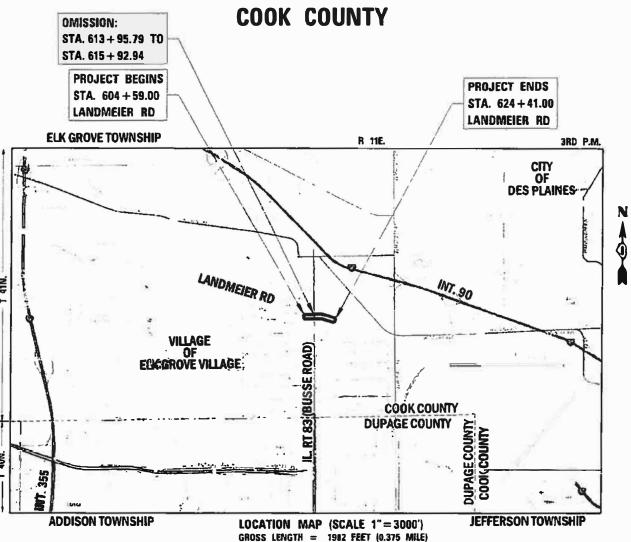
STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

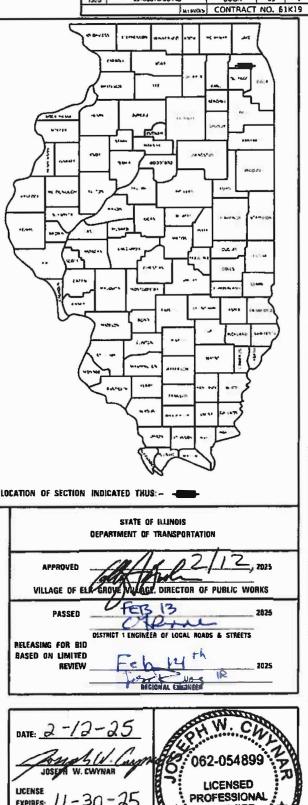
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1333 (LANDMEIER ROAD) AND F.A.P. ROUTE 344 (IL ROUTE 83 — BUSSE ROAD) INTERSECTION RESURFACING SECTION 22-00078-00-RS PROJECT NUMBER: RTQ0(408) JOB NO. C-91-148-23

VILLAGE OF ELK GROVE VILLAGE



NET LENGTH = 1785 FEET (0.338 MILE)



COUNTY SHEETS NO.

PROFESSIONAL ENGINEER



Engineering Enterprises, inc. 52 Wheeler Road Sugar Grove Illinois 60554 Phone: (630) 466-6700

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FOR INDEX OF SHEETS, HIGHWAY STANDARDS AND LEGEND SEE SHEET NO. 02

FUNCTIONAL CLASSIFICATION IL ROUTE 83 (BUSSE ROAD) -PRINCIPAL ARTERIAL LANDMEIER ROAD -MINOR ARTERIAL

TRAFFIC DATA

IL ROUTE 83 (BUSSE ROAD) ADT: 31,400 (2023) LANDMÉIER ROAD ADT: 10,800 (2022)

POSTED SPEED LIMIT IL ROUTE 83 (BUSSE ROAD) = 45 MPH LANDMEIER ROAD = 40 MPH

DESIGN SPEED LIMIT IL ROUTE 83 (BUSSE ROAD) = 50 MPH LANDMEIER ROAD = 45 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



CONTRACT NO. 61K19

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COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (44 HOURS NOTIFICATION IS REQUIRED.)

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE VILLAGE.

CONTRACTOR SHALL SELECT CONCRETE MIX FOR STRUCTURE ADJUSTMENTS THAT MEETS LANE CLOSURE

SOIL EROSION AND SEDIMENT CONTROL NOTES

- UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL, LATEST EDITION.
 - A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE ENGINEER AND/OR THE NCCSWCD.
- IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL PLANS
- WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN SEVEN (7) DAYS OF COMPLETION. WHERE WORK HAS TEMPORARILY CEASED FOR FOURTEEN (14) DAYS OR MORE, TEMPORARY STABILIZATION SHALL OCCUR BY THE SEVENTH DAY AFTER WORK HAS CEASED
- 6. ALL ADJACENT STREETS AND PARKING LOTS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH ALL GENERAL AND SPECIAL REQUIREMENTS, CONDITIONS AND PROVISIONS FOR ALL APPLICABLE LOCAL, STATE, AND FEDERAL PERMITS ISSUED FOR THIS PROJECT
- NO STOCKPILING WILL BE ALLOWED ON THE PROJECT SITE.
- 9. INLET FILTERS SHALL BE PLACED ON ALL OPEN LID STRUCTURES

MAINTENANCE OF TRAFFIC GENERAL NOTES

- TRAFFIC CONTROL DEPICTED IN THE APPLICABLE IDOT AND DISTRICT ONE DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL CONSTRUCTION WARNING SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
- ALL SIGNS, OTHER THAN THOSE WHICH WILL FREQUENTLY BE TURNED, RELOCATED OR COVERED, SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
- DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 20' CENTERS ALONG TAPERS, AND 10' CENTERS IN CURVES AND RADII.
- DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL
- THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
- EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- W21-1 "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. WORKERS SIGNS MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
- "FRESH OIL" SIGNS (W21-2-48) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING.
- DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS ALONG COOK COUNTY HIGHWAYS, TWO-WAY TRAFFIC SHALL REMAIN OPEN FOR ALL TRAFFIC AT ALL TIMES. IF ANY ACTIVITY REQUIRES ENCROACHMENT INTO A LANE OPEN FOR TRAFFIC, THAT ACTIVITY SHALL BE RESTRICTED TO WITHIN THE HOURS OF 9:00 AM TO 3:00 PM FOLLOWING THE APPLICABLE IDOT AND DISTRICT ONE TRAFFIC CONTROL STANDARDS INCLUDED IN THE PLANS FOR OFF-ROAD AND ON-ROAD APPLICATIONS.
- IF ANY EXISTING PAVEMENT MARKING AND/OR SIGNING ALONG COOK COUNTY ROADWAYS IS DAMAGED DUE TO CONSTRUCTION OF PROPOSED IMPROVEMENTS, THE CONTRACTOR SHALL REPLACE THE DAMAGED TRAFFIC CONTROL DEVICES TO THE SATISFACTION OF COOK COUNTY PER IDOT AND DISTRICT ONE STANDARDS FOR TRAFFIC CONTROL DEVICES, PAVEMENT MARKING, AND SIGNAGE INCLUDED IN THE PLANS.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK

