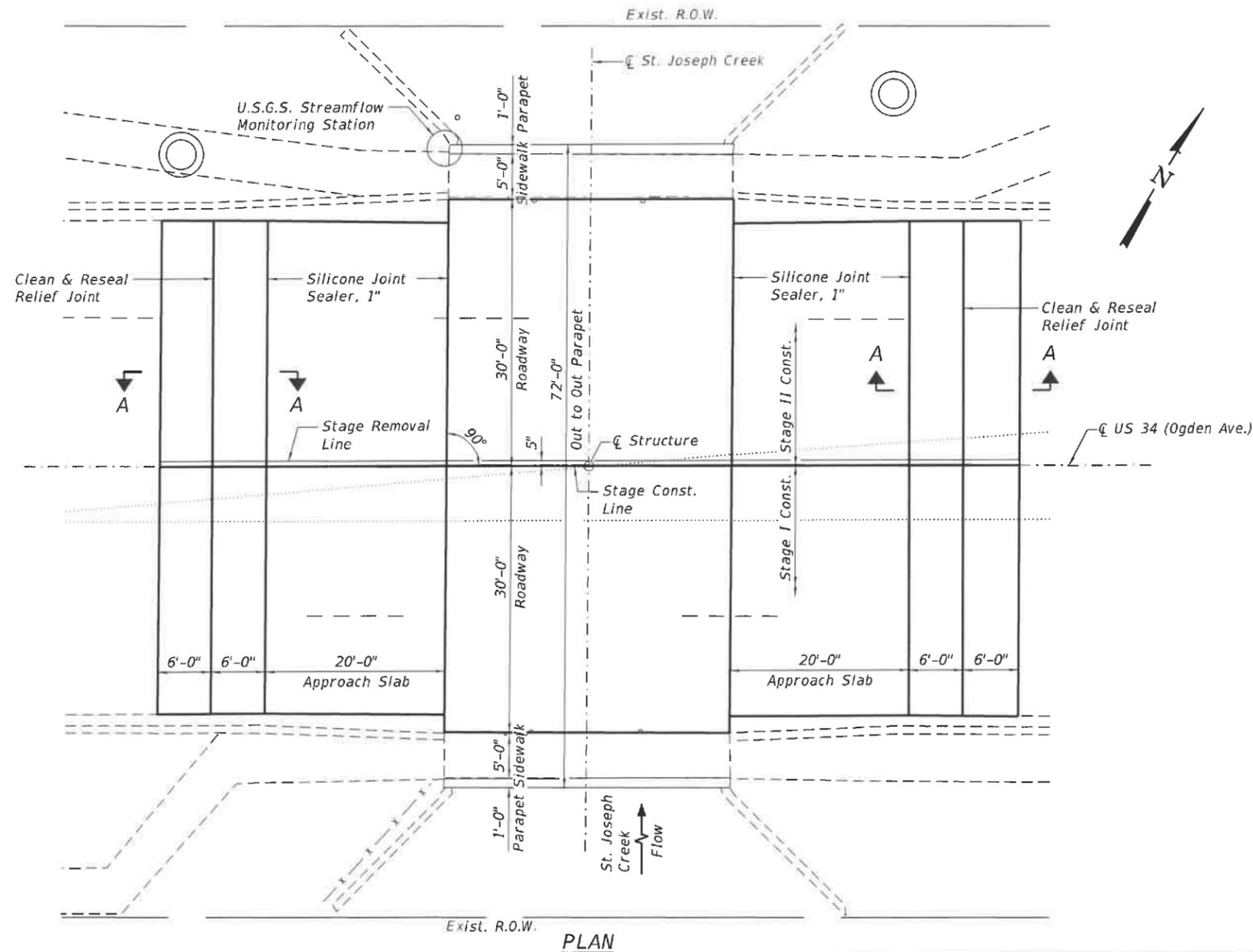
Existing Structure:
 SN 022-0034 built in 1968, Section 10YB-L-1. Structure consists of a single span. Existing bridge is a slab bridge supported on closed wall abutments. Overall length of the bridge is 32'-0" back to back abutments and overall width of the bridge is 72'-0" out to out. The structure was repaired in 1981 and 1998. The repairs included deck slab repair and deck overlay.

Traffic will be maintained utilizing staged construction during construction.

No Salvage.



ELEVATION



PLAN

INDEX OF SHEETS

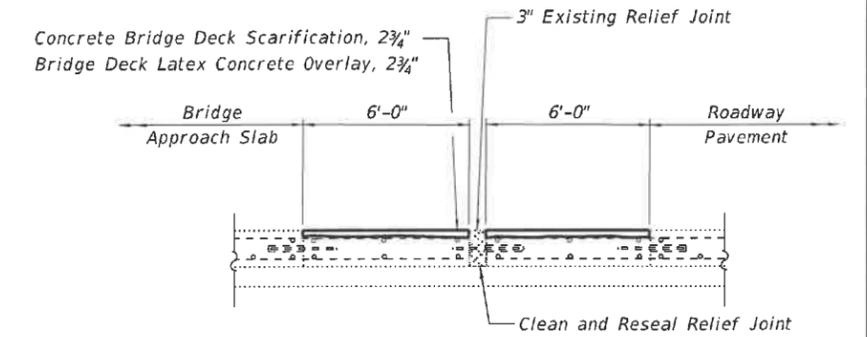
1. General Plan and Elevation
2. General Data and Stage Construction Details
3. Temporary Concrete Barrier for Stage Construction
4. Deck Repair Details
5. Abutment Repair Details

SCOPE OF WORK

1. Scarify bridge deck and approach slabs.
2. Perform deck slab repairs and approach slab repairs.
3. Perform Abutment and Bridge Parapet Repairs.
4. Clean and Reseal Pavement Relief Joint.
5. Construct Bridge Deck Latex Concrete Overlay.

DESIGN SPECIFICATIONS

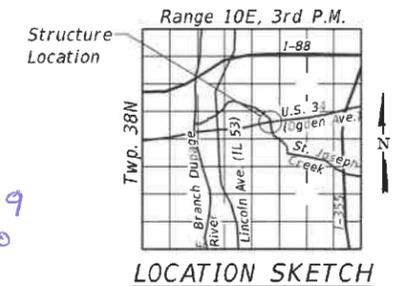
2002 AASHTO LRFD Bridge Design Specifications, 17th Edition



SECTION A-A



DATE SIGNED: 02/21/19
 EXP. DATE: 11/30/20



GENERAL PLAN AND ELEVATION
U.S. 34 (OGDEN AVE.) OVER
ST. JOSEPH CREEK
F.A.P. RTE. 311
SECTION 10Y-BJR
DuPAGE COUNTY
STRUCTURE NO. 022-0034

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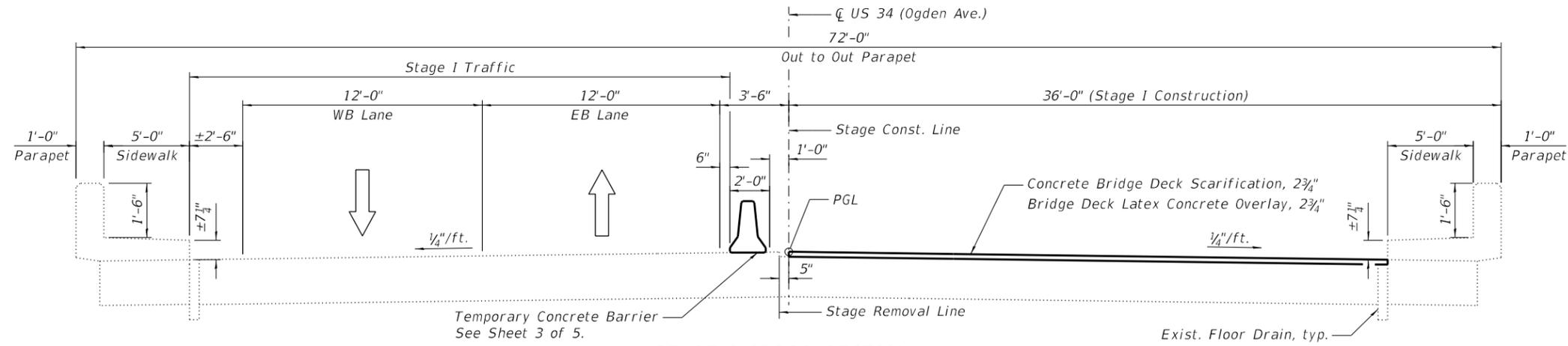


