

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	1
ILLINOIS		CONTRACT NO. 62V56		

* 50 + 3 = 53 TOTAL SHEETS

D-91-034-24

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**THIS IMPROVEMENT IS LOCATED IN THE VILLAGES OF
SUMMIT, MCCOOK AND LYONS**

ROADWAY DATA
SPEED LIMIT: 30MPH - 50 MPH
ADT: 40,100 (2023)

**PROPOSED
HIGHWAY PLANS**

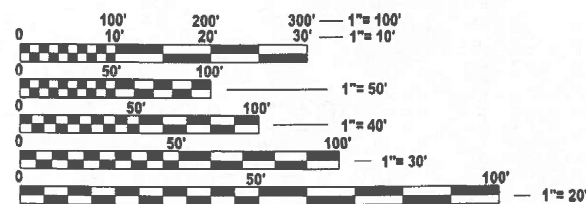
**FAP ROUTE 372: IL. ROUTE 171 / ARCHER AVE
SOUTH OF OGDEN AVE. TO WEST OF HARLEM AVE.
SECTION FAP 0372 23 SMART
PROJECT NHPP T687(197)
STANDARD AND DESIGNED OVERLAY, ADA IMPROVEMENT
COOK COUNTY**

C-91-042-24

START OF PROJECT:
STA 26+74

RESURFACING OMISSIONS:

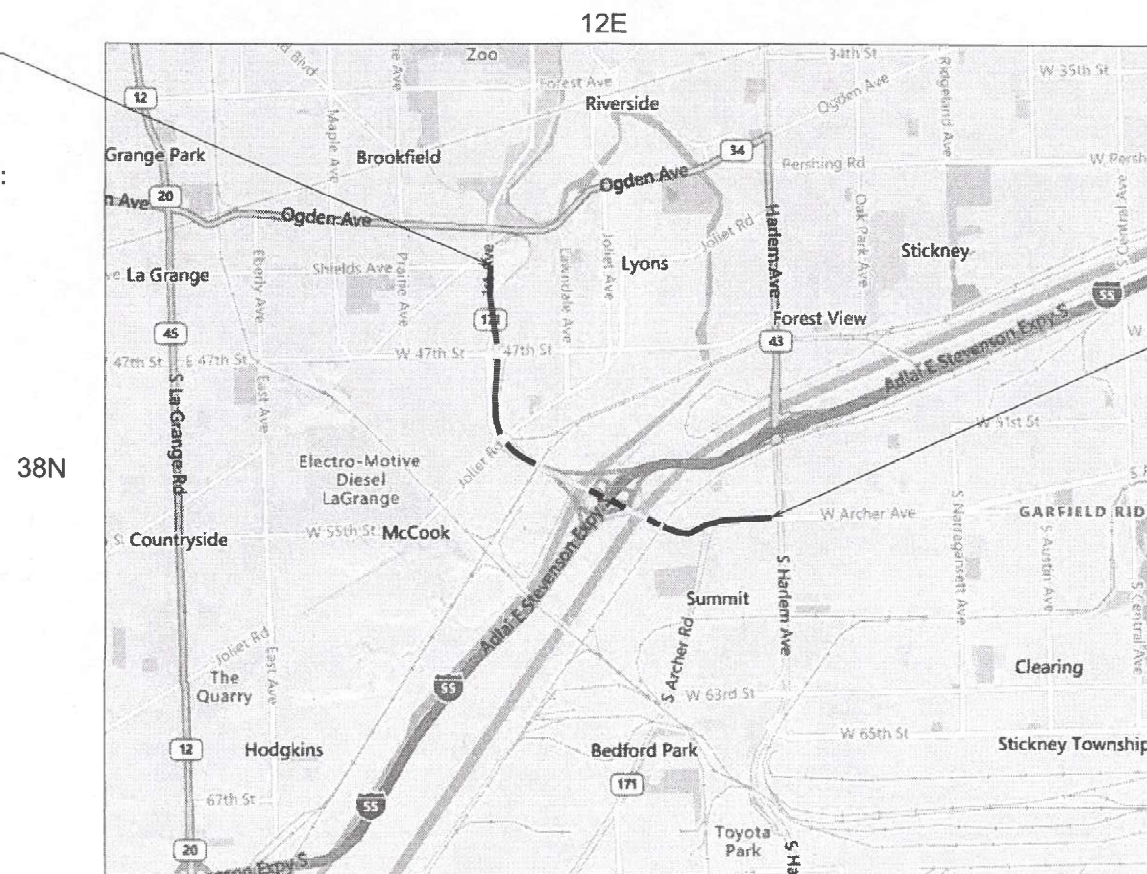
STA 51+62 TO STA 53+98
STA 76+95 TO STA 81+34
STA 90+33 TO STA 109+40
STA 114+60 TO STA 117+45
STA 121+36 TO STA 130+21
STA 133+52 TO STA 136+00



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705 - 4255
PROJECT MANAGER: VESELIN VELICHKOV

CONTRACT NO. 62V56

LYONS TOWNSHIP

GROSS LENGTH = 14264 FT. = 2.70 MILES
NET LENGTH = 9964 FT. = 1.88 MILES



LOCATION OF SECTION INDICATED THUS: -

END OF PROJECT:
STA 169+38

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED

Jan 29th 2025
Joseph J. R.

REGIONAL ENGINEER

March 21, 2025

ENGINEER OF DESIGN AND ENVIRONMENT

March 21, 2025

DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

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INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.

DESCRIPTION

STANDARD NO.

DESCRIPTION

3. THE RESIDENT ENGINEER SHALL CONTACT ABDULLA ALI AREA TRAFFIC FIELD ENGINEER, AT ABDULLA.ALI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

4. ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

5. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGHTS.

7. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

8. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

9. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

10. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

11. SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

12. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

13. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.

14. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR FOR EXPRESSWAYS AT (847) 705-4151 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

15. THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 1 WEEK IN ADVANCE OF BEGINNING FORESTRY WORK, WEED SPRAYING AND SEEDING.

16. LANDSCAPING WORK SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

17. PAVING OPERATIONS SHALL STOP 1 FOOT AWAY FROM ANY EXISTING GUARDRAIL AND THE CONTRACTOR SHALL NOT DAMAGE ANY EXISTING GUARDRAIL DURING PAVING OPERATIONS.

18. WHEN SEVERELY DETERIORATED SECTIONS OF STABILIZED HMA MEDIAN SURFACE ARE ENCOUNTERED DURING THE MEDIAN MILLING AND RESURFACING, THE SECTIONS SHALL BE PATCHED WITH CLASS D PATCH, 10 INCH. THE TYPE AND LOCATION SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.

19. QUANTITIES HAVE BEEN PROVIDED TO CLEAN ALL BRIDGE APPROACH SLABS WITHIN THE LIMITS OF THIS CONTRACT.

20. PROPOSED PAVEMENT PATCHING AROUND ADA RAMPS TO CORRECT SLOPE CORRECTION TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER AND TO BE PAID UNDER HMA SURFACE REMOVAL AND BY INCREASING HMA BINDER AND SURFACE COURSE ITEMS. THE 4 FOOT WIDTH SHOWN IN ADA DETAILS IS NOMINAL AND CAN BE ADJUSTED IN THE FIELD AS DIRECTED BY THE RESIDENT ENGINEER.

GENERAL NOTES

1. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

2. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
IL, ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	2
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SAFETY	ROADWAY			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0005			
	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	5	5					
	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	10	10					
	20200100	EARTH EXCAVATION	CU YD	505	505					
	20800150	TRENCH BACKFILL	CU YD	38	38					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1223	1223					
	25003210	INTERSEEDING, CLASS 2A	ACRE	2	2					
	25200110	SODDING, SALT TOLERANT	SQ YD	78	78					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	78560	78560					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	64298	64298					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	175	175					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1084	1084					
	40600990	TEMPORARY RAMP	SQ YD	40	40					
	40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	1473	1473					
	40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, MIX "D", N70	TON	1247	1247					
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	4342	4342					
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	36	36					
	40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	12048	12048					
	42001300	PROTECTIVE COAT	SQ YD	32	32					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3518		3518				
	42400800	DETECTABLE WARNINGS	SQ FT	295		295				
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	325	325					
* = SPECIALTY ITEM										

SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SAFETY	ROADWAY			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0005			
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	105249	105249					
	44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	11135	11135					
	44000800	SIDEWALK REMOVAL	SQ FT	4311		4311				
	44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	6576	6576					
	44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	2328	2328					
	44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	1746	1746					
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	1746	1746					
	550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	38	38					
	56109210	WATER VALVES TO BE ADJUSTED	EACH	1	1					
	60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	1	1					
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4					
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2					
	60255500	MANHOLES TO BE ADJUSTED	EACH	2	2					
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					
	64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	5300	5300					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	505	505					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2					
	67100100	MOBILIZATION	L SUM	1	1					

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
								372	FAP 0372 23 SMART	COOK	50	3
								CONTRACT NO. 62V56				
								ILLINOIS FED. AID PROJECT				
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				DRAWN -	REVISED -					SHEET 1 OF 3 SHEETS		
				CHECKED -	REVISED -					STA. TO STA.		
PLOT DATE = 1/29/2025				DATE -	REVISED -							

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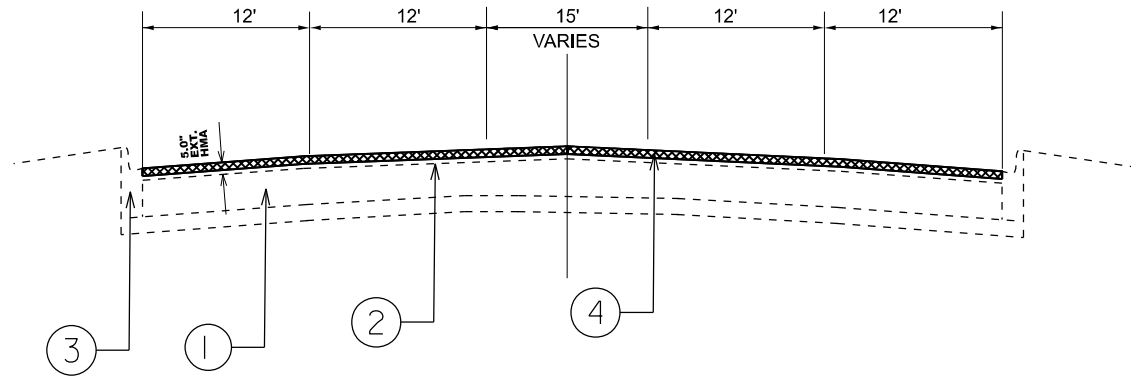
SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SAFETY	ROADWAY			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0005			
	70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1					
	70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2	2					
	70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	43983	43983					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	11729	11729					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	367	367					
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	41206	41206					
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	1895	1895					
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	11375	11375					
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	7905	7905					
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	493	493					
*	72400205	REMOVE AND RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4					
*	72400210	REMOVE AND RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	3	3					
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	367	367					
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	41206	41206					
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1895	1895					
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	11735	11735					
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	7904	7904					
* = SPECIALTY ITEM										

SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SAFETY	ROADWAY			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0005			
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	493	493					
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	873	873					
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	698	698					
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	34697	34697					
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	45		45				
	84200804	REMOVAL OF POLE FOUNDATION	EACH	1		1				
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1		1				
*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2073		2073				
*	88500100	INDUCTIVE LOOP DETECTOR	EACH	2		2				
*	88600100	DETECTOR LOOP, TYPE I	FOOT	951		951				
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2073		2073				
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1				
	K0026700	TREE CARE	EACH	50	50					
	K0029624	WEED CONTROL, TEASEL	GALLON	5	5					
	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
	X0320090	PARK BENCH REMOVAL AND RELCOATION	EACH	1		1				
	X0325222	WEED CONTROL, BASAL TREATMENT	GALLON	50	50					
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	100	100					
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	873	873					
	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1000			1000			
	X5538200	STORM SEWERS TO BE CLEANED 24"	FOOT	150	150					
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	24	24					
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12					

	USER NAME = connor.mullane	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						372	FAP 0372 23 SMART	COOK	50	4
		CHECKED -	REVISED -										
	PLOT DATE = 1/29/2025	DATE -	REVISED -						CONTRACT NO. 62V56				
									ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET 2	OF 3	SHEETS	STA.	TO STA.			

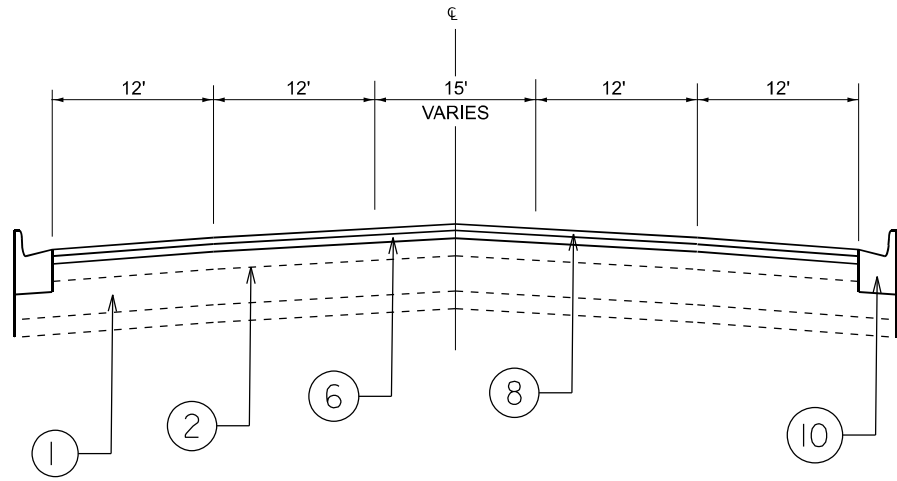
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EXISTING TYPICAL SECTION

FROM STA 26+74 TO STA 37+74



PROPOSED TYPICAL SECTION

FROM STA 26+74 TO STA 37+74

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON HMA BC IL-9.5 N70

LEGEND

- ① EXISTING PCC BASE COURSE: 9"
- ② EXISTING HMA PAVEMENT
- ③ EXISTING CURB AND GUTTER
- ④ PROPOSED HMA SURFACE REMOVAL, 3.75"
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2.5"
- ⑥ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑦ PROPOSED POLYMERIZED HOT MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- ⑧ PROPOSED POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX 'F', N80, 1.75"
- ⑨ PROPOSED SHOULDER RUMBLE STRIP 8"
- ⑩ PROPOSED CURB AND GUTTER (LOCATIONS DETERMINED IN FIELD BY ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"	4.0% @ 70 GYR	QCP
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"	3.5% @ 50 GYR	QCP
	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1.75"	3.5% @ 80 GYR	QCP
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0)	4.0% @ 70 GYR	QC/QA
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0)	4.0% @ 70 GYR	QC/QA
MEDIAN SURFACE	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2.0"	4.0% @ 50 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)			

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

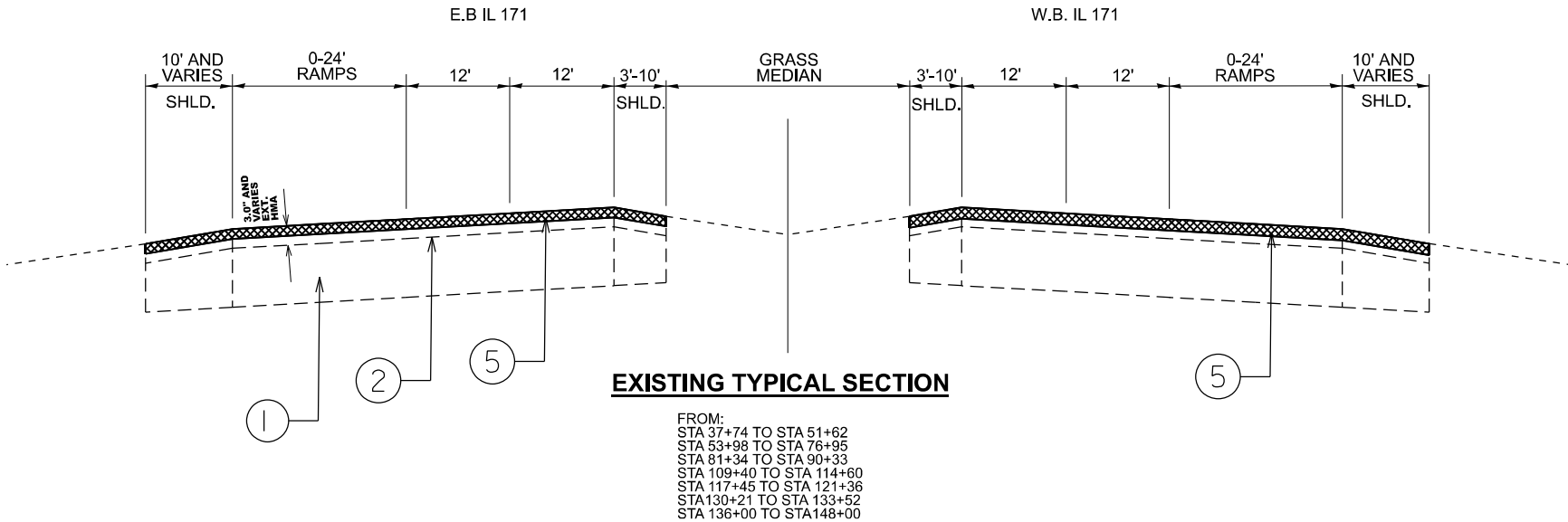
NOTE 3: THE CONTRACTOR SHALL PATCH FIRST THEN MILL.

	USER NAME = connor.mullane	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL, ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					372	FAP 0372 23 SMART	COOK	50	6
		CHECKED -	REVISED -					CONTRACT NO. 62V56				
	PLOT DATE = 2/25/2025	DATE -	REVISED -		SCALE:	SHEET 1	OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

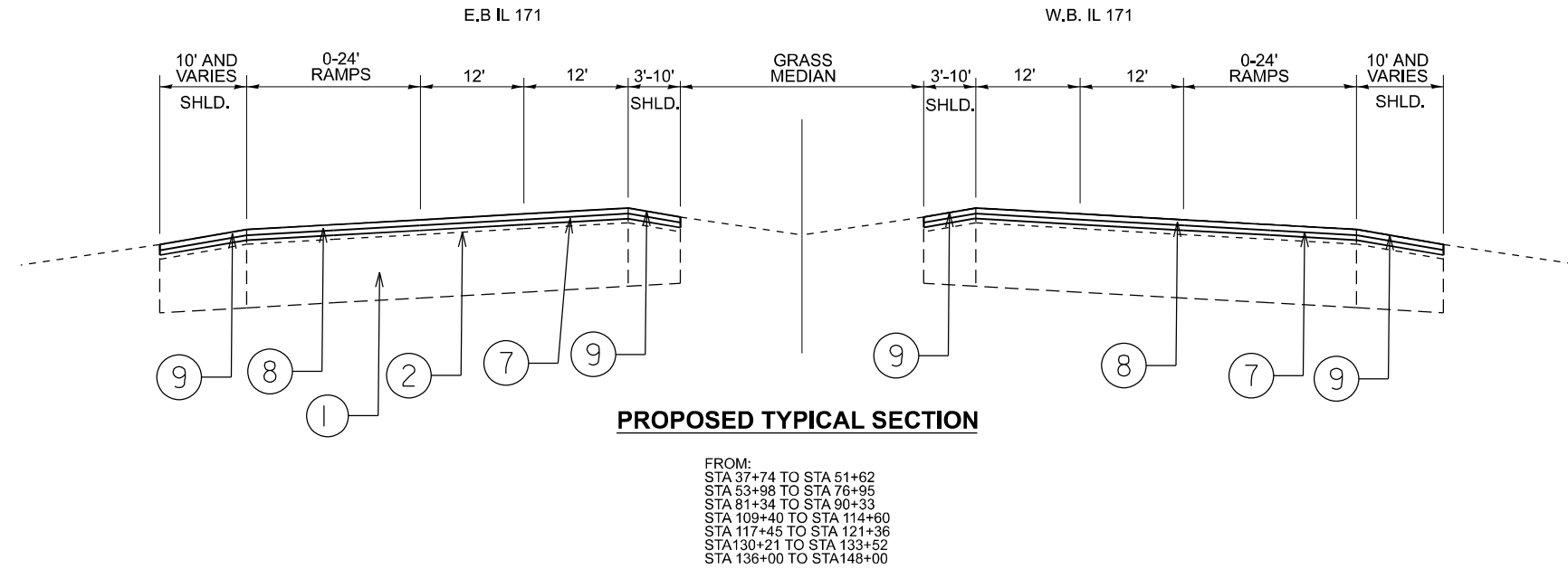
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LEGEND

- ① EXISTING PCC BASE COURSE: 9"
- ② EXISTING HMA PAVEMENT
- ③ EXISTING CURB AND GUTTER
- ④ PROPOSED HMA SURFACE REMOVAL, 3.75"
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2.5"
- ⑥ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑦ PROPOSED POLYMERIZED HOT MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- ⑧ PROPOSED POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX F, N80; 1.75"
- ⑨ PROPOSED SHOULDER RUMBLE STRIP 8"
- ⑩ PROPOSED CURB AND GUTTER (LOCATIONS DETERMINED IN FIELD BY ENGINEER)



PROPOSED RUMBLE STRIP LOCATIONS:
STA 136+00 TO STA 147+00



THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON HMA BC IL-9.5 N70

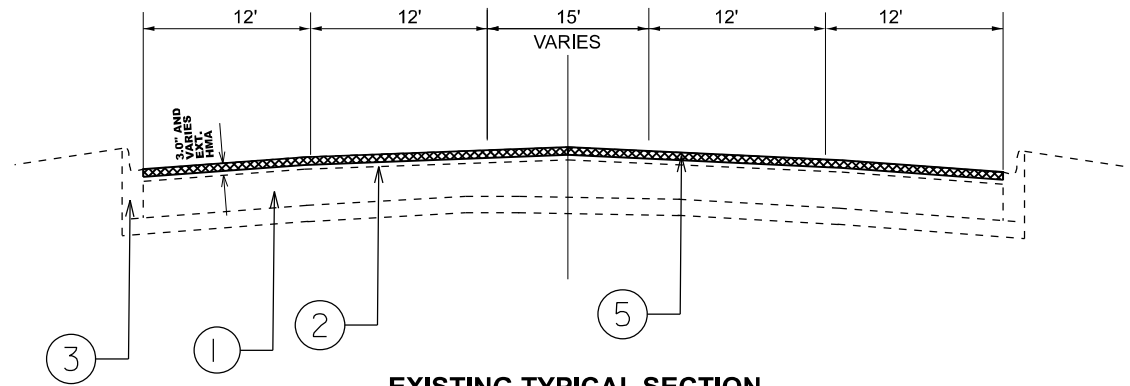
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 2/25/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS			
IL, ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)			
SCALE:	SHEET 2	OF 3 SHEETS	STA. TO STA.

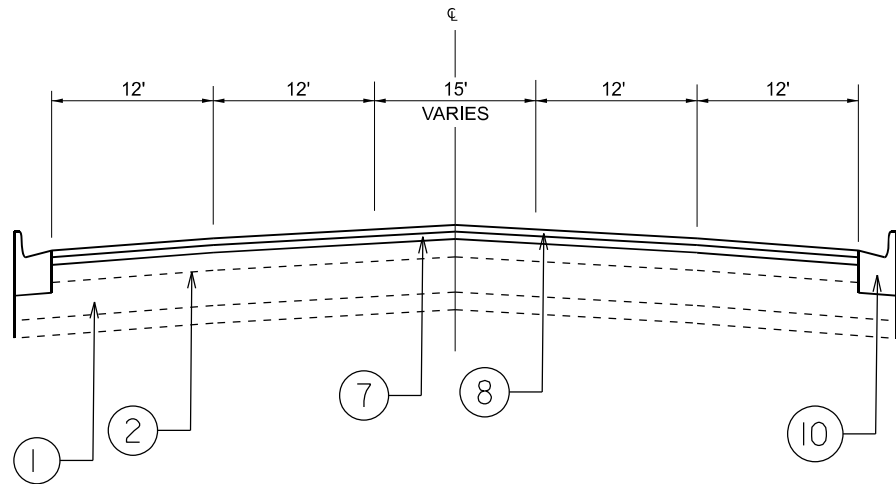
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	7
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

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EXISTING TYPICAL SECTION

FROM STA 148+00 TO STA 169+38



PROPOSED TYPICAL SECTION

FROM STA 148+00 TO STA 169+38

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON HMA BC IL-9.5 N70

LEGEND

- ① EXISTING PCC BASE COURSE: 9"
- ② EXISTING HMA PAVEMENT
- ③ EXISTING CURB AND GUTTER
- ④ PROPOSED HMA SURFACE REMOVAL, 3.75"
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2.5"
- ⑥ PROPOSED HOT MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑦ PROPOSED POLYMERIZED HOT MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- ⑧ PROPOSED POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX F, N80; 1.75"
- ⑨ PROPOSED SHOULDER RUMBLE STRIP 8"
- ⑩ PROPOSED CURB AND GUTTER (LOCATIONS DETERMINED IN FIELD BY ENGINEER)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL, ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)

SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	8
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

PROPOSED ROADWAY WORK

A

PROP. HMA SURF REM 2 1/2"
PROP. POLY. HMA BINDER COURSE IL-4.75 N50, 3/4"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

B

PROP. HMA SURF REM 3 3/4"
PROP. HMA BINDER COURSE IL-9.5, N70, 2"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

C

PROP. HMA SURF REM BUTT JT

D

PROP. PCC SIDEWALK CURB RAMP IMPROVEMENT
SEE DETAIL SHEET

PROPOSED PAVEMENT STRIPING LEGEND

THERMOPLASTIC

1

LETTERS & SYMBOLS, SOLID, WHITE (TYP.)

2

LINE 4" 10' DASH - 30' SKIP, WHITE (TYP.)

3

LINE 4" DOUBLE SOLID YELLOW, 2 @ 11" C-C (TYP.)

4

LINE 4" SOLID WHITE (TYP.)

5

LINE 4" SOLID YELLOW (TYP.)

6

LINE 6" SOLID WHITE, TURN LANE (TYP.)

7

LINE 6" SKIP-DASH WHITE, 2' DASH - 6' SKIP (TYP.)

8

LINE 8" SOLID WHITE (TYP.)

9

LINE 8" SKIP-DASH WHITE, 3' DASH - 9' SKIP (TYP.)

10

LINE 12" SOLID WHITE, DIAGONALS (TYP.)

11

LINE 12" SOLID YELLOW, DIAGONALS (TYP.)

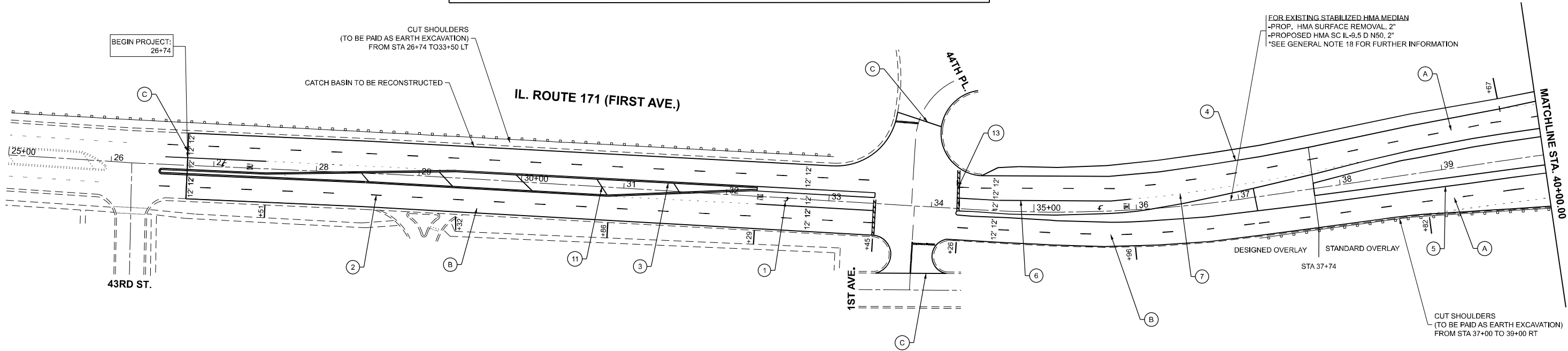
12

LINE 12" SOLID WHITE, CROSSWALK (TYP.)

13

LINE 24" SOLID WHITE STOP BAR (TYP.)

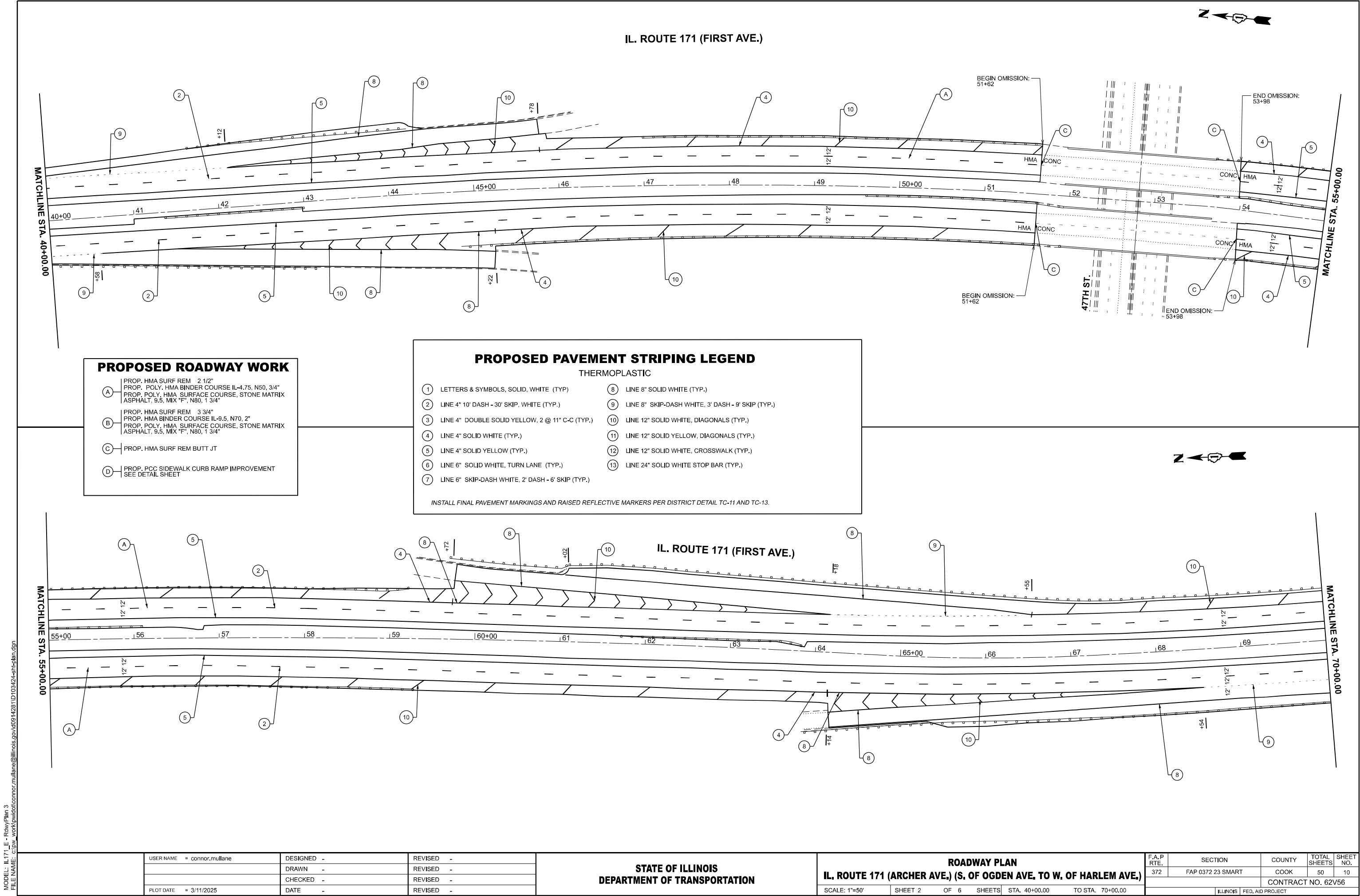
INSTALL FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS PER DISTRICT DETAIL TC-11 AND TC-13.

