FOR INDEX OF SHEETS, SEE SHEET NO. 2 FOR HIGHWAY STANDARDS, SEE SHEET NO. 2

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 3696 (COMMERCIAL STREET) FAP 0303 (ILLINOIS ROUTE 173) TO FAP 0334 (U.S. ROUTE 12) RESURFACING SECTION 11-00020-00-RS

PROJECT M-9003 (929) **VILLAGE OF RICHMOND**

MCHENRY COUNTY C-91-188-12

> PROJECT LOCATION MAP RICHMOND TOWNSHIP N.T.S. **RANGE 8 EAST**

PROJECT END STA 249+18

William G. & Carol M. McHenry Co. Trust No. 51958 District Trust No. 74-2683 William G. Hansen Samuel S. 81.42 (101.92)

> PROJECT BEGIN STA 200+77

> > PROJECT LENGTH

Trust

V.B. & L.A.

Popenhagen

114.59

National

Bank Trust No. 51958

YW&TI No

NET LENGTH OF IMPROVEMENT (COMMERCIAL STREET) = 4,841 FT (0.92 MI)

GROSS LENGTH OF PROJECT = 4,841 FT (0.92 MI)

DIVISION OF HIGHWAYS

 \circ

0

ADT: COMMERCIAL STREET 1,000 VPD (2009)

POSTED SPEED

TRAFFIC DATA

20 MPH (EXISTING) 20 MPH (PROPOSED)

25 MPH (EXISTING)

DESIGN DESIGNATION

FAU 3696 (COMMERCIAL STREET) - COLLECTOR

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR **EXCAVATION**

Know what's below. Call before you dig.

CALL 811

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



420 NORTH FRONT STREET, SUITE 100 | McHENRY, ILLINOIS 60050 Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.cor ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

PROJECT ENGINEER: J. STRZALKA PROJECT MANAGER: A. CHAUDHRY

CONTRACT NO. 63776

 \circ



11-00020-00-RS

MCHENRY

ILLINOIS CONTRACT NO. 63776

12 1

DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS APPROVED December 1720 12

PASSED DECEMBER 31 2017 DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID

JANUARY Z. 20 /3 DEPUTY DIRECTOR OF HIGHWAYS REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

AKRAM CHAUDHRY, P.E.

EXPIRES: 11-30-2013

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

- 1 COVER SHEE?
- 2 INDEX OF SHEETS, HIGHWAY STANDARDS, BENCHMARKS AND GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5-6 ROADWAY PLANS
- 7-12 CONSTRUCTION DETAILS

HIGHWAY STANDARDS

000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS

442201-03 CLASS C AND D PATCHES

482011-03 HMA SHLD STRIPS / SHLDS WITH RESURFACING OR WIDENING

AND RESURFACING PROJECTS

701101-03 OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM

PAVEMENT EDGE

701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED

701901-02 TRAFFIC CONTROL DEVICES

720001-01 SIGN PANEL MOUNTING DETAILS 720006-03 SIGN PANEL ERECTING DETAILS

BLR 24-2 MAILBOX TURNOUT FOR LOCAL ROADS

DISTRICT ONE DETAILS

BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

BD~22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

BD-32 BUTT JOINT AND HMA TAPER DETAILS

TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS.

INTERSECTIONS, AND DRIVEWAYS

TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS

TC-22 ARTERIAL ROAD INFORMATION SIGN

BENCHMARKS

BMI: NORTH RIM OF VALVE VAULT LOCATED EAST OF U.S. ROUTE 12 AT THE SOUTH SIDE OF THE DRIVEWAY ENTRANCE TO SAVE INN HOTEL. ELEVATION-809.93 NAVD88

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT), JANUARY 1, 2012.
- 2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT
- 3. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES, IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- 5. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- 6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT OR VILLAGE.
- 7. SAW CUTTING OF PAVEMENTS, AND OTHER SURFACES SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- 8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARXERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- 9. WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
- 10. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE, IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY, A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
- 12. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS
 THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE
 MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING
 AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND
 BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE
 PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE
- 13. TEMPORARY RAMPS WILL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE CONSTRUCTION DETAILS. THE INSTALLATION AND REMOVAL OF THE TEMPORARY RAMPS WILL NOT BE PAID FOR SEPARALLY BUT INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT.
- 14. AGGREGATE SHOULDERS, TYPE 8 SPALL BE CONSTRUCTED WITH GRADATION CA-6 CRUSHED GRAVEL OR CRUSHED STONE. RECYCLED CONCRETE WILL NOT BE PERMITTED.
- 15. THE THICKNESS OF THE AGGREGATE SHOULDER SHALL EQUAL THE RESURFACING THICKNESS AT THE EDGE OF PAVEMENT AND WILL BE TAPERED TO ONE INCH (1") AT THE EDGE OF SHOULDER. THE CONTRACTOR WILL CONSTRUCT AND COMPACT THE SHOULDER TO THE SATISFACTION OF THE ENGINEER.
- 16. EXISTING AGGREGATE DRIVEWAYS WILL RECEIVE A TWENTY-FOUR INCH (24")
 WIDE ASPHALT APRON MATCHING THE PROPOSED RESURFACING LIFT THICKNESS. THE
 DRIVEWAY APRON WILL BE CONSTRUCTED IN CONJUNCTION WITH THE MAINLINE PAVING
 AND WILL NOT BE PAID FOR SEPARATELY.
- 17. EXISTING ASPHALT DRIVEWAYS WILL RECEIVE A FORTY-EIGHT INCH (48") WIDE ASPHALT APRON MATCHING THE PROPOSED RESURFACING LIFT THICKNESS. THE DRIVEWAY APRON WILL BE CONSTRUCTED IN CONJUNCTION WITH THE MAINLINE PAVING AND WILL NOT BE PAID FOR SEPARATELY.

