TOTAL SHEETS: 63 VE059

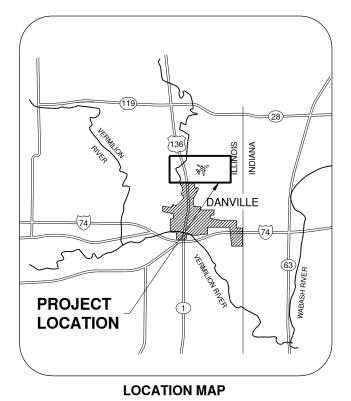
CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT AUTHORITY

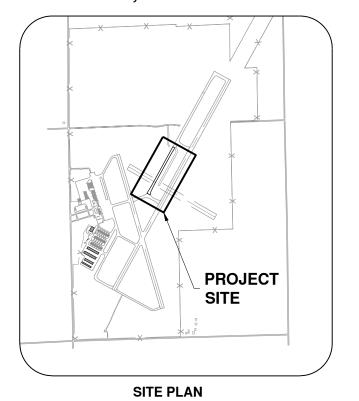
VERMILION REGIONAL AIRPORT DANVILLE, IL

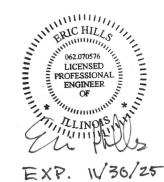
IL. PROJ. NO: DNV-5169 FED. PROJ. NO. 3-17-SBGP-TBD

REALIGN TAXIWAY A, PHASE 3

SUBMITTAL DATE: FEBRUARY 28, 2025







COMMON TRAFFIC ADVISORY FREQUENCY 122.7 APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT



COMMON GROUND ALLIANCE www.call811.com or

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND JILILIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND JILILIES, PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL MOTIFY THE TILLITES PRIOR ECONSTRUCTION, HE CONTRACTOR SHALL NOTH'T IN TILLITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM HE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND SSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE ORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT HIFER REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL MINEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE NOINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND ERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR.

> CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS

> > CALL JULIE BEFORE EXCAVATING 1-800-892-0123

TOWNSHIP: 20 NORTH RANGF: 11 WEST SECTION: 15 COUNTY: VERMILION

BASE BID SUMMARY OF QUANTITIES					
PAY ITEM	ITEM DESCRIPTION	UNIT	QTY		
AR108158	1/C #8 5 KV UG CABLE IN UD	FOOT	3,100.0		
AR108258	2/C #8 5 KV UG CABLE IN UD	FOOT	1,850.0		
AR108706	1/C #6 COUNTERPOISE	FOOT	3,100.0		
AR108960	REMOVE CABLE	FOOT	3,400.0		
AR125410	MITL - STAKE MOUNTED	EACH	16.0		
AR125415	MITL - BASE MOUNTED	EACH	7.0		
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2.0		
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	1.0		
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2.0		
AR125565	SPLICE CAN	EACH	1.0		
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	1.0		
AR125906	REMOVE SPLICE CAN	EACH	2.0		
AR125913	REMOVE EDGE LIGHT	EACH	18.0		
AR150510	ENGINEER'S FIELD OFFICE	LSUM	1.0		
AR150520	MOBILIZATION	LSUM	1.0		
AR150550	CONSTRUCTION ACCESS	LSUM	1.0		
AR150552	RESTORE CONSTRUCTION ACCESS	LSUM	1.0		
AR152410	UNCLASSIFIED EXCAVATION	CUYD	16,950.0		
AR152490	CRUSHED AGGREGATE BACKFILL	CUYD	650.0		
AR152515	SUBGRADE UNDERCUT	CUYD	650.0		
AR156510	SILT FENCE	FOOT	350.0		
AR156513	SEPARATION FABRIC	SQYD	1,300.0		
AR156515	STRAW WATTLE	FOOT	350.0		
AR156520	INLET PROTECTION	EACH	4.0		
AR209607	CRUSHED AGG. BASE COURSE - 7"	SQYD	6.500.0		
AR401010	CONTRACTOR QUALITY CONTROL PROGRAM	LSUM	1.0		
AR401610	BITUMINOUS SURFACE COURSE	TON	720.0		
AR401921	REMOVE PAVEMENT	SQYD	4,900.0		
AR403610	BITUMINOUS BASE COURSE	TON	1,830.0		
AR602510	BITUMINOUS PRIME COAT	GALLON	2,000.0		
AR603510	BITUMINOUS TACK COAT	GALLON	650.0		
AR620520	PAVEMENT MARKING - WATERBORNE	SQFT	2,600.0		
AR620525	PAVEMENT MARKING - BLACK BORDER	SQFT	2,800.0		
AR620900	PAVEMENT MARKING REMOVAL	SQFT	500.0		
AR701518	18" RCP, CLASS IV	FOOT	470.0		
AR701530	30" RCP, CLASS IV	FOOT	180.0		
AR701900	REMOVE PIPE	FOOT	650.0		
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	FOOT	2,700.0		
AR705544	4" NON PERFORATED UNDERDRAIN	FOOT	60.0		
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	1.0		
AR705640	UNDERDRAIN CLEANOUT	EACH	7.0		
AR705645	UNDERDRAIN CONNECTION	EACH	1.0		
AR705900	REMOVE UNDERDRAIN	FOOT	600.0		
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	3.0		
AR751411	INLET - TYPE A	EACH	2.0		
AR751550	MANHOLE 5'	EACH	2.0		
AR751900	REMOVE INLET	EACH	2.0		
AR800194	REMOVE ELEVATED RETROREFLECTIVE MARKER	EACH	4.0		
AR801456	UNDERDRAIN DIRECT CONNECT	EACH	2.0		
AR801512	JUMPER CABLE	FOOT	300.0		
AR901510	SEEDING	ACRE	23.5		
AR904510	SODDING	SQYD	1,520.0		
AR908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	23.5		
VI 19009 14	EIGHT DOTT HTDHAOLIO WIOLOTT	AURL	۵۵		

ΑC	DITIVE ALTERNATE #1 SUMMARY OF QU	JANTITIE	S
PAY ITEM	ITEM DESCRIPTION	UNIT	QTY
AS152490	CRUSHED AGGREGATE BACKFILL	CUYD	180.0
AS152515	SUBGRADE UNDERCUT	CUYD	180.0
AS156513	SEPARATION FABRIC	SQYD	400.0
AS209607	CRUSHED AGG. BASE COURSE - 7"	SQYD	1,800.0
AS401610	BITUMINOUS SURFACE COURSE	TON	200.0
AS403610	BITUMINOUS BASE COURSE	TON	500.0
AS403630	BITUMINOUS BASE TEST SECTION	EACH	1.0
AS602510	BITUMINOUS PRIME COAT	GALLON	550.0
AS603510	BITUMINOUS TACK COAT	GALLON	200.0
AS620520	PAVEMENT MARKING - WATERBORNE	SQFT	75.0
AS620525	PAVEMENT MARKING - BLACK BORDER	SQFT	150.0

	ADDITIVE ALTERNATE #2 SUMMARY OF QUANTITIES						
P/	PAY ITEM ITEM DESCRIPTION UNIT QTY						
AT	152410	UNCLASSIFIED EXCAVATION	CUYD	100.0			
AT	156520	INLET PROTECTION	EACH	1.0			
AT	701518	18" RCP, CLASS IV	FOOT	300.0			
AT	701900	REMOVE PIPE	FOOT	530.0			
AT	751411	INLET - TYPE A	EACH	1.0			
AT	901510	SEEDING	ACRE	2.0			
AT	908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	2.0			

	HOT MIX ASPHALT (HMA) MIXTURE REQUIREMENTS TABLE							
ITEM	UNDER 60K LBS. RUNWAY/TAXIWAY	PG BINDER	AGGREGATE QUALITY	MAX RAP	DENSITY ACCEPTANCE			
	NDES		QUALITY	NAP				
401: SURFACE (2")					<u>METHOD I</u>			
	N30@3.0%	SBS PG70-28		MAINLINE: NUCLEAR DENSITY				
					JOINTS: CORES			
403: BINDER (5")		TOP LIFT:			METHOD I (METHOD II IF AA #1 IS AWARDED)			
	Noo Co oo	SBS PG70-28	В		METHOD I: MAINLINE - NUCLEAR GAUGE			
MIN. LIFT THICKNESS	N30@3.0%	LOWER LIFT:	P	20%	JOINTS - CORES			
- 2.25"		PG 64-22			METHOD II: PWL			
	PG 64-22			CORES				

	She	et List Table
Sheet No.	Sheet Index	Sheet Title
1	GI001	COVER SHEET
2	GI002	INDEX TO SHEETS & SUMMARY OF QUANTITIES
3	GI003	WORK LIMITS
4	GI101	AIRPORT SITE PLAN & CONSTRUCTION ACCESS
5	GC001	CONSTRUCTION ACTIVITY NOTES & DETAILS 1
6	GC002	CONSTRUCTION ACTIVITY NOTES & DETAILS 2
7	GC003	CONSTRUCTION ACTIVITY NOTES & DETAILS 3
8	GC101	CONSTRUCTION ACTIVITY PLAN OVERVIEW
9	GC102	CONSTRUCTION ACTIVITY PLAN WORK AREA A
10	GC103	CONSTRUCTION ACTIVITY PLAN WORK AREA B
11	GC104	PROJECT CONTROL PLAN
12	CD101	DEMOLITION PLAN 0
13	CD102	DEMOLITION PLAN 2 EXISTING PAVEMENT SECTIONS
14 15	CD301 CD501	PAVEMENT DEMOLITION AND SUBGRADE DETAIL
16	CP101	PROPOSED GEOMETRY PLAN 1
17	CP102	PROPOSED GEOMETRY PLAN 2
18	CP201	PLAN & PROFILE TWY A
19	CP202	PLAN & PROFILE TURF RWY
20	CP301	NEW PAVEMENT SECTIONS
21	CS101	STAKING PLAN
22	CU101	GRADING & DRAINAGE PLAN 1
23	CU102	GRADING & DRAINAGE PLAN 2
24	CU103	GRADING & DRAINAGE PLAN 3
25	CU201	UNDER DRAIN PROFILE & SCHEDULE
26	CU202	STORM SEWER PROFILE & SCHEDULE
27	CU501	UNDERDRAIN DETAILS 1
28	CU502	UNDERDRAIN DETAILS 2
29	CU503	DRAINAGE TRENCH DETAILS
30	CU504	DRAINAGE MANHOLE DETAIL 1
31	CU505	DRAINAGE MANHOLE DETAIL 2
32	CU506	DRAINAGE MANHOLE DETAIL 3
33	CU507	FRAMES & LIDS
34	CU508	DRAINAGE STRUCTURE DETAIL 1
35	CU509	DRAINAGE STRUCTURE DETAIL 2
36	CE101	ELECTRICAL & SIGN LAYOUT 1
37	CE102	ELECTRICAL & SIGN LAYOUT 2
38	CE103	PROPOSED JUMPER SCHEMATIC
39	CE501	ELECTRICAL DETAILS 1
40	CE502	ELECTRICAL DETAILS 2
41	CE503	ELECTRICAL DETAILS 3
42	CE504	ELECTRICAL DETAILS 4
43	CM101	PAVEMENT MARKING 1
44	CM102	PAVEMENT MARKING 2
45	CM501	PAVEMENT MARKING DETAILS
46	CL101	EROSION CONTROL & TURFING 1
47	CL102	EROSION CONTROL & TURFING 2
48	CL103	EROSION CONTROL & TURFING 3
49	CL501	EROSION CONTROL DETAILS
50	CG600	CROSS SECTION INDEX
51	CG601	PROPOSED TAXIWAY A CROSS SECTION 1
52	CG602	PROPOSED TAXIWAY A CROSS SECTION 2
53	CG603	PROPOSED TAXIWAY A CROSS SECTION 3
54	CG604	PROPOSED TAXIWAY A CROSS SECTION 4
55	CG611	PROPOSED TURE RUNWAY SECTION 1
56	CG612	PROPOSED TURE RUNWAY SECTION 2
57	CG613	PROPOSED TURE RUNWAY SECTION 4
58	CG614	PROPOSED TURF RUNWAY SECTION 4
59	CG621	PROPOSED TAXIWAY A1 CROSS SECTION 1
60	CG631	PROPOSED STOCKPILE SECTION 1
61	CG632	PROPOSED STOCKPILE SECTION 2
62	CG633	PROPOSED STOCKPILE SECTION 3
63	GC634	PROPOSED STOCKPILE DETAIL



License No. 184-00613

100% SUBMITTAL APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNE



VERMILION REGIONAL

AIRPORT AUTHORITY

DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-GI002.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: EMH

COPYRIGHT:

HEET TITLE

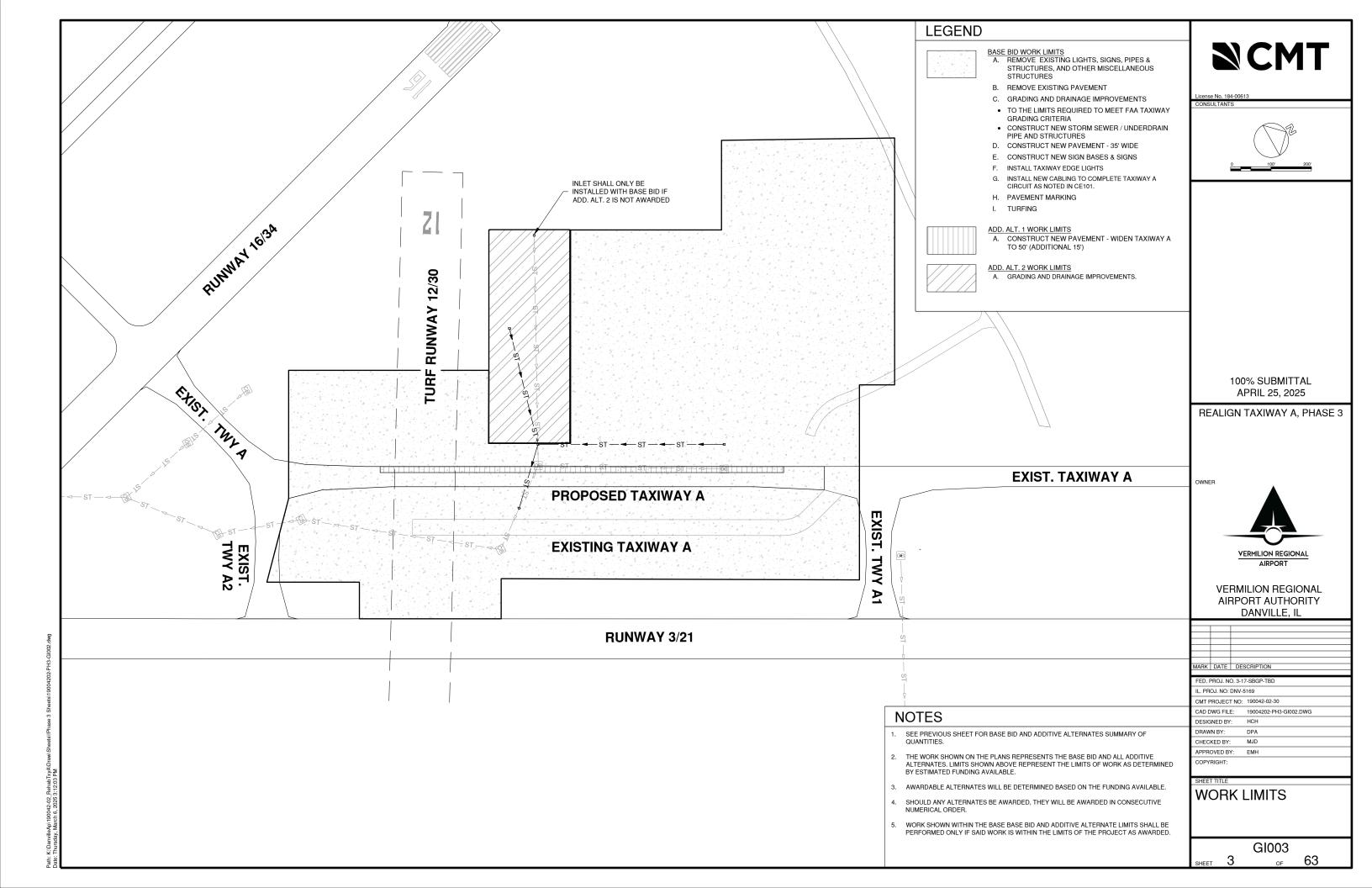
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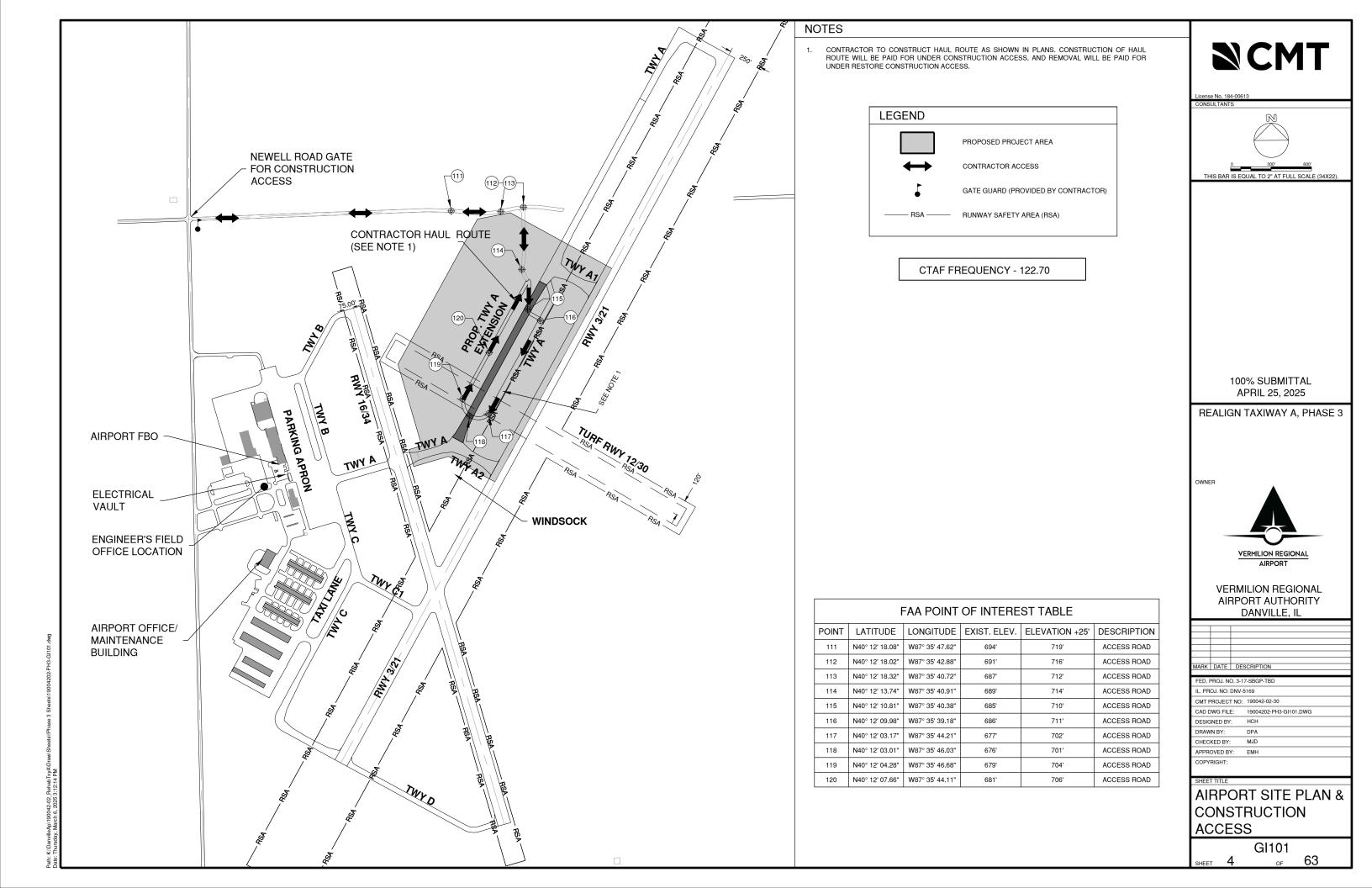
GI002

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2 of

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- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNI ESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL
 ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT,
 ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF
 PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE
 SHALL BE INCIDENTAL TO THE CONTRACT.
- 2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- I. PHASING SHALL BE AS SHOWN ON THE CONSTR. ACTIVITY PLAN SHEETS.
- 2. THE PROJECT SHALL BE COMPLETED WITHIN 159 CALENDAR DAYS FOR BASE BID + ADDITIVE ALTERNATE #1 & #2.
- PRIOR TO BALLOONS OVER VERMILION EVENT, THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL BY JULY 4TH, WITH NO WORK OCCURRING UNTIL JULY 13TH. TRAFFIC CONTROL WILL NEED TO BE RE-INSTALLED PRIOR TO COMMENCING WORK AFTER THIS EVENT.

3. CONSTRUCTION ACTIVITY AREAS

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS ALLOWED IN THE PLANS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- 3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAID FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN IN THE PLANS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING AND AFTER WORK HOURS OR THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED AIRPORT MANAGEMENT OR THE DESCIENT ENGINEED
- 3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T. THE CONTRACTOR SHALL ENTER A ROAD USE AGREEMENT WITH THE ROAD DISTRICT OF VERMILION COUNTY, ILLINOIS.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER THE PLAN DETAILS AND SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.

5. CONTRACTOR ACCESS (CONTINUED)

- 5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) SHALL BE UNDER THE CONTROL OF AND SUPERVISED BY THE CONTRACTOR.
- 6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH AIR TRAFFIC AT ALL TIMES WHEN WITHIN THE PERIMETER FENCE AND MOVEMENT AREA. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY PASSED THE APPROVED AIRPORT TESTS MAY OPERATE THESE RADIOS.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH AIR TRAFFIC. THE CONTRACTOR SHALL PROVIDE HIS/HER OWN FLAGMEN
- ALL CONSTRUCTION PERSONNEL AND VEHICLES SHALL BE RESTRICTED TO THE WORK AREA THROUGH THE USE OF FLAGGING AND BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING ESCORTS AS SHOWN IN PLANS OR OTHERWISE DESIGNATED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR'S STORAGE AND STAGING AREAS WILL BE AS SHOWN IN THE PLANS.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- 11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT OBSTRUCT ANY RUNWAY VISUAL AIDS, SIGNS OR NAVIGATIONAL AIDS OR PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- 12. WHEN NOT IN USE, THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE RETURNED TO & STORED AT THE STAGING AREA OR IN THE LOCATION SHOWN ON THE PLANS OR IN AN AREA OUTSIDE THE RUNWAY SAFETY AREAS (RSA'S), RUNWAY OBJECT FREE ZONES (ROFZ'S), AND OBJECT FREE AREA (OFA'S). THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL NOT BE PARKED ON A CLOSED TAXIWAY OR RUNWAY
- 13. THE STAGING SHALL BE USED FOR PARKING BY THE CONTRACTOR'S EMPLOYEES UNLESS OTHERWISE DESIGNATED BY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. USE OF PERSONAL VEHICLES BEYOND THE STAGING AREA WILL NOT BE ALLOWED
- 14. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
- 15. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAYEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- 16. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- . ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- . THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF ANY WILDLIFE IS SEEN ON OR ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND SECURED WHEN THE CONTRACTOR IS NOT WORKING.
- 3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
- 4. THE CONTRACTOR SHALL CONTROL GRASS HEIGHTS THROUGH MOWING TO ASSIST WITH WILDLIFE CONTROL.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MGMT

 THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.

- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE AIRPORT PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT. THE AIRPORT RETAINS THE AUTHORITY FOR THE FINAL DECISION ON PAVEMENT CLOSURES.
- 3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE RIPPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2 (LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST AND ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS SUITABLE TO BE OPENED.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. THE AIRPORT, AND CONTACT THE LOCAL FAA OFFICE (847-294-7336) TO ARRANGE FOR UTILITY LOCATES.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP, THE CONTRACTOR'S APPROVED SPCD OR THE SECURITY PLAN MAY RESULT IN FINES AS ALLOWED BY LAW.
 - FINES CAN BE LEVIED AGAINST THE CONTRACTOR FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

13. SPECIAL CONDITIONS

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE PLANS.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF THE PLANS & FAA AC 150/5370-2 (LATEST VERSION.)
- 3. IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE/WORK AREA, THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED RUNWAYS AND TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVER OR DE-ENERGIZE AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS ASSOCIATED WITH THIS SHALL BE INCIDENTAL TO THE CONTRACT.

15. MARKING AND SIGNS FOR ACCESS ROUTES

 BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED IN THE PLANS.

9. NOTIFICATIONS OF CONSTRUCTION ACTIVITIES 16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 AND 150/5210-5 (LATEST EDITION) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE AIRPORT.
- 4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. NIGHTTIME WORK ZONE LIGHTING

- THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING DURING NIGHTTIME CONSTRUCTION.
- ARTIFICIAL AREA LIGHTING SHALL CONSIST OF VEHICLE OR POLE MOUNTED FLOODLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL ONLY BE ALLOWED IN ADDITION TO THE AREA LIGHTING.
- 3. ARTIFICIAL AREA LIGHTING SHALL NOT RESTRICT OR VISUALLY INTERFERE WITH AIR TRAFFIC.
- PLACEMENT & AIMING OF ARTIFICIAL LIGHTING SHALL BE APPROVED BY THE AIRPORT PRIOR TO START OF OPERATIONS.

18. PROTECTION OF AREAS & SURFACES

- I. MINIMUM DISTANCES SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS AND THE CENTERLINE OF ALL ACTIVE TAXIWAYS, TAXILANES AND CENTERLINE OF ACTIVE RUNWAYS. IT IS INTENDED TO PLAN, CONDUCT, AND COMPLETE THE WORK IN THESE CRITICAL TRAFFIC AREAS IN SUCH A MANNER THAT THE LENGTH AND AMOUNT OF INTERRUPTION TO AIRCRAFT TRAFFIC AT THE AIRPORT IS MINIMIZED. THE MINIMUM DISTANCES TO BE MAINTAINED SHALL BE AS SPECIFIED IN THE CONSTRUCTION ACTIVITY AND SAFETY PLAN.
- ALL WORK REQUIRED INSIDE OF ANY RUNWAY SAFETY AREA WILL
 REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL
 COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE
 REQUESTED CLOSURE TIME.
- 3. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 85.5' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- 4. THE CONTRACTOR SHALL COMPLY WITH FEDERAL AVIATION REGULATIONS PART 107 (AIRPORT SECURITY), FEDERAL AIR REGULATION PART139 (AIRPORT CERTIFICATION), AND WITH ALL RULES AND REGULATIONS OF THE AIRPORT, INCLUDING, BUT NOT LIMITED TO, CONTROL AND ACCESS TO THE AIRFIELD BY CONTRACTOR'S, EMPLOYEES AND AGENTS, IN THE EVENT THE OWNER IS ASSESSED A FINE BY THE FEDERAL AVIATION ADMINISTRATION FOR BREACH OF SECURITY RESULTING FROM ACTIONS OF CONTRACTOR'S EMPLOYEES AND AGENTS, THE CONTRACTOR SHALL FULLY REIMBURSE THE AUTHORITY FOR THE AMOUNT OF SUCH FINE IN THE FORM OF ADDITIONAL RENTS.

19. OTHER LIMITATIONS ON CONSTRUCTION

- . IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES. PERSONNEL AND EQUIPMENT.
- 2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED. THE AIRPORT SHALL HAVE THE OPTION FOR THE USE OF THE BITUMINOUS MILLINGS GENERATED FROM THE WORK COMPLETED ON THIS PROJECT. OWNERSHIP OF THE MILLINGS WILL BE DECIDED BEFORE MILLING OPERATIONS BEGIN AND LOCATION FOR ON-SITE STOCKPILING IS DETERMINED IF AIRPORT BETAINS OWNERSHIP.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE SPECIFICATIONS.
- THE CONTRACTOR SHALL MITIGATE DUST PRODUCED DURING CONSTRUCTION OPERATIONS BY WETTING ALL HIGHLY TRAFFICKED AREAS AS NECESSARY. AT NO POINT SHALL DUST IMPACT AIR OPERATIONS OR COMPROMISE SAFETY.