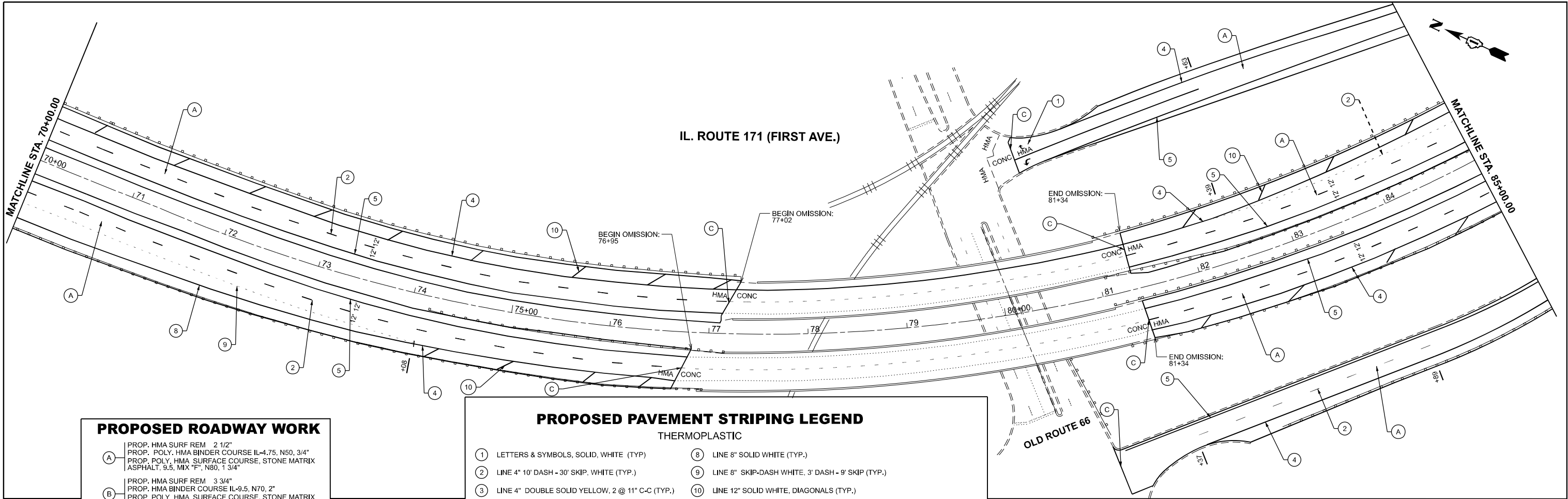


MODEL: IL171_E - RdwyPlan 1
FILE NAME: c:\pwworking\tdotconnor.mullane@illinois.gov\d0914281\ID103424-shh-plan.dgn

	USER NAME = connor.mullane		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN -	REVISED -		IL. ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)			372	FAP 0372 23 SMART	COOK	50	9
			CHECKED -	REVISED -					CONTRACT NO. 62V56				
	PLOT DATE = 3/11/2025		DATE -	REVISED -		SCALE:	SHEET 1	OF 6 SHEETS	STA. 10+00.00	TO STA. 40+00.00	ILLINOIS FED. AID PROJECT		

MODEL: IL171_E - RdwyPlan 3
FILE NAME: c:\pwworking\connor.mullane@illinois.gov\14281\103424-eh-plan.dgn





PROPOSED ROADWAY WORK

(A) PROP. HMA SURF REM 2 1/2"
PROP. POLY. HMA BINDER COURSE IL-4.75, N50, 3/4"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

(B) PROP. HMA SURF REM 3 3/4"
PROP. HMA BINDER COURSE IL-9.5, N70, 2"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"

(C) PROP. HMA SURF REM BUTT JT

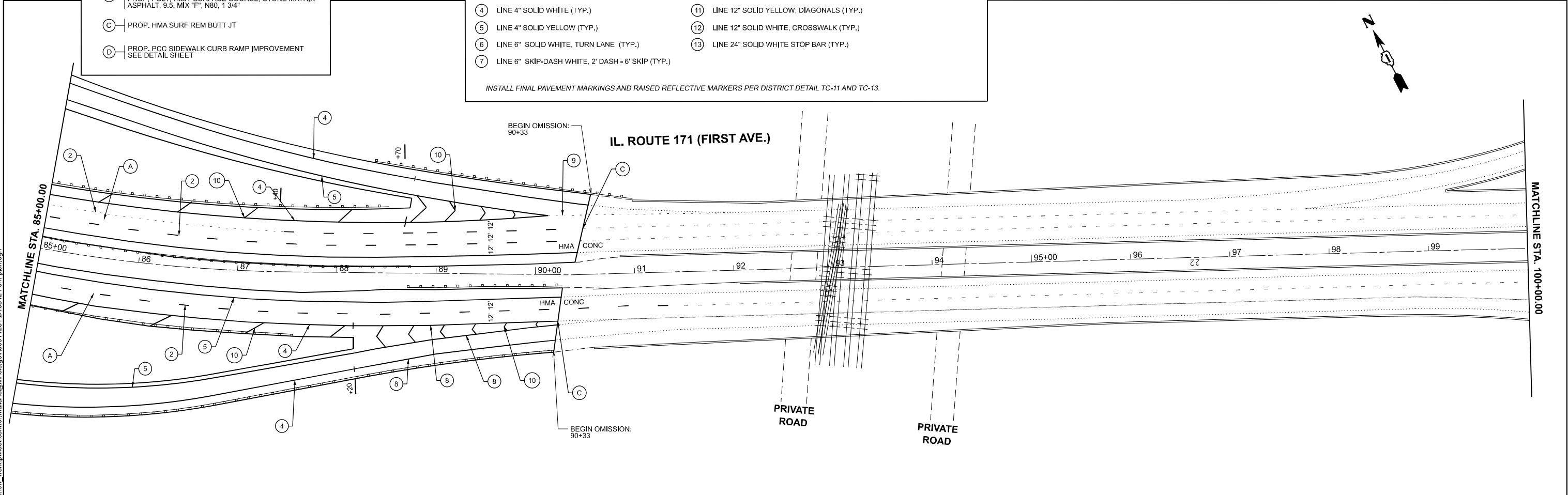
(D) PROP. PCC SIDEWALK CURB RAMP IMPROVEMENT
SEE DETAIL SHEET

PROPOSED PAVEMENT STRIPING LEGEND

THERMOPLASTIC

(1) LETTERS & SYMBOLS, SOLID, WHITE (TYP.)	(8) LINE 8" SOLID WHITE (TYP.)
(2) LINE 4" 10' DASH - 30' SKIP, WHITE (TYP.)	(9) LINE 8" SKIP-DASH WHITE, 3' DASH - 9' SKIP (TYP.)
(3) LINE 4" DOUBLE SOLID YELLOW, 2 @ 11" C-C (TYP.)	(10) LINE 12" SOLID WHITE, DIAGONALS (TYP.)
(4) LINE 4" SOLID WHITE (TYP.)	(11) LINE 12" SOLID YELLOW, DIAGONALS (TYP.)
(5) LINE 4" SOLID YELLOW (TYP.)	(12) LINE 12" SOLID WHITE, CROSSWALK (TYP.)
(6) LINE 6" SOLID WHITE, TURN LANE (TYP.)	(13) LINE 24" SOLID WHITE STOP BAR (TYP.)
(7) LINE 6" SKIP-DASH WHITE, 2' DASH - 6' SKIP (TYP.)	

INSTALL FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS PER DISTRICT DETAIL TC-11 AND TC-13.



MODEL: IL171_E - RdwyPlan 5
FILE NAME: c:\p\work\p\work\connor.mullane@illinois.gov\0914281\103424-eh-plan.dgn

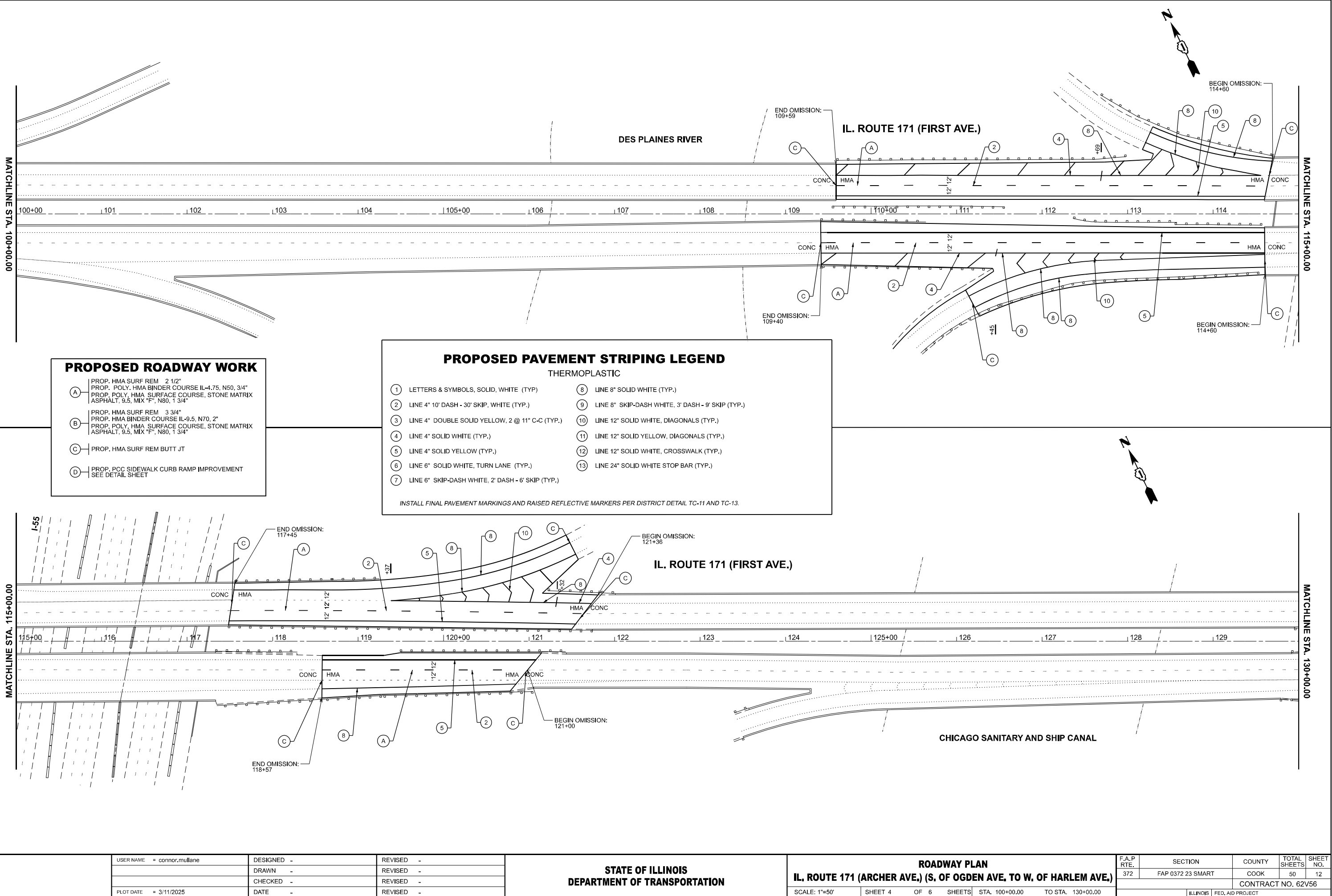
USER NAME = connor.mullane	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
	DATE -	REVISED -
PLOT DATE = 3/11/2025		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

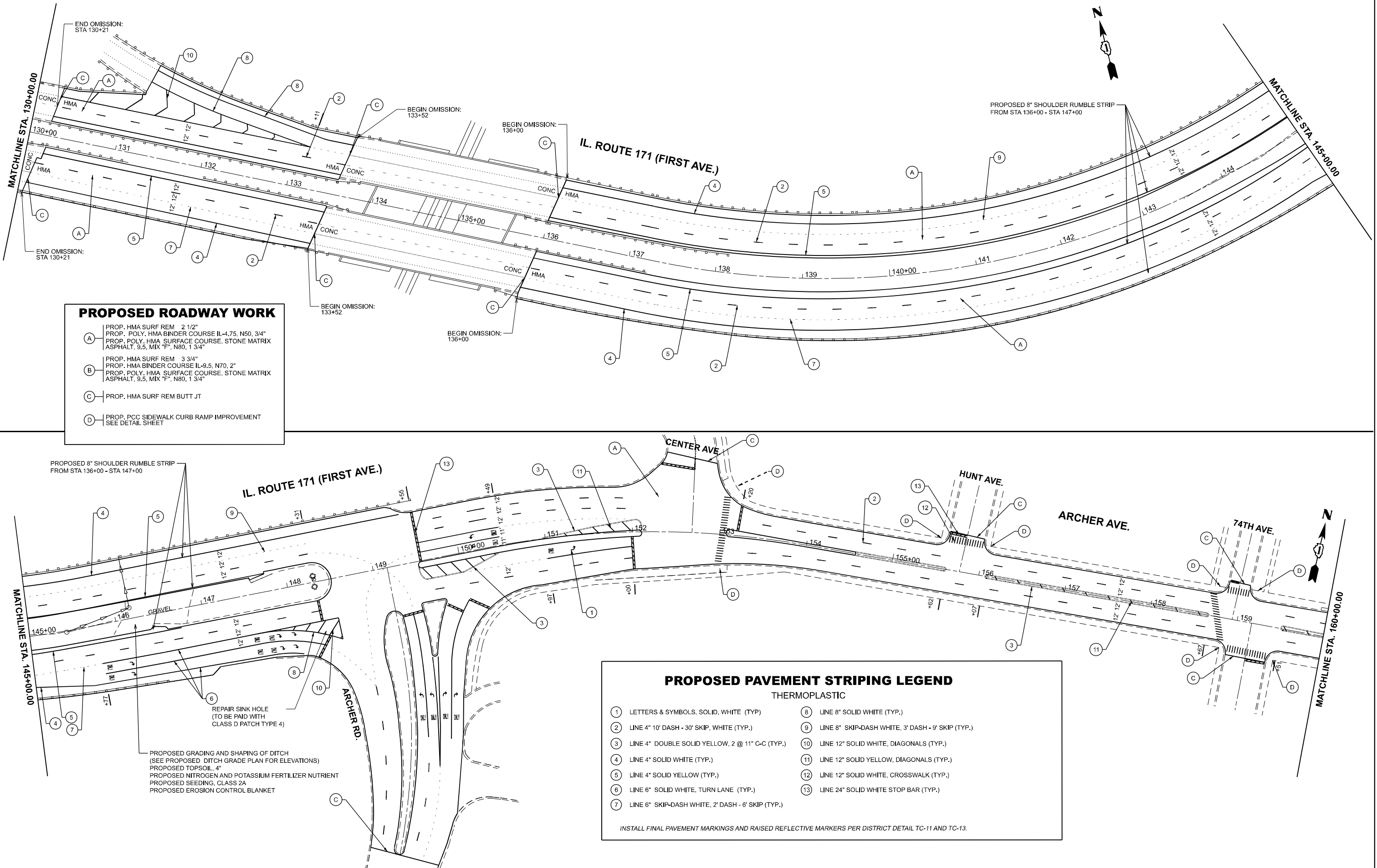
ROADWAY PLAN			
IL. ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)			
SCALE: 1"=50'	SHEET 3	OF 6 SHEETS	STA. 70+00.00 TO STA. 100+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	11
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

MODEL: IL171_E - RdwyPlan 7
FILE NAME: c:\p\work\p\work\connor.mullane@illinois.gov\0914281\03424-sh-plan.dgn



MODEL: IL171_E - RdwyPlan 9
FILE NAME: c:\pwwork\pwwork\connor.mullane\14281\103424-sh-plan.dgn



PLOT DATE	USER NAME	DESIGNED	REVISED
	= connor.mullane	-	-
		DRAWN	REVISED
		-	-
DATE		CHECKED	REVISED
		-	-
		DATE	REVISED
		-	-

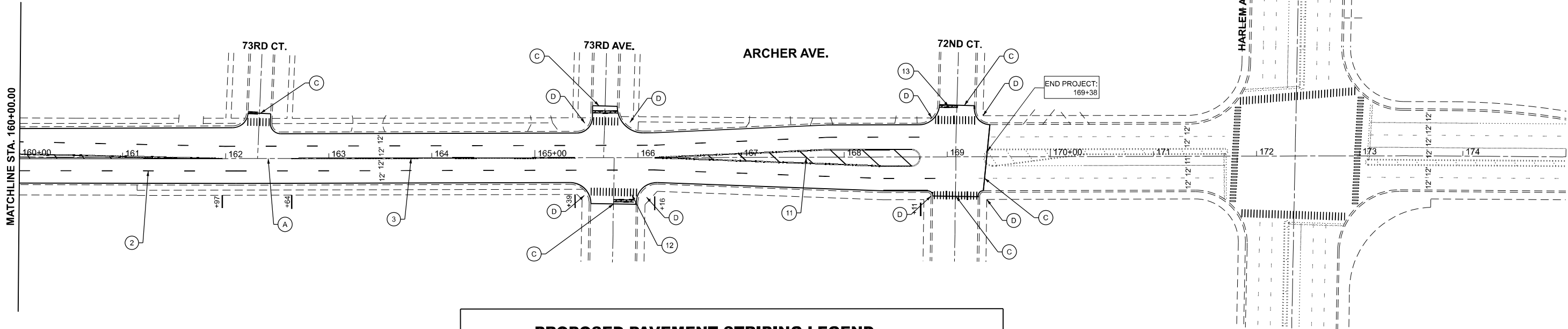
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL. ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)

SCALE: 1"=50' SHEET 5 OF 6 SHEETS STA. 130+00.00 TO STA. 160+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	13
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

MODEL: IL171_E - RdwyPlan 11
FILE NAME: c:\pwwork\pwwork\connor.mullane@illinois.gov\0914281\103424-rh-plan.dgn



PROPOSED ROADWAY WORK

- (A) PROP. HMA SURF REM 2 1/2"
PROP. POLY. HMA BINDER COURSE IL-4.75, N50, 3/4"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"
- (B) PROP. HMA SURF REM 3 3/4"
PROP. HMA BINDER COURSE IL-9.5, N70, 2"
PROP. POLY. HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"
- (C) PROP. HMA SURF REM BUTT JT
- (D) PROP. PCC SIDEWALK CURB RAMP IMPROVEMENT
SEE DETAIL SHEET

PROPOSED PAVEMENT STRIPING LEGEND

THERMOPLASTIC

- (1) LETTERS & SYMBOLS, SOLID, WHITE (TYP.)
- (2) LINE 4" 10' DASH - 30' SKIP, WHITE (TYP.)
- (3) LINE 4" DOUBLE SOLID YELLOW, 2 @ 11" C-C (TYP.)
- (4) LINE 4" SOLID WHITE (TYP.)
- (5) LINE 4" SOLID YELLOW (TYP.)
- (6) LINE 6" SOLID WHITE, TURN LANE (TYP.)
- (7) LINE 6" SKIP-DASH WHITE, 2' DASH - 6' SKIP (TYP.)
- (8) LINE 8" SOLID WHITE (TYP.)
- (9) LINE 8" SKIP-DASH WHITE, 3' DASH - 9' SKIP (TYP.)
- (10) LINE 12" SOLID WHITE, DIAGONALS (TYP.)
- (11) LINE 12" SOLID YELLOW, DIAGONALS (TYP.)
- (12) LINE 12" SOLID WHITE, CROSSWALK (TYP.)
- (13) LINE 24" SOLID WHITE STOP BAR (TYP.)

INSTALL FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS PER DISTRICT DETAIL TC-11 AND TC-13.

USER NAME	= connor.mullane	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 3/11/2025	DATE	-	REVISED	-

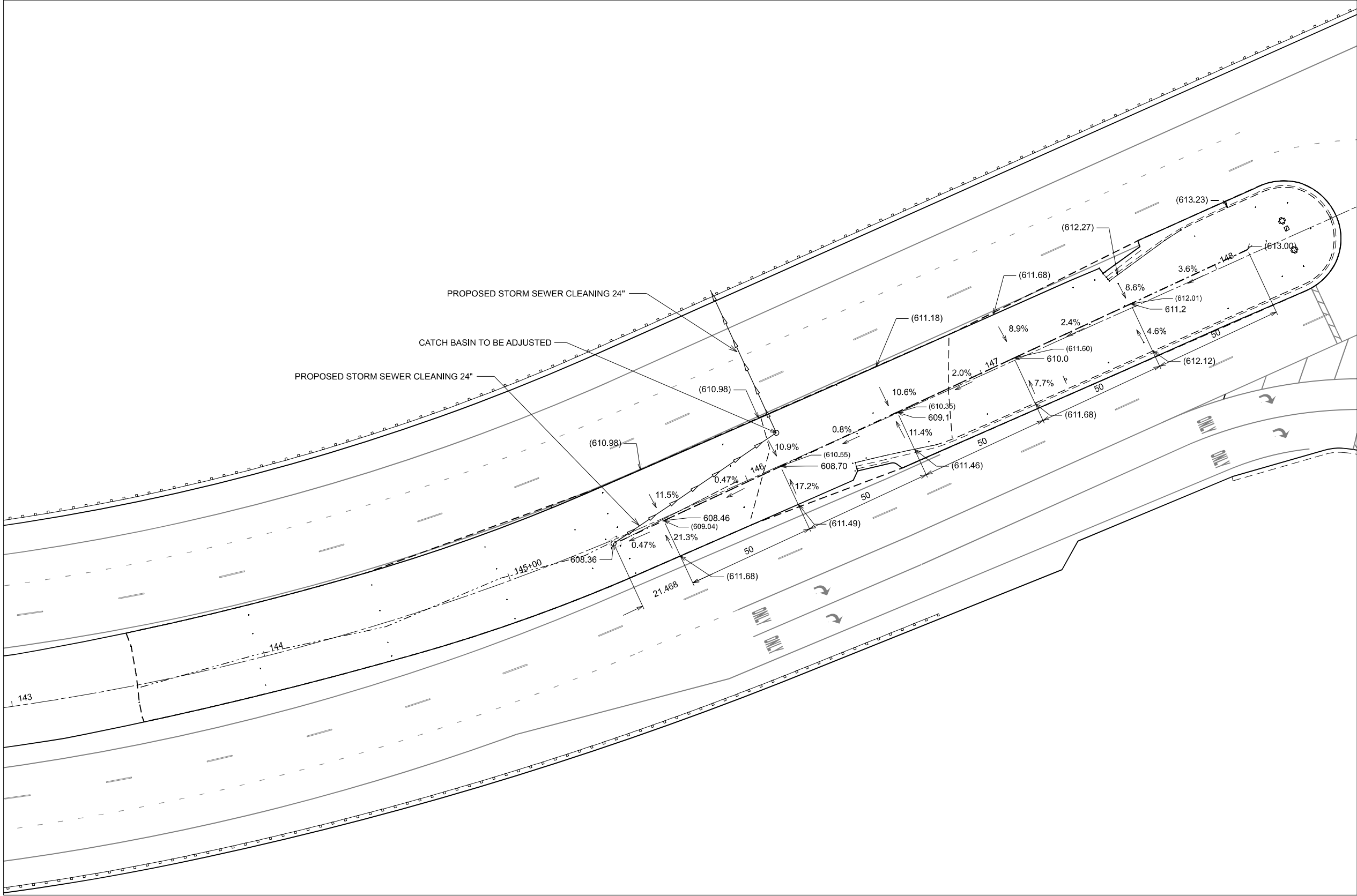
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL, ROUTE 171 (ARCHER AVE.) (S. OF OGDEN AVE. TO W. OF HARLEM AVE.)

SCALE: 1"=50' SHEET 6 OF 6 SHEETS STA. 160+00.00 TO STA. 175+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	14
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

MODEL: Drainage exhibit (Sheet)
FILE NAME: c:\p\work\p\drainage\connor.mullane\103424-eh-exhibits.dgn



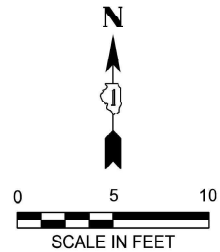
USER NAME = connor.mullane	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 3/11/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED DITCH GRADING PLAN
IL 171 FROM SOUTH OF OGDEN TO WEST OF HARLEM

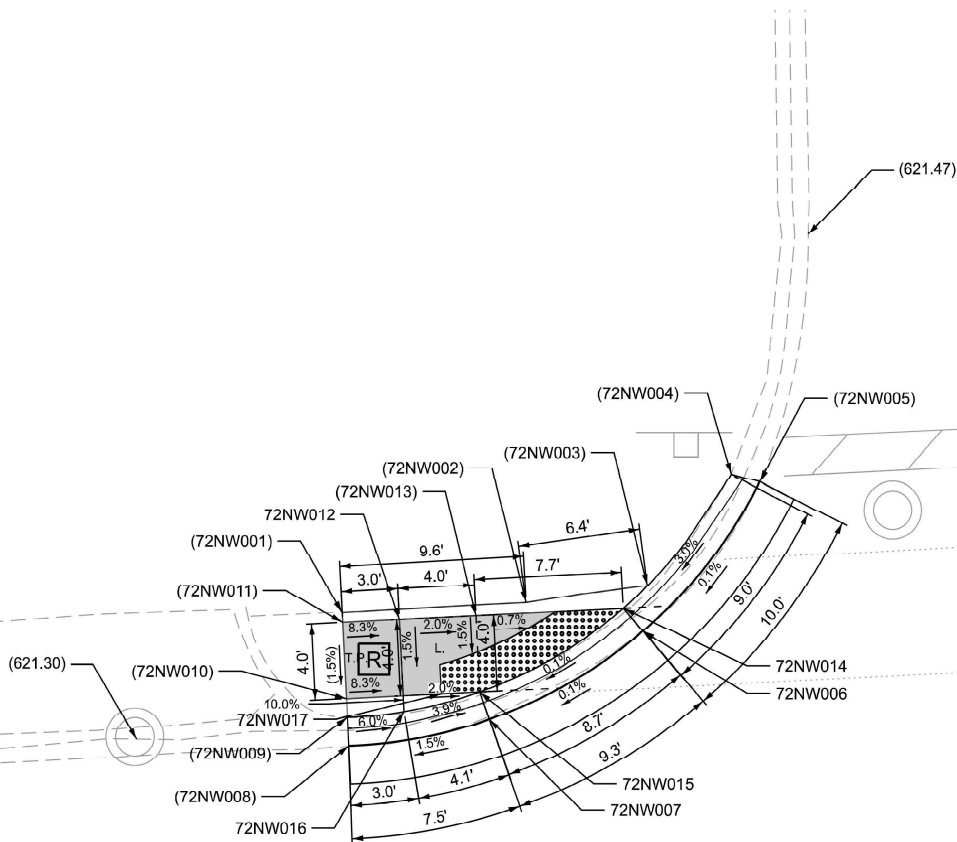
SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	51	14A
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

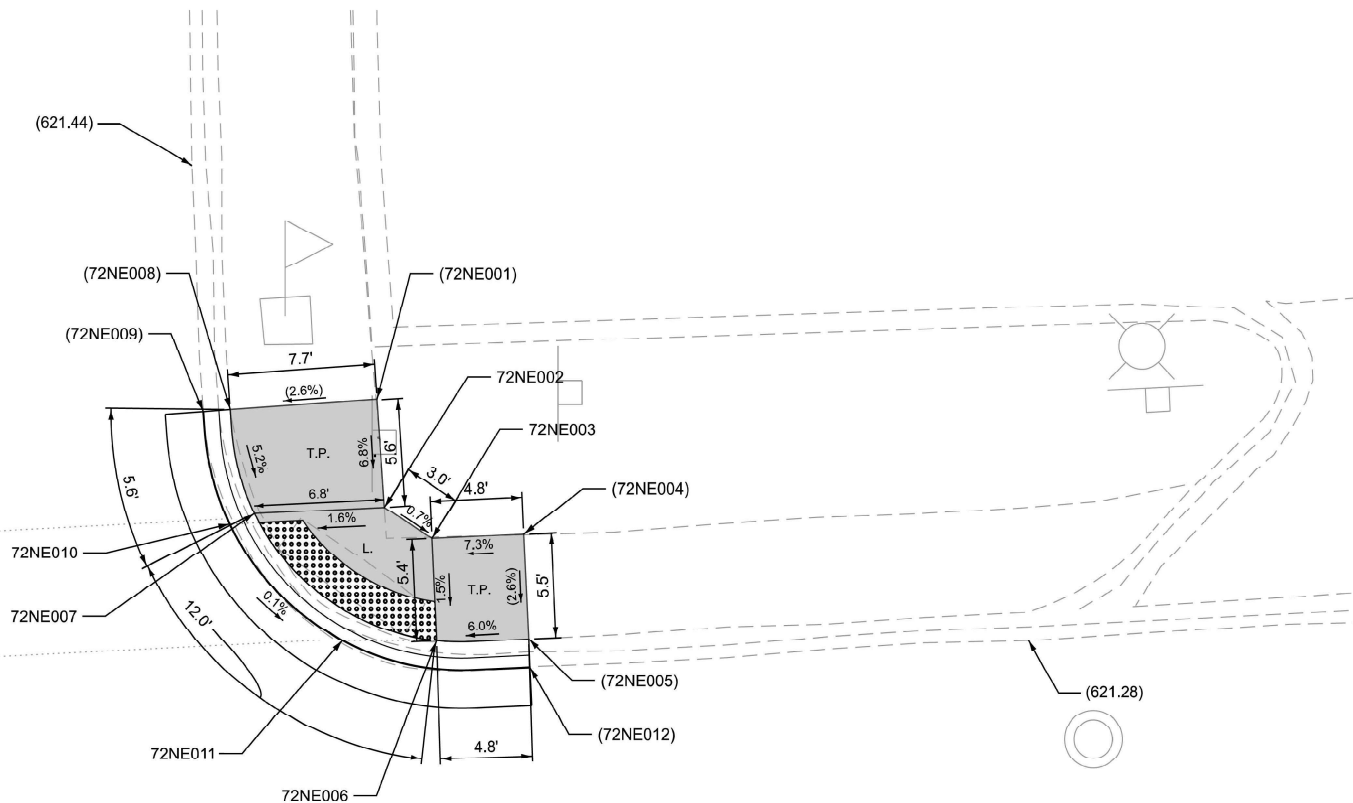


72ND CT.

ARCHER AVE.



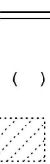
COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
72NW001	621.85	1867233.40	1128901.96
72NW002	621.53	1867233.92	1128911.57
72NW003	621.33	1867234.80	1128917.91
72NW004	621.75	1867240.60	1128922.27
72NW005	621.38	1867240.23	1128923.75
72NW006	621.37	1867232.43	1128917.66
72NW007	621.36	1867227.82	1128909.64
72NW008	621.25	1867226.44	1128902.31
72NW009	621.81	1867227.91	1128902.24
72NW010	621.80	1867228.91	1128902.19
72NW011	621.86	1867232.90	1128901.99
72NW012	621.61	1867233.05	1128904.95
72NW013	621.53	1867233.24	1128908.94
72NW014	621.48	1867233.62	1128916.64
72NW015	621.47	1867229.25	1128909.14
72NW016	621.63	1867228.22	1128905.19
72NW017	621.55	1867229.15	1128907.14



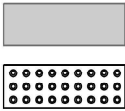
COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
72NE001	621.96	1867244.52	1128970.23
72NE002	621.58	1867238.90	1128970.62
72NE003	621.56	1867237.29	1128973.10
72NE004	621.91	1867237.51	1128977.90
72NE005	621.77	1867232.02	1128978.15
72NE006	621.48	1867231.94	1128973.34
72NE007	621.47	1867238.58	1128963.83
72NE008	621.76	1867243.99	1128962.57
72NE009	621.45	1867243.89	1128961.16
72NE010	621.42	1867238.02	1128962.54
72NE011	621.36	1867231.95	1128968.43
72NE012	621.33	1867230.56	1128978.22

LEGEND

T.P. TRANSITION PANEL
L. LANDING
L.L. LOWER LANDING
U.L. UPPER LANDING



PROPOSED SIDE CURB
EXISTING ELEVATIONS / SLOPE
SODDING, SALT TOLERANT



PROPOSED SIDEWALK
DETECTABLE WARNINGS



REBUILD EXISTING HANDHOLE (SPECIAL)
ADJUST EXISTING VALVE OR STRUCTURE
REMOVAL OF POLE FOUNDATION

REFERENCE BENCHMARK ELEV 621.80
BENCHMARK : CUT CROSS IN CONCRETE WALK AT THE
NE CORNER OF ARCHER AVE. AND 72ND CT.
LOCATION : NORTH EAST CORNER OF ARCHER AVE. AND 72ND CT.

LOCATION : NORTH WEST CORNER OF ARCHER AVE. AND 72ND CT.



USER NAME = connor.mullane
DESIGNED - AS
DRAWN - MB
CHECKED - MB
PLOT DATE = 1/28/2025

DESIGNED - AS
DRAWN - MB
CHECKED - MB
DATE - 1/24/25

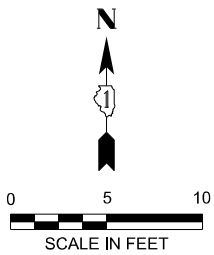
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 72ND CT. NW, NE

SCALE: 1:10 SHEET 1 OF 10 SHEETS STA. TO STA.