NEW PAVEMENT SHALL MEET THE ELEVATION OF THE EXISTING PAVEMENT TO REMAIN.

REMOVAL OF THE EXISTING DRIVEWAY APRON WILL BE SAWCUT AND MILLED TO THREE AND THREE QUARTER INCHES (3 3/4") FOR A SATISFACTORY TRANSITION BETWEEN THE ROADWAY PAVEMENT AND THE DRIVEWAY PAVEMENT REMAINING AT EXISTING GRADE. THE REMOVAL WILL BE INCLUDED IN THE CONTRACT UNIT COST FOR HOT-MIX ASPHALT SURFACE REMOVAL, 2".

VILLAGE OF RICHMOND

GENERAL NOTES (CONT.)

- 18. EXISTING CONCRETE AND BRICK DRIVEWAYS WILL RECEIVE A SAW CUT ALONG THE FACE THE DRIVEWAY.
- 19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- 20. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- 21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE
- 22. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENMANCES THAT MUST BE KEPT IN OPERATION.
- 23. THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VALLTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- 24. REMOVAL OF EXISTING SIGN PANELS AND POSTS WILL BE INCLUDED IN THE CONTRACT UNIT COST OF METAL POST TYPE B. REMOVED SIGN PANELS WILL BE DELIVERED TO THE RICHMOND PUBLIC WORKS FACILITY.
- 25. ALL SIGNS SHALL CONFORM WITH MUTCH AND INCLUDE DIAMOND GRADE DG3 REFLECTIVE SHEETING.

 BOXED ITE	EMS INDICA	TE WORK	. TO BE	INCLUDED	IN	THE
 CONTRACT	UNIT PRIC	ES OR E	BY OTHE	RS.		

SCALE: NTS

SUMMARY OF QUANTITIES

SPECIALTY ITEMS (Δ)	SPECIAL PROVISION (*)	PAYITEM NUMBER	PAY ITEM DESCRIPTION	UNITS	TOTAL QUANTITY	ROADWAY 80% FEDERAL 20% LOCAL 0005
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,860	1,860
		40600300	A GGREGATE (PRIME COAT)	TON	38	38
:		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
		40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	2,355	2,355
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQYD	175	175
-		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,685	1,685
		44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQYD	19,130	19,130
		44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQYD	125	125
		44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQYD	125	125
	:	44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQYD	125	125
		48101200	AGGREGATE SHOULDERS, TYPE B	TON	379	379
		67100100	MOBILIZATION	L SUM	1	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
Δ		72000100	SIGN PANEL - TYPE 1	SQ FT	121.25	121.25
Δ		72900200	METAL POST - TYPE B	FOOT	279.5	279.5
Δ		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	154	154
, , , , , , , , , , , , , , , , , , ,	*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.40	51.40
	*	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	13

VILLAGE OF RICHMOND

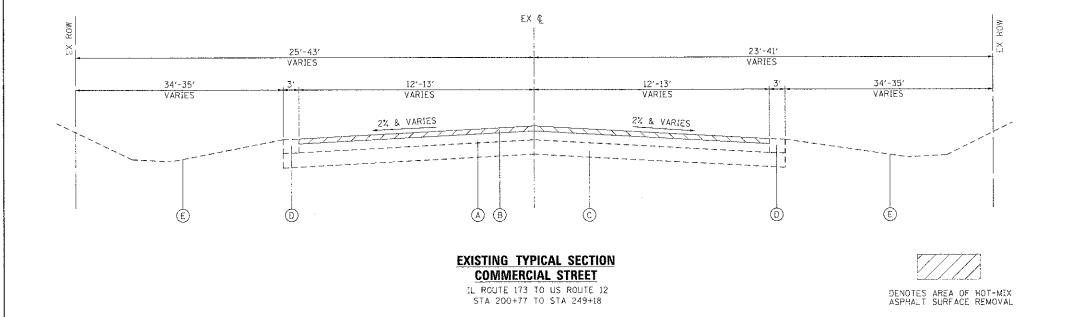
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USER NAME = jstrzal	DESIGNED - JJS	REVISED -
	DRAWN - JUS	REVISED -
PLOT SCALE = NTS	CHECKED - AC	REVISED -
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COMMERCIAL	STREET	
SUMMARY OF C	NUANTITIES	
	STA.	TO STA.