COVER SHEET INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES SUMMARY OF QUANTITIES

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53-54 CCDOTH STANDARDS 55-63 DISTRICT ONE STANDARDS

STATE OF I	LLINOIS HIGHWAY STANDARDS
STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-12	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-05	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-06	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442101-09	CLASS B PATCHES
442201-03	CLASS C&D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-04	INLET- TYPE A
602401-07	PRECAST MANHOLE TYPE A 4' DIAMETER
604001-05	FRAMES AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
604091-05	FRAME AND GRATE TYPE 24
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-ROAD OPERATIONS, MULTILANE
	15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE,
	MORE THAN 15' AWAY
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING
	OPER., FOR SPEEDS > 45 MPH _
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT
	OR MOVING OPER. FOR SPEEDS <40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH
	NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT

TELESCOPING STEEL SIGN SUPPOR BASE FOR TELESCOPING STEEL SIGN SUPPORT 731001-01

780001-05 TYPICAL PAVEMENT MARKINGS 836001-05 LIGHT POLE FOUNDATION 838001-01 BREAKAWAY DEVICES 873001-02 TRAFFIC SIGNAL GROUNDING AND BONDING

876001-04 PEDESTRIAN PUSH BUTTON POST 877001-08 STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55' CONCRETE FOUNDATION DETAILS 878001-11

TRAFFIC SIGNAL MOUNTING DETAILS 880006-01 DETECTOR LOOP INSTALLATIONS 886001-01 886006-01 TYPICAL LAYOUTS FOR DETECTON LOOPS

ROADWAY RESURFACING

IDOT DISTRICT ONE STANDARD DETAILS

BD-07	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS,
	INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
	(SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
	(TO REMAIN OPEN TO TRAFFIC)
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	DISTRICT ONE ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN
TS-07	DISTRICT ONE- DETECTOR LOOP INSTALLATION DETAILS FOR

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS", ADOPTED JANUARY 1 2022; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1 2025; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" SEVENTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECICIAL PROVISIONS AND THE IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT
- NO WORK SHALL COOMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFT ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE EVERY PRECAUTION NECESSARY TO ENRURE THAT THERE WILL BE NO DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE
- THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE PROJECT LIMITS. ALL EXCESS OR WASTE MATERIAL SHALL BE EITHER HAULED AWAY FROM THE PRPJECT SITE BY THE CONTRACTOR AND DEPOSITED AT THE LOCATIONS PROVIDED BY HIM, OR DISPOSED OF WITHIN THE RIGHT-OF-WAY IN A MANNER OTHER THAN BURNING, SUBJECT TO APPROVAL OF THE ENGINEER.
- THE ENGINEER AND ALL UTILITY COMPANIES, LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.

PAVING, SHOULDERS, CURB & GUTTER AND SIDEWALK

- IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.
- HOT-MIX ASPHALT BINDER SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION AND TOPSOIL PLACEMENT HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER, AND THE HOT-MIX ASPHALT BINDER COURSE HAS BEEN COMPLETED PER SPECIFICATION.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE
- ALL SIDEWALK CONSTRUCTED OVER A UTILITY TRENCH SHALL BE REINFORCED WITH THREE #4 REBARS WHICH EXTEND 5 FEET BEYOND THE TRENCH WALLS. AT LOCATIONS WHERE THE SIDEWALK IS ADJACENT TO THE BACK OF CURB, A 1/2" PREFORMED EXPANSION JOINT FILLER SHALL BE INSTALLED BETWEEN THE CURB AND SIDEWALK.
- THE MAXIMUM CROSS SLOPE AT ANY POINT IN THE TRAVERSABLE AREA OF THE SIDEWALK, INCLUDING THE AREAS THROUGH DRIVEWAYS, SHALL BE 2.00%. ALL AREAS OF NEW SIDEWALK THAT EXCEED THIS MAXIMUM SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
- CONTRACTOR SHALL SELECT CONCRETE MIX FOR CLASS B PATCHES THAT MEETS LANE CLOSURE REQUIREMENTS.
- GEUTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH THE ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE
- ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTORS
- ALL JOINTS IN THE PROPOSED PCC SHARED USE PATH SHALL BE SAWCUT. 10.
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1

MISCELLANEOUS

- THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.

STAKING

- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE OF RIGHT-OF-WAY.
- RIGHT OF WAY LINES ARE APPROXIMATE. CONTRACTOR SHALL VERIFY ANY POTENTIAL CONFLICTS WHEN SIDEWALK MAINTENANCE NOTES

THE SIDEWALK ON ONE SIDE OF THE STREET MUST REMAIN OPEN AND ACCESSIBLE AT ALL TIMES. SIGNING DIRECTING PEDESTRIANS TO THE OPEN SIDEWALK SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701801. COORDINATION

ALL CONTRACTOR'S EQUIPMENT STAGED INSIDE OR OUTSIDE OF THE WORKZONE SHALL NOT IMPEDE ACCESS TO ANY BUSINESS/RESIDENT DRIVEWAY. IF EQUIPMENT IS TO BE STAGED ON PRIVATE PROPERTY, WRITTEN CONSENT FROM THE PROPERTY OWNER SHALL BE ACQUIRED BY THE CONTRACTOR PRIOR TO USAGE WITH A COPY OF THE AGREEMENT PROVIDED TO THE VILLAGE.