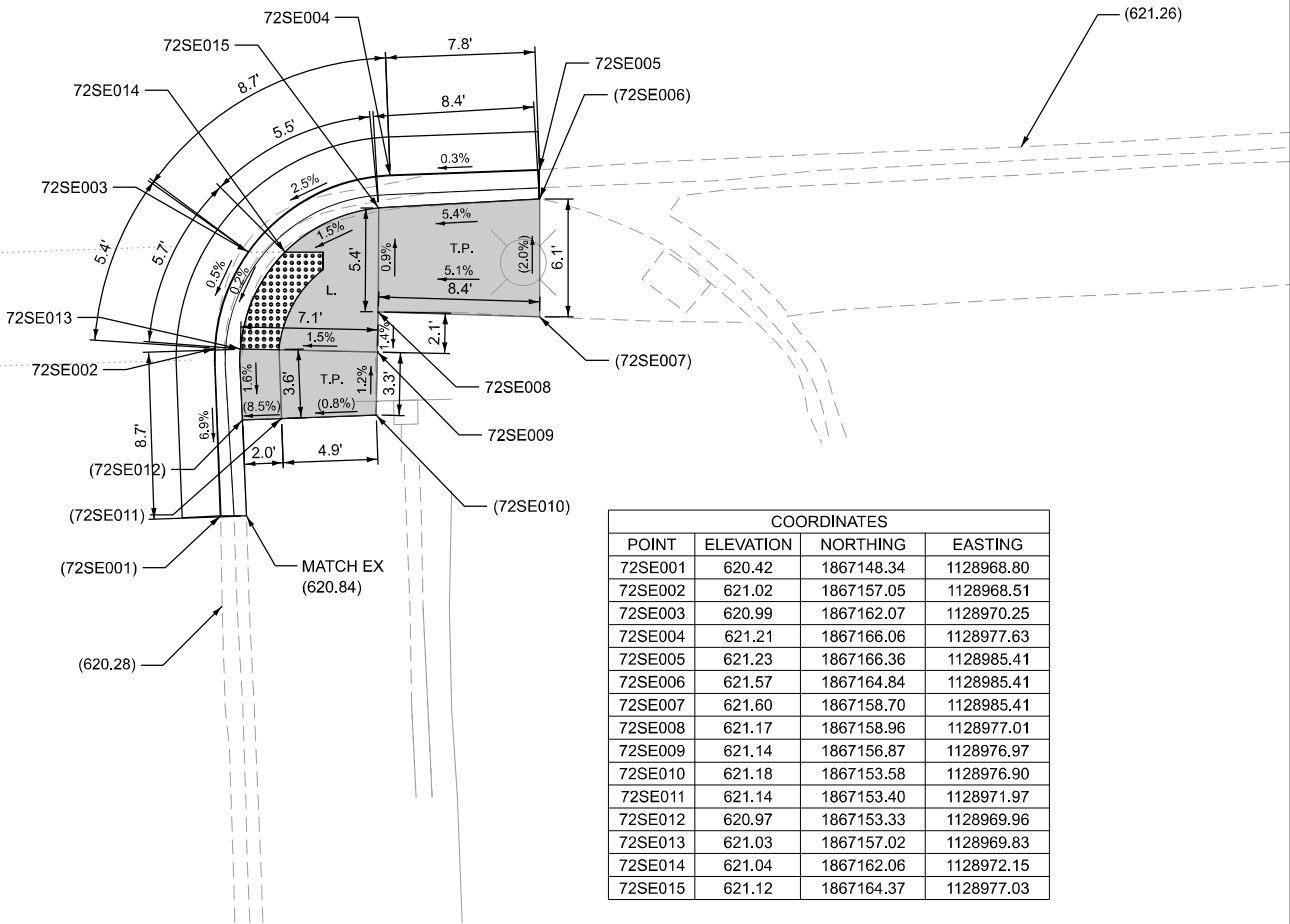
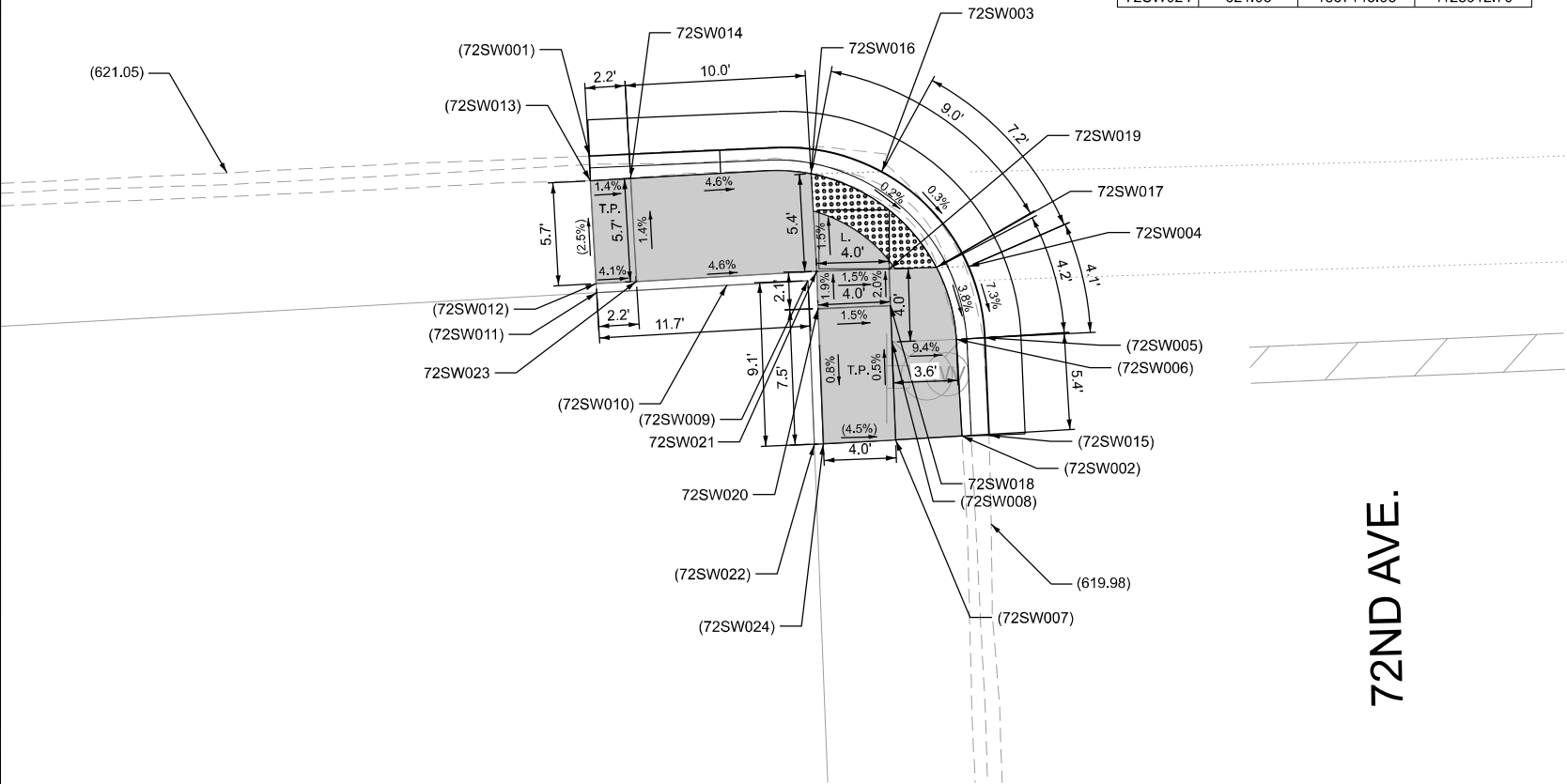
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	15
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
72SW001	621.09	1867161.91	1128899.73
72SW002	620.62	1867146.38	1128920.39
72SW003	620.95	1867161.03	1128915.95
72SW004	620.93	1867155.78	1128920.74
72SW005	620.45	1867151.83	1128921.65
72SW006	620.76	1867151.74	1128920.07
72SW007	621.10	1867146.16	1128916.71
72SW008	621.36	1867151.60	1128916.50
72SW009	621.56	1867154.99	1128911.84
72SW010	621.61	1867154.74	1128907.35
72SW011	621.63	1867154.33	1128900.14
72SW012	621.63	1867154.83	1128900.11
72SW013	621.49	1867160.54	1128899.79
72SW014	621.46	1867160.67	1128902.02
72SW015	620.23	1867146.41	1128921.89
72SW016	621.00	1867160.93	1128912.02
72SW017	620.98	1867155.73	1128918.96
72SW018	621.06	1867153.60	1128916.42
72SW019	621.02	1867155.65	1128916.36
72SW020	621.12	1867153.44	1128912.40
72SW021	621.08	1867155.52	1128912.32
72SW022	621.08	1867145.92	1128912.20
72SW023	621.54	1867154.96	1128902.34
72SW024	621.06	1867145.95	1128912.70

ARCHER AVE.

72ND AVE.



COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
72SE001	620.42	1867148.34	1128968.80
72SE002	621.02	1867157.05	1128968.51
72SE003	620.99	1867162.07	1128970.25
72SE004	621.21	1867166.06	1128977.63
72SE005	621.23	1867166.36	1128985.41
72SE006	621.57	1867164.84	1128985.41
72SE007	621.60	1867158.70	1128985.41
72SE008	621.17	1867158.96	1128977.01
72SE009	621.14	1867156.87	1128976.97
72SE010	621.18	1867153.58	1128976.90
72SE011	621.14	1867153.40	1128971.97
72SE012	620.97	1867153.33	1128969.96
72SE013	621.03	1867157.02	1128969.83
72SE014	621.04	1867162.06	1128972.15
72SE015	621.12	1867164.37	1128977.03

LEGEND

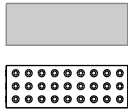
T.P. TRANSITION PANEL
L. LANDING
L.L. LOWER LANDING
U.L. UPPER LANDING

PROPOSED SIDE CURB

() EXISTING ELEVATIONS / SLOPE



SODDING, SALT TOLERANT



PROPOSED SIDEWALK
DETECTABLE WARNINGS



REBUILD EXISTING HANDHOLE (SPECIAL)



ADJUST EXISTING VALVE OR STRUCTURE



REMOVAL OF POLE FOUNDATION

REFERENCE BENCHMARK ELEV 618.06

BENCHMARK : CUT CROSS IN CONCRETE WALK AT 180 SOUTH OF SE CORNER OF ARCHER AVE. AND 72ND CT.

LOCATION : SOUTH EAST CORNER OF ARCHER AVE. AND 72ND CT.

LOCATION : SOUTH WEST CORNER OF ARCHER AVE. AND 72ND CT.



USER NAME = BARTUSIEWICZM
PLOT SCALE = 0.16666633 1/ IN.
PLOT DATE = 3/12/2025

DESIGNED - AS
DRAWN - MB
CHECKED - MB
DATE - 3/14/25

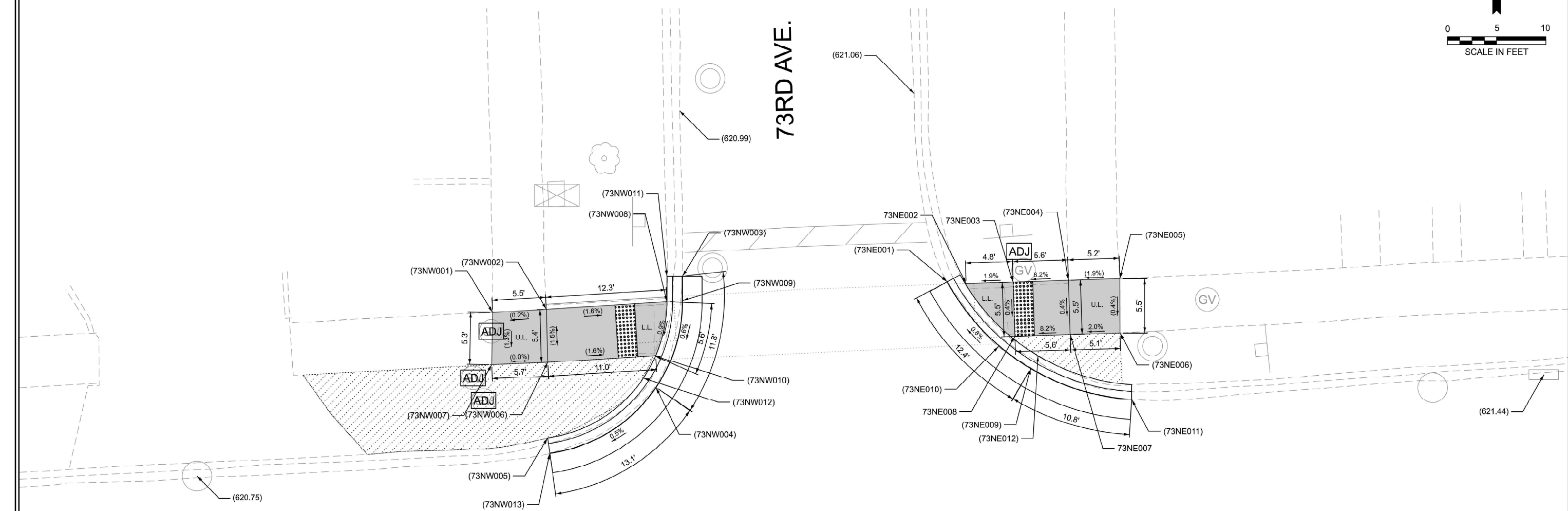
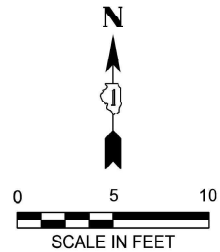
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 72ND CT. SW, SE

SCALE: 1:10 SHEET OF STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0372	FAP 0372 23 SMART	COOK	50	16
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



ARCHER AVE.

73RD AVE.

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
73NW001	621.30	1867216.03	1128571.42
73NW002	621.31	1867216.35	1128576.88
73NW003	621.08	1867219.70	1128590.71
73NW004	621.01	1867208.43	1128588.01
73NW005	621.21	1867203.30	1128577.01
73NW006	621.23	1867210.97	1128577.03
73NW007	621.23	1067210.74	1120571.32
73NW008	621.11	1867217.12	1128589.14
73NW009	621.05	1867217.21	1128590.70
73NW010	621.06	1867211.65	1128587.85
73NW011	621.46	1867219.70	1128589.13
73NW012	621.21	1867209.37	1128586.73
73NW013	620.95	1867201.73	1128577.24

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
73NE001	621.17	1867219.15	1128617.61
73NE002	621.23	1867218.96	1128619.53
73NE003	621.32	1867219.12	1128624.31
73NE004	621.78	1867219.30	1128629.94
73NE005	621.88	1867219.46	1128635.17
73NE006	621.86	1867213.96	1128635.18
73NE007	621.76	1067213.00	1120630.11
73NE008	621.30	1867213.62	1128624.48
73NE009	621.27	1867210.36	1128626.10
73NE010	621.24	1867212.60	1128622.84
73NE011	621.34	1867207.20	1128636.36
73NE012	621.44	1867211.64	1128626.84

LEGEND

T.P. TRANSITION PANEL
L. LANDING
L.L. LOWER LANDING
U.L. UPPER LANDING



PROPOSED SIDE CURB



EXISTING ELEVATIONS / SLOPE



SODDING, SALT TOLERANT



PROPOSED SIDEWALK



DETECTABLE WARNINGS



REBUILD EXISTING HANDHOLE (SPECIAL)



ADJUST EXISTING VALVE OR STRUCTURE



REMOVAL OF POLE FOUNDATION

REFERENCE BENCHMARK ELEV 622.01

BENCHMARK : CUT CROSS IN CONCRETE WALK 50'± EAST OF
NW CORNER OF ARCHER AVE. AND 73RD AVE.

LOCATION : NORTH EAST CORNER OF ARCHER AVE. AND 73RD AVE.



USER NAME	=	connor.mullane
PLOT SCALE	=	0.10666033" / IN.
PLOT DATE	=	1/28/2025

DESIGNED	-	AS
DRAWN	-	MB
CHECKED	-	MB
DATE	-	1/24/25

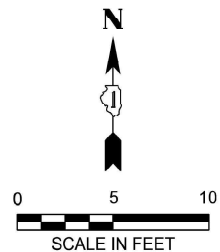
REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 73RD AVE. NW, NE

SCALE: 1:10 SHEET 3 OF 10 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	17
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



ARCHER AVE.

73RD AVE.

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
73SW001	621.24	1867146.44	1128569.90
73SW002	621.06	1867146.92	1128579.86
73SW003	620.99	1867147.16	1128584.07
73SW004	620.91	1867149.72	1128576.71
73SW005	620.63	1867151.58	1128577.11
73SW006	620.92	1867147.34	1128587.15
73SW007	620.87	1867141.94	1128590.96
73SW008	620.00	1867126.63	1128593.03
73SW009	620.69	1867134.16	1128590.38
73SW010	620.48	1867126.40	1128583.68
73SW011	620.79	1867133.65	1128583.43
73SW012	621.04	1867138.68	1128583.26
73SW013	621.01	1867141.41	1128580.18
73SW014	621.34	1867141.11	1128570.50
73SW015	620.95	1867140.24	1128589.39
73SW016	620.19	1867126.35	1128590.78
73SW017	621.18	1867141.25	1128575.18
73SW018	621.14	1867146.68	1128574.87
73SW019	621.06	1867149.53	1128569.90

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
73SE001	620.39	1867137.58	1128638.08
73SE002	620.88	1867144.33	1128639.04
73SE003	620.94	1867150.06	1128642.83
73SE004	620.91	1867154.25	1128651.70
73SE005	621.42	1867150.75	1128656.94
73SE006	621.43	1867145.38	1128657.25
73SE007	621.53	1867141.66	1128657.46
73SE008	621.53	1867140.89	1128652.16
73SE009	621.36	1867140.70	1128648.40
73SE010	621.21	1867137.99	1128648.50
73SE011	621.19	1867137.97	1128648.00
73SE012	620.85	1867137.58	1128640.07
73SE013	621.00	1867140.78	1128640.11
73SE014	620.98	1867144.45	1128641.09
73SE015	621.04	1867150.21	1128645.49
73SE016	621.07	1867150.32	1128647.49
73SE017	621.08	1867144.83	1128647.78
73SE018	621.11	1867141.18	1128647.88
73SE019	621.36	1867152.99	1128651.80

REFERENCE BENCHMARK ELEV 618.02

BENCHMARK : CUT CROSS IN CONCRETE WALK 60'± SOUTH OF ARCHER AVE. AND 73RD AVE.

LOCATION : SOUTH WEST CORNER OF ARCHER AVE. AND 73RD AVE.

T.P. TRANSITION PANEL

L. LANDING

L.L. LOWER LANDING

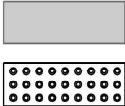
U.L. UPPER LANDING

PROPOSED SIDE CURB

EXISTING ELEVATIONS / SLOPE

SODDING, SALT TOLERANT

LEGEND



PROPOSED SIDEWALK

DETECTABLE WARNINGS



REBUILD EXISTING HANDHOLE (SPECIAL)



ADJUST EXISTING VALVE OR STRUCTURE



REMOVAL OF POLE FOUNDATION

LOCATION : SOUTH EAST CORNER OF ARCHER AVE. AND 73RD AVE.



USER NAME = connor.mullane

PLOT SCALE = 0.100666633' / IN.

PLOT DATE = 1/28/2025

DESIGNED - AS

DRAWN - MB

CHECKED - MB

DATE - 1/24/25

REVISED -

REVISED -

REVISED -

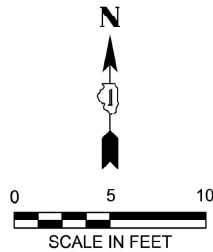
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 73RD AVE. SW, SE

SCALE: 1:10 SHEET 4 OF 10 SHEETS STA. TO STA.

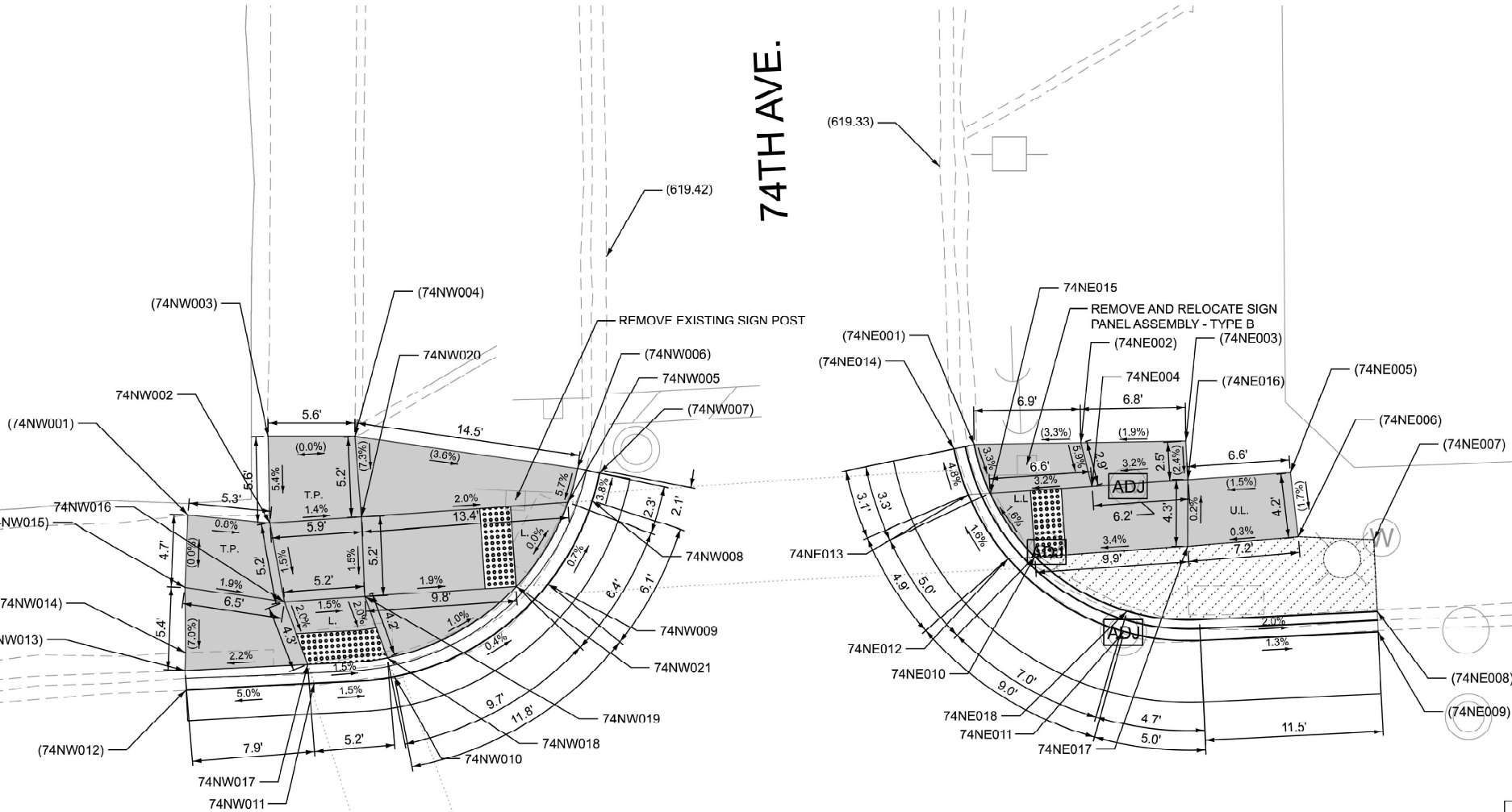
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	18
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
74NW001	619.95	1867182.68	1127894.10
74NW002	619.91	1867182.17	1127899.41
74NW003	620.09	1867187.76	1127899.29
74NW004	620.07	1867187.76	1127904.91
74NW005	619.56	1867183.48	1127918.67
74NW006	619.69	1867185.69	1127919.30
74NW007	619.43	1867185.40	1127920.72
74NW008	619.51	1867183.59	1127920.26
74NW009	619.55	1867178.21	1127917.37
74NW010	619.60	1867172.23	1127907.46
74NW011	619.68	1867171.81	1127902.28
74NW012	619.27	1867171.37	1127893.97
74NW013	619.57	1867172.61	1127893.90
74NW014	619.77	1867173.78	1127893.94
74NW015	619.95	1867178.01	1127893.91
74NW016	619.83	1867177.06	1127900.35
74NW017	619.74	1867173.01	1127901.84
74NW018	619.66	1867173.44	1127907.01
74NW019	619.75	1867177.41	1127905.55
74NW020	619.83	1867182.57	1127905.33
74NW021	619.56	1867178.07	1127915.35

74TH AVE.

ARCHER AVE.



COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
74NE001	619.63	1867187.24	1127944.95
74NE002	619.86	1867187.35	1127951.85
74NE003	619.99	1867187.46	1127958.66
74NE004	619.73	1867184.54	1127952.59
74NE005	620.03	1867185.42	1127965.42
74NE006	619.96	1867181.28	1127965.93
74NE007	619.89	1867181.09	1127970.81
74NE008	619.61	1867176.44	1127971.03
74NE009	619.27	1867175.11	1127971.09
74NE010	619.60	1867179.99	1127948.89
74NE011	619.49	1867175.08	1127954.61
74NE012	619.55	1867179.87	1127947.17
74NE013	619.47	1867184.00	1127944.58
74NE014	619.32	1867186.97	1127943.65
74NE015	619.52	1867184.09	1127946.00
74NE016	619.93	1867184.97	1127958.79
74NE017	619.94	1867180.67	1127958.77
74NE018	619.94	1867176.47	1127954.86

REFERENCE BENCHMARK ELEV 620.29

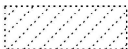
BENCHMARK : CUT CROSS IN CONCRETE WALK AT SW CORNER OF ARCHER AVE. AND 74TH AVE. 50'± NORTH OF INTERSECTION

LOCATION : NORTH WEST CORNER OF ARCHER AVE. AND 74TH AVE.

T.P. TRANSITION PANEL
L. LANDING
L.L. LOWER LANDING
U.L. UPPER LANDING

PROPOSED SIDE CURB

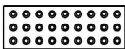
() EXISTING ELEVATIONS / SLOPE



SODDING, SALT TOLERANT

LEGEND

PROPOSED SIDEWALK



DETECTABLE WARNINGS

REC

REBUILD EXISTING HANDHOLE (SPECIAL)

ADJ

ADJUST EXISTING VALVE OR STRUCTURE

R

REMOVAL OF POLE FOUNDATION

LOCATION : NORTH EAST CORNER OF ARCHER AVE. AND 74TH AVE.



USER NAME = connor.mullane
PLOT SCALE = 0.100000033' / IN.
PLOT DATE = 1/28/2025

DESIGNED - AS
DRAWN - MB
CHECKED - MB
DATE - 1/24/25

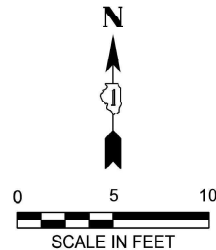
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 74TH AVE. NW, NE

SCALE: 1:10 SHEET 5 OF 10 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	19
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



ARCHER AVE.

74TH AVE.

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
74SW001	619.50	1867119.99	1127913.54
74SW002	619.67	1867119.61	1127920.42
74SW003	619.50	1867111.70	1127930.61
74SW004	618.93	1867104.09	1127932.64
74SW005	619.35	1867103.96	1127931.19
74SW006	619.38	1867106.19	1127930.98
74SW007	619.77	1867105.71	1127924.19
74SW008	619.87	1867105.68	1127923.69
74SW009	619.84	1867111.03	1127923.37
74SW010	620.08	1867110.88	1127914.77
74SW011	620.08	1867111.38	1127914.76
74SW012	619.64	1867118.36	1127913.83
74SW013	619.86	1867118.31	1127918.61
74SW014	619.73	1867117.67	1127921.25
74SW015	619.60	1867111.66	1127928.87
74SW016	619.64	1867111.54	1127923.84
74SW017	619.86	1867111.46	1127919.30

COORDINATES			
POINT	ELEVATION	NORTHING	EASTING
74SE001	618.75	1867101.84	1127979.30
74SE002	619.09	1867107.41	1127979.26
74SE003	619.46	1867112.93	1127980.27
74SE004	619.51	1867116.98	1127982.45
74SE005	619.52	1867120.05	1127985.37
74SE006	619.54	1867107.45	1127980.60
74SE007	619.53	1867123.82	1127996.58
74SE008	619.84	1867122.32	1127996.69
74SE009	620.13	1867118.58	1127997.73
74SF010	620.12	1867117.36	1127997.80
74SE011	620.10	1867113.37	1127998.02
74SE012	619.76	1867113.26	1127993.78
74SE013	619.64	1867113.12	1127987.90
74SE014	619.65	1867107.63	1127988.04
74SE015	619.66	1867102.03	1127988.20
74SE016	619.26	1867101.85	1127980.91
74SE017	619.55	1867112.96	1127981.72
74SE018	619.57	1867117.03	1127984.18
74SE019	619.58	1867119.01	1127986.20
74SE020	619.65	1867117.11	1127987.76
74SE021	619.77	1867117.26	1127993.69

REFERENCE BENCHMARK ELEV 619.83

BENCHMARK : CUT CROSS IN CONCRETE WALK AT SW CORNER OF ARCHER AVE. AND 74TH AVE

LOCATION : SOUTH WEST CORNER OF ARCHER AVE. AND 74TH AVE.

T.P. TRANSITION PANEL

L. LANDING

L.L. LOWER LANDING

U.L. UPPER LANDING

PROPOSED SIDE CURB

() EXISTING ELEVATIONS / SLOPE

SODDING, SALT TOLERANT

LEGEND

PROPOSED SIDEWALK

DETECTABLE WARNINGS

REC REBUILD EXISTING HANDHOLE (SPECIAL)

ADJ ADJUST EXISTING VALVE OR STRUCTURE

R REMOVAL OF POLE FOUNDATION

LOCATION : SOUTH EAST CORNER OF ARCHER AVE. AND 74TH AVE.



USER NAME = connor.mullane
PLOT SCALE = 0.100660633" / IN.
PLOT DATE = 1/28/2025

DESIGNED - AS
DRAWN - MB
CHECKED - MB
DATE - 1/24/25

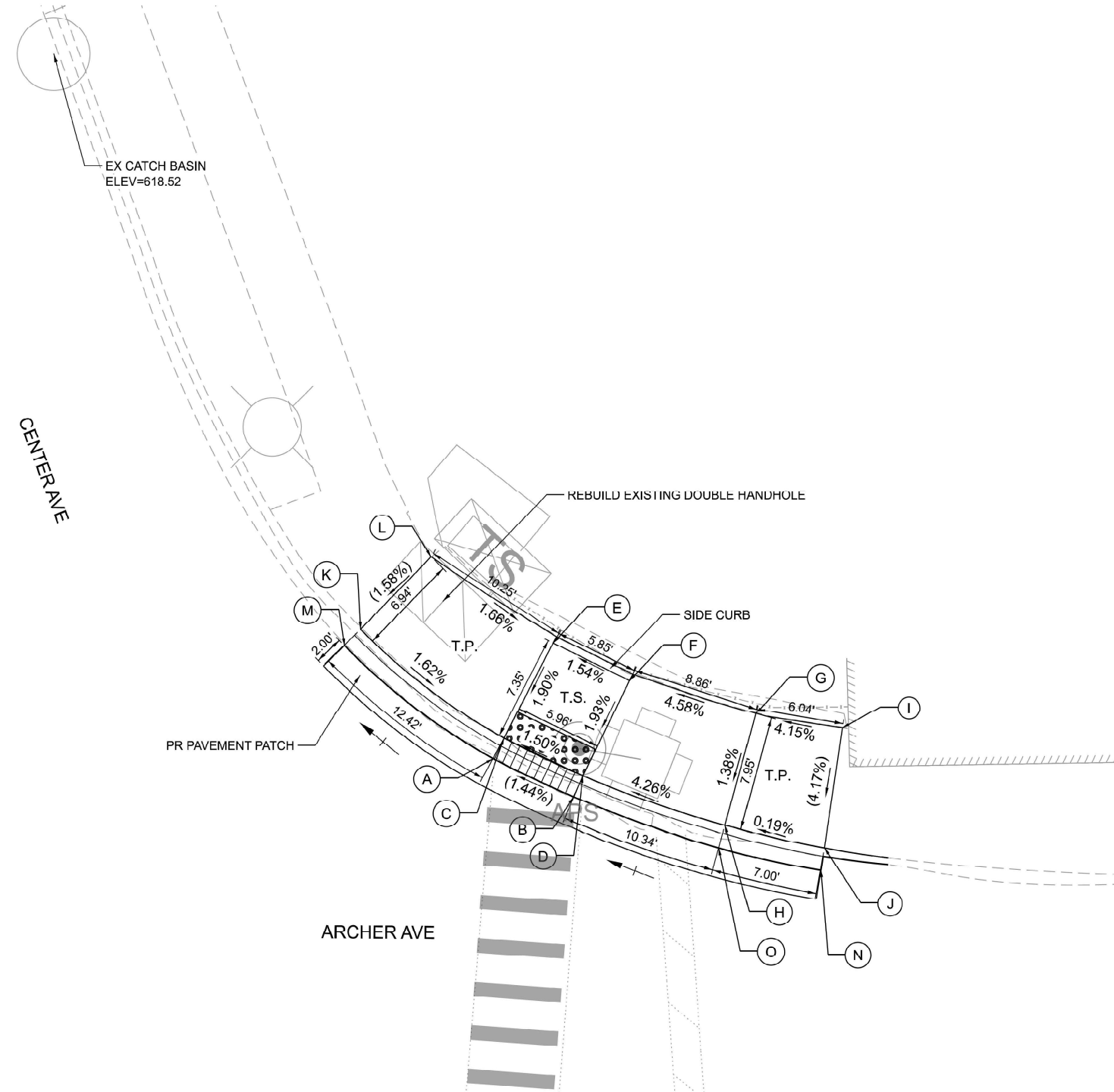
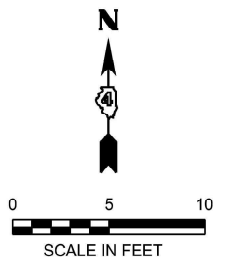
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAIL
ARCHER AVE. & 74TH AVE. SW, SE




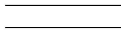
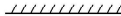

SCALE: 1:10 SHEET 6 OF 10 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	20
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



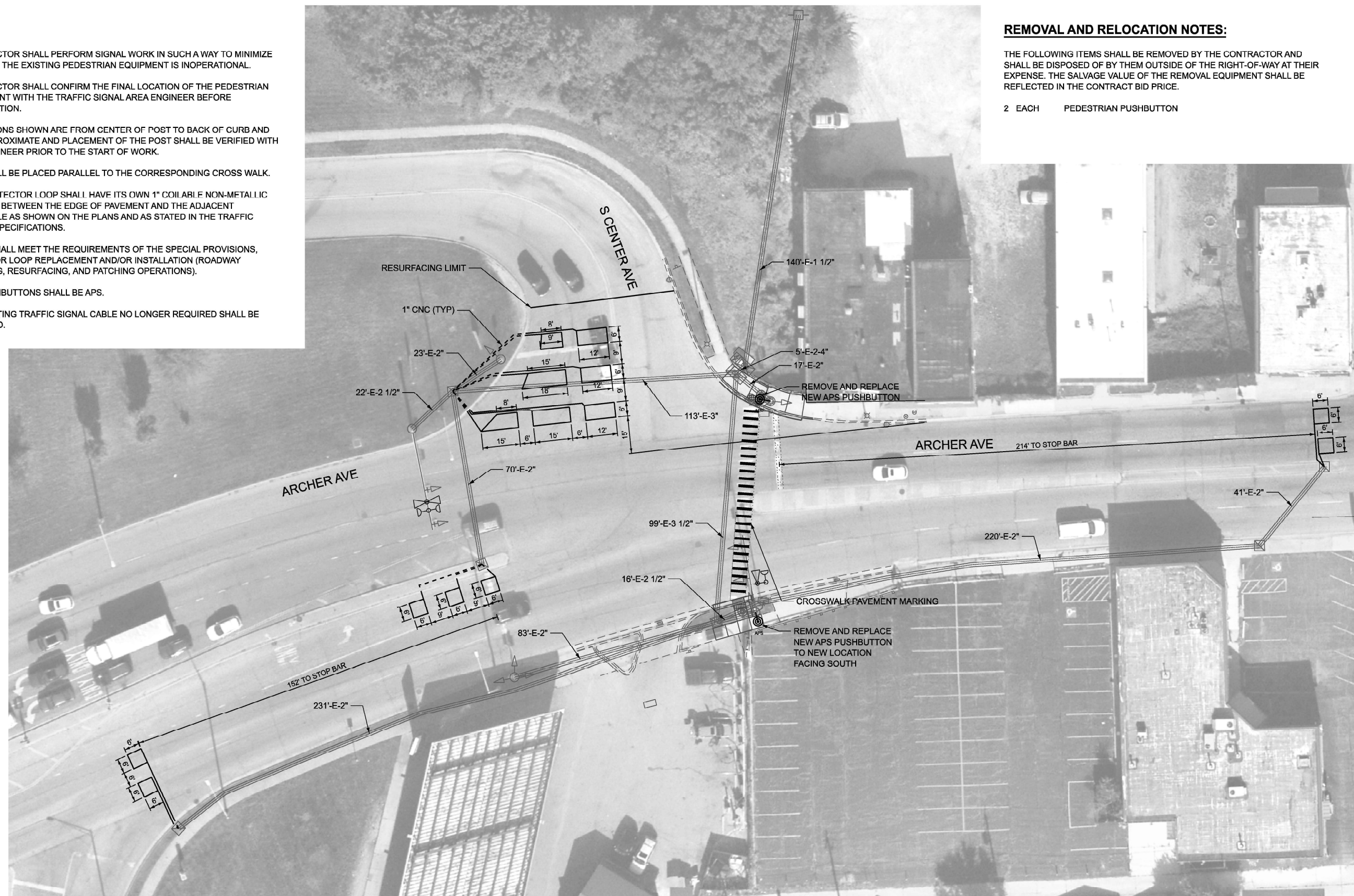
POINT	ELEVATION	NORTHING	EASTING
A	(617.72)	1867154.19	1127344.48
B	(617.81)	1867151.55	1127350.03
C	617.75	1867155.58	1127345.24
D	617.84	1867153.00	1127350.65
E	617.89	1867162.11	1127348.61
F	617.98	1867159.51	1127353.87
G	618.39	1867157.27	1127362.53
H	618.28	1867149.60	1127360.41
I	(618.64)	1867156.28	1127368.48
J	(618.29)	1867148.05	1127367.23
K	(617.95)	1867163.06	1127335.38
L	(618.06)	1867168.03	1127340.23
M	(617.57)	1867161.92	1127334.28
N	(617.97)	1867146.52	1127367.00
O	(617.95)	1867148.05	1127359.98

REFERENCE BENCHMARK ELEVATION: 618.03
BENCHMARK: X CUT IN CURB
LOCATION: NW CORNER OF
ARCHER AVE AND S CENTER AVE

	DETECTABLE WARNINGS
	DEPRESSED CURB AND GUTTER
	TOPSOIL FURNISH AND PLACE 4" SODDING, SALT TOLERANT
	PROPOSED SIDE CURB
	BUILDING LINE
	FLOW DIRECTION ARROW
(123.45)	EXISTING ELEVATION/ MATCH EXISTING ELEVATION
123.45	PROPOSED ELEVATION
T.P.	TRANSITION PANEL
T.S.	TURNING SPACE
L.L.	LOWER LANDING
U.L.	UPPER LANDING

1. CONTRACTOR SHALL MAINTAIN SIGNAL WORK IN SUCH A WAY TO MINIMIZE THE TIME THE EXISTING PEDESTRIAN EQUIPMENT IS INOPERATIONAL.
2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
3. DIMENSIONS SHOWN ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE AND PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.
4. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSS WALK.
5. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
6. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
7. ALL PUSHBUTTONS SHALL BE APS.
8. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE OF THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVAL EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.