SCALE: NTS

F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEE NO.
3696	11-00020-00-RS	MCHENRY	12	3
		CONTRACT	NO.	63776
	ILLINOIS FED. A	D PROJECT		



PROPOSED TYPICAL SECTION COMMERCIAL STREET

IL ROUTE 173 TO US ROUTE 12 STA 20C+77 TO STA 249+18

EXISTING LEGEND

- (A) HOT-MIX ASPHALT PAVEMENT; DEPTH 6"±
- (B) HOT-MIX ASPHALT SURFACE REMOVAL, 2 INCH
- (C) AGGREGATE SUBBASE; 12"±
- D AGGREGATE SHOULDERS
- (E) EXISTING GROUND

PROPOSED LEGEND

- 1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; 1 1/2"
- EVELING BINDER (M.M.), N50: 2 1/4"
- 3 BITUMINOUS MATERIALS (PRIME COAT)
- (4) AGGREGATE (PRIME COAT)
- 5 CLASS D PATCHES, 4 INCH (AS DIRECTED BY THE ENGINEER)
- 6 AGGREGATE SHOULDERS, TYPE B (9% MAX SLOPE)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes		
RESURFACING			
LEVELING BINDER (MACHINE METHOD), N50; (IL-9.5mm); 2 1/4"	4% @ 50 GYR.		
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50; (IL-9.5mm); 1 1/2"	4% @ 50 GYR.		
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19mm); 4"	4% @ 70 GYR.		

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUATITIES IS 112 LBS/SQ YD/IN THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -28" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

COUNTY TOTAL SHEET NO. MCHENRY 12 4

CONTRACT NO. 63776

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

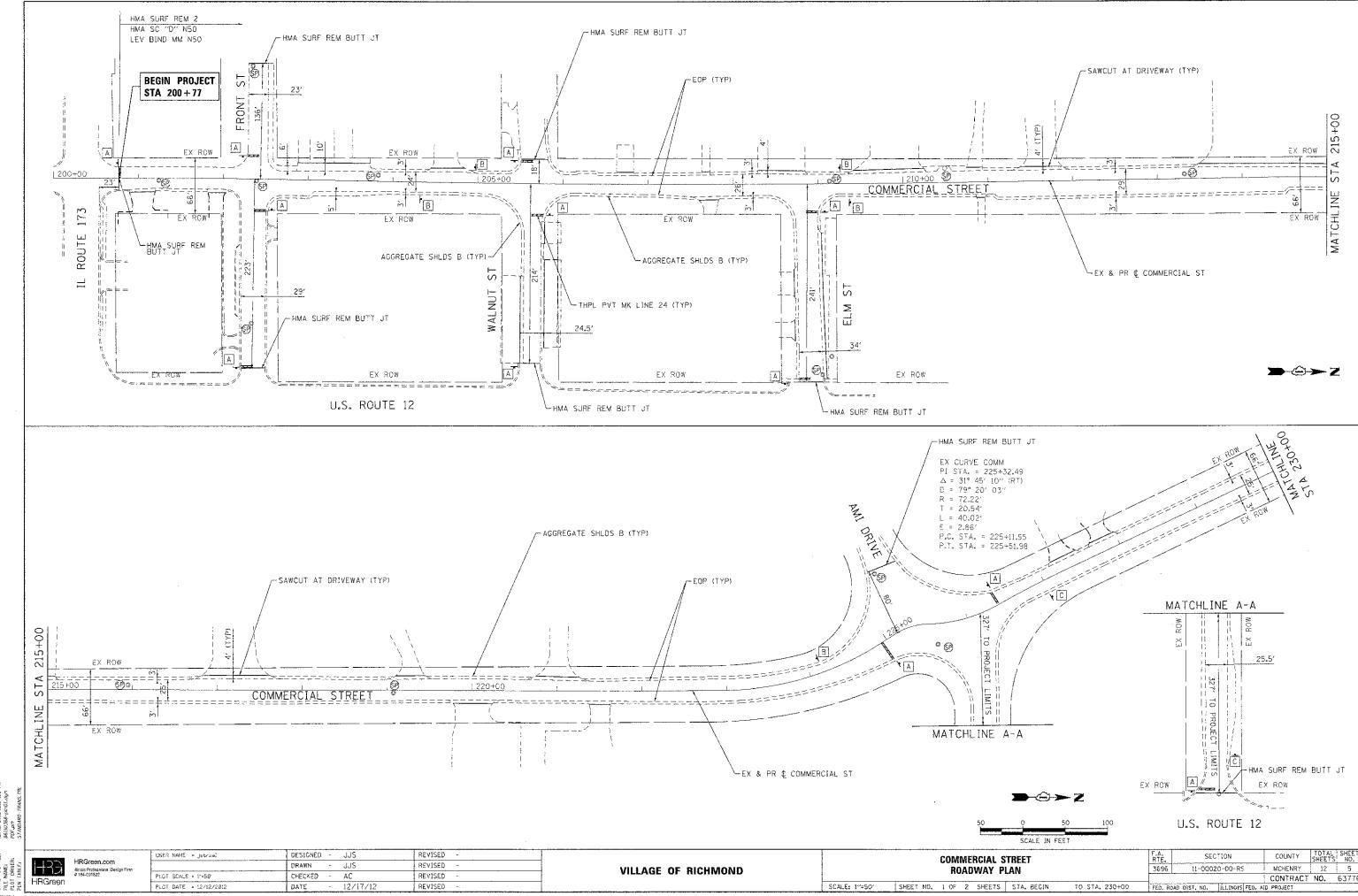
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USER NAME = Jatrzei	DESIGNED - JJS	REVISED -
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PLOT SCALE = NTS	CHECKED AC	REVISED -
9LOT DATE = 12/12/2012	DATE - 12/17/12	REVISED -