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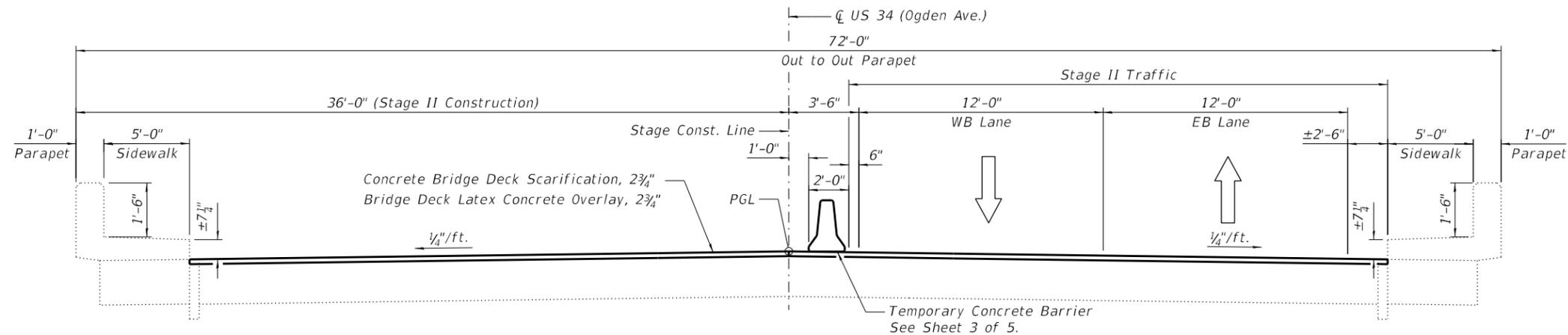
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 5 SHEETS

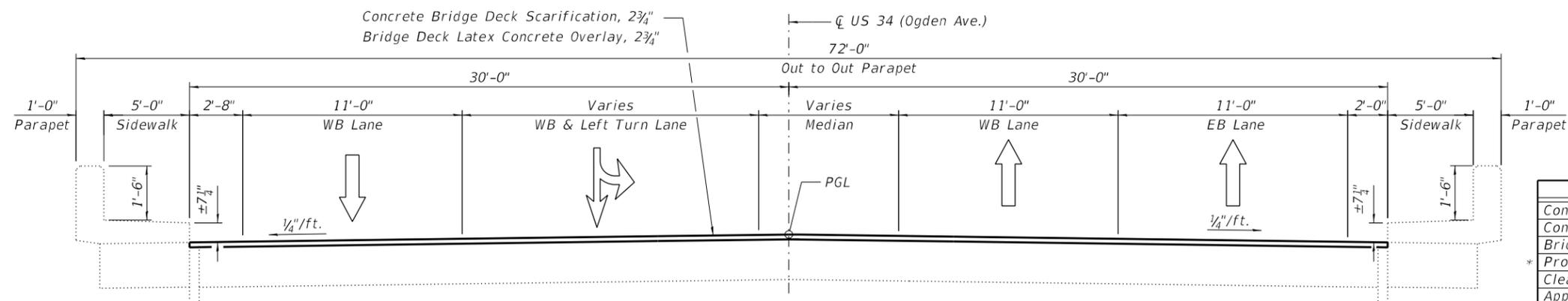
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	23
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				



STAGE I CROSS SECTION
(Looking East)



STAGE II CROSS SECTION
(Looking East)



DECK CROSS SECTION
(Looking East)

GENERAL NOTES

- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Areas of deck repairs are estimated. Actual type, location and dimensions are to be determined by the Engineer during construction and documented on as-built plans.
- The Department has determined that in-stream work is not required for the work specified in this Contract. The Department has not obtained a 404 Permit. If the Contractor chooses to use activities requiring an USACE 404 Permit, it is the Contractor's responsibility to secure the proper USACE Permits.
- Protective Coat shall be applied to the top and inside face of the bridge sidewalk, bridge parapet and new concrete overlay areas.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	1.8		1.8
Concrete Superstructure	Cu. Yd.	1.8		1.8
Bridge Deck Grooving	Sq. Yd.	591		591
Protective Coat	Sq. Yd.	670		670
Clean and Reseal Relief Joint	Foot	120		120
Approach Slab Repair (Partial Depth)	Sq. Yd.	20		20
Bridge Deck Scarification, 2 3/4"	Sq. Yd.	612		612
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	612		612
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq. Ft.	18	474	492
Structural Repair of Concrete (Depth Greater than 5 Inches)	Sq. Ft.	12	32	44
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	27		27
Silicone Joint Sealer, 1"	Foot	240		240

* See note 4.

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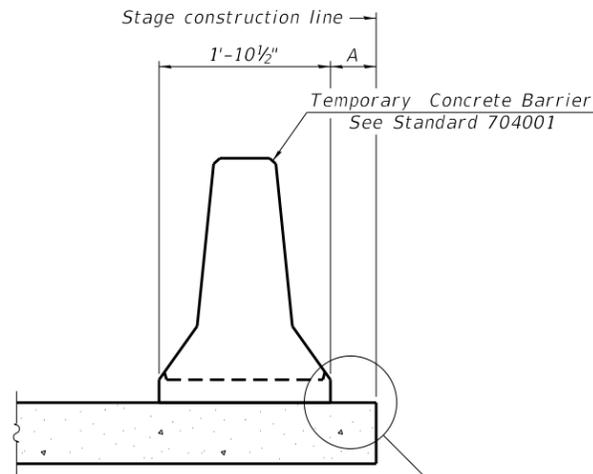
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA AND STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0034**

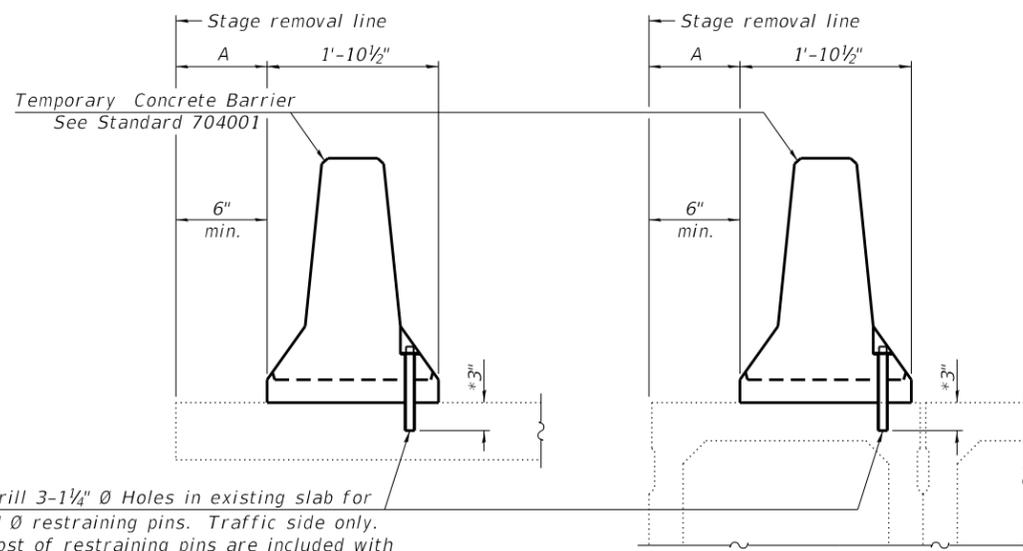
SHEET 2 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	24
CONTRACT NO. 62G51				
ILLINOIS		FED. AID PROJECT		