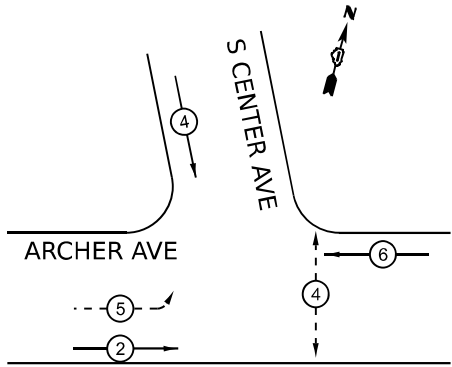
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0372	FAP 0372 23 SMART	COOK	50	25
		CONTRACT NO. 62V56		
		ILLINOIS	FED. AID PROJECT	



QUIGG ENGINEERING INC

MODEL: Unnamed Plan - Plan 1 [Sheet]
FILE NAME: S:\2018\1818\1818\42-AES IDOT PTB 185-C2\WO22 ADA Ramp Design\CADD\CADD Sheets\182V56-Sht-TS.dgn

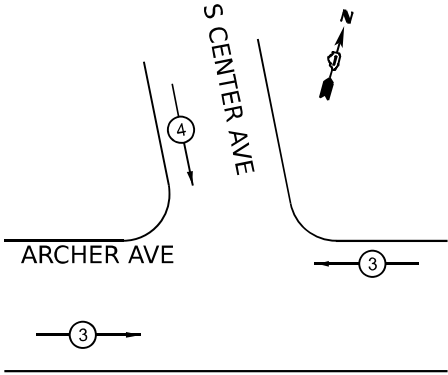
EXISTING CONTROLLER SEQUENCE



LEGEND

- PROTECTED PHASE
- RIGHT TURN OVERLAP
- NUMBER REFERRING TO ASSOCIATED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE

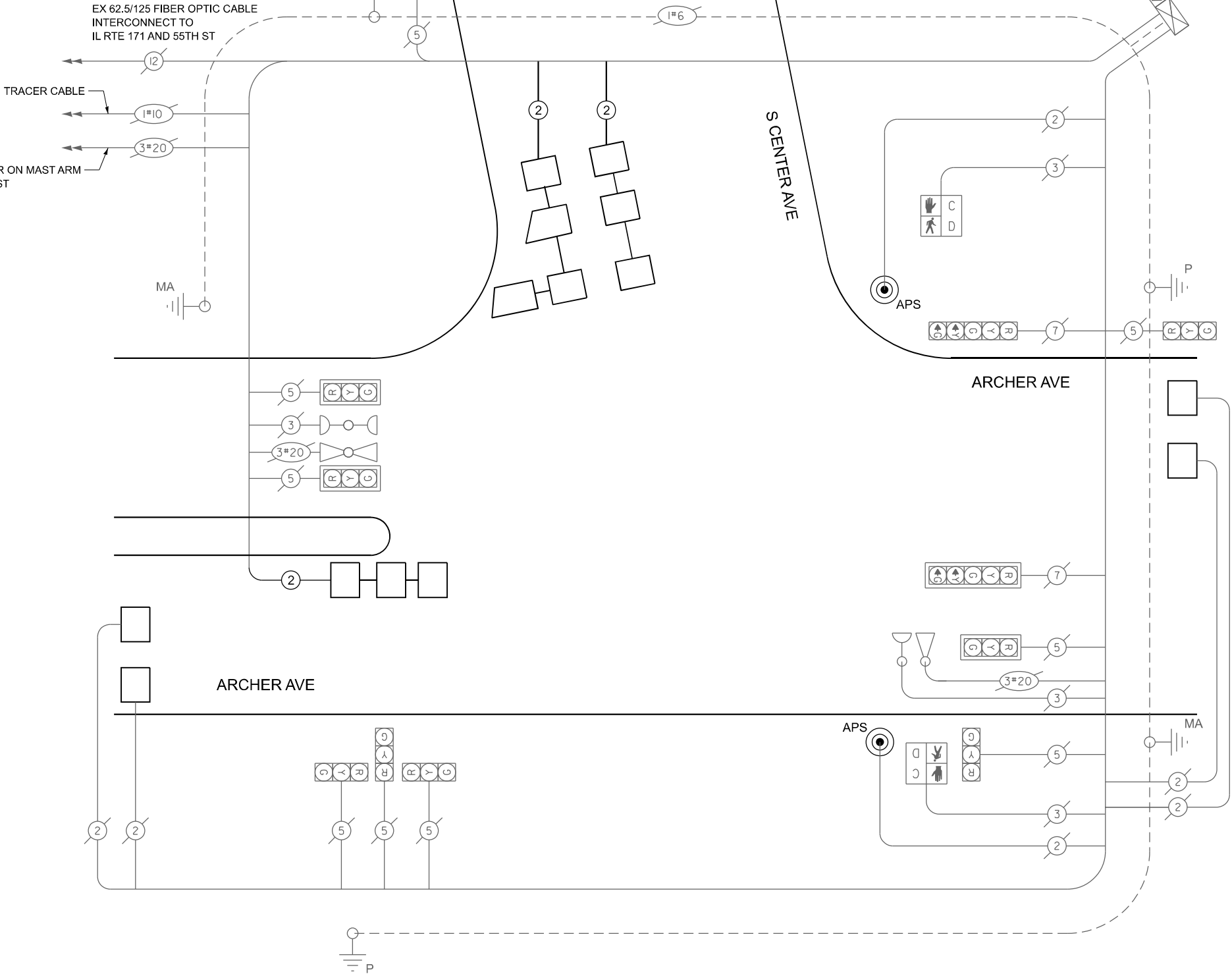
EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



PAY ITEM DESCRIPTION	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2073
INDUCTIVE LOOP DETECTOR	EACH	2
DETECTOR LOOP, TYPE I	FOOT	951
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2073
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
REBUILD EXISTING DOUBLE HANDHOLE	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS ARCHER AVE AND S CENTER AVE				
EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE	
SIGNAL HEAD	1 OR 3-SECTION	9	11	99
	4-SECTION	-	14	-
	5-SECTION	2	13	26
PROGRAMMABLE	3-SECTION	-	22	-
	4-SECTION	-	32	-
	5-SECTION	-	28	-
PEDESTRIAN SIGNAL	2	15	30	
CONTROLLER	1	150	150	
MASTER CONTROLLER	-	100	-	
UPS	1	25	25	
DETECTION	RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-	
NETWORK SWITCH	II OR III	-	35	-
CELLULAR MODEM	-	15	-	
TOTAL UPS SIZING			330	
UPS CHARGING	1	225	225	
BATTERY HEATER MAT	1	180	180	
CABINET HEATER	1	200	200	
FLASHER	-	15	-	
LED STREET NAME SIGN	-	120	-	
LUMINAIRE	-	240	-	
TOTAL SERVICE WIRE SIZING			935	

ENERGY COSTS TO:
VILLAGE OF SUMMIT
7321 W 59TH ST
SUMMIT, IL 60501
ENERGY SUPPLY:
CONTACT: NICK TELUJA
PHONE: (440) 796-8979
COMPANY: COMED
ACCOUNT NUMBER:
METER NUMBER:



CABLE PLAN

TS 3835
ECON 131

MODEL: Cable Plan - Cable Plan [Sheet]
FILE NAME: S:\2018\1811\0424-LES\DOT PTB 185-02\W022_ADA Ramp Design\CADD\CADD Sheets\162V56-SHV-TS_CablePlan.dgn



QUIGG ENGINEERING INC

USER NAME =	afarooqi
PLOT SCALE =	0.16666633' / in.
PLOT DATE =	3/12/2025

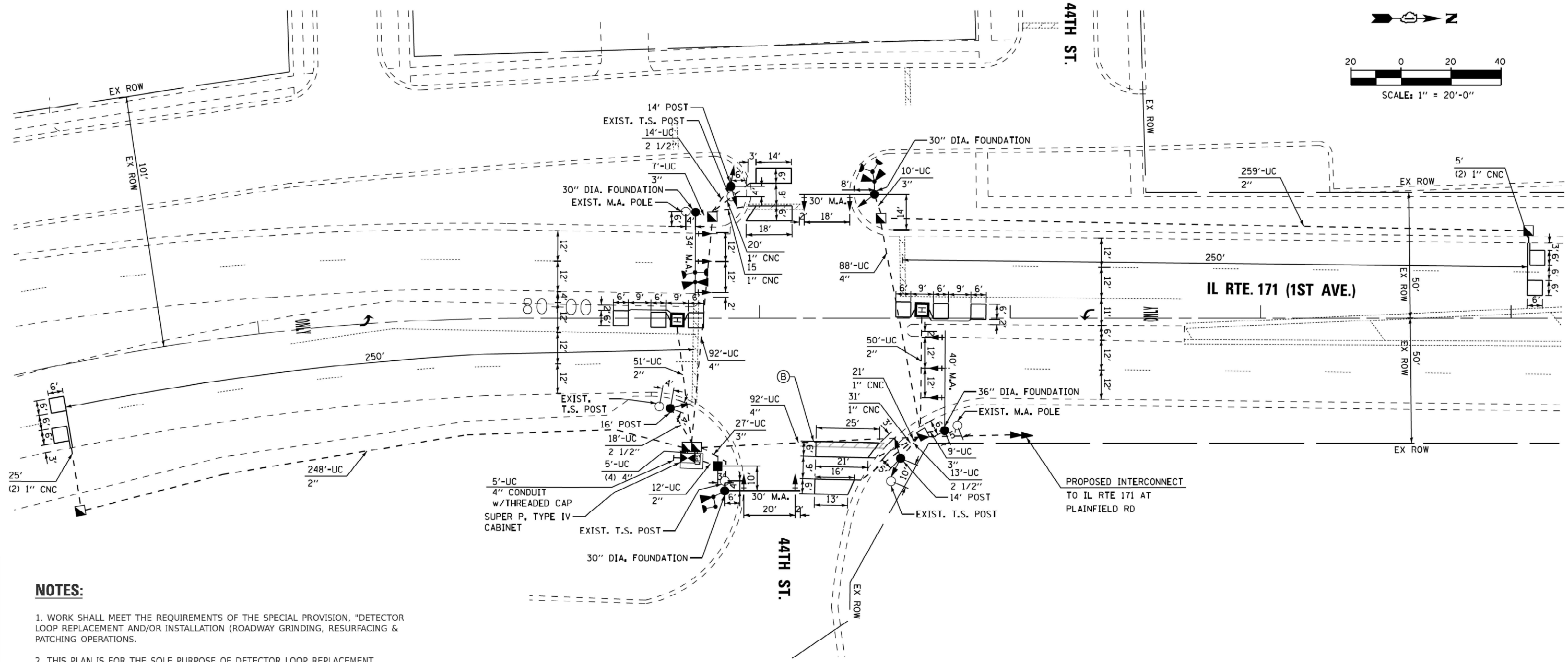
DESIGNED -	AAF	REVISED -	
DRAWN -	AAF	REVISED -	
CHECKED -	JA	REVISED -	
DATE -	12/09/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SYSTEM
& SCHEDULE OF QUANTITIES IL 171 (ARCHER AVE) AND CENTER AVE

SCALE: 1"=10' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0372	FAP 0372 23 SMART	COOK	50	26
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				



NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
- 2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
X8860105	DETECTOR LOOP REPLACEMENT	519	FOOT

TS 14165

USER NAME = connor.mullane	DESIGNED - RR	REVISED -
	DRAWN - RR	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - LP	REVISED -
PLOT DATE = 3/18/2025	DATE - 02/23/2018	REVISED -

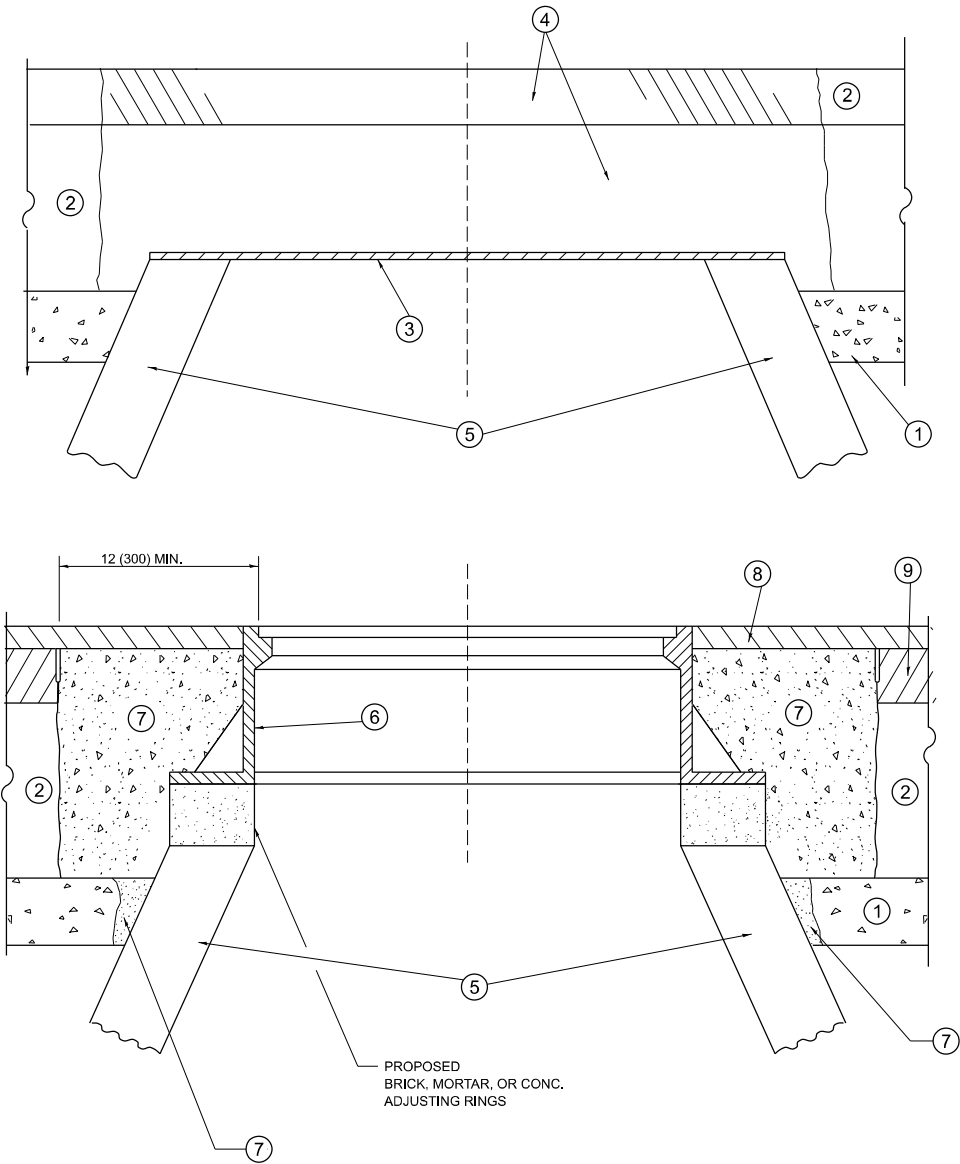
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETECTOR LOOP REPLACEMENT PLAN IL 171 AT 44TH ST			
SCALE: 1"=20'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	26A
CONTRACT NO. 62V56				
ILLINOIS FED. AID PROJECT				

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MODEL: BD-08 [Sheet]
FILE NAME: c:\pav_work\pav\tdot\connor.mullane@illinois.gov\0914281\03424-eh-DetShts.dgn



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

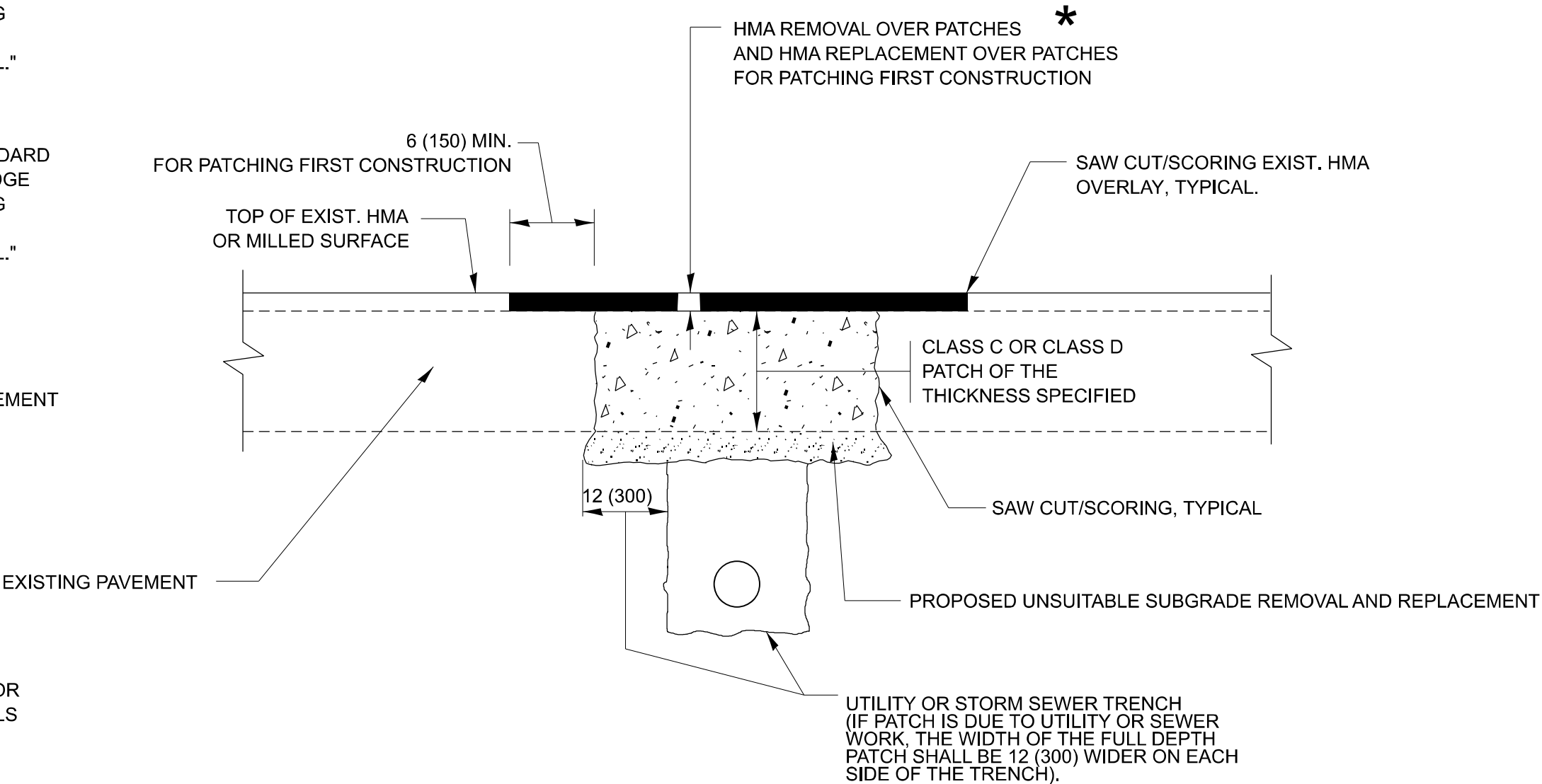
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			DRAWN	-	REVISED	- R. BORO 12-06-11					372	FAP 0372 23 SMART	COOK	50	27
			CHECKED	-	REVISED	- K. SMITH 11-18-22					BD600-03 (BD-08)		CONTRACT NO. 62V56		
	PLOT DATE	= 1/28/2025	DATE	- 10-25-94	REVISED	- K. SMITH 09-15-23		SCALE:NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

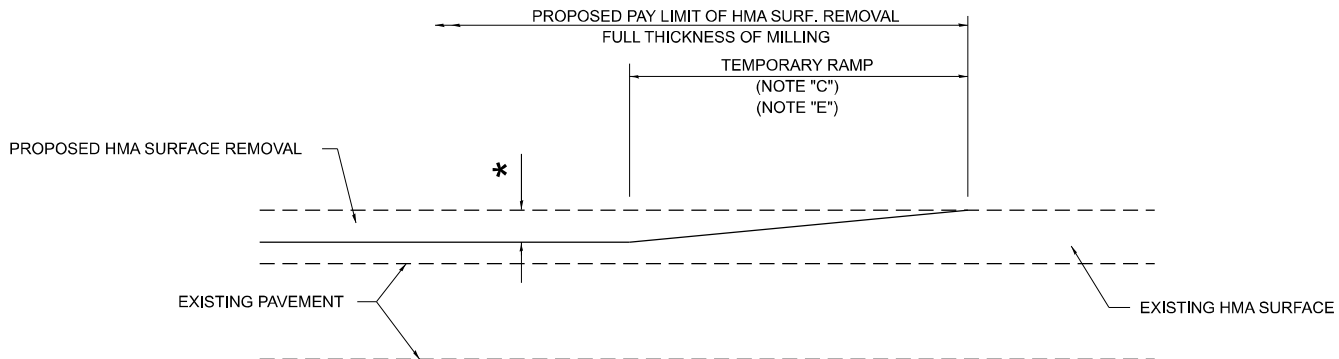
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

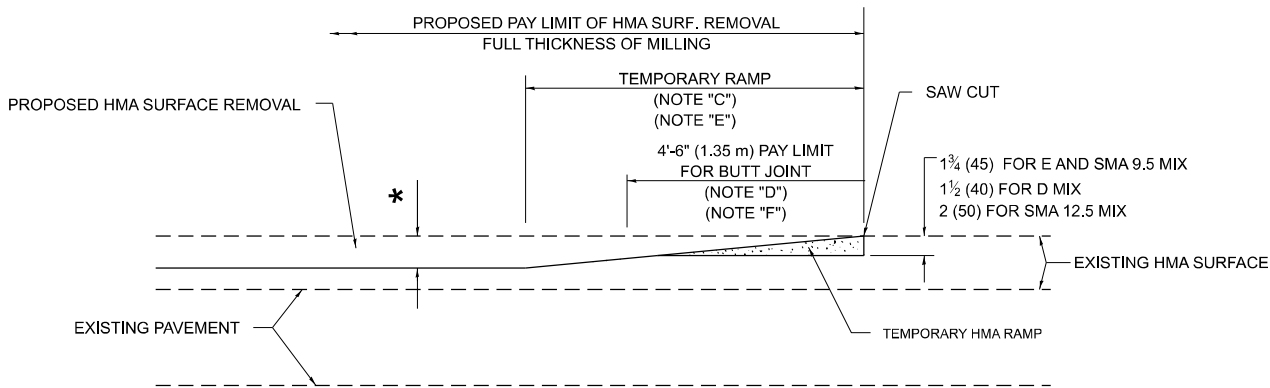
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	USER NAME = connor.mullane	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 09-04-07						372	FAP 0372 23 SMART	COOK	50	28
		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22)				
	PLOT DATE = 1/28/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

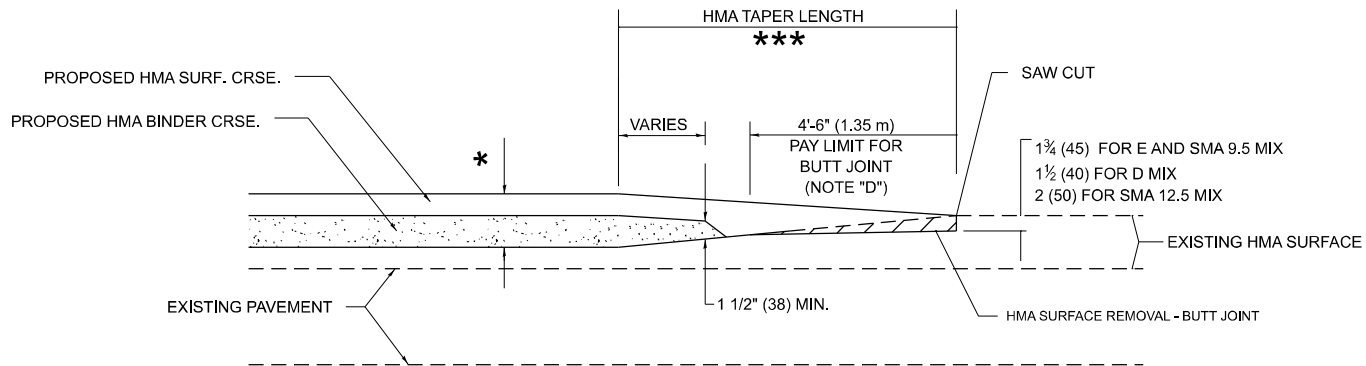
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

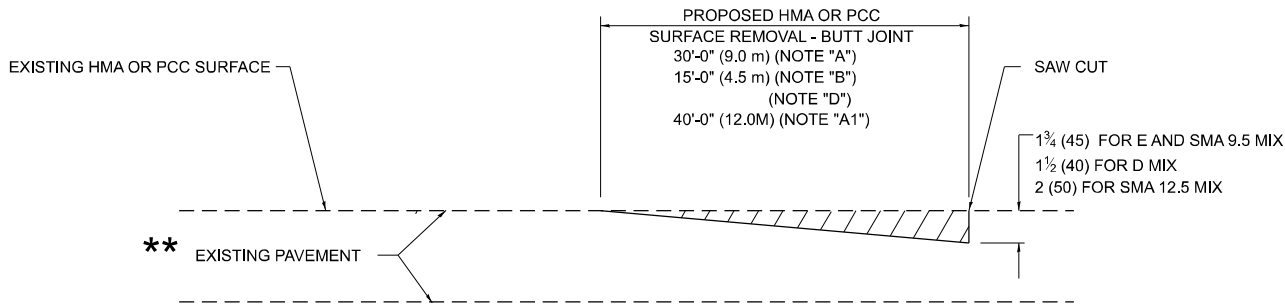
OPTION 2

TYPICAL TEMPORARY RAMP

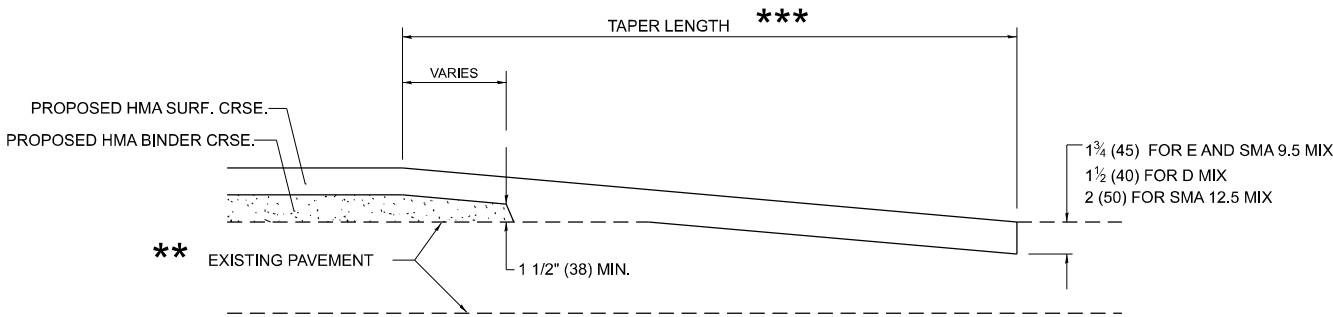


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME: c:\pav_work\pav\tdot\connor.mullane@illinois.gov\d0914281\ID103424-eh-DistStd.dgn

USER NAME = connor,mullane	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN - M. GOMEZ 04-06-01	REVISED - M. GOMEZ 04-06-01
	CHECKED - R. BORO 01-01-07	REVISED - R. BORO 01-01-07
PLOT DATE = 1/28/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

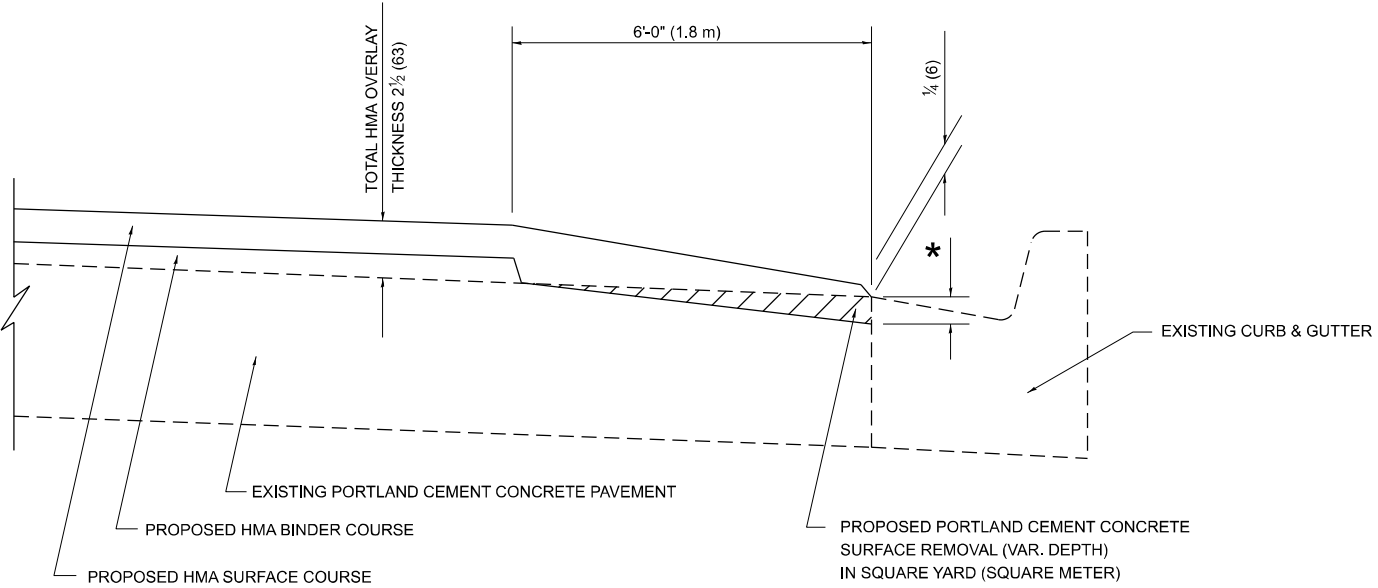
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	30
BD400-05 BD-32		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

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**HMA TAPER AT
EDGE OF PCC PAVEMENT**

HMA SURFACE COURSE		HMA BINDER COURSE	
MIX	THICKNESS	THICKNESS	* MILLING AT GUTTER FLAG
D	1 1/2 (38)	1 (25)	1 1/4 (33)
E OR SMA 9.5	1 3/4 (44)	3/4 (19)	1 1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS
OTHERWISE SHOWN.