(NOTES CONTINUE ON SHEET GC002)



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REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

Γ							
	MARK	DATE	DESCRIPTION				
	FED. PROJ. NO. 3-17-SBGP-TBD						
	IL. PR	OJ. NO: I	DNV-5169				

FED. PROJ. NO. 3-1.	FED. PROJ. NO. 3-17-5BGP-1BD			
IL. PROJ. NO: DNV-5169				
CMT PROJECT NO:	190042-02-30			
CAD DWG FILE:	19004202-PH3-GC001.DWG			
DESIGNED BY:	HCH			
DRAWN BY:	DPA			
OUEOKED BY:	MID			

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APPROVED BY: EMH

CONSTRUCTION ACTIVITY NOTES & DETAILS 1

GC001

внеет 5

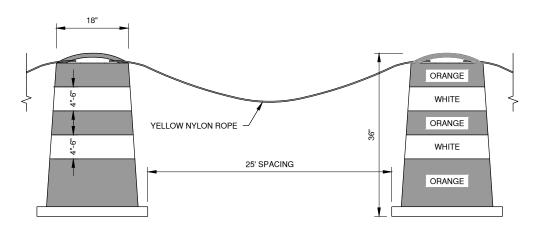
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20. AIRPORT SECURITY REQUIEMENTS

(NOTES CONTINUED FROM SHEET GC001)

- MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
- A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR. THE SUPERINTENDENT AND FOREMAN SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES.
- THE CONTRACTOR SHALL PROVIDE A SIGN AT ALL ACCESS GATES STATING " AUTHORIZED PERSONNEL ONLY." ALL COSTS RELATING TO CONTRACTOR'S ACCESS, GATE GUARDS, FLAGMEN, AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- THE AIRPORT WILL BE HOSTING EVENTS THROUGHOUT THE YEAR AND RESERVES THE RIGHT TO SUSPEND CONSTRUCTION ACTIVITY. REFER TO THE CONSTRUCTION ACTIVITY PLANS FOR NOTES REGARDING THIS PLANNED SUSPENSION.





DRUM BARRICADE NOTES

- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- 2. PLACE AND MAINTAIN UNTIL REMOVAL ACCORDING TO CSPP.
- 3. BARRELS SHALL BE WEIGHTED SUFFICIENTLY TO PREVENT DISPLACEMENT.

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MARK DATE DESCRIPTION

IL. PROJ. NO: DNV-5169 CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-GC001.DWG

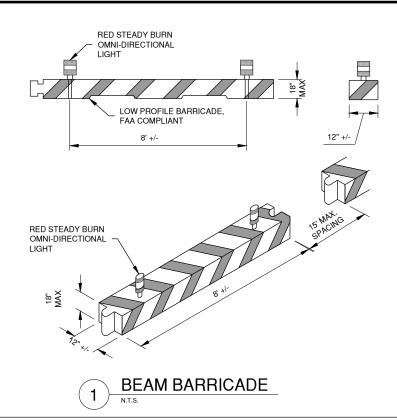
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SHEET 6

CONSTRUCTION **ACTIVITY NOTES &** DETAILS 2

> GC002 63

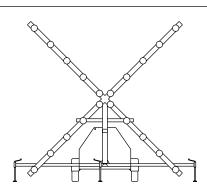


6. FINAL BEAM BARRICADE SPACING

WILL BE BASED ON THE DISCRETION

BEAM BARRICADE NOTES

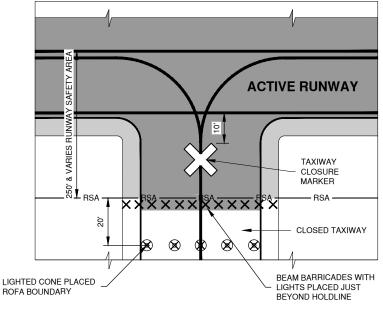
- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
- BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE
- PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE **FNGINFFR**
- BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



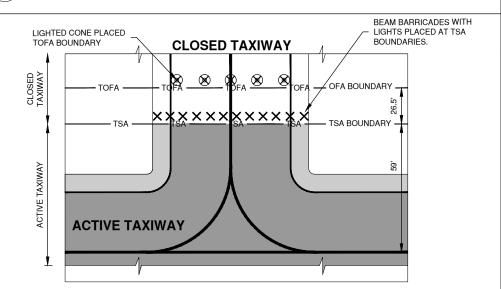
LIGHTED RUNWAY CLOSURE MARKER 2

LIGHTED RUNWAY CLOSURE MARKER NOTES

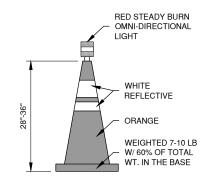
- 1. TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS FOR CLOSURE.
- THE CONTRACTOR SHALL PROVIDE TWO LIGHTED CLOSURE MARKERS (1 PAIR) AND MAINTAIN THEM (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS ASSOCIATED WITH PROVIDING AND MAINTAINING THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE **PROJECT**



CLOSED TAXIWAY / ACTIVE RUNWAY BARRICADES



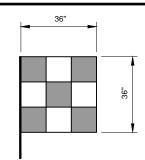
CLOSED TAXIWAY / ACTIVE TAXIWAY BARRICADES





LIGHTED CONE NOTES

- CONE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST. IF USING SAND BAGS, THEY MUST BE LESS THAN 3" HIGH AND ORANGE IN COLOR. IF THE CONTRACTOR IS UNABLE TO ADEQUATELY SECURE THE CONES TO PREVENT OVERTURNING, THEY SHALL BE REPLACED WITH BEAM BARRICADES.
- CONES SHALL BE NEW TO MAXIMIZE THE VISIBILITY OF THE ORANGE AND WHITE COLORS
 - 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE
 - BARRICADES AND CONES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).
 - 5. CONES SHALL BE PLACED WITH MAXIMUM 15' SPACING.

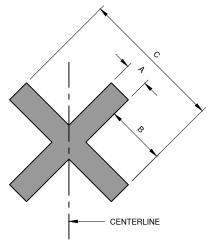




EQUIPMENT & VEHICLE SIGNAL FLAG 6

EQUIPMENT FLAG NOTES

- 1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG OR LIGHTED BEACON.
- WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS
- CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



<u> </u>			
SYMBOL TYPE DIMENSION	Α	В	С
CLOSED TAXIWAY	5'-0"	12'-6"	30'-0"
CLOSED RUNWAY	10'-0"	25'-0"	60'-0"

NON-LIGHTED CLOSED MARKER

NOTES

- 1. CLOSURE MARKERS SHALL BE SOLID YELLOW.
- 2. NON-LIGHTED CLOSURE MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA
- 3. THE CONTRACTOR SHALL PROVIDE TWO (1 PAIR) NON-LIGHTED RUNWAY CLOSURE MARKERS TO BE PLACED ON EACH END OF THE TURF RUNWAY
- 4. MARKERS SHALL BE PLACED ON THE TURF RUNWAY ON BOTH ENDS.
- MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT
- 7. REFER TO THE CONSTRUCTION ACTIVITY PLAN (CAP) FOR QUANTITIES OF

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REALIGN TAXIWAY A, PHASE 3

OWNER



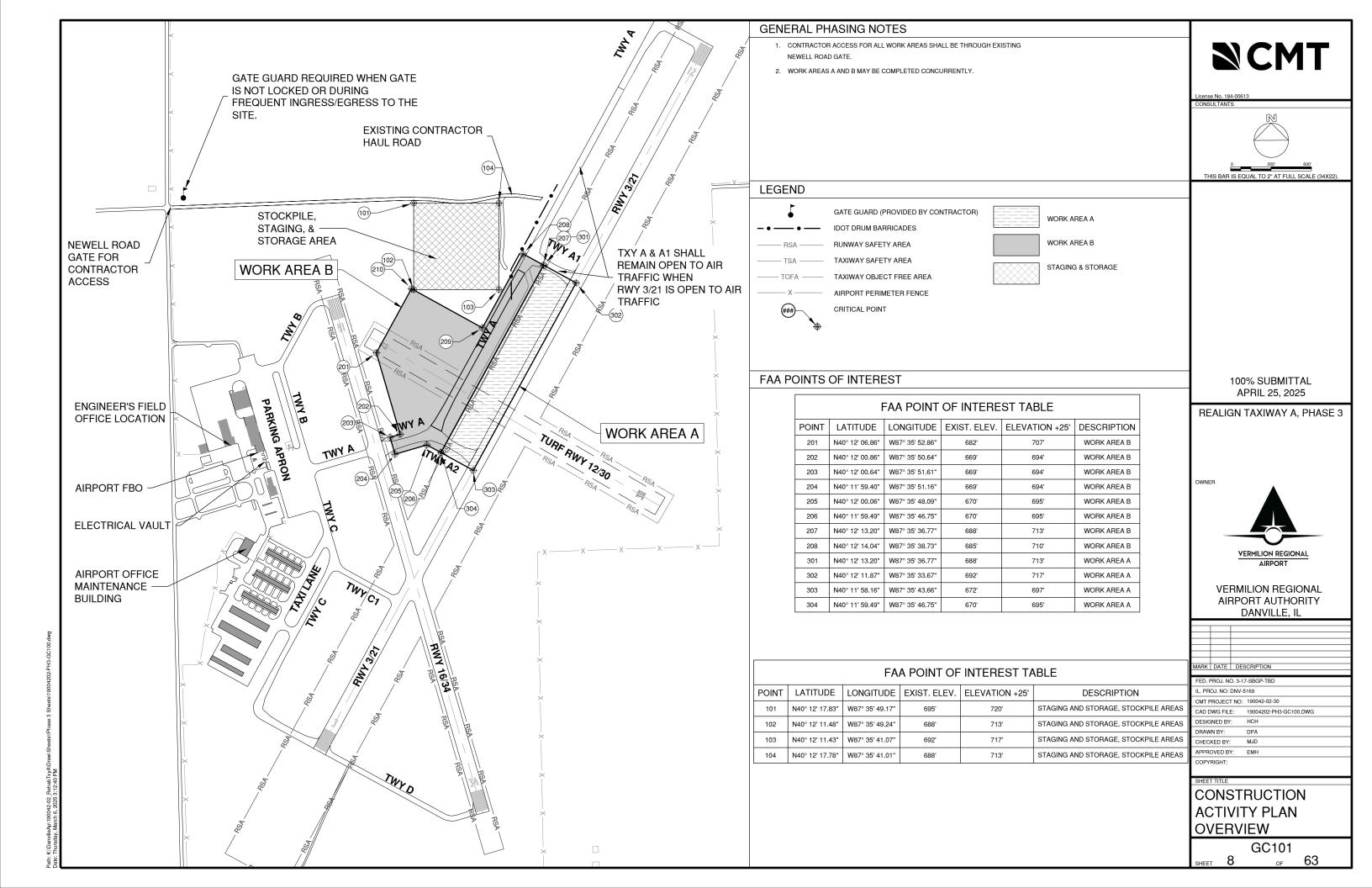
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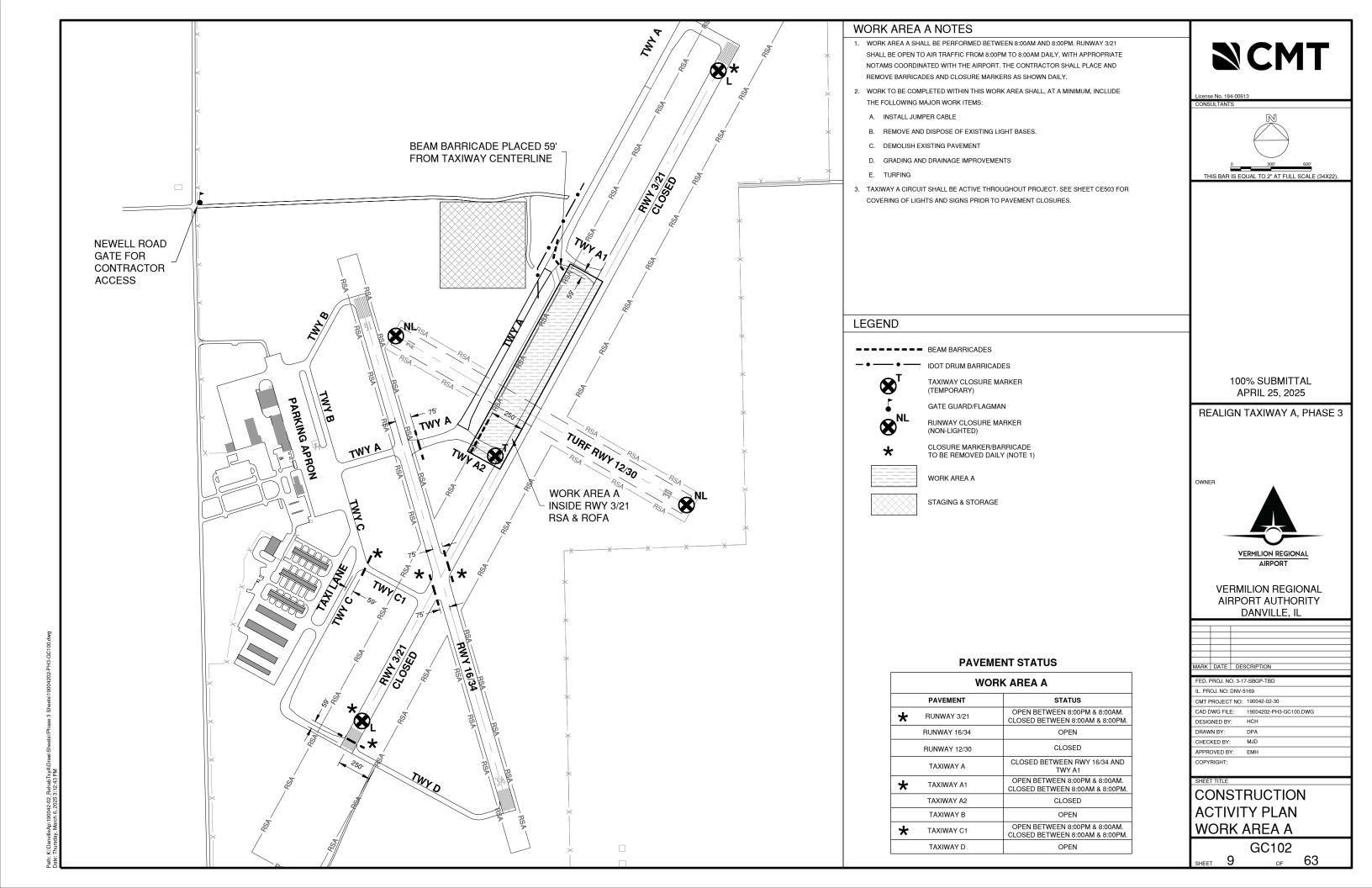
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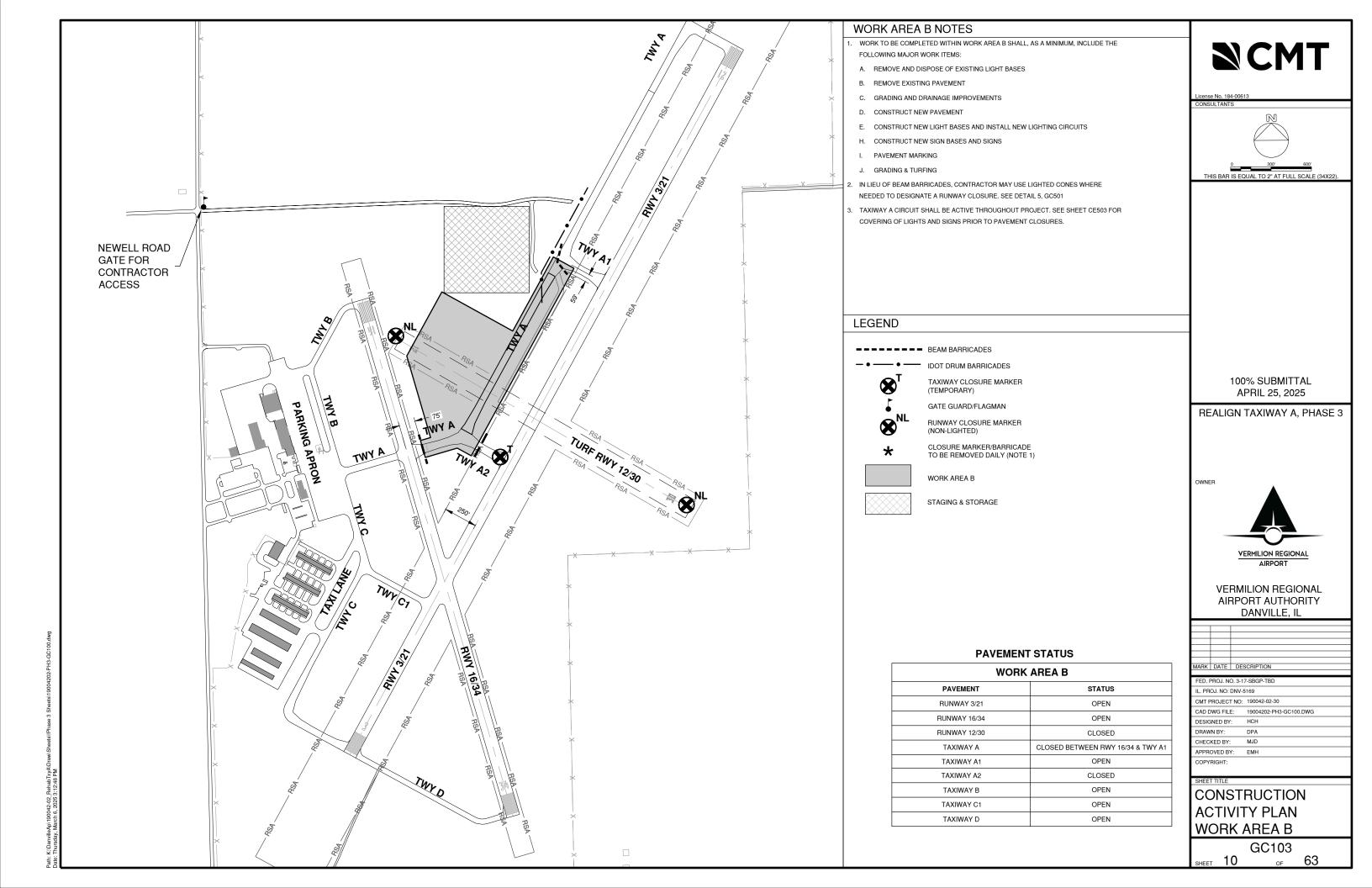
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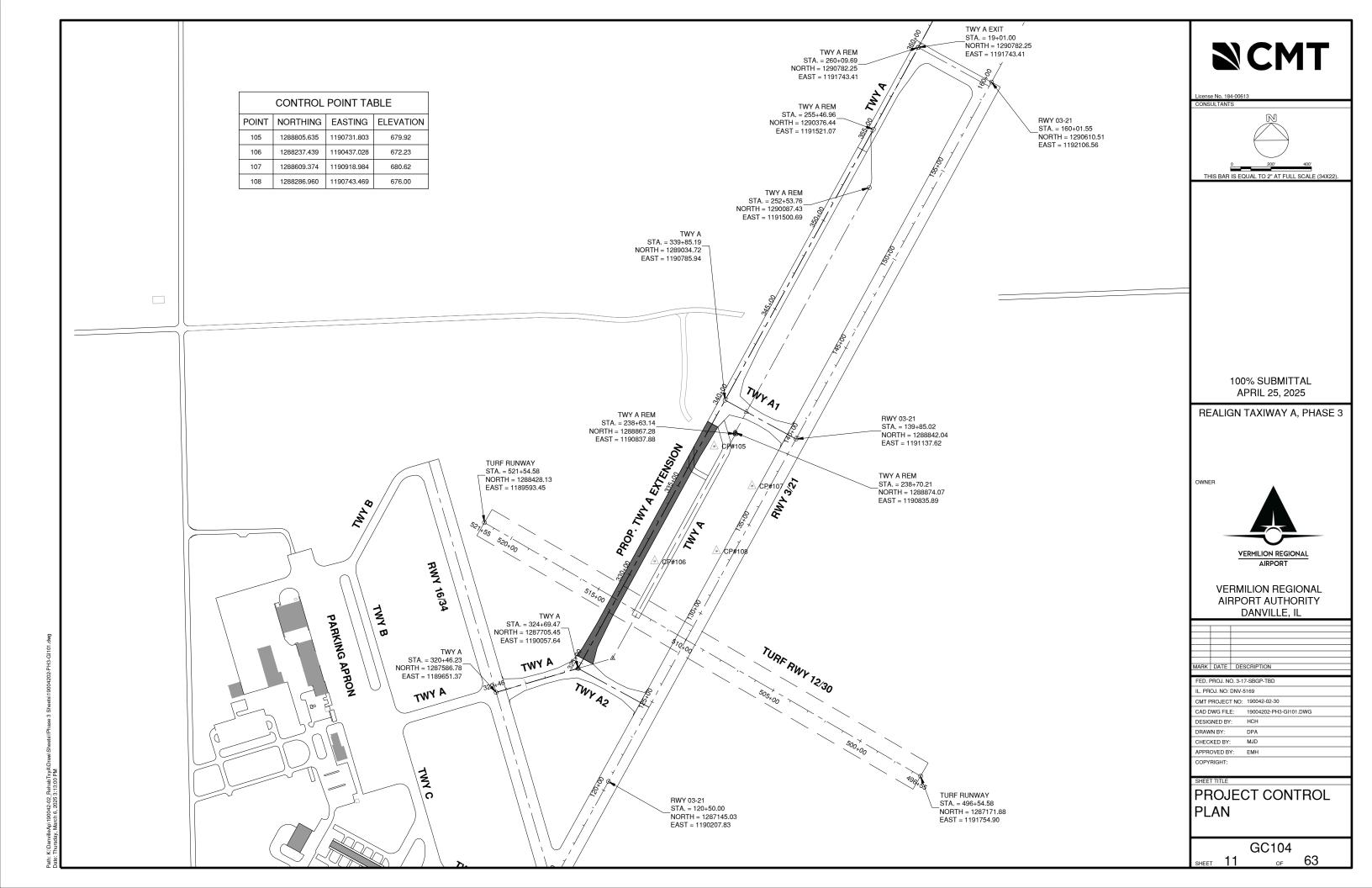
CONSTRUCTION **ACTIVITY NOTES & DETAILS 3**

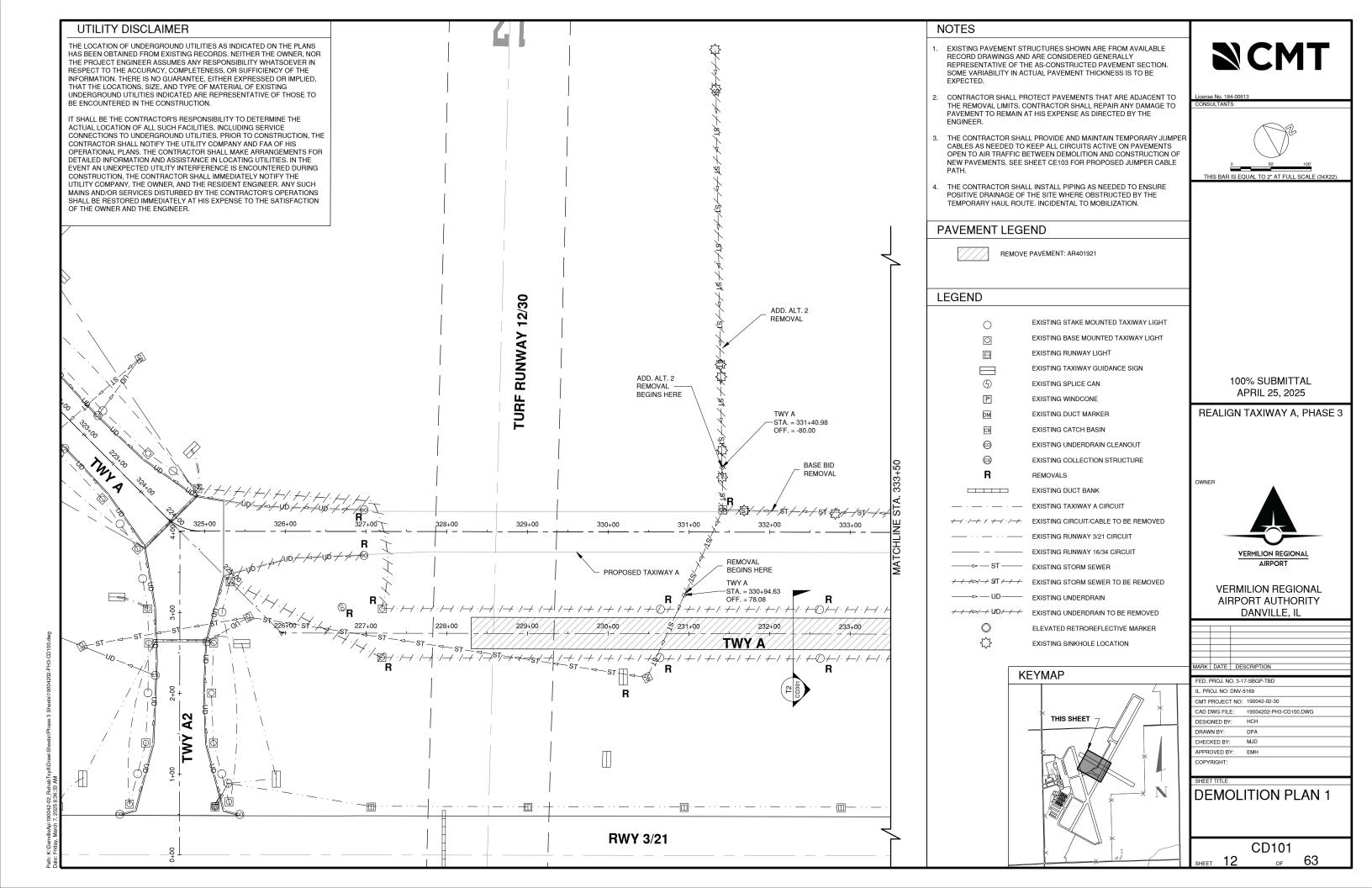
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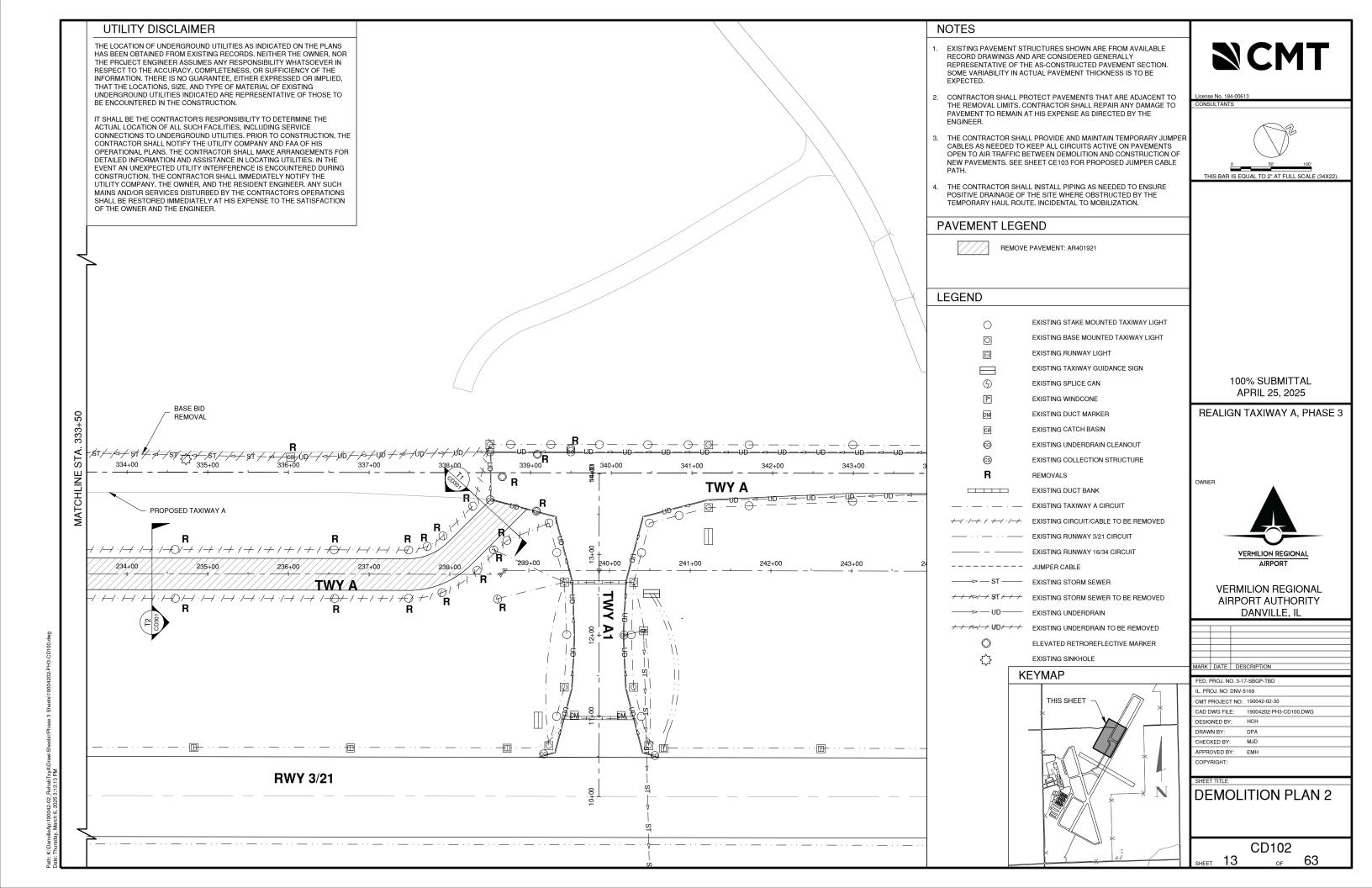