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

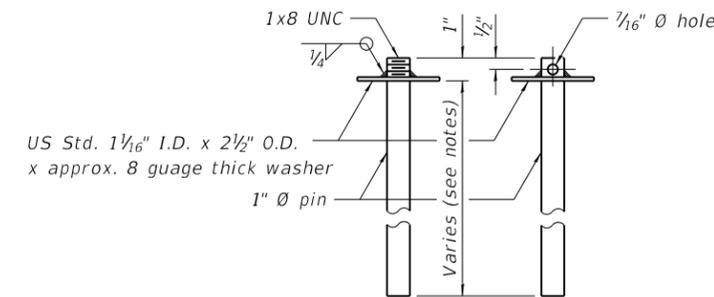


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

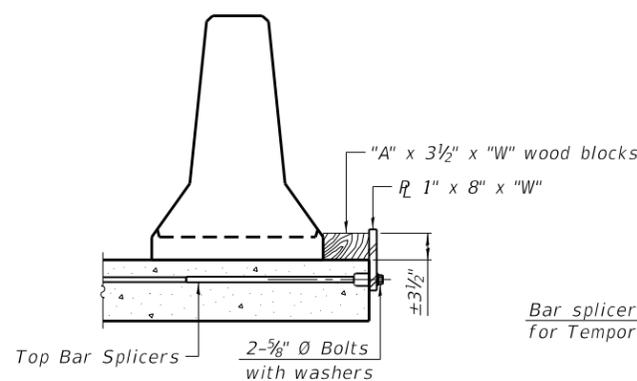
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

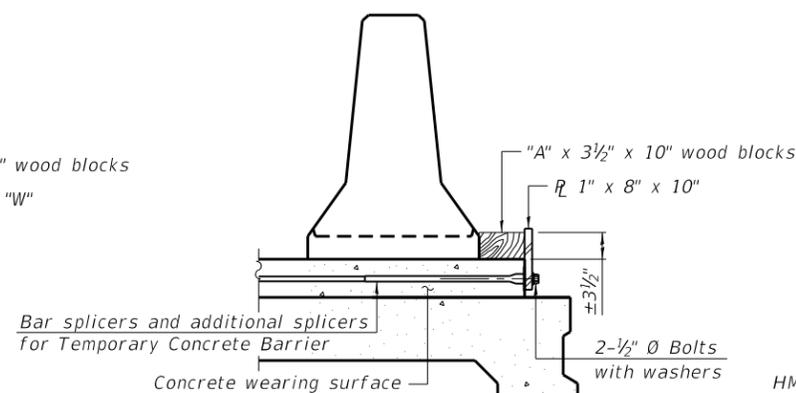


RESTRAINING PIN

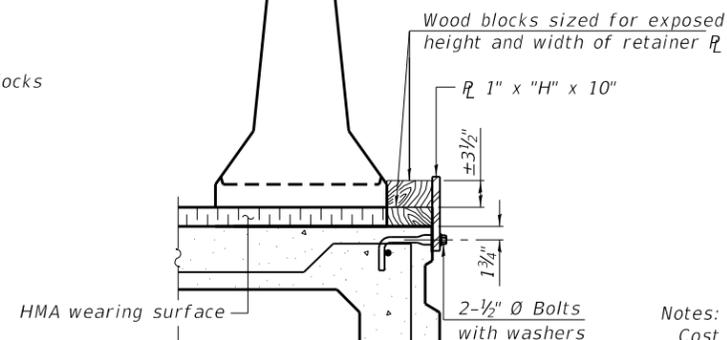
SECTIONS THRU SLAB OR DECK BEAM



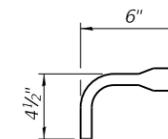
DETAIL I



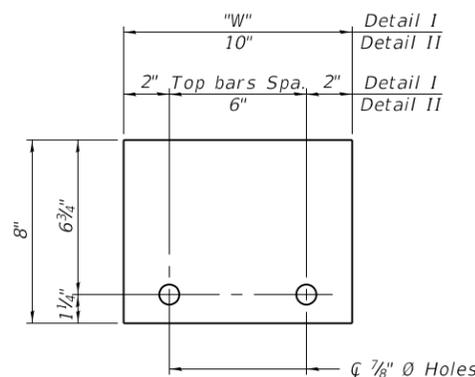
DETAIL II



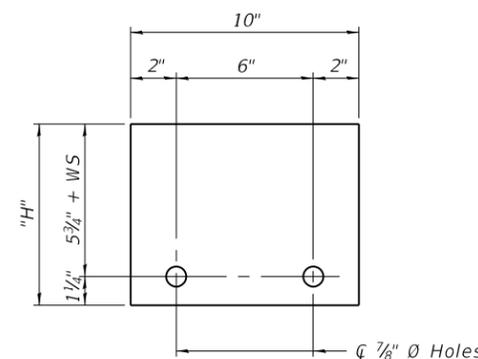
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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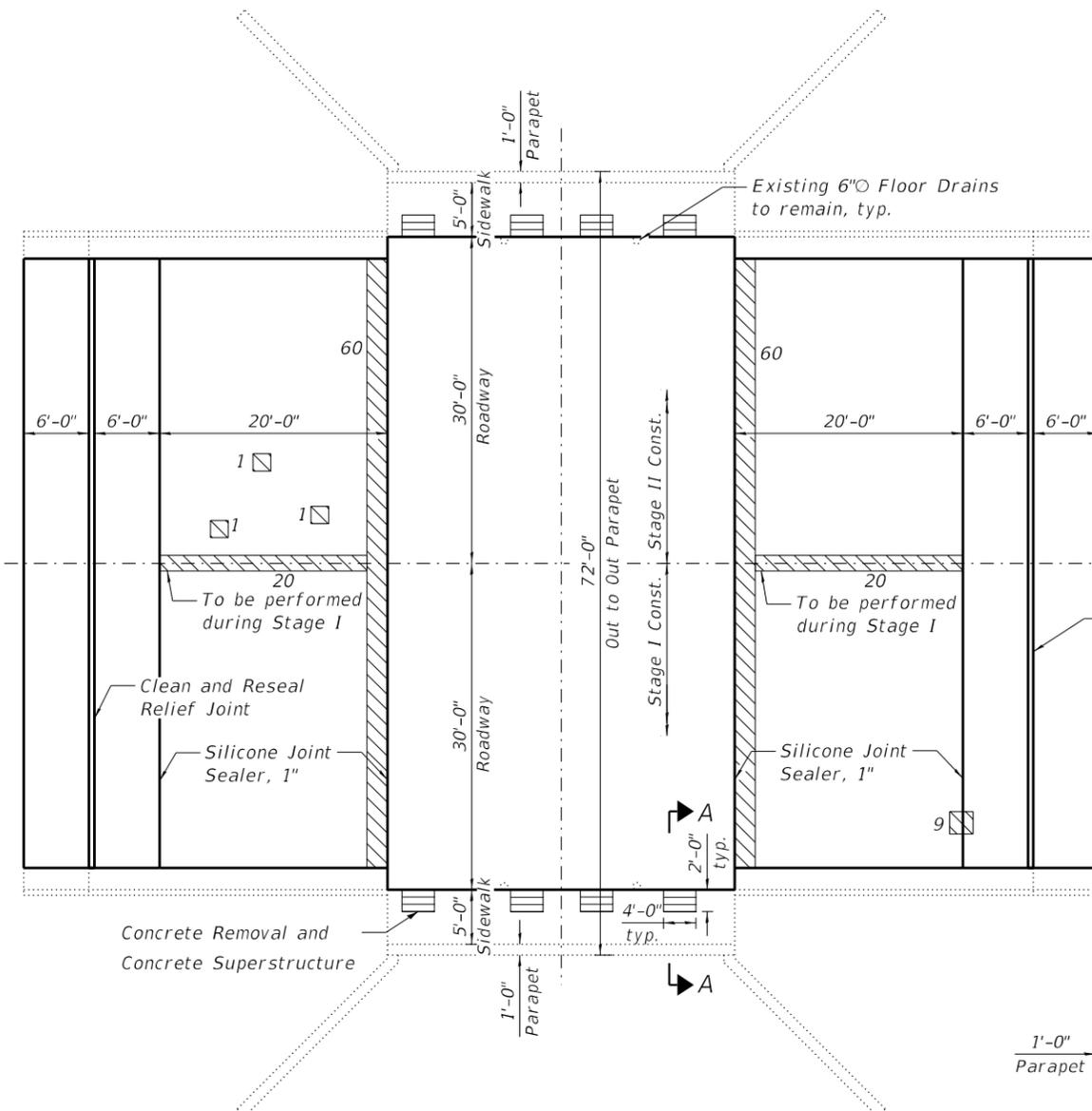
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 STRUCTURE NO. 022-0034