COMMITMENTS

NONE

STANDARD NO. DESCRIPTION

	USER NAME -	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE -	CHECKED -	REVISED -
n	PLOT DATE -	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

	LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) GENERAL NOTES		SECTION	COUNTY	TOTAL SHEETS	SHE
			22-00078-00-RS	COOK	63	2
				CONTRACT	Γ NO. 61	<19
	CHEET 1 OF 1 CHEETC CTA N/A TO CTA N/A			 		

SUMMARY OF QUANTITIES

SPTY. CODE	R ITEM DESCRIPTION	UNIT	TOTAL QUANT ROADWAY 75% STP-L 25% LA 0005
201014	NITROGEN FERTILIZER NUTRIENT	POUND	5
201015	0 PHOSPHORUS FERTILIZER NUTRIENT	POUND	5
201016	00 POTASSIUM FERTILIZER NUTRIENT	POUND	5
202001	00 EARTH EXCAVATION	CU YD	124
204008	00 FURNISHED EXCAVATION	CU YD	16
208001	TRENCH BACKFILL	CU YD	65
210010	00 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	550
211016	.5 TOPSOIL FURNISH AND PLACE, 4"	SQ YD	384
252001	.0 SODDING, SALT TOLERANT	SQ YD	384
252002	00 SUPPLEMENTAL WATERING	UNIT	20
280002	TEMPORARY EROSION CONTROL SEEDING	POUND	8
280004	00 PERIMETER EROSION BARRIER	FOOT	388
280005	0 INLET FILTERS	EACH	44
303000	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	327
303001	.2 AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	992
311012	00 SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	477
311014	00 SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	79
406002	00 BITUMINOUS MATERIALS (TACK COAT)	POUND	8187
406003	O LONGITUDINAL JOINT SEALANT	FOOT	4911
406004	00 MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20

SPTY.	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTIT ROADWAY 75% STP-L 25% LA 0005
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	138
	40603090	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	1743
	40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	1220
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	132
	44000600	SIDEWALK REMOVAL	SQ FT	2961
	44003100	MEDIAN REMOVAL	SQ FT	47
	44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	7123
	44200994	CLASS B PATCHES, TYPE II, 12 INCH	SQ YD	229
	44200998	CLASS B PATCHES, TYPE III, 12 INCH	SQ YD	367
	44201000	CLASS B PATCHES, TYPE IV, 12 INCH	SQ YD	350
	44201705	CLASS D PATCHES, TYPE II, 5 INCH	SQ YD	29
	44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	17
	550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	33
	55100500	STORM SEWER REMOVAL 12"	FOOT	5
	55100700	STORM SEWER REMOVAL 15"	FOOT	5
	60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1
	60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	1
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	2

SPECIALTY ITEM



SUMMARY OF QUANTITIES

PTY. CODE NUMBE	ITEM DESCRIPTION	UNIT	TOTAL QUANTI ROADWAY 75% STP-L 25% LA 0005
6026660	VALVE BOXES TO BE ADJUSTED	EACH	3
6050005	D REMOVING CATCH BASINS	EACH	1
6060380	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	94
6062374	5 CONCRETE MEDIAN TRANSITION	SQ FT	47
* 6690020	D NON-SPECIAL WASTE DISPOSAL	CU YD	211
* 6690053	D SOIL DISPOSAL ANALYSIS	EACH	5
* 6690100	1 REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1
* 6690100	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1
* 6690100	5 REGULATED SUBSTANCES MONITORING	CAL DA	50
6710010	D MOBILIZATION	L SUM	1
7010263	D TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1
7010263	5 TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
7010264	D TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
7010702	5 CHANGEABLE MESSAGE SIGN	CAL DA	320
7030010	D SHORT TERM PAVEMENT MARKING	FOOT	2151
7030015	D SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	5254
7030022	1 TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	27611
7030024	1 TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	4472
7030026	1 TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	4593
7030028	1 TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	426

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTI ROADWAY 75% STP-L 25% LA 0005
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	328
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	8217
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	1491
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	1157
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	142
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	18
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	328
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	8217
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	2546
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	1485
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	142
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	86
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	86
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	5254
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	59
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	37
* 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	8
* 84200804	REMOVAL OF POLE FOUNDATION	EACH	2
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1



DESIGNED -REVISED DRAWN REVISED CHECKED REVISED

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) SUMMARY OF QUANTITIES SHEET 2 OF 3 SHEETS STA.

 COUNTY
 TOTAL SHEETS NO.

 COOK
 63
 4

 CONTRACT NO. 61K19
 F.A.U. RTE. 1333 SECTION 22-00078-00-RS

SUMMARY OF QUANTITIES

SPTY.	CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANT ROADWAY 75% STP-L 25% LA 0005
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1652
*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2163
*	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	450
*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1116
*	87700290	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT	EACH	2
*	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	23
*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	18
*	87900200	DRILL EXISTING HANDHOLE	EACH	4
*	88600100	DETECTOR LOOP, TYPE I	FOOT	470
*	89501250	RELOCATE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
*	89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1
*	89502200	MODIFY EXISTING CONTROLLER	EACH	1
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	6673.5
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
*	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
*	X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	441
*	X1400378	PEDESTRIAN SIGNAL POST, 5 FT	EACH	2
*	X1400424	ELECTRIC CABLE IN CONDUIT, STREET NAME SIGN, NO. 14 3C, TYPE SOOW	FOOT	403
	X2130010	EXPLORATION TRENCH (SPECIAL)	FOOT	100
	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)	SQ FT	2087