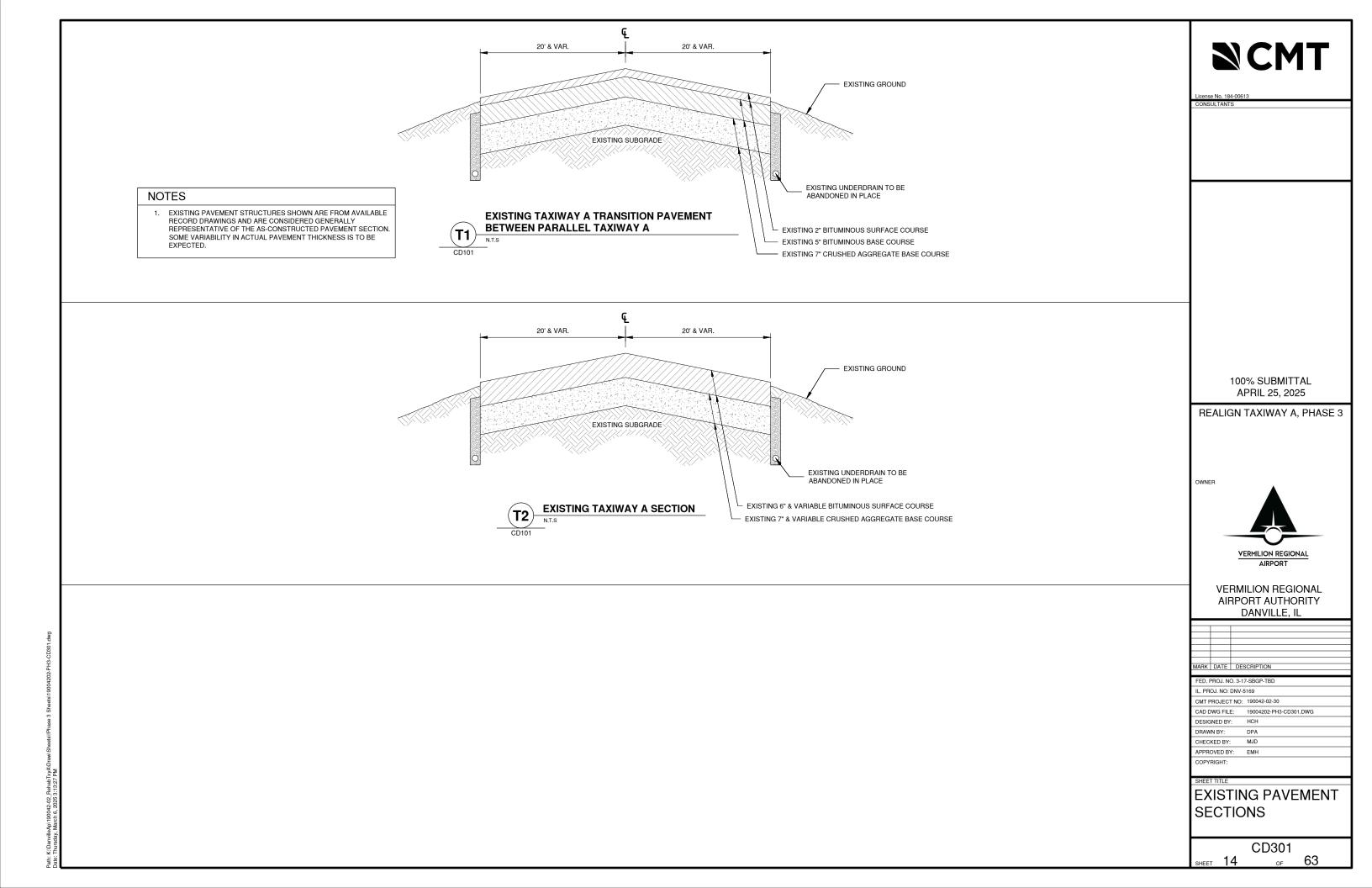


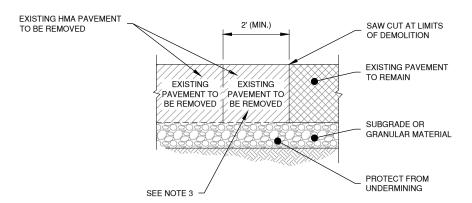




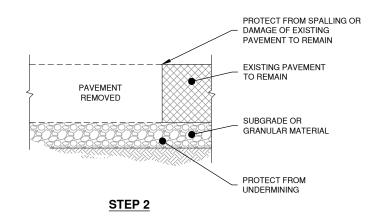








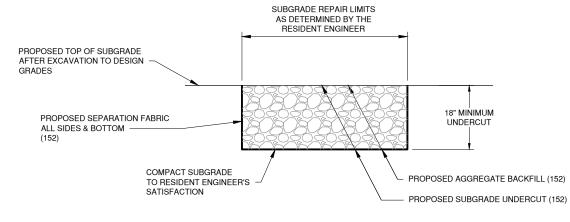
STEP 1





SAW CUT AND OTHER DEMOLITION NOTES

- 1. PROTECT PAVEMENT TO REMAIN IN PLACE FROM UNDERMINING.
- 2. SAW CUTS SHALL BE FULL DEPTH ON PAVEMENT PRIOR TO REMOVAL OF
- 3. DO NOT REMOVE 2' OF HMA PAVEMENT AGAINST EXISTING HMA PAVEMENT TO REMAIN UNTIL AREA IS READY TO RECEIVE MODIFIED SUBGRADE OR AGGREGATE BASE COURSE. WORK ON EXPOSED SUBGRADE PRIOR TO RECEIVING MODIFIED SUBGRADE OR AGGREGATE BASE COURSE SHALL BE COMPLETED WITHIN 48 HOURS AFTER THE REMOVAL OF HMA PAVEMENT



PROPOSED SUBGRADE UNDERCUT & AGGREGATE BACKFILL

NCMT

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REALIGN TAXIWAY A, PHASE 3



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL. PROJ. NO: DNV-5169 CMT PROJECT NO: 190042-02-30

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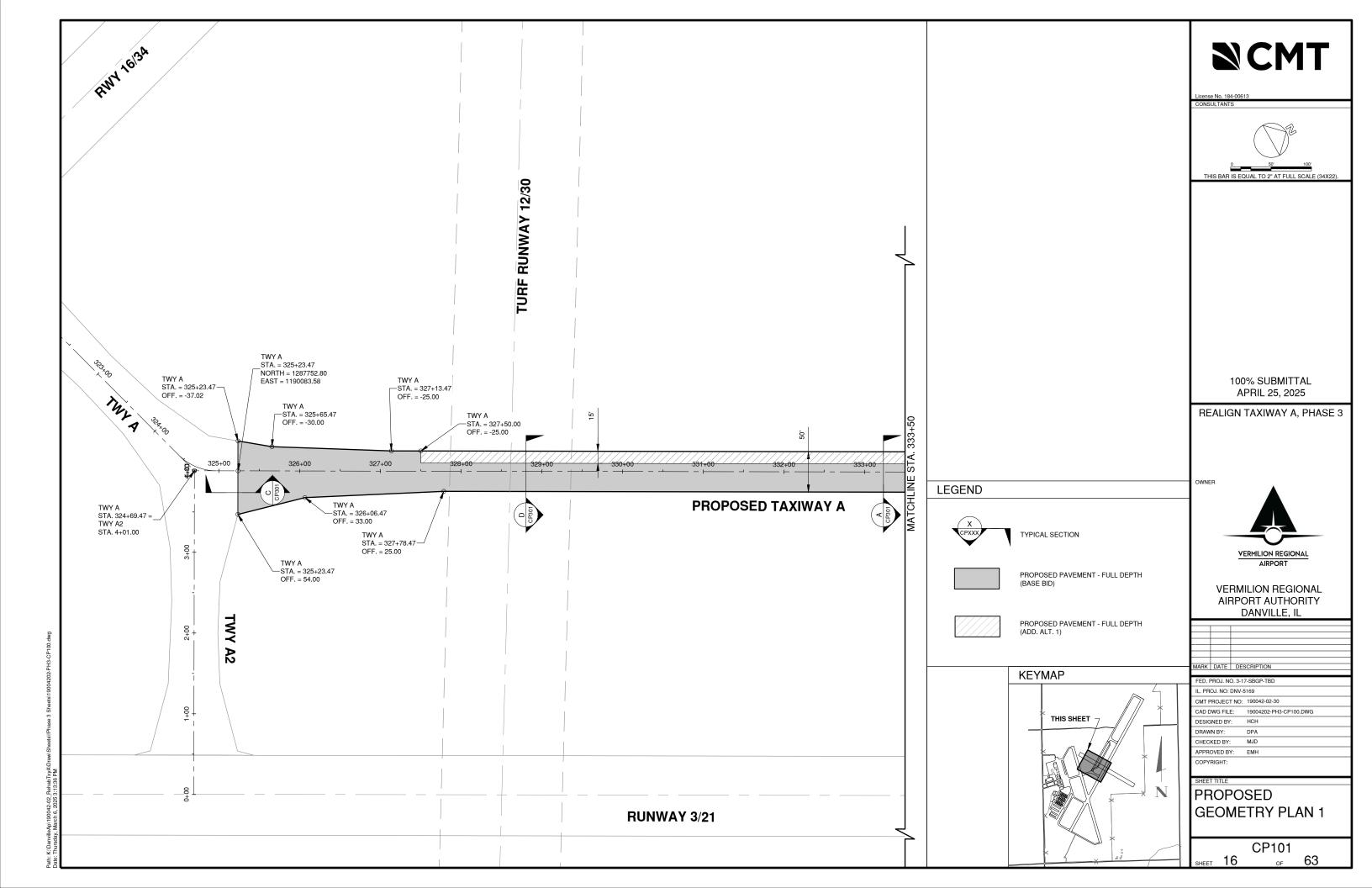
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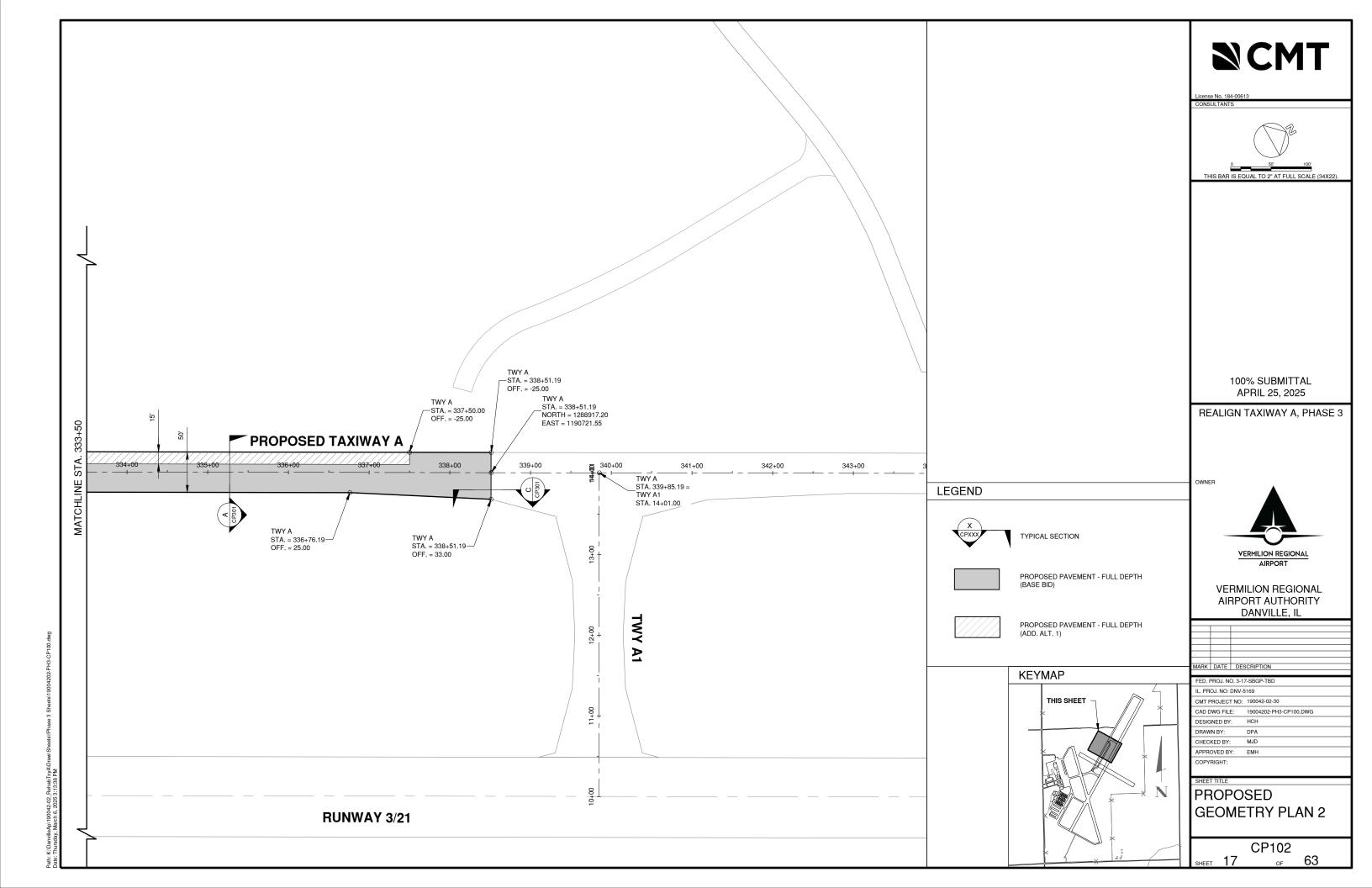
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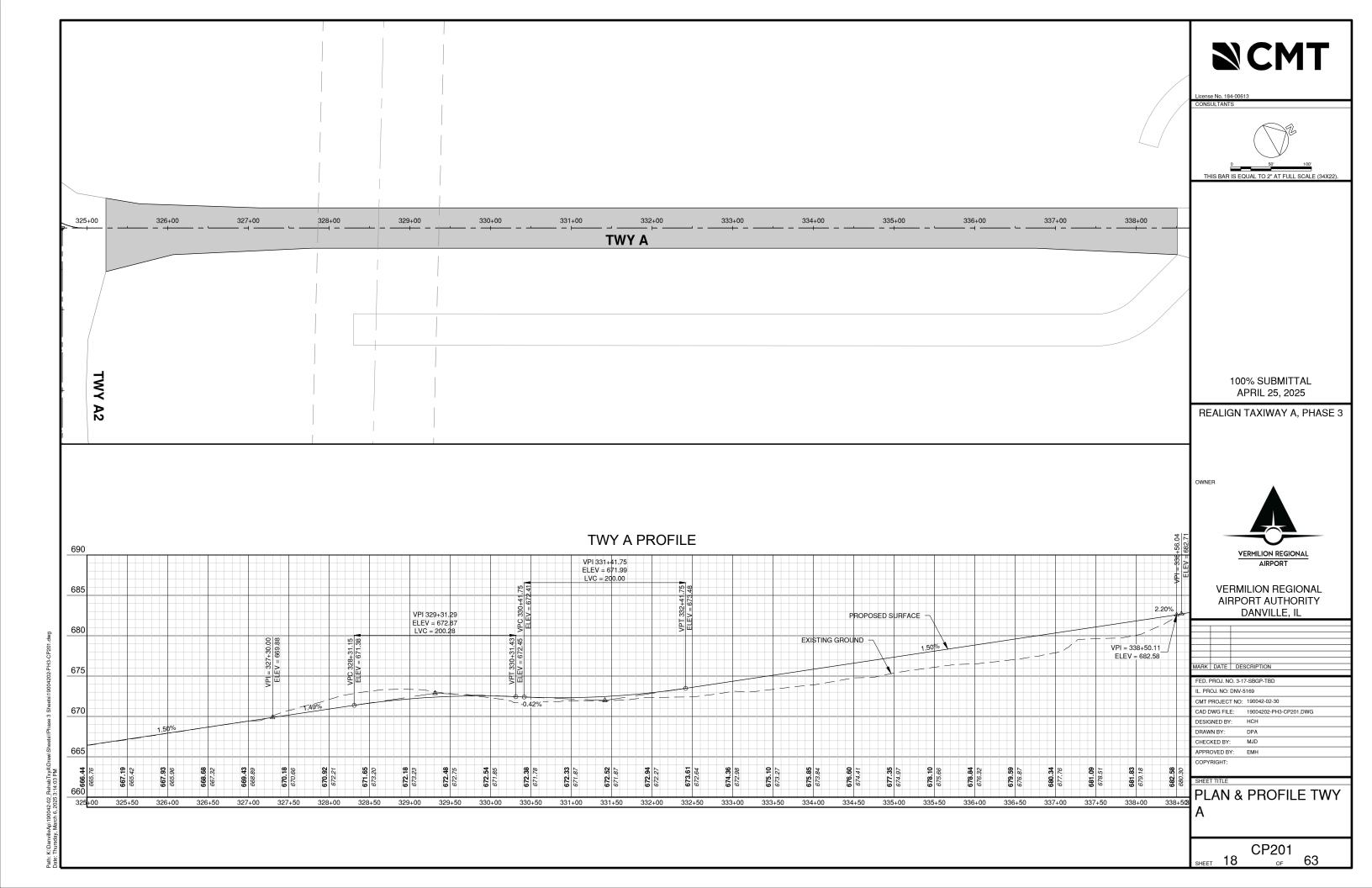
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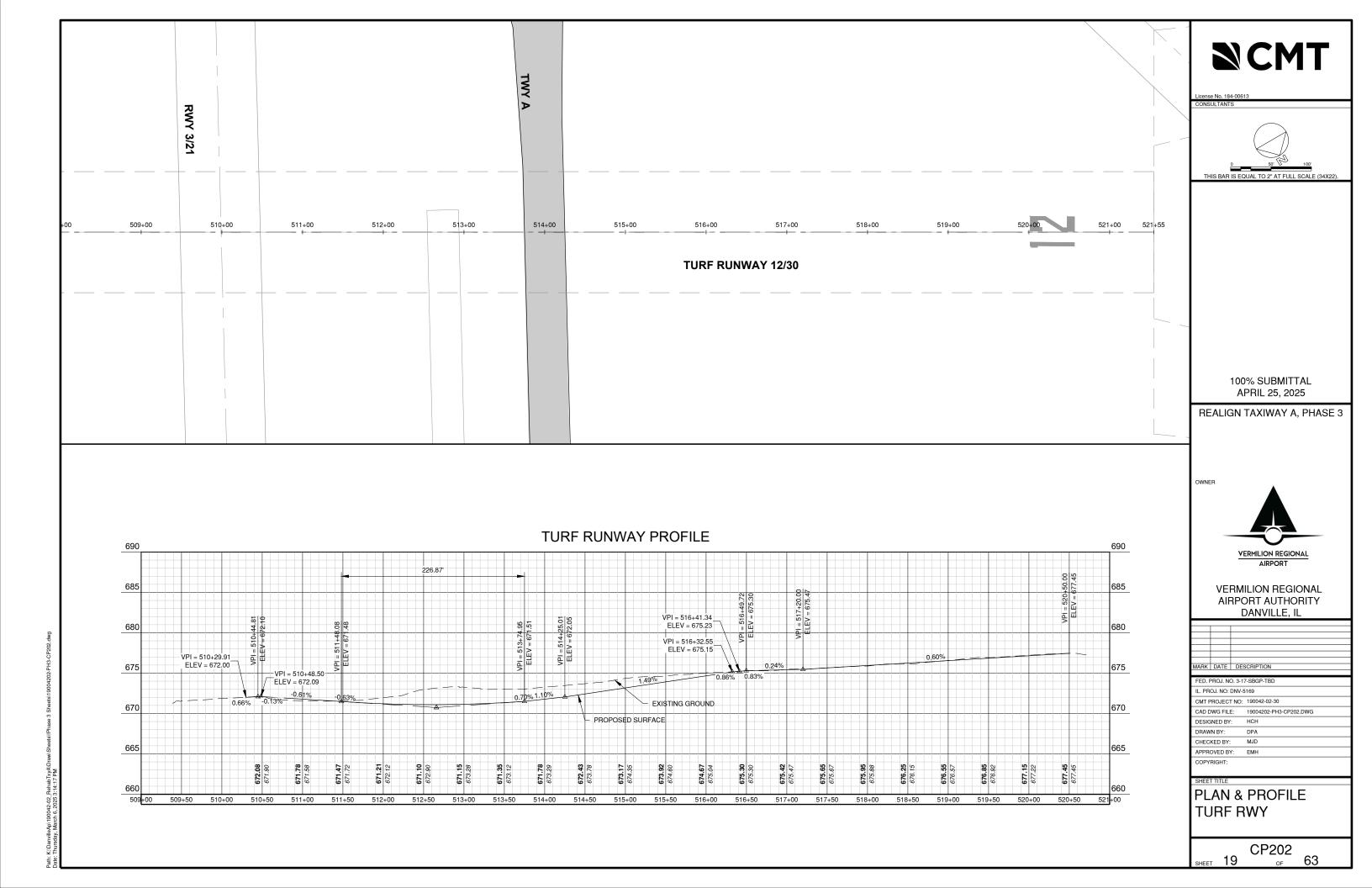
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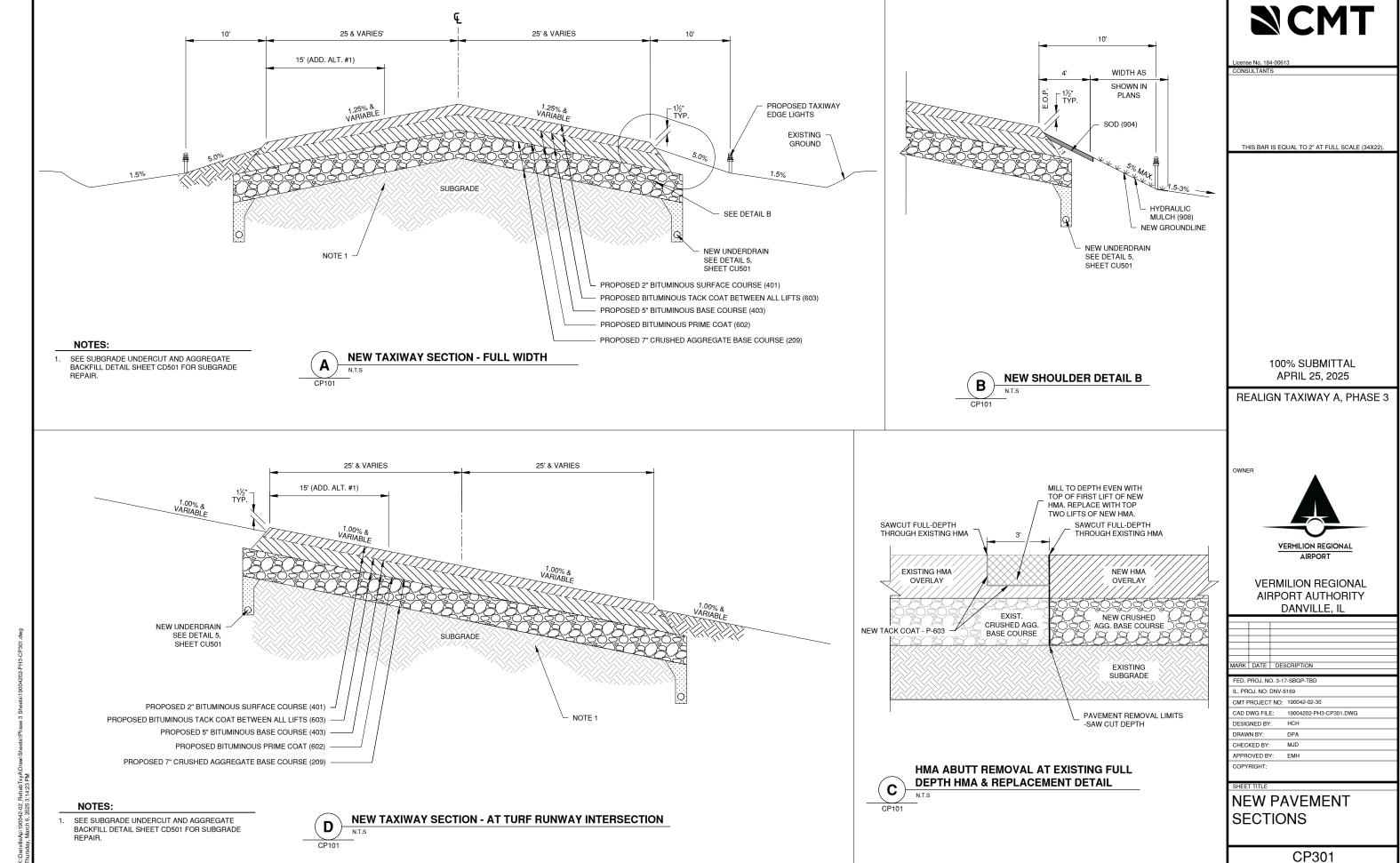
SHEET 15





