

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 62J72	

D-91-256-20



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED Jan 24 20 25  
Joseph IR  
REGIONAL ENGINEER  
March 21 20 25  
Scott A. Etk  
ENGINEER OF DESIGN AND ENVIRONMENT  
March 21 20 25  
Harry 5  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

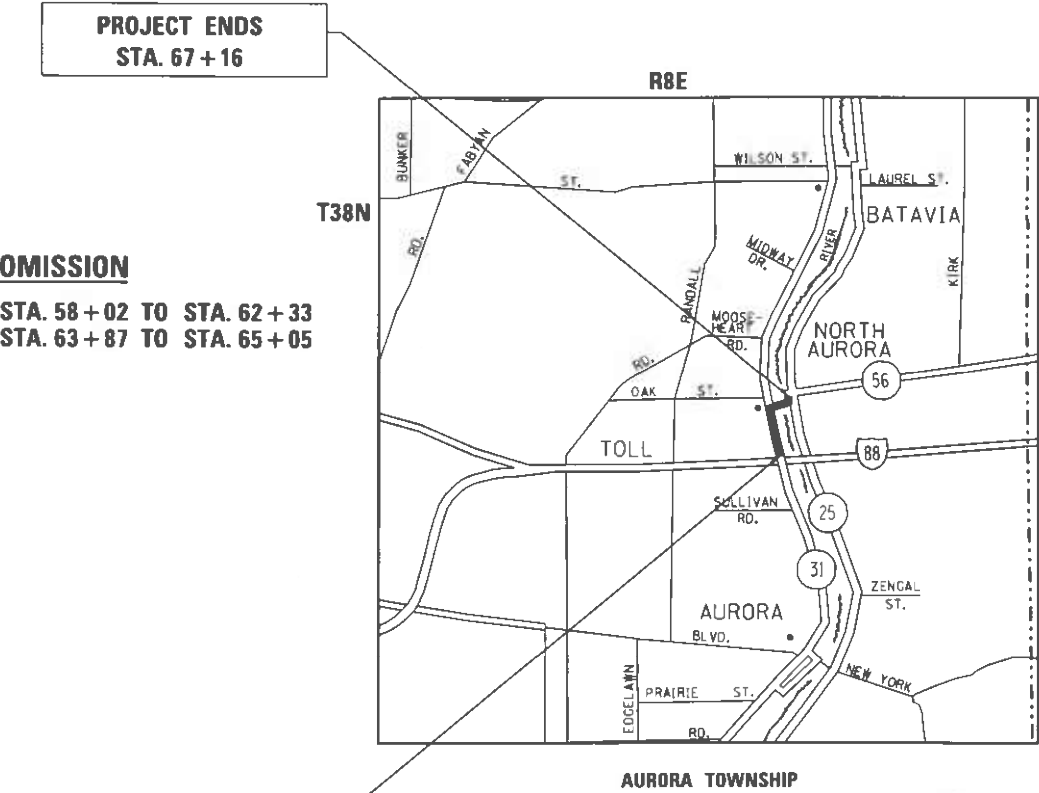
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED WITHIN:  
VILLAGE OF NORTH AURORA

TRAFFIC DATA: ILL 59

2023 ADT - 15,760 VPD  
POSTED SPEED LIMIT - 25-35 MPH

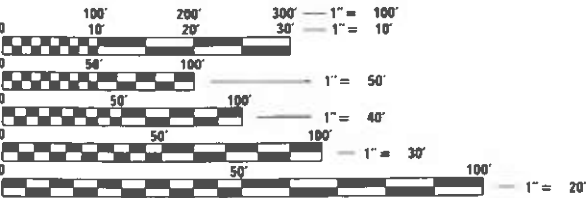
PROPOSED  
HIGHWAY PLANS  
ROUTE F.A.P. 365: ILL 31/ILL 56 (STATE STREET)  
I-88 TO NORTH RIVER ROAD  
DESIGNED OVERLAY & ADA IMPROVEMENTS  
SECTION: 2019-114RS&SW  
PROJECT: NHPP-1YZV(792)  
KANE COUNTY  
C-91-036-20



OMISSION  
STA. 58+02 TO STA. 62+33  
STA. 63+87 TO STA. 65+05

PROJECT BEGINS  
STA. 16+09

PROJECT ENDS  
STA. 67+16



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62J72

GROSS LENGTH = 5,107 FT = 0.97 MI.  
NET LENGTH = 4,558 FT = 0.86 MI.

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3-5	SUMMARY OF QUANTITIES
6	EXISTING & PROPOSED TYPICAL SECTIONS
7-9	ROADWAY & PAVEMENT MARKINGS PLANS
10-13	PEDESTRIAN RAMP DETAILS
14-19	PROJECT DETAILS FOR CURB RAMPS
20-29	TRAFFIC SIGNAL IMPROVEMENT PLANS (APS AND DETECTOR LOOPS)
30	BD-8: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
31	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
32	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
33	BD-32: BUTT JOINT AND HMA TAPER DETAILS
34	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
35	TC-11: TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
36	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS
37	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
38	TC-16: SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
39	TC-22: ARTERIAL ROAD INFORMATION SIGN
40	TC-26: DRIVEWAY ENTRANCE SIGNING
41	TS-07: DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-12	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-06	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-05	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-06	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-07	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
604091-05	FRAME AND GRATES, TYPE 24
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER

LIST OF STATE STANDARDS - CONTINUED

STANDARD NO.	DESCRIPTION
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600 MM) FROM PAVEMENT EDGE
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
814001-03	HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
878001-11	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

GENERAL NOTES

- 1) BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- 2) THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES. WITH UTILITY COMPANIES AND THE VILLAGE OF NORTH AURORA.
- 3) THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- 4) DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- 5) TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- 6) ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT DURING THE CONSTRUCTION OF THIS PROJECT.

GENERAL NOTES - CONTINUED

- 7) BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- 8) THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, EMAD ALHUSSEINI AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 9) PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- 10) ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 11) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 12) THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 13) FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- 14) ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- 15) THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY OR ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- 16) ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- 17) BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER SO THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 18) DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- 19) LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 20) DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 21) WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (85 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (85 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- 22) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 23) EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 25) ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- 26) THE ROAD CONSTRUCTION AHEAD SIGNS SHALL REMAIN INSTALLED UNTIL COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.

FILE NAME =  pw\\illdot-pw.bentley.com\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI23822\CD\Drawings\B123822-sht-pln.dgn	USER NAME = yeseen.queshi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILL 31/ILL 56 (STATE ST.) FROM I-88 TO RIVER RD. INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -							
	PLOT DATE = 1/24/2025	DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		
						ILLINOIS FED. AID PROJECT				
						CONTRACT NO. 62J72				
						365	2019-114R5&SW	KANE	41	2



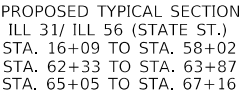
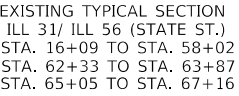
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED /20% STATE	0005 ROADWAY 100% STATE				
20200100	EARTH EXCAVATION	CU YD	23	23					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	25	25					
25200110	SODDING, SALT TOLERANT	SO YD	25	25					
25200200	SUPPLEMENTAL WATERING	UNIT	5	5					
35101400	AGGREGATATE BASE COURSE, TYPE B	TON	4	4					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	21377	21377					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	16136	16136					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	36	36					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	275	275					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	615	615					
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	2676	2676					
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	2342	2342					
42001300	PROTECTIVE COAT	SO YD	555	555					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2402	2402					
42400800	DETECTABLE WARNINGS	SO FT	200	200					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED /20% STATE	0005 ROADWAY 100% STATE				
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SO YD	23889	23889					
44000600	SIDEWALK REMOVAL	SO FT	1850	1850					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	895	895					
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	25	25					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	300	300					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	120	120					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	450	450					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	49	49					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	7	7					
60600605	CONCRETE CURB, TYPE B	FOOT	150	150					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	23	23					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5					
67100100	MOBILIZATION	L SUM	1	1				* SPECIALTY ITEM	
								# NON PARTICIPATING ITEM	

FILE NAME =		USER NAME = yosenureshi	DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				ILL 31/ILL 56 (STATE ST.) FROM I-88 TO RIVER RD. SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.	
pw\N\dot-pw\entirey.com\PI\DOT\Documents\DOT		DISTRICT =		DRAWN -		REVISED -									365	2019-114RS&SW		KANE	41	3	
		PLOT SCALE = 100,0000 ' / 1in		CHECKED -		REVISED -									CONTRACT NO. 62J72						
		PLOT DATE = 2/4/2025		DATE -		REVISED -									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE															
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 ROADWAY 80% FED /20% STATE	0005 ROADWAY 100% STATE					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 ROADWAY 80% FED /20% STATE	0005 ROADWAY 100% STATE													
70102620	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1						* 78000100	THERMOPLASTIC PAVEMENT MARKING -	SO FT	225	225														
	STANDARD 701501										LETTERS AND SYMBOLS																	
70102625	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1						* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12945	12945														
	STANDARD 701606																											
										* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1250	1250														
70102634	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1																								
	STANDARD 701611									* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	125	125														
70102635	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	300	300														
	STANDARD 701701																											
										* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	325	325														
70102640	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	1																								
	STANDARD 701801									78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	394	394														
70300100	SHORT TERM PAVEMENT MARKING	FOOT	5117	5117						* 81028200	UNDERGROUD CONDUIT, GALVANIZED STEEL,	FOOT	158	158														
											2" DIA.																	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	3525	3525																								
										* 81028240	UNDERGROUD CONDUIT, GALVANIZED STEEL,	FOOT	140	140														
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND	SO FT	225	225							4" DIA.																	
	SYMBOLS - PAINT																											
										* 81400200	HEAVY-DUTY HANDHOLE	EACH	4	4														
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"-PAINT	FOOT	12945	12945																								
										* 85000200	MAINTENANCE OF EXISTING TRAFFIC	EACH	3	3														
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"-	FOOT	1250	1250							SIGNAL INSTALLATION																	
	PAINT																											
										* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	291	291														
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"-PAINT	FOOT	125	125																								
										* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	167	167														
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"-	FOOT	300	300														* SPECIALTY ITEM										
	PAINT																	# NON PARTICIPATING ITEM										
FILE NAME =		USER NAME = yoseenqureshi		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			ILL 31/ILL 56 (STATE ST.) FROM I-88 TO RIVER RD. SUMMARY OF QUANTITIES				F.A.P. RTE.		SECTION		COUNTY		TOTAL SHEETS		SHEET NO.					
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											SCALE:		SHEET NO.		OF		SHEETS		STA.		TO STA.		FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE															
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED /20% STATE	0005 ROADWAY 100% STATE														
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN,  NO. 14 1 PAIR	FOOT	1322	1322															
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT  GROUNDING CONDUCTOR, NO. 6 1C	FOOT	358	358															
* 87900200	DRILL EXISTING HANDHOLE	EACH	10	10															
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE,  BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4	4															
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	2	2															
* 88600100	DETECTOR LOOP, TYPE I	FOOT	1094	1094															
* 89502200	MODIFY EXISTING CONTROLLER	EACH	1	1															
* 89502300	REMOVE ELECTRICAL CABLE FROM CONDUIT	FOOT	480	480															
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	3	3															
* 89502376	REBUILD EXISTING HANDHOLE	EACH	6	6															
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	1	1															
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1															
* X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	1	1															
* X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1	1															
FILE NAME =		USER NAME = yaseenzuresht	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION					ILL 31/ILL 56 (STATE ST.) FROM I-88 TO RIVER RD. SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
pws\jldot-pw.bentley.com\FW\DOT\Documents\JDOT - Office\District 1\Projects\DI23822\CADD\Drawn\Design\DI23822-sh-1.dwg		DRAWN -	CHECKED -	REVISED -						365				2019-114RS&SW		KANE	41	5	
PLOT SCALE = 100.0000 ' / 1in.		DATE -	REVISED -																
PLOT DATE = 2/2/2025		DATE -	REVISED -																
										SCALE:		SHEET NO. OF SHEETS STA.		TO STA.		FED. ROAD DIST. NO. 1  ILLINOIS  FED. AID PROJECT			
																		# SPECIALTY ITEM	
																		# NON PARTICIPATING ITEM	



1. EXISTING COMBINATION CONCRETE CURB AND GUTTER
2. EXISTING P.C. CONCRETE PAVEMENT | 10"
3. EXISTING HMA SURFACE COURSE | 4" (BEFORE MILLING)
4. EXISTING SUB-BASE | 4"
5. PROPOSED HMA SURFACE REMOVAL, 3¾"
6. PROPOSED COMBINATION CONCRETE CURB AND  
GUTTER (LOCATIONS TO BE DETERMINED BY THE RE IN THE FIELD)
7. PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"
8. PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80; 1 3/4"

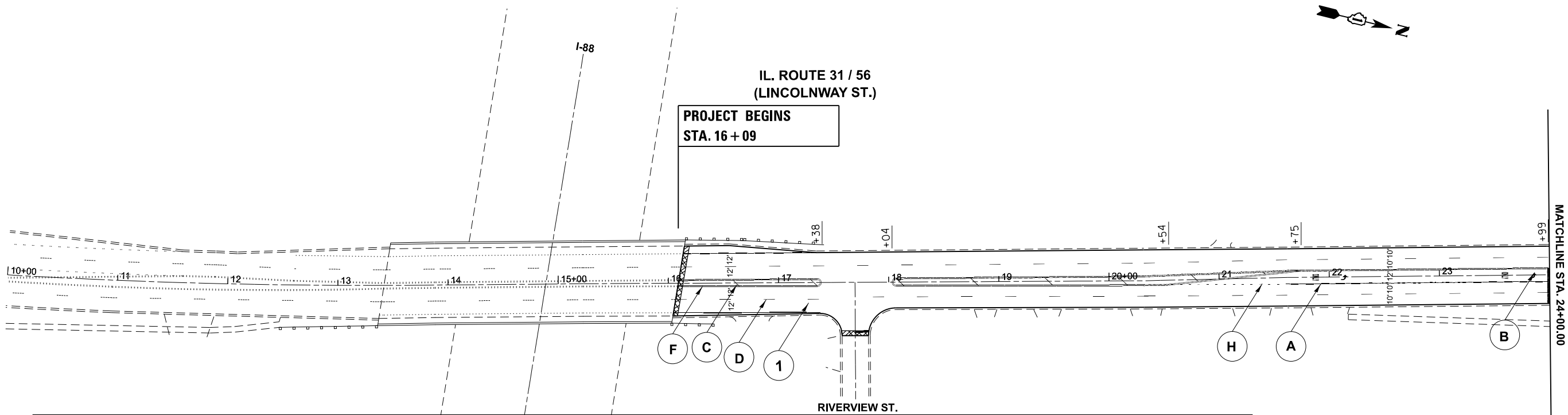
1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING
2. LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER COURSE IL-9.5.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
PAVEMENT RESURFACING		
POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80; 1 3/4"	3.5% @ 80 GYR.	QCP
HMA BINDER COURSE, IL-9.5, N70; 2"	4.0% @ 70 GYR.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0)	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.

FILE NAME :	USER NAME = yeseen.qureshi	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div>	ILL 31/ILL 56 (STATE ST.) FROM I-88 TO RIVER RD.				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pr\\nildot-pw.bentley.com\PIDOT\Documents\NIDOT Offices\District 1\Projects\DI23822\CDRAWING\B123822-sht-plen.dgn									365	2019-114RS&SW	KANE	41	6
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -		CONTRACT NO. 62J72								
	PLOT DATE = 2/7/2025	DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
						SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					



MATCHLINE STA. 24+00.00

00+00+38 VAS ENLITWATM

- 1

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"  
PROP. HMA BINDER COURSE, IL-9.5, N70, 2"  
PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"

A

PROP. THERMOPLASTIC PLASTIC PAVEMENT TURN LANE MARKING-LINE 6", WHITE (TYP.)

B

PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)

C

PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 75' C-C, YELLOW (TYP.)

D

PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", 10' DASH-30' SKIP, WHITE (TYP.)

E

PROP. THERMOPLASTIC PAVEMENT STOP MARKING-LINE 24", WHITE (TYP.)
- F

PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)

G

PROP. THERMOPLASTIC PAVEMENT PEDESTRIAN CROSSWALK MARKING-LINE 6", WHITE (TYP.)

H

PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 6", 2' DASH-6' SKIP, WHITE (TYP.)

I

PROP. THERMOPLASTIC PAVEMENT SCHOOL CROSSWALK MARKING-LINE 12", WHITE (TYP.)

J

PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", WHITE (TYP.)

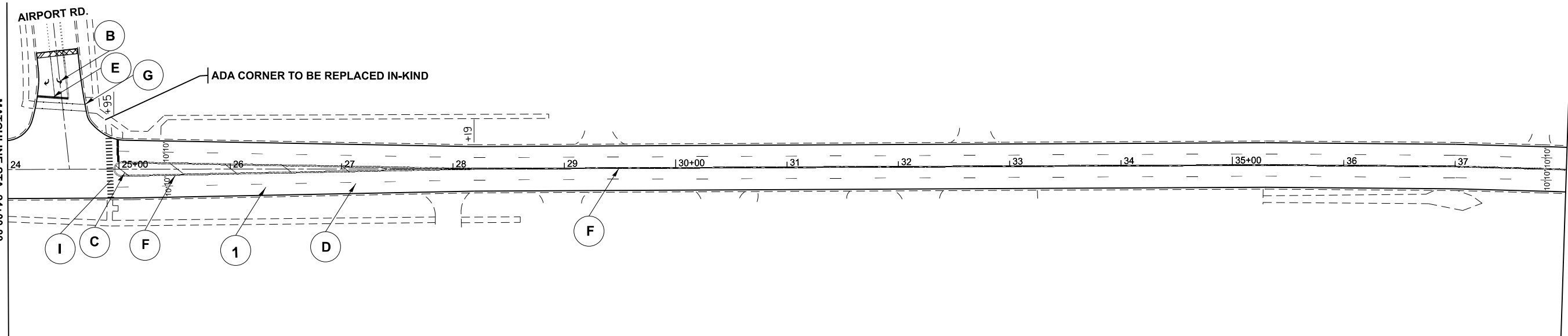
K

PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", YELLOW (TYP.)
- PROPOSED CURB RAMP IMPROVEMENT

USE PROJECT DETAIL PD-01A 1  
USE PROJECT DETAIL PD-01B 2  
USE PROJECT DETAIL PD-01C 3  
USE PROJECT DETAIL PD-02A 4  
USE PROJECT DETAIL PD-02B 5  
USE PROJECT DETAIL PD-02C 6  
USE PROJECT DETAIL PD-03A 7  
USE PROJECT DETAIL PD-03B 8  
USE PROJECT DETAIL PD-04A 9  
USE PROJECT DETAIL PD-04B 10  
USE PROJECT DETAIL PD-05A 11  
USE PROJECT DETAIL PD-05B 12  
USE PROJECT DETAIL PD-06A 13  
USE PROJECT DETAIL PD-06B 14  
USE PEDESTRIAN RAMP DETAILS 15  
(SEE SHEETS 10)

\* = ADA RAMPS
- NOTE: ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S DIDETAIL TC-13  
AND TC-23B.

IL. ROUTE 31 / 56  
(LINCOLNWAY ST.)



HMA SURFACE REMOVAL  
BUTT JOINT, 4.5'

MODEL: EXCL - Plan 1 (Sheet)  
FILE NAME: c:\p\work\id\qureshi\41034478\0125620-shr-plan.dgn

USER NAME	= yaseen.qureshi	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 2/4/2025	DATE	-	REVISED	-

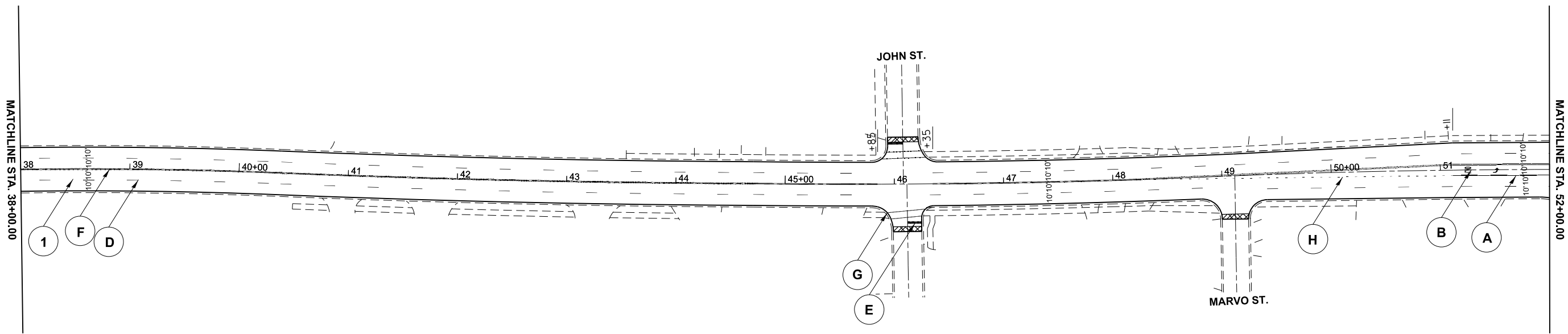
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN  
IL RTE. 56 / IL RTE. 31 (I-88 TO RIVER ST.)

SCALE: 1"=50'  
SHEET 1 OF 3 SHEETS  
STA. 10+00.00 TO STA. 38+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	7
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

IL. ROUTE 31 / 56  
(LINCOLNWAY ST.)



- 1 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"  
PROP. HMA BINDER COURSE, IL-9.5, N70, 2"  
PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"

A PROP. THERMOPLASTIC PLASTIC PAVEMENT TURN LANE MARKING-LINE 6", WHITE (TYP.)

B PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)

C PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 75' C-C, YELLOW (TYP.)

D PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", 10' DASH-30' SKIP, WHITE (TYP.)

E PROP. THERMOPLASTIC PAVEMENT STOP MARKING-LINE 24", WHITE (TYP.)
- F PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)

G PROP. THERMOPLASTIC PAVEMENT PEDESTRIAN CROSSWALK MARKING-LINE 6", WHITE (TYP.)

H PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 6", 2' DASH-6' SKIP, WHITE (TYP.)

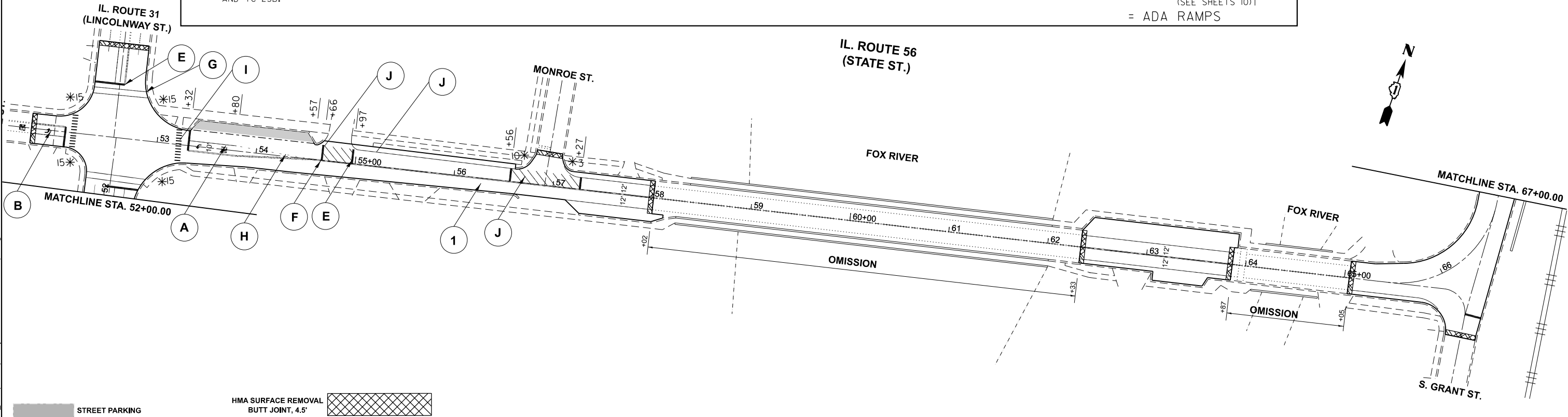
I PROP. THERMOPLASTIC PAVEMENT SCHOOL CROSSWALK MARKING-LINE 12", WHITE (TYP.)

J PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", WHITE (TYP.)

K PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", YELLOW (TYP.)
- PROPOSED CURB RAMP IMPROVEMENT

  - USE PROJECT DETAIL PD-01A 1-1
  - USE PROJECT DETAIL PD-01B 1-2
  - USE PROJECT DETAIL PD-01C 1-3
  - USE PROJECT DETAIL PD-02A 1-4
  - USE PROJECT DETAIL PD-02B 1-5
  - USE PROJECT DETAIL PD-02C 1-6
  - USE PROJECT DETAIL PD-03A 1-7
  - USE PROJECT DETAIL PD-03B 1-8
  - USE PROJECT DETAIL PD-04A 1-9
  - USE PROJECT DETAIL PD-04B 1-10
  - USE PROJECT DETAIL PD-05A 1-11
  - USE PROJECT DETAIL PD-05B 1-12
  - USE PROJECT DETAIL PD-06A 1-13
  - USE PROJECT DETAIL PD-06B 1-14
  - USE PEDESTRIAN RAMP DETAILS 1-15 (SEE SHEETS 10)

= ADA RAMPS
- NOTE: ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S DETAIL TC-13 AND TC-23B.



STREET PARKING

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

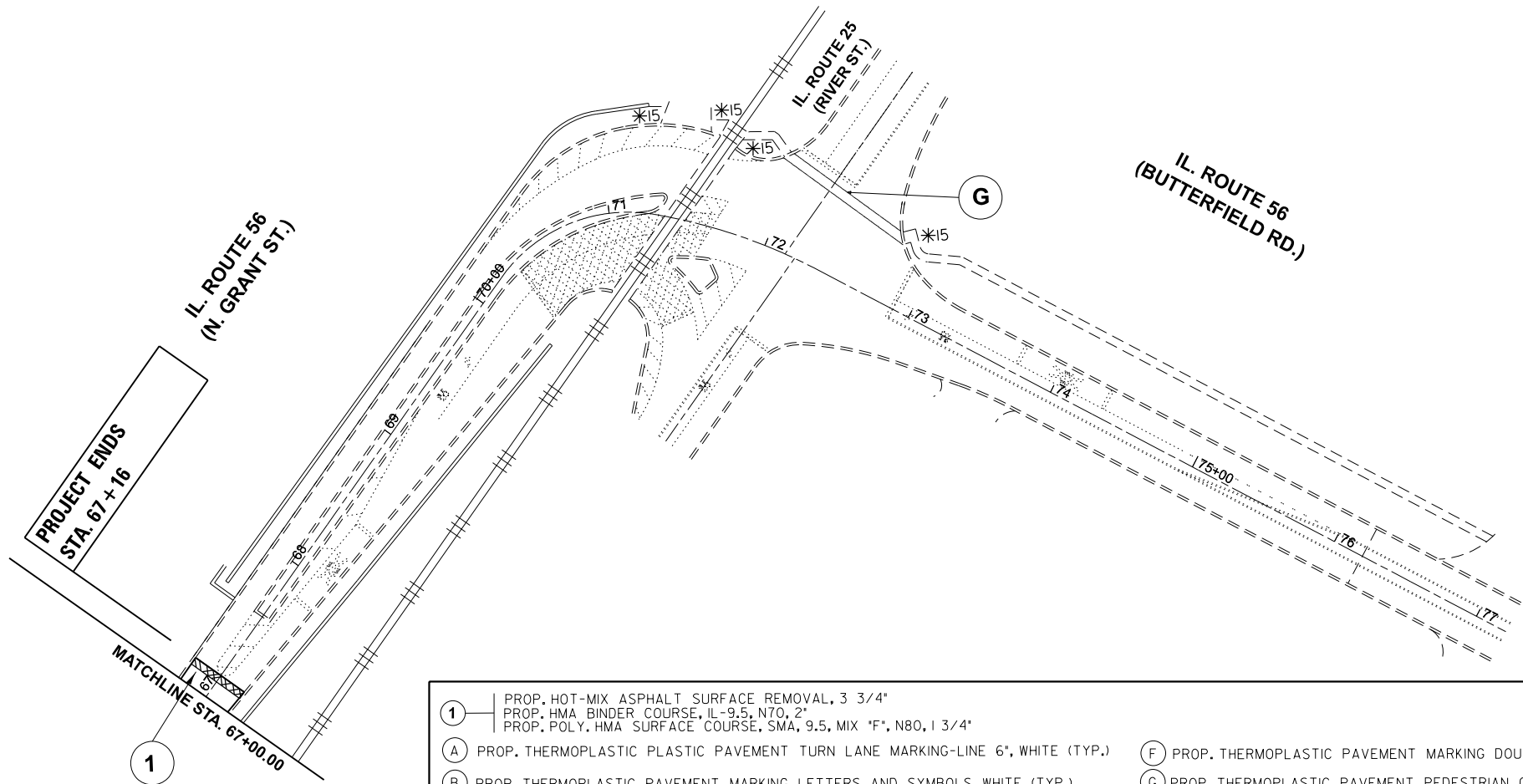
EXISTING AND PROPOSED ROADWAY PLAN  
IL RTE. 56 / IL RTE. 31 (I-88 TO RIVER ST.)

SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 38+00.00 TO STA. 67+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	8
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

MODEL: ExCL - Plan 3 (Sheet)  
FILE NAME: c:\p\work\id\qureshi\41034478\015620-sh-plan.dgn





- 1 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"  
PROP. HMA BINDER COURSE, IL-9.5, N70, 2"  
PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"

A PROP. THERMOPLASTIC PLASTIC PAVEMENT TURN LANE MARKING-LINE 6", WHITE (TYP.)

B PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)

C PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 75' C-C, YELLOW (TYP.)

D PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", 10' DASH-30' SKIP, WHITE (TYP.)

E PROP. THERMOPLASTIC PAVEMENT STOP MARKING-LINE 24", WHITE (TYP.)

NOTE: ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S DIDETAIL TC-13 AND TC-23B.
- F PROP. THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW (TYP.)

G PROP. THERMOPLASTIC PAVEMENT PEDESTRIAN CROSSWALK MARKING-LINE 6", WHITE (TYP.)

H PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 6", 2' DASH-6' SKIP, WHITE (TYP.)

I PROP. THERMOPLASTIC PAVEMENT SCHOOL CROSSWALK MARKING-LINE 12", WHITE (TYP.)

J PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", WHITE (TYP.)

K PROP. THERMOPLASTIC PAVEMENT EDGE MARKING-LINE 4", YELLOW (TYP.)
- PROPOSED CURB RAMP IMPROVEMENT

USE PROJECT DETAIL PD-01A 1\*1

USE PROJECT DETAIL PD-01B 1\*2

USE PROJECT DETAIL PD-01C 1\*3

USE PROJECT DETAIL PD-02A 1\*4

USE PROJECT DETAIL PD-02B 1\*5

USE PROJECT DETAIL PD-02C 1\*6

USE PROJECT DETAIL PD-03A 1\*7

USE PROJECT DETAIL PD-03B 1\*8

USE PROJECT DETAIL PD-04A 1\*9

USE PROJECT DETAIL PD-04B 1\*10

USE PROJECT DETAIL PD-05A 1\*11

USE PROJECT DETAIL PD-05B 1\*12

USE PROJECT DETAIL PD-06A 1\*13

USE PROJECT DETAIL PD-06B 1\*14

USE PEDESTRIAN RAMP DETAILS 1\*15  
(SEE SHEETS 10)

\* = ADA RAMPS

MODEL: EXCL - Plan 5 [Sheet]  
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USER NAME	= yaseen.qureshi	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 2/4/2025	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

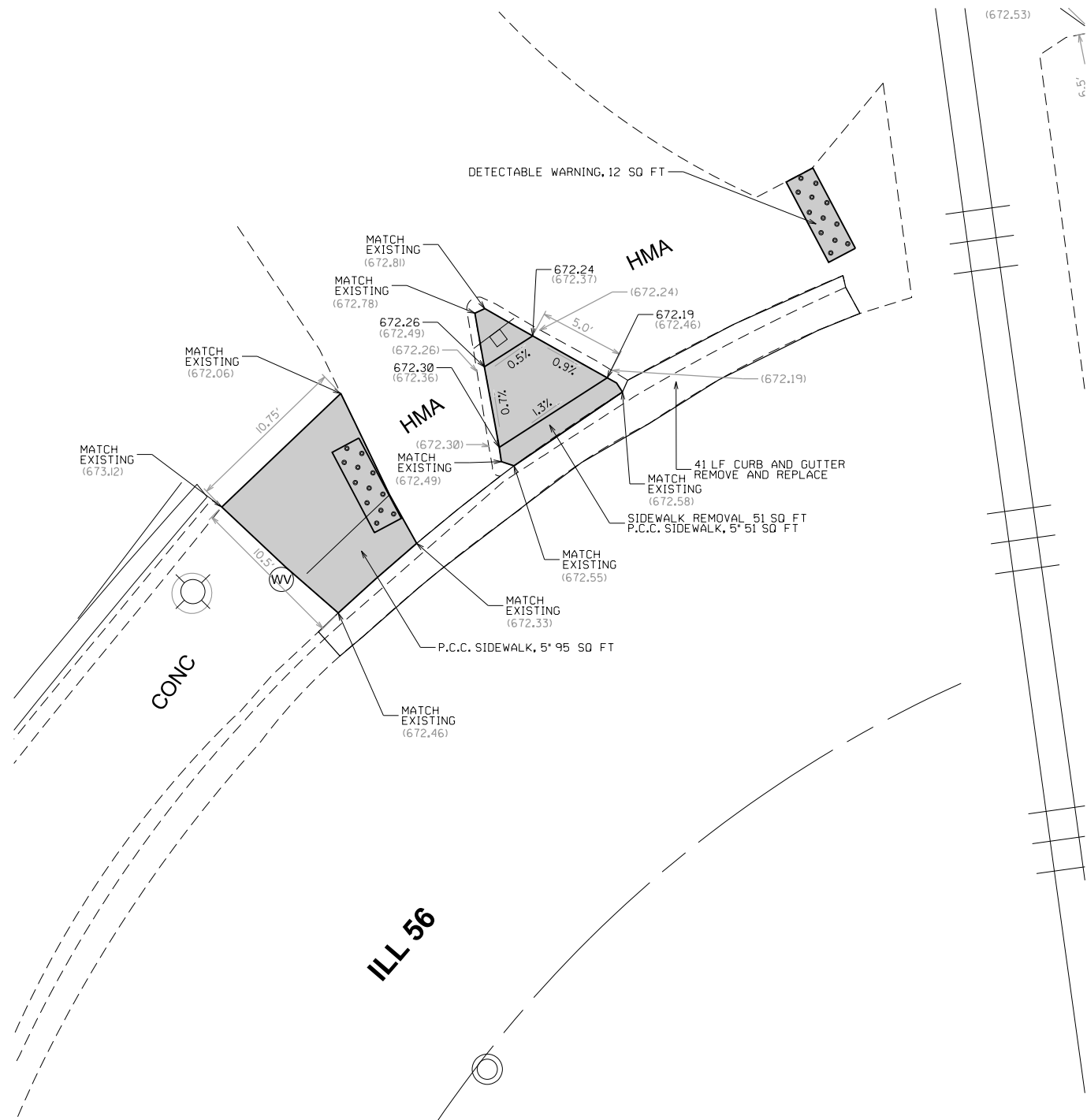
EXISTING AND PROPOSED ROADWAY PLAN  
IL RTE. 56 / IL RTE. 31 (I-88 TO RIVER ST.)

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 67+00.00 TO STA. 82+00.00

F.A.P RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365		2019-114RS&SW	KANE	41	9
		CONTRACT NO. 62J72			
		ILLINOIS	FED. AID PROJECT		







REFERENCE BENCH MARK ELEV 677.84

BENCHMARK : CUT SQUARE IN SOUTHEAST CORNER OF TRAFFIC SIGNAL HANDHOLE

LOCATION : AT THE NE CORNER OF ILL 56 AT ILL 25.

## LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

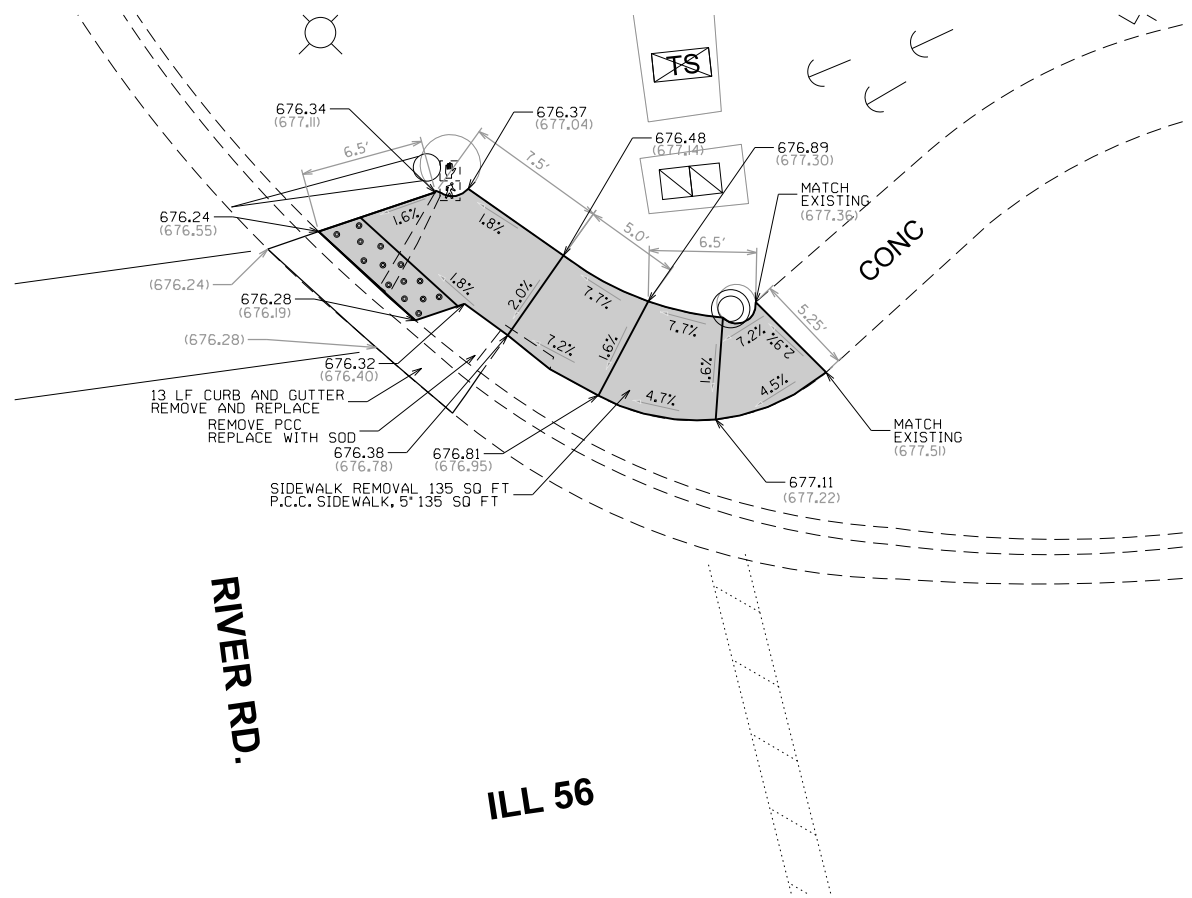
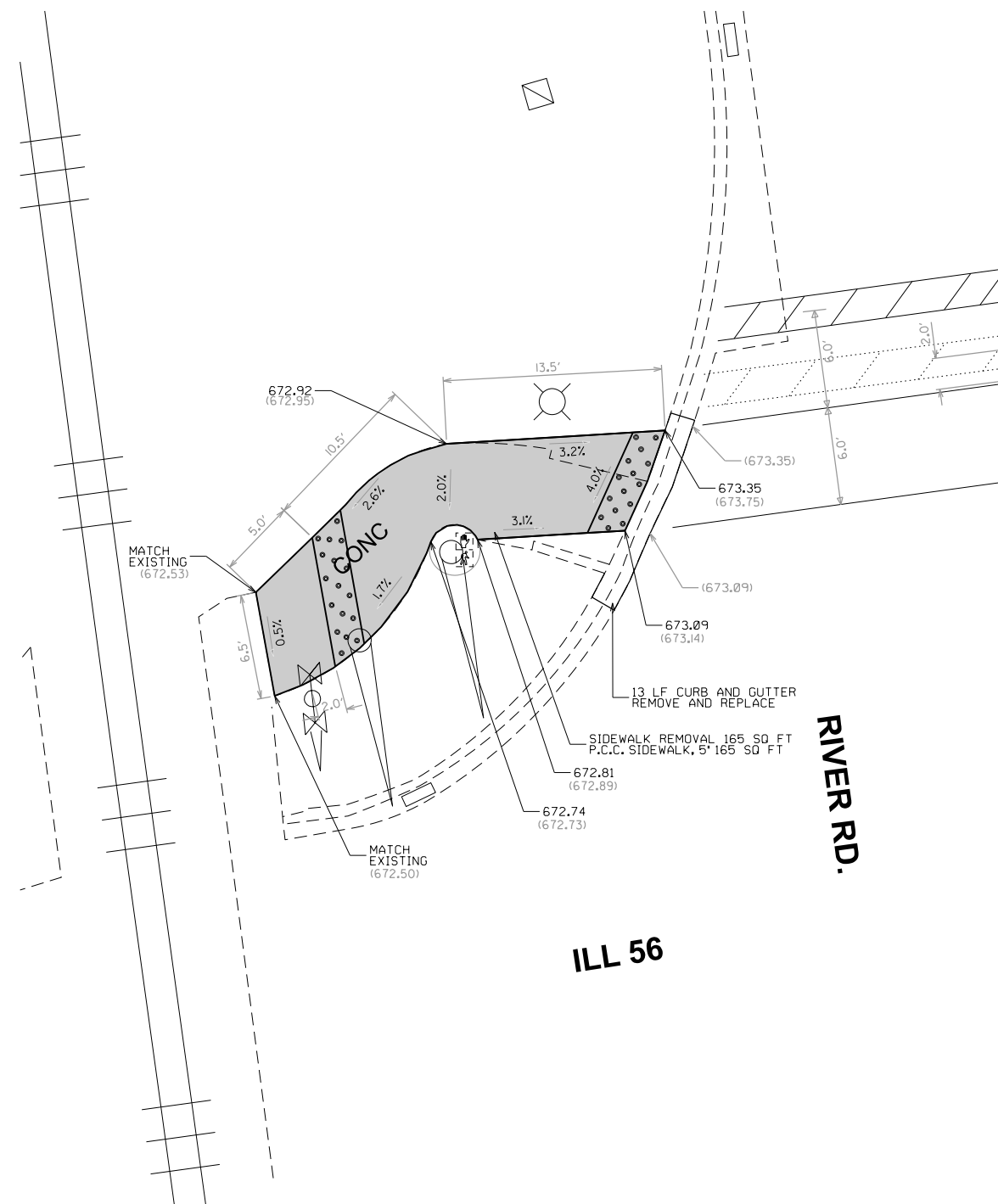
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Default	PLOT SCALE = 9.995' / 1" in.	CHECKED -	REVISED -
	PLOT DATE = 3/14/2025	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS (ILL 56 AT RIVER RD.)  
NW CORNER

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	12
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				



REFERENCE BENCH MARK ELEV 677.84

LOCATION : AT THE NE CORNER OF ILL 56 AT ILL 25.

## LEGEND

EXISTING LENGTH

( ) EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK

## DETECTABLE WARNINGS

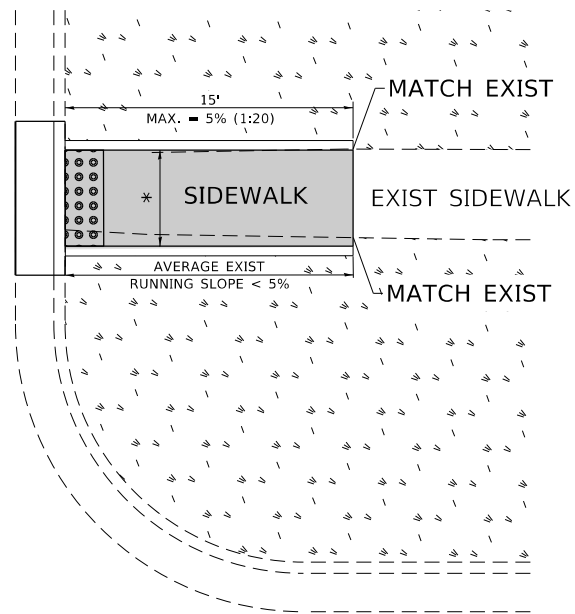
REFERENCE BENCH MARK ELEV 677.84

LOCATION : AT THE NE CORNER OF ILL 56 AT ILL 25.