SPTY. CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTI ROADWAY 75% STP-L 25% LA 0005
X4240460	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH (SPECIAL)	SQ FT	2207
X4240800	DETECTABLE WARNINGS (SPECIAL)	SQ FT	205
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	46
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	348
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	11337
X4406812	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 1/2"	SQ YD	78
X5510010	STORM SEWER CONNECTION	EACH	4
X6025300	CATCH BASINS TO BE ADJUSTED (SPECIAL)	EACH	1
X6025600	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH	12
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1
X6026200	INLETS TO BE ADJUSTED (SPECIAL)	EACH	16
X6060034	COMBINATION CONCRETE CURB AND GUTTER TRANSITION	FOOT	64
X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	103
* X8130130	JUNCTION BOX TO BE ADJUSTED	EACH	1
* X8440110	RELOCATE EXISTING LIGHT POLE WITH LUMINAIRE	EACH	1
* X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
* X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
Z0013798	CONSTRUCTION LAYOUT	L SUM	1
Z0018905	DRILL AND GROUT BARS	EACH	184
* Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
	LANDMEIED DD AND II DTE 02 (DUCCE DOAD)	F.A.U.	SECTION

SPECIALTY ITEM

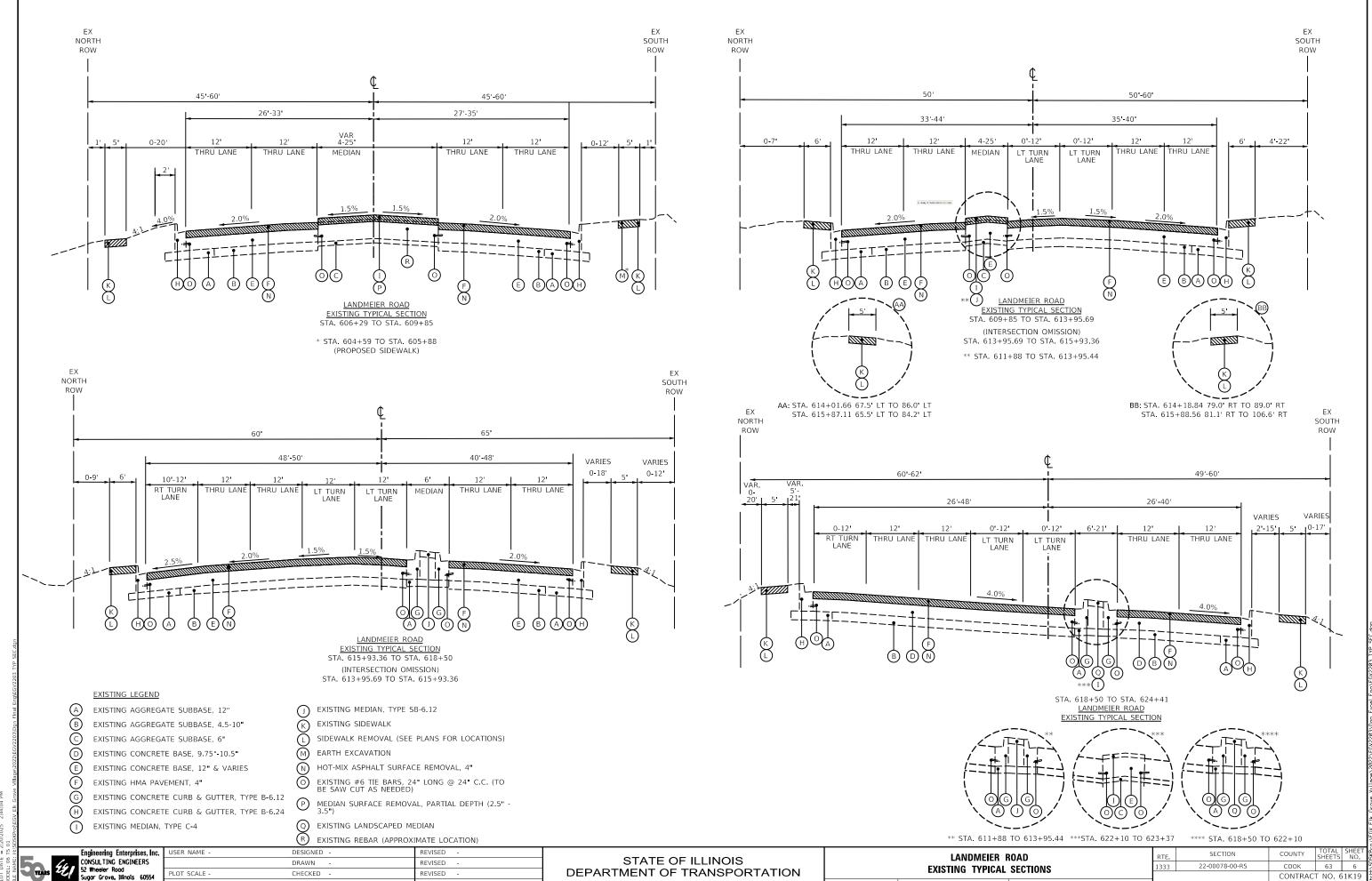
Engineering Enterprises, Inc. USER NAME CONSULTING ENGINEERS
52 Wheeler Rood
52 Wheeler Rood
52 Upc Crove, lilhols 60554
630.466.6700 / www.eelweb.com PLOT DATE -