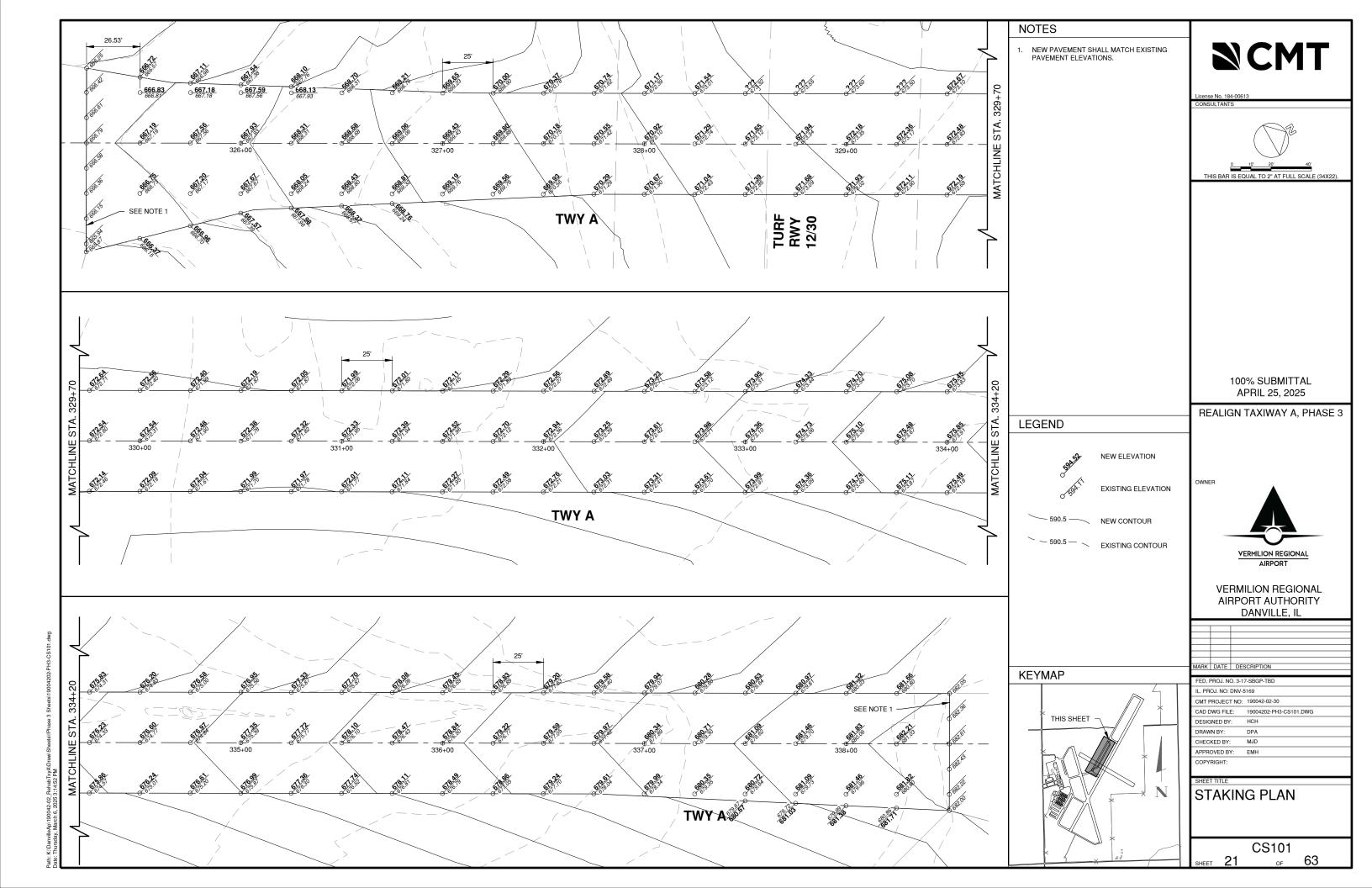


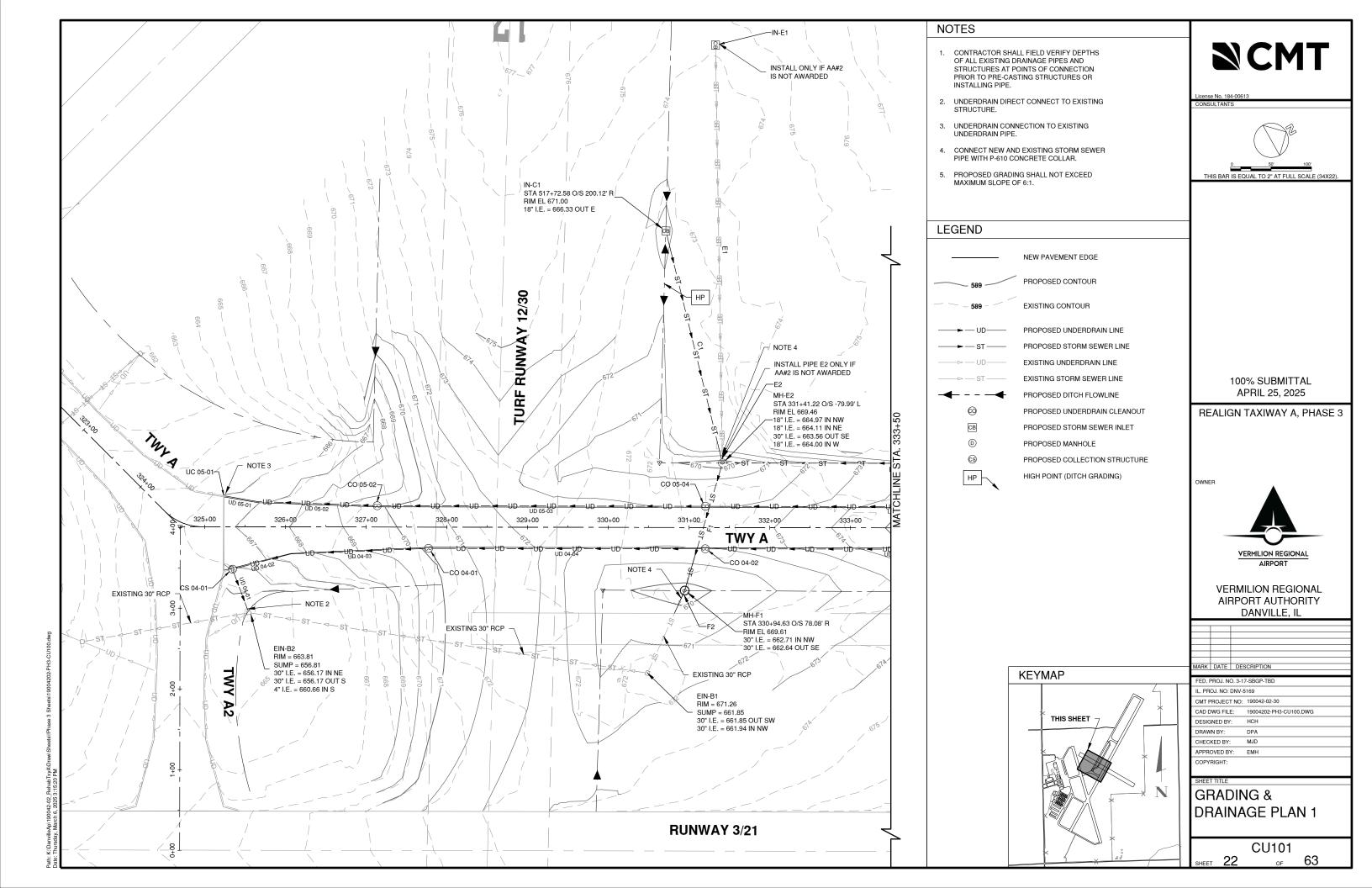


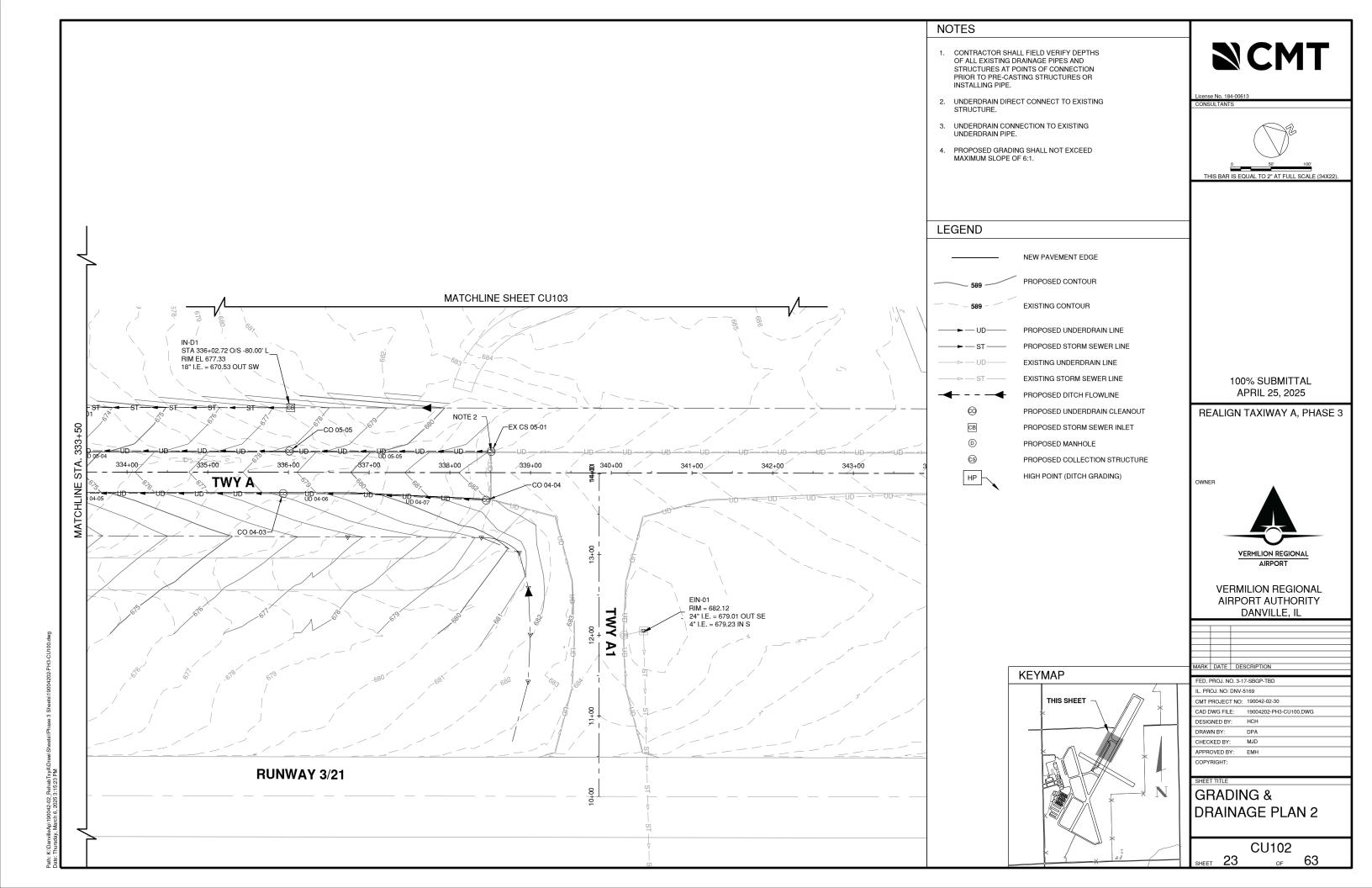


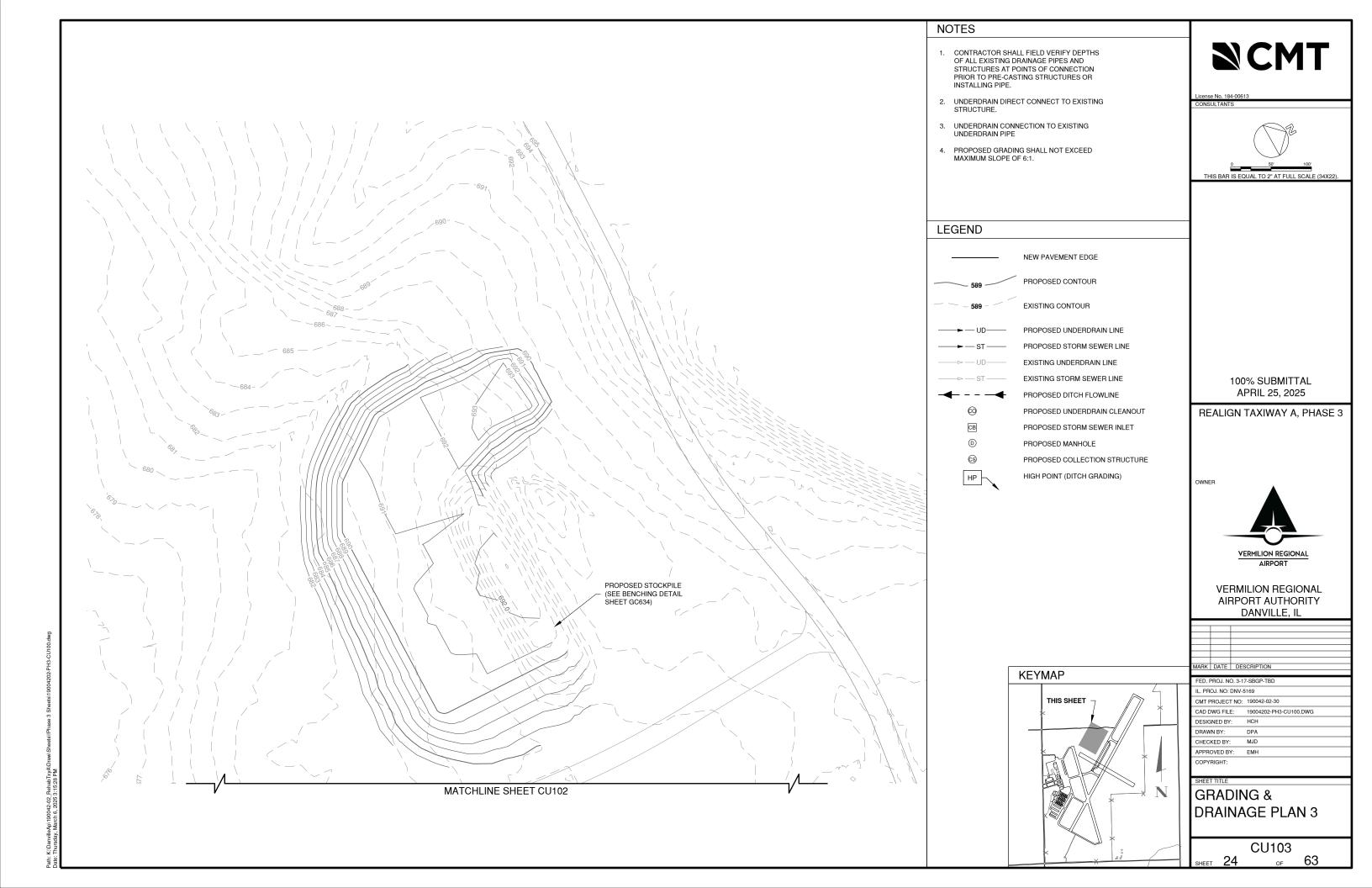
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SHEET 20









PROPOSED NORTHWEST UNDERDRAIN PROFILE 685 DIRECT CONNECT TO EXISTING COLLECTION STRUCTURE PROPOSED SURFACE 680 680 4" UD @ 1.50% - EXISTING GROUND 675 CONNECT TO EXISTING UNDERDRAIN EX CS 05-01 675 INSIDE COLLECTION STRUCTURE 4" UD @ 1.25% CO 05-05-670 670 4" UD @ 0.80% 665 665 - 4" UD @ 1.41% CO 05-04-CO 05-02

NORTHWEST UNDERDRAIN STRUCTURE TABLE					
STRUCTURE NAME AND TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL			
CO 05-02 TYPE 3	RIM = 669.67 SUMP = 665.33 UD 05-03 INV IN = 665.35 UD 05-02 INV OUT = 665.35	TWY A STA 327+13.49 OFFSET -26.32 L			
CO 05-04 TYPE 3	RIM = 671.84 SUMP = 667.12 UD 05-04 INV IN = 668.60 UD 05-03 INV OUT = 668.60	TWY A STA 331+19.99 OFFSET -26.32 L			
CO 05-05 TYPE 3	RIM = 678.31 SUMP = 674.58 UD 05-05 INV IN = 674.61 UD 05-04 INV OUT = 674.61	TWY A STA 336+01.19 OFFSET -26.32 L			
EX CS 05-01 DIRECT CONNECT TO EXIST. COLLECTION STRUCTURE	RIM = 681.86 SUMP = 678.36 UD 05-05 INV OUT = 678.36	TWY A STA 338+51.19 OFFSET -26.32 L			
UC 05-01 UNDERDRAIN CONNECTION	RIM = N/A SUMP = N/A UD 05-01 INV IN = 662.67	TWY A STA 325+23.69 OFFSET -38.32 L			

NORTHWEST UNDERDRAIN PIPE TABLE							
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
UD 05-01		UC 05-01	663.25	662.67	40	1.41%	PERFORATED WITH SOCK - 4"
UD 05-02	CO 05-02		665.35	663.25	147	1.41%	PERFORATED WITH SOCK - 4"
UD 05-03	CO 05-04	CO 05-02	668.60	665.35	402	0.80%	PERFORATED WITH SOCK - 4"
UD 05-04	CO 05-05	CO 05-04	674.61	668.60	477	1.25%	PERFORATED WITH SOCK - 4"
UD 05-05	EX CS 05-01	CO 05-05	678.36	674.61	246	1.50%	PERFORATED WITH SOCK - 4"

10+00 10+50 11+00 11+50 12+00 12+50 13+00 13+50 13+00 13+50 14+00 14+50 15+00 15+00 15+50 16+00 16+50 17+00 17+50 18+00 18+50 19+00 19+50 20+00 20+50 21+00 21+50 22+00 22+50 23+00 23+50 24+00

SOUTHEAST UNDERDRAIN PIPE TABLE							
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
UD 04-01	CS 04-01	DC-04-01	662.36	661.25	53	2.01%	NON-PERFORATED - 4"
UD 04-02		CS 04-01	663.60	662.55	73	1.40%	PERFORATED WITH SOCK - 4"
UD 04-03	CO 04-01		665.98	663.60	169	1.40%	PERFORATED WITH SOCK - 4"
UD 04-04	CO 04-02	CO 04-01	668.43	665.98	339	0.72%	PERFORATED WITH SOCK - 4"
UD 04-05	CO 04-03	CO 04-02	675.02	668.43	470	1.39%	PERFORATED WITH SOCK - 4"
UD 04-06		CO 04-03	676.23	675.02	81	1.46%	PERFORATED WITH SOCK - 4"
UD 04-07	CO 04-04		678.49	676.23	167	1.33%	PERFORATED WITH SOCK - 4"

SOUTHEAST UNDERDRAIN STRUCTURE TABLE					
STRUCTURE NAME AND TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL			
CO 04-01 TYPE 3	RIM = 670.28 SUMP = 665.48 UD 04-04 INV IN = 665.98 UD 04-03 INV OUT = 665.98	TWY A STA 327+77.17 OFFSET 26.36 R			
CO 04-02 TYPE 3	RIM = 671.89 SUMP = 667.72 UD 04-05 INV IN = 668.43 UD 04-04 INV OUT = 668.43	TWY A STA 331+20.00 OFFSET 26.35 R			
CO 04-03 TYPE 3	RIM = 678.20 SUMP = 674.43 UD 04-06 INV IN = 675.02 UD 04-05 INV OUT = 675.02	TWY A STA 335+93.50 OFFSET 26.32 R			
CO 04-04 TYPE 1	RIM = 681.74 SUMP = 678.49 UD 04-07 INV OUT = 678.49	TWY A STA 338+45.13 OFFSET 34.05 R			
CS 04-01 COLLECTION STRUCTURE	RIM = 665.86 SUMP = 662.54 UD 04-02 INV IN = 662.55 UD 04-01 INV OUT = 662.36	TWY A STA 325+34.36 OFFSET 52.66 R			
DC-04-01 UNDERDRAIN DIRECT CONNECT	RIM = N/A SUMP = N/A UD 04-01 INV IN = 661.25	TWY A STA 325+53.81 OFFSET 104.17 R			

			PROPO	SED SOUTI	HEAST UND	ERDRAIN	PROFILE	Ξ							
80														04-04	680
75		EXISTING GROUND						PROPOSED SURFA	ACE		-4" UD @	1.46%	4" UD @ 1.33 /e		675
70							4" UD @ 1.	.39%		Ž	-CO 04-03				670
65	4" UD @	0 1.40%	4" UD @ 0.72%		Z _{CO 04-02}										665
60	4" UD @ 1.40%	400 04-01													660
555	DIRECT CONNECTION TO EXISTING INLET														655
9+50 10+00 10+5	50 11+00 11+50 12+00	12+50 13+00 13+50 14	+00 14+50 15+00	15+50 16+00	16+50 17+00	17+50 18+00	18+50 19	9+00 19+50	20+00 20-	-50 21+00	21+50	22+00 22	+50 23+00	23+50	24-00

SCMT

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

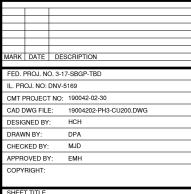
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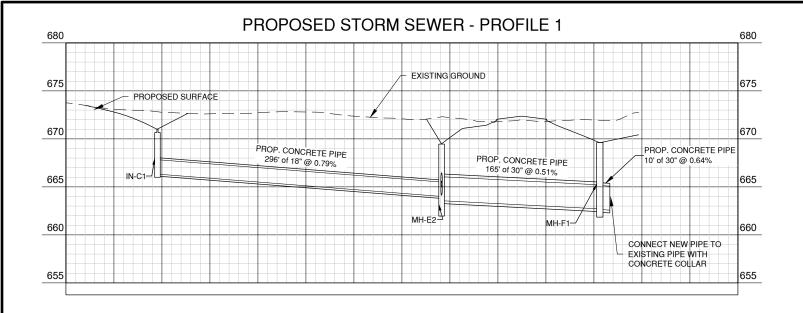
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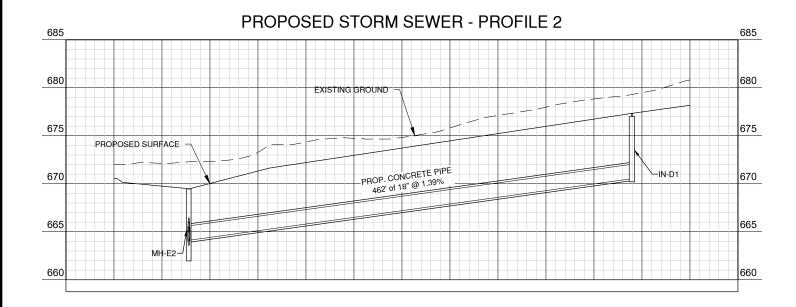


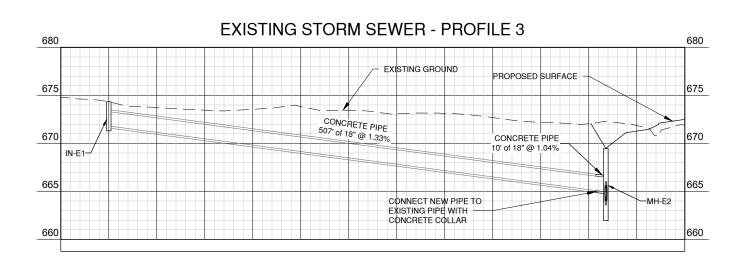
UNDER DRAIN PROFILE & SCHEDULE

CU201 SHEET 25 OF 63

nt: K:⊔DanvilleAp\190042-02_Fenab 1xyAlDraw\Sheets\!Phase 3 Sheets\19004202-FH3-CU200.dwg te: Thursday, March 6, 2025 3:16:04 PM 660 UC 05-01







STRUCTURE TABLE STORM SEWER STRUCTURE NAME AND TYPE STRUCTURE DETAILS RIM = N/ASUMP = N/A E1 INV IN = 665.08 E2 INV OUT = 665.08 CONNECTION TWY A STA 331+41.06 CONCRETE COLLAR OFFSET -90.33 L RIM = 671.00 TURF RUNWAY STA 517+72.58 SUMP = 666.33 C1 INV OUT = 666.33 INLET TYPE A OFFSET 200.12 R RIM = 677.33 SUMP = 670.53 TWY A STA 336+02.72 INLET - TYPE A OFFSET -80.00 L D1 INV OUT = 670.53 RIM = 674.34 SUMP = 671.81 E1 INV OUT = 671.81 TWY A STA 331+31.71 OFFSET -597.73 L IN-E1 INLET TYPE A RIM = 669.46 SUMP = 662.46 E2 INV IN = 664.97 D1 INV IN = 664.11 C1 INV IN = 664.00 MH-E2 MANHOLE-5' OFFSET -79.99 L (OPEN LID) 1 INV OUT = 663.56 RIM = 669.61 SUMP = 662.35 TWY A STA 330+94.63 MANHOLE-5' F1 INV IN = 662.71 F2 INV OUT = 662.64 OFFSET 78.08 R

PIPE SCHEDULE STORM SEWER									
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE		
C1	IN-C1	MH-E2	666.33	664.00	296	0.79%	CONCRETE PIPE - 18"		
D1	IN-D1	MH-E2	670.53	664.11	462	1.39%	CONCRETE PIPE - 18"		
E1	IN-E1	CONNECTION	671.81	665.08	507	1.33%	CONCRETE PIPE - 18"		
E2	CONNECTION	MH-E2	665.08	664.97	10	1.04%	CONCRETE PIPE - 18"		
F1	MH-E2	MH-F1	663.56	662.71	165	0.51%	CONCRETE PIPE - 30"		
F2	MH-F1	EXISTING PIPE	662.64	662.57	10	0.64%	CONCRETE PIPE - 30"		



CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP

IL. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-CU200,DWG

DESIGNED BY: HCH

DRAWN BY: DPA
CHECKED BY: MJD
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SHEET **26**

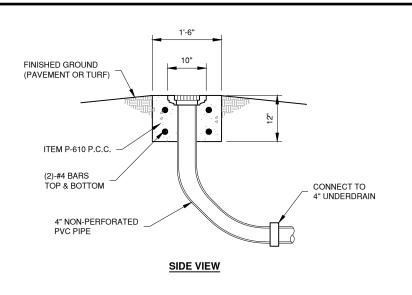
STORM SEWER PROFILE & SCHEDULE

CU202 ₀₅ 63

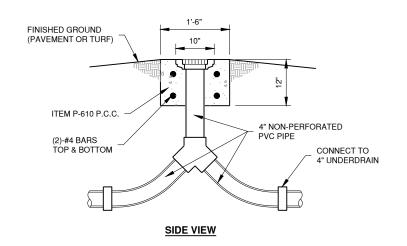
NOTES

IF ADD. ALT. 2 IS NOT AWARDED, PIPE C1 WILL NOT CONNECT TO MH-E2. IF AWARDED, PIPE E2 WILL NOT CONNECT TO MH-E2.

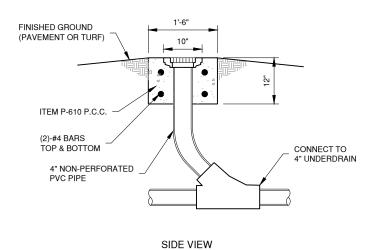
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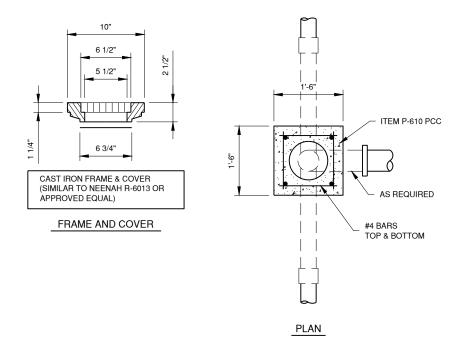
1 UNDERDRAIN CLEAN-OUT - TYPE 1



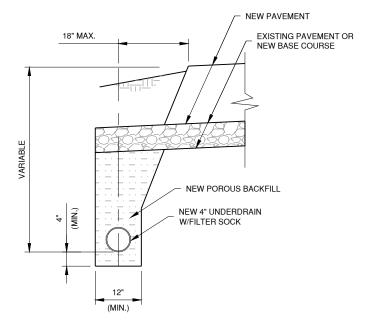
2 UNDERDRAIN CLEAN-OUT - TYPE 2



UNDERDRAIN CLEAN-OUT - TYPE 3



4 UNDERDRAIN CLEAN-OUT



5 UNDERDRAIN ALONG PAVEMENT EDGE

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE.

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CMT PROJECT NO: 190042-02-30

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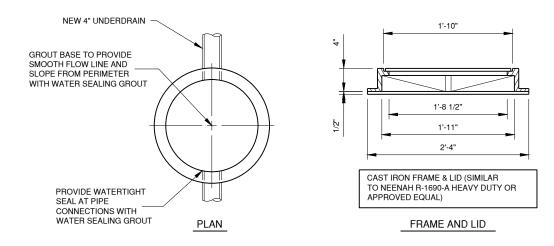
UNDERDRAIN DETAILS 1

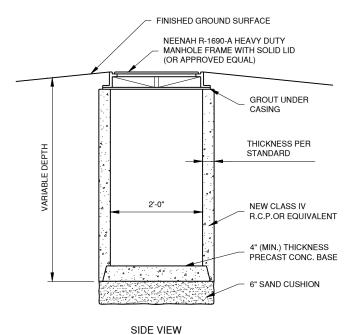
CU501

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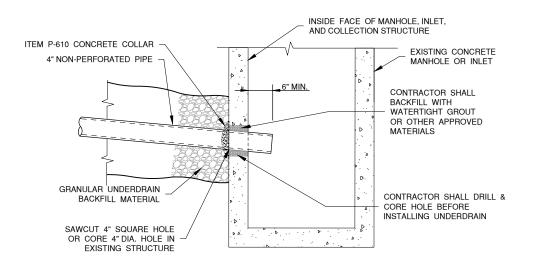
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63

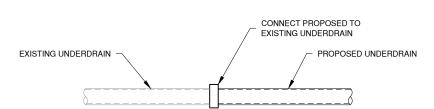




1 UNDERDRAIN COLLECTION STRUCTURE



2 UNDERDRAIN DIRECT CONNECTION TO STRUCTURE



3 UNDERDRAIN CONNECTION TO EXISTING UNDERDRAIN

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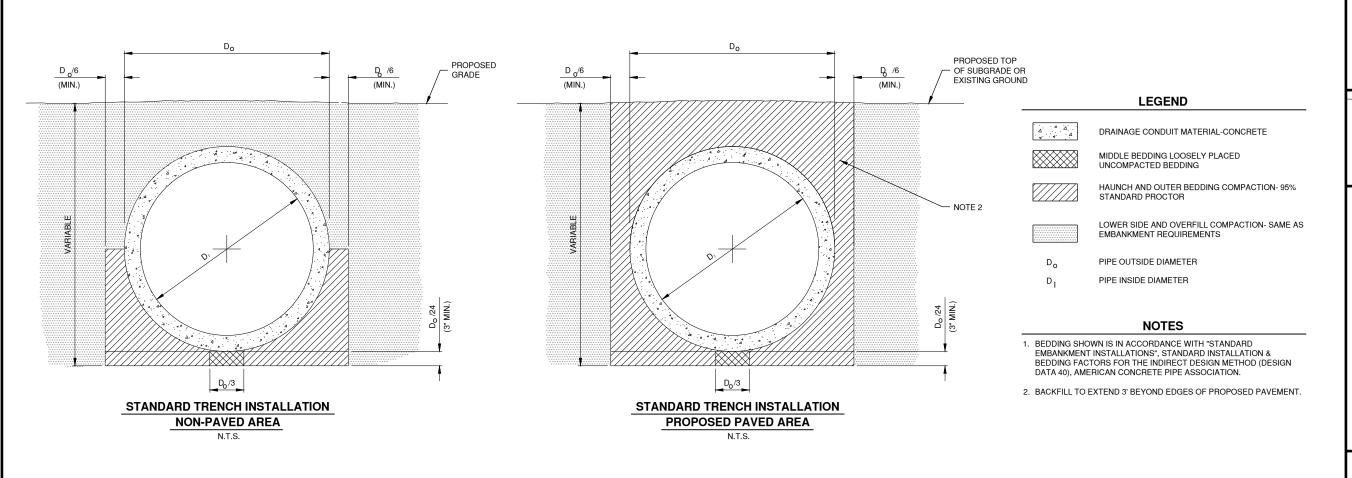
HEET TITLE

SHEET 28

UNDERDRAIN DETAILS 2

CU502

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DRAINAGE TRENCH DETAILS

CU503 SHEET 29 63



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REALIGN TAXIWAY A, PHASE 3

#3 or #4

(#10) (#13)

(#13)

(450)

spacing and this table for bar size



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SHEET 30

DRAINAGE MANHOLE **DETAIL 1**

CU504

FLAT SLAB TOP REINFORCEMENT WWR (each direction) Rebar (each direction except as noted) Location As (min.) Spacing (max.) A_s (min.) Spacing (max.) Bar Size 0.11 sq. in./ft. Mat (233 sq. mm/m) (450) (233 sq. mm/m) ** 0.40 sq. in./ft. See plan view for rebar orientation and

** Only one layer of WWR permitted to avoid congestion.

(150)

(847 sq. mm/m)

WALL REINFORCEMENT

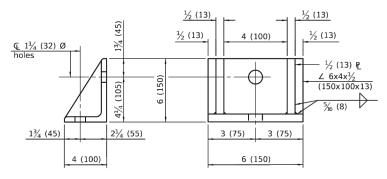
Location	Orientation	WWR or Rebar				
Location	Orientation	A _s (min.)	Spacing (max.)			
Riser	Circumferential	0.15 sq. in./ft. (318 sq. mm/m)	6 (150)			
Riser	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)			
Darrel	Circumferential	0.15 sq. in./ft. (318 sq. mm/m)	6 (150)			
Barrel	Vertical	0.16 sq. in./ft. (339 sq. mm/m)	4 (100)			

BASE SLAB REINFORCEMENT

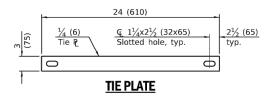
Location	Total Height	WWR or Rebar (each direction)				
Location	Total Height	A _s (min.)	Spacing (max.)			
	≤ 20 ft. (6.10 m)	0.24 sq. in./ft.	10			
Тор	3 20 10. (0.10 111)	(508 sq. mm/m)	(250)			
Mat	> 20 ft. (6.10 m)	0.28 sq. in./ft.	8			
	= 20 lt. (0:10 lll)	(593 sq. mm/m)	(200)			
Bottom	All	0.11 sq. in./ft.	18			
Mat	All	(233 sq. mm/m)	(450)			

6 (150) € 1(25) Ø Threaded rods Inside of with $2\frac{1}{4}x2\frac{1}{4}x\frac{5}{16}$ (55x55x8) manhole wall P washers under each nut. All nuts shall be brought to a snug tight condition. Holes in the walls may be drilled using core bits in lieu of formed holes. - ¼ (6) Tie ₽ Connection angle

JOINT SPLICE



CONNECTION ANGLE



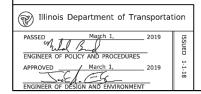
PLAN - FLAT SLAB TOP

(Showing layout of welded wire reinforcement and c bars)

6'-0" (1.83 m)

PLAN - FLAT SLAB TOP

6'-0" (1.83 m)



PRECAST MANHOLE TYPE A 5' (1.52 m) DIAMETER

(Sheet 2 of 2)

IDOT STANDARD 602402-02

(50) Bar c #5 (#16), — 7'-7" (2.31 m) length, 32 (815) radius top and bottom #4 (#13) bars bottom. Bundle first bar with closest WWR bar to the opening and place second bar ±3 (75) away.