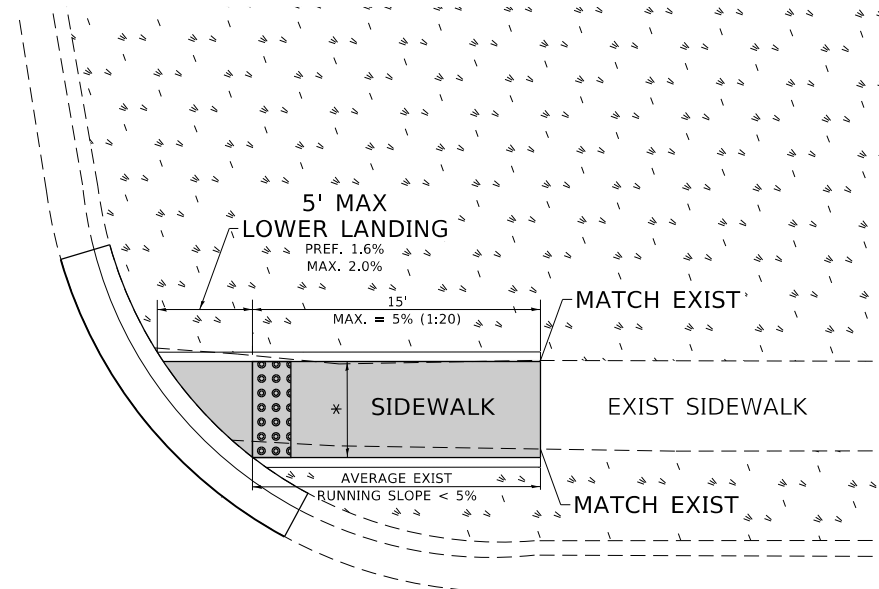
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pwi:///idot-pw.bentley.com/PWIDOT/Documents/IDOT Offices/District 1/ORD Projects/D125620-ADA-Design/Drawings/Design/D125620-ADA-Design.dgn		CHECKED -	REVISED -							365	2019-114RS&SW	KANE	41	13
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PLOT DATE = 3/14/2025														
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ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

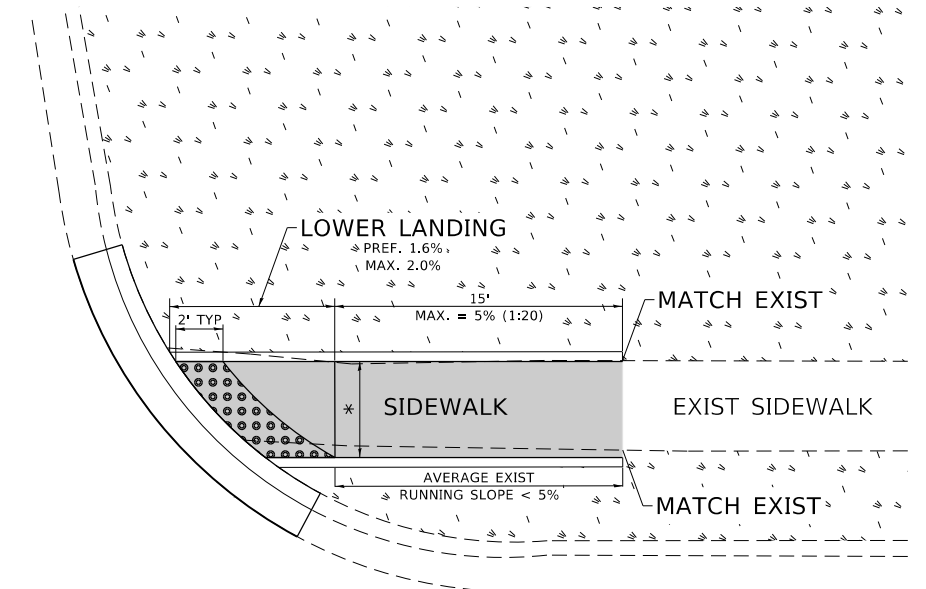
**PD-01A**



**PD-01B**



**PD-01C**



### LEGEND



EXIST. GRASS



PROPOSED SIDEWALK



## DETECTABLE WARNINGS

===== PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

USER NAME = yaseen.quereshi	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000 ' f in.	CHECKED -	REVISED -
PLOT DATE = 1/24/2025	DATE = 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS**  
**(PD-01)**

SCALE: NONE	SHEET	OF	SHEETS	STA.	TO STA.
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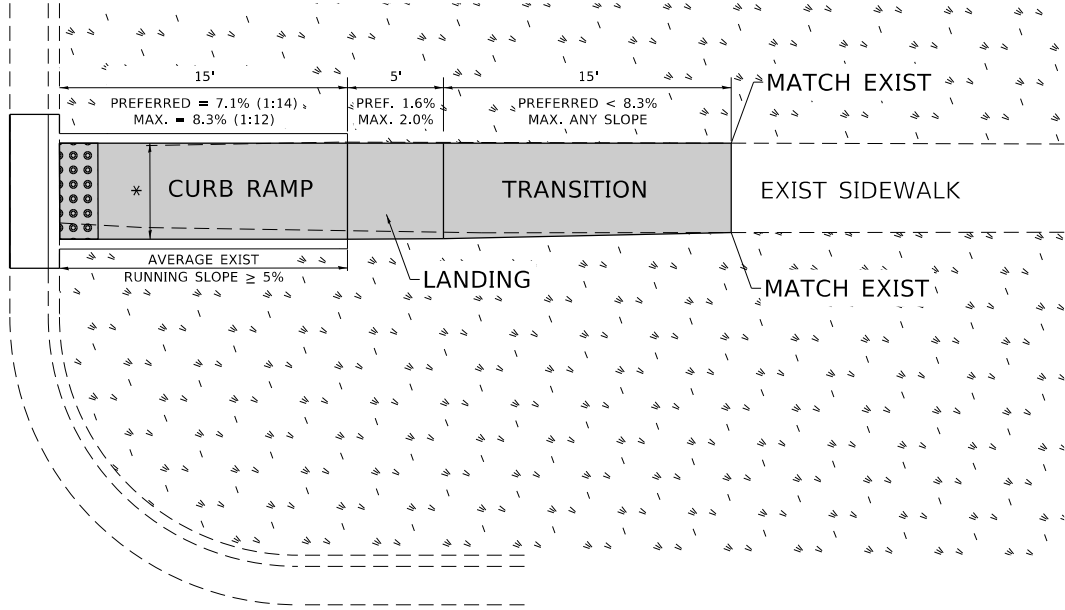
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365	2019-114RS&SW	KANE	41	14
PD-01		CONTRACT NO. 62P98		
	ILLINOIS	FED. AID PROJECT		

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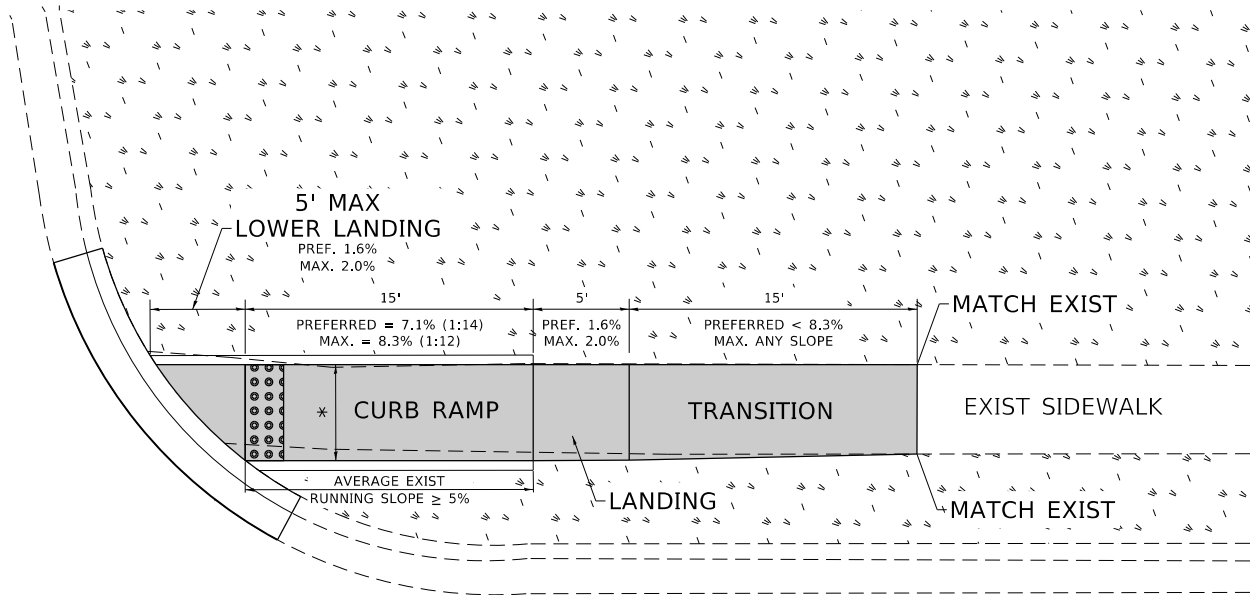


ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

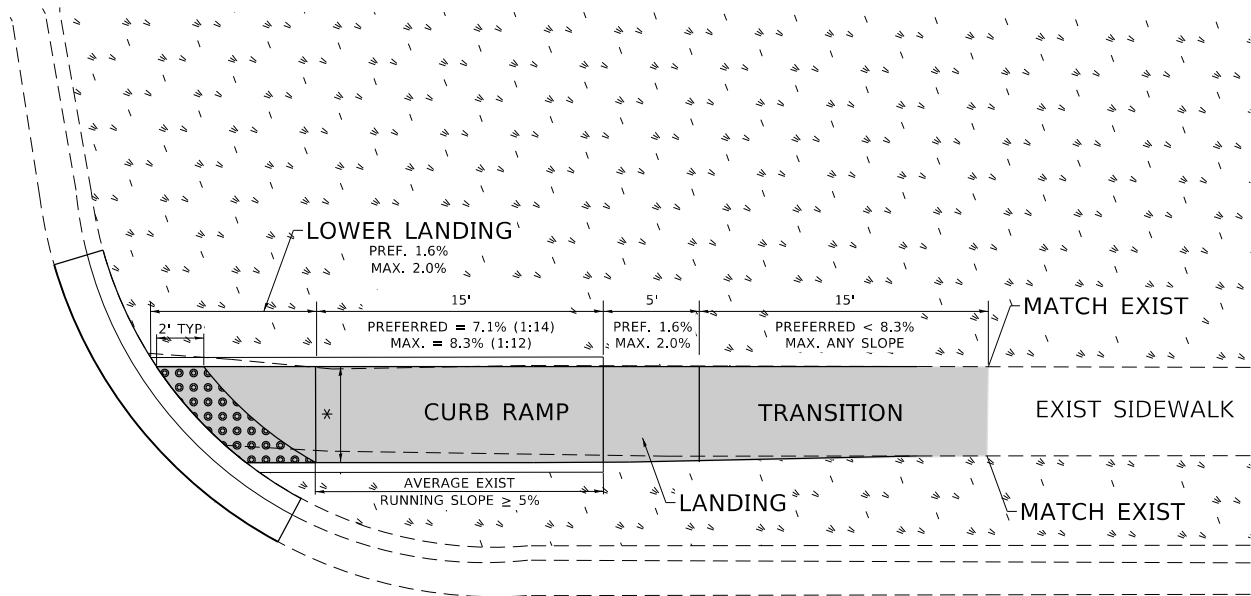
PD-02A



PD-02B



PD-02C



LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

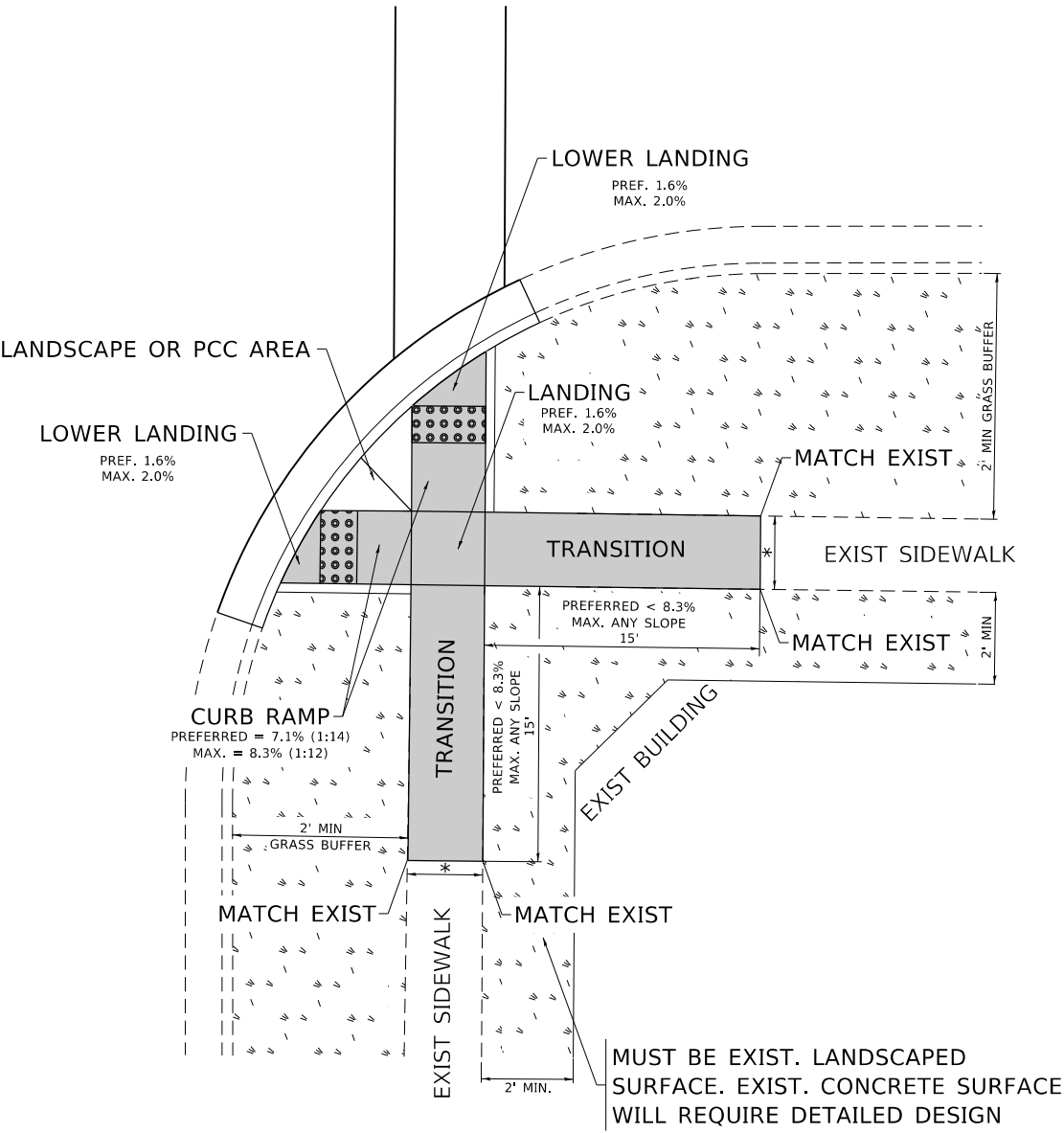
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-02)

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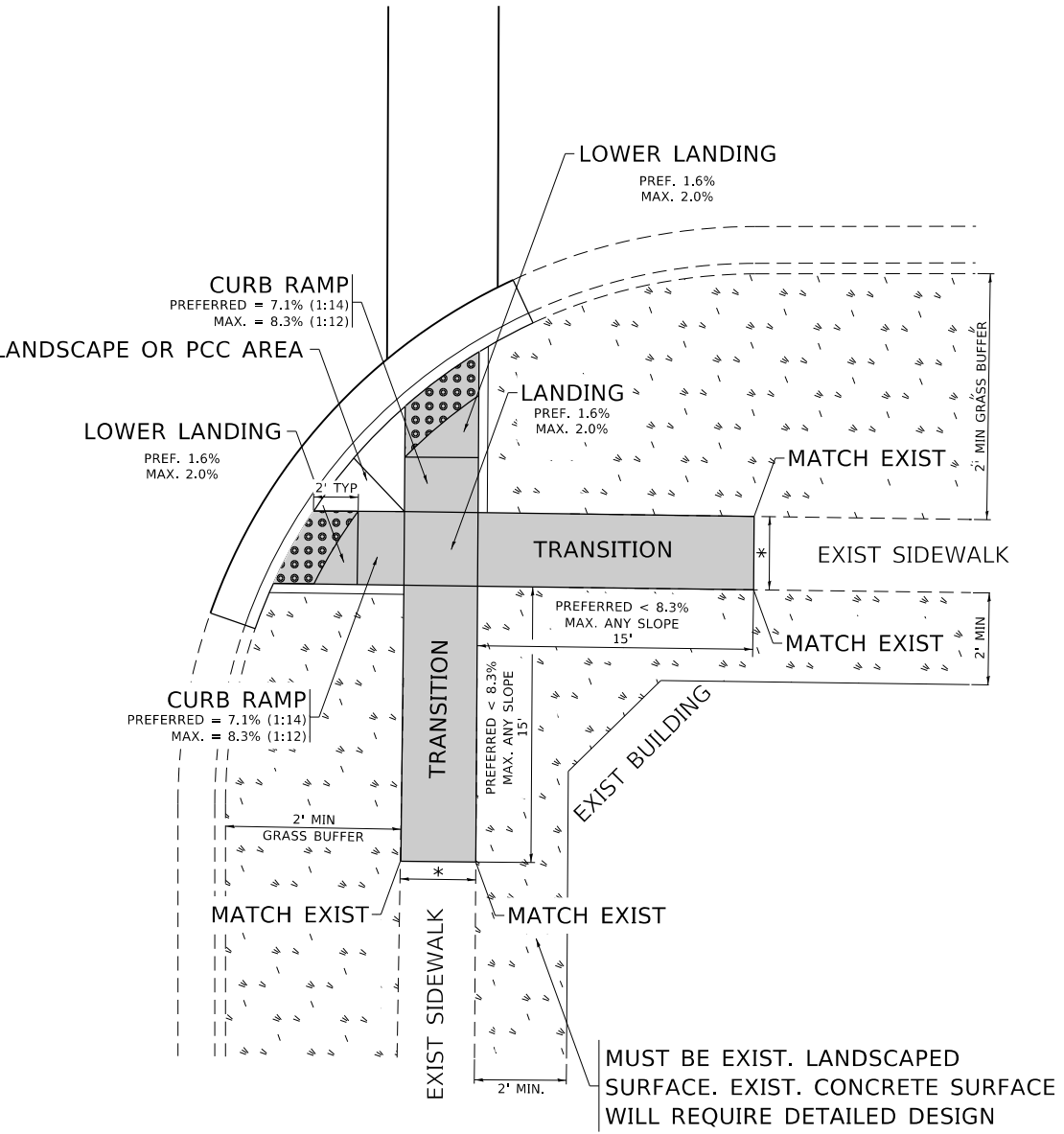
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365	2019-114RS&SW	KANE	41	15
PD-02		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS  
(PD-03)

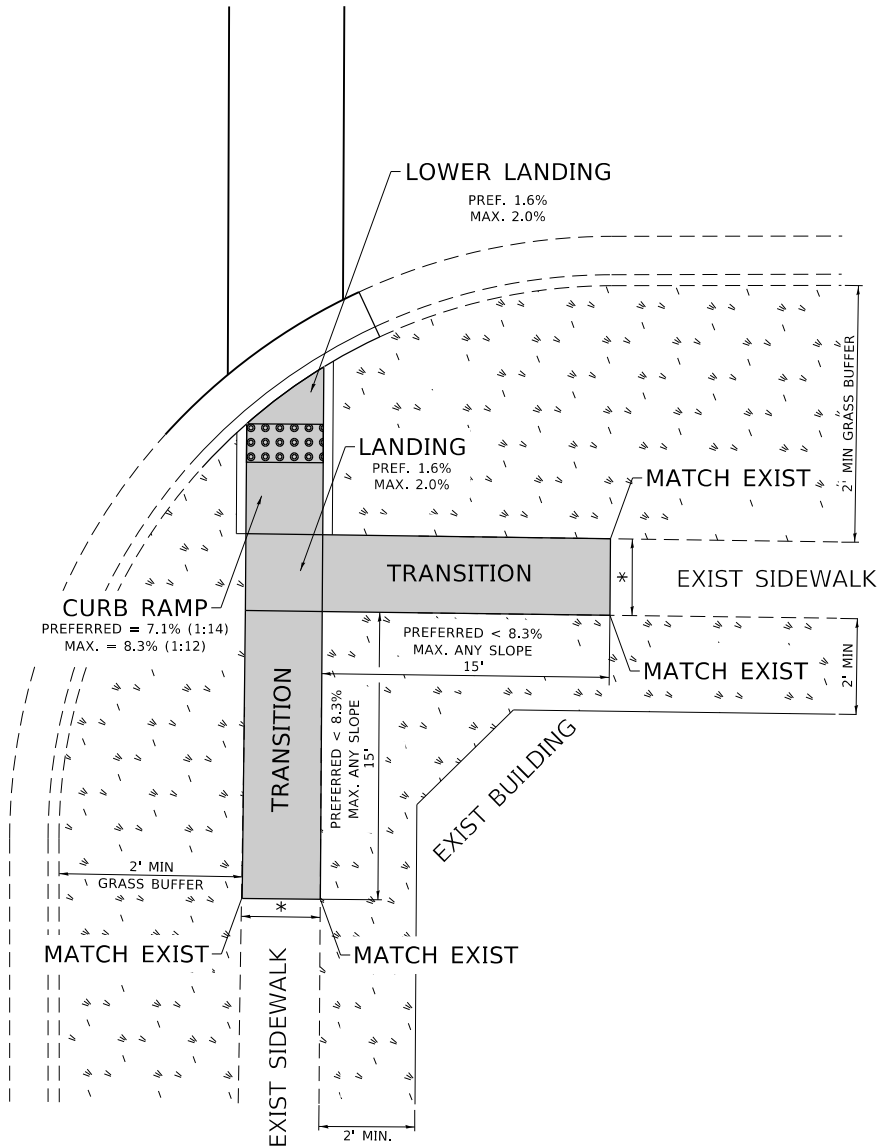
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	16
PD-03		CONTRACT NO. 62J72		
ILLINOIS		FED. AID PROJECT		

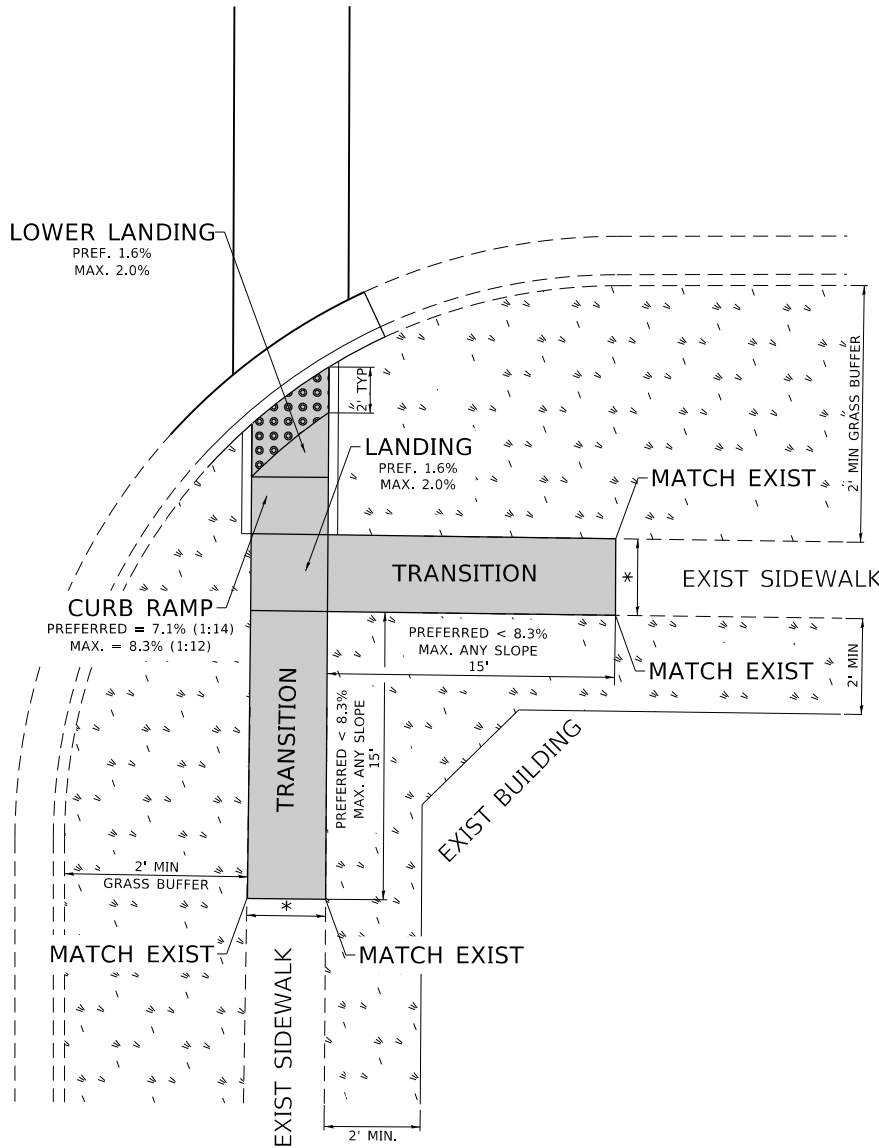
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ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