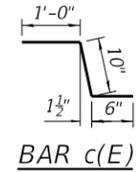
SHEET 3 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	25
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

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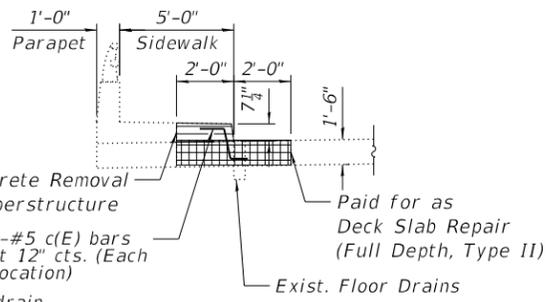


PLAN

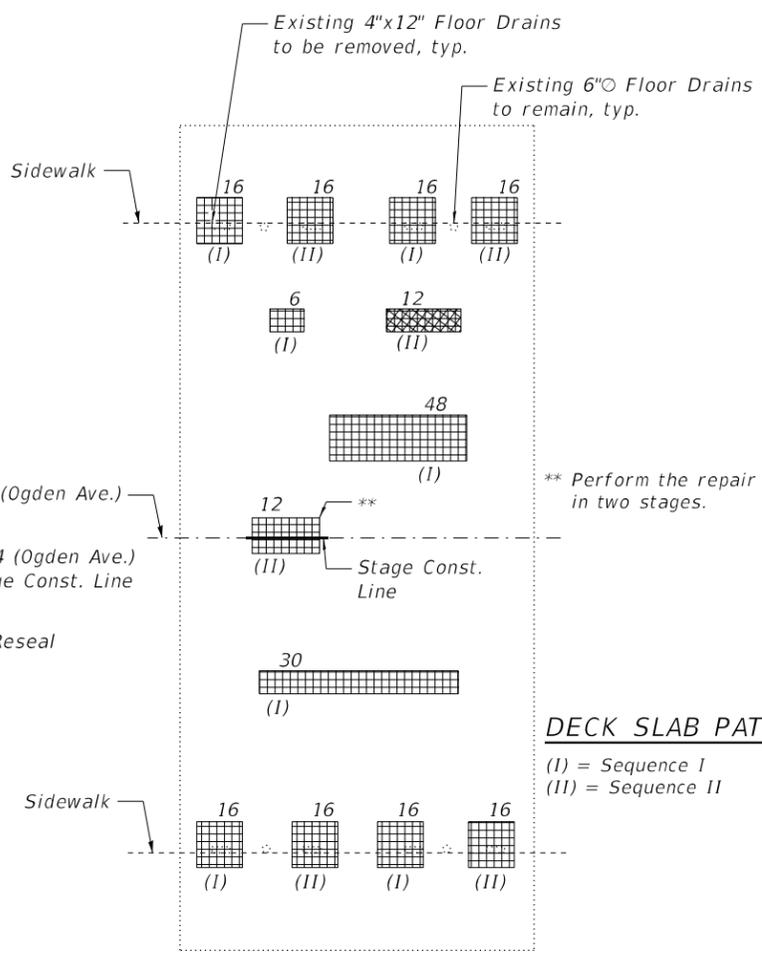


BAR c(E)

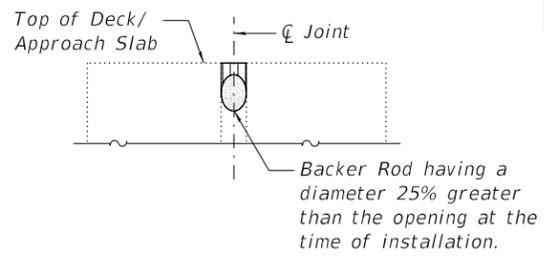
* Place c(E) bars to avoid the deck drain. c(E) bar shall be epoxy coated. Cost included with the cost of Concrete Superstructures.



SECTION A-A



DECK BOTTOM REPAIR



SILICONE JOINT SEALER DETAIL
 (See Plan view for locations)

NOTES

- Construction of the Deck Slab Repair (Full Depth and Partial Depth) shall be performed in sequence I and II in each stage. Sequence II removal areas shall not be started until the new concrete in sequence I removal areas has reached a minimum of 650 psi modulus of rupture.
- Areas shown for Structural Repair of Concrete, Deck Repairs, and Approach Slab Repairs are estimated. Actual area should be determined by the Engineer at the time of repair.
- Removal of existing HMA pavement over the pavement relief joint at the end of the approach slabs shall not be paid separately. The cost shall be included with the cost of Clean and Reseal Relief Joint.
- Existing reinforcement extended into the removal area shall be cleaned, straightened and incorporated into the new construction.
- Hatched areas indicate concrete section to be removed. Perimeters of Concrete Removal areas shall be saw cut 3/4" prior to the removal of the concrete. Any reinforcement bars that are damaged or cut during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Cost of removing the floor drains is included with the cost of Deck Slab Repair (Full Depth, Type II).
- Existing Name Plate shall be removed, cleaned and reinstalled if in conflict with the Structural Repair of Concrete. Cost included with Structural Repair of Concrete (Depth Equal to or Less Than 5").
- All areas shown in the plan view and elevation view are in Square Feet.