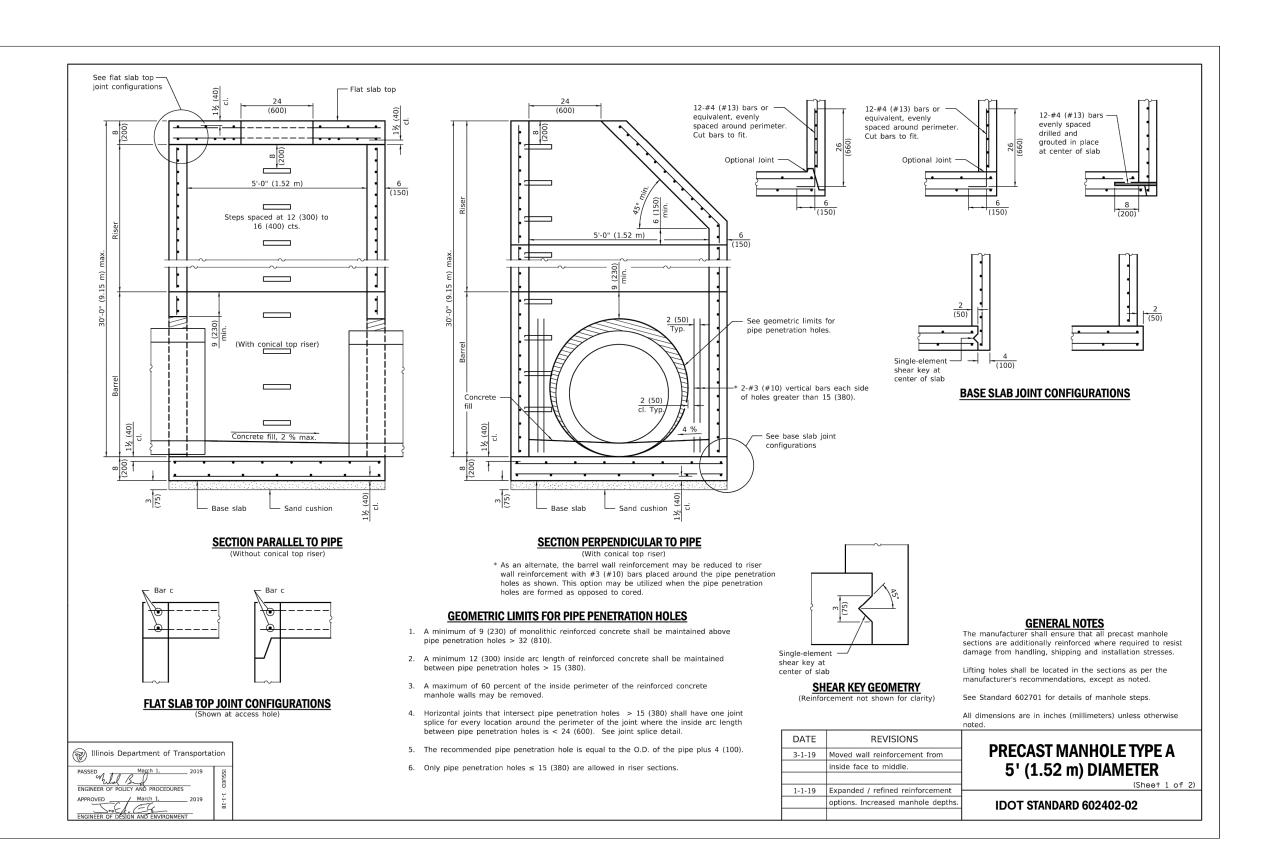
(50)

Bar c #5 (#16),

length, 32 (815) radius top and bottom

7'-7" (2.31 m)

63



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APRIL 25, 2025
REALIGN TAXIWAY A, PHASE 3

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VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

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DESIGNED BY: HCH
DRAWN BY: DPA
CHECKED BY: MJD

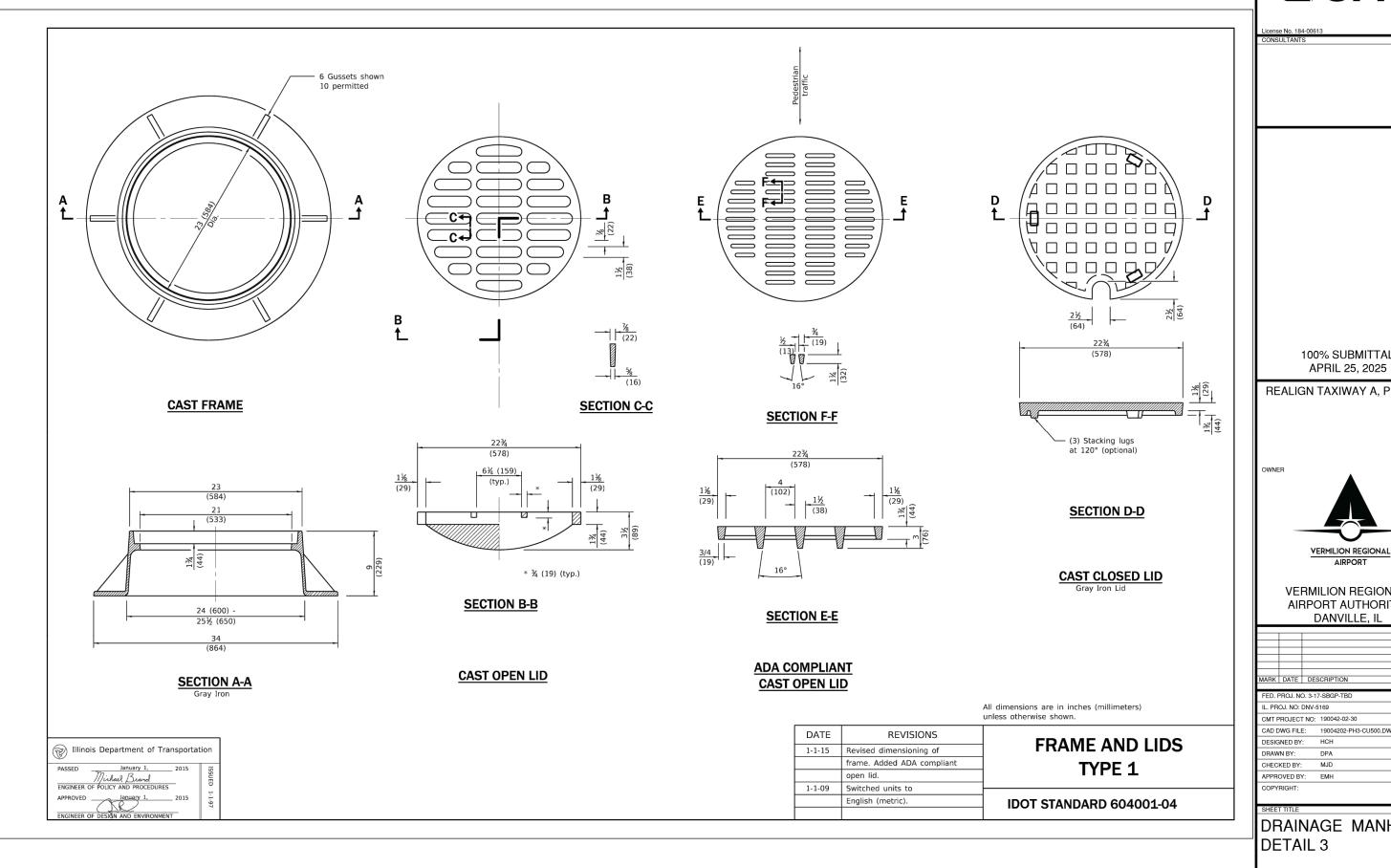
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SHEET TITL

DRAINAGE MANHOLE DETAIL 2

CU505

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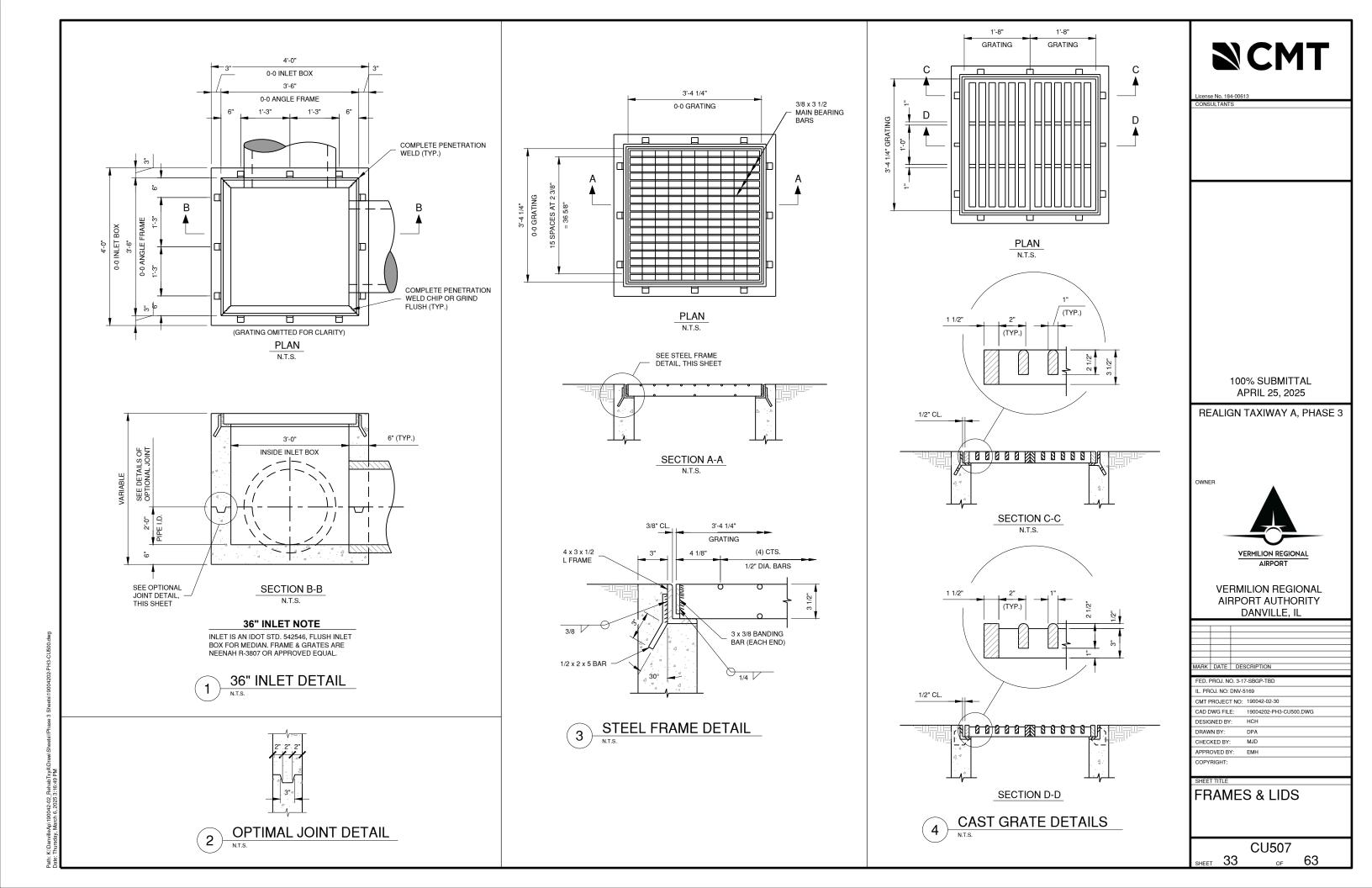
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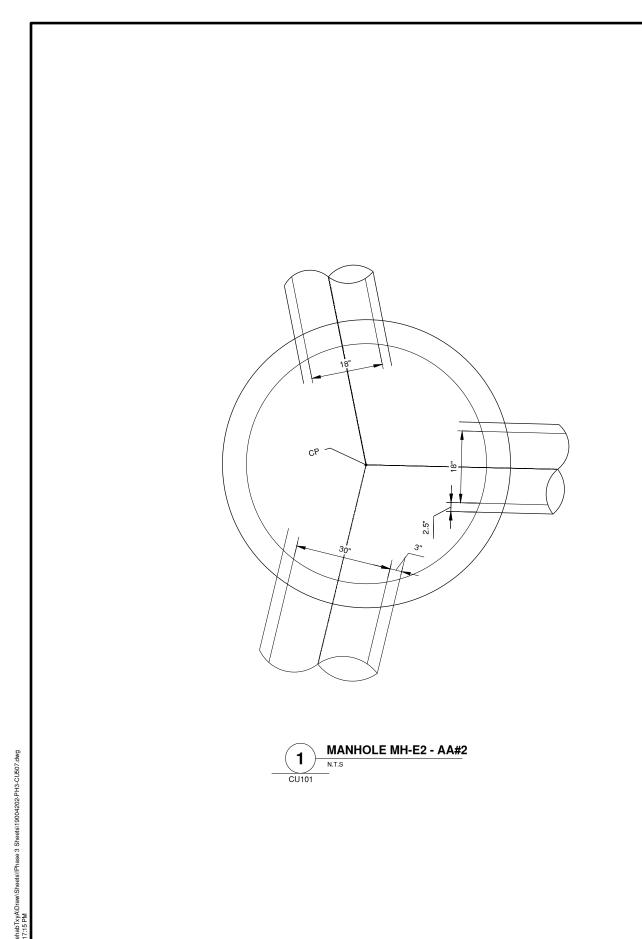
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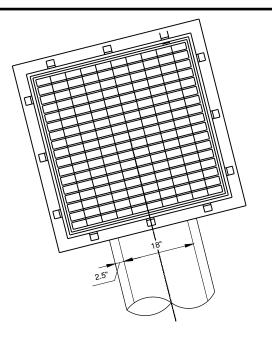
SHEET 32

DRAINAGE MANHOLE

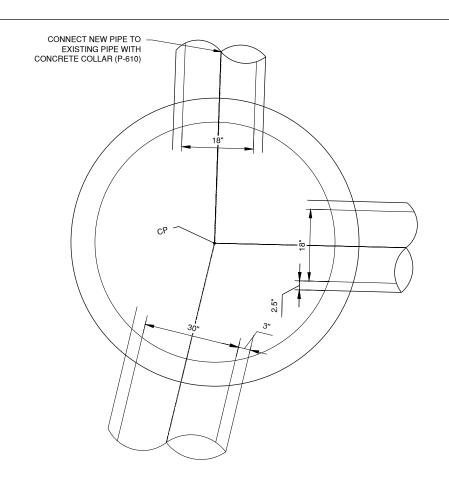
CU506 63







2 INLET IN-C1 - TYPE A



MANHOLE MH-E2 - BB

CU101

N.T.S



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CAD DWG FILE: 19004202-PH3-CU507.DWG
DESIGNED BY: HCH
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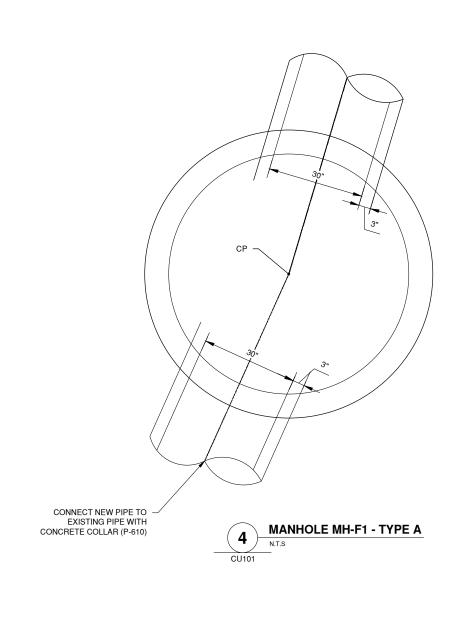
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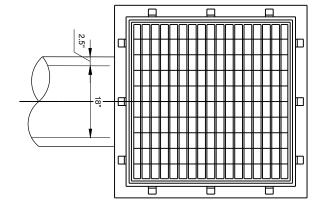
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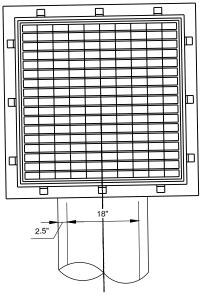
DRAINAGE STRUCTURE DETAIL 1

CU508 SHEET 34 OF 63





5 INLET IN-D1 - TYPE A



6 N.T.S



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CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-CU507.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

CHECKED BY: MJD

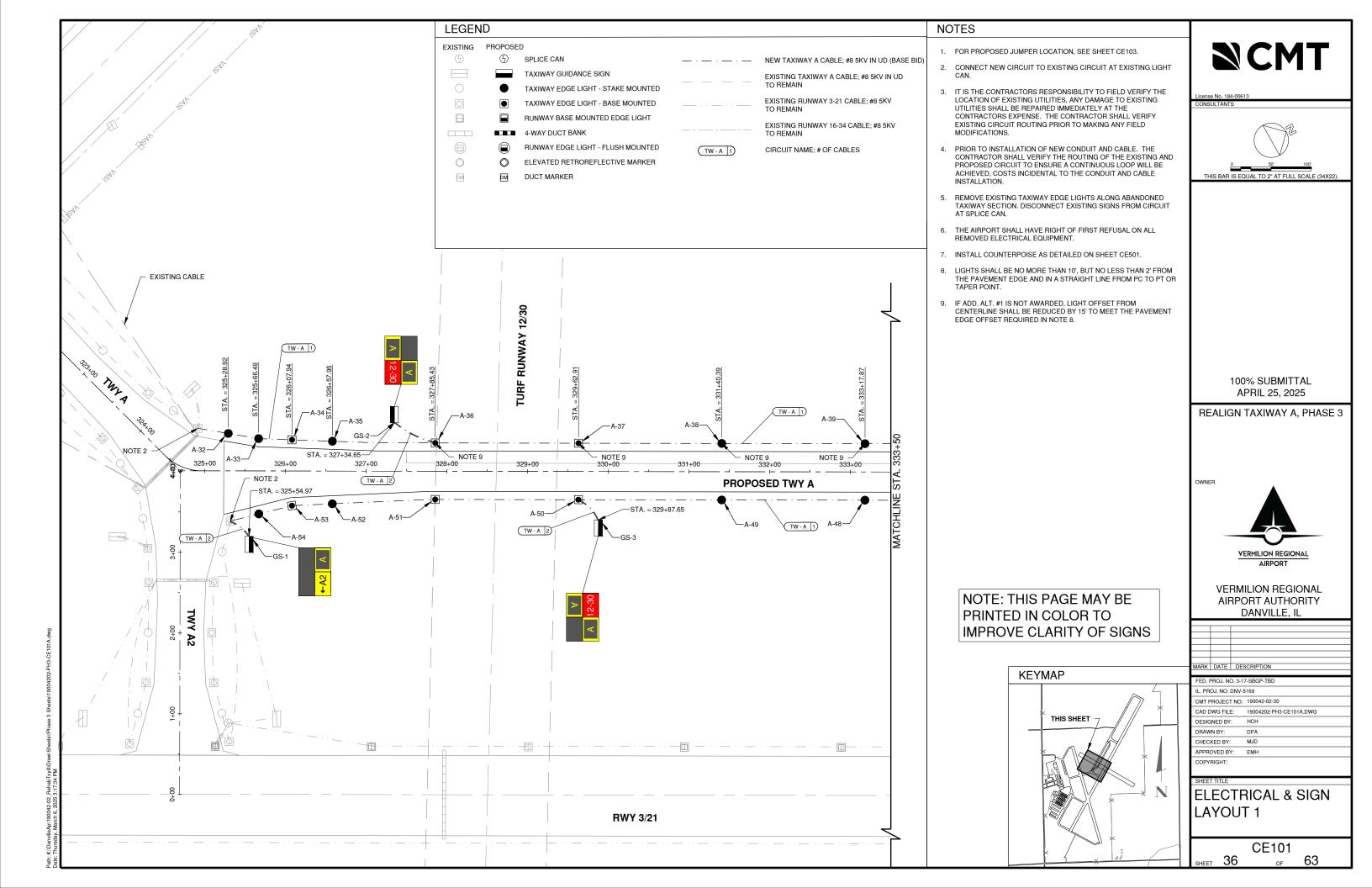
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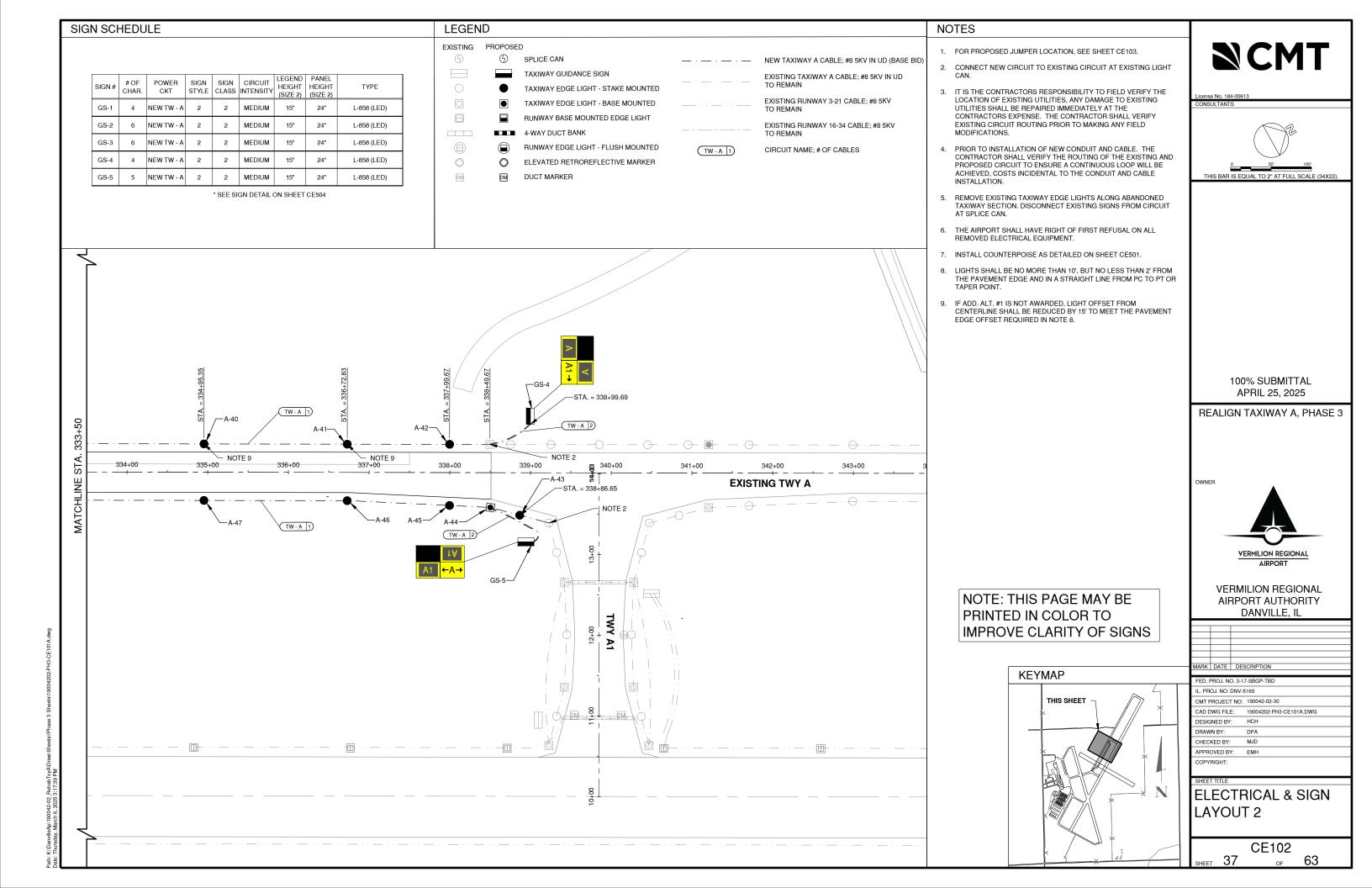
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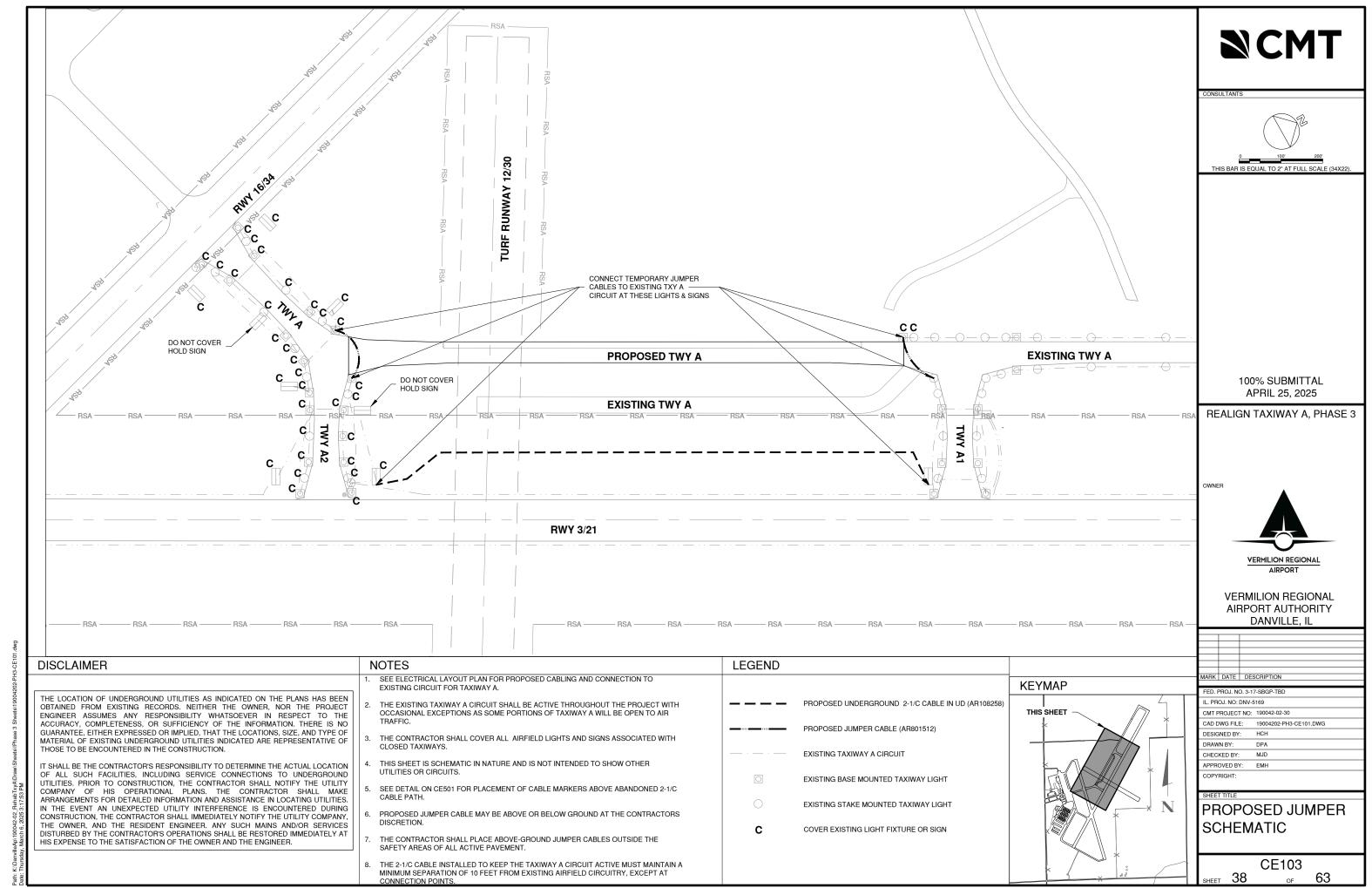
DRAINAGE STRUCTURE DETAIL 2