USER NAME	= yaseen.ureshi
DESIGNED	-
DRAWN	- R. LEDEZMA
PLOT SCALE	= 100.0000 ' / in.
PLOT DATE	= 1/24/2025

DESIGNED	-
REVIS	-
CHECKED	-
REVIS	-
DATE	- 10/02/2019
REVIS	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

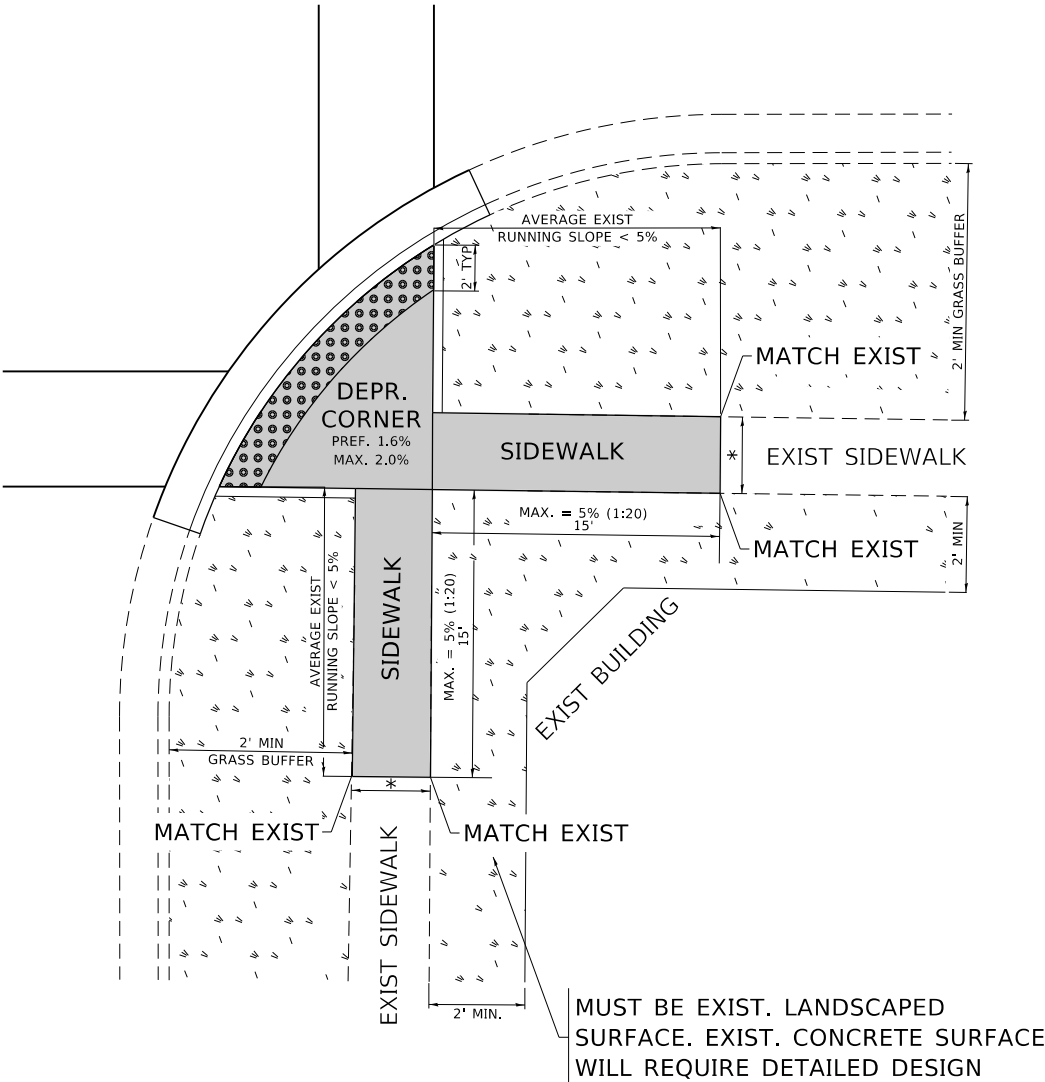
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH  
TURNING SPACE (PD-04)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

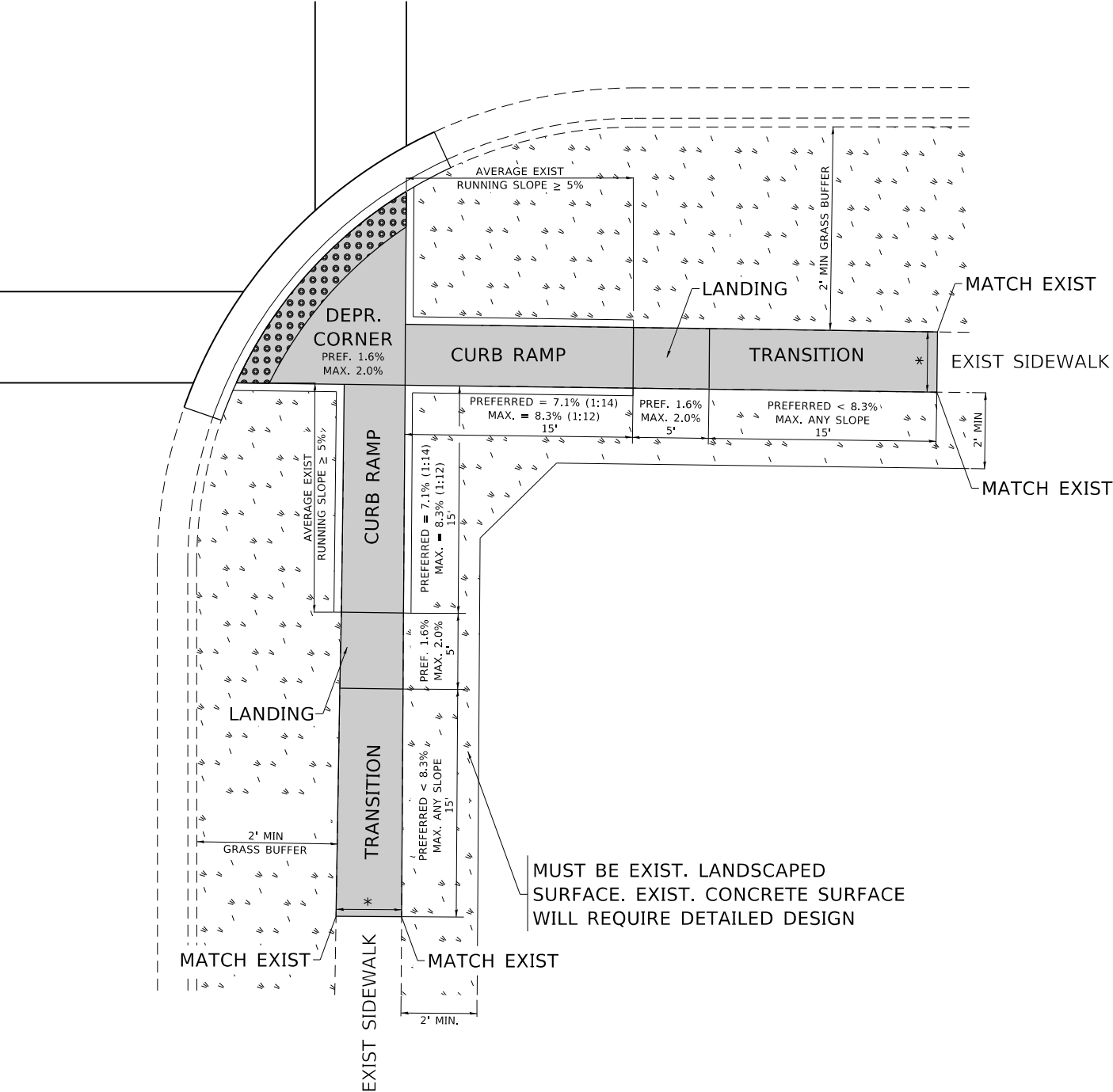
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365	2019-114RS&SW	KANE	41	17
PD-04		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

Model: Default  
File: \\bentley.com\p001\dot\Documents\DOT Office\Bentley\1\Projects\10123822\CAD\Drawings\Design\Bentley.dgn

USER NAME	= yaseen.ureshi	DESIGNED	-	REVISED	-	
		DRAWN	-	R. LEDEZMA	REVISED	-
PLOT SCALE	= 100.0000 ' / in.	CHECKED	-	REVISED	-	
PLOT DATE	= 1/24/2025	DATE	-	10/02/2019	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

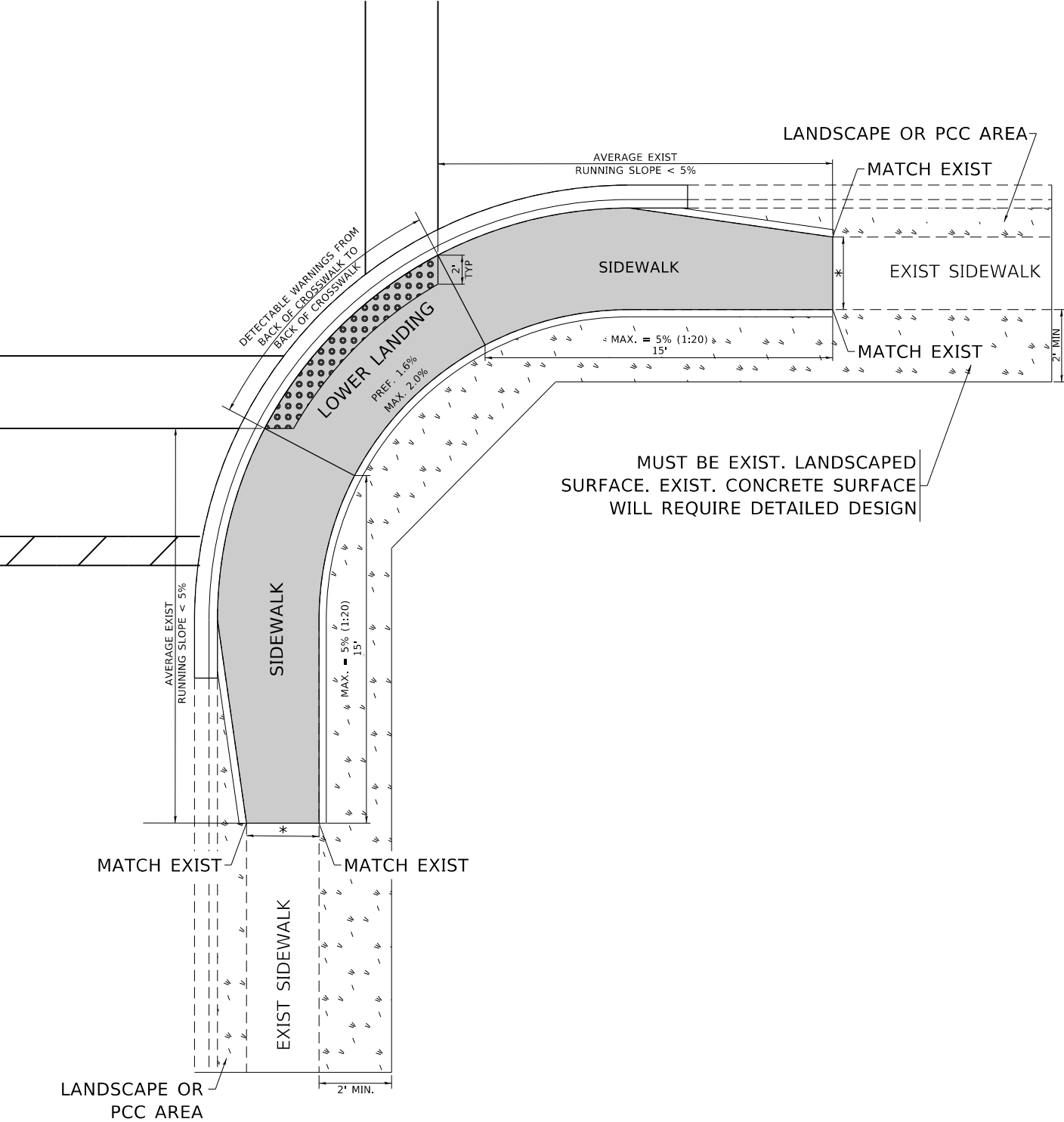
PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS  
(PD-05)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

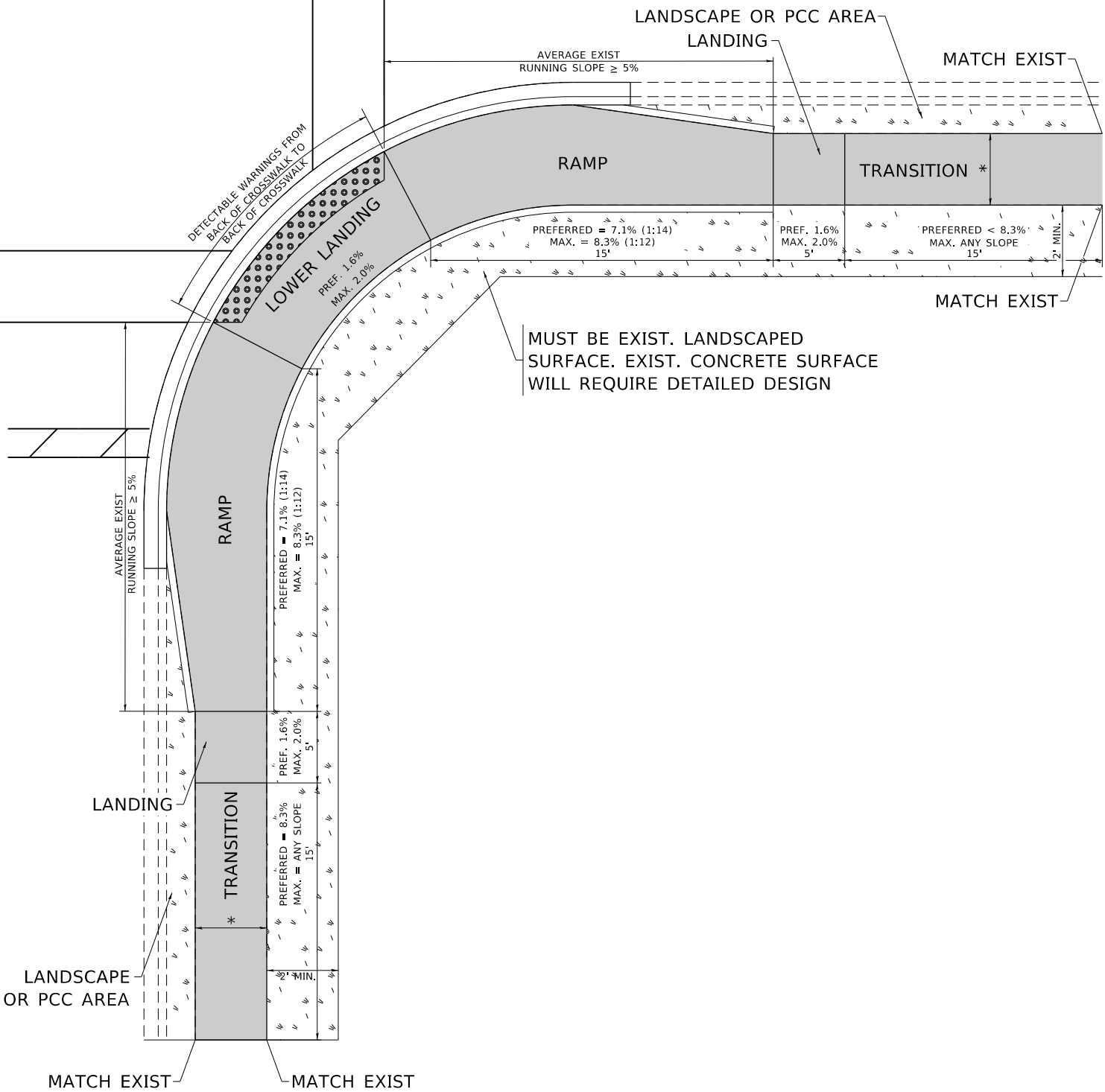
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365	2019-114RS&SW	KANE	41	18
PD-05		CONTRACT NO. 62J72		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
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USER NAME	= yaseen.ureshi
DESIGNED	-
DRAWN	- R. LEDEZMA
CHECKED	-
DATE	- 10/02/2019

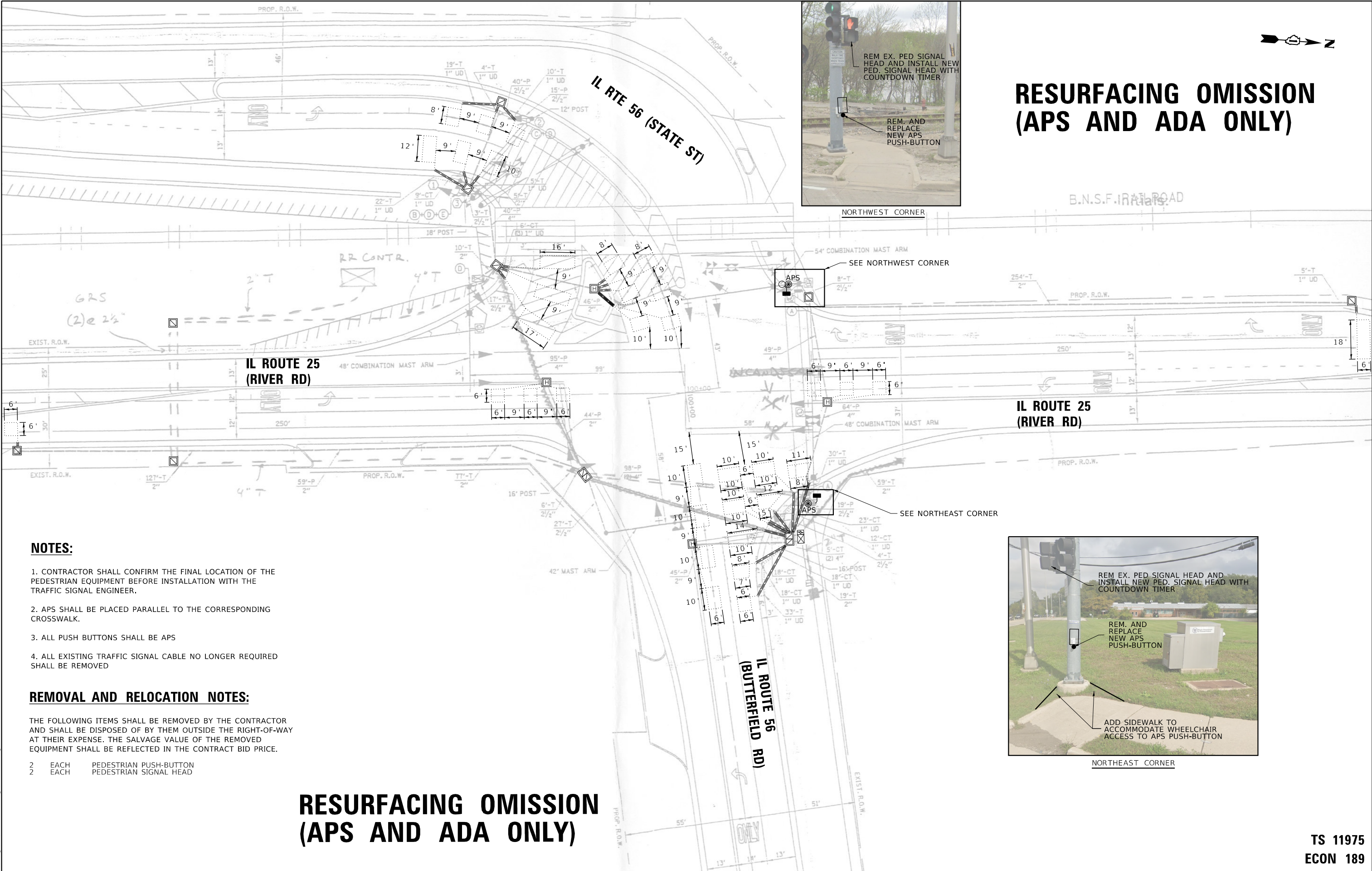
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

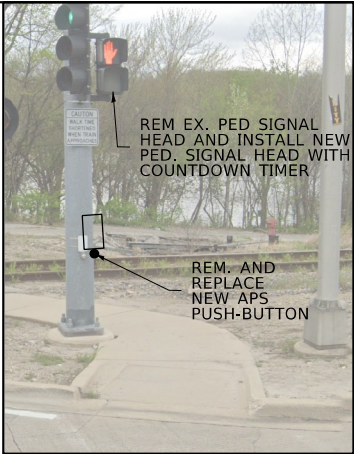
PROJECT DETAIL FOR PARALLEL CURB RAMPS  
(PD-06)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	19
PD-06		CONTRACT NO. 62J72		
		ILLINOIS FED. AID PROJECT		



RESURFACING OMISSION  
(APS AND ADA ONLY)

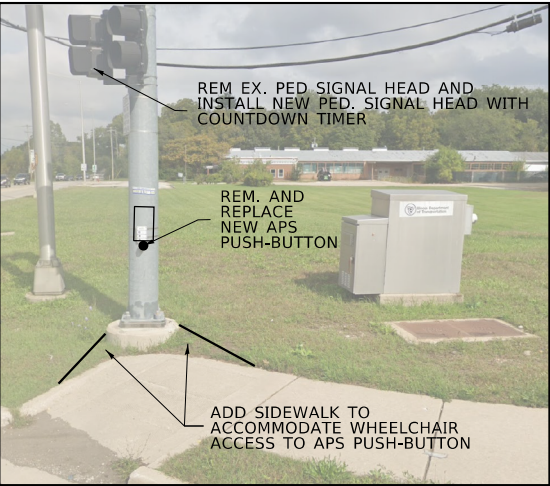


REM EX. PED SIGNAL  
HEAD AND INSTALL NEW  
PED. SIGNAL HEAD WITH  
COUNTDOWN TIMER

REM. AND  
REPLACE  
NEW APS  
PUSH-BUTTON

NORTHWEST CORNER

IL ROUTE 25  
(RIVER RD)



REM EX. PED SIGNAL HEAD AND  
INSTALL NEW PED. SIGNAL HEAD WITH  
COUNTDOWN TIMER

REM. AND  
REPLACE  
NEW APS  
PUSH-BUTTON

ADD SIDEWALK TO  
ACCOMMODATE WHEELCHAIR  
ACCESS TO APS PUSH-BUTTON

NORTHEAST CORNER

NOTES:

1. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
2. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
3. ALL PUSH BUTTONS SHALL BE APS
4. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- |   |      |                        |
|---|------|------------------------|
| 2 | EACH | PEDESTRIAN PUSH-BUTTON |
| 2 | EACH | PEDESTRIAN SIGNAL HEAD |

RESURFACING OMISSION  
(APS AND ADA ONLY)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
IL RTE 25 (RIVER RD) AND IL RTE 56 (BUTTERFIELD RD)

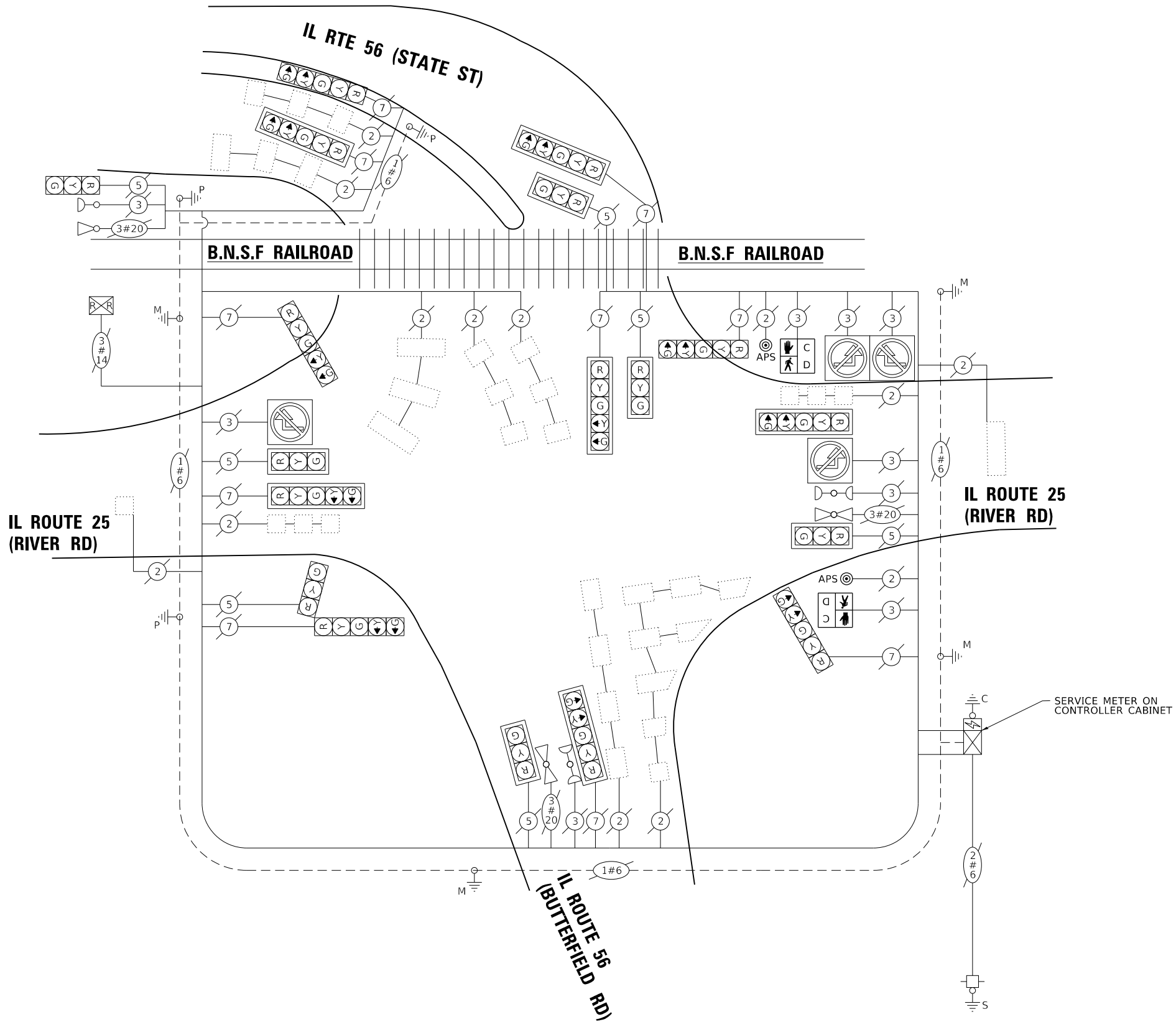
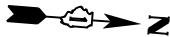
TS 11975  
ECON 189

USER NAME = Jakob.Larson	DESIGNED - J.LARSON	REVISED -
	DRAWN - J.LARSON	REVISED -
PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/19/2024	DATE - 12/17/2024	REVISED -

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G5W	KANE	41	20
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				





TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	7	11	77
4-SECTION	-	14	-
5-SECTION	11	13	143
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	2	15	30
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	4	25	100
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			525
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1130

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 W CENTER CT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: CASSIE EVANS  
PHONE: 773-241-0741  
COMPANY: COMED  
ACCOUNT NUMBER: ---  
METER NUMBER: ---

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNAL	EACH	2
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN AND SCHEDULE OF QUANTITIES  
IL RTE 25 (RIVER RD) AND IL RTE 56 (BUTTERFIELD RD)

SCALE: SHEET OF SHEETS STA. TO STA.

TS 11975  
ECON 189

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G5W	KANE	41	21
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

SEQUENCE OF OPERATION

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IL RTE. 56 (STATE ST.) (EAST OF TRACKS) NEAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	G	G	G	G	Y	R	R	R	G	R	G	
IL RTE. 56 (STATE ST.) (EAST OF TRACKS) NEAR LEFT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	G	G	G	G	Y	R	R	R	G	R	G	
IL RTE. 56 (STATE ST.) (EAST OF TRACKS) MAST ARM AND FAR RIGHT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	G	G	G	G	G	G	Y	R	G	R	G		
IL RTE. 56 (STATE ST.) (EAST OF TRACKS) FAR LEFT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	G	G	G	G	G	G	Y	R	G	R	G		
PEDESTRIAN SIGNALS CROSSING IL RTE. 25 ON NORTH SIDE OF IL RTE. 56	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H		

P = ILLUMINATED PERSON = WALK  
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK  
H = ILLUMINATED SOLID HAND = DON'T WALK  
◇ EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

MODEL Default  
FILE Name: 33019Design\DL03 Inhouse Design\62J72.dgn

USER NAME = Jakob.Larson	DESIGNED - J.LARSON	REVISED -
	DRAWN - J.LARSON	REVISED -
PLOT SCALE = 40,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/19/2024	DATE - 10/29/2024	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION  
IL RTE 25 (RIVER RD) AND IL RTE 56 (BUTTERFIELD RD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G/SW	KANE	41	23
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

TS 11975  
ECON 189

RAILROAD PREEMPTION SEQUENCE OF OPERATION

RAILROAD PREEMPTION SEQUENCE OF OPERATION																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 2						
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	7	9	11	15	19	22																
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																	2	3						
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	2	3	4	5	CLEAR TO NORMAL SEQUENCE
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2	1C	2	1E	2	1G	2	1J	2	1L	2	1N	2	1Q	2	1S	2	1U	2	3	4	5		
IL RTE 25 (RIVER RD.) FAR RIGHT SIGNAL	N/B	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	G	△
IL RTE 25 (RIVER RD.) FAR LEFT AND END MAST ARM SIGNALS	N/B	R ←Y	R	R	Y	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	G	△
IL RTE 25 (RIVER RD.) FAR RIGHT SIGNAL	S/B	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	G	△
IL RTE 25 (RIVER RD.) FAR LEFT AND END MAST ARM SIGNALS	S/B	R ←Y	Y	R	R	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	G	△
IL RTE 56 (BUTTERFIELD RD.) FAR RIGHT SIGNALS	W/B	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (BUTTERFIELD RD.) FAR LEFT AND END MAST ARM SIGNALS	W/B	R	R	R	R	R	R	R ←Y	R	Y	R	R	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (STATE ST.) (WEST OF TRACKS) NEAR RIGHT SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (STATE ST.) (WEST OF TRACKS) NEAR LEFT AND CANTILEVER MOUNTED SIGNALS	E/B	R	R	R	R	R	R	R ←Y	R	R	R	Y	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (STATE ST.) (EAST OF TRACKS) NEAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (STATE ST.) (EAST OF TRACKS) NEAR LEFT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R ←Y	R	R	R	Y	R	Y	R	R	R	Y	R	R	R	R	R	△
IL RTE 56 (STATE ST.) (EAST OF TRACKS) MAST ARM AND FAR RIGHT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	G	G	G	G	R	R	G	G	G	Y	R	R	△
IL RTE 56 (STATE ST.) (EAST OF TRACKS) FAR LEFT SIGNALS	E/B	R	R	R	R	R	R	R ←G	R	R	R	G	G	G	G	R	R	G	G	G ←G	Y	R	R	△
PEDESTRIAN SIGNALS CROSSING IL RTE 25 ON NORTH SIDE OF IL RTE 56	H	H	H	H	H	H	H	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	△
INTERNALLY ILLUMINATED NRT SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	△
INTERNALLY ILLUMINATED NLT SIGNS	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	△
																							HOLD	

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE  
INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER  
CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL  
(IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

MODEL Default  
FILE Name: 33019Design\DL02 Inhouse Design\62J72.dgn

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	DRAWN - J.LARSON	REVISED -
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PLOT DATE = 12/19/2024	DATE - 10/29/2024	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RAILROAD PREEMPTION SEQUENCE OF OPERATION  
IL RTE 25 (RIVER RD) AND IL RTE 56 (BUTTERFIELD RD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G/SW	KANE	41	24
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

TS 11975  
ECON 189



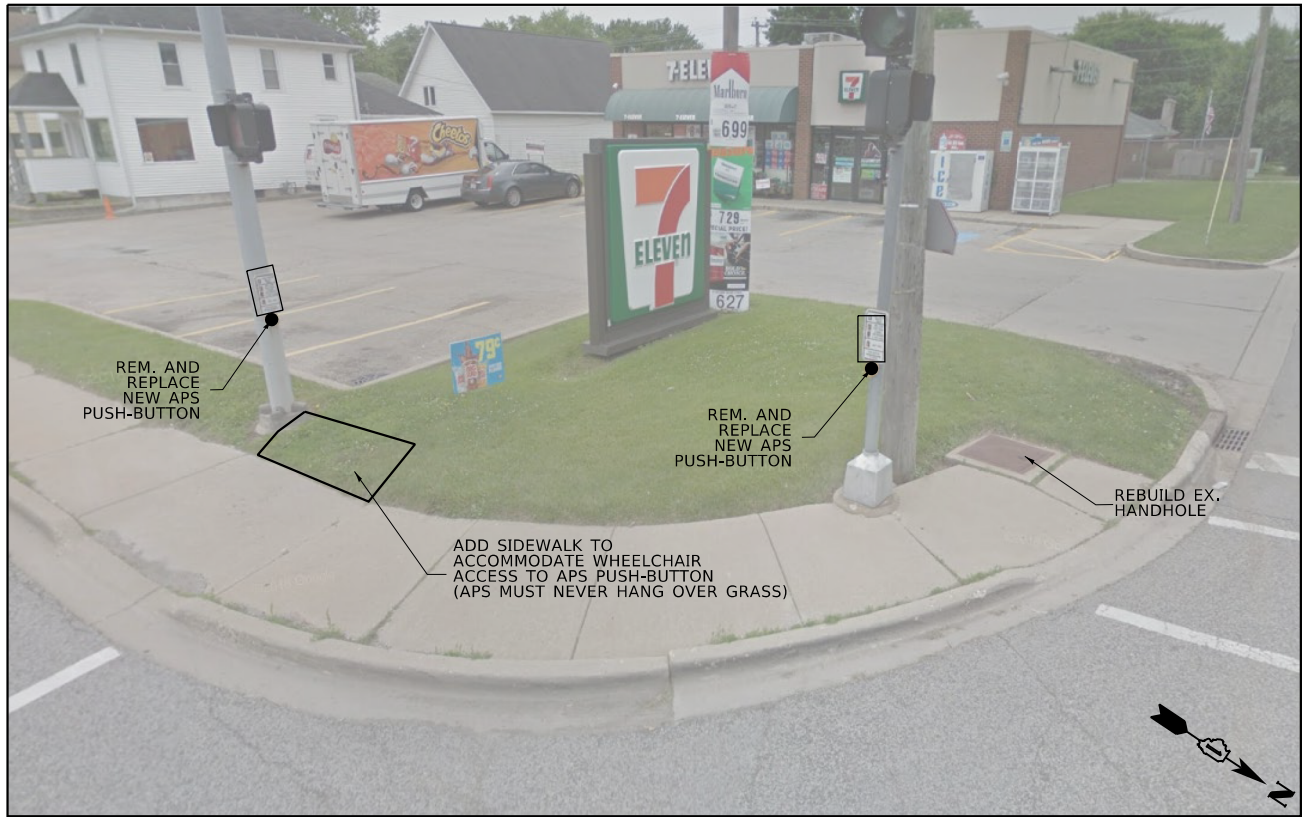




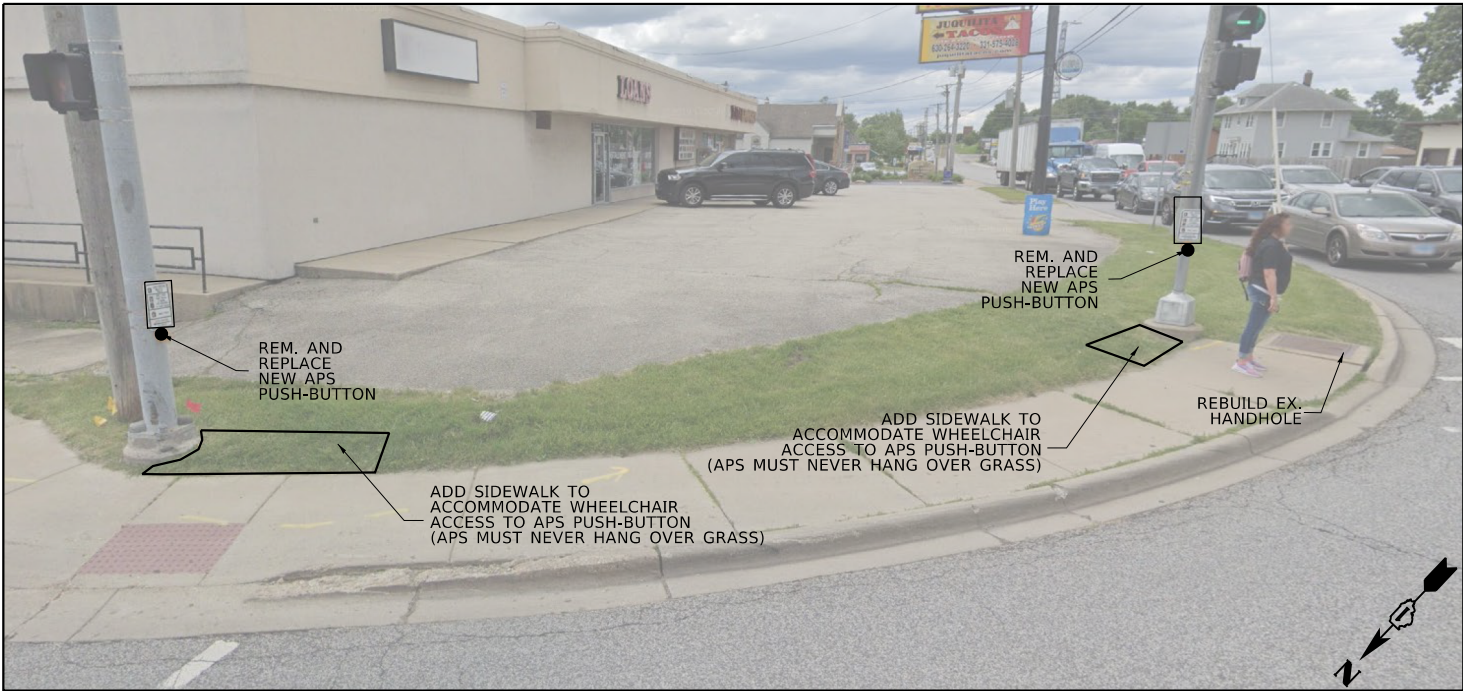
NORTHWEST CORNER



NORTHEAST CORNER



SOUTHWEST CORNER



SOUTHEAST CORNER

TS 11483  
EAGLE 8P

MODEL Default  
FILE Name: 33019Design\DL03 InHouse Design\62J72\62J72.dgn

USER NAME = Jakob.Larson	DESIGNED - J.LARSON	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

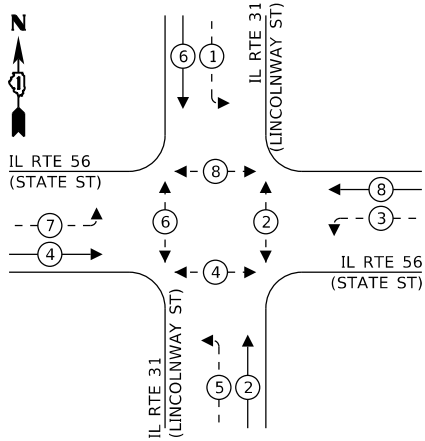
TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 2 OF 2)  
IL RTE 31 (LINCOLNWAY ST) AND IL RTE 56 (STATE ST)

SCALE: SHEET OF SHEETS STA. TO STA.

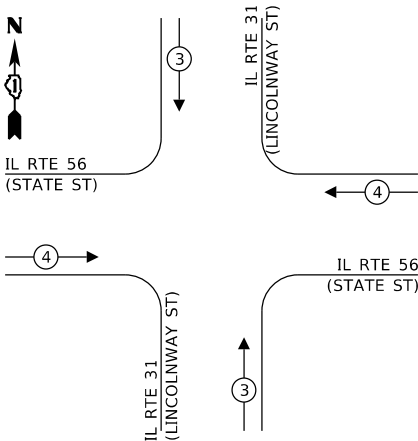
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R56SW	KANE	41	26
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				



EXISTING CONTROLLER SEQUENCE



EXISTING EMERGENCY VEHICLE  
PREEMPTION SEQUENCE



LEGEND:

- ← (⊙) → PROTECTED PHASE
- ← (⊙) - - (⊙) → PROTECTED/PERMITTED PHASE
- ← (⊙) → PEDESTRIAN PHASE
- ← (⊙) OL → OVERLAP

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	60
HEAVY-DUTY HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	298
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	97
DRILL EXISTING HANDHOLE	EACH	2
DETECTOR LOOP, TYPE 1	FOOT	670
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	244
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	3
ACCESSIBLE PEDESTRIAN SIGNAL	EACH	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

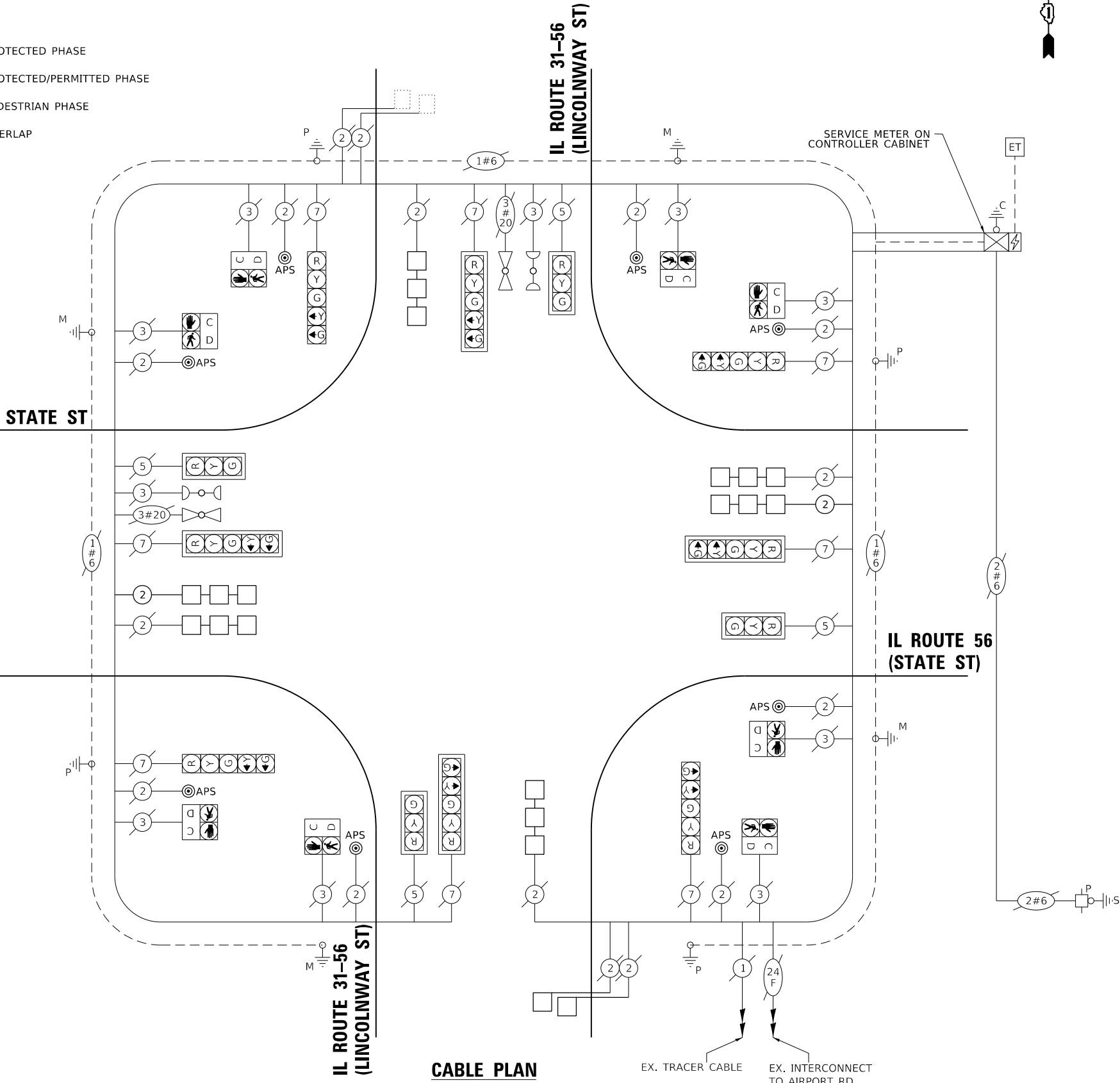
TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	4	11	44
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	8	15	120
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING		443	
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING		1048	

ENERGY COSTS TO:

VILLAGE OF SOUTH ELGIN  
10 N WATER ST  
SOUTH ELGIN, IL 60177

ENERGY SUPPLY: CONTACT: CASSIE EVANS  
PHONE: 773-241-0741  
COMPANY: COMED  
ACCOUNT NUMBER: ---  
METER NUMBER: ---



CABLE PLAN

TS 11483  
EAGLE 8P

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY  
VEHICLE PREEMPTION SEQUENCE AND SCHEDULE OF QUANTITIES  
IL RTE 31 (LINCOLNWAY ST) AND IL RTE 56 (STATE ST)

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G5W	KANE	41	27
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