DESIGNED -REVISED -DRAWN -REVISED -REVISED -CHECKED -DATE

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

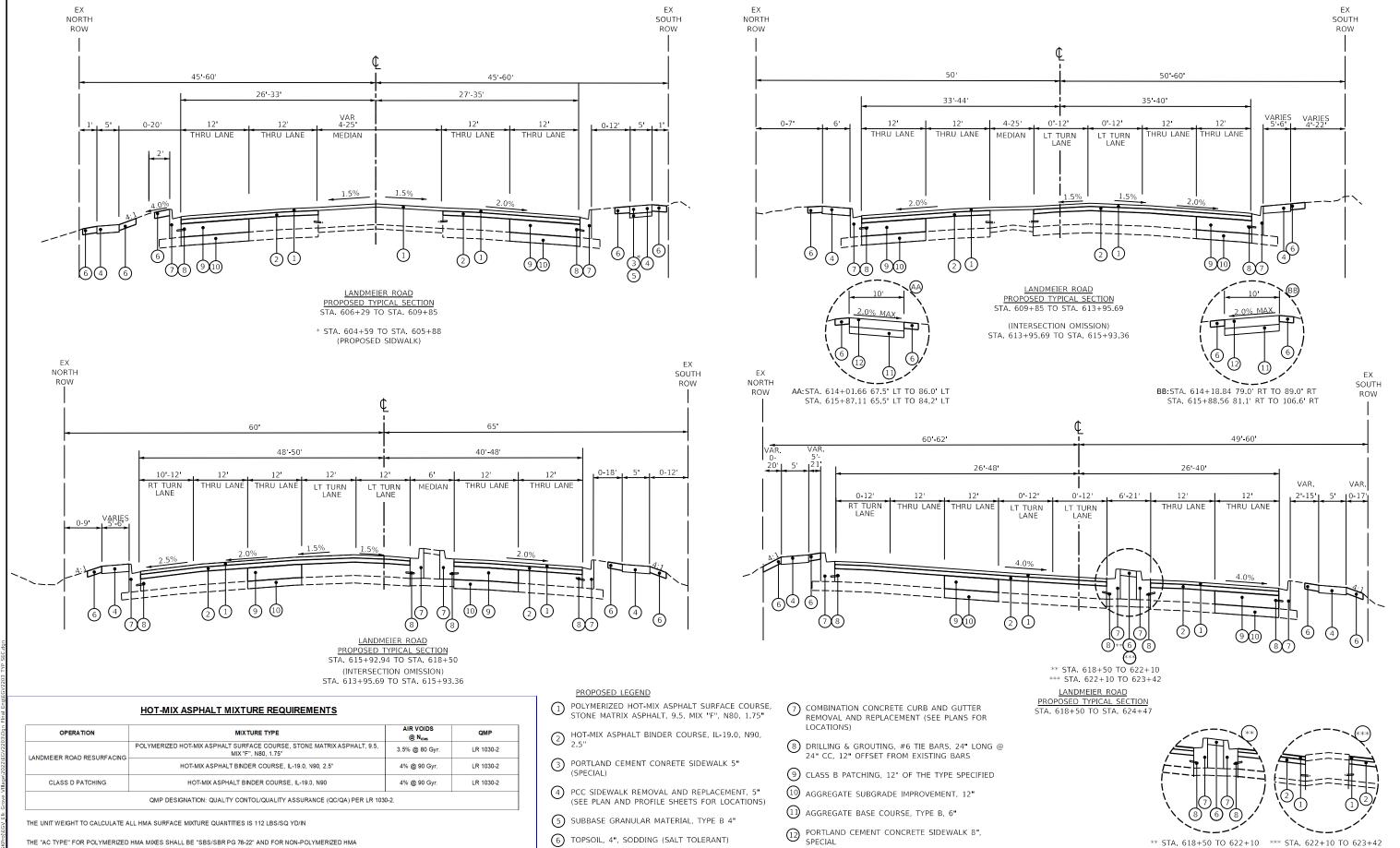
LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) SUMMARY OF QUANTITIES SHEET 3 OF 3 SHEETS STA. TO STA.

COUNTY SHEETS NO.
COOK 63 5
CONTRACT NO. 61K19 22-00078-00-RS



OF 2 SHEETS STA.

USER NAME = JMarvig PLOT SCALE = 1:10 PLOT DATE = 2/20/2025 2:44:04 PM MODEL: 06 TS 01



STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

LANDMEIER ROAD

PROPOSED TYPICAL SECTIONS

OF 2 SHEETS STA.

SCALE: N.T.S.

22-00078-00-RS

63 7

CONTRACT NO. 61K19

USER NAME = JMarvig PLOT SCALE = 1:10 PLOT DATE = 2/20/2025 2:44:04 PM MODEL: 08 TS 03

THE "AC TYPE" SHALL BE "PG 64-22", UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