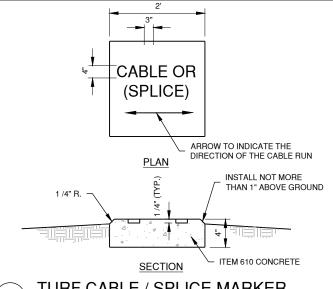
CU509 SHEET 35 OF 63

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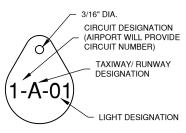




TURF CABLE / SPLICE MARKER

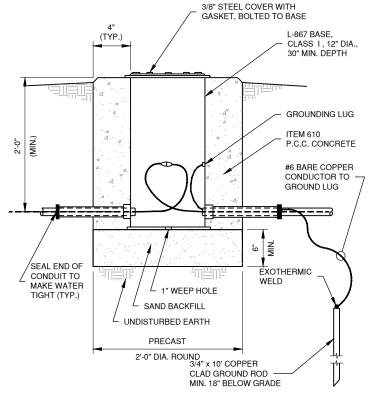
NOTES:

- 1. DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT WHERE PROPOSED ELECTRICAL DUCTS CROSS BOTH NEW AND EXISTING PAVEMENTS.
- 2. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- 3. ITEM 610 CONCRETE SHALL BE USED.
- 4. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- 5. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 6. 0.049 CU. YD. CONCRETE PER MARKER.
- 7. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



LIGHT IDENTIFICATION TAG

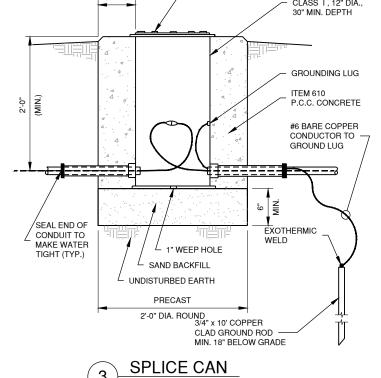
- 1. INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
- 2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
- 3 THE CONTRACTOR SHALL NUMBER THE EXISTING/ PROPOSED. LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
- 4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED

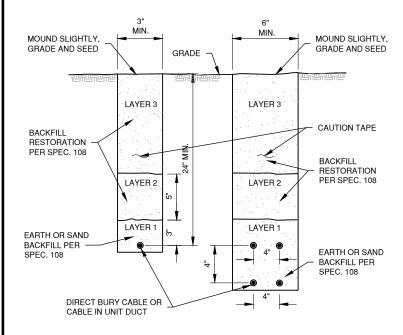


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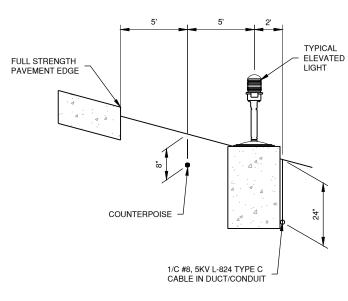




NOTES:

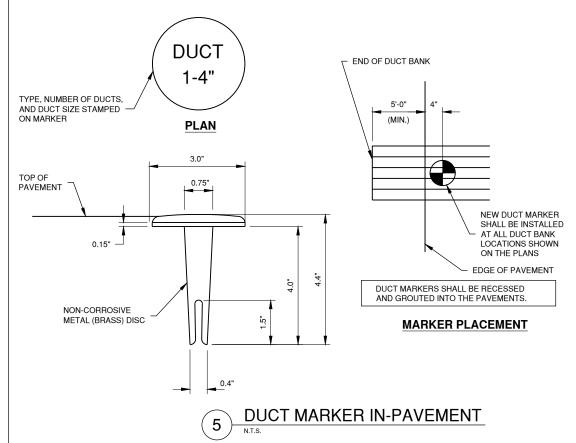
- 1. CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER.
- 2. WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.





#6 BARE COUNTERPOISE WITH 3/4" x 10' GROUND ROD INSTALLED AT MAX. 500' SPACING. ALSO USE GROUND ROD TO TERMINATE THE COUNTERPOISE AT BOTH ENDS OF DUCT. GROUND RODS SHALL BE CONSIDERED INCIDENTAL TO OTHER PAY ITEMS.







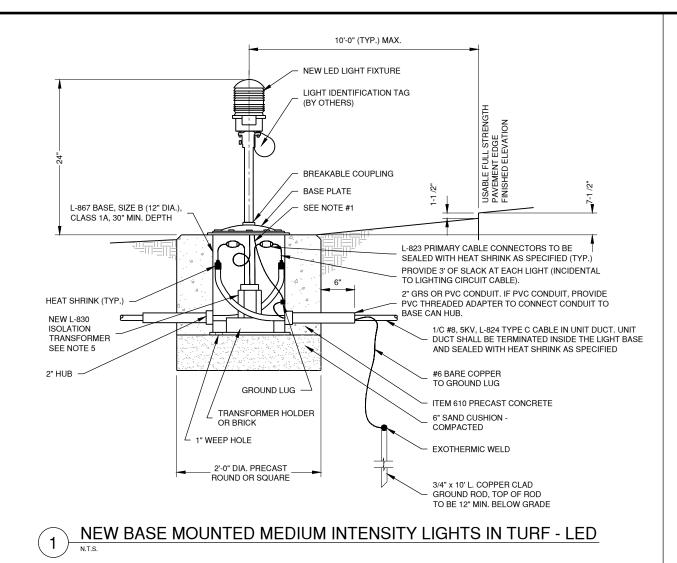
VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

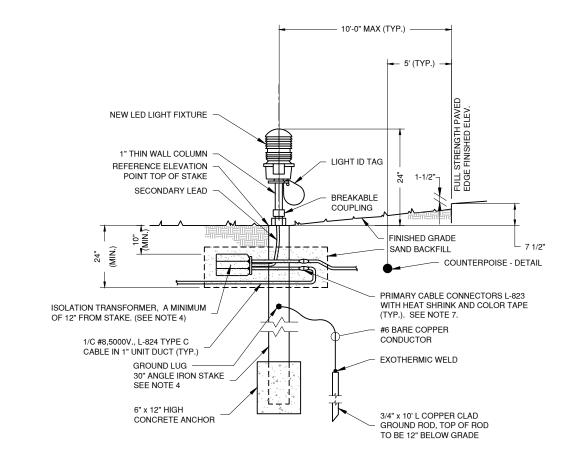
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ELECTRICAL DETAILS

CE501 39 HEET

63





APRIL 25, 2025 **REALIGN TAXIWAY A, PHASE 3**

VERMILION REGIONAL

AIRPORT

VERMILION REGIONAL

AIRPORT AUTHORITY

DANVILLE, IL

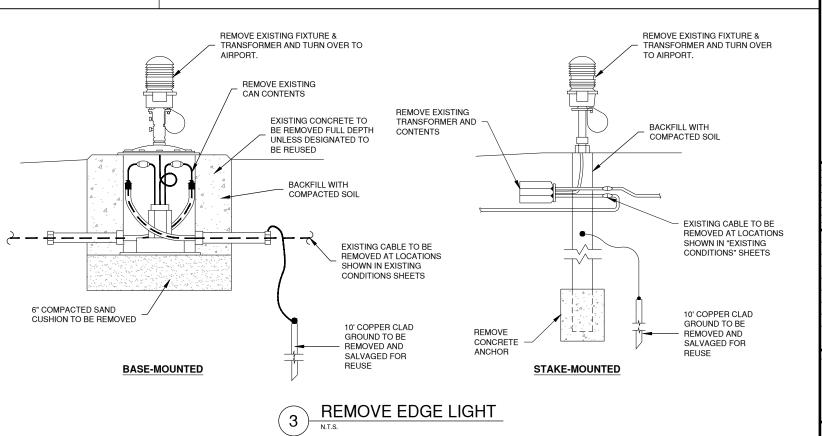
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NEW STAKE MOUNTED MEDIUM INTENSITY LIGHTS IN TURF - LED

EDGE LIGHT NOTES

- . THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE
- 2. LIGHT FIXTURES SHALL BE NEW LED.
- . NEW LIGHT FIXTURES SHALL BE L-861T, AS INDICATED ON THE PLANS AND SPECIFICATIONS. ALL NEW LIGHTS SHALL BE LED.
- DO NOT INSTALL ANGLE IRON STAKE BY DRIVING. MAKE ELECTRICAL CONNECTIONS AND BACKFILL AROUND THE STAKE WITH EARTH PASSING THE 1-INCH SIEVE. COMPACT AS REQUIRED TO PROVIDE FIRM SUPPORT FOR STAKE, AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.
- 5. NEW TRANSFORMERS FOR EDGE LIGHTS AND SIGNS SHALL BE COMPATIBLE WITH THE REGULATORS SERVING
- 6. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE \pm 1 INCH. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH.
- DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH RED AND CABLE TO THE RIGHT IS CODED BLUE.
- . APPLY A CORROSION INHIBITING, ANTI-SEIZE COMPOUND TO ALL SCREWS, NUTS AND FRANGIBLE COUPLING THREADS. IF COATED BOLTS ARE USED PER ENGINEERING BRIEF #83 DO NOT APPLY ANTI-SEIZE COMPOUND.

- 9. ELECTRICAL INSULATING GREASE MUST BE APPLIED WITHIN THE L-830 ISOLATION TRANSFORMER SECONDARY TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THE CONNECTORS MUST NOT BE TAPED
- 10. ENTRANCES IN L-867 BASES MUST BE PLUGGED FROM THE INSIDE WITH DUCT SEAL TO MAKE WATERTIGHT.
- 11.LIGHT BASES SHALL BE PRECAST
- 12. CABLES SHALL NOT BE LESS THAN 24 INCHES DEEP. ENCASE ISOLATION TRANSFORMER, L-823 CONNECTORS, AND 2 FOOT MINIMUM OF L-824 CABLE SLACK IN SAND
- 13.L-830 ISOLATION TRANSFORMERS FOR LED EDGE LIGHTS SHALL BE L-830-1 30/45 WATT. LED EDGE LIGHTS WITHOUT HEATERS SHALL BE L-830-16, 10/15 WATT OR L-830-17, 20/25 WATT, AS RECOMMENDED BY LIGHT MANUFACTURER.
- 14. UNIT DUCT, WHERE INSTALLED, SHALL BE TERMINATED AT L-823 CONNECTORS AND SEALED TO MAKE
- 15.ENTRANCES IN L-867 BASES MUST BE PLUGGED FROM THE INSIDE WITH DUCT SEAL TO MAKE WATERTIGHT.
- 16 EDGE LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2' FROM THE FULL STRENGTH PAVEMENT EDGE, IN A STRAIGHT LINE PARALLEL WITH CENTERI INF



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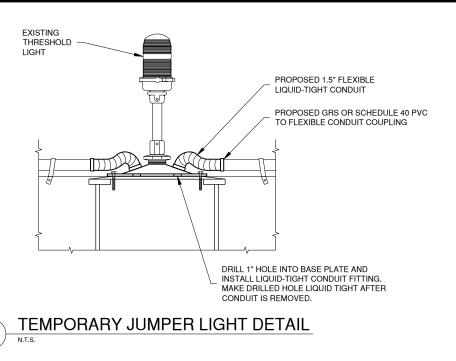
OWNER

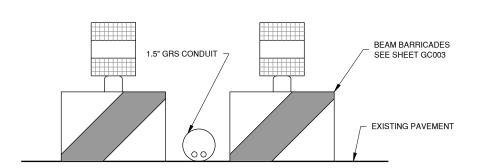
ELECTRICAL DETAILS

MJD

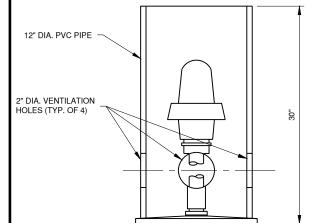
CE502 HEET 40

63





TEMPORARY JUMPER CABLE CONDUIT PROTECTION DETAIL



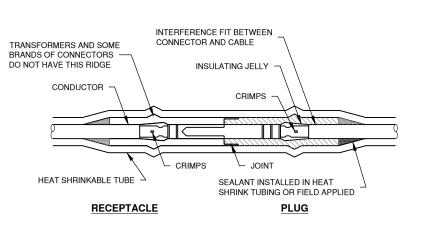
TAXIWAY EDGE LIGHT COVER DETAIL

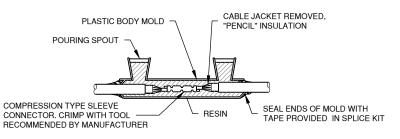
NOTES

 3° TALL TRAFFIC CONES MAY BE SUBSTITUTED FOR PVC PIPE.

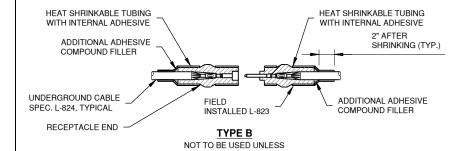
INSTALLATION INSTRUCTIONS TO SUPPLEMENT THE **MANUFACTURER'S INSTRUCTIONS**

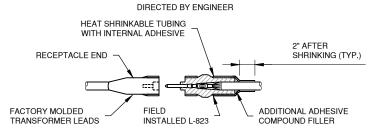
- 1. CLEAN THE CABLE THOROUGHLY 9" MIN. FROM THE END.
- REMOVE INSULATION PER MANUFACTURER'S INSTRUCTIONS. DO NOT NICK THE CONDUCTOR. DO NOT PENCIL INSULATION ON L-824 TYPE C CABLE.
- 3. INSTALL PIN AND/OR RECEPTICAL WITH CRIMPING TOOL WHICH MUST BE COMPLETELY CLOSED BEFORE THE TOOL MAY BE REMOVED.
- BE SURE CABLE AND CONDUCTOR FITTINGS ARE CLEAN. COAT THE CABLE INSULATION WITH INSULATION JELLY FROM THE CONNECTOR
- 5. CAREFULLY INSERT CABLE INTO CONNECTOR TO THE PROPER DEPTH.
- SLIP 14 INCH LENGTH OF HEAT SHRINK TUBING ON TRANSFORMER LEAD RAYCHEM TCS-13-14-U OR APPROVED EQUAL
- COMPLETE CONNECTION BY MATING THE PLUG AND RECEPTICAL. **CAUTION** BE SURE THE CABLE DOES NOT SLIP WHEN THE CONNECTION IS MADE.
- 8. APPLY RUBBER TAPE AND PLASTIC TAPE, ONE HALF LAPPED 1-1/2" ON EACH SIDE OF JOINT.
- ANY CONNECTOR WHICH IS CONTAMINATED BY DIRT OR OTHER DELETERIOUS MATERIAL SHALL BE REMOVED NOT REINSTALLED.
- 10. CLEAN CONNECTOR AND CABLE INSULATION WITH WAX OR GREASE SOLVENT TO REMOVE SURFACE SILICONE JELLY.
- . WRAP SEALANT SECURELY AROUND THE CABLE. INSULATION TO EXTEND 1-1/2" BEYOND BOTH ENDS OF CONNECTORS. SEALANT SHALL BE RAYCHEM S-1052 (STRIPS) OR APPROVED EQUAL.
- CENTER HEAT SHRINK OVER THE CONNECTOR. APPLY HEAT EVENLY BEGINNING AT THE CENTER AND WORKING AROUND CABLE TO ENDS. THERMOCHROMIC PAINT SHALL SHOW PROPER HEAT HAS BEEN USED. *** DO NOT OVER HEAT ***.
- 13. THE HEAT SOURCE SHALL BE AN ELECTRIC HEAT GUN OR A PROPANE WITH FLAME



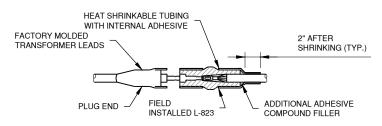


TYPE A FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY





TYPE C FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS



TYPE D FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

CABLE SPLICES

NOTES

- MATCH THE OUTSIDE DIAMETER OF CABLE INSIDE DIAMETER OF CONNECTOR SHALL PROPERL
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.



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REALIGN TAXIWAY A, PHASE 3



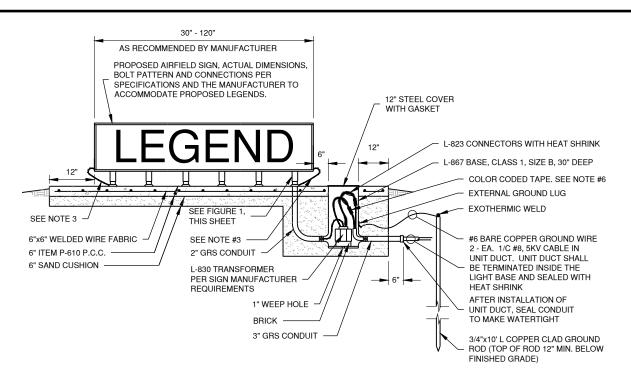
VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

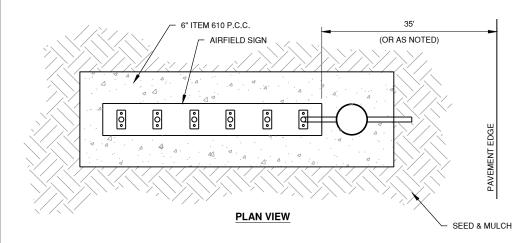
MARK DATE DESCRIPTION IL. PROJ. NO: DNV-5169 CMT PROJECT NO: 190042-02-30 CAD DWG FILE: 19004202-PH3-CE500.DWG DESIGNED BY: DRAWN BY: CHECKED BY: MJD APPROVED BY: COPYRIGHT

ELECTRICAL DETAILS

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14. CONTRACTOR MAY ELECT TO INSTALL "COMPLETE KIT" IN LIEU OF L-823 CONNECTOR WITH HEAT SHRINK.

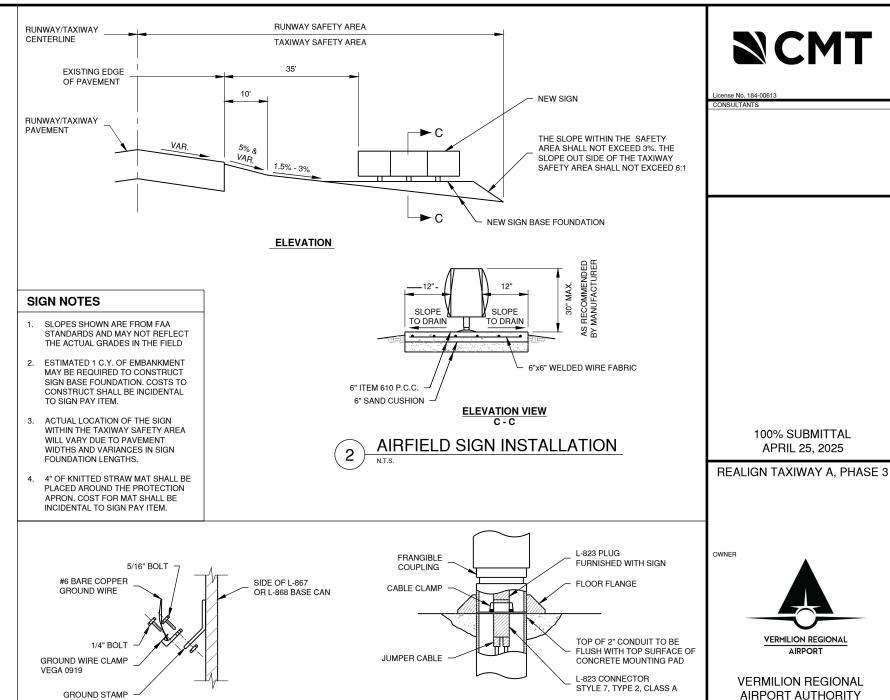






AIRFIELD GUIDANCE SIGN NOTES

- 1 TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY LED SIGN MANUFACTURER SIGNS ON RUNWAY CIRCUITS SHALL BE STYLE 2 OR 3 DEPENDING ON REGULATOR.
- 2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN. CHANGES TO NEW LEGENDS MAY OCCUR
- 3. SIGN ANCHOR TETHERS AND GROUND WIRES ARE REQUIRED. SEE SPECIFICATIONS.
- 4. SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2, AND MODE 2. SEE SIGN SCHEDULE FOR DETAILS
- 5. LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN
- 6. DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING SIGN IN BACK FACING THE RELATED RUNWAY OR TAXIWAY PAVEMENT, THE CABLE FOR THE CIRCUIT TO THE LEFT IS CODED RED AND CABLE FOR THE CIRCUIT TO THE RIGHT IS CODED BLUE.



FACTORY GROUND LUG

DANVILLE, IL MARK DATE DESCRIPTION FED. PROJ. NO. 3-17-SBGP-TBD IL. PROJ. NO: DNV-5169 CMT PROJECT NO: 190042-02-30 CAD DWG FILE: 19004202-PH3-CE500.DWG DESIGNED BY: CHECKED BY: MJD APPROVED BY:

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ELECTRICAL CONNECTION

ELECTRICAL DETAILS

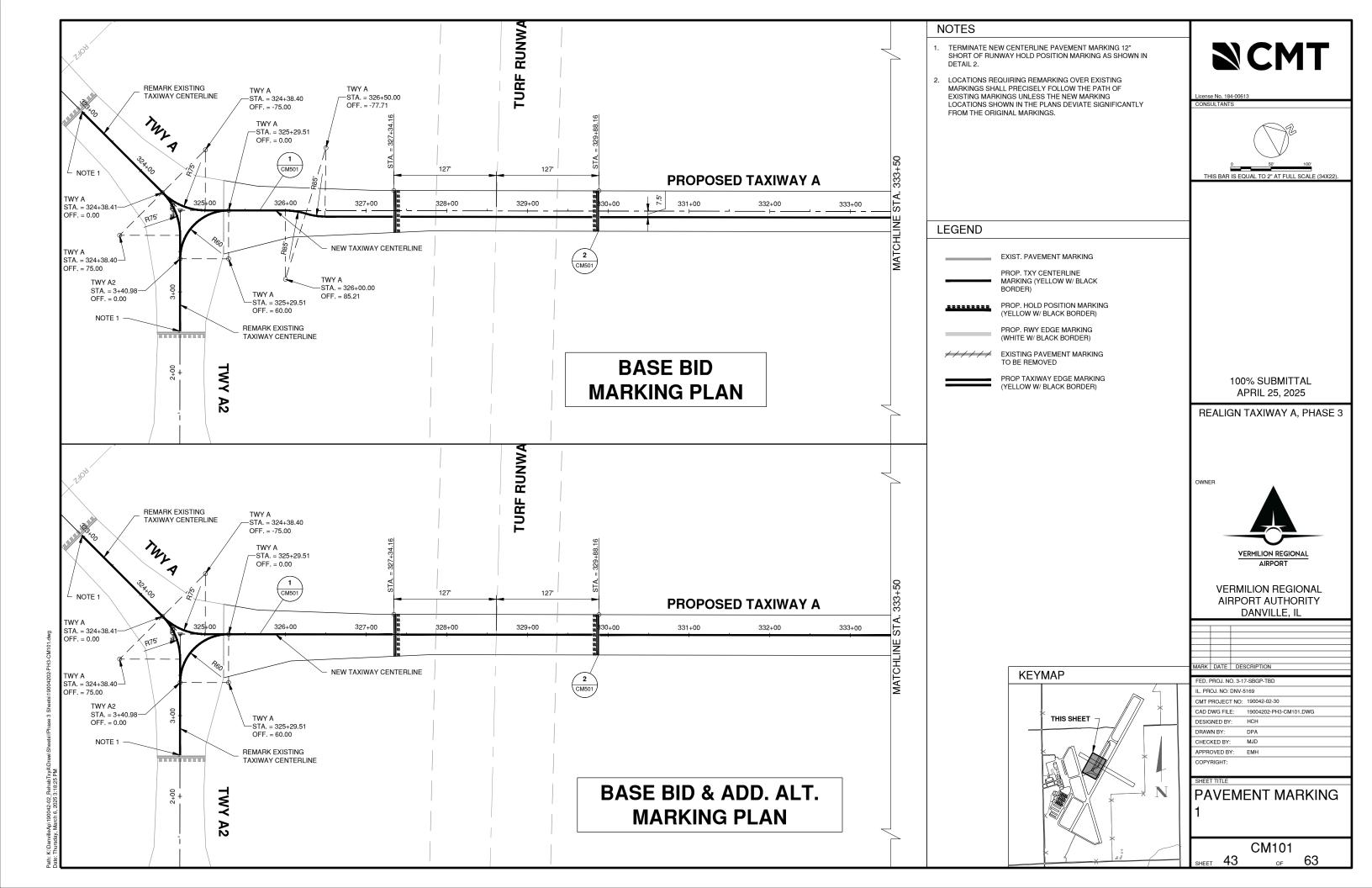
100% SUBMITTAL

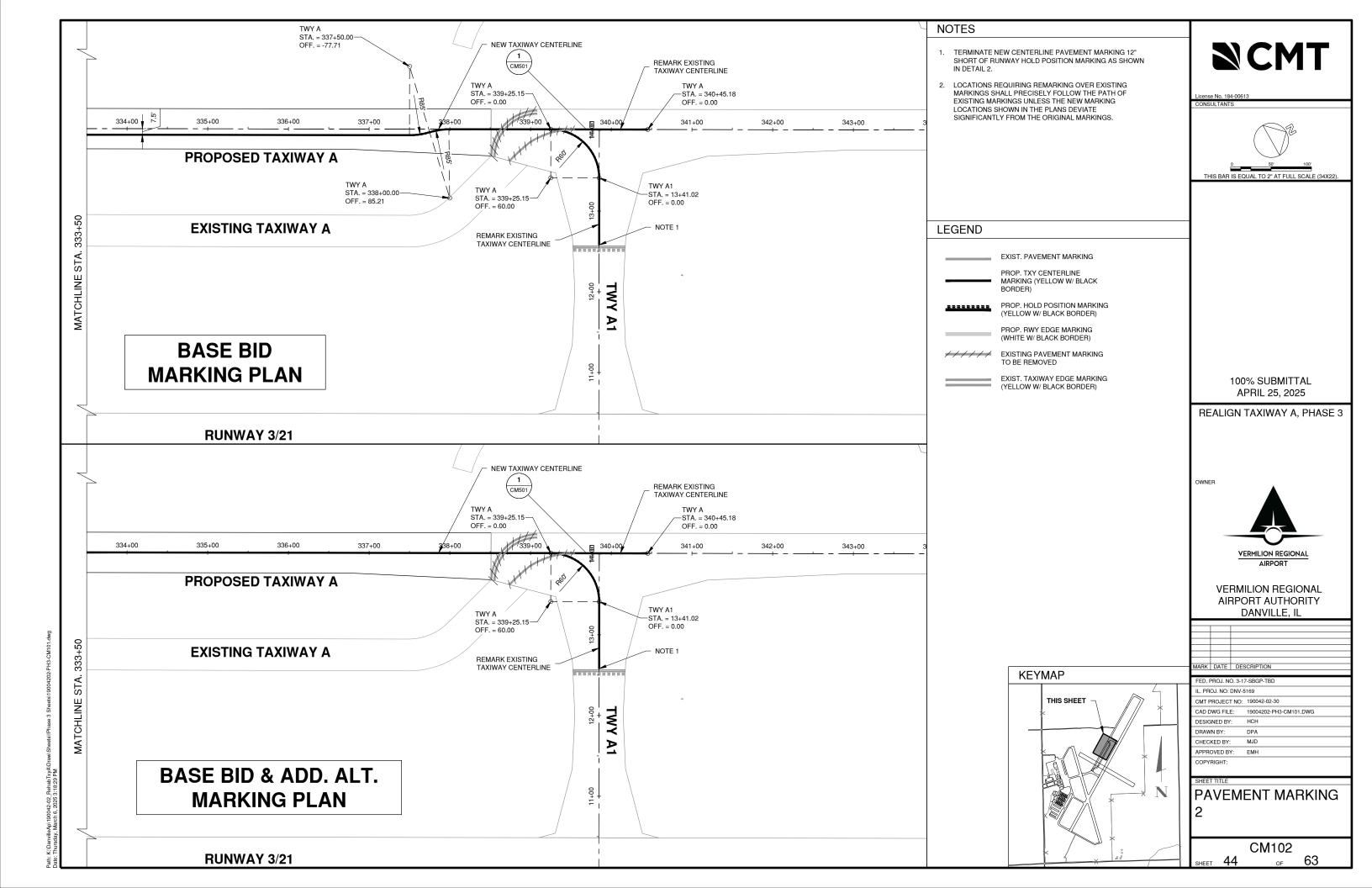
APRIL 25, 2025

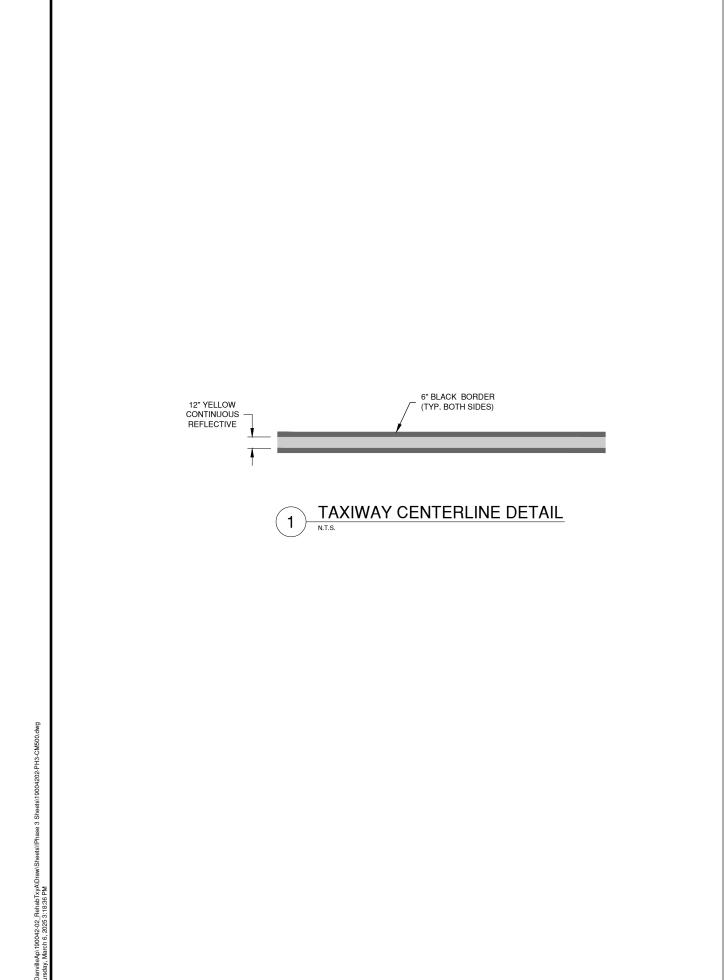
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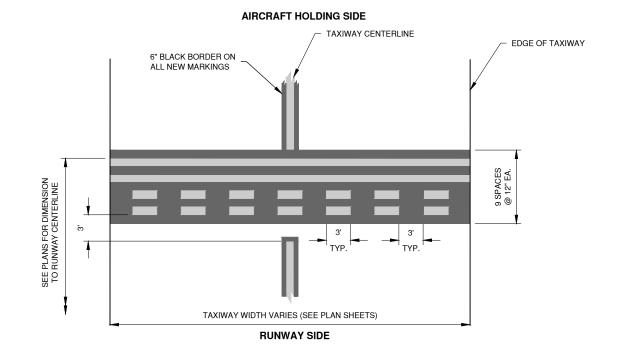
AIRPORT

CE504 SHEET 42 63









2 RUNWAY HOLDING POSITION

N.T.S.



icense No. 184-0061

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REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-CM500.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

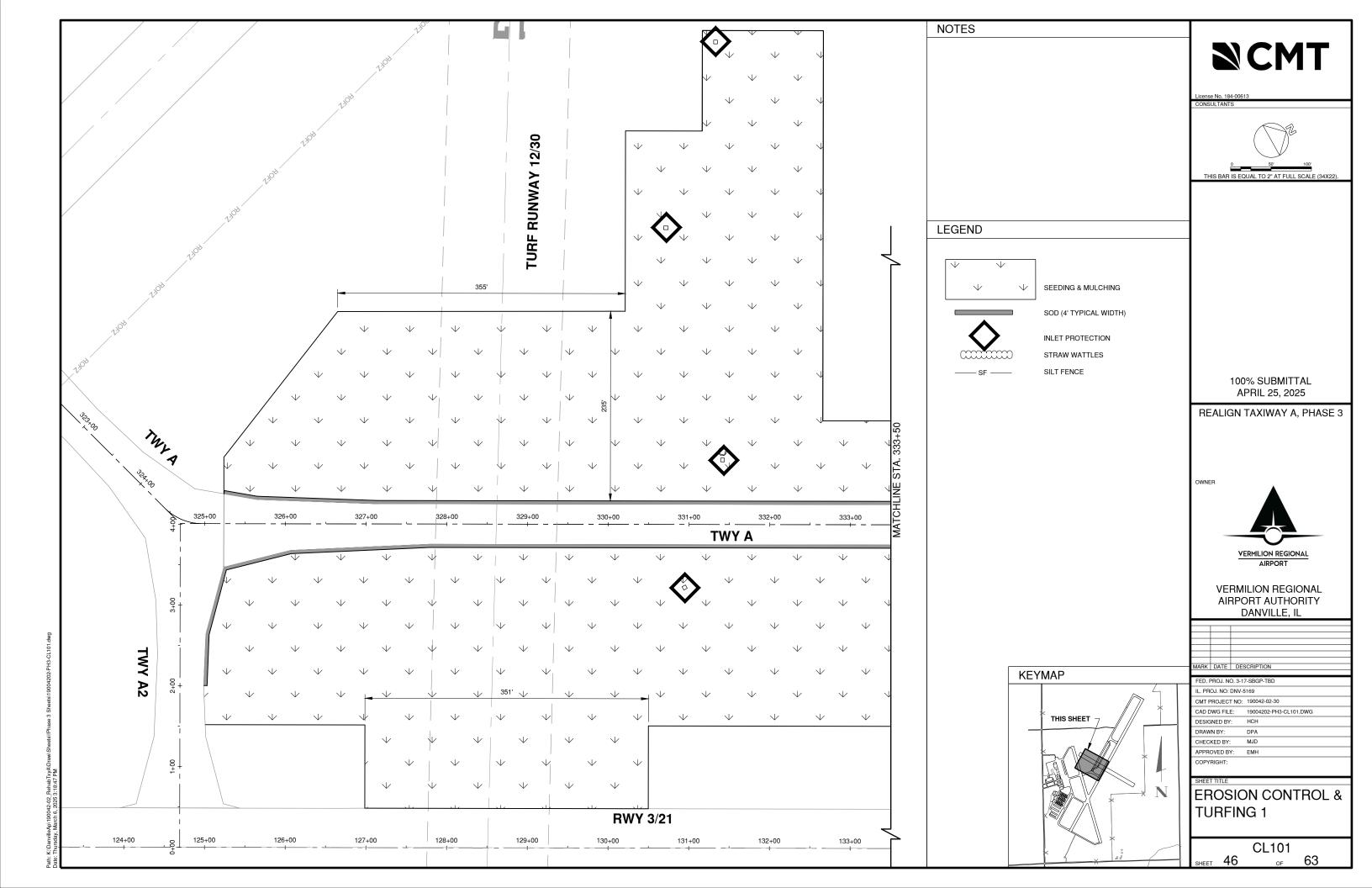
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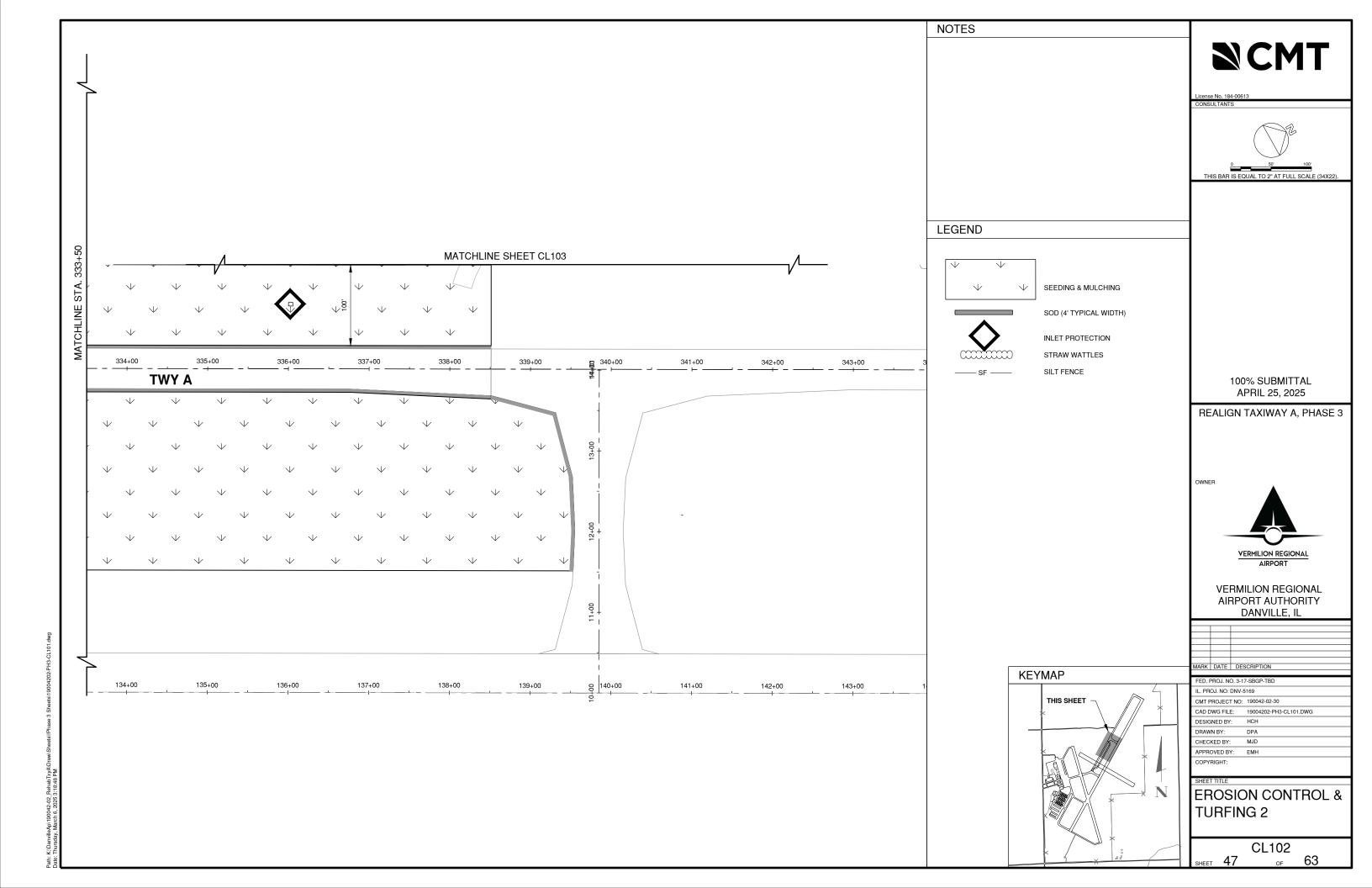
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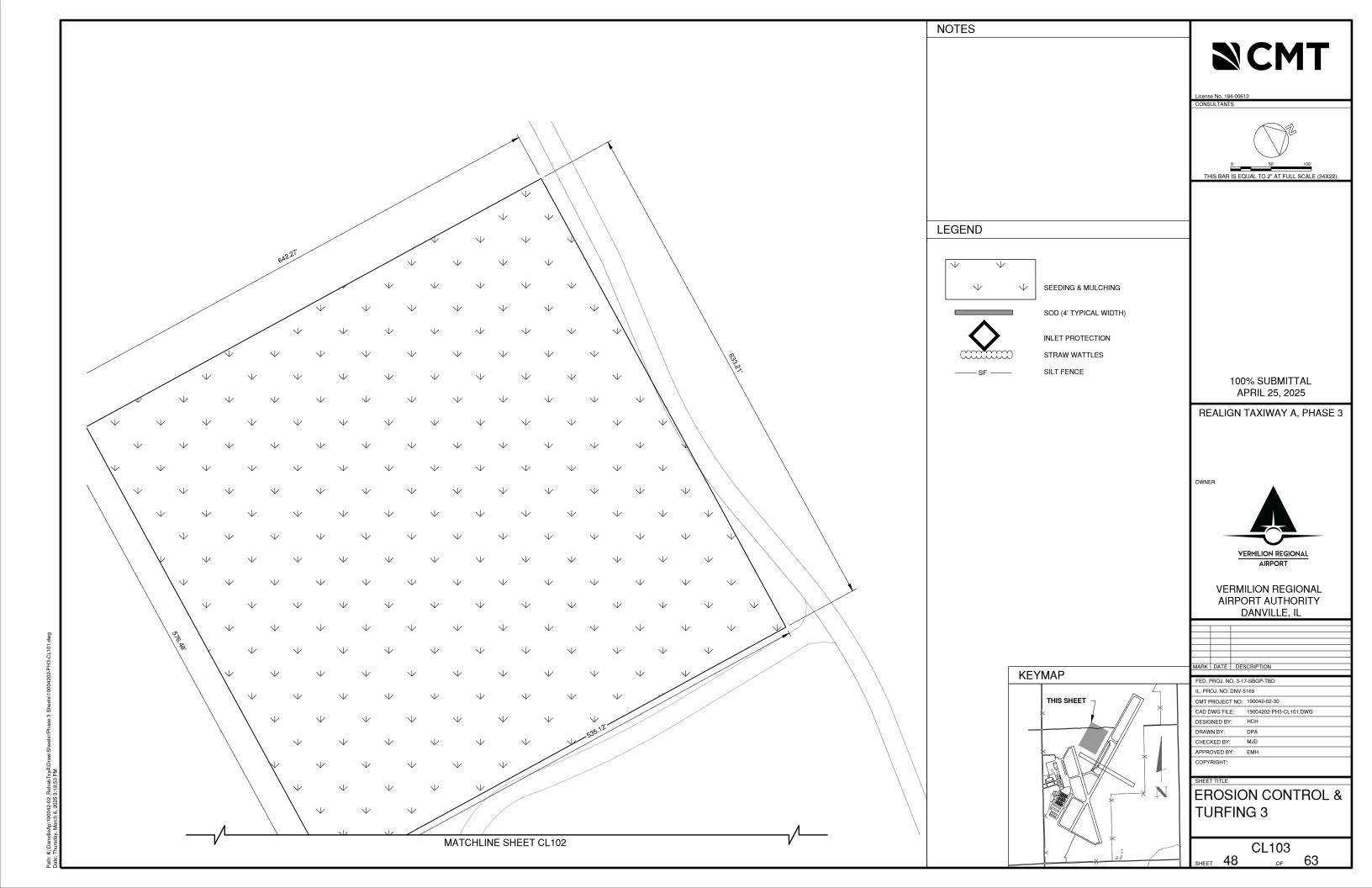
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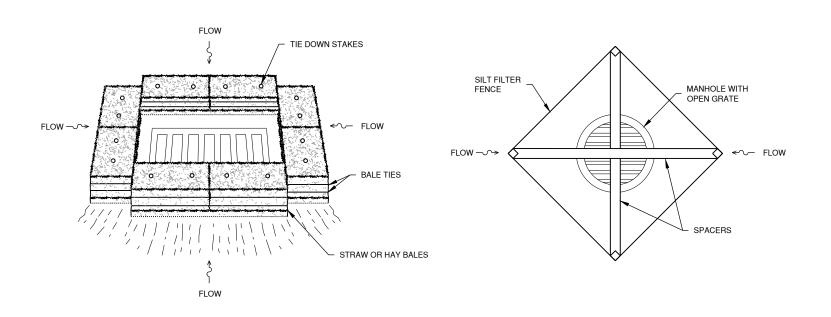
PAVEMENT MARKING DETAILS

CM501 SHEET 45 OF 63









INLET PROTECTION

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NCMT

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

III. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-CL500.DWG

DESIGNED BY: HCH

 DESIGNED BY:
 HCH

 DRAWN BY:
 DPA

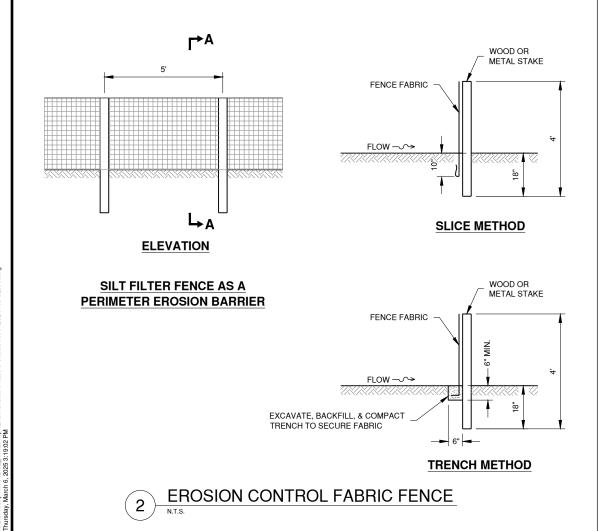
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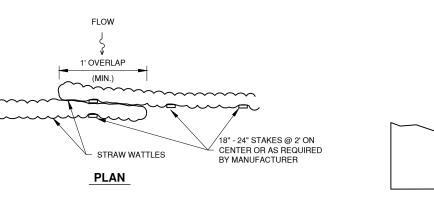
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EROSION CONTROL DETAILS

CL501 sheet 49 of 63





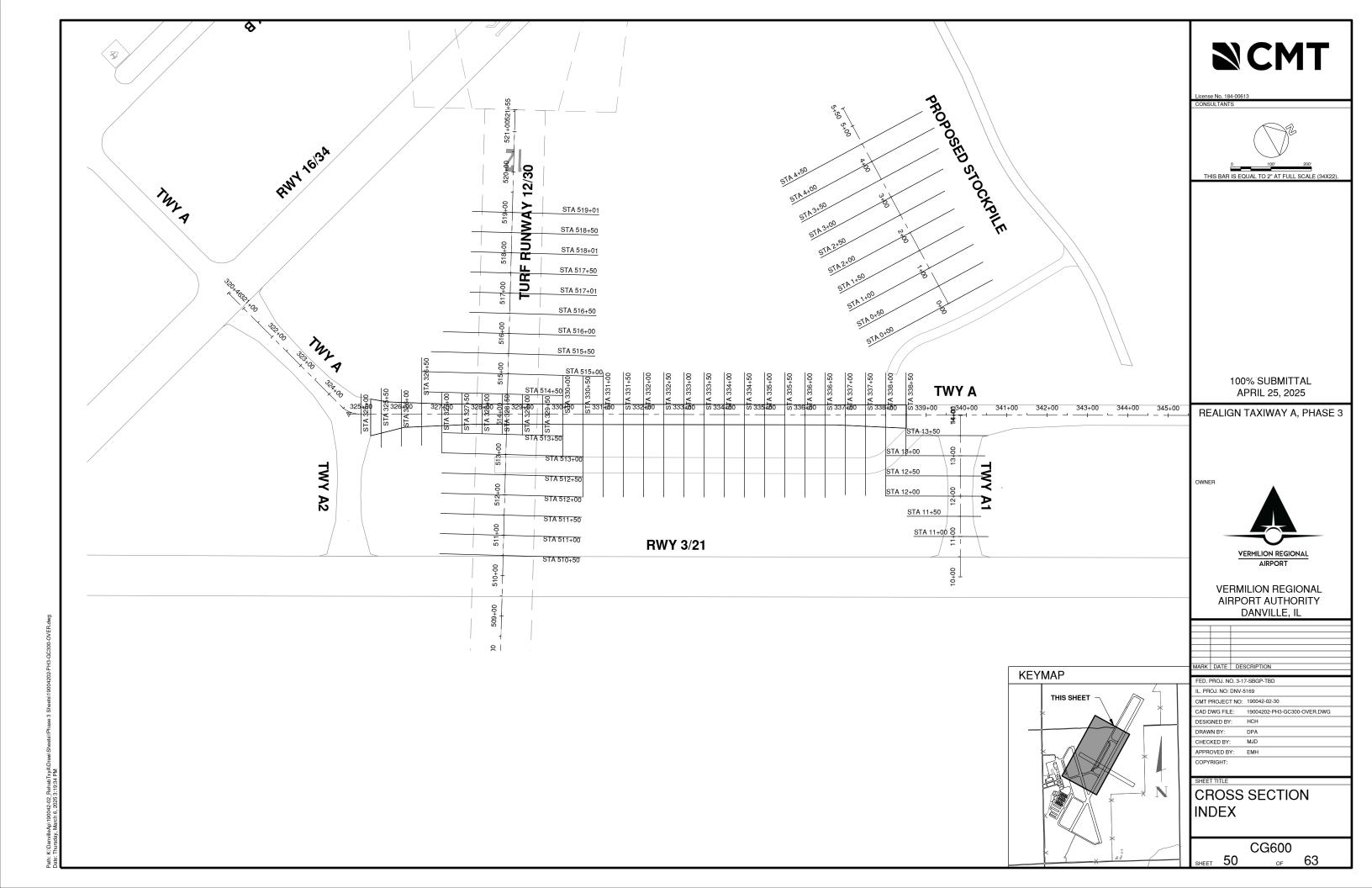
12" STRAW WATTLE

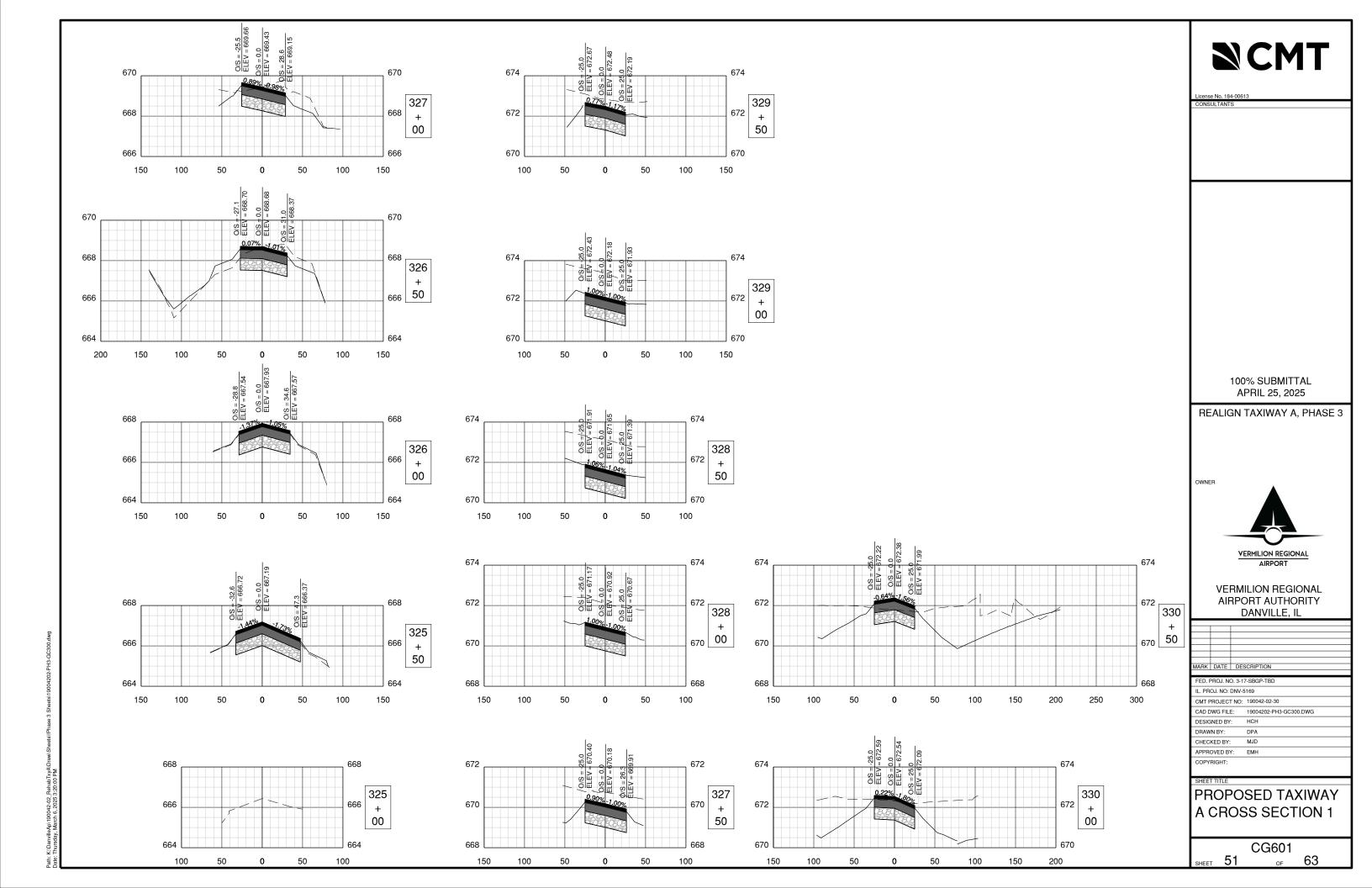
2" TRENCH OR AS REQUIRED
BY MANUFACTURER