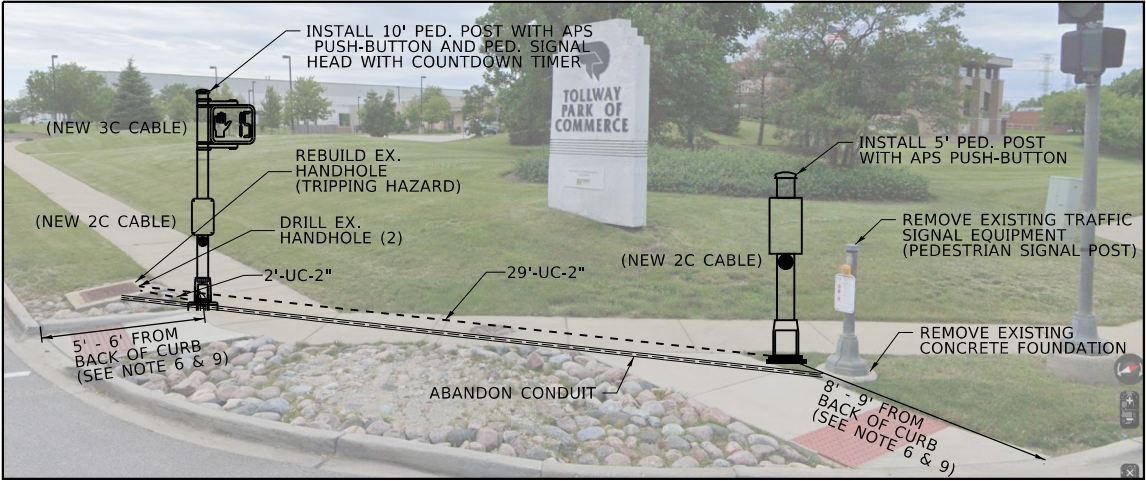
NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
3. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
4. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
5. ALL PUSH BUTTONS SHALL BE APS
6. NO PROPOSED PEDESTRIAN POST SHALL EXCEED 10 FT FROM BACK OF CURB
7. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED
8. ALL PROPOSED TRAFFIC SIGNAL CABLE SHALL BE ROUTED THROUGH THE PROPOSED CONDUIT
9. THERE SHALL BE A MINIMUM OF 4' SIDEWALK CLEARANCE NEXT TO TRAFFIC SIGNAL FOUNDATIONS TO BE ADA COMPLIANT
10. REUSE THE INDUCTIVE LOOP DETECTOR FROM THE OLD LEFT TURN LANE LOOPS FOR THE PROPOSED LEFT TURN LANE LOOPS. REMOVE THE OLD LEAD-IN FROM THE OLD LEFT TURN LANE LOOPS AND USE BRAND NEW LEAD-IN FOR THE PROPOSED LEFT TURN LANE LOOPS

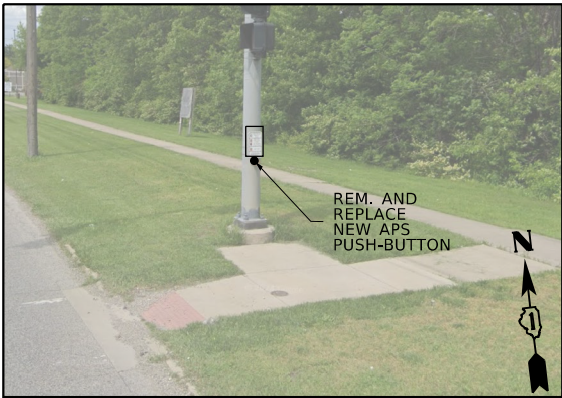
REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

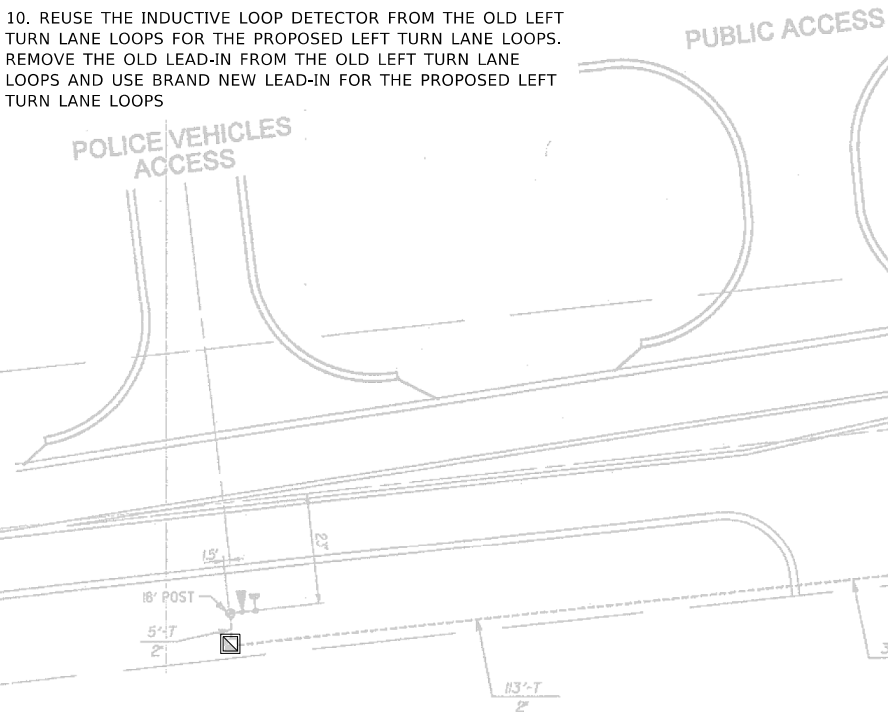
2	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	PEDESTRIAN SIGNAL POST



NORTHWEST CORNER



NORTHEAST CORNER



SOUTHWEST CORNER

SEE RIGHT  
MATCH LINE A-A

IL RTE 31-56  
LINCOLNWAY ST

SB FAR OUTS

IL RTE 31-56  
LINCOLNWAY ST

SEE ABOVE  
MATCH LINE A-A

SEE BELOW  
MATCH LINE B-B

NB FAR OUTS

IL RTE 31-56  
LINCOLNWAY ST

IL RTE 31-56  
LINCOLNWAY ST

SEE RIGHT  
MATCH LINE B-B

TS 11482  
EAGLE 8P

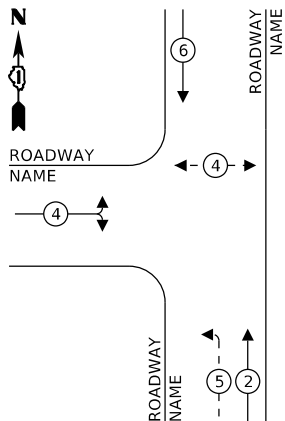
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN  
IL RTE 31 (LINCOLNWAY ST) AND AIRPORT RD

SCALE: SHEET OF SHEETS STA. TO STA.

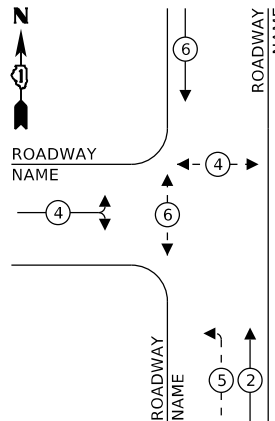
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G5W	KANE	41	28
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

## EXISTING CONTROLLER SEQUENCE



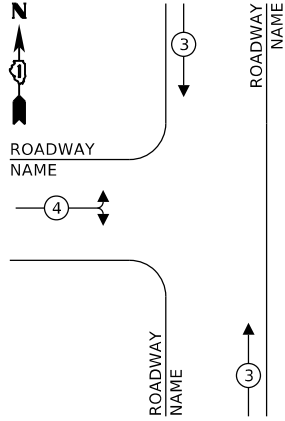
## PROPOSED CONTROLLER SEQUENCE

LEADING PEDESTRIAN INTERVAL ON PHASE 6 AND PHASE 4



## EXISTING EMERGENCY VEHICLE

## PREEMPTION SEQUENCE



## SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	98
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	140
HEAVY-DUTY HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	291
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	167
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1024
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	261
DRILL EXISTING HANDHOLE	EACH	8
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
INDUCTIVE LOOP DETECTOR	EACH	2
DETECTOR LOOP, TYPE 1	FOOT	424
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	236
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	3
REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT	EACH	1
ACCESSIBLE PEDESTRIAN SIGNAL	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	9	11	99
4-SECTION	-	14	-
5-SECTION	2	13	26
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	4	15	60
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			360
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			965

## NOTES:

1. TIMING CONSULTANT SHALL PROGRAM LEADING PEDESTRIAN INTERVAL AT THIS INTERSECTION. THIS WORK SHALL BE PAID FOR UNDER RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1

## ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 W. CENTER CT  
SCHAUMBURG, IL 60196

## ENERGY SUPPLY:

CONTACT: CASSIE EVANS

PHONE: 773-241-0741

COMPANY: COMED

ACCOUNT NUMBER: ---

METER NUMBER: ---

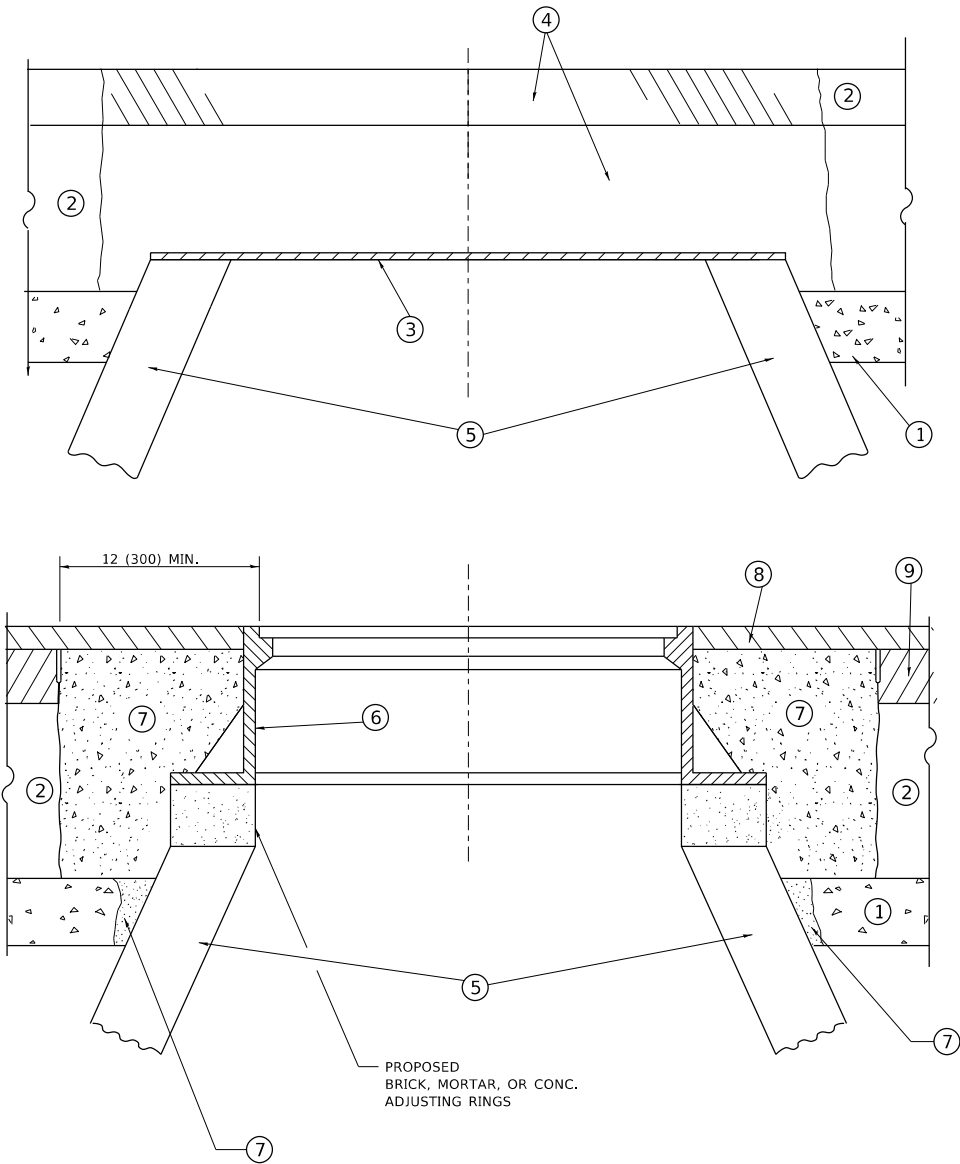
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY  
VEHICLE PREEMPTION SEQUENCE AND SCHEDULE OF QUANTITIES  
IL RTE 31 (LINCOLNWAY ST) AND AIRPORT RD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114R5G/SW	KANE	41	29
CONTRACT NO. 62J72				
ILLINOIS FED. AID PROJECT				

TS 11482  
EAGLE 8P



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: Default  
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	USER NAME = yaseen.ureshi	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 12-06-11					365	2019-114RS&SW	KANE	41	30
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. SMITH 11-18-22		BD600-03 (BD-08)			CONTRACT NO. 62J72				
	PLOT DATE = 1/24/2025	DATE - 10-25-94	REVISED - K. SMITH 09-15-23		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

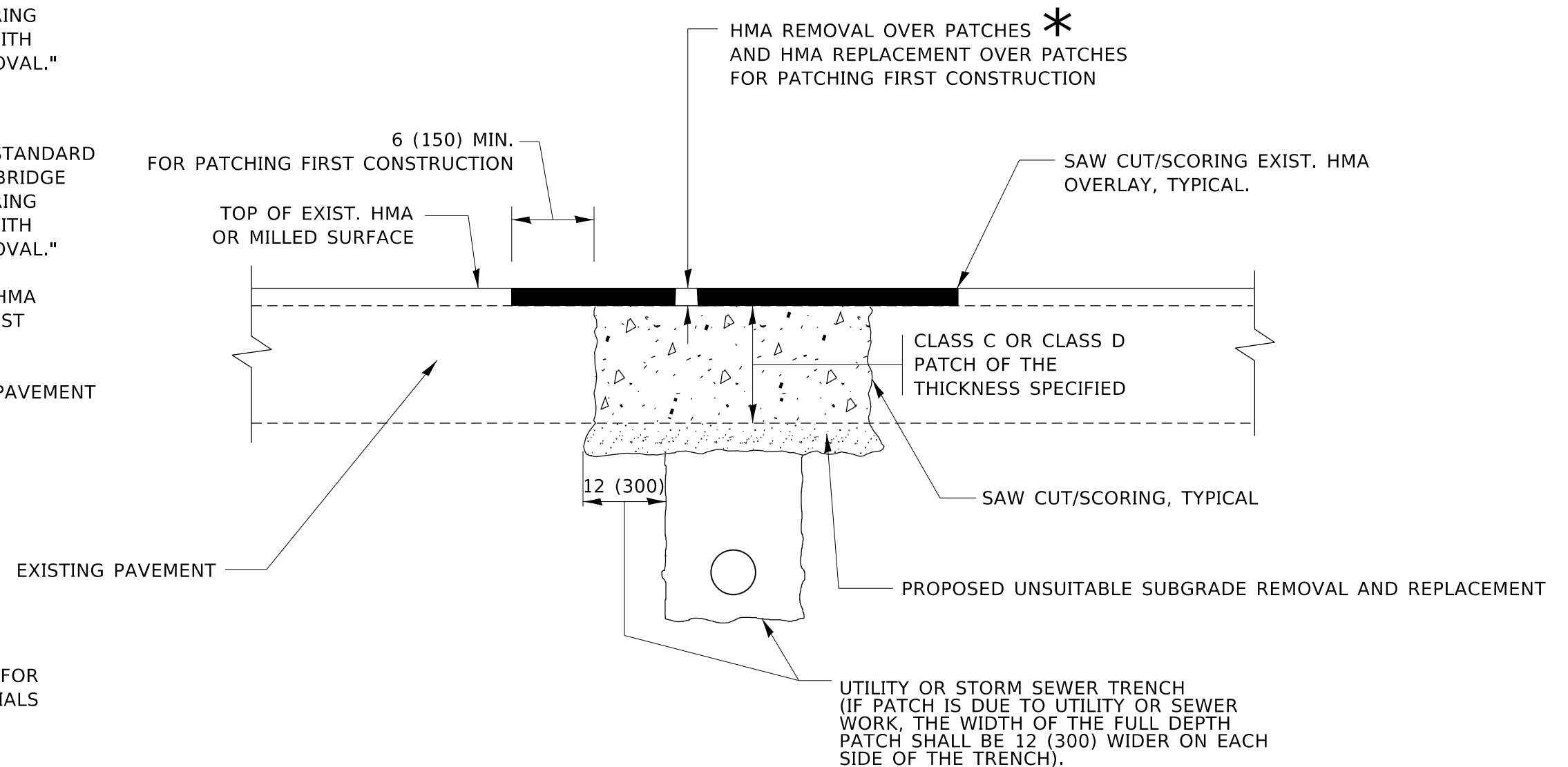


## METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

### **BASIS OF PAYMENT**

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

### SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

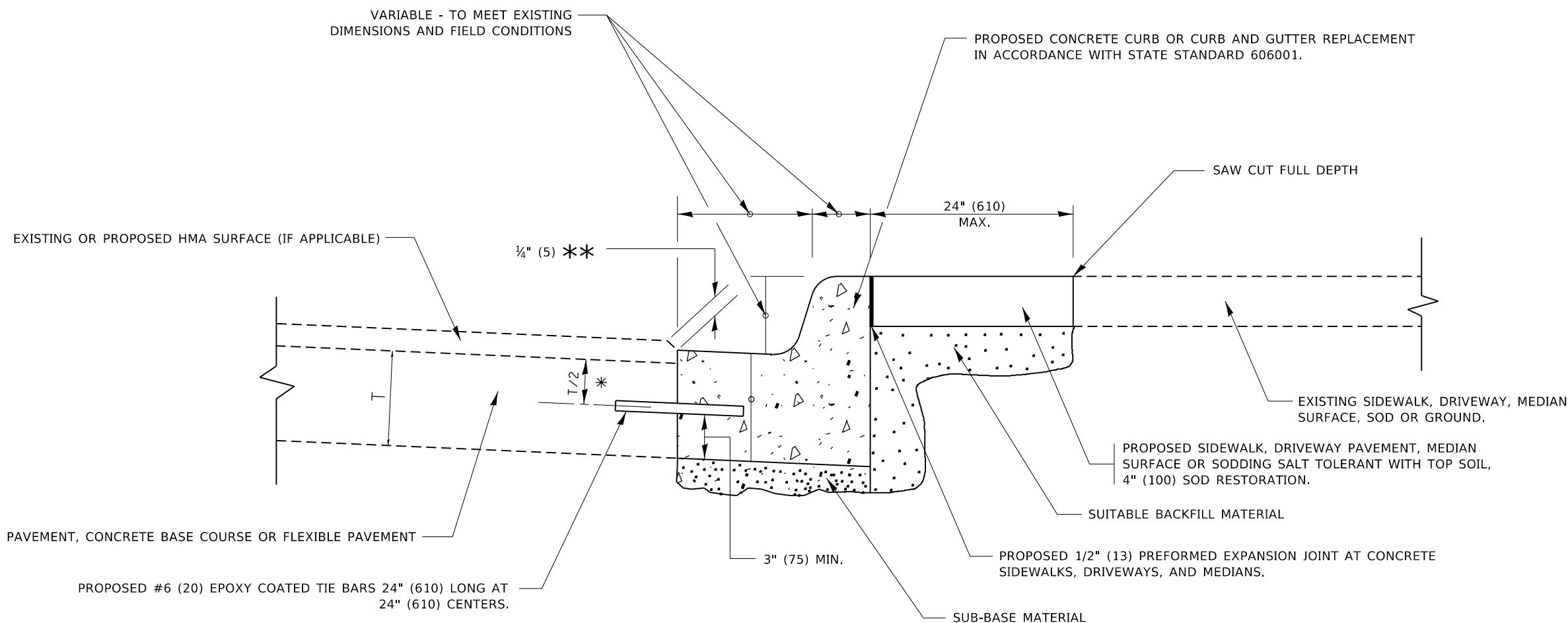
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

### SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME: DMS	USER NAME = yaseen.qureshi	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 09-04-07						365	2019-114RS&SW	KANE	41	31
	PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)								
	PLOT DATE = 1/24/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22										
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 62J72		



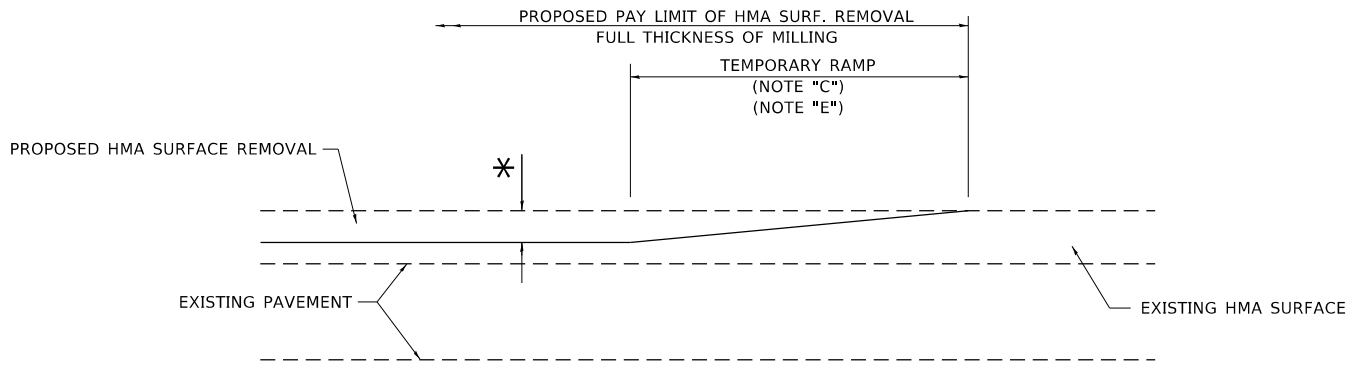
- ✱ 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ✱✱ IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

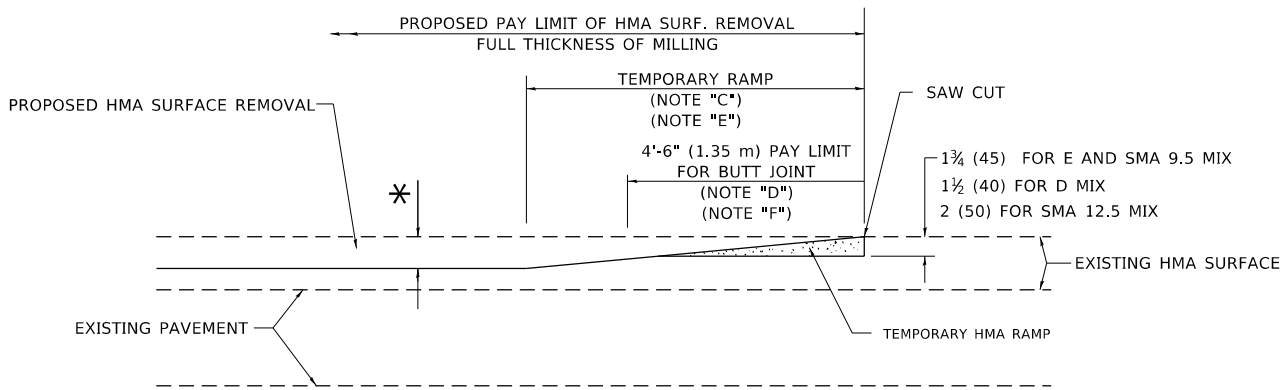
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	USER NAME = yaseen.queeshi	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - M. GOMEZ 01-22-01					365	2019-114RS&SW	KANE	41	32
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09					BD600-06 (BD-24)				
	PLOT DATE = 1/24/2025	DATE - 03-11-94	REVISED - K. SMITH 07-11-19		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