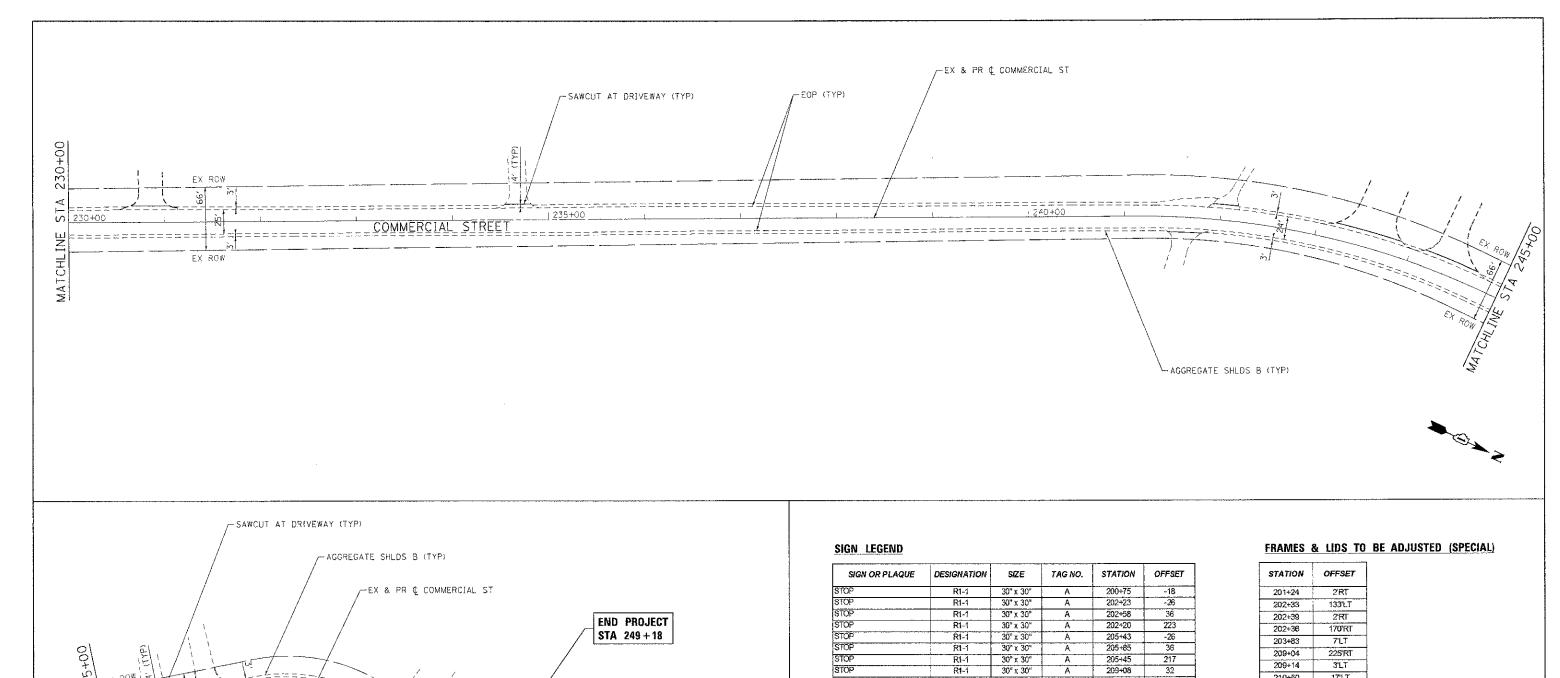
VILLAGE OF RICHMOND

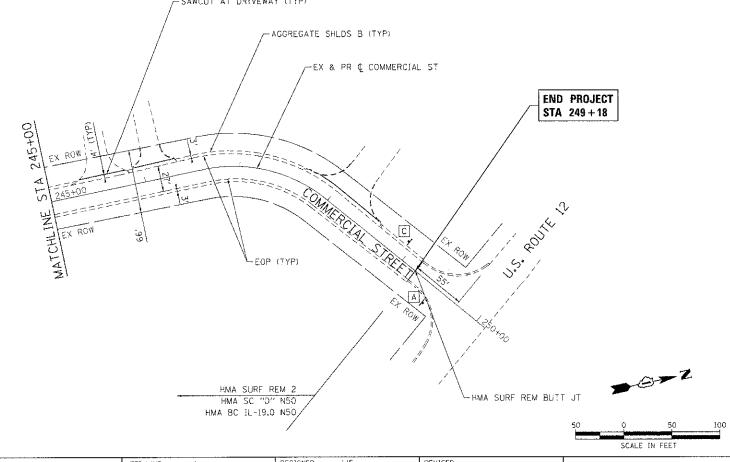
SCALE: NTS

COMMERCIAL STREET				F.A.U. RTE.	SECTION	COUNTY	
	TVO	ICAL SECT	LONG		3696	11-00020-00-RS	MCHENRY
	117	IUAL SEGI	IUNO				CONTRA
SHEET NO.	1 OF	1 SHEETS	STA. BEGIN	TO STA. END		ILLINOIS FED. A	ID PROJECT