USER NAME = connor.mullane	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN - JIS	REVISED - JP CHANG 07-08-16
	CHECKED - A. ABBAS	REVISED - K. SMITH 02-01-22
PLOT DATE = 1/28/2025	DATE - 09-10-94	REVISED - K. SMITH 11-18-22

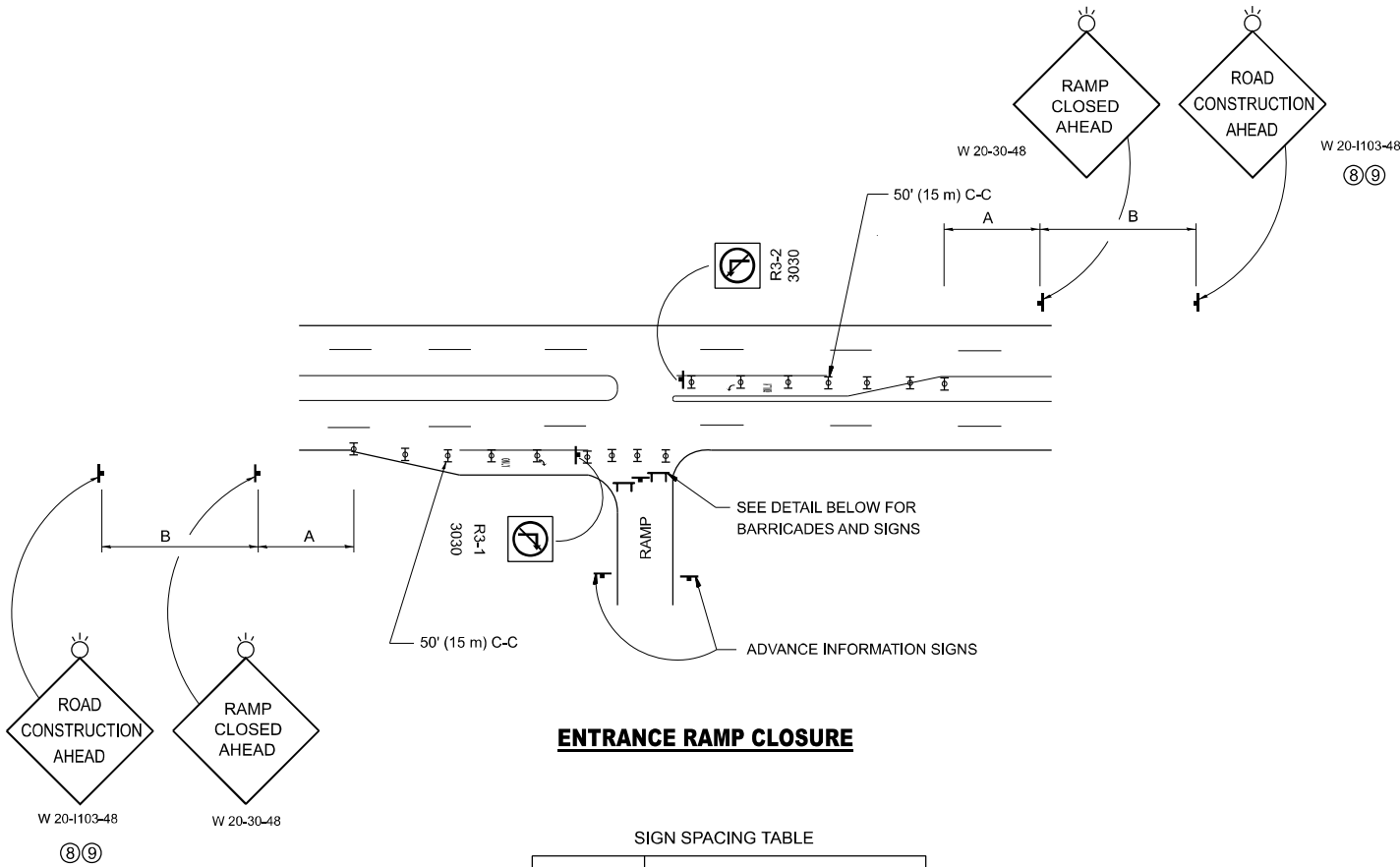
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	31
BD400-06 BD-33		CONTRACT NO. 62V56		
		ILLINOIS	FED. AID PROJECT	

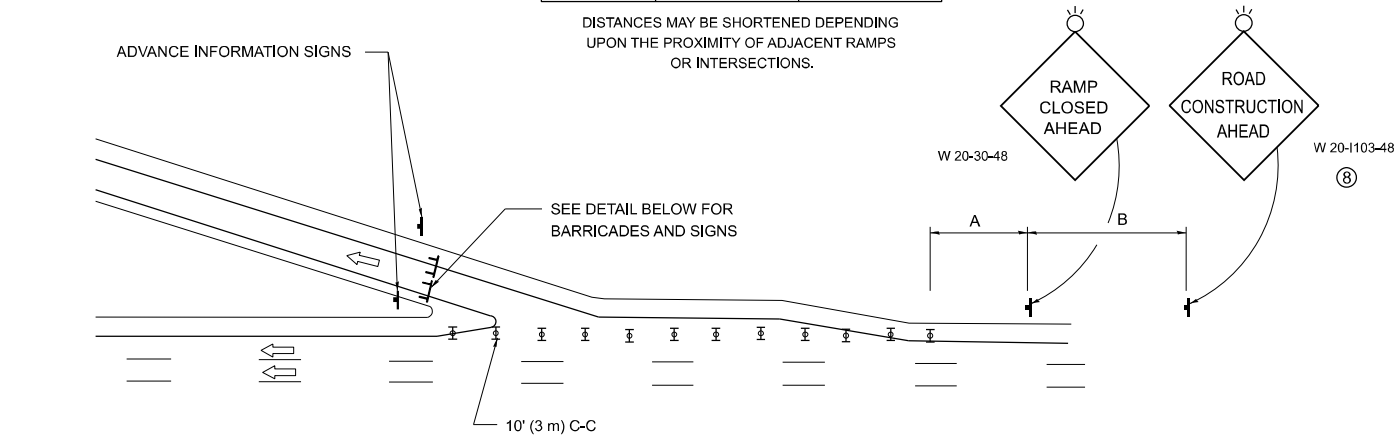
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ENTRANCE RAMP CLOSURE

FACILITY	SIGN SPACING TABLE	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

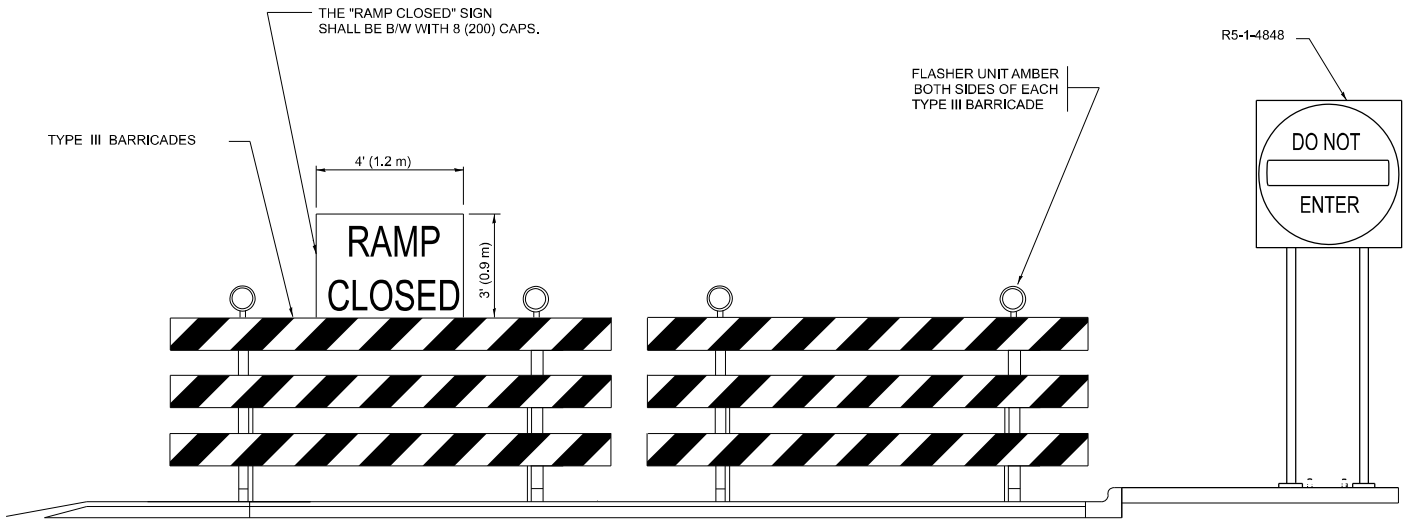
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

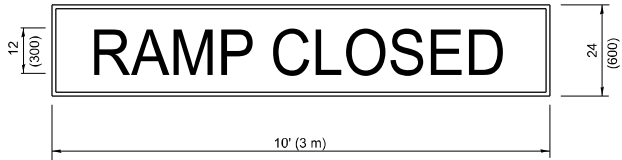
SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



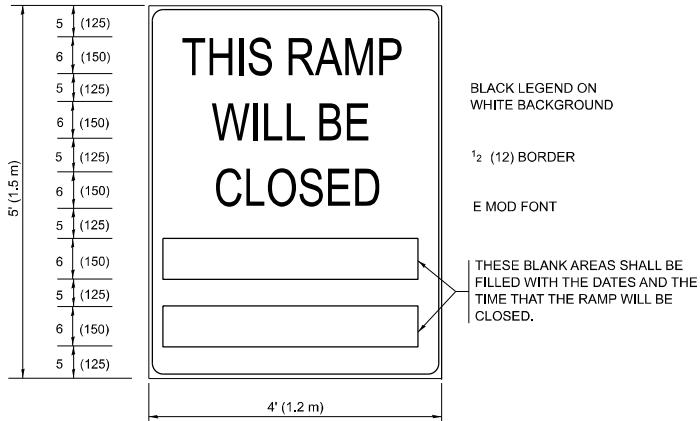
BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

GENERAL NOTES:

- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = connor.mullane	DESIGNED - D.W.S.	REVISED - S.P.B._12-09
	DRAWN -	REVISED - M.D._06-13
	CHECKED -	REVISED - M.D._01-18
PLOT DATE = 1/28/2025	DATE - 02-83	REVISED - D.S._05-24

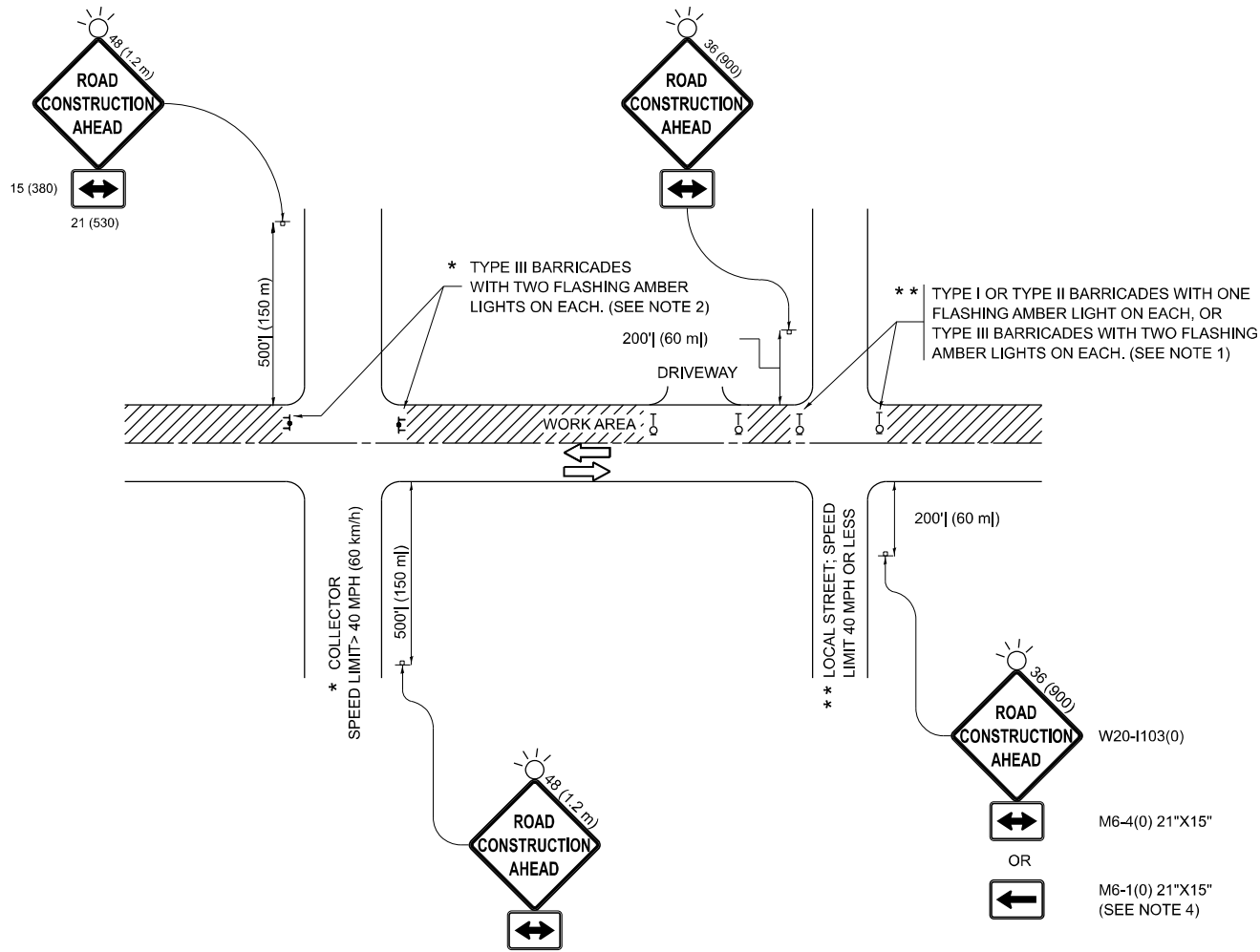
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENTRANCE AND EXIT_RAMP CLOSURE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	32
TC-08		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

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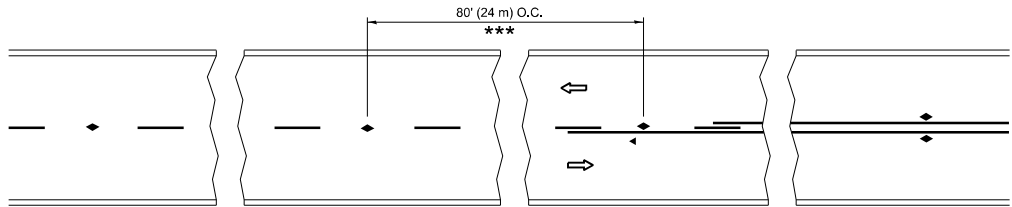


NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

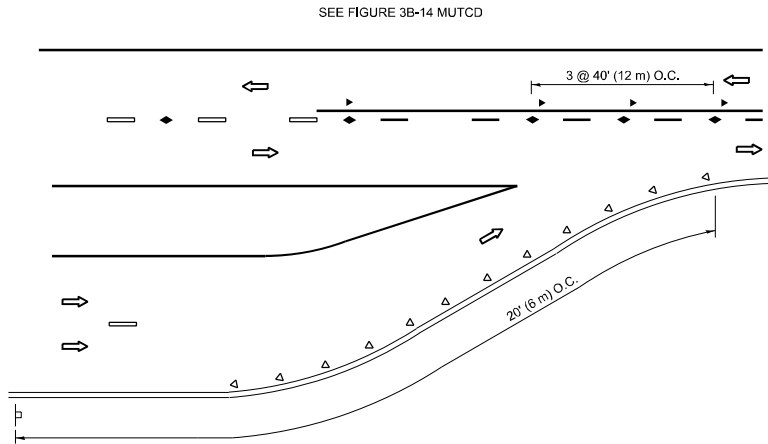
All dimensions are in inches (millimeters)
unless otherwise shown.

	USER NAME = connor.mullane	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
		DRAWN -	REVISED - A. SCHUETZE 07-01-13						372	FAP 0372 23 SMART	COOK	50	33			
		CHECKED -	REVISED - A. SCHUETZE 09-15-06						TC-10					CONTRACT NO. 62V56		
	PLOT DATE = 1/28/2025	DATE - 06-89	REVISED - D. SENDERAK 05-03-24						ILLINOIS					FED. AID PROJECT		
	SCALE:		SHEET OF SHEETS						STA.		TO STA.					

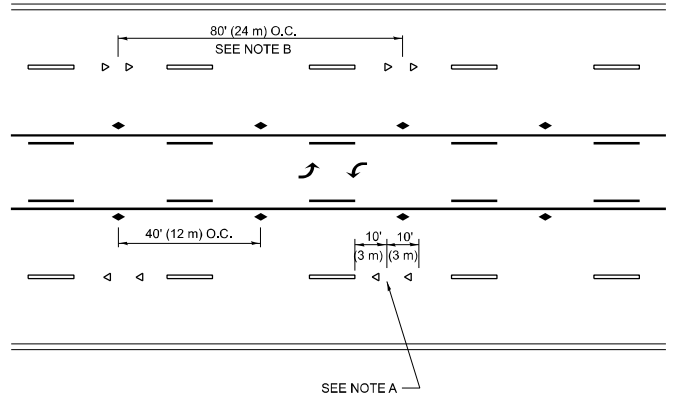


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

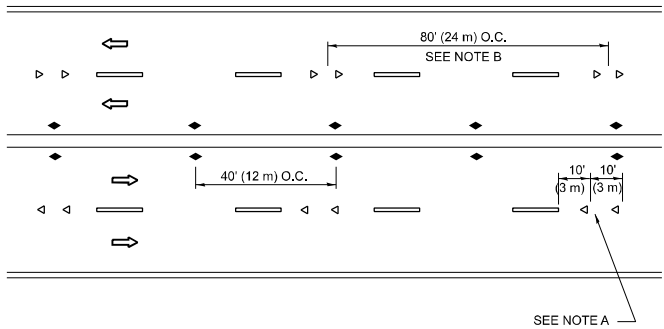
TWO-LANE/TWO-WAY



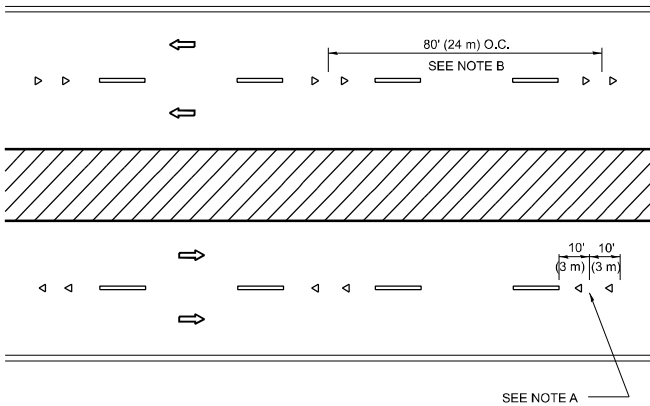
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

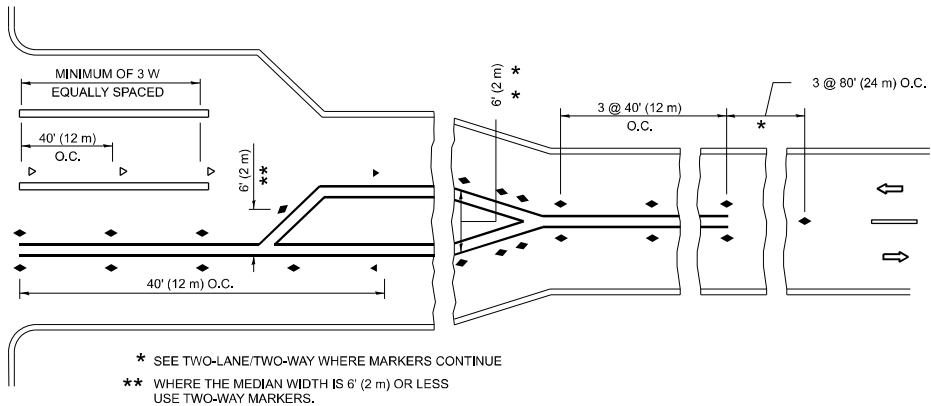
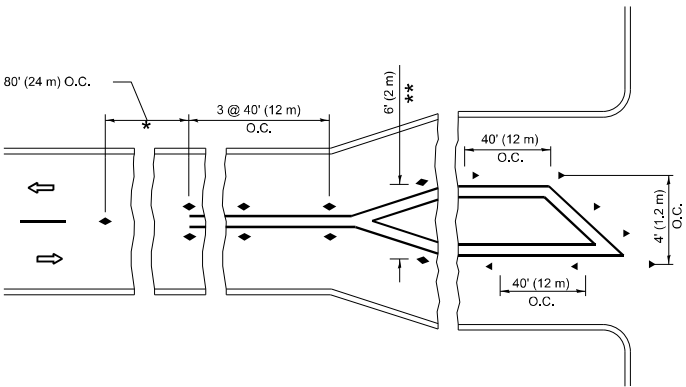
- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

TURN LANES



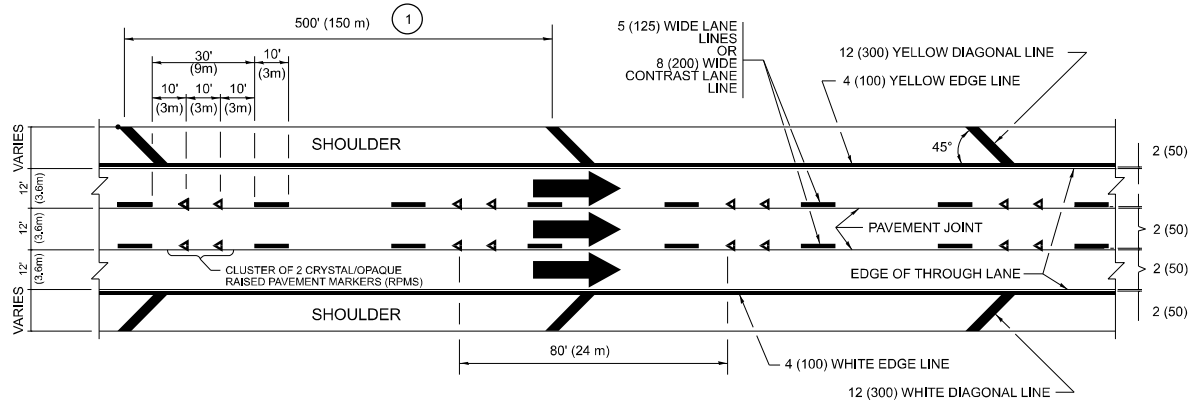
- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

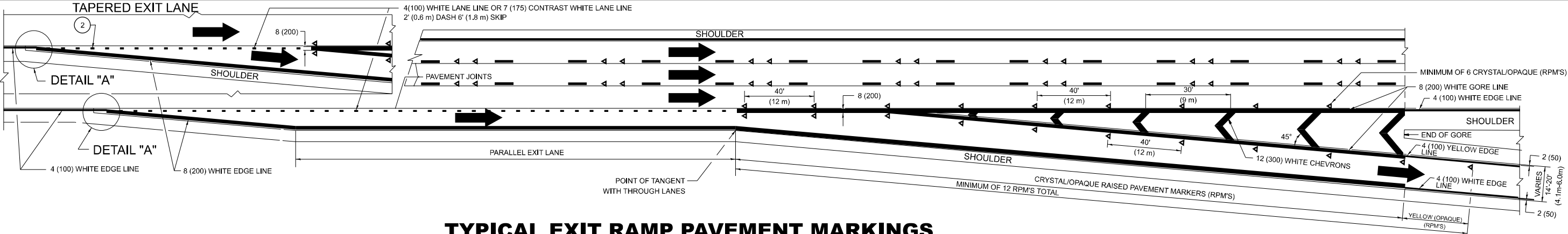
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	34
TC-11		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		



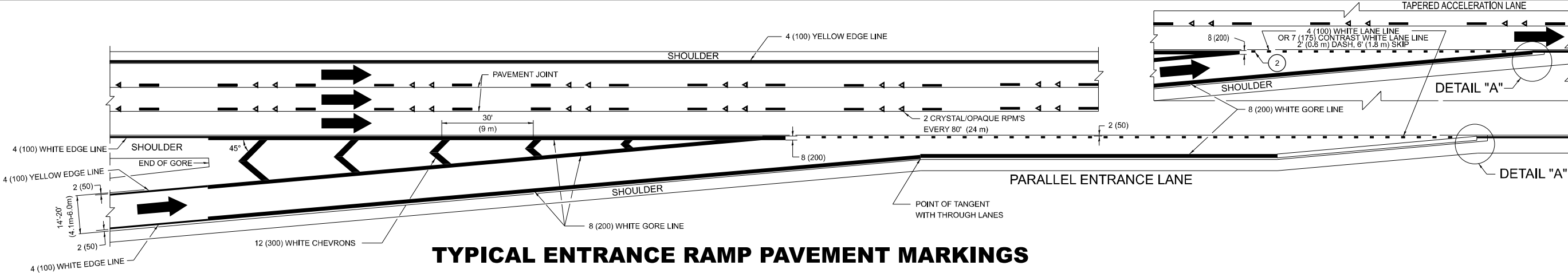
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

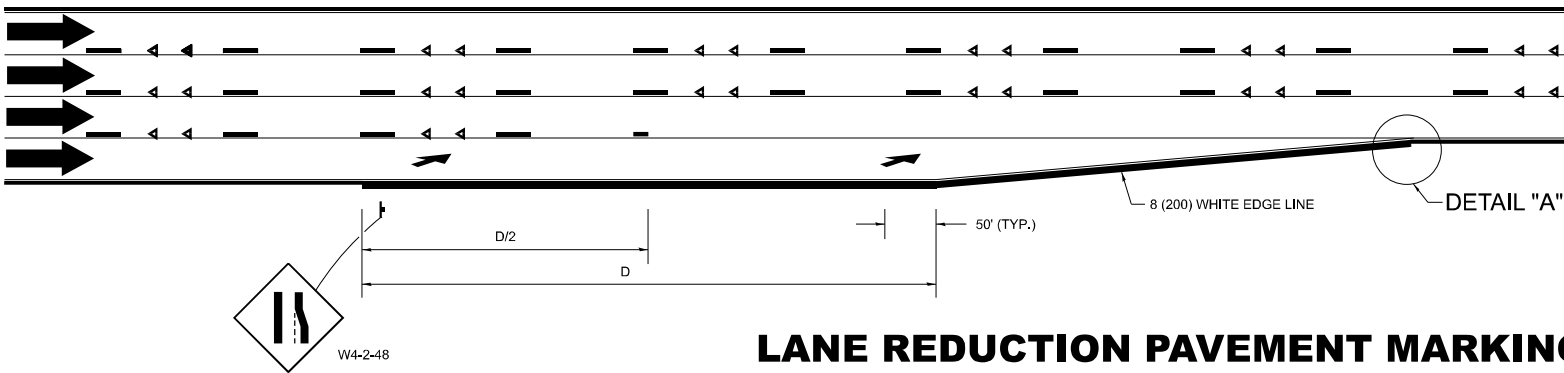
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE D, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



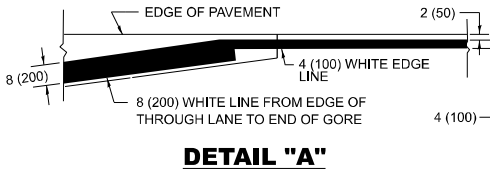
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS



NOTES:

- 1 THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- 2 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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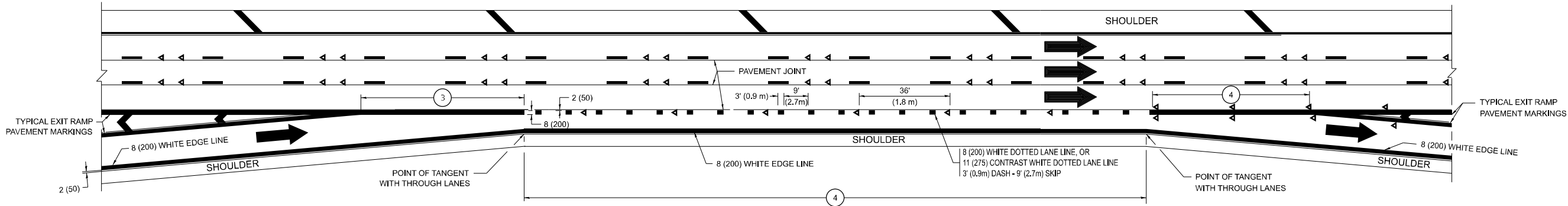
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	DRAWN - M.D. 05-13	REVISED - M.D. 05-13
	CHECKED -	REVISED - M.D. 09-17
PLOT DATE = 1/28/2025	DATE - 01-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

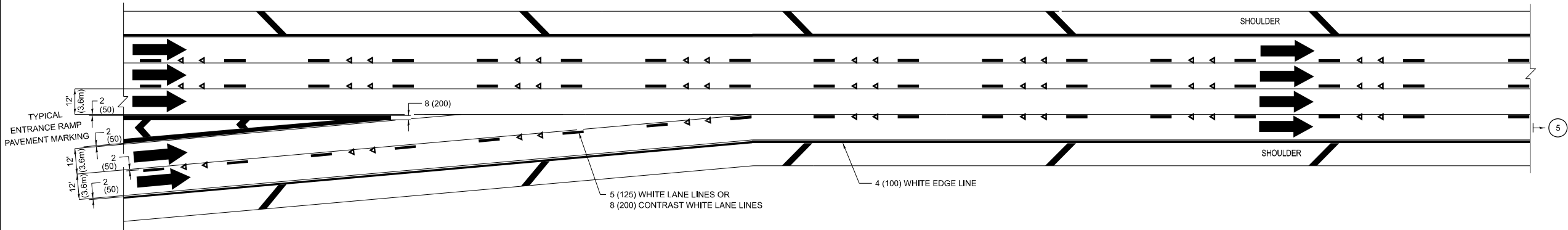
MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

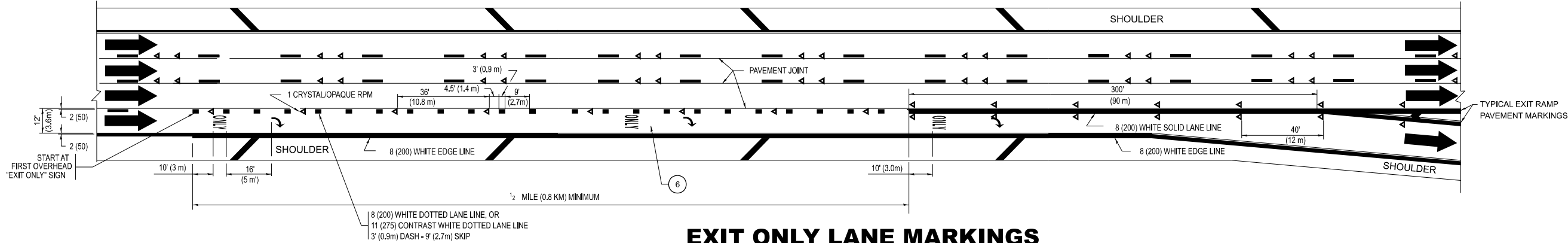
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TC-12		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		



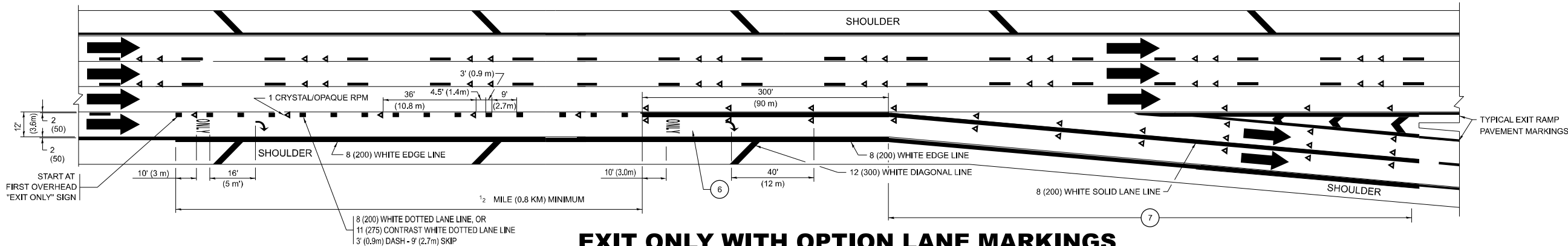
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