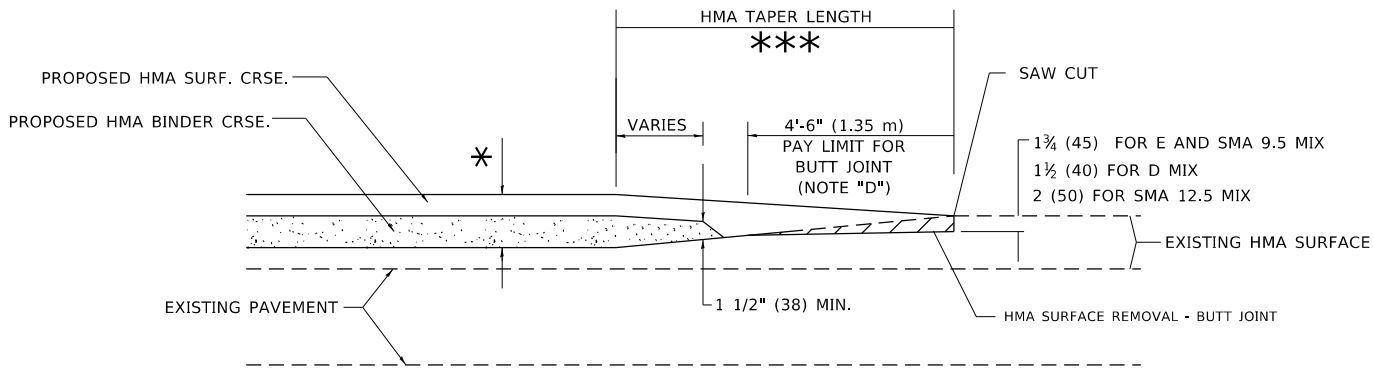
**OPTION 1**



**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

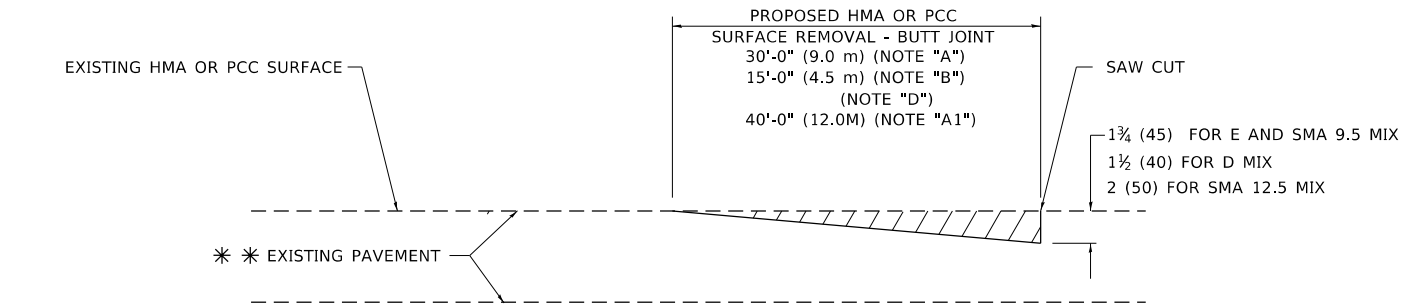
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

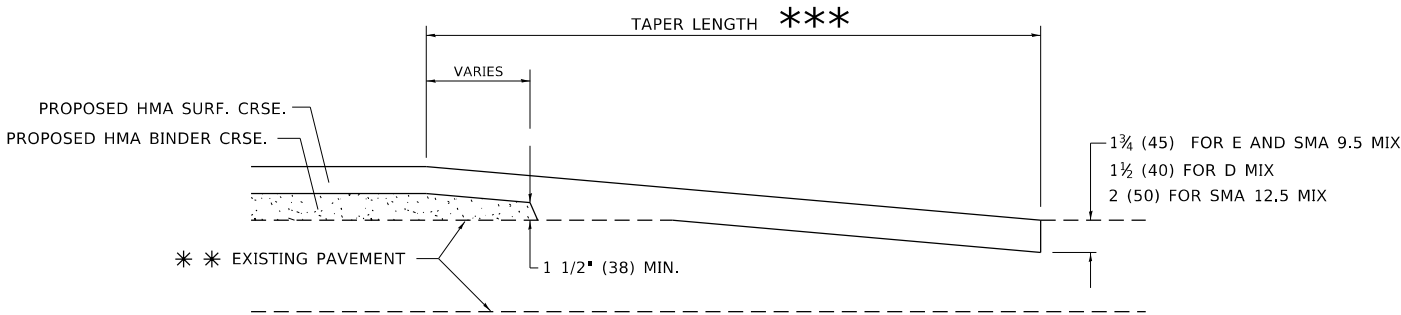


**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**GENERAL NOTES**

- MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- INTERSTATES
- MINOR SIDE ROADS.
- THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = yaseen.ureshi	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 1/24/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

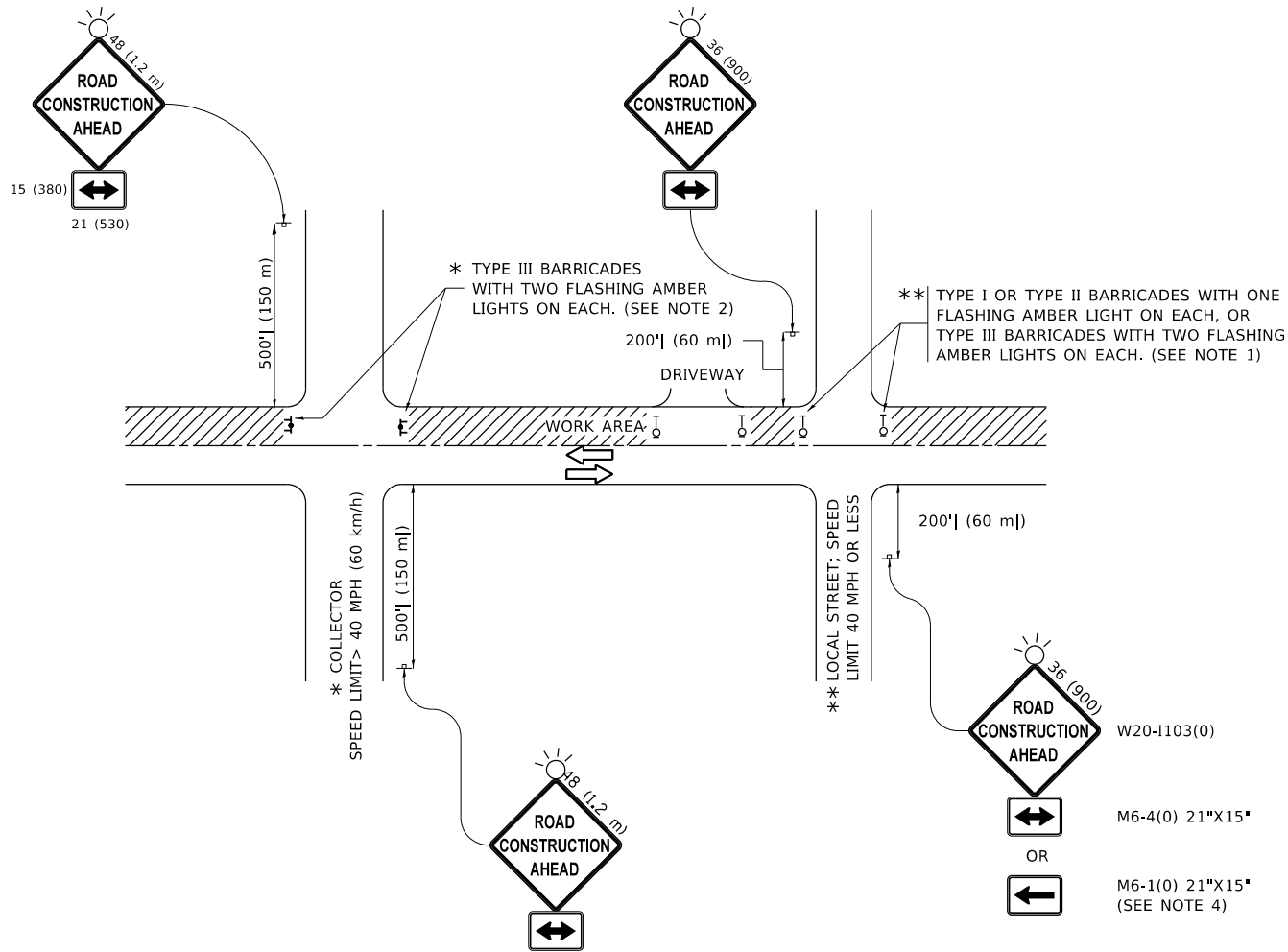
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	33
BD400-05 BD-32		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	

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NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters)  
unless otherwise shown.

USER NAME = yaseen.quareshi	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00
	DRAWN -	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 1/24/2025	DATE - 06-89	REVISED - D. SENDERAK 05-03-24

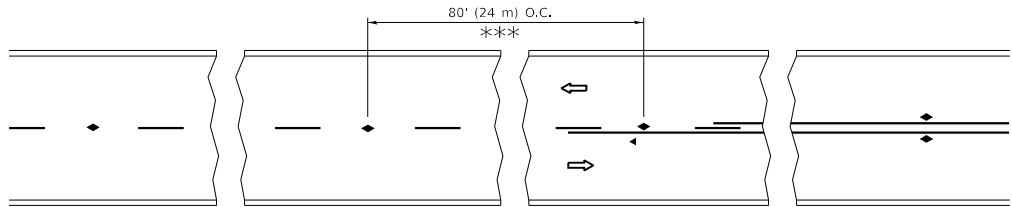
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

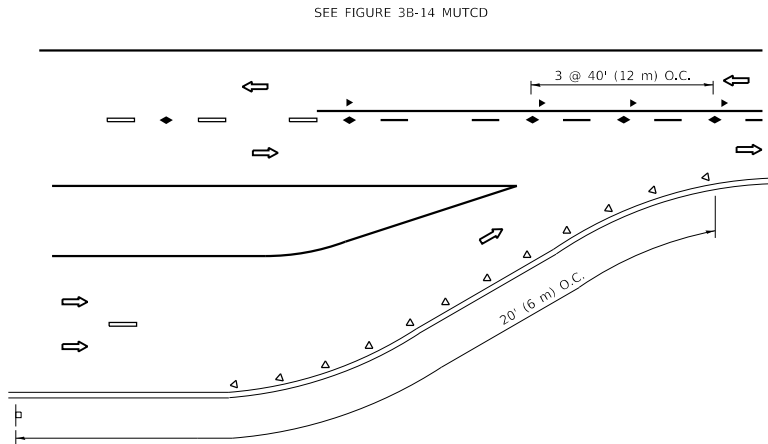
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TC-10		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	



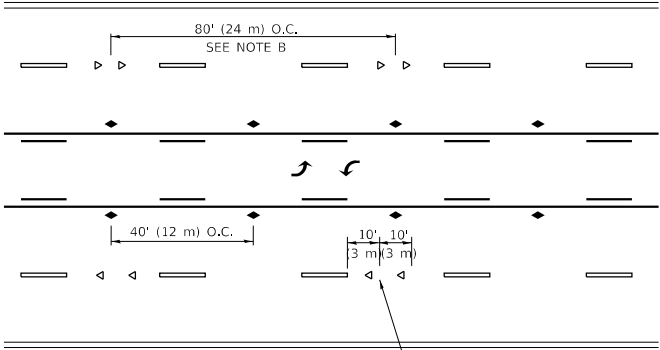


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

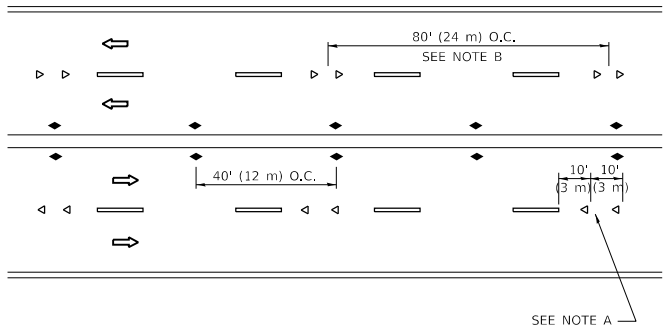
### TWO-LANE/TWO-WAY



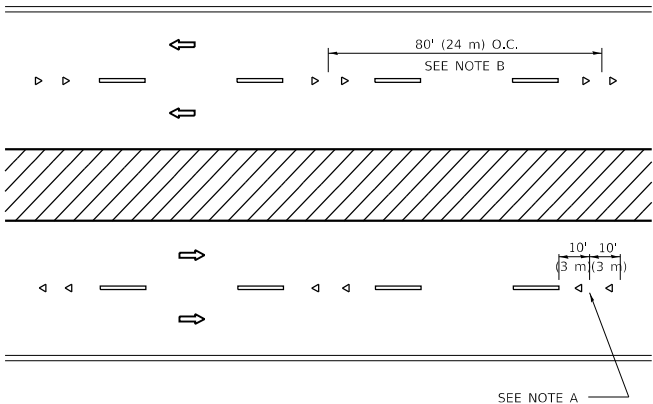
### LANE REDUCTION TRANSITION



### TWO-WAY LEFT TURN



### MULTI-LANE/UNDIVIDED



### MULTI-LANE/DIVIDED

### GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

### LANE MARKER NOTES

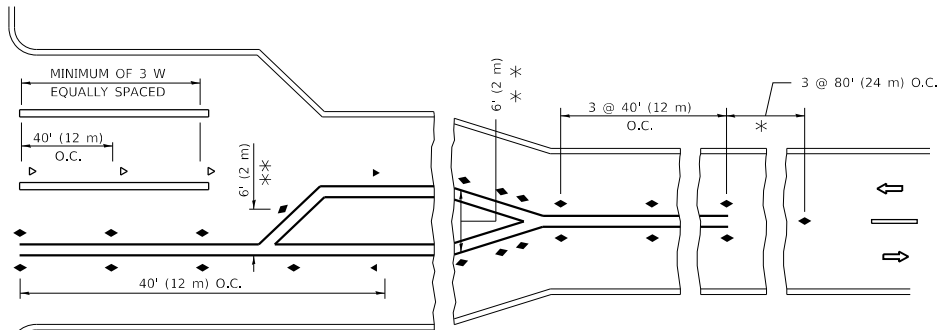
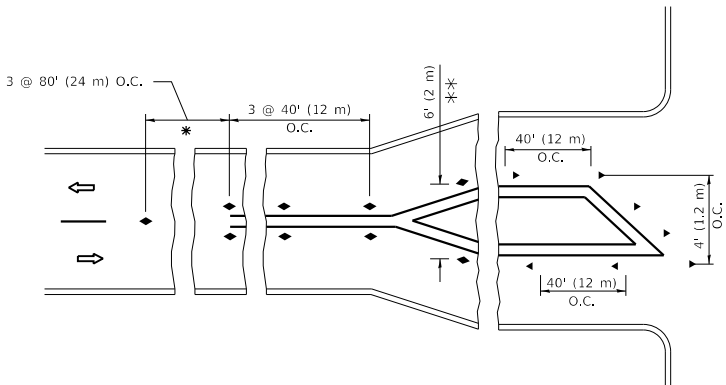
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

### SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

### DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
\*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

### TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = yaseen.ureshi	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 1/24/2025	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	35
TC-11		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	

## 2-LANE ROADWAY

## MULTI-LANE UNDIVIDED

**MULTI-LANE DIVIDED**  
WITH MEDIAN

## TYPICAL LANE AND EDGE LINE MARKING

**DETAIL "A"**

**DETAIL "B"**

## TYPICAL CROSSWALK MARKING

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

**4' (1.2 m) WIDE MEDIANS ONLY**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

### **MEDIANS OVER 4' (1.2 m) WIDE**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

### **MEDIAN WITH TWO-WAY LEFT TURN LANE**

## TYPICAL PAINTED MEDIAN MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) **ONLY** AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

## TYPICAL TURN LANE MARKING

ISLAND OFFSET FROM PAVEMENT EDGE

ISLAND AT PAVEMENT EDGE

## TYPICAL ISLAND MARKING

## COMBINATION LEFT AND U-TURN

## U-TURN

## LANE REDUCTION TRANSITION

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION  8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW  WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "R" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8' )	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

TURN BAY ENTRANCE AT START  
OF LANE CLOSURE TAPER

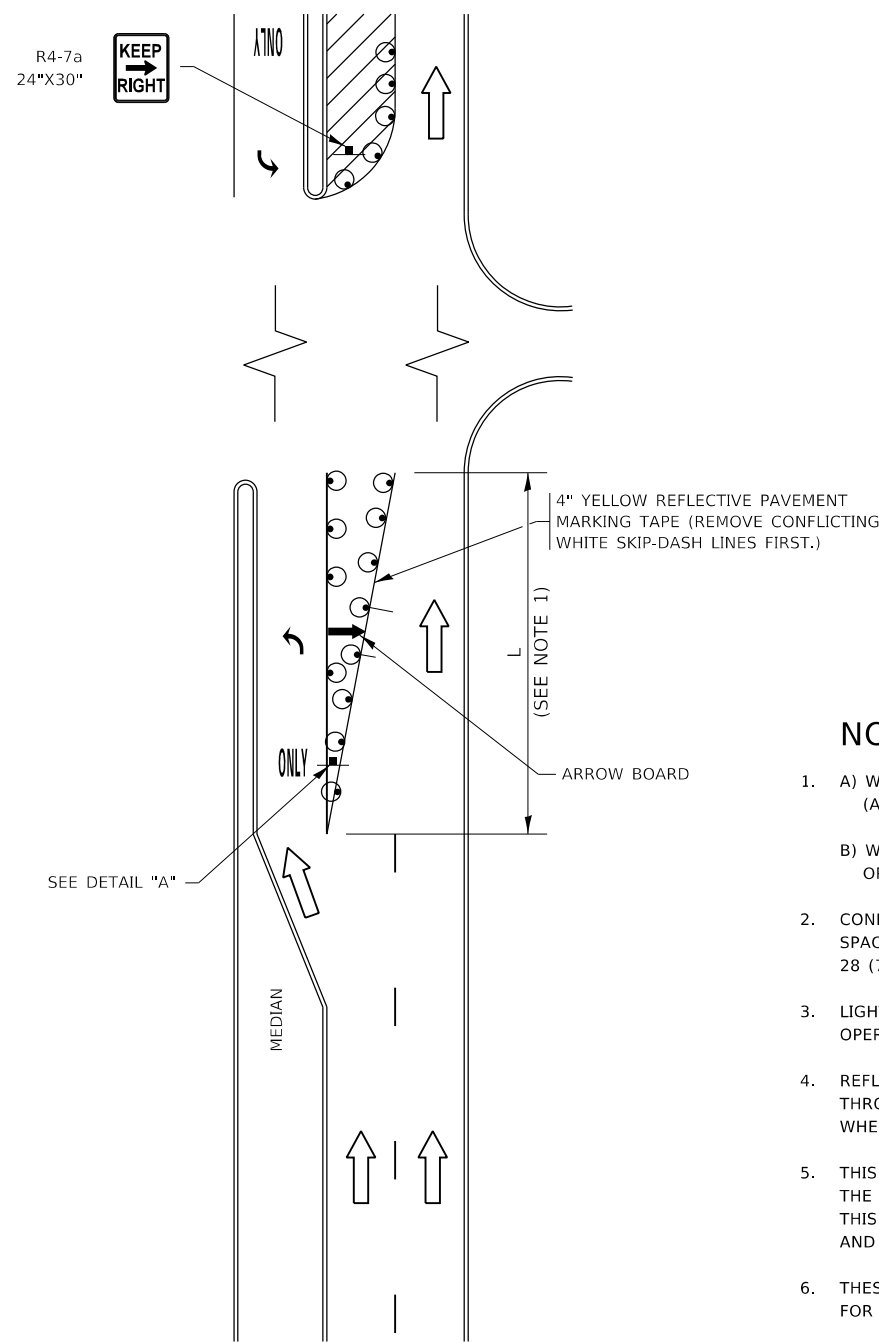


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

- A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE  
WITHIN A LANE CLOSURE

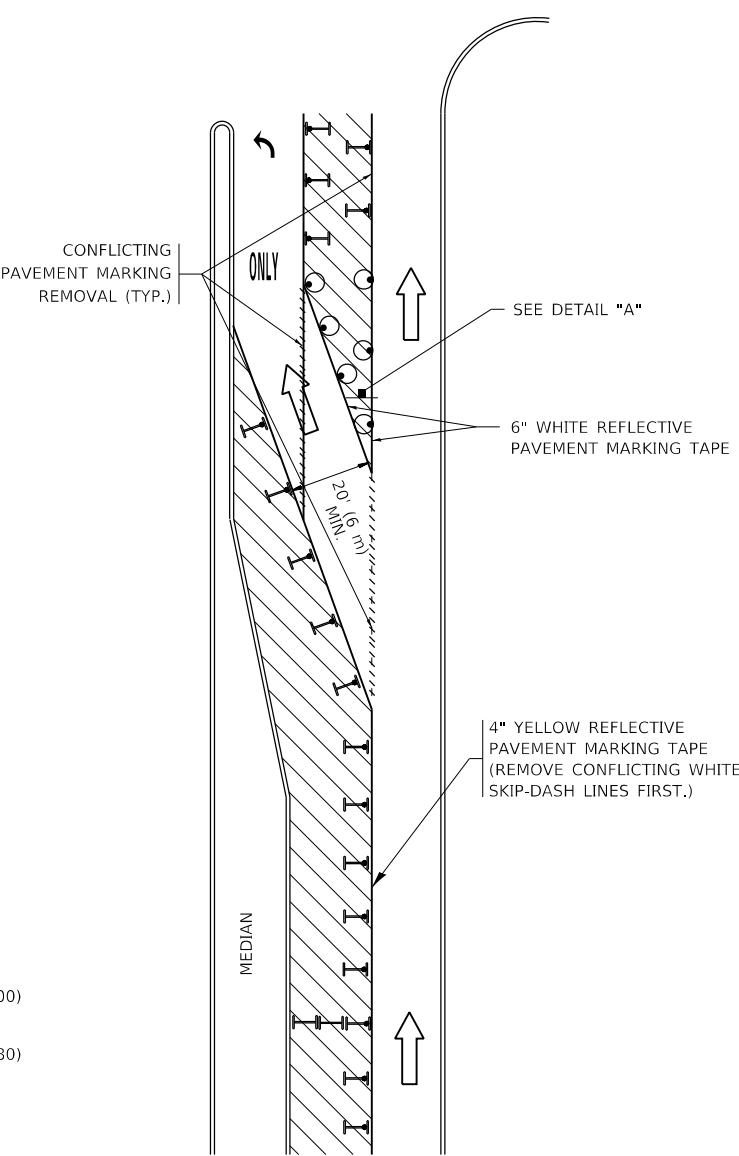
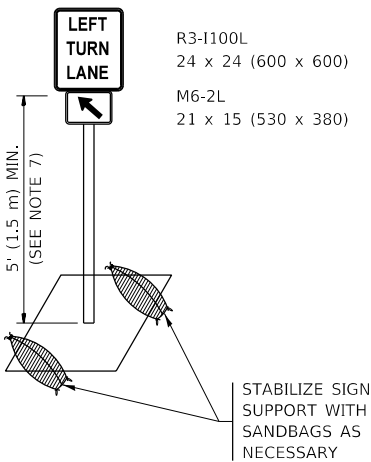