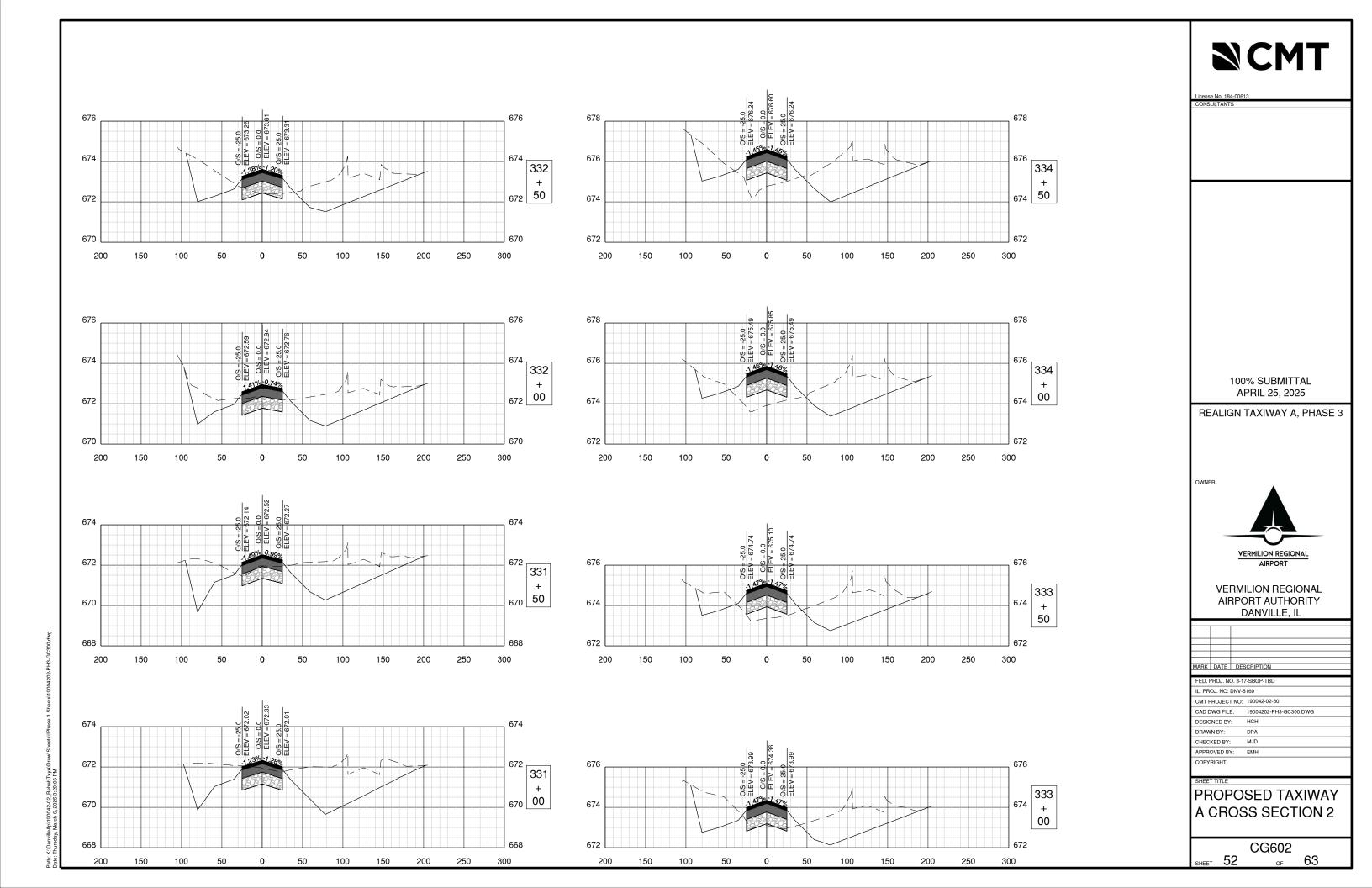
SECTION

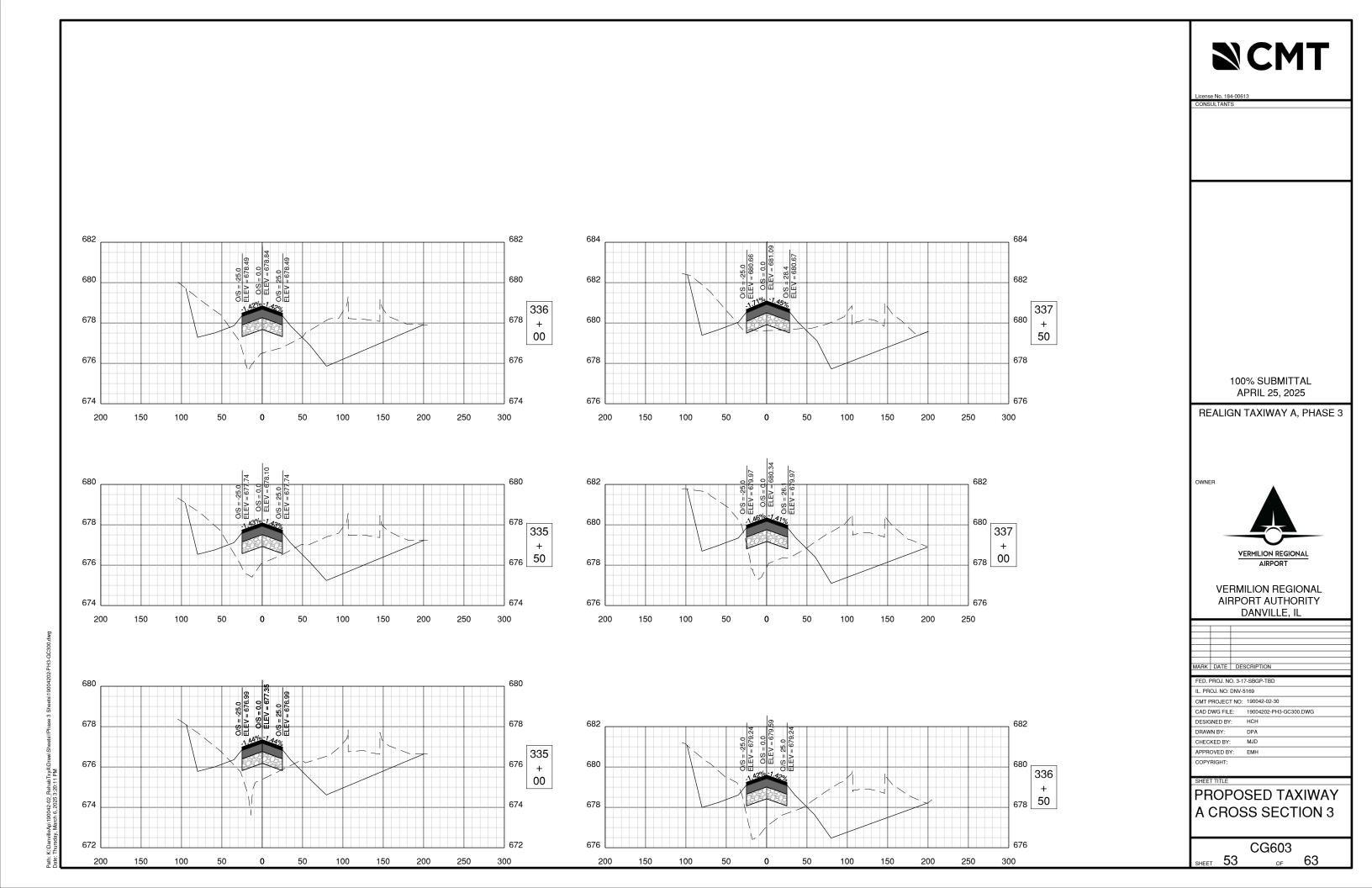
WOOD STAKES - SIZE, LENGTH, AND SPACING PER MANUFACTURER'S RECOMMENDATIONS

STRAW WATTLES

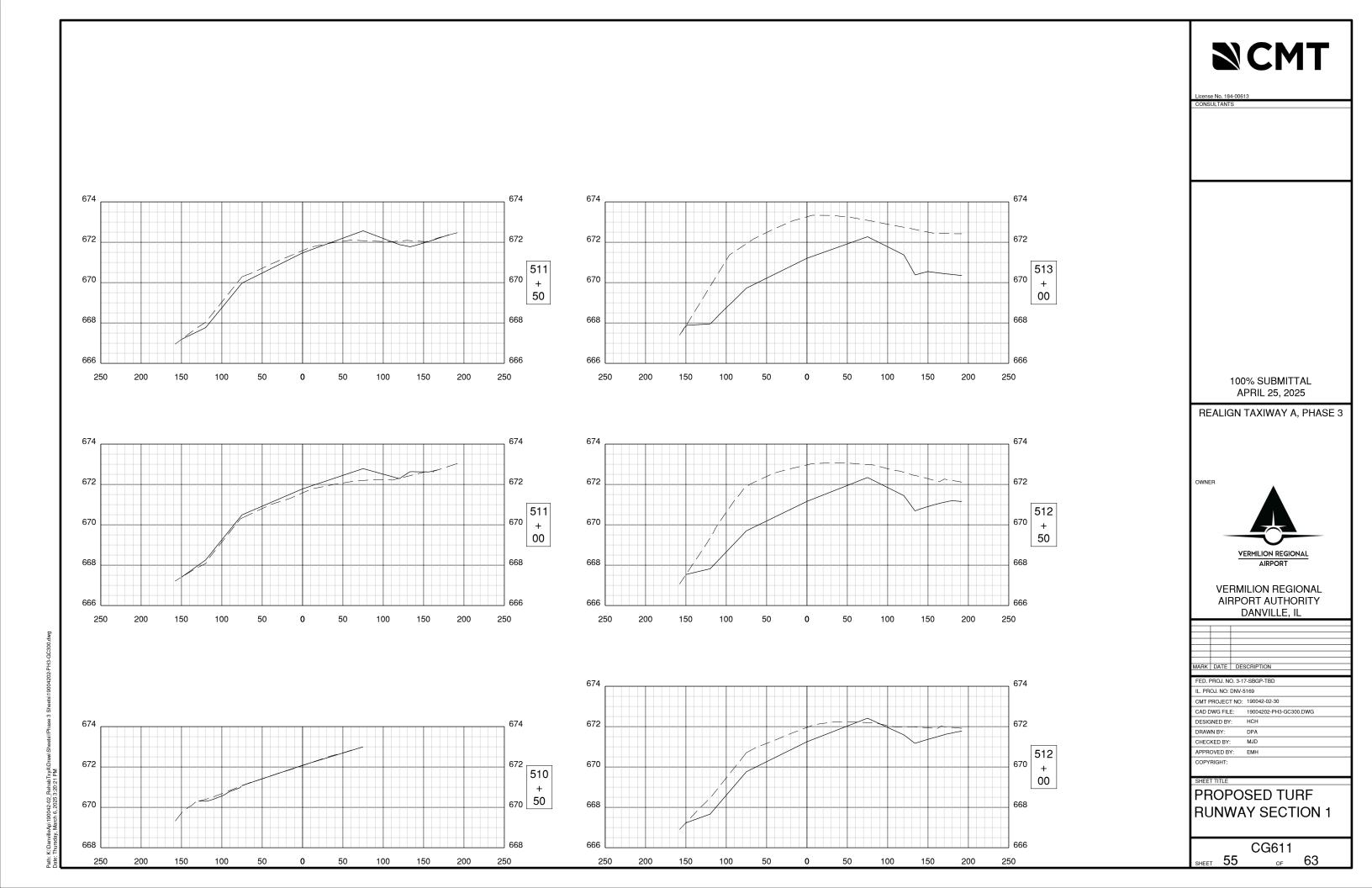








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NCMT COPYRIGHT:

674 674 672 513 + ₆₇₀ 50 670 668 668 200 150 50 100 150

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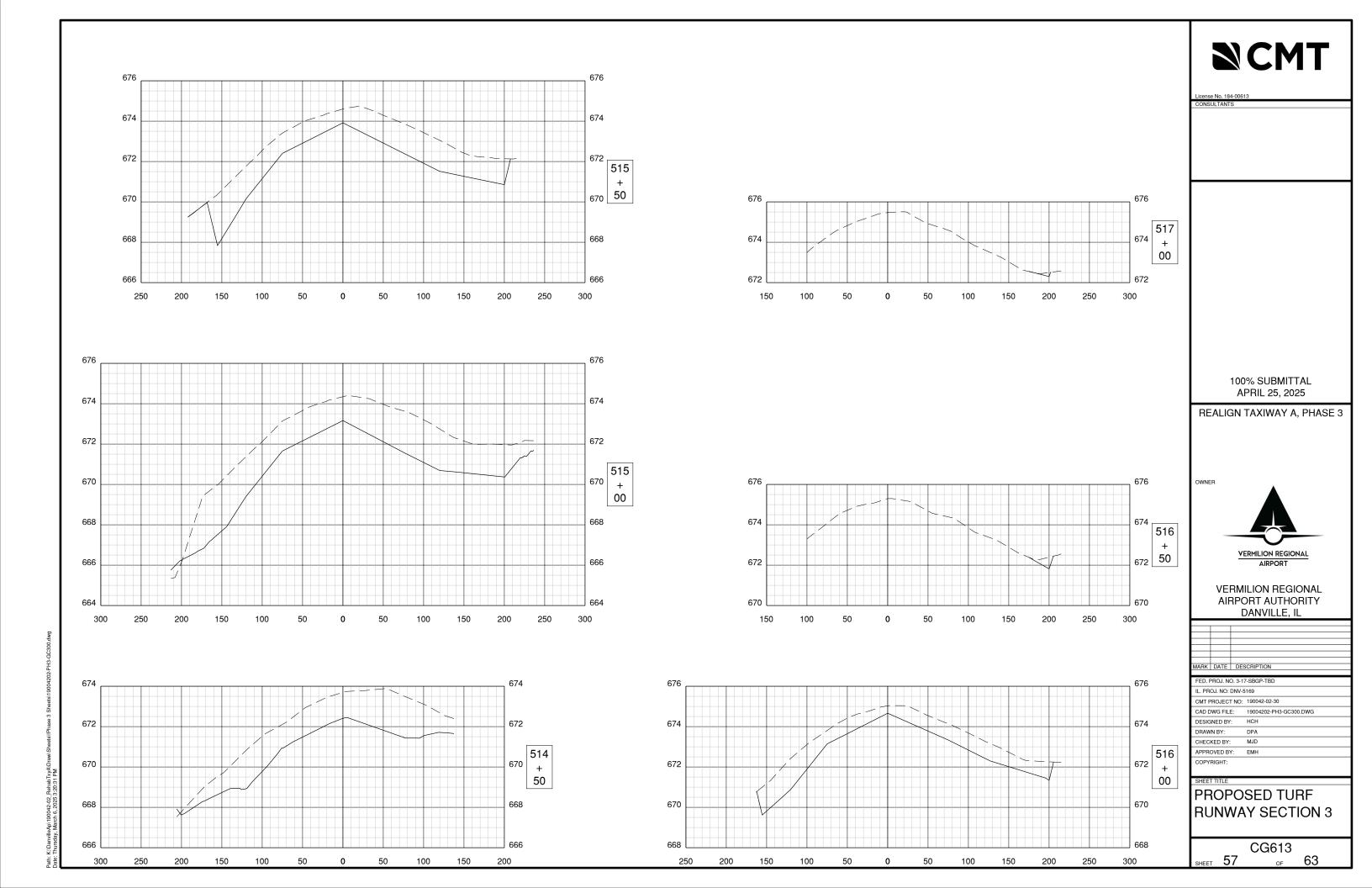
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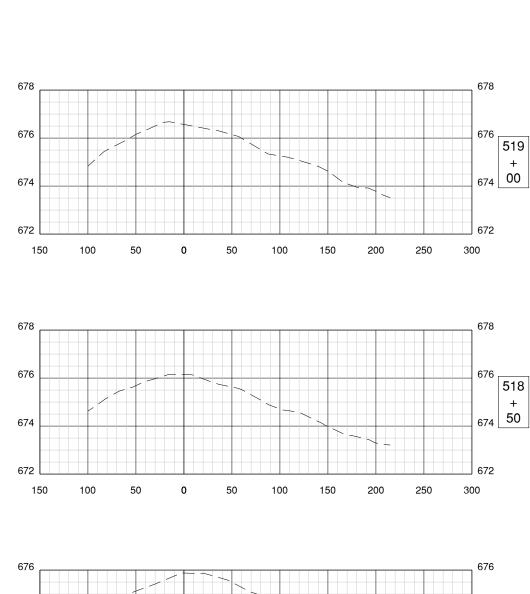
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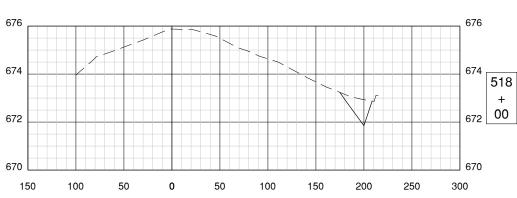
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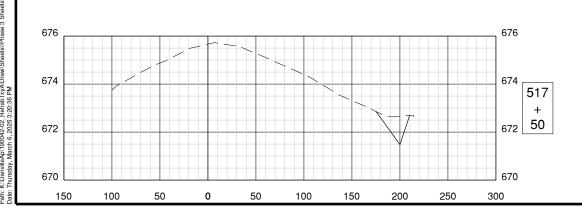
PROPOSED TURF RUNWAY SECTION 2

CG612 SHEET **56** 63 OF









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REALIGN TAXIWAY A, PHASE 3

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CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-GC300.DWG

DESIGNED BY: HCH

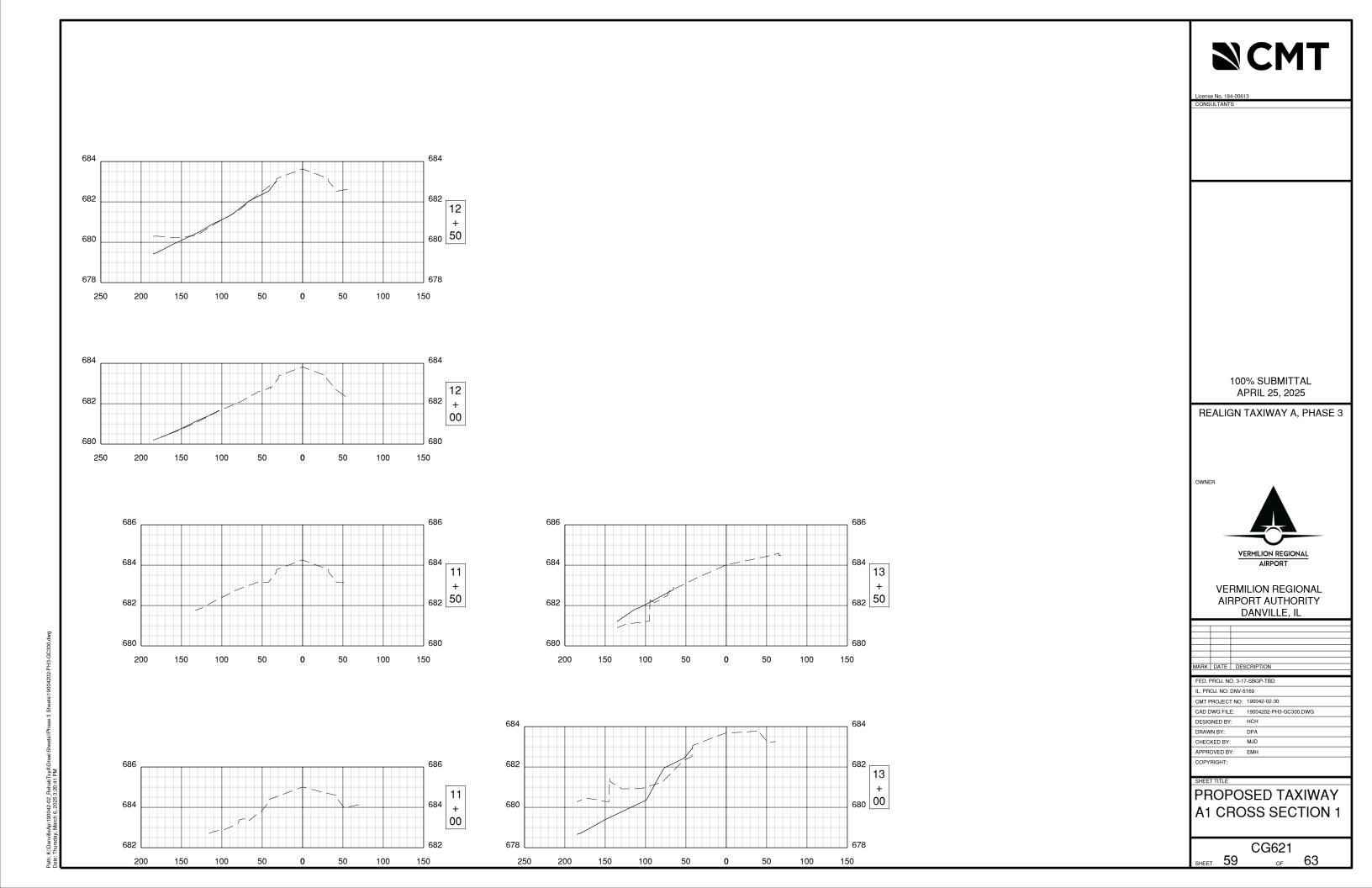
DRAWN BY: DPA
CHECKED BY: MJD
APPROVED BY: EMH

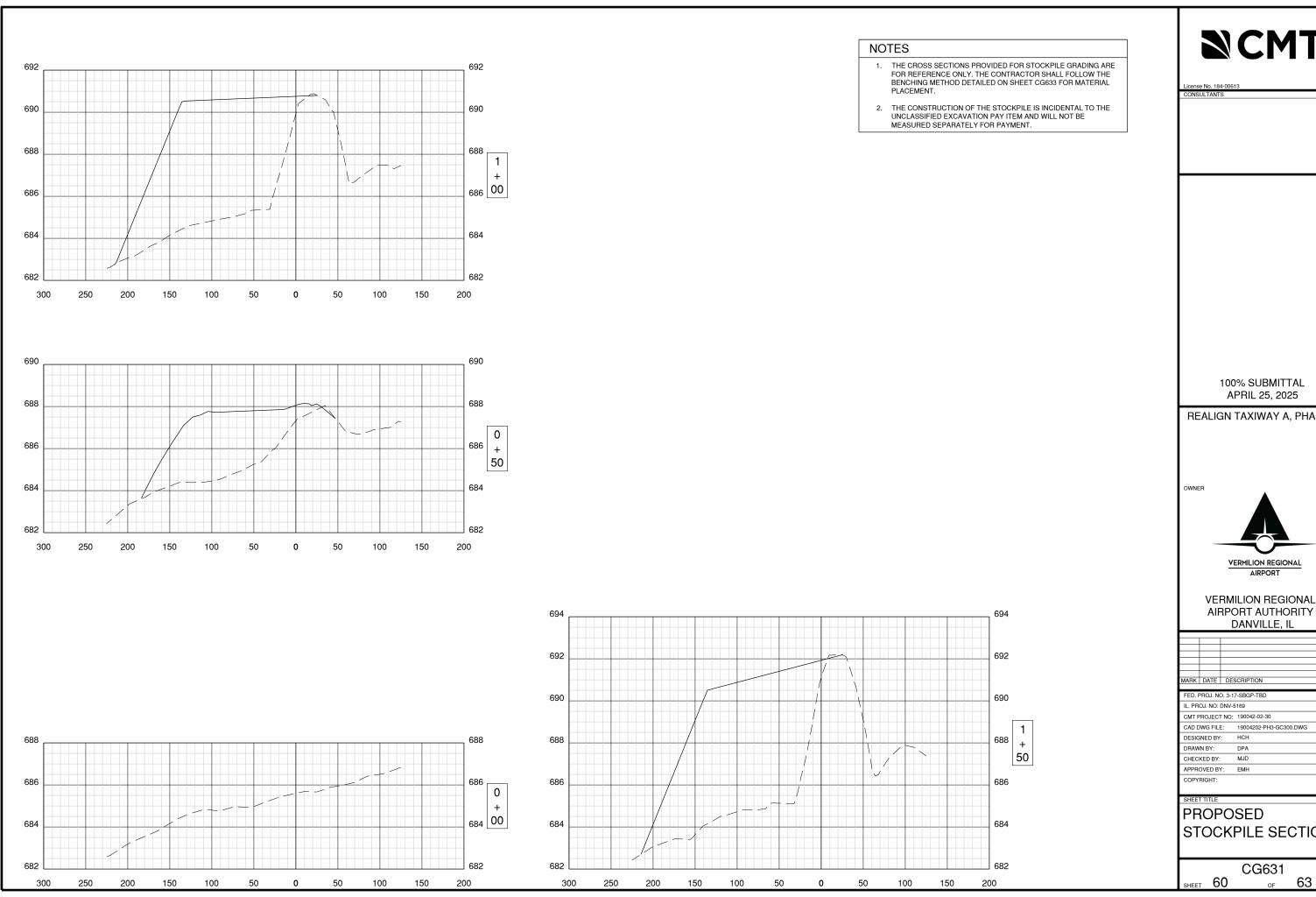
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PROPOSED TURF
RUNWAY SECTION 4

CG614 SHEET 58 OF 63





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MARK DATE DESCRIPTION

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CAD DWG FILE: 19004202-PH3-GC300.DWG

APPROVED BY: EMH

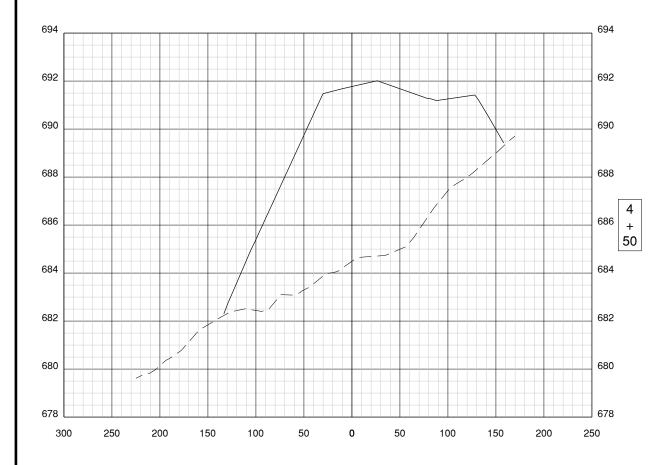
STOCKPILE SECTION 1

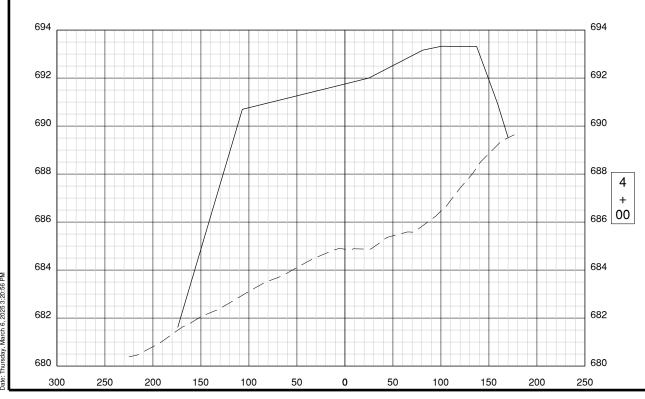
CG631 63

NCMT NOTES 1. THE CROSS SECTIONS PROVIDED FOR STOCKPILE GRADING ARE FOR REFERENCE ONLY. THE CONTRACTOR SHALL FOLLOW THE BENCHING METHOD DETAILED ON SHEET CG633 FOR MATERIAL 2. THE CONSTRUCTION OF THE STOCKPILE IS INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT. 100% SUBMITTAL APRIL 25, 2025 ₆₈₆ 50 ₆₈₆ 50 REALIGN TAXIWAY A, PHASE 3 OWNER VERMILION REGIONAL AIRPORT **VERMILION REGIONAL** AIRPORT AUTHORITY DANVILLE, IL MARK DATE DESCRIPTION FED. PROJ. NO. 3-17-SBGP-TBD IL. PROJ. NO: DNV-5169 CMT PROJECT NO: 190042-02-30 CAD DWG FILE: 19004202-PH3-GC300.DWG DESIGNED BY: DRAWN BY: CHECKED BY: MJD APPROVED BY: EMH COPYRIGHT: PROPOSED STOCKPILE SECTION CG632 SHEET **61**

NOTES

- THE CROSS SECTIONS PROVIDED FOR STOCKPILE GRADING ARE FOR REFERENCE ONLY. THE CONTRACTOR SHALL FOLLOW THE BENCHING METHOD DETAILED ON SHEET CG633 FOR MATERIAL PLACEMENT.
- THE CONSTRUCTION OF THE STOCKPILE IS INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.







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REALIGN TAXIWAY A, PHASE 3

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VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

MARK DATE DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-GC300.DWG
DESIGNED BY: HCH

DESIGNED BY: HCH
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SHEET TITLE

PROPOSED
STOCKPILE SECTION

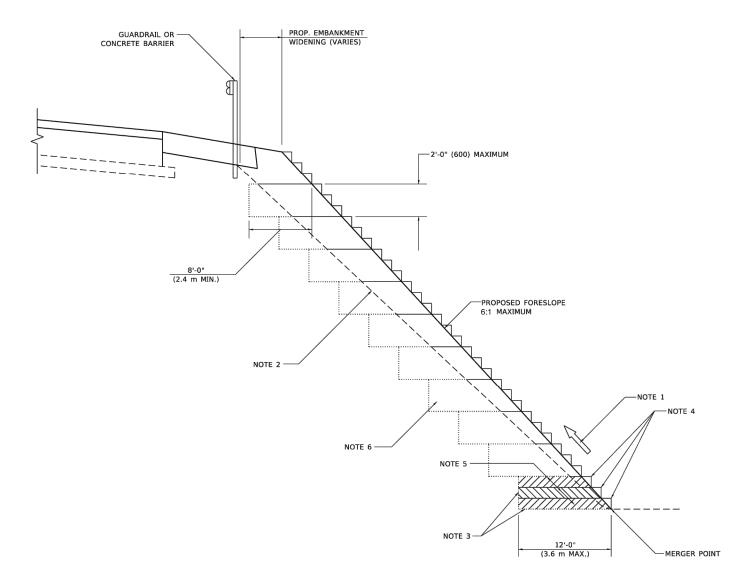
CG633

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License No. 184-00613

CONSULTANTS



TYPICAL BENCHING DETAIL FOR EMBANKMENT

GENERAL NOTES

- CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- 2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- 3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- 4. TRIM TO FINAL SLOPE.
- EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.

BASIS OF PAYMENT

 EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISED - K. SMITH 11-18-22 USER NAME = Lawrence.DeManche DESIGNED -BENCHING DETAIL COUNTY SECTION CADD STATE OF ILLINOIS FOR EMBANKMENT WIDENING S.E.B. **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. BD-51 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

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VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, IL

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CHECKED BY:			MJD
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SHEET TITLE

PROPOSED STOCKPILE DETAIL

GC634 SHEET 63 OF 63