Engineering Enterprises, Inc

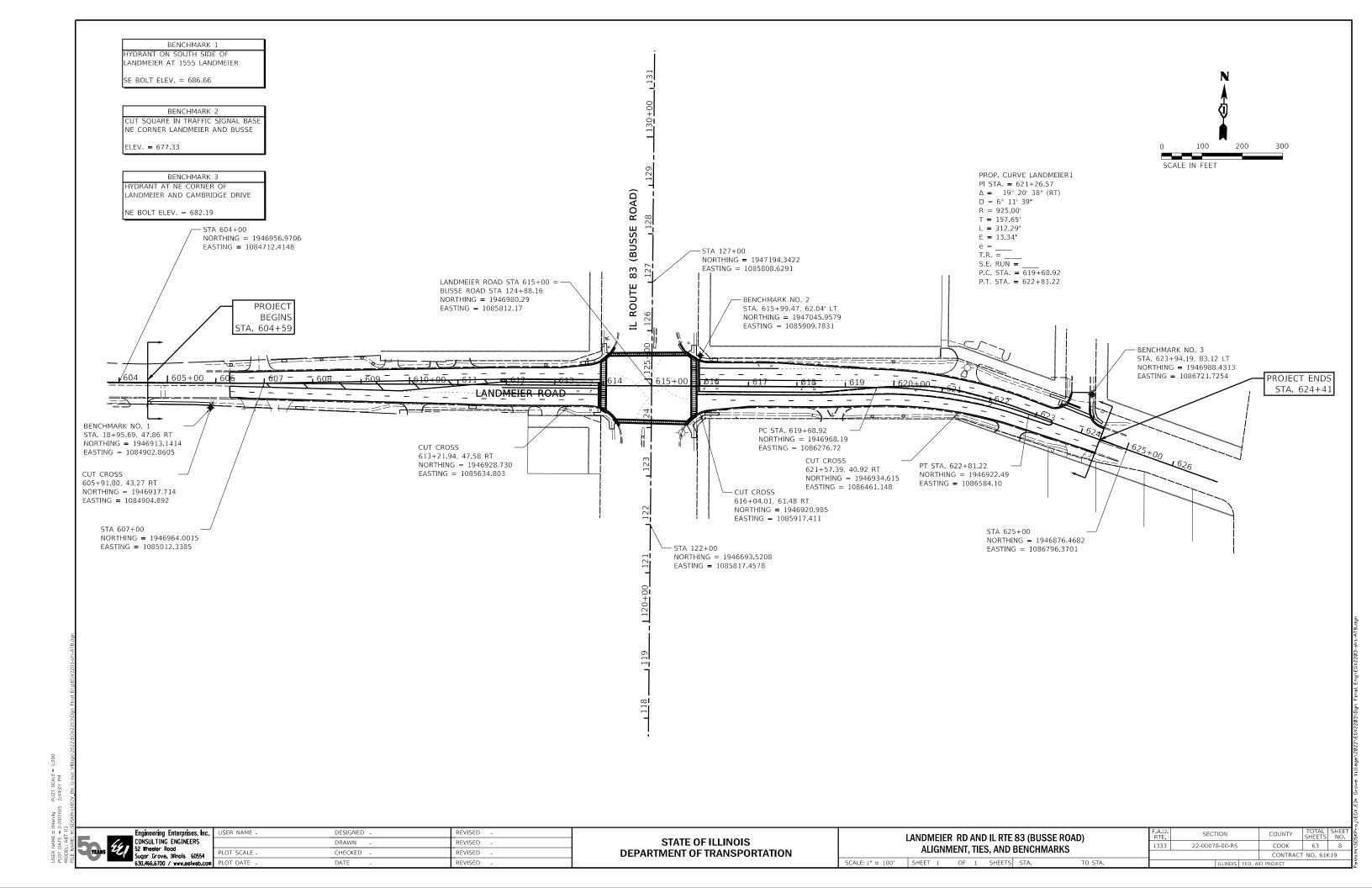
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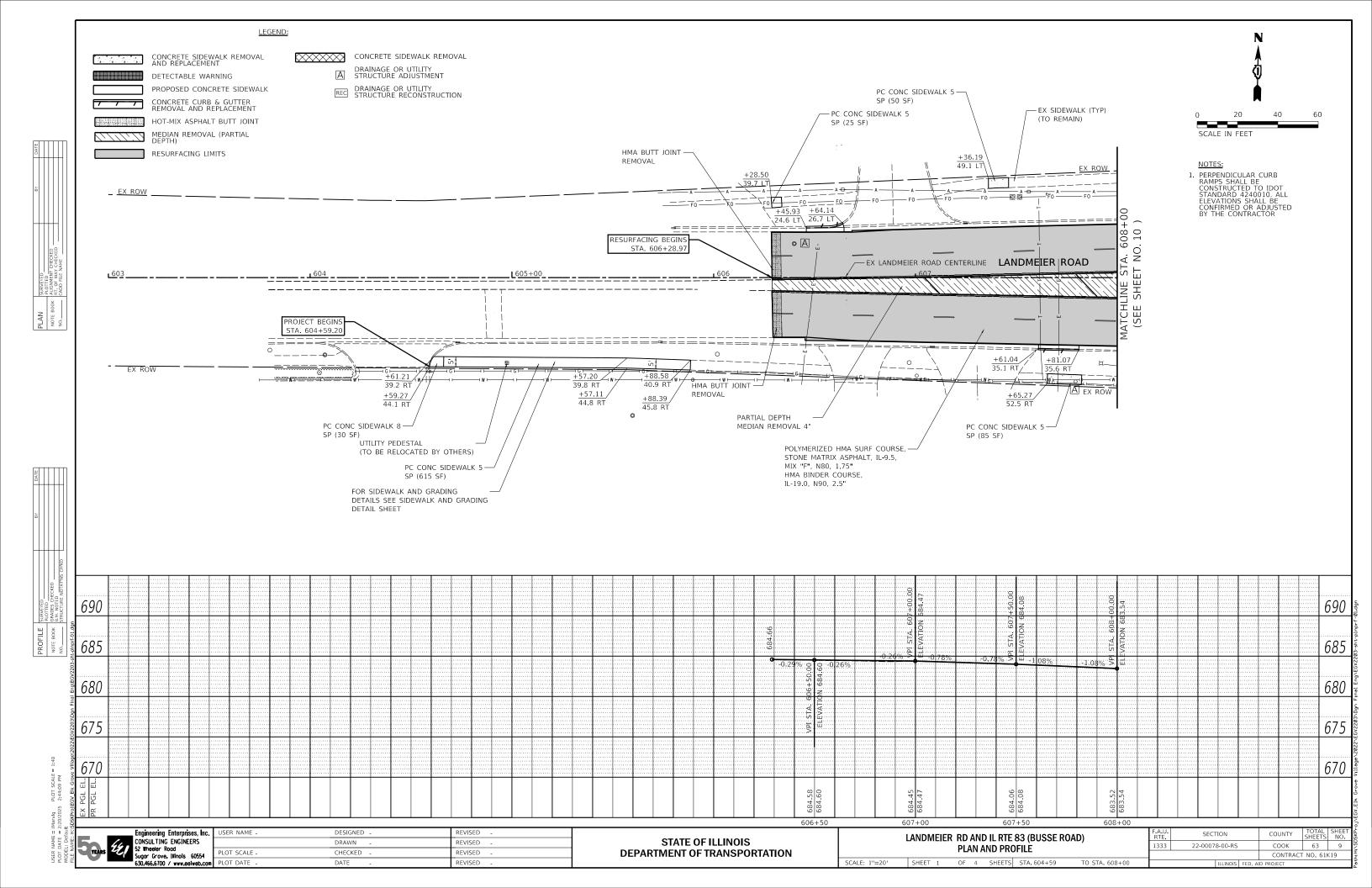