NOTES:

- 3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
- 4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
- 5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
- 6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED..
- 7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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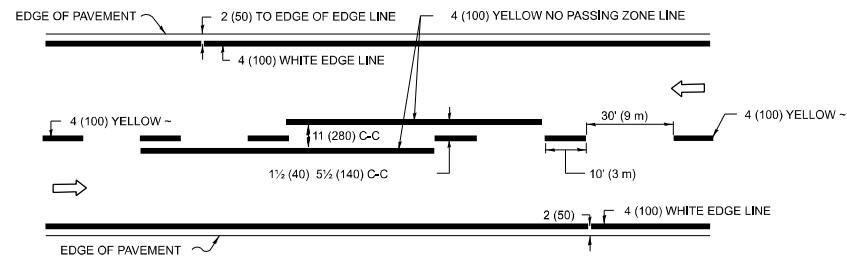
PLOT DATE	USER NAME	DESIGNED	REVISED
	= connor.mullane	- D.W.S.	- J.A.F. 02-06
		DRAWN	REVISED
		-	- S.P.B. 01-07
DATE	CHECKED	REVISED	
	-	- S.P.B. 01-10	
		REVISED	
		- M.D. 09-17	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

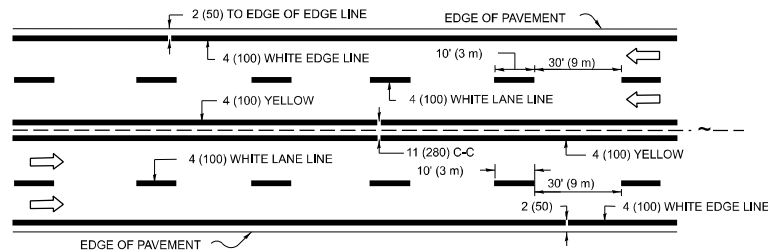
MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

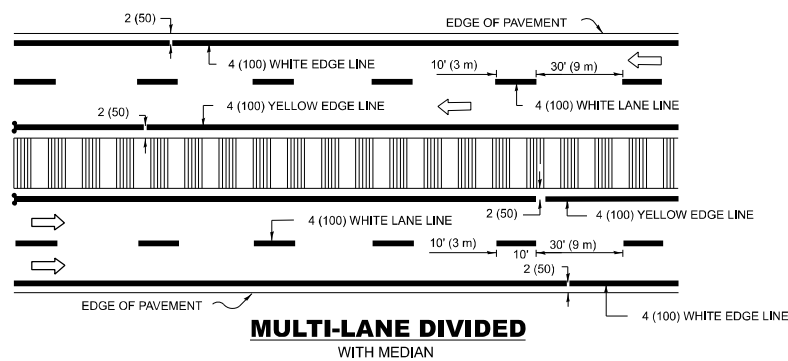
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TC-12		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		



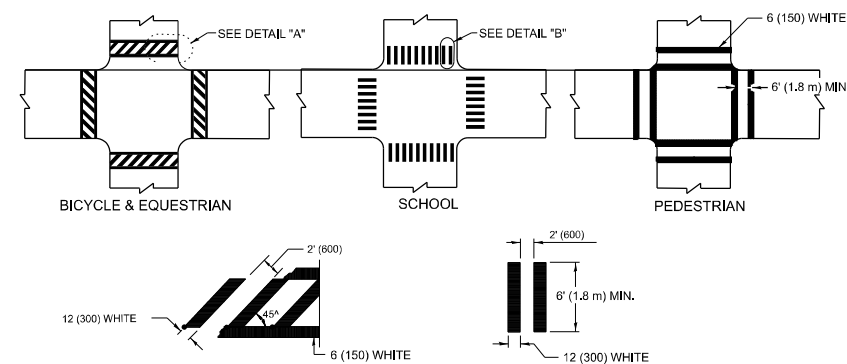
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING

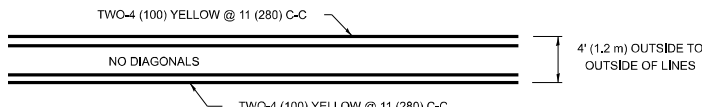


DETAIL "A"

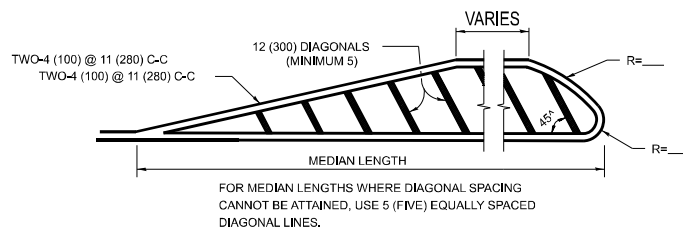
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

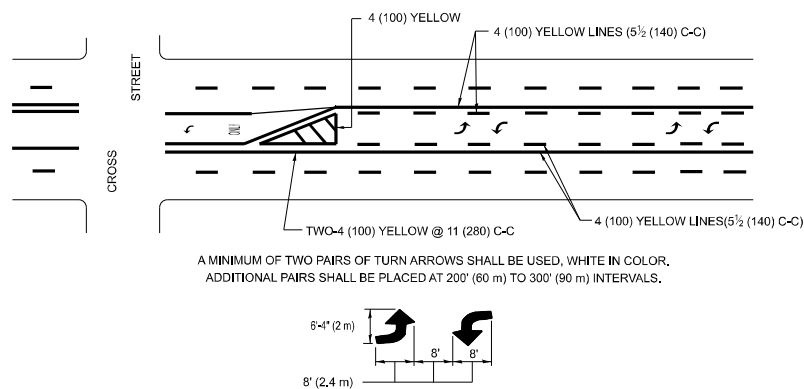


4' (1.2 m) WIDE MEDIANS ONLY



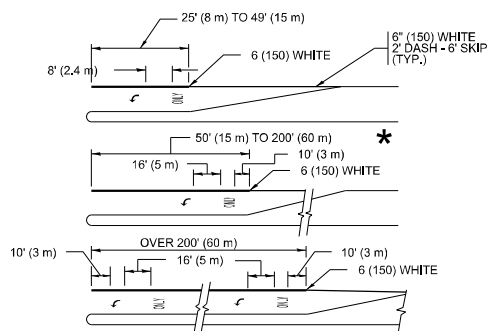
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

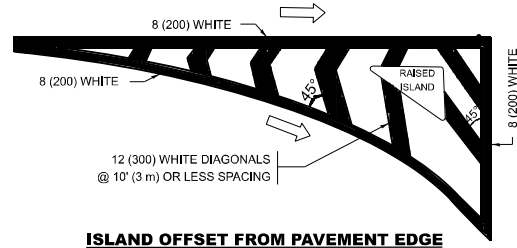
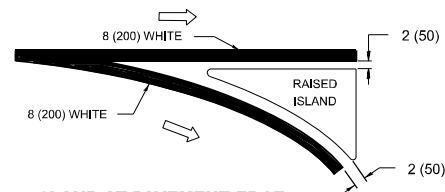


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

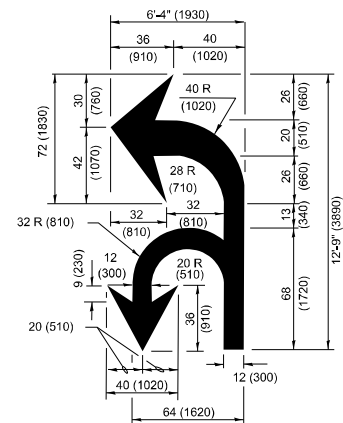
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

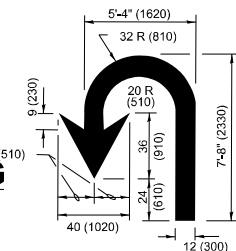
**ISLAND OFFSET FROM PAVEMENT EDGE****ISLAND AT PAVEMENT EDGE**

20 (5) TYPICAL ISLAND MARKING



COMBINATION

LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (6' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16,3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = connor,mullane	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 1/28/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS				
SCALE: NONE	SHEET 1	OF 1	SHEETS	STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	37
TC-13		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

MODEL: TC-13 [Sheet]
FILE NAME: c:\pw work\pwwidott\connor.mullane@illinois.gov\0914281\1D103424-sh1-DistStds.dgn

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

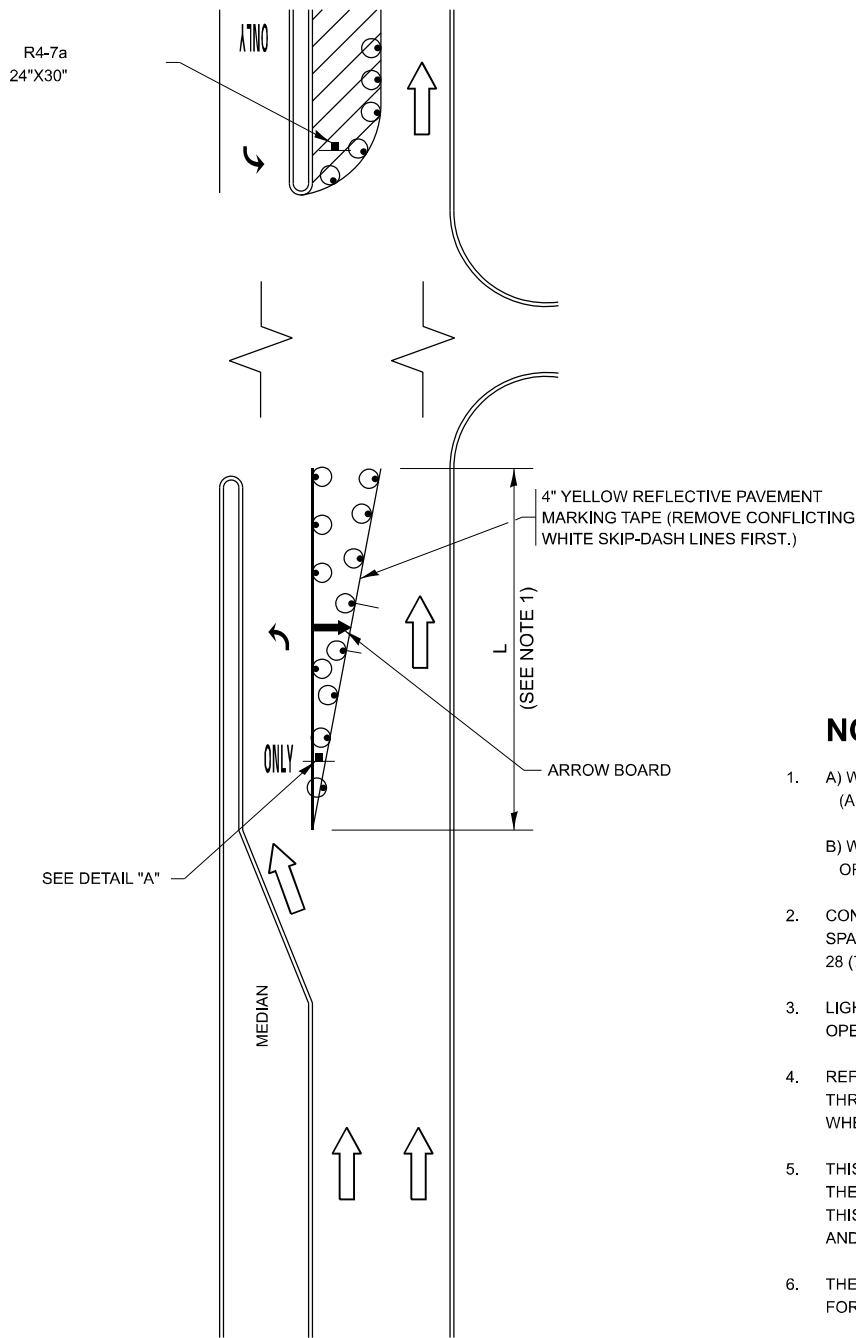
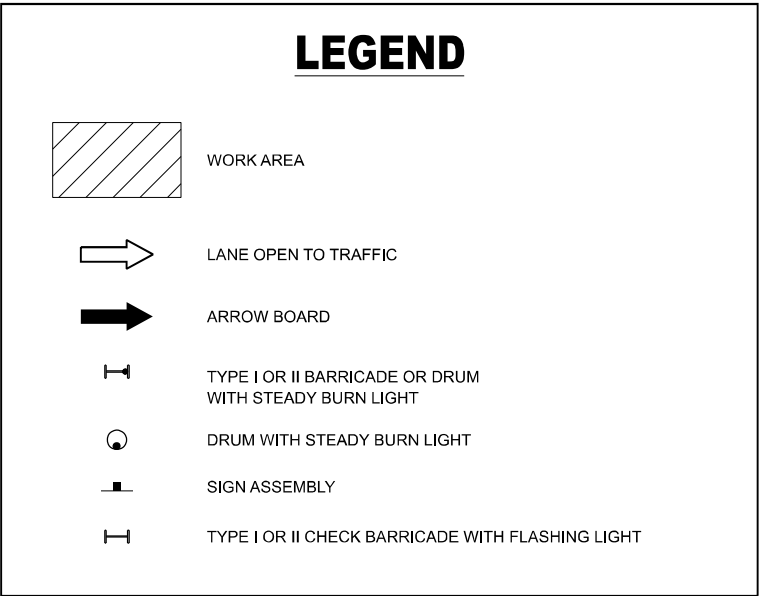


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

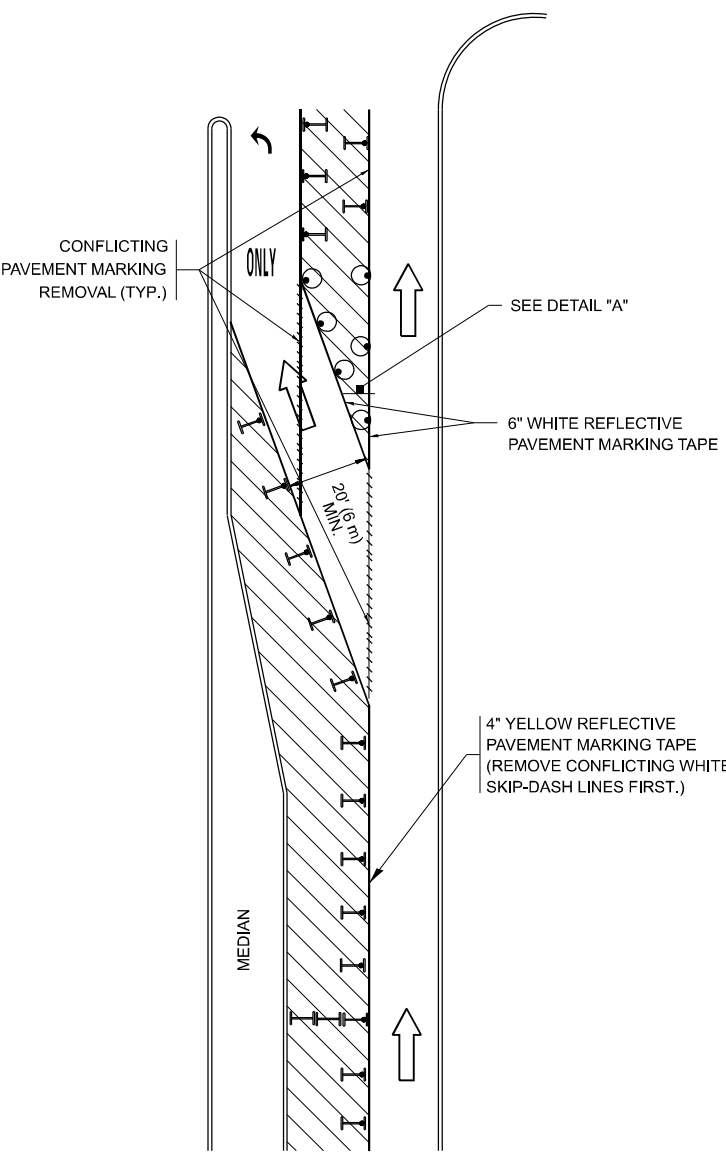
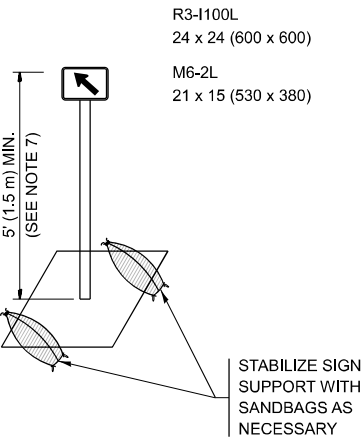


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-14 (Sheet)
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USER NAME	= connor,mullane
DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
CHECKED	- A. HOUSEH 10-12-96
PLOT DATE	= 1/28/2025
DATE	- T. RAMMACHER 01-06-00

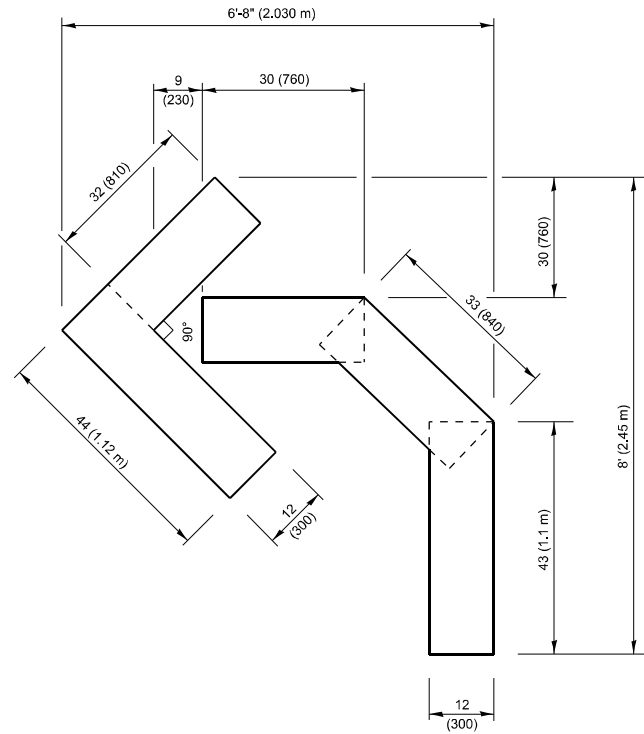
REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

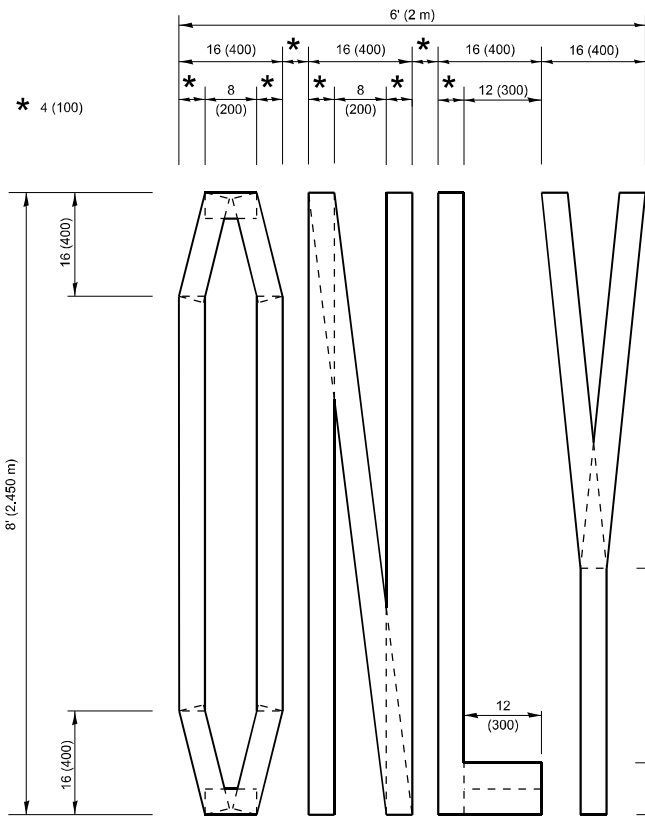
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	38
TC-14		CONTRACT NO. 62V56		
		ILLINOIS FED. AID PROJECT		



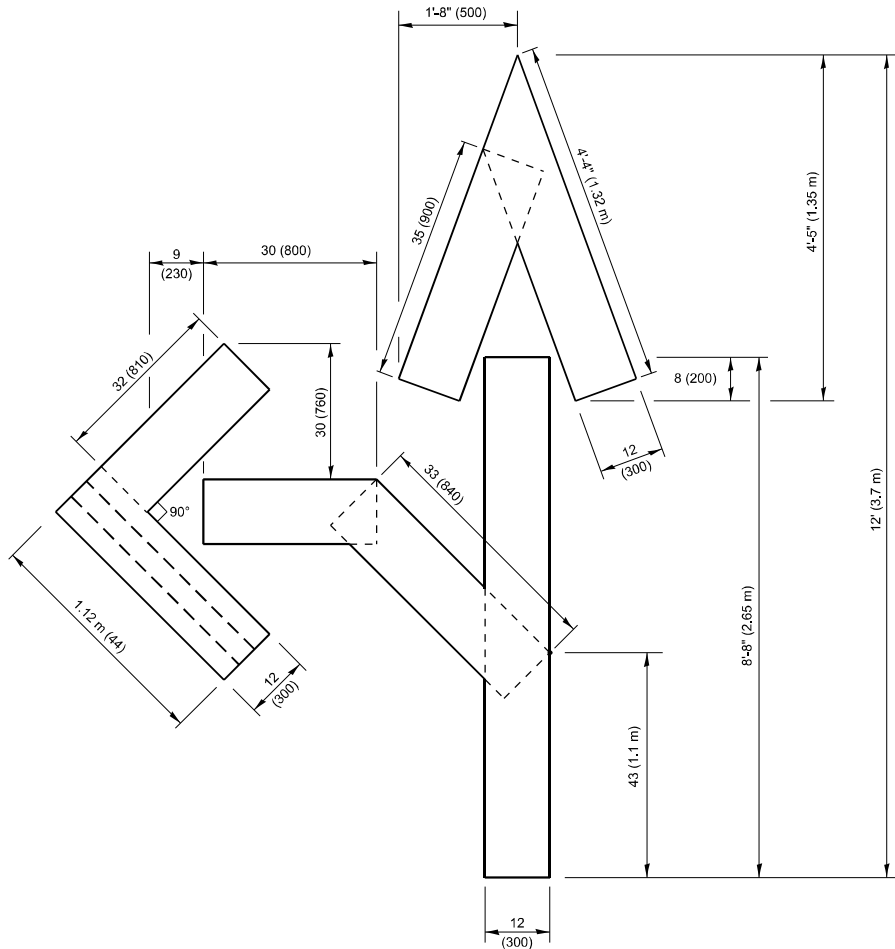
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

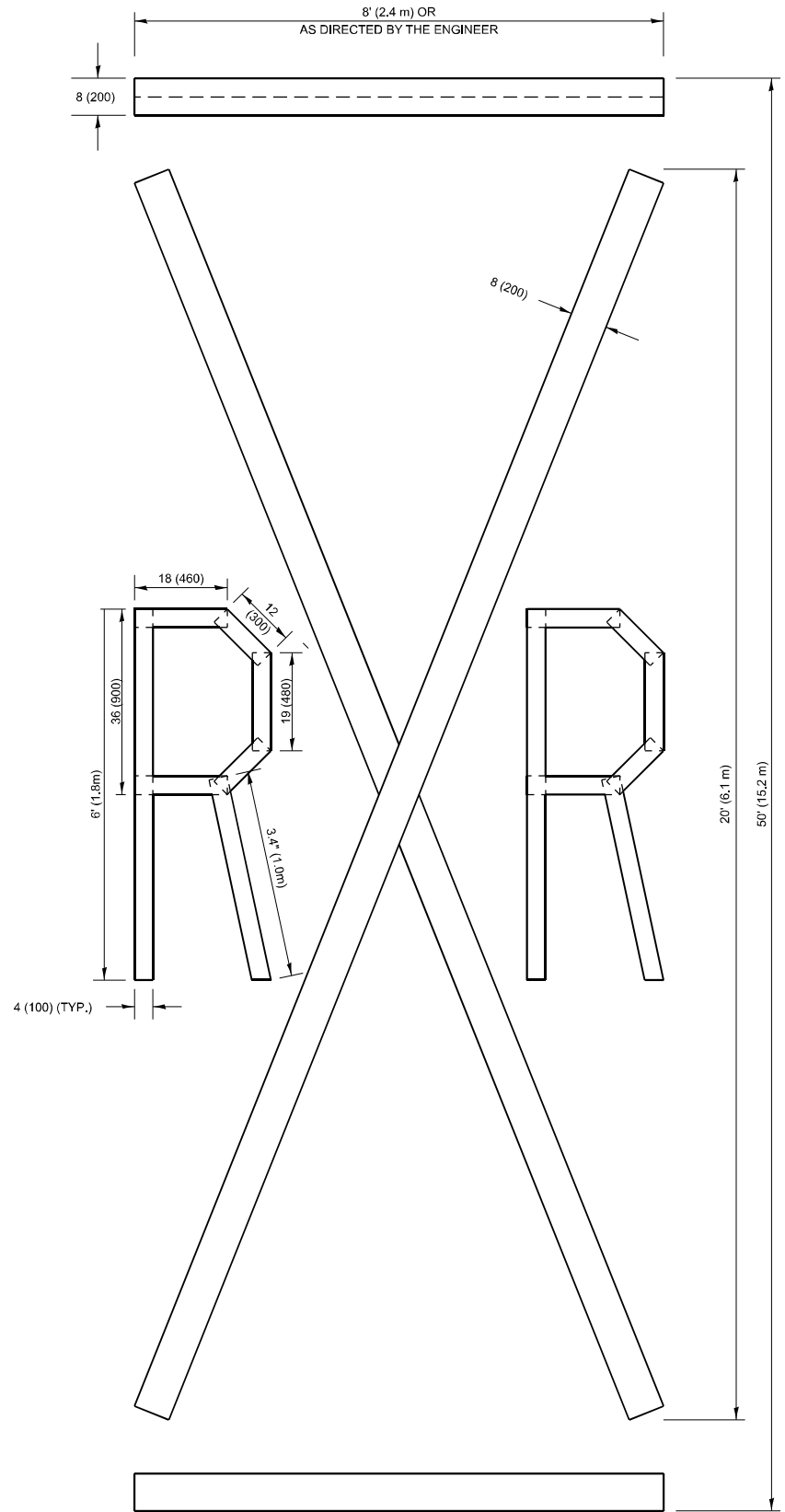


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

MODEL: TC-16 (Sheet)
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		DRAWN	-	REVISED	- E. GOMEZ 08-28-00
		CHECKED	-	REVISED	- E. GOMEZ 08-28-00
PLOT DATE	= 1/28/2025	DATE	- 09-18-94	REVISED	- A. SCHUETZE 09-15-16

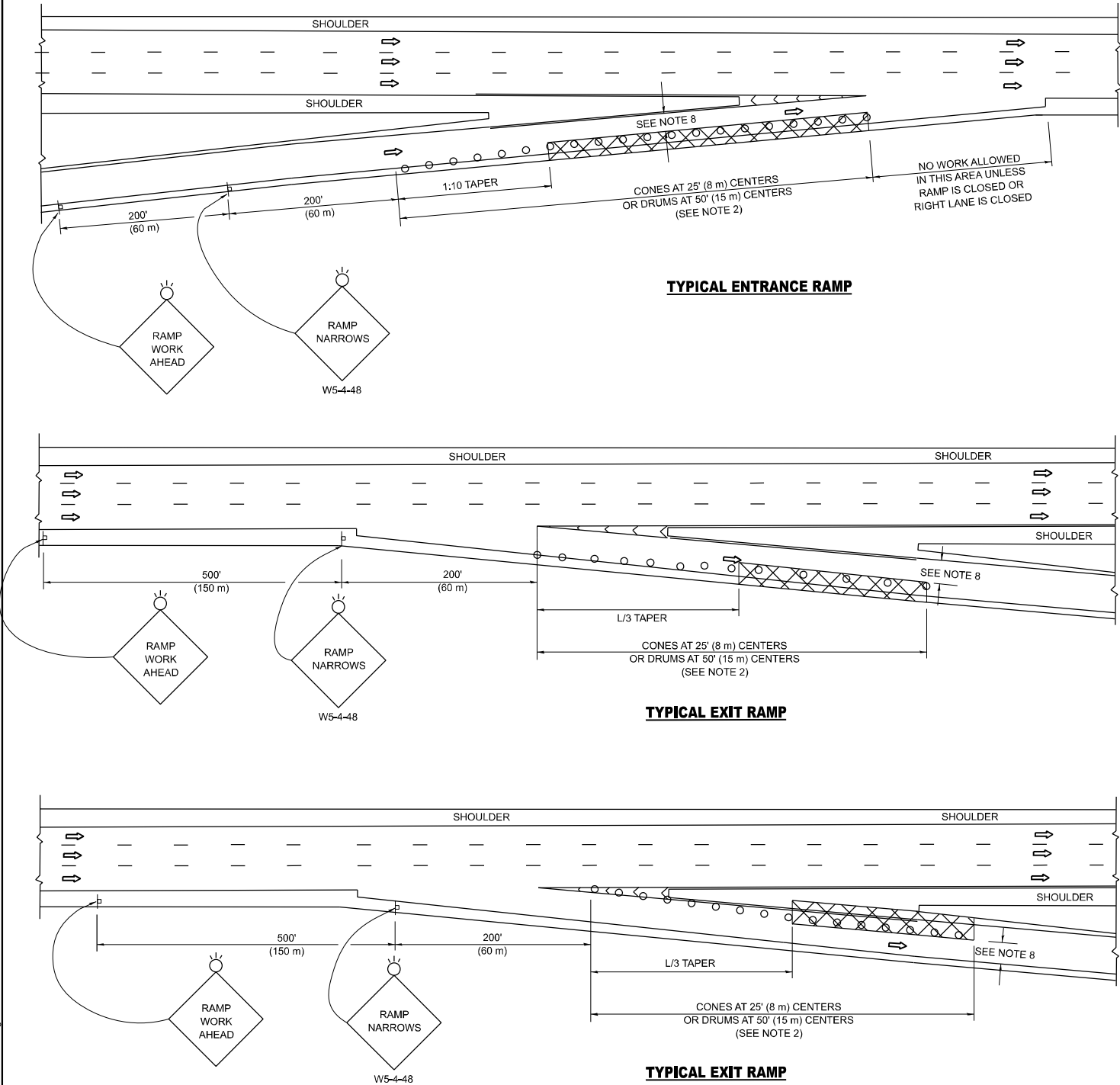
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

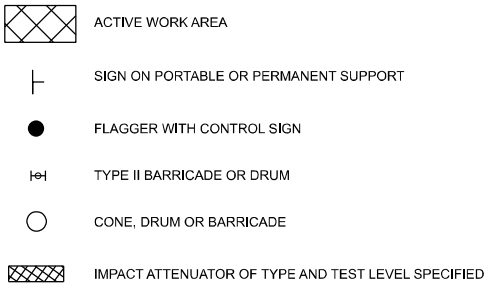
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	39
TC-16		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

PARTIAL RAMP CLOSURE DETAILS



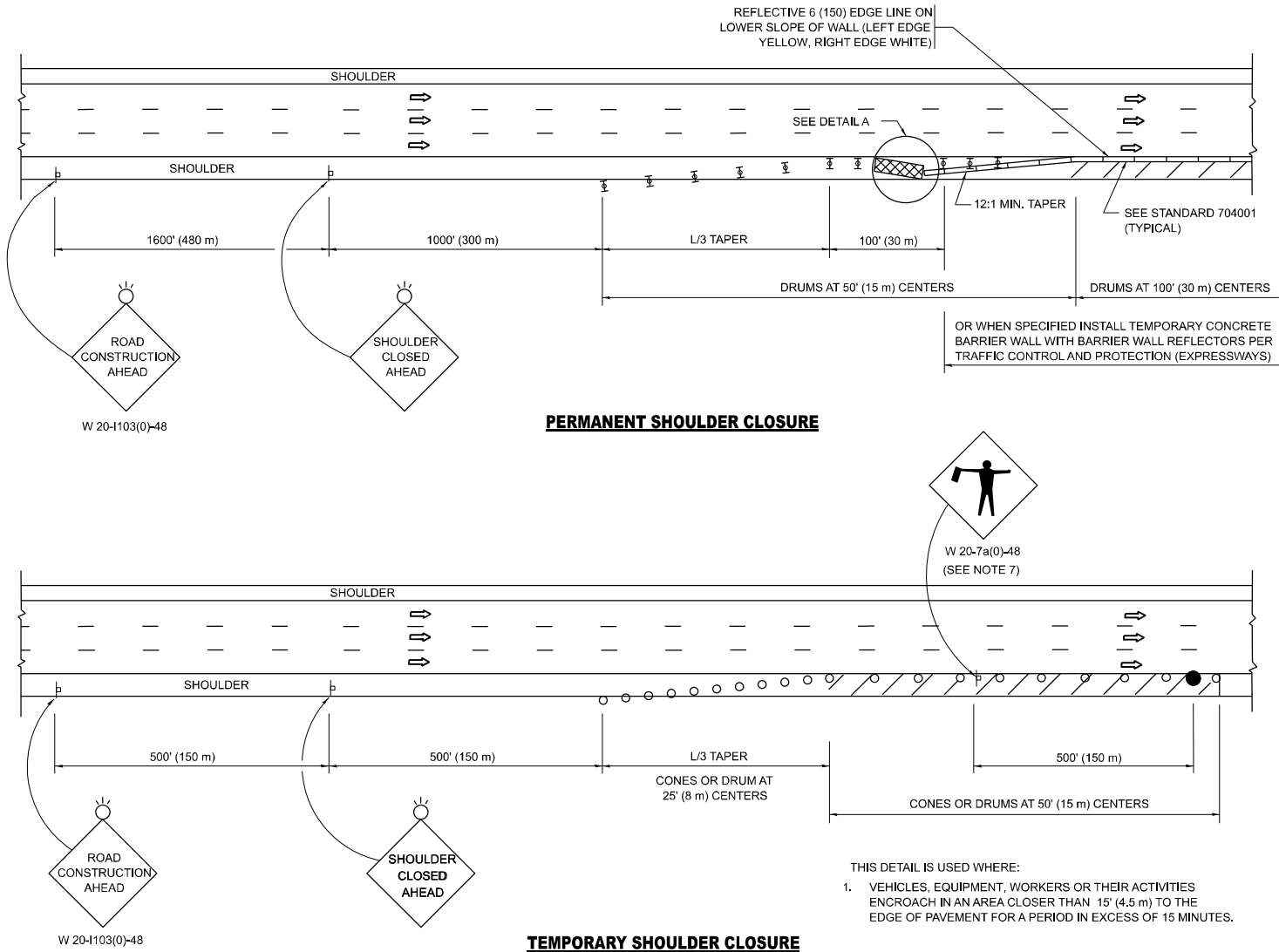
SYMBOLS



GENERAL NOTES:

- THE "L" DISTANCE EQUALS:
SPEED LIMIT FORMULAS
45 mph (80 km/h) METRIC ENGLISH
OR GREATER: L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

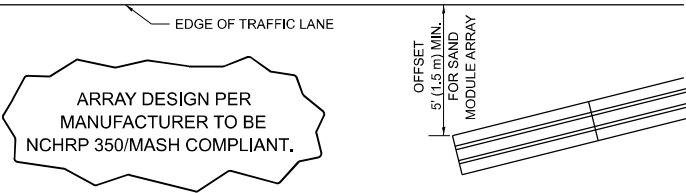
SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE

TEMPORARY SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
b. THE WORK ACTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.
- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

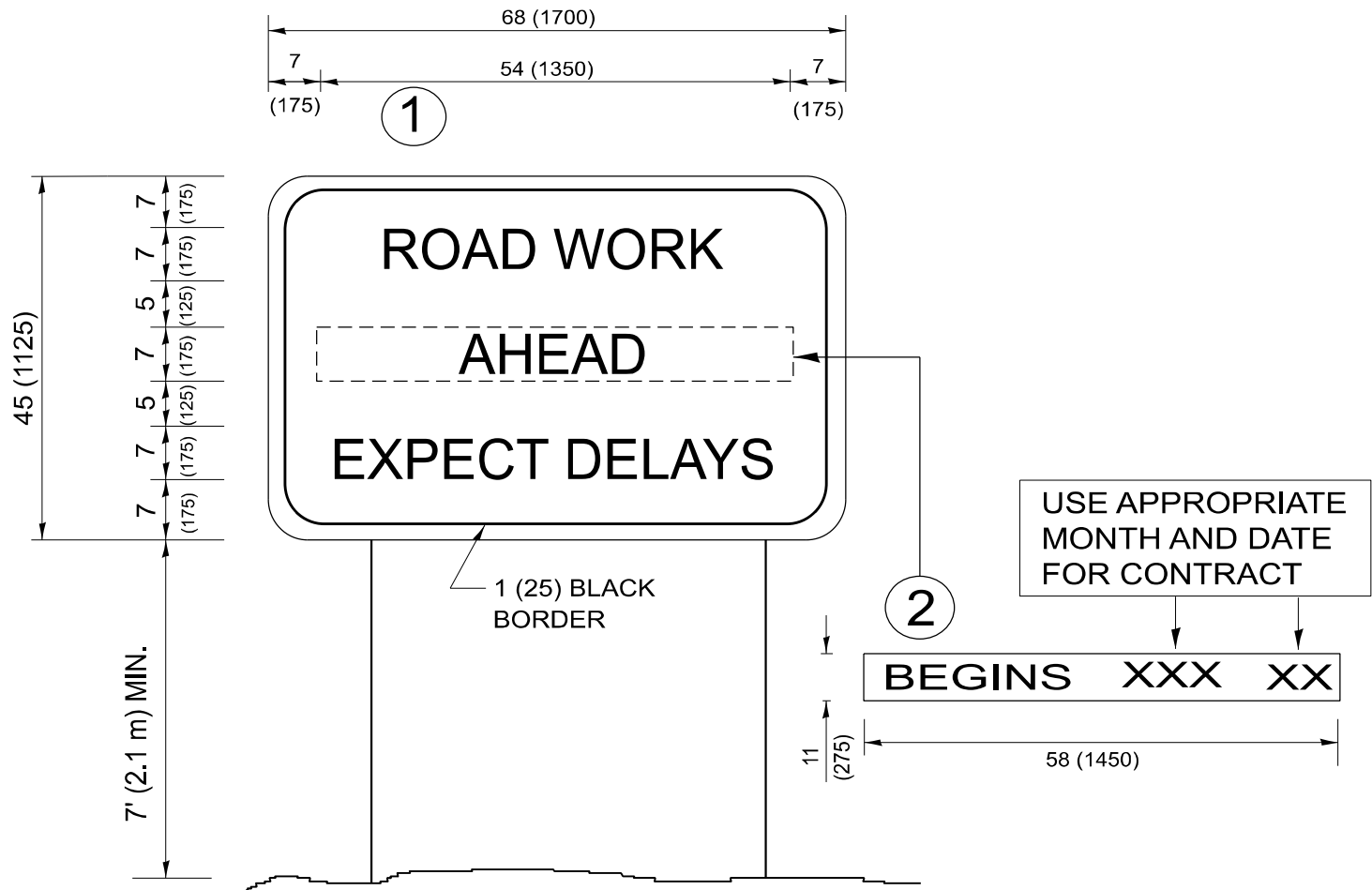
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	40
TC-17		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

MODEL: TC-22 [Sheet]
FILE NAME: c:\p\work\pwt\tdt\connor.mullane@illinois.gov\0914281\ID103424-eh-DistSigs.dgn

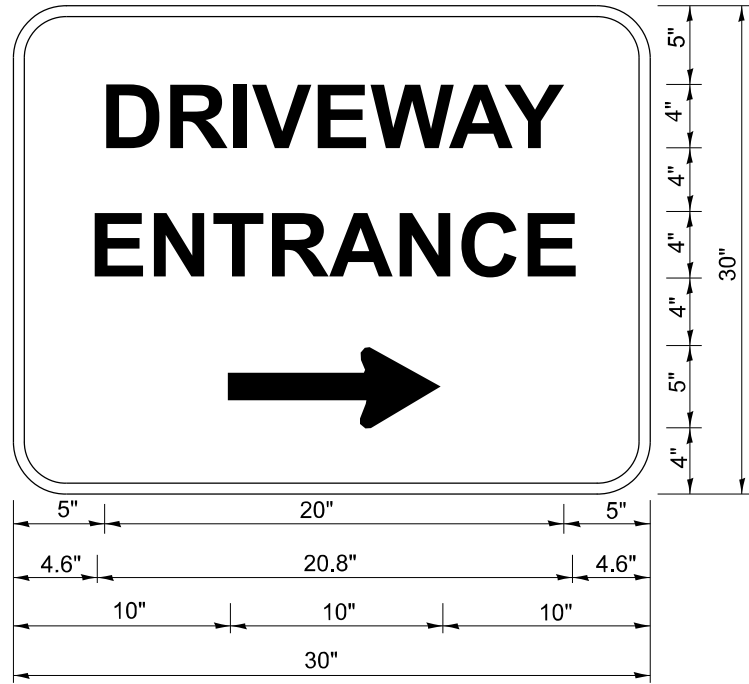


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = connor.mullane	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97						372	FAP 0372 23 SMART	COOK	50	41
		CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22		CONTRACT NO. 62V56		
	PLOT DATE = 1/28/2025	DATE -	REVISED - C. JUCIUS 01-31-07		ILLINOIS FED. AID PROJECT								
						SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:**
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: tc-26 [Sheet]
FILE NAME: c:\p\work\pwt\concor.mullane@illinois.gov\d0914281\ID103424-eh-DistStd.dgn

USER NAME = connor,mullane	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 1/28/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	42
TC-26		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

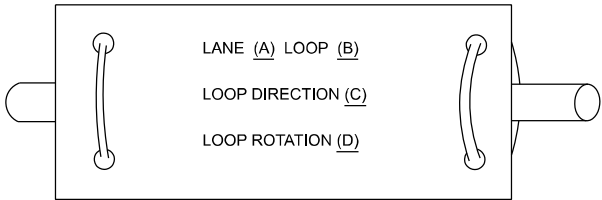
ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD	 	
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE	 	
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"	 	
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

	USER NAME = connor.mullane	DESIGNED - IP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN - IP	REVISED -		SCALE: NONE	SHEET 1	OF 7 SHEETS	STA.	TO STA.	372	FAP 0372 23 SMART	COOK	50	43
		CHECKED - LP	REVISED -		TS-05									
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ILLINOIS FED. AID PROJECT														

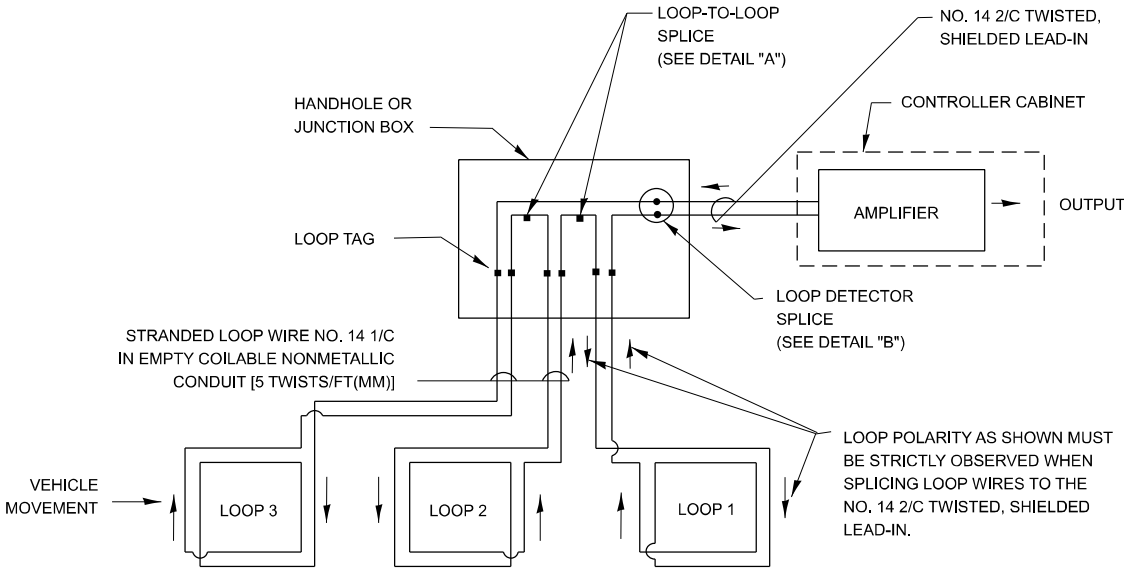
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE, SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE
7. PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

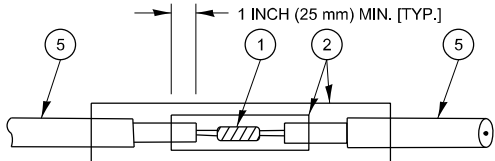


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

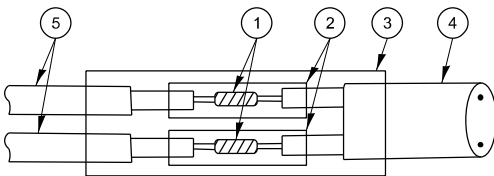


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

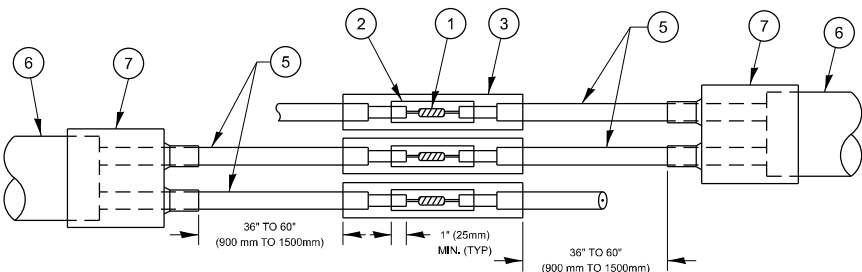


DETAIL "A"
LOOP-TO-LOOP SPLICE

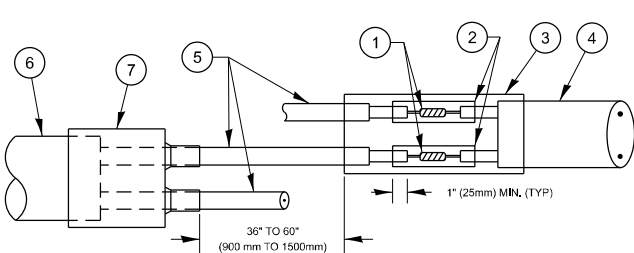


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

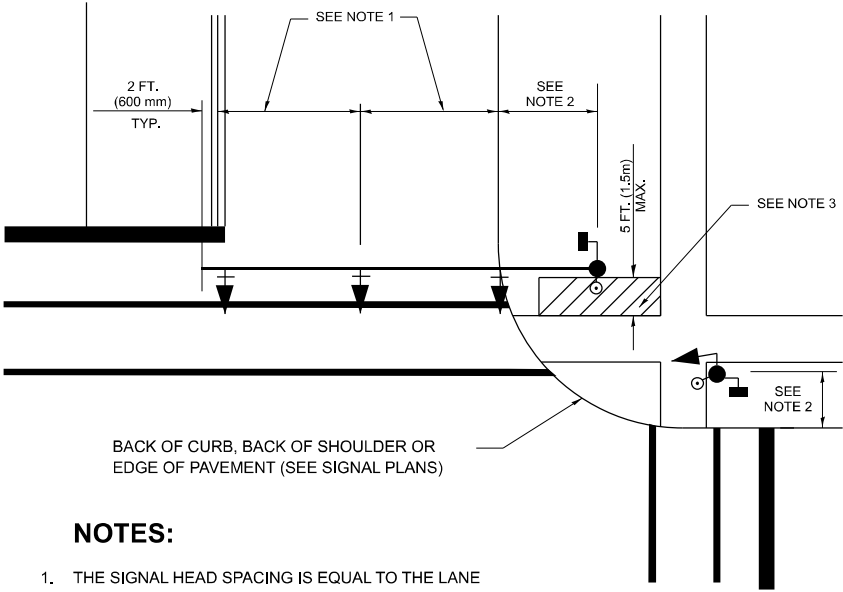
1. WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
2. WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
3. WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
4. NO. 14 2/C TWISTED, SHIELDED CABLE.
5. LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
6. XL POLYOLEFIN 2 CONDUCTOR
7. BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

MODEL: TS-05-2 (Sheet)
FILE NAME: c:\p\work\pwtol\connor.mullane@illinois.gov\d0914281\D103424-eh-DistStdts.dgn

	USER NAME = connor.mullane	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					372	FAP 0372 23 SMART	COOK	50	44
		CHECKED -	REVISED -					TS-05		CONTRACT NO. 62V56		
	PLOT DATE = 1/28/2025	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
	SCALE: NONE		SHEET 2					OF 7	SHEETS	STA.	TO STA.	

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

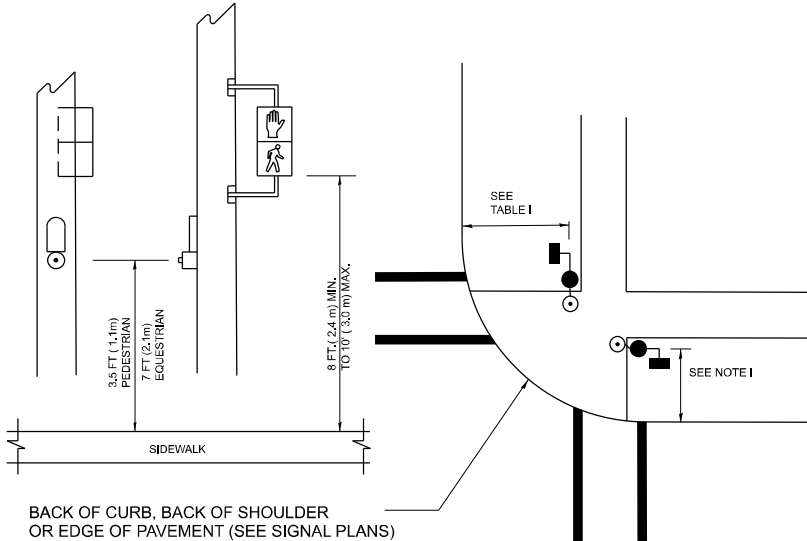
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

- 1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

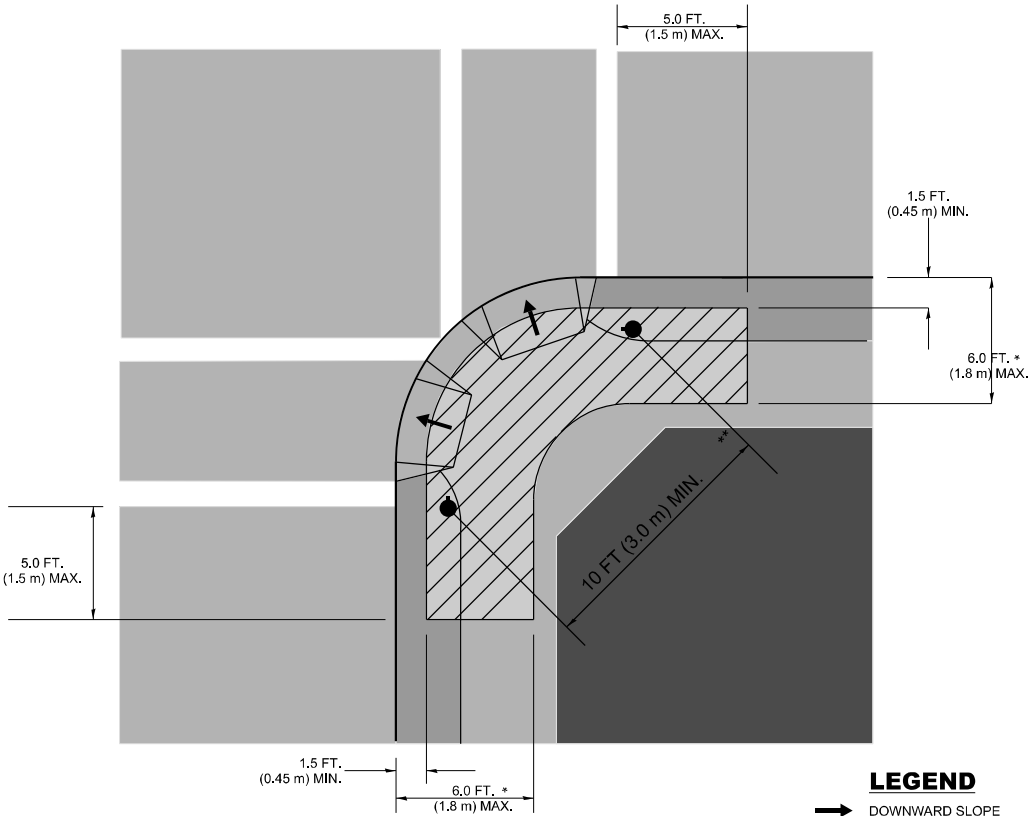
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- 1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- 2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

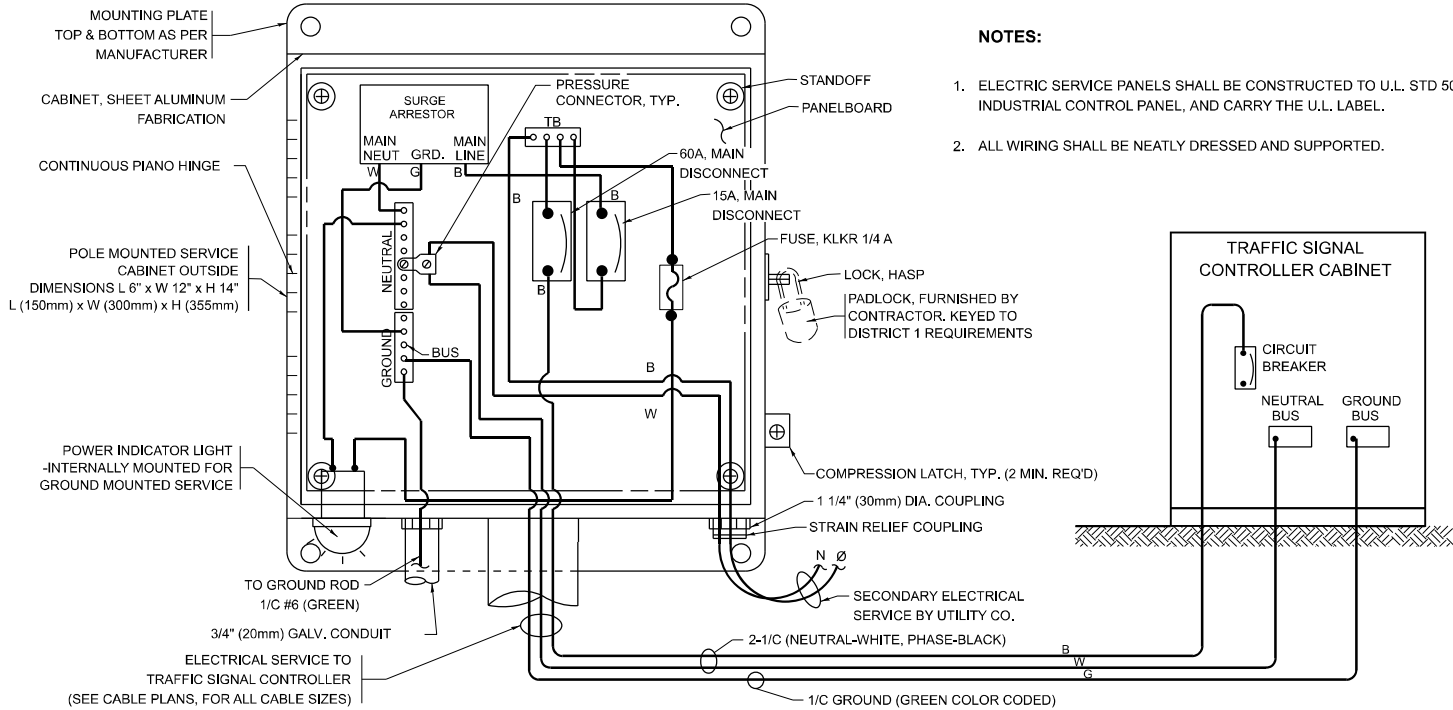
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

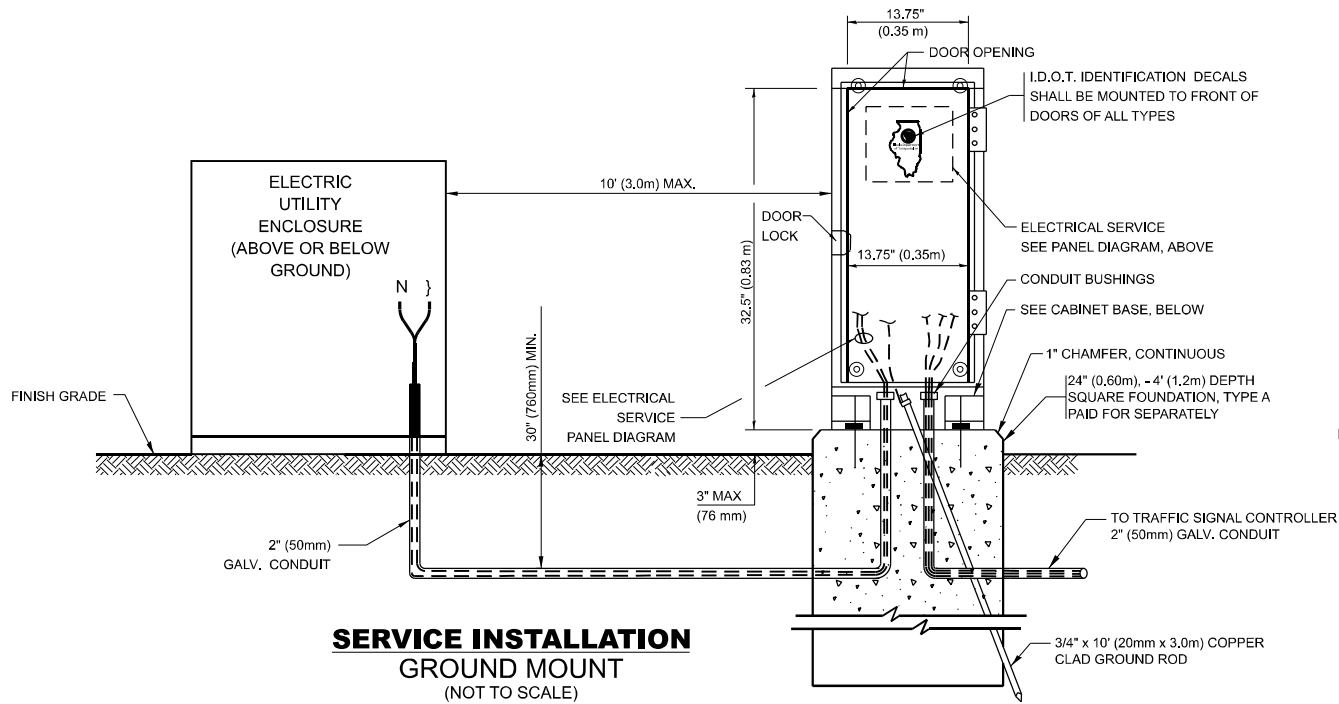
- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

MODEL: TS-05-3 (Sheet)
FILE NAME: c:\p\work\lwtol\connor.mullane@illinois.gov\0914281\1D103424-4gh-DistStd.dgn

USER NAME = connor.mullane	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
	DRAWN -	REVISED -												
	CHECKED -	REVISED -												
PLOT DATE = 1/28/2025	DATE -	REVISED -												
				SCALE: NONE	SHEET 3	OF 7	SHEETS	STA.	TO STA.	372	FAP 0372 23 SMART	COOK	50	45
								TS-05		CONTRACT NO. 62V56				
										ILLINOIS FED. AID PROJECT				

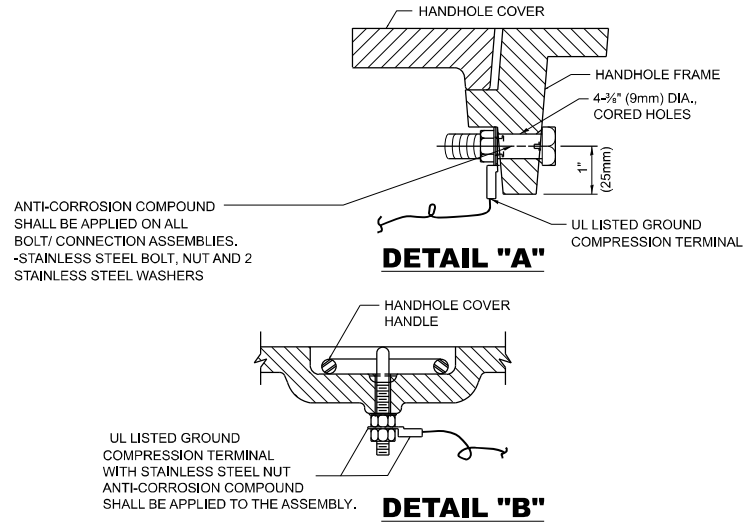
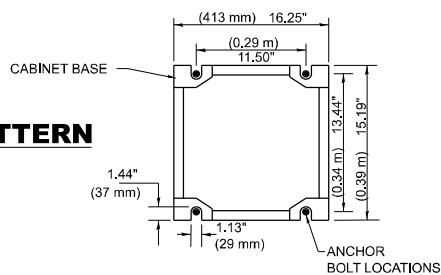


ELECTRICAL SERVICE - PANEL DIAGRAM
(TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)

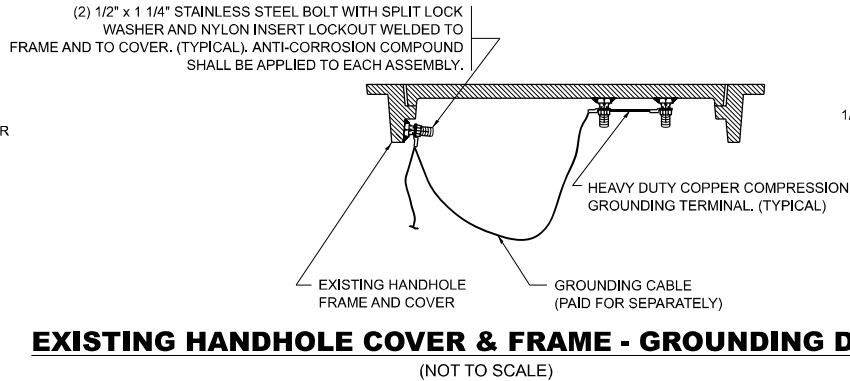


SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET - BASE BOLT PATTERN
(NOT TO SCALE)



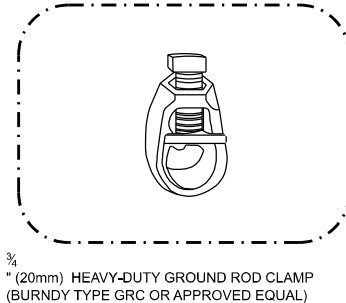
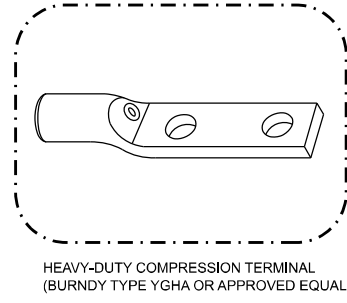
HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)



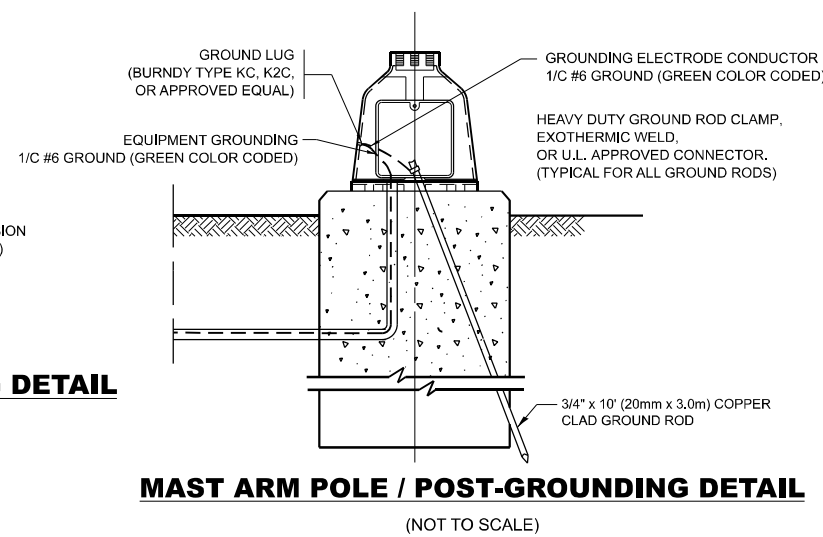
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)

NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



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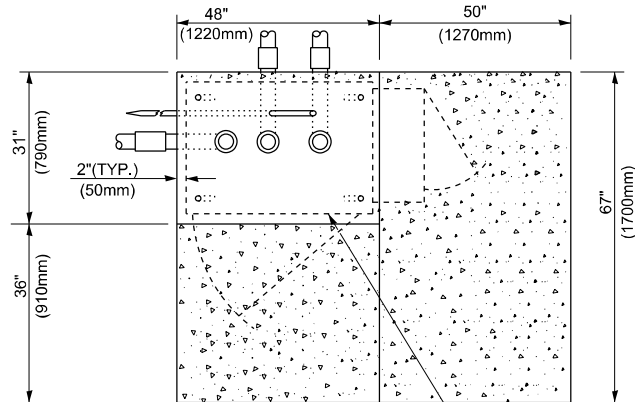
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	DRAWN -	REVISED -
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PLOT DATE = 1/28/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

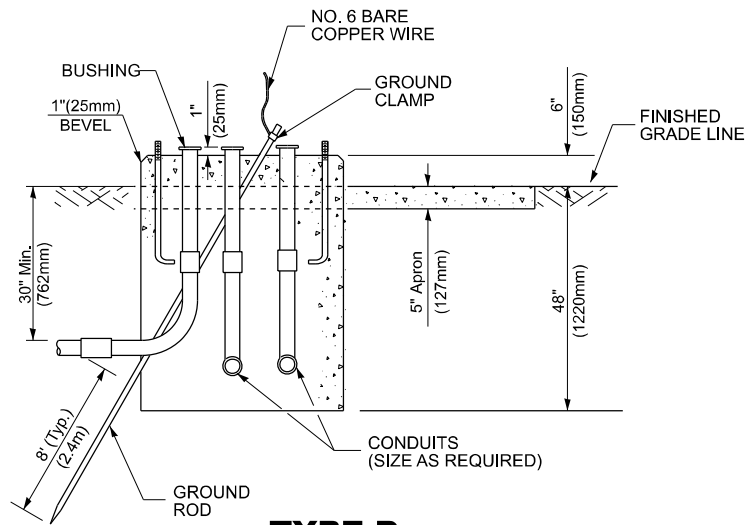
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