C-91-188-12 COMMERCIAL STREET RESURFACTI B5110368-01001.dec





SIGN OR PLAQUE	DESIGNATION	SIZE	TAG NO.	STATION	OFFSET
STOP	R1-1	30" x 30"	Α	200+75	-18
STOP	R1-1	30" x 30"	A	202+23	-26
STOP	R1-1	30" x 30"	Α	202+58	36
STOP	R1-1	30" x 30"	A	202+20	223
STOP	R1-1	30" x 30"	Α	205+43	-26
STOP	R1-1	30" x 30"	A	205+85	36
STOP	R1-1	30° x 30°	A	205+45	217
STOP	R1-1	30° х 30°	Α	209+08	32
STOP	R1-1	30" x 30"	A	208+60	235
STOP	R1-1	30" x 30"	A	224+97	32
STOP	R1-1	30" x 30"	Α	224+49	298
STOP	R1-1	30" x 30"	A	226+40	-26
STOP	R1-1	30" x 30"	A	249+45	21
SPEED LIMIT 20 MPH	R2-1	24" x 30"	В	204+33	20
SPEED LIMIT 20 MPH	R2-1	24" x 30"	В	204+96	-19
SPEED LIMIT 20 MPH	R2-1	24" x 30"	В	209+25	-19
SPEED LIMIT 20 MPH	R2-1	24" x 30"	В	209+38	22
SPEED LIMIT 20 MPH	R2-1	24" x 30"	В	224+14	-18
SPEED LIMIT 30 MPH	R2-1	24" x 30"	Ċ	225+85	278
SPEED LIMIT 30 MPH	R2-1	24" x 30"	С	227+01	22
SPEED LIMIT 30 MPH	R2-1	24" x 30"	C	248+93	-20

STATION	OFFSET
201+24	2'RT
202+33	133'LT
202+39	2'87
202+36	170'RT
203+83	7'LT
209+04	225'RT
209+14	3°LT
210+50	17°LT
213+32	6'LT
215+95	5LT
219+07	3'RT
225+21	74'LT
225+63	24'RT
TOTAL =	13

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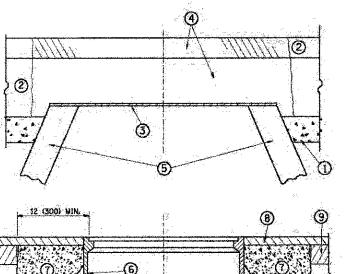
HRGreen.com Prois Professional Design Firm # 194-001822

DESIGNED - JJS REVISED USER NAME ⊊ jst⁄zal REVISED DRAWN PLOF SCALE = 1'=50' CHECKED AC REVISED REVISED PLOT DATE = 12/12/2012 DATE - 12/17/12

VILLAGE OF RICHMOND

COMMERCIAL STREET ROADWAY PLAN		SECTION	COUNTY	TOTAL C	
		11-00020-00-RS	MCHENRY	12	6
			CONTRACT	NO.	63776
T NO. 2 OF 2 SHEETS STA, 230+00 TO STA, END	FED, R	DAD DIST, NO. ILLINOIS FED. AL	D PROJECT		

SCALE: 1"=50" SHEET



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS LINESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAYERENT. SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLAYE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS,

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE CORST OF THE CORRESPONDING PAY ITEM.

SCALE: NONE

CONSTRUCTION PROCEDURES

STAGE 1 BEFORE PAVENENT MILLINGS

- AN REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM
- AROUND THE STRUCTURE. BI REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- CI COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER WETAL PLATE.

 D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40)
- DI BACKFILL WITH CRUSHED STONE AND A MINIMUM 11/2 (40 THICK HAS SUFFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 LAFTER PAVEMENT WILLINGS

- AT REMOVE THE HIMA SURFACE MIX AND CRUSHED STONE.
- HI INSTALL THE FRAME AND LID, ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-18 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *INLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 405, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LÉCEND

- SUB-BASE CHANULAR
- (6) FRAME AND LED (SEE NOTES)
- (2) EXISTING PAVENENT
- (7) CLASS PP-1# CONCRETE
- (3) 36 (900) DIAMETER METAL PLATE
- (8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HAVE SURFACE MIX
 EXISTING STRUCTURE
- (9) PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURNED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND LITLLITY STRUCTURES IN THE PAYEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT LINIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED ISPECIAL!"

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND LITILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (WILLIMETERS) UNLESS OTHERWISE SHOWN



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USER NAME = jetrzel	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
	DRAWN	REVISED - R. BORO 01-01-07
PLCT SCALE = ,	CHECKED -	REVISED - R. BORO 03-09-11
PLCT DATE = 12/12/2012	DATE 10-25-94	REVISED - R. BORO 12-05-11