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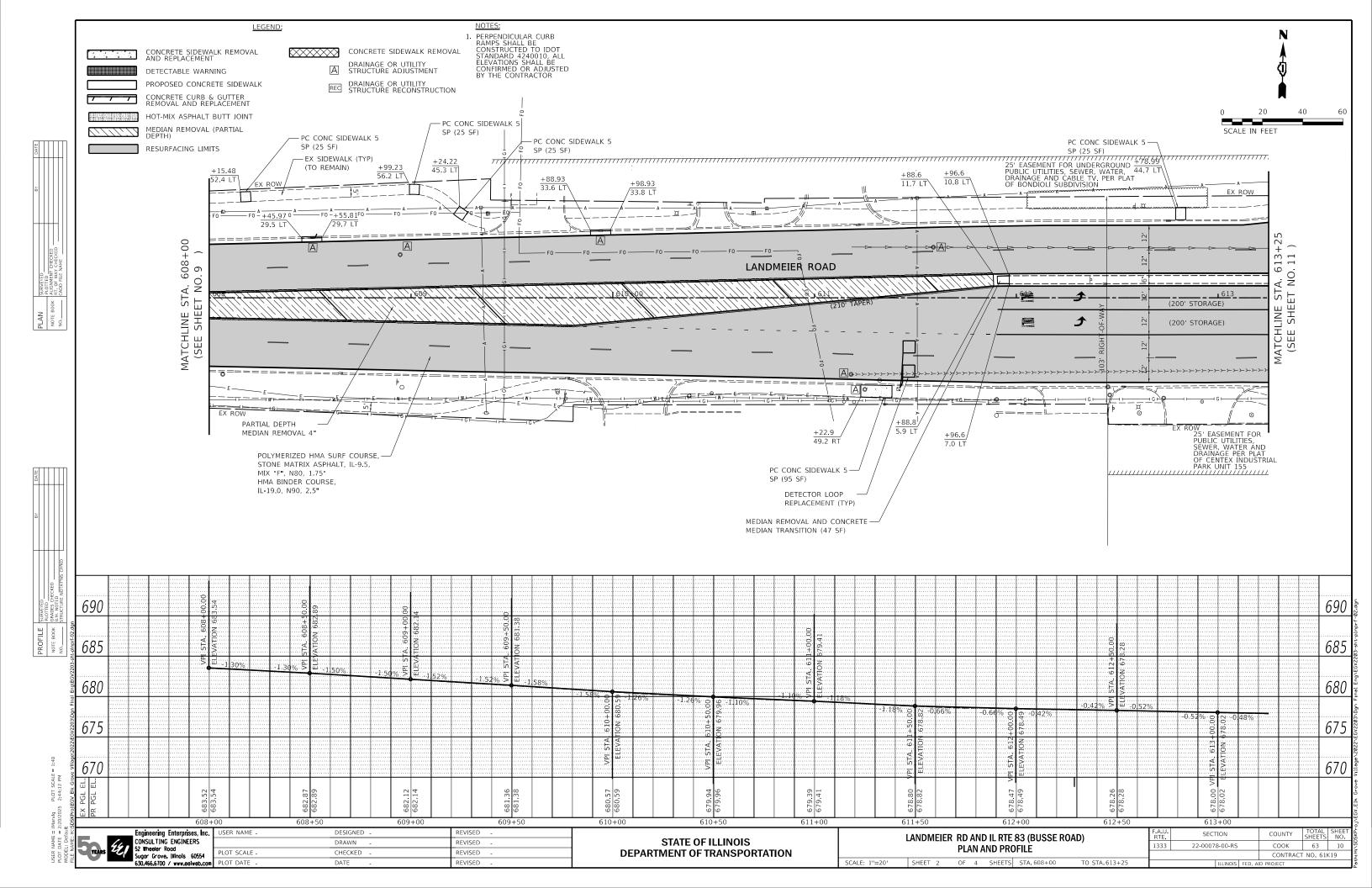
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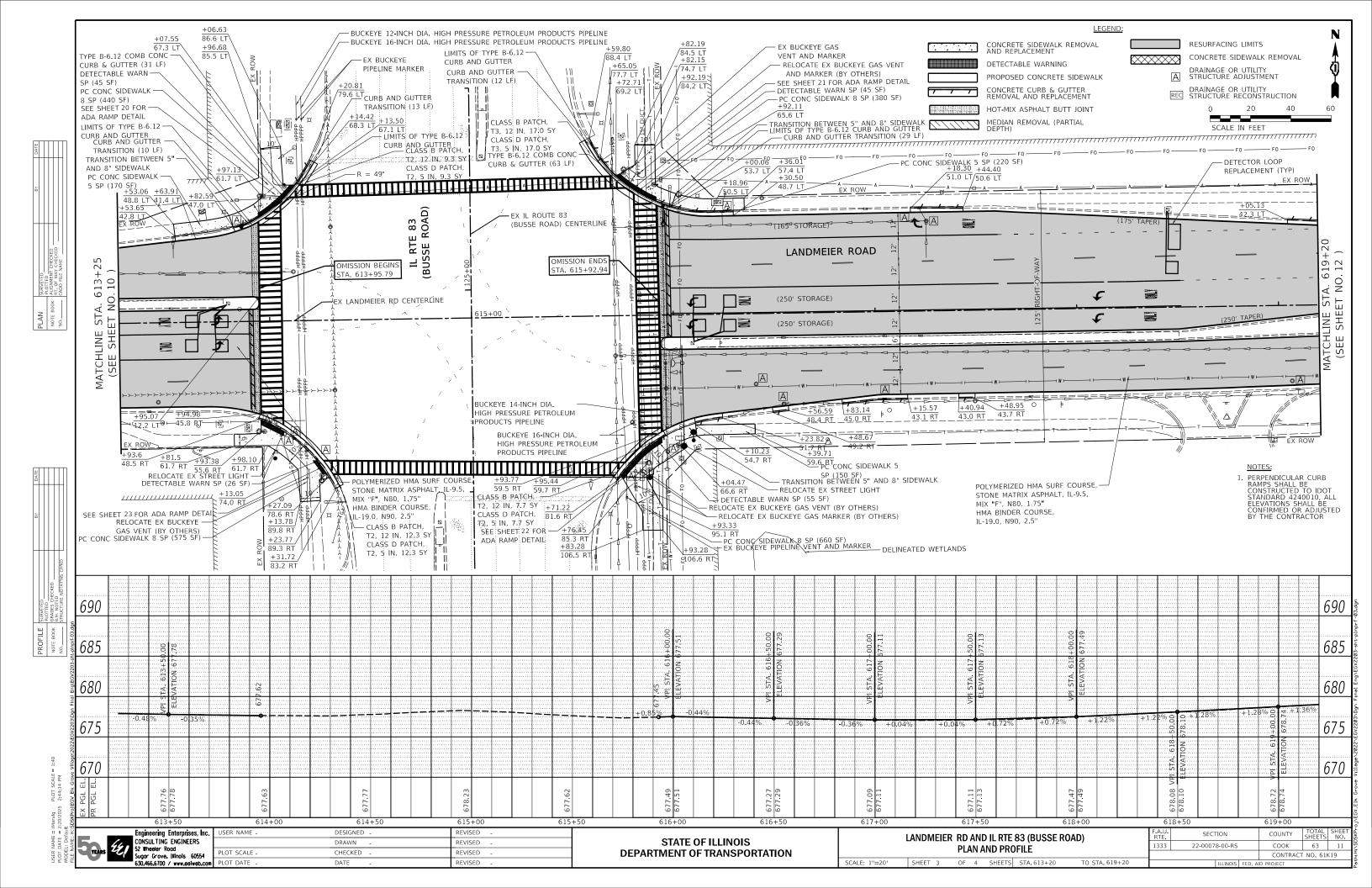