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME	= yaseen.ureshi
DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
PLOT SCALE	= 100,0000 ' / in.
PLOT DATE	= 1/24/2025
CHECKED	- A. HOUSEH 10-12-96
DATE	- T. RAMMACHER 01-06-00

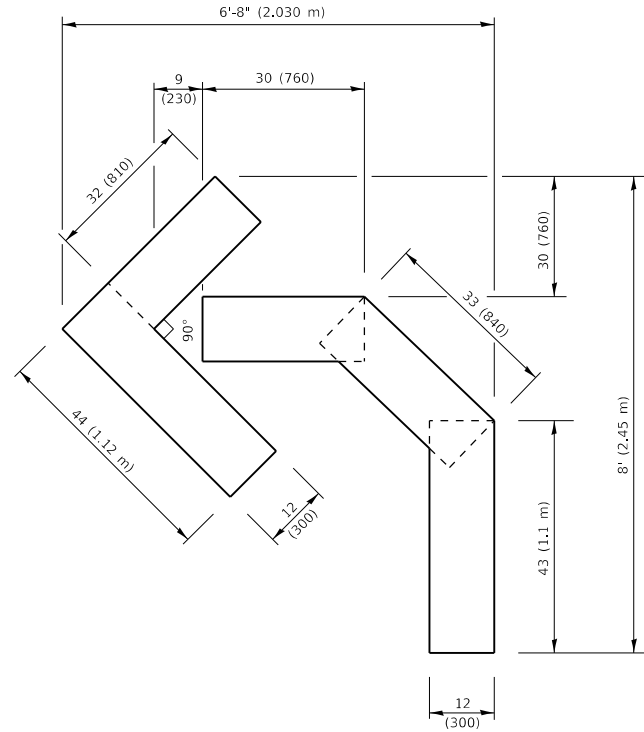
REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

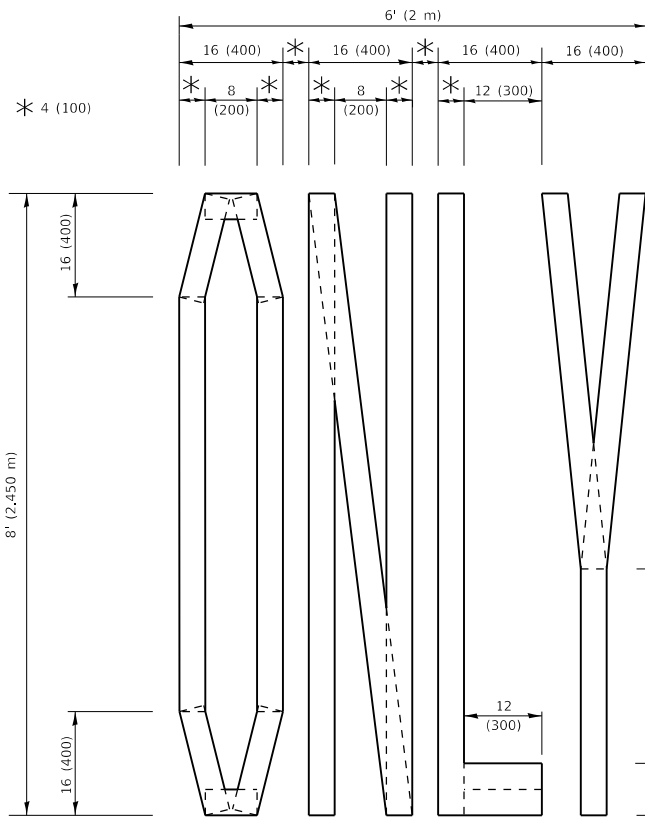
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	37
TC-14		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	



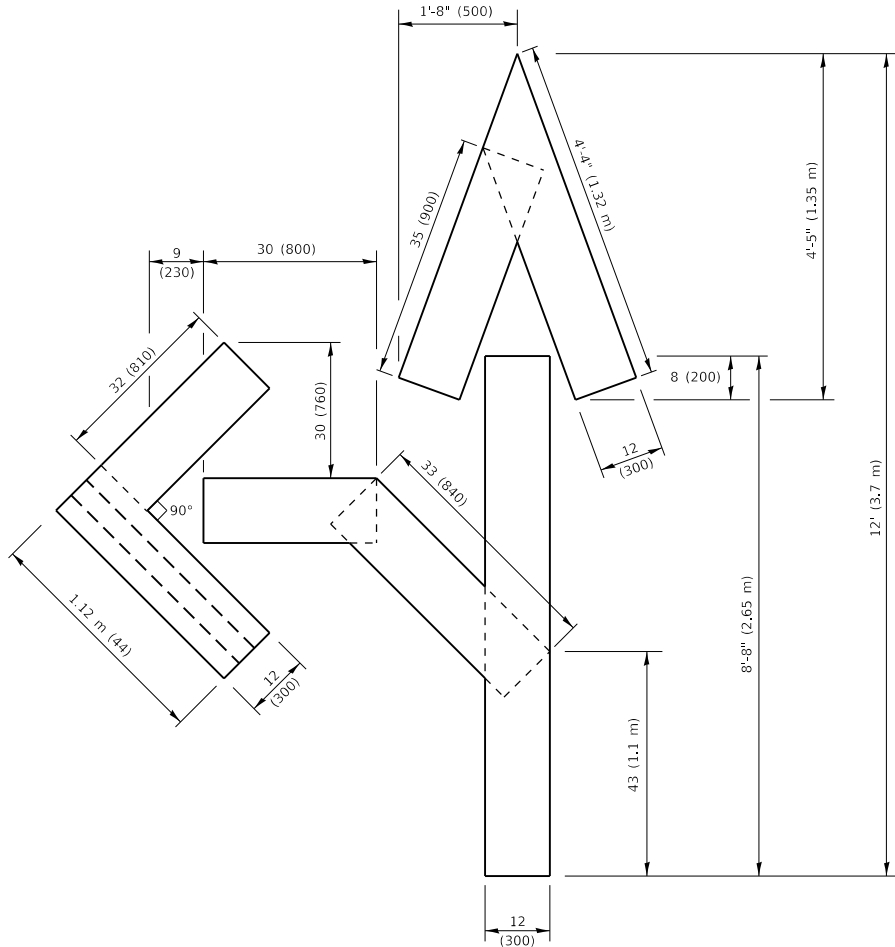
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

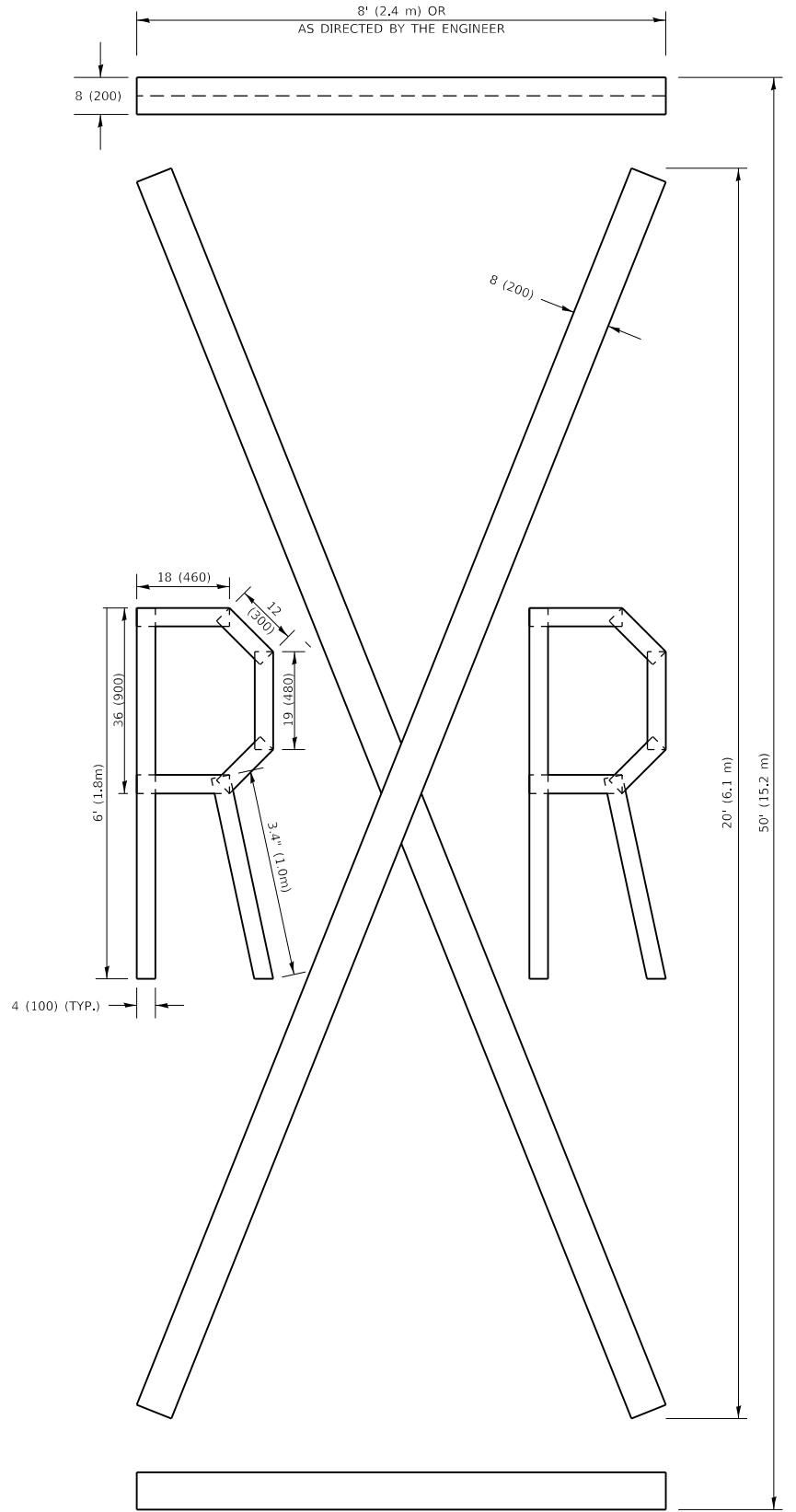


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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	USER NAME = yaseen,qureshi	DESIGNED -	REVISED - T. RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - E. GOMEZ 08-28-00						365	2019-114RS&SW	KANE	41	38
	PLOT SCALE = 100,0010 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00						TC-16				
	PLOT DATE = 1/24/2025	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.		ILLINOIS	

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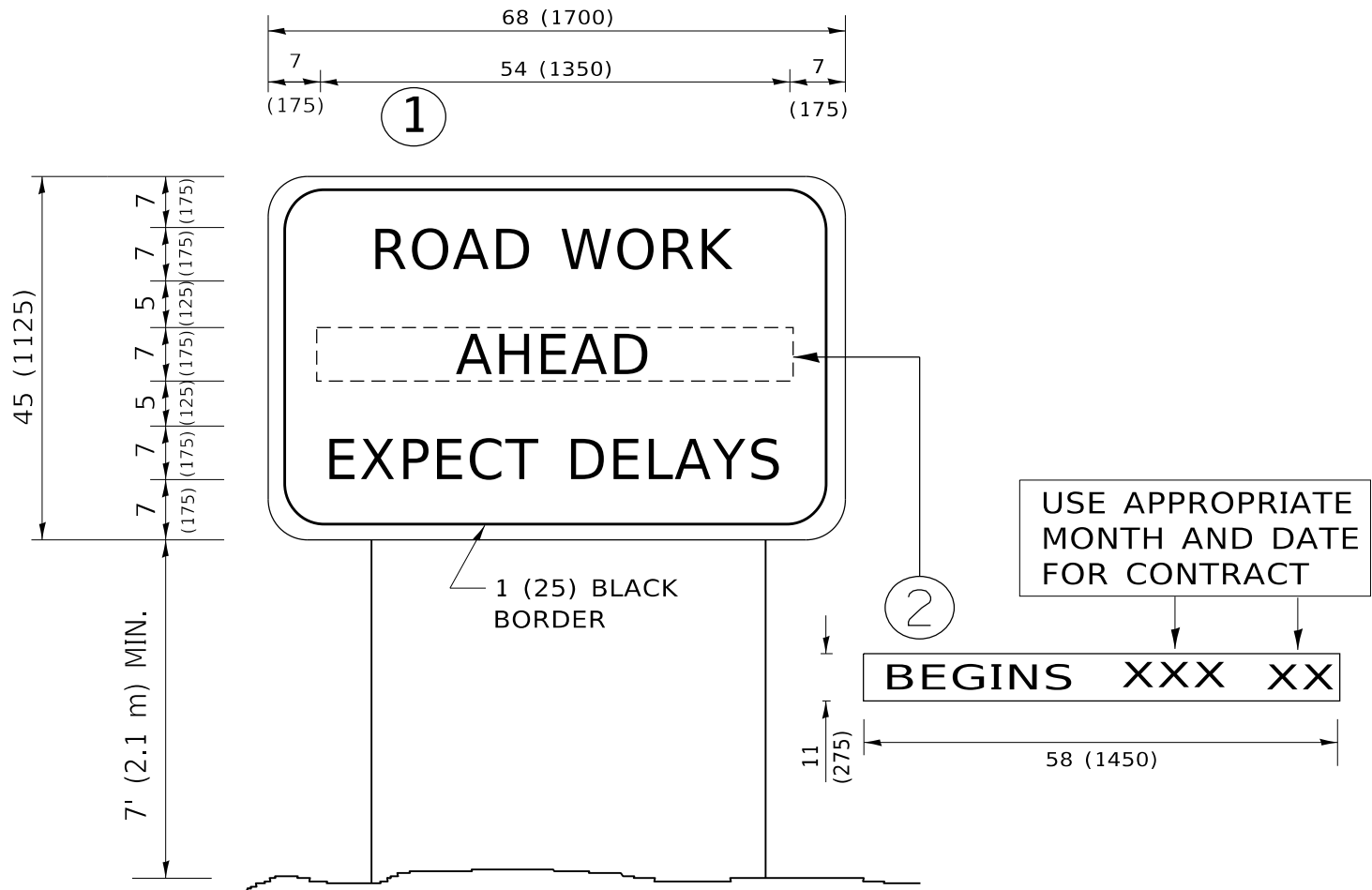
USER NAME	= yaseen.queeshi	DESIGNED	-	REVISED	-	R. MIRS	09-15-97
		DRAWN	-	REVISED	-	R. MIRS	12-11-97
PLOT SCALE	= 100.0010 ' / in.	CHECKED	-	REVISED	-	T. RAMMACHER	02-02-99
PLOT DATE	= 1/24/2025	DATE	-	REVISED	-	C. JUCIUS	01-31-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD  
INFORMATION SIGN

SCALE: NONE    SHEET 1    OF 1    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	39
TC-22		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	



## NOTES:

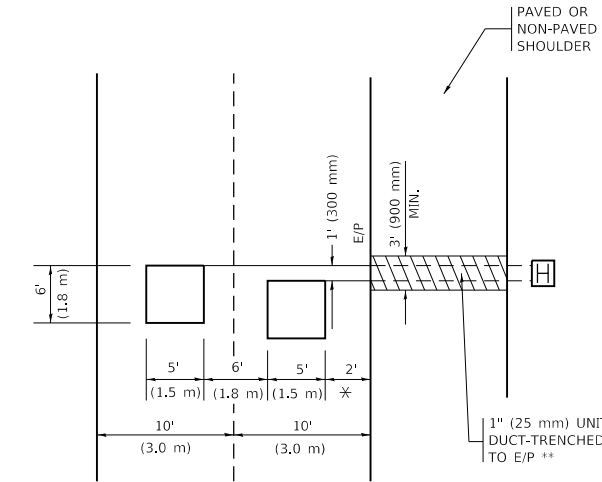
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT  
NOTE WHICH SHOULD EQUAL  
3' (900 mm) X WIDTH OF  
PAVED SHOULDER.

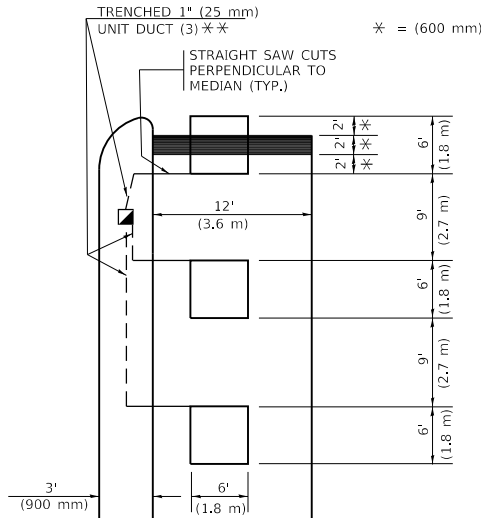


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

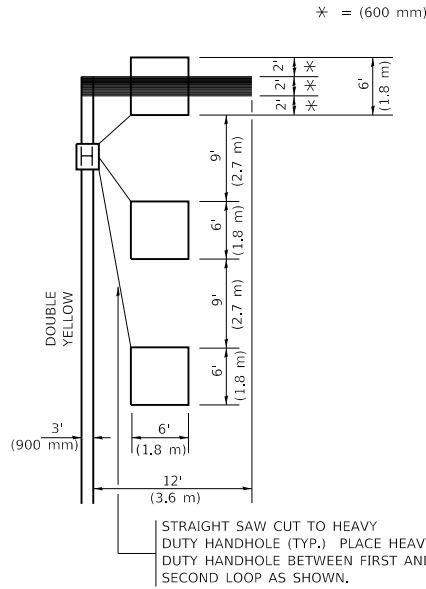
HANDHOLE LOCATION MAY  
VARY DEPENDING ON GEOMETRICS  
AND DESIGN OF TRAFFIC SIGNALS.  
HEAVY-DUTY HANDHOLES TO BE  
USED WHEN THE MEDIAN IS  
MOUNTABLE. REFER TO STANDARD  
814001 TO ENSURE THAT HANDHOLE  
FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

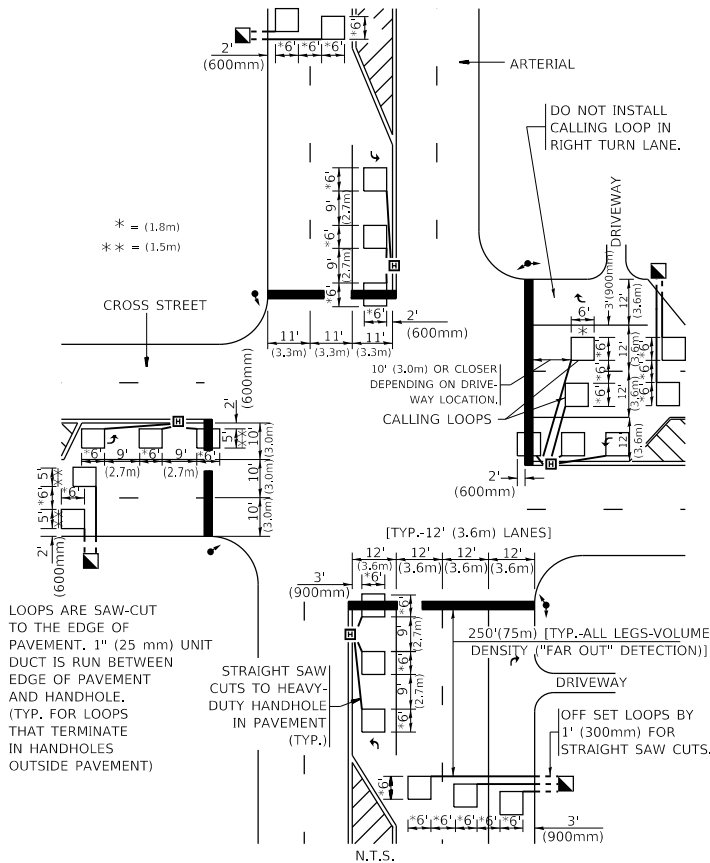
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)



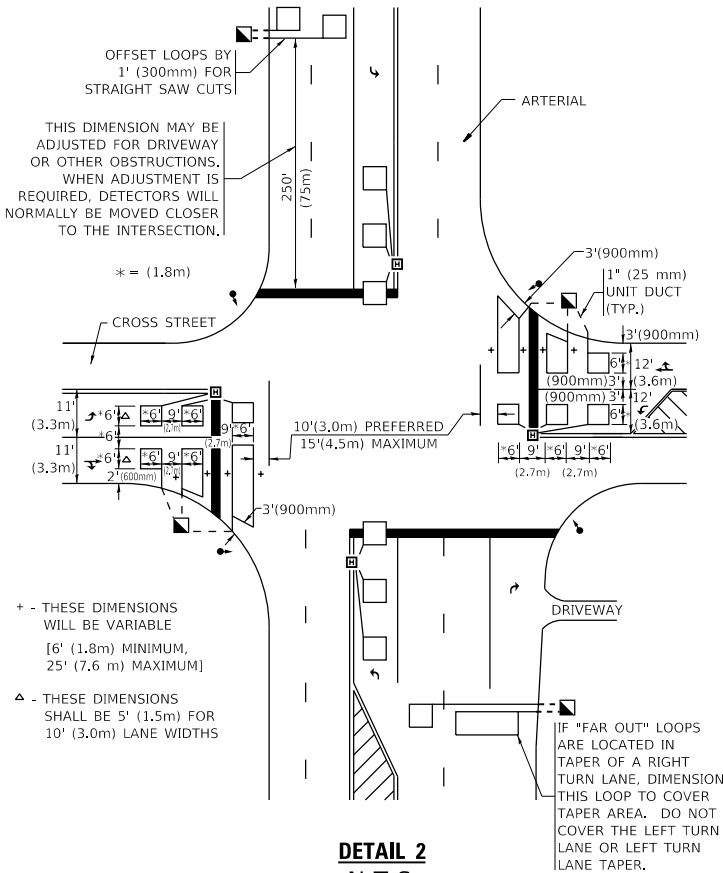
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

\* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED,  
SHIELDED.

\* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE  
LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE  
PAVEMENT.

\* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT  
DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST  
HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE  
SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID  
FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM  
FOR DETECTOR LOOPS.

\* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET  
(1.8 m)

\* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH  
LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE  
INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.

\* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE  
THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR  
(i.e. 1-1/2, 1-3/4, 2).

\* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN  
INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND  
INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM  
DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A  
SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE  
AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW  
CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE  
TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM)  
DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN  
AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS  
UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE  
LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR  
LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR  
SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED  
ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE  
DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT  
TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN  
ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO  
NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND  
10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF  
PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1  
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER  
FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE  
DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS  
FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

MODEL Default  
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USER NAME = yaseen.ureshi	DESIGNED -	REVISED -
PLOT SCALE = 100.0010 ' / in.	CHECKED - R.K.F.	REVISED -
PLOT DATE = 1/24/2025	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
365	2019-114RS&SW	KANE	41	41
TS-07		CONTRACT NO. 62J72		
		ILLINOIS	FED. AID PROJECT	