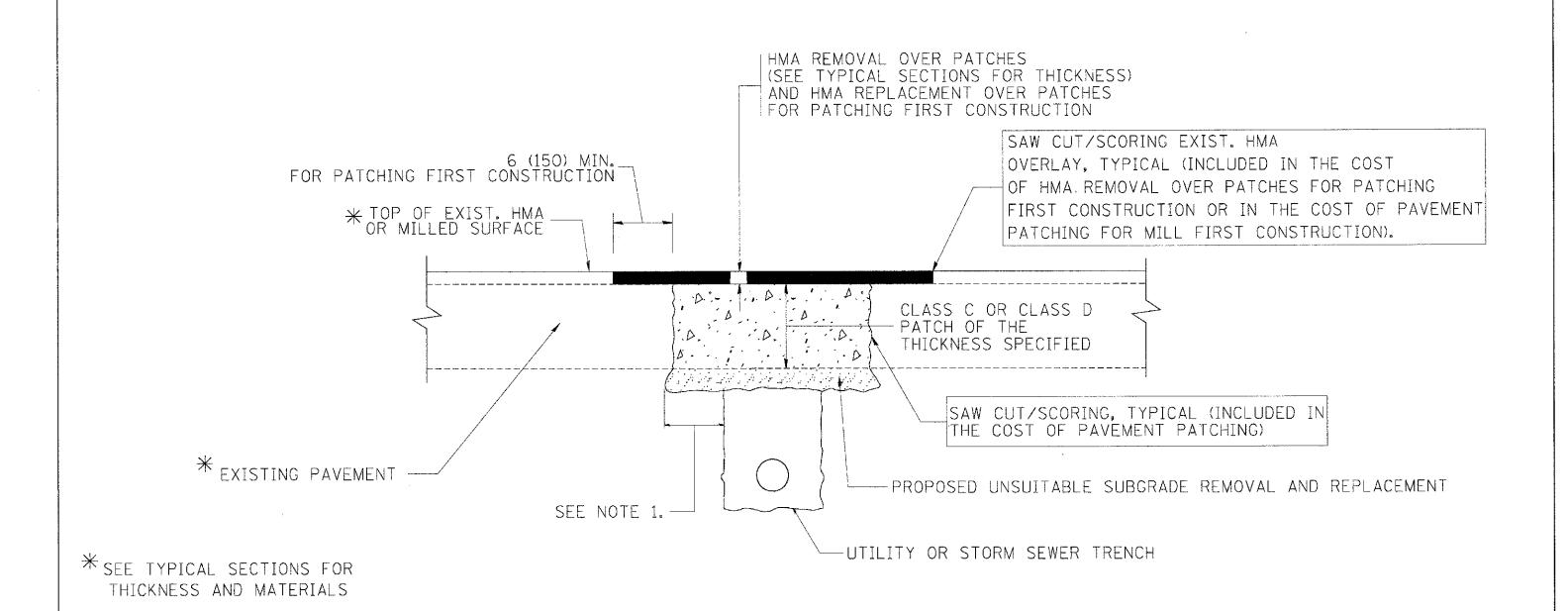
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH NILLING
SHEET NO. 1 OF 1 SHEETS STA, TO STA.

F.A.U. SECTION COUNTY TOTAL SHEETS NO. 3696 11-00020-00-RS MCHENRY 12 7

***BD600-03 (RD-8) CONTRACT NO. 63776

FED. ROME DIST. NO. 1 [ILLINOIS] FED. AD PROJECT



NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

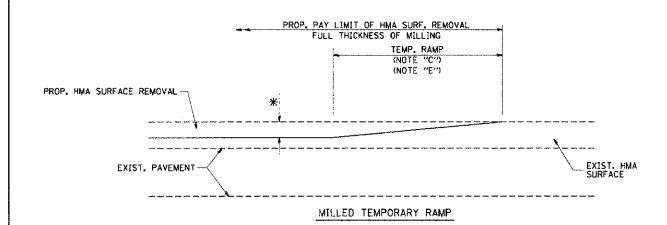
- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

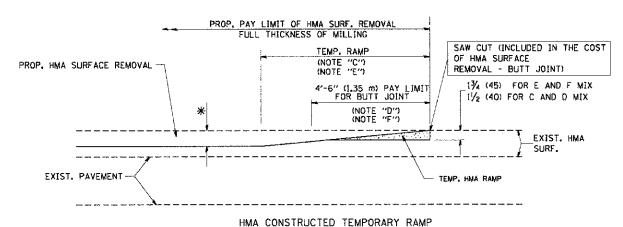
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

١.		100	USER NAME = Jstrzel	DESIGNED - R. SHAH	REVISED -	A. A8BAS 04-27-98		PAVEMENT PATCHING FOR	RTE.	, SECTION	COUNTY SF	HEETS	NO.
	H33 #	INGreen.com		DRAWN -	REVISED -	R. BORO 01-01-07	STATE OF ILLINOIS	1	3696	11-00020-00-RS	MCHENRY	12	8
!		184-001322	PLOT SCALE =	CHECKED -	REVISED -	R. BORO 09-04-07	DEPARTMENT OF TRANSPORTATION	HMA SURFACED PAVEMENT		8D400-04 (BD-22)	CONTRACT N	10. 6	3776
'	-IRGreen		PLOT DATE = 12/12/2012	DATE - 10-25-94	REVISED -	K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED, R	OAD DIST. NO. 1 ILLINOIS FED. A	ID PROJECT		



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

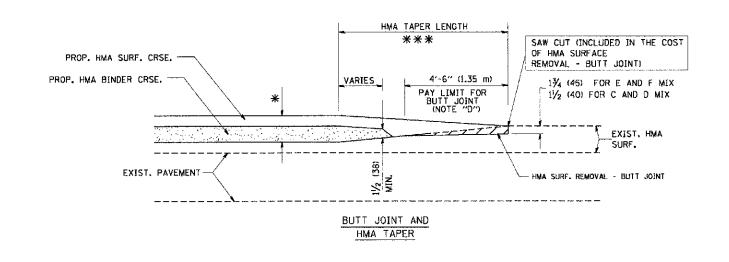
OPTION 1



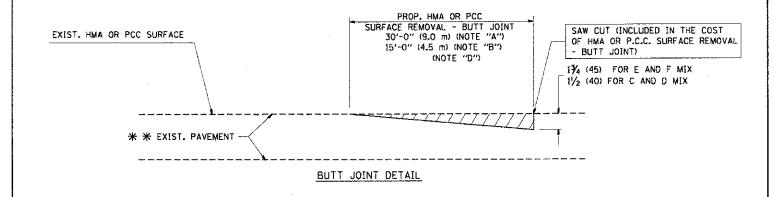
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