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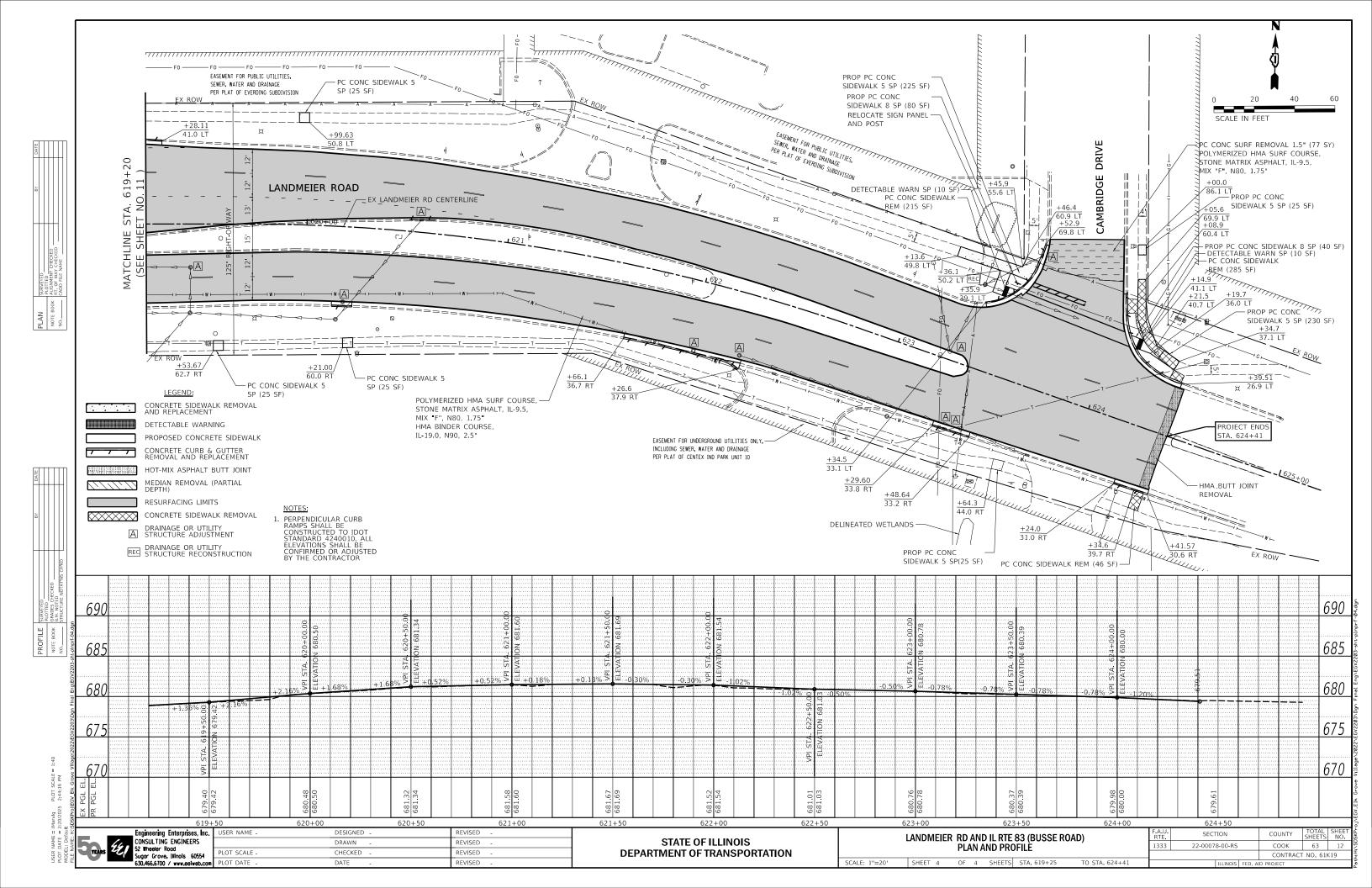
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