DECK SLAB PATCHING SEQUENCE

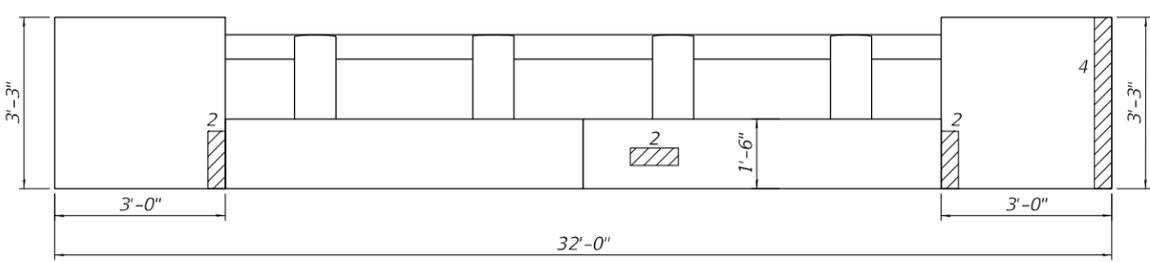
(I) = Sequence I
 (II) = Sequence II

BILL OF MATERIAL

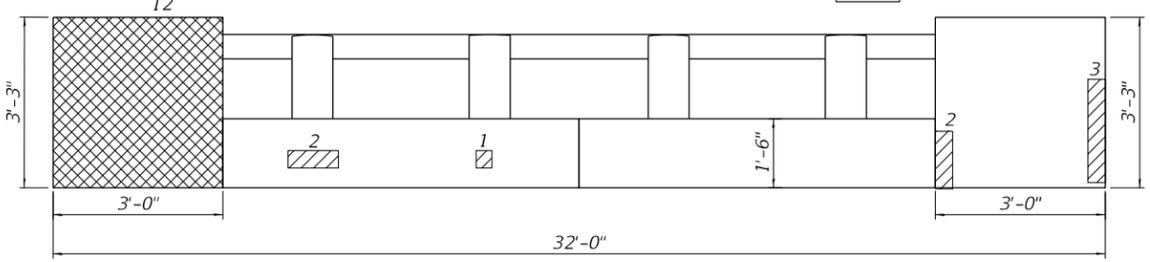
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Concrete Removal	Cu. Yd.	1.8
Concrete Superstructure	Cu. Yd.	1.8
Bridge Deck Grooving	Sq. Yd.	591
Clean & Reseal Relief Joint	Foot	120
Approach Slab Repair (Partial Depth)	Sq. Yd.	20
Bridge Deck Scarification, 2 3/4"	Sq. Yd.	612
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	612
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	27
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Yd.	18
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Yd.	12
Silicone Joint Sealer, 1"	Foot	240

LEGEND

- Approach Slab Repair (Partial Depth)
- Deck Slab Repair (Full Depth, Type II)
- Structural Repair of Concrete (Depth Less Than or Equal to 5")
- Structural Repair of Concrete (Depth Greater Than 5")
- Concrete Removal and Concrete Superstructure



BRIDGE RAILING NORTH



BRIDGE RAILING SOUTH



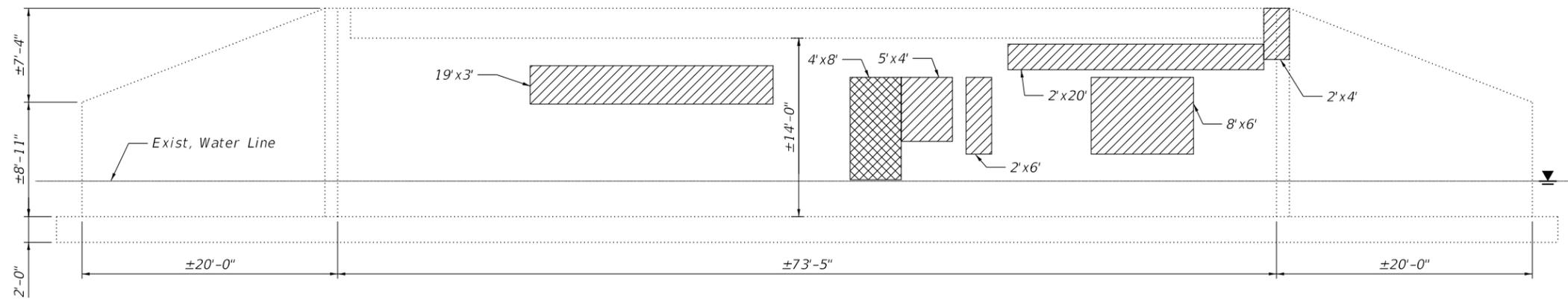
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIR DETAILS
 STRUCTURE NO. 022-0034**

SHEET 4 OF 5 SHEETS

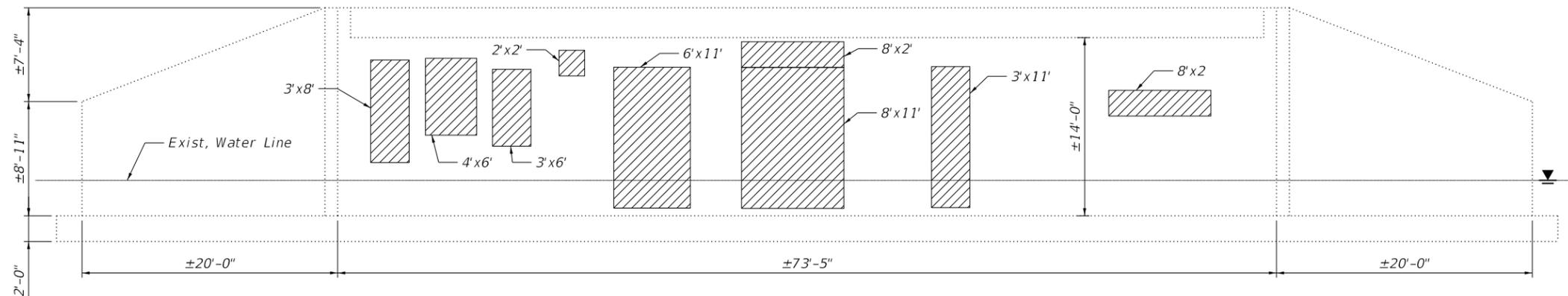
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311	10Y-BJR	DUPAGE	34	26
CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				



WEST ABUTMENT
(Looking West)

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- Structural Repair of Concrete (Depth Greater Than 5")



EAST ABUTMENT
(Looking East)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	474
Structural Repair of Concrete (Depth Greater Than 5")	Sq. Ft.	32

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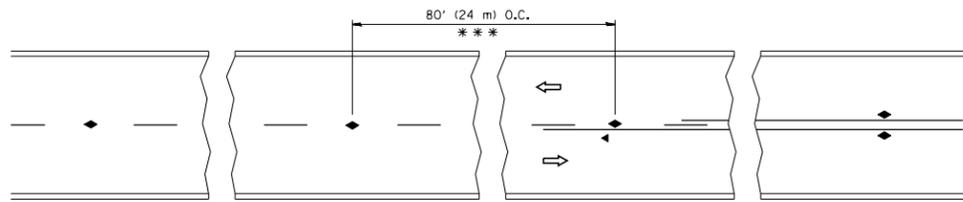
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT REPAIR DETAILS
STRUCTURE NO. 022-0034

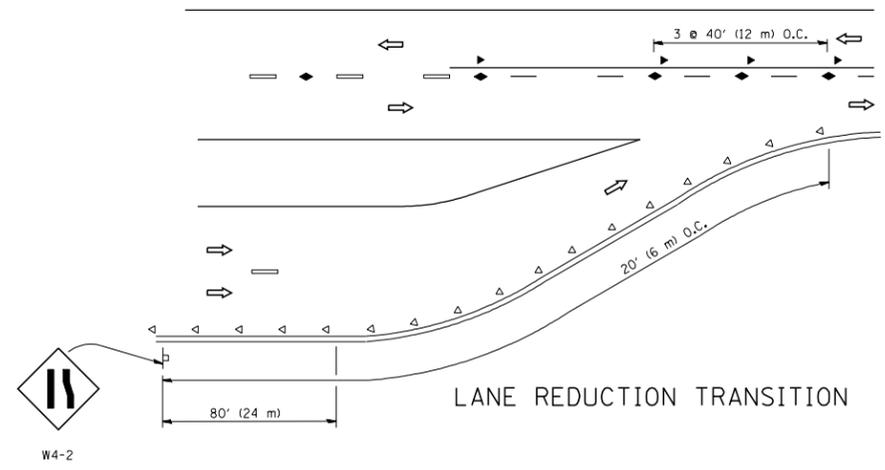
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62G51				
ILLINOIS FED. AID PROJECT				