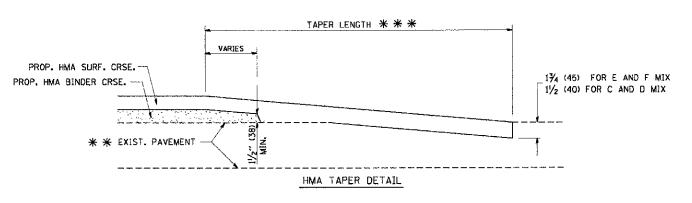
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE. HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP, RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE,
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP, RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SOUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

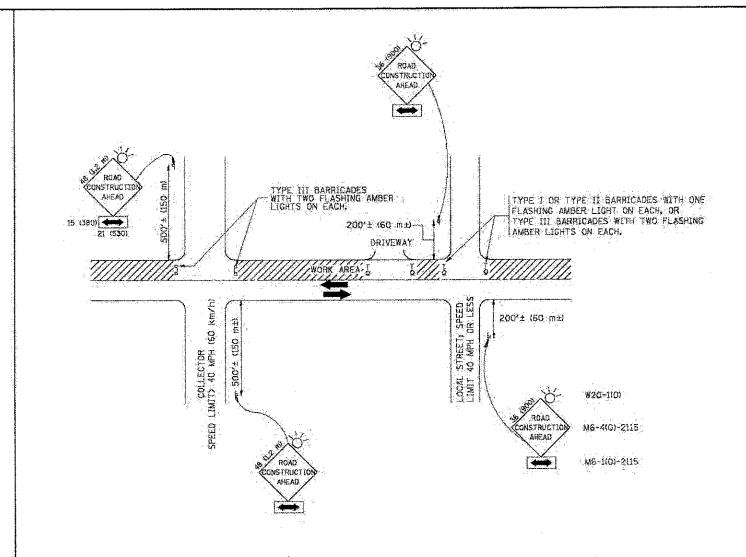
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



USER NAME = jstrzal	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE =	CHECKED -	REVISED - M. COMEZ 04-06-01
PLOT DATE = 12/12/2012	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS		SECTION	COUNTY TOTAL SHEETS		SHEET NO.
		11-00020-00-RS	MCHENRY	12	9
		BD400-05 BD32	CONTRACT	NO.	63776
SCALE NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. RO	DAD DIST. NO. 1 ILLINOIS FED. AL	D PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 11 SIDE ROAD WITH A SPEED LIMIT OF 40 MPH 150 KM/H) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
- OU ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x930). WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200" (60 m) IN ADVANCE OF THE MAIN BOUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I; TYPE II OF TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 KM/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
- OF ONE ROAD CONSTRUCTION AHEAD SIGN 48 % 48 C.2 TH % LZ TH WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500 1150 THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROJECTED BY BLOCKING WITH TYPE III BARRICADES, 172 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE HOAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW IMG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW IMG-4).

SCALE: NONE

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- O. ADVANCE WARNING SIGNS ARE TO BE GMITTED ON DRIVEWAY LINLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE BOADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS

All dimensions are in millimaters dinanes) Unless otherwise shown.

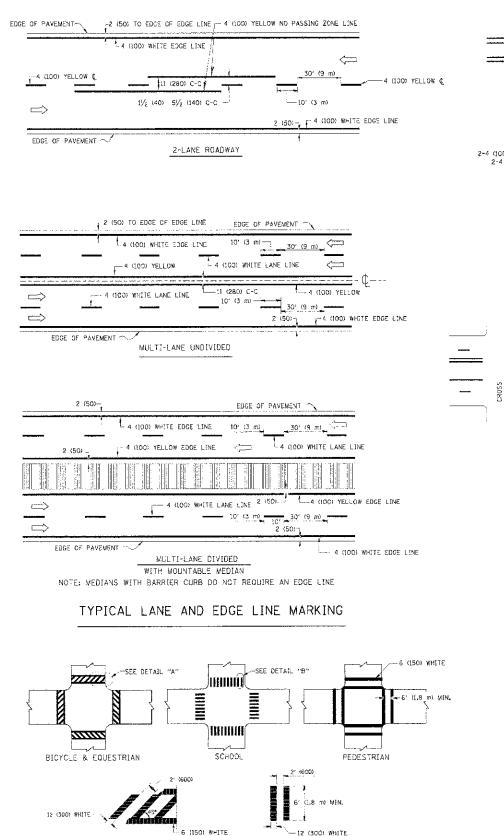
HRGreen

HRGreen.com

| USER NAME = jstrza: | DESIGNED - LHA | REVISED - L DBERLE 10-18-95 |
| DRAWN - REVISED - A. HOUSER 03-06-96 |
| PLCT SCALE = CHECKED - REVISED - A. HOUSER 10-15-96 |
| PLCT DATE = 12/12/2012 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-06

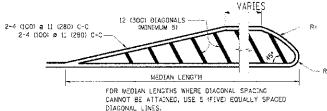
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
SHEET NO. 1 OF 1 SHEETS | STA. 10 STAL