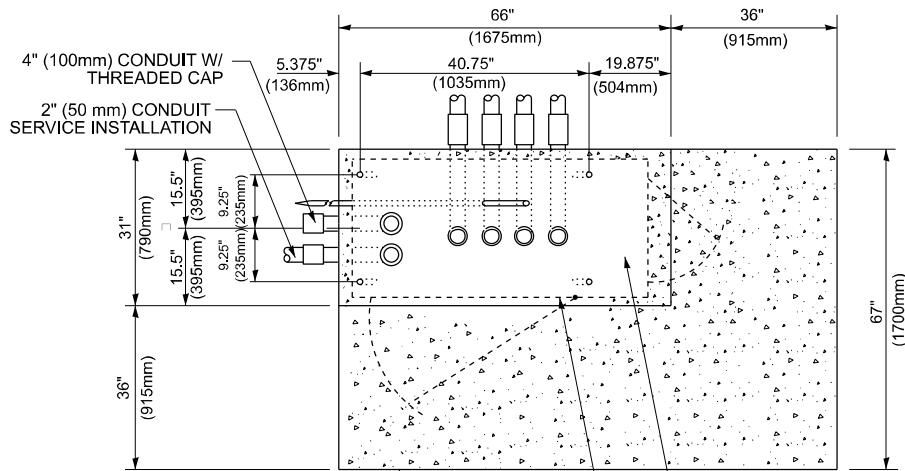
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372	FAP 0372 23 SMART	COOK	50	46
TS-05		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		



TOP VIEW



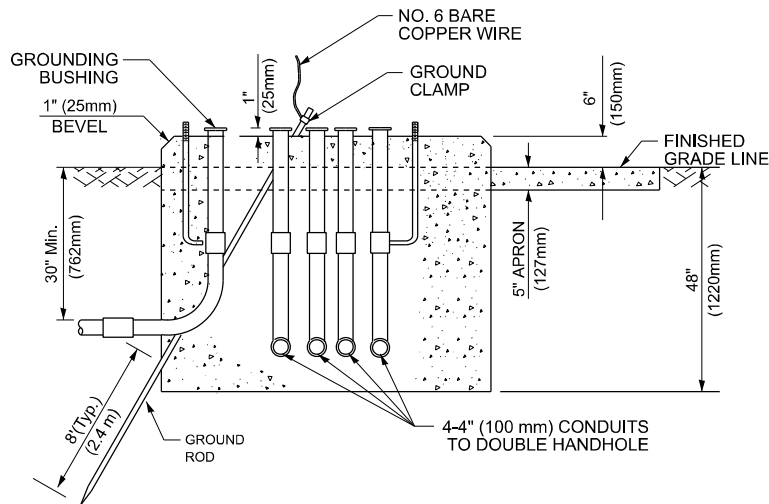
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



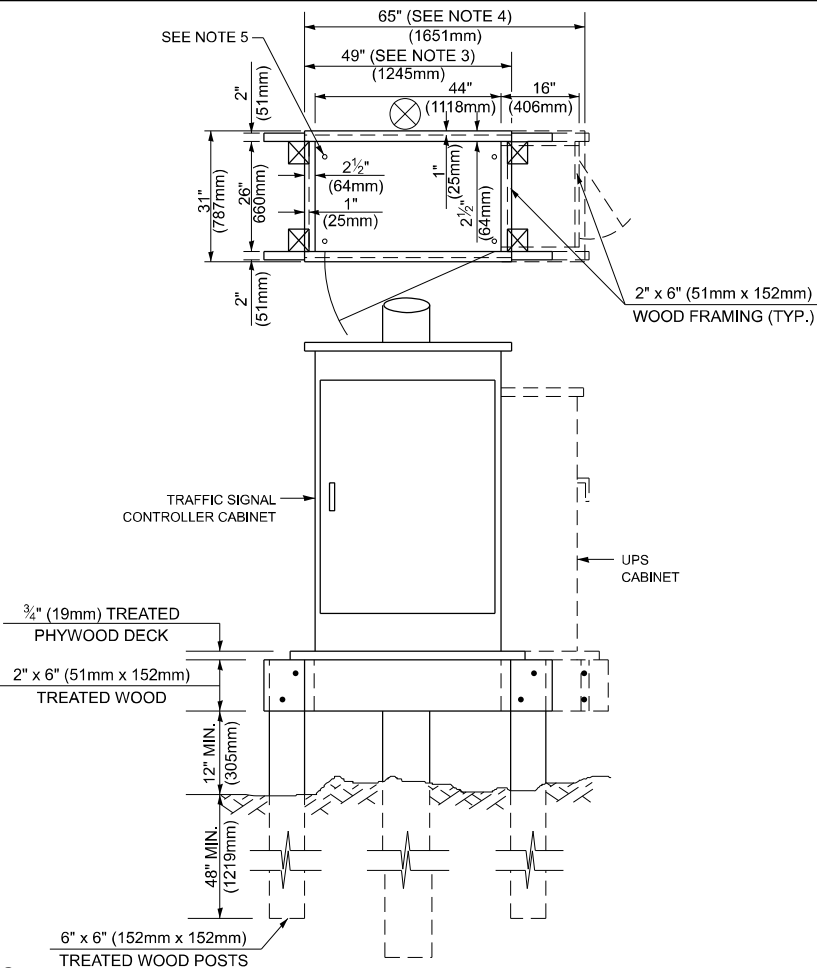
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

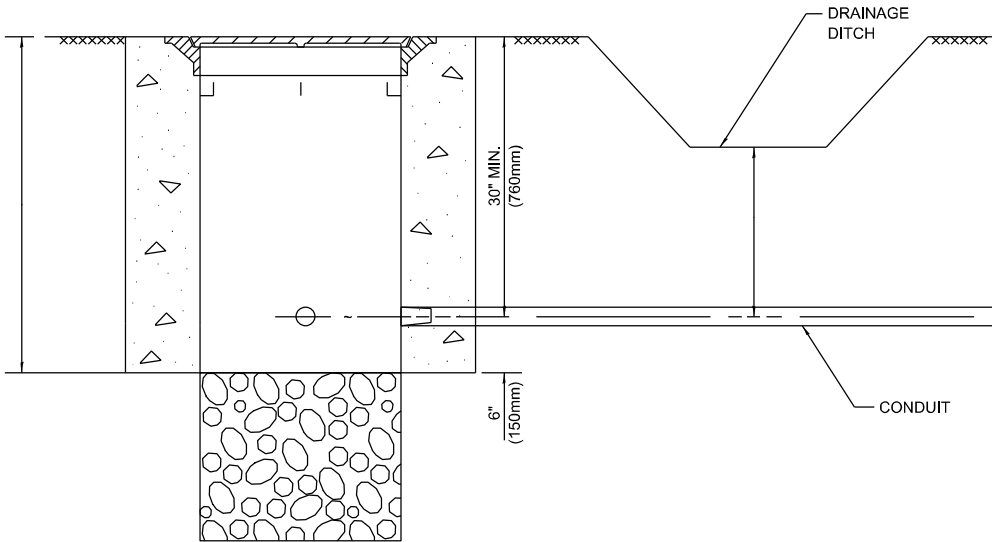
NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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		DRAWN -	REVISED -						372	FAP 0372 23 SMART	COOK	50	47
		CHECKED -	REVISED -						TS-05		CONTRACT NO. 62V56		
	PLOT DATE = 1/28/2025	DATE -	REVISED -						ILLINOIS		FED. AID PROJECT		
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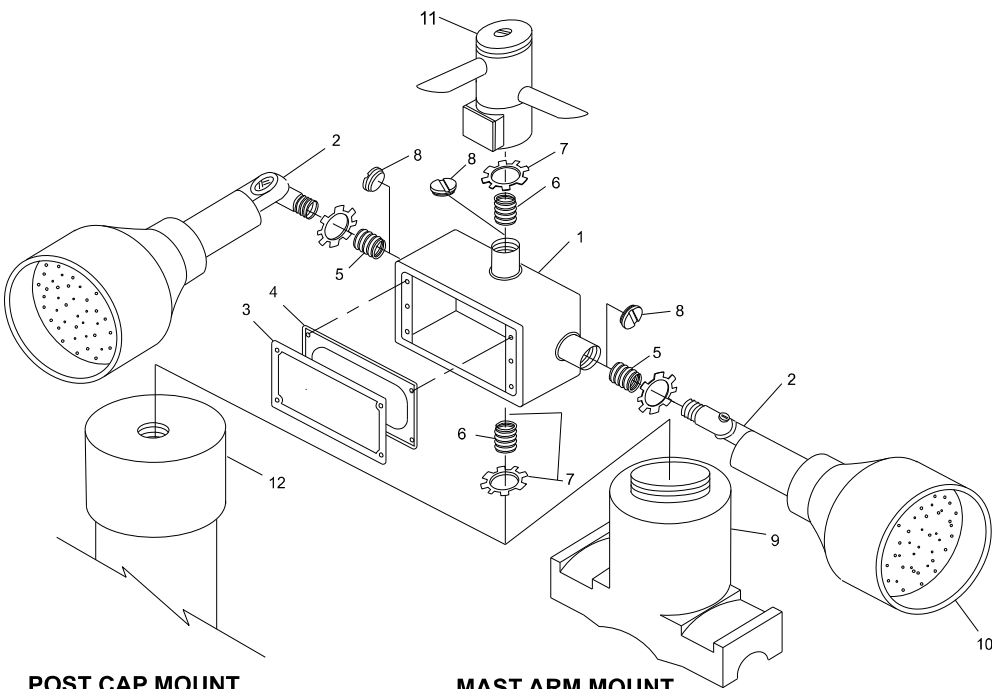


NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

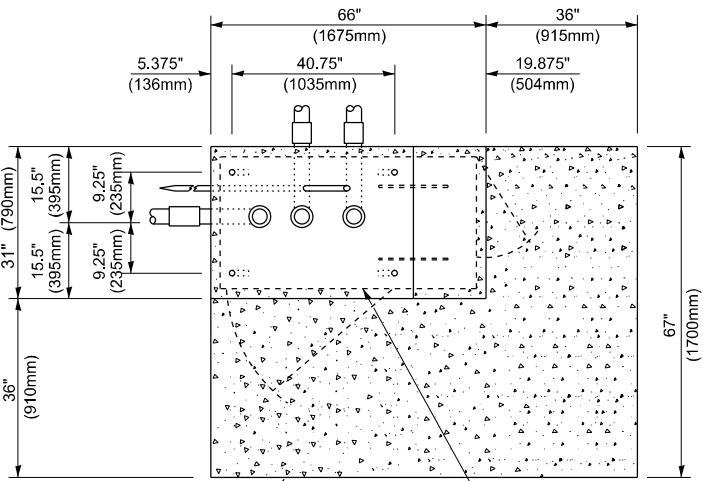
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POST CAP MOUNT

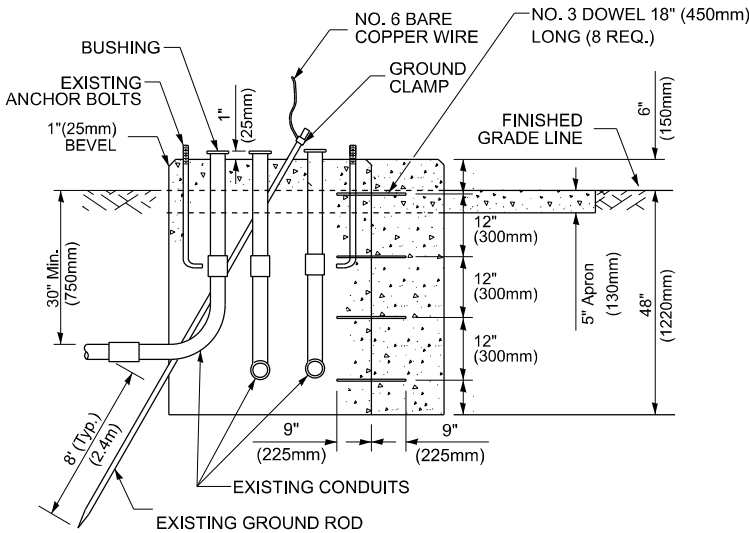
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW

(NOT TO SCALE)



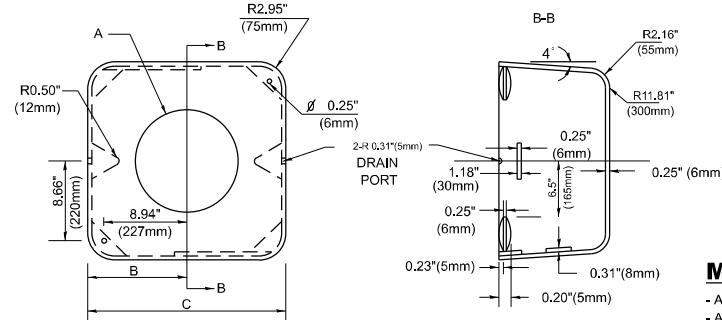
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

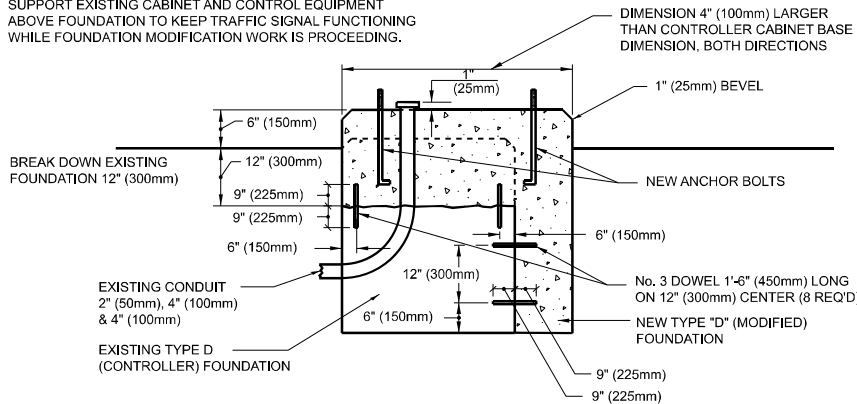
SHROUD

NOTES:

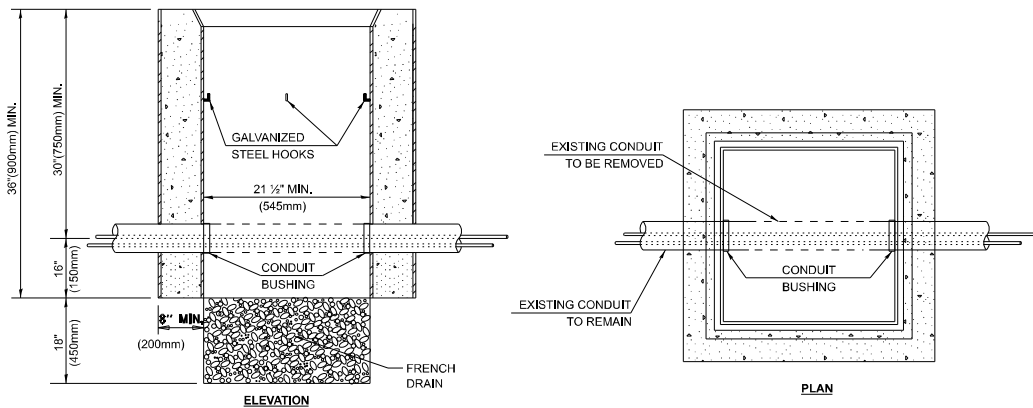
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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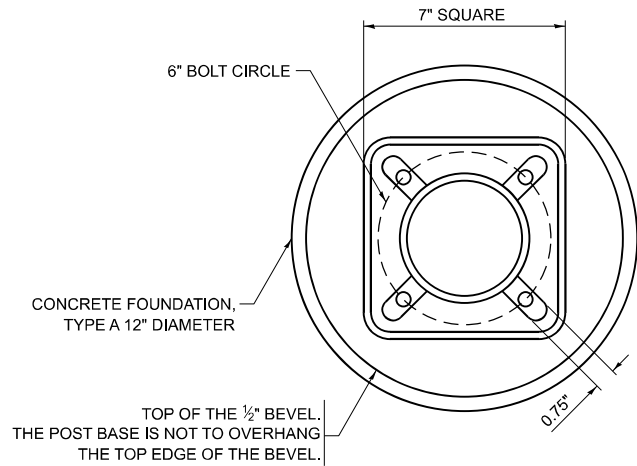
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

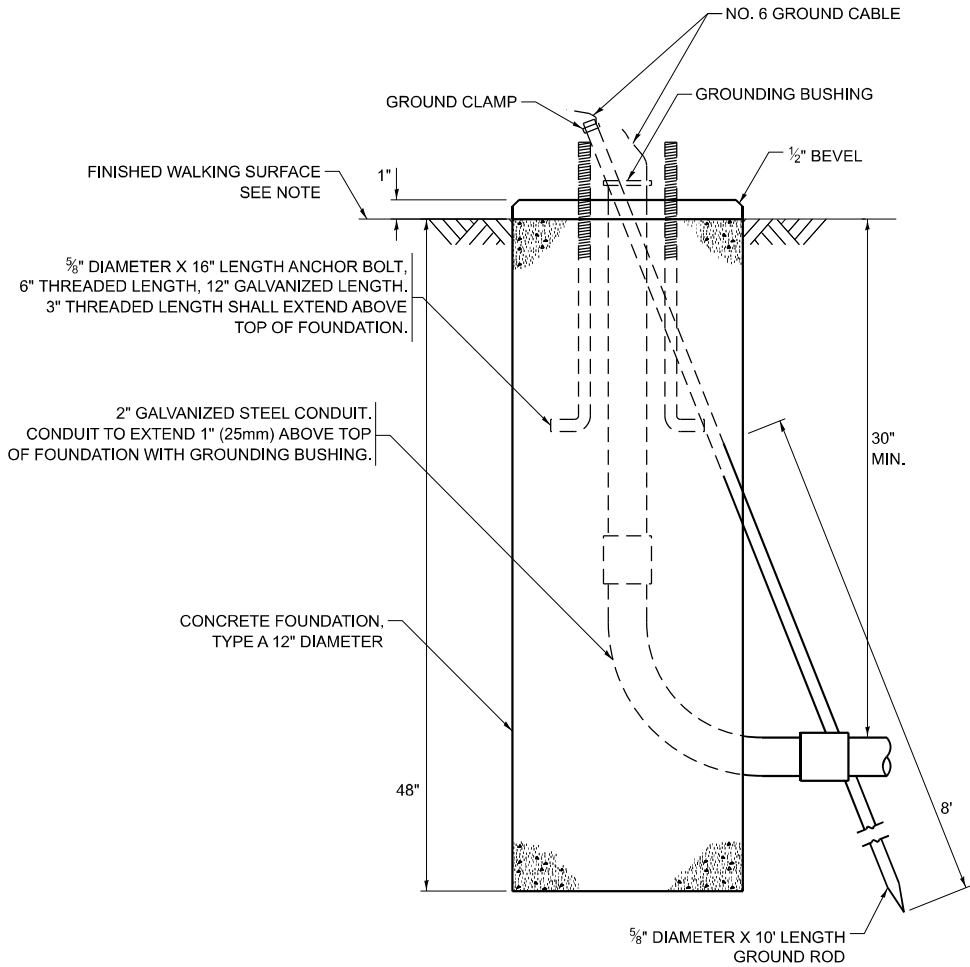
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TS-05		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		



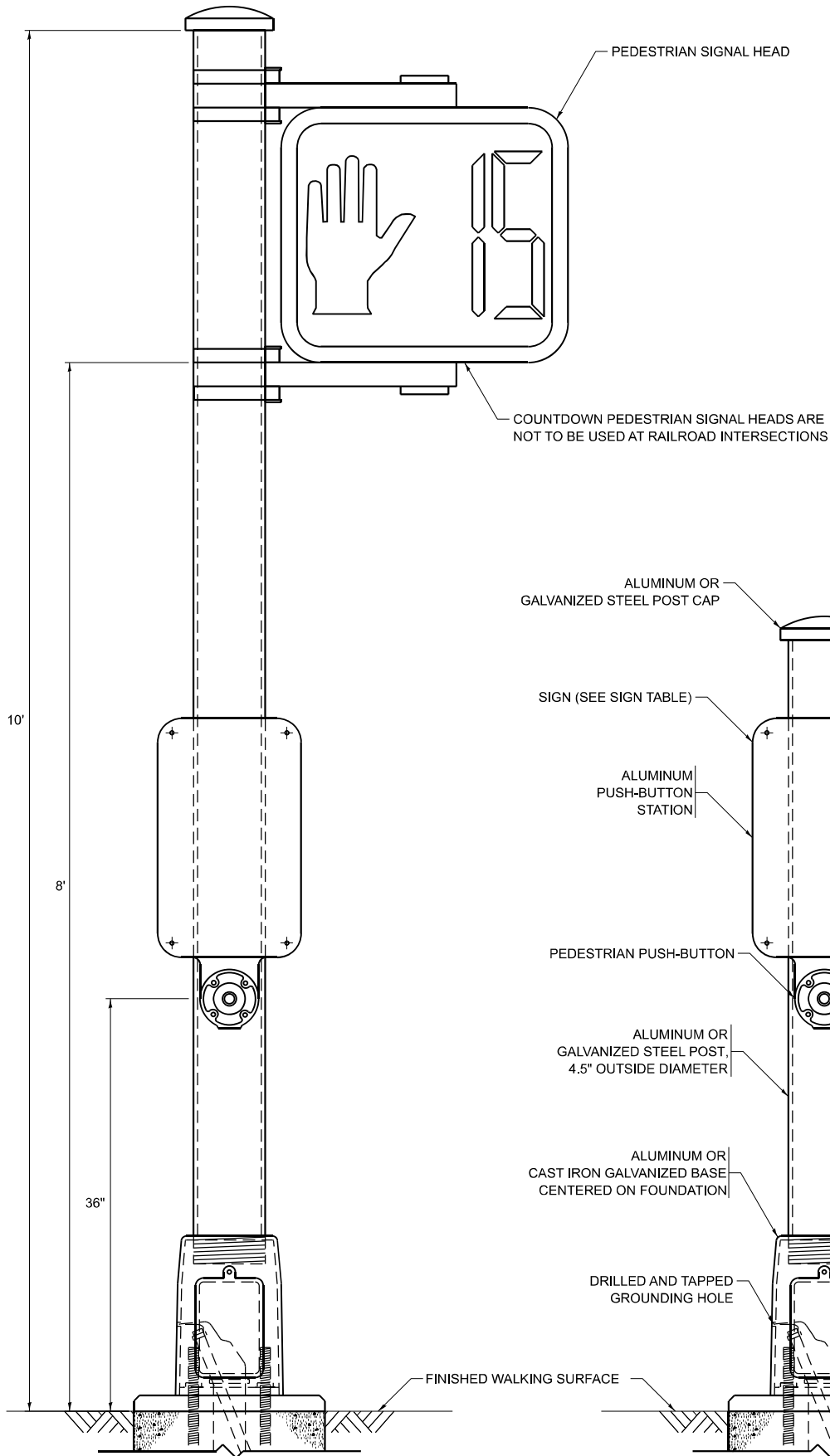
BOLT PATTERN

NOTE:

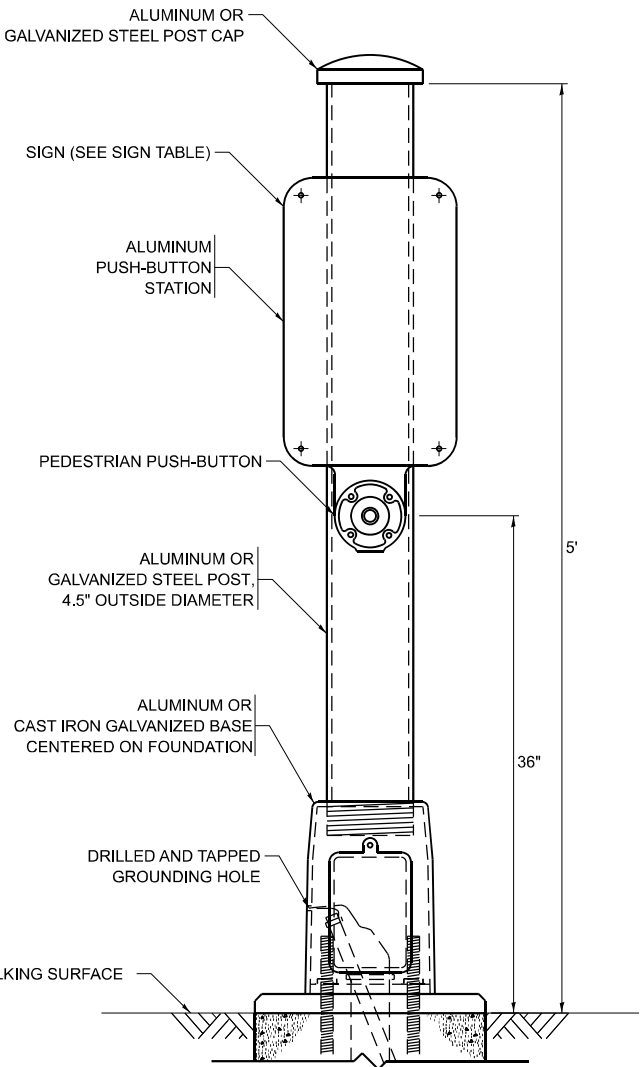
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



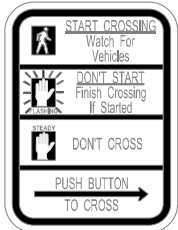
**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



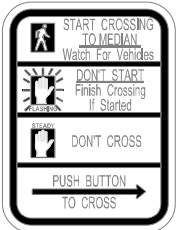
PEDESTRIAN SIGNAL POST, 10 FT.



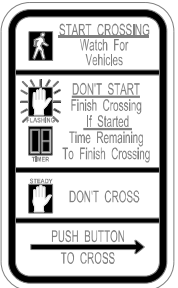
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

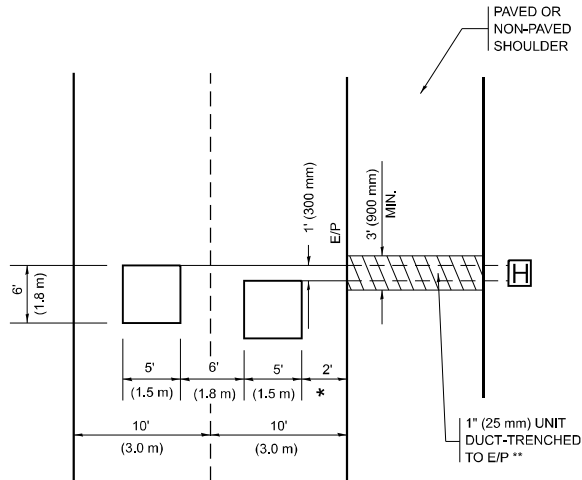
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	49
TS-05		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

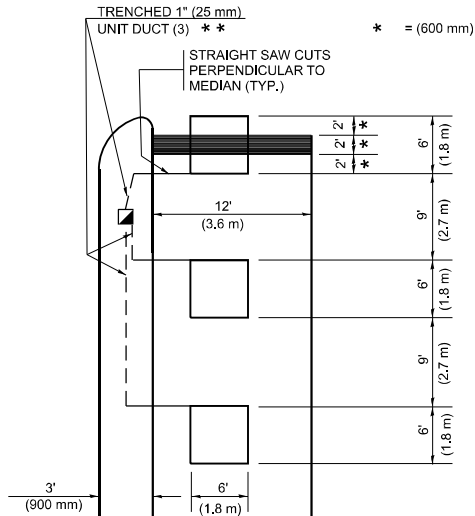


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

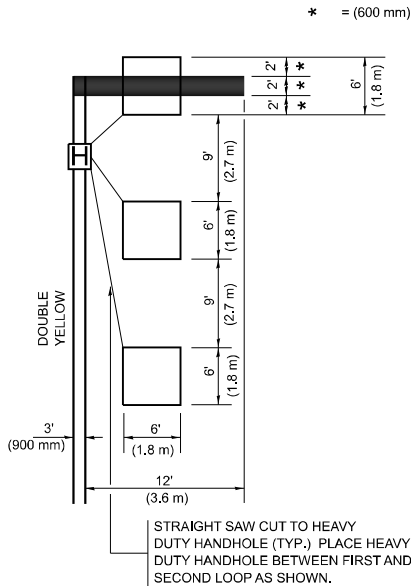


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

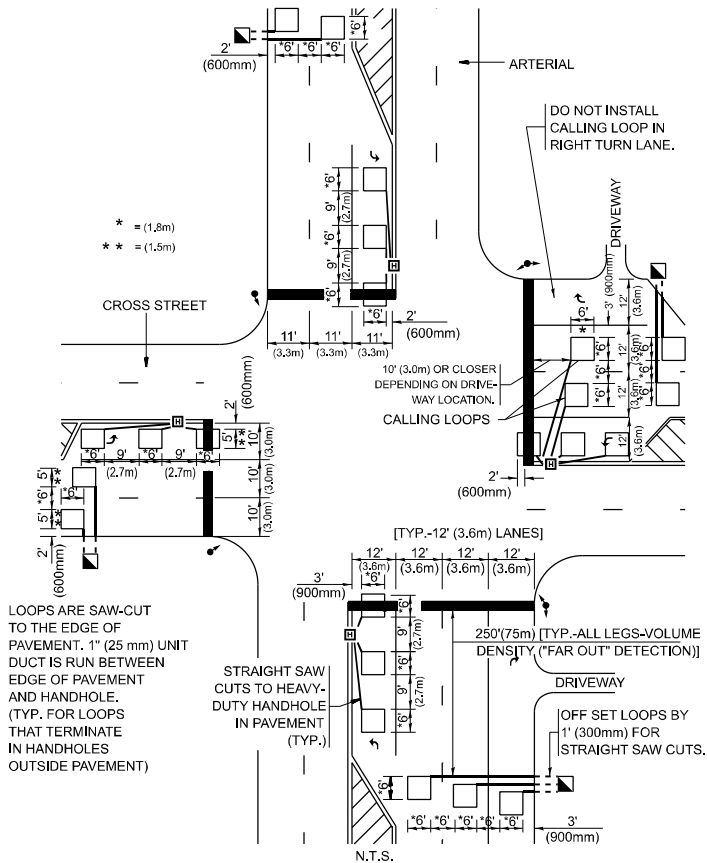
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



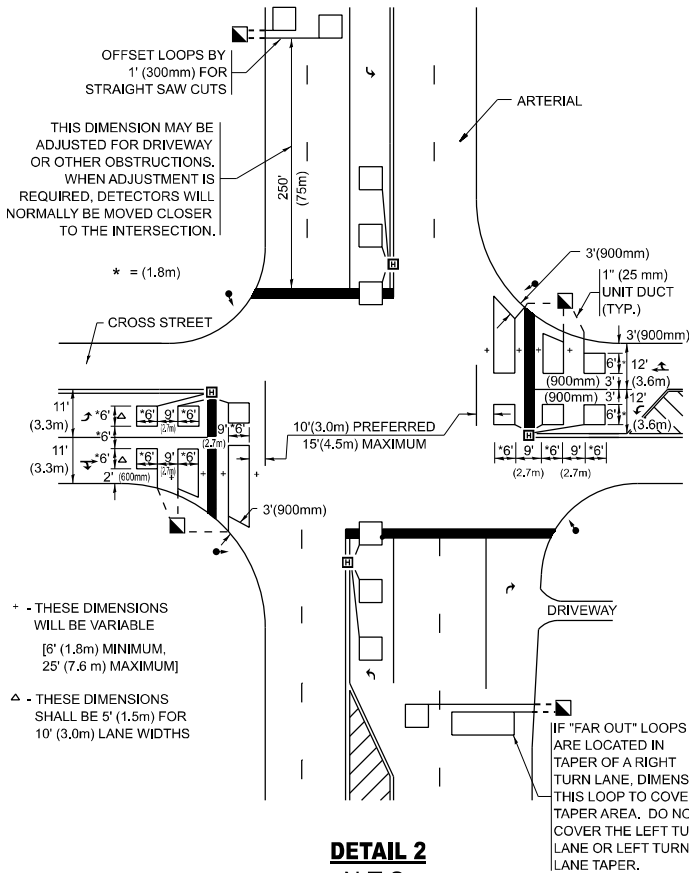
* = (1.8m)
** = (1.5m)

LOOPS ARE SAW-CUT
TO THE EDGE OF
PAVEMENT. 1" (25 mm) UNIT
DUCT IS RUN BETWEEN
EDGE OF PAVEMENT
AND HANDHOLE.
(TYP. FOR LOOPS
THAT TERMINATE
IN HANDHOLES
OUTSIDE PAVEMENT)

STRAIGHT SAW
CUTS TO HEAVY-
DUTY HANDHOLE
IN PAVEMENT
(TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY
1' (300mm) FOR
STRAIGHT SAW CUTS

THIS DIMENSION MAY BE
ADJUSTED FOR DRIVEWAY
OR OTHER OBSTRUCTIONS.
WHEN ADJUSTMENT IS
REQUIRED, DETECTORS WILL
NORMALLY BE MOVED CLOSER
TO THE INTERSECTION.

* = (1.8m)

- THESE DIMENSIONS
WILL BE VARIABLE
[6' (1.8m) MINIMUM,
25' (7.6 m) MAXIMUM]

- THESE DIMENSIONS
SHALL BE 5' (1.5m) FOR
10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

USER NAME = connor,mullane

DESIGNED -

REVISED -

DRAWN -

REVISED -

CHECKED - R.K.F.

REVISED -

PLOT DATE = 1/28/2025

DATE -

REVISED -

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
372	FAP 0372 23 SMART	COOK	50	50
TS-07		CONTRACT NO. 62V56		
ILLINOIS		FED. AID PROJECT		