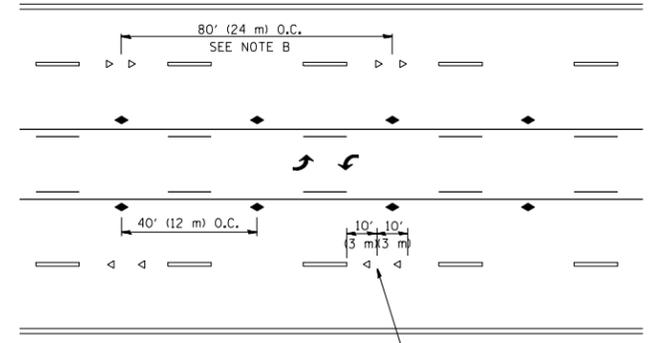


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

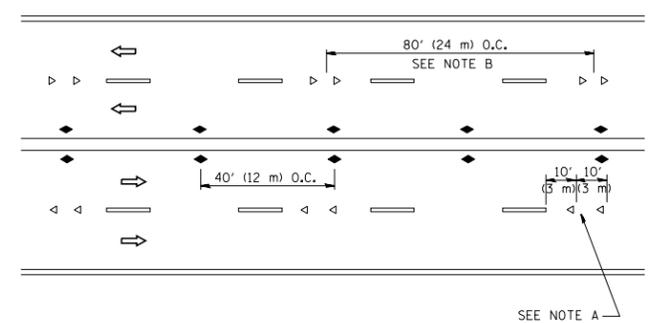
TWO-LANE/TWO-WAY



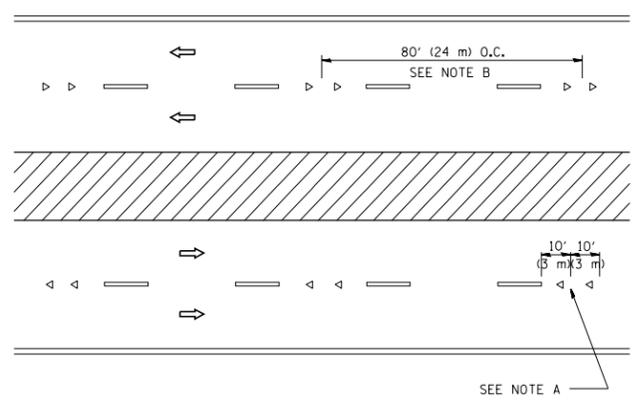
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

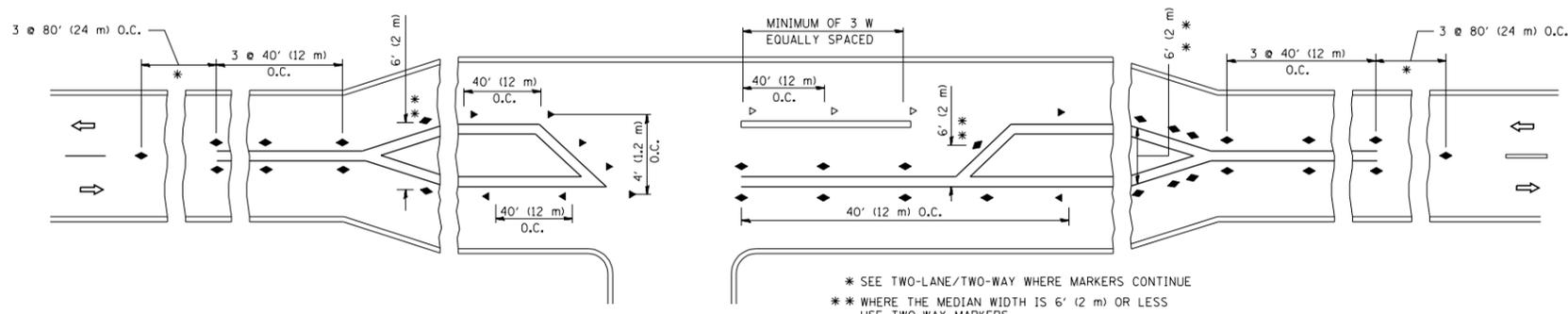
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

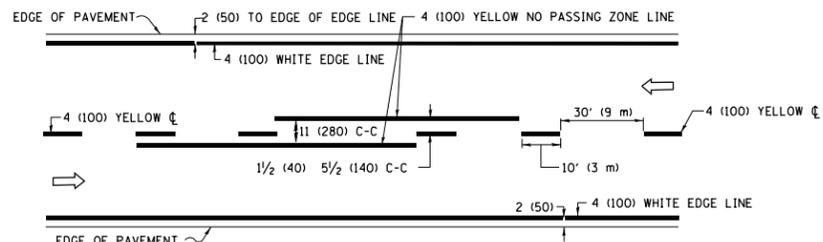
All dimensions are in inches (millimeters) unless otherwise shown.

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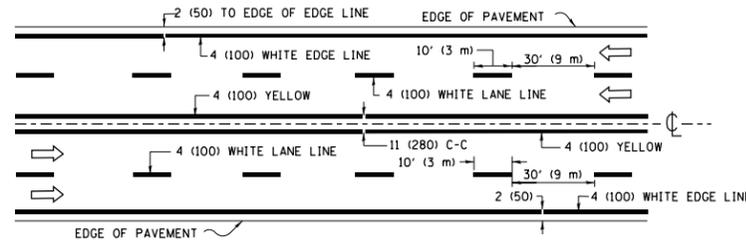
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
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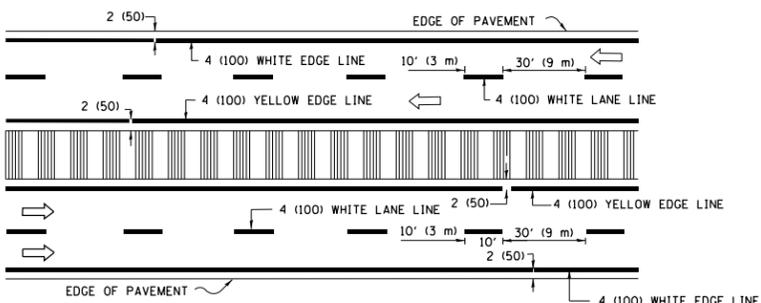
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311	10Y-BJR	DUPAGE	34	29
TC-11			CONTRACT NO. 62G51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

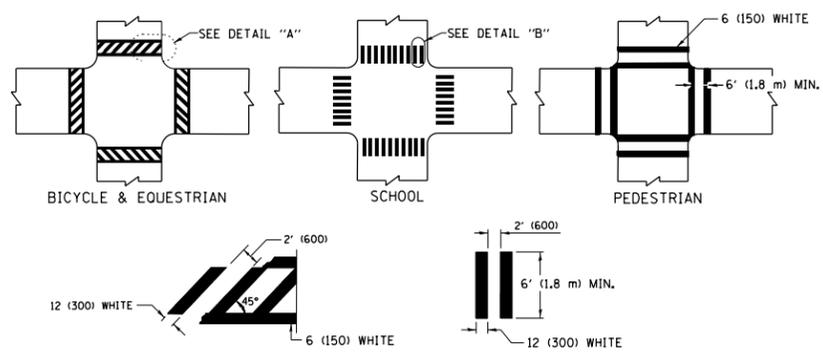


MULTI-LANE UNDIVIDED



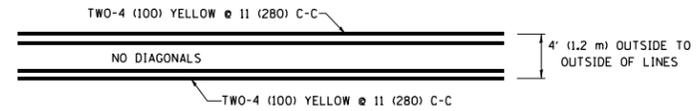
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