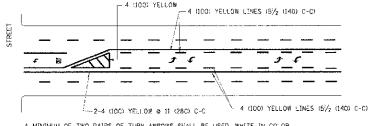
2-4 (100) YELLOW @ 11 (280) C-C-4' (1.2 m) CUTSIDE TO NO DIAGONALS OUTSIDE OF LINES - 2-4 (190) YELLOW & 11 (280) C-C

4' (1.2 m) WIDE MEDIANS ONLY



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TC 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

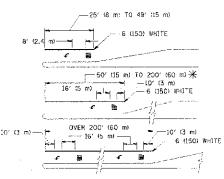


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANG

TYPICAL PAINTED MEDIAN MARKING

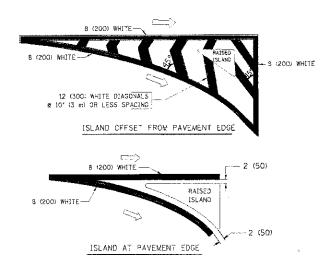


FULL SIZE LETTERS 8° (2.4 m) AND ARROWS SHALL BE USED. \P_1 AREA = 15.6 SQ. FT. (1.5 m²) \P_1 AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH WAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 2 4 (100)	SOLIC	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 g 4 (100)	SOLID SOLID	YELLOW .	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	MHILE MHILE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLIO	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2,4mi)	SCLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 to 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHETE	SEE TYPICAL TWO-WAY LEST TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1,8 m) APART 2' (500) APART 2' (500) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' 11.2 TO IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PASSENT. OTHERMISE, PLACE AT DESERRE STOPPING POINT. FARALLEL TO EROSSROAD CENTERLINE, WHERE POSSENEE
PAINTED MEDIANS	2 g 4 (100) WITH 12 (300) DIAGONALS	SOLID	YELLOW: TWO WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE
	0 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS		WHITE: DNE WAY TRAFFIC	SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	01ACONALS: 15' 14,5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) 70 45MPH (TC km/h)) 30' (9 m) C-C (OVER 45MPH (TO km/h))
RAILROAD CROSSING	24 (6CO) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "M"=3.6 SO, FT. (0.33 m ²) EACH "X"=54.0 SO, FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) # 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (I.ESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) T0 45MPH (70 km/h)) 150' (45 m) C-C (CVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT WARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

SCALE

All dimensions are in inches (millimeters)

untess otherwise shown.

	USER NAME = jetrzal	DESIGNED - EVERS	REVISED	-7. RAMMACHER 10-27-94
HRGreen.com tilinois Professional Design Firm #184-001322		DRAWN -	REVISED	-C. JUCIUS 09-09-09
	PLOT SCALE =	CHECKED -	REVISED	~

- 03-19-90

REVISED

DETAIL "B"

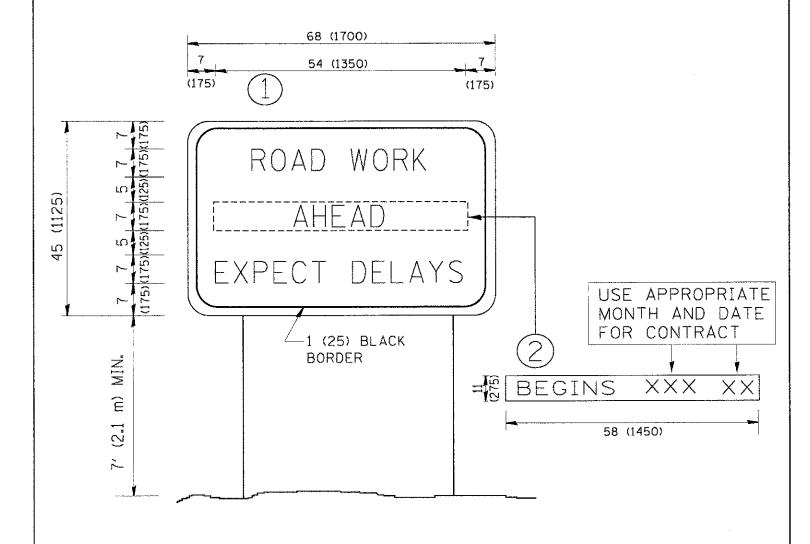
TYPICAL CROSSWALK MARKING

PLGT DATE = 12/12/2012

HRGreen

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		SECTION	COUNTY	TOTAL	SHEET NO.
		11-00020-00-RS	MCHENRY	12	11
		TC-13	CONTRACT	NO.	63776
LE NONE SHEET NO. 1 OF 1 SHEETS (STA. TO STA.	EED, RO	AS BIST, NO. 1 !!ELINOIS: FED. A	ID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.

SCALE: NONE

- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN () WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

	HRGreen.com
	Hinois Professional Design Firm # 184-001322
HRGreen	

USER NAME = jatrzel	DESIGNED -	REVISED - R. MIRS 09-15	-97
	DRAWN -	REVISED - R. MIRS 12-11-	97
PLOT SCALE =	CHECKED -	REVISED -T. RAMMACHER 02	-02-99
PLC1 DATE = 12/12/2012	DATE -	REVISED - C. JUCIUS 01-	31-07

STATI	E OI	FILLINOIS
DEPARTMENT	OF	TRANSPORTATION

		F.A.U. SECTION		TOTAL SHE SHEETS NO	
		11-00020-00-RS	MCHENRY	12	12
		TC-22	CONTRACT	NO.	63776
SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FEO. RO	DAD DIST, NO. 1 ILLINOIS FED. A			