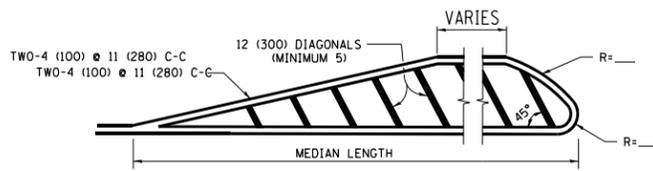


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

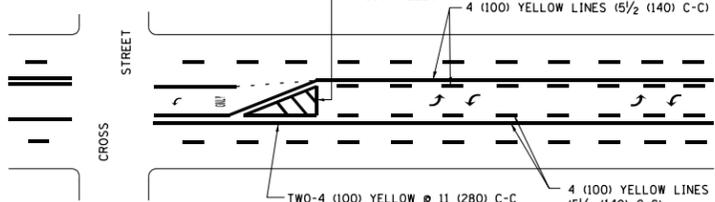


4' (1.2 m) WIDE MEDIANS ONLY

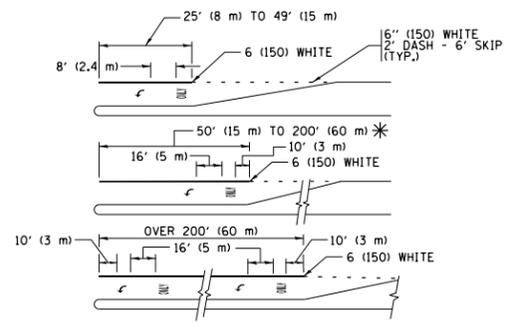


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

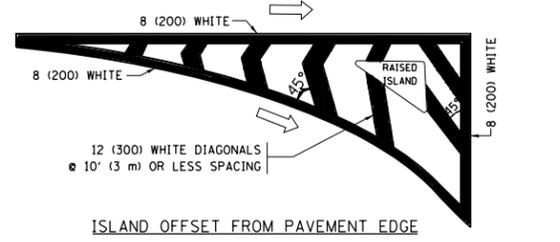


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

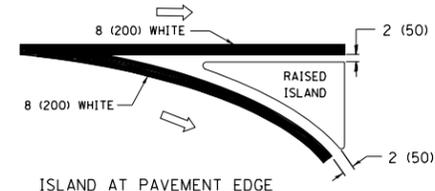


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

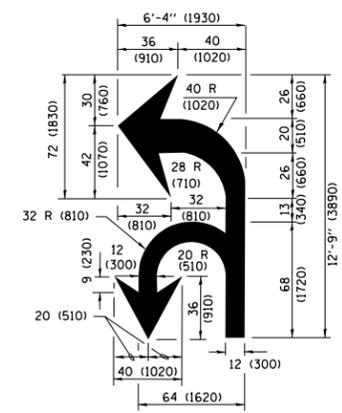
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



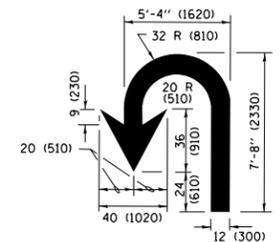
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to13.dgn	USER NAME = lqyso	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
Default	PLOT SCALE = 50.000' / in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE. 311	SECTION 10Y-BJR	COUNTY DUPAGE	TOTAL SHEETS 34	SHEET NO. 30
TC-13		CONTRACT NO. 62G51	ILLINOIS FED. AID PROJECT	

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

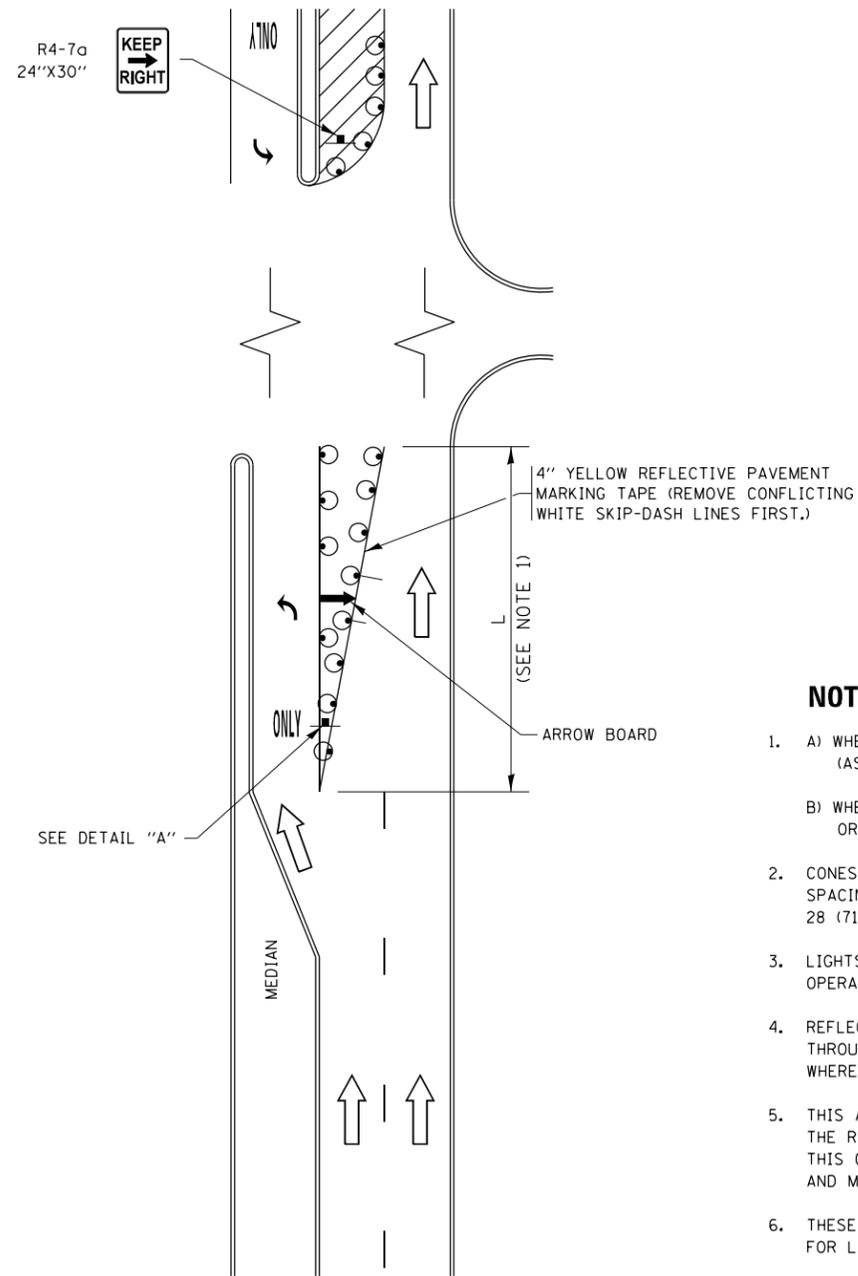


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

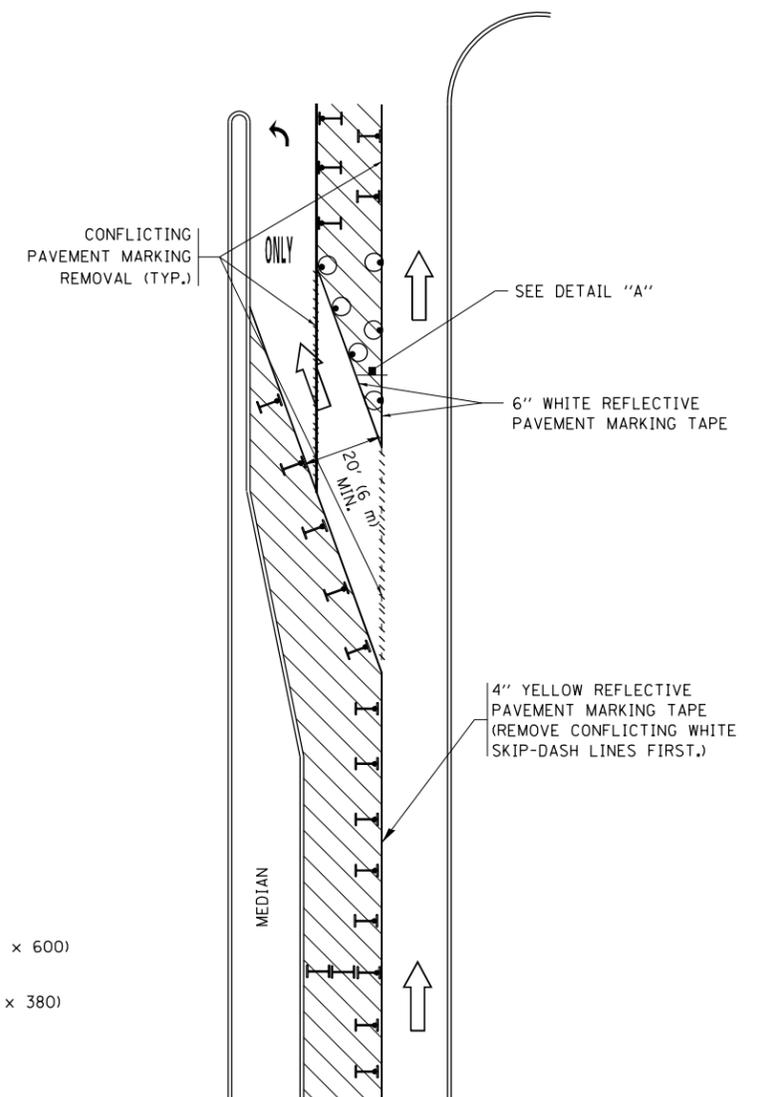


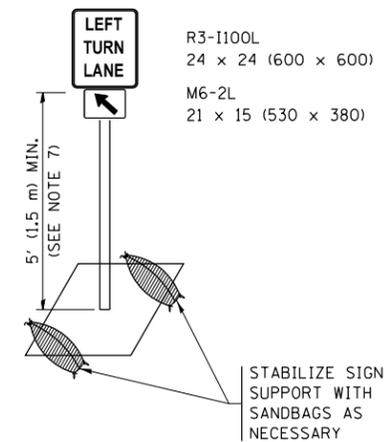
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

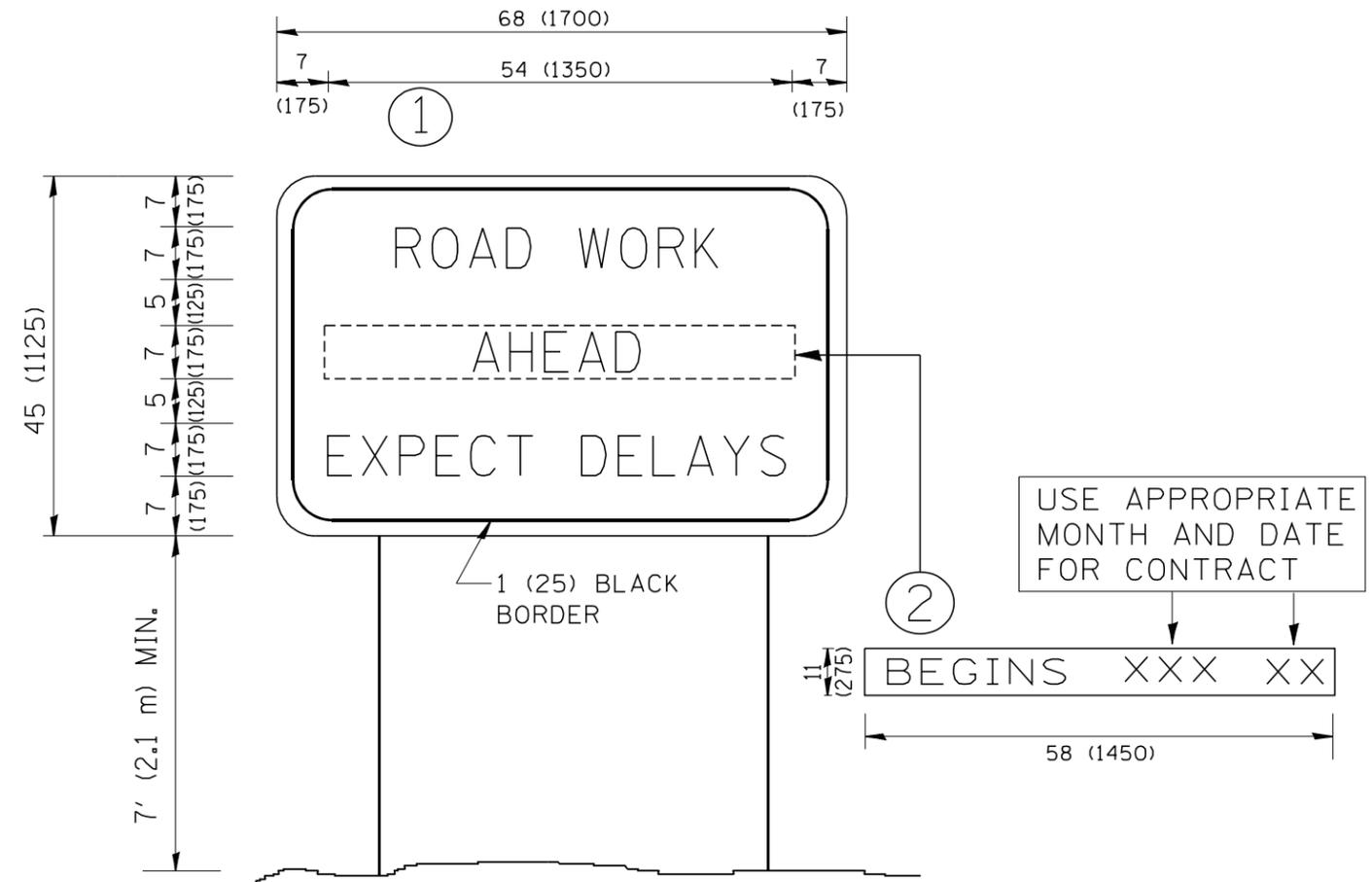
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
pw:\IL084EBIDINTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\ADDData\CAHOUSEH\10-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	311	10Y-BJR	DUPAGE	34	31
Default	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -					TC-14		CONTRACT NO. 62G51				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

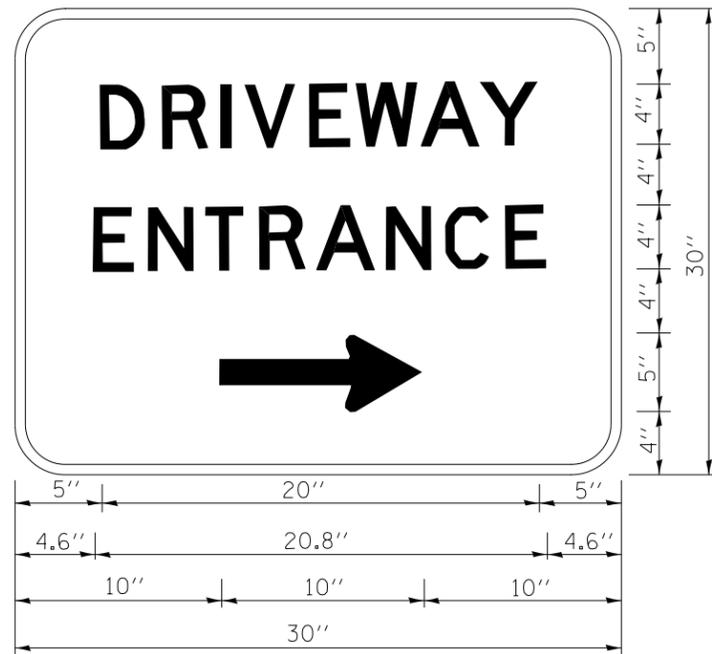
FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED - REVISED -
		REVISOR - CHECKED -	REVISOR - CHECKED -
		DATE -	DATE -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	33
TC-22			CONTRACT NO. 62G51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gegl1enobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pw\work\p1dot\gagl1enobt\d0108315\to26.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -
	PLOT SCALE = 50.000' / in.		
	PLOT DATE = 12/13/2012		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10Y-BJR	DUPAGE	34	34
TC-26			CONTRACT NO. 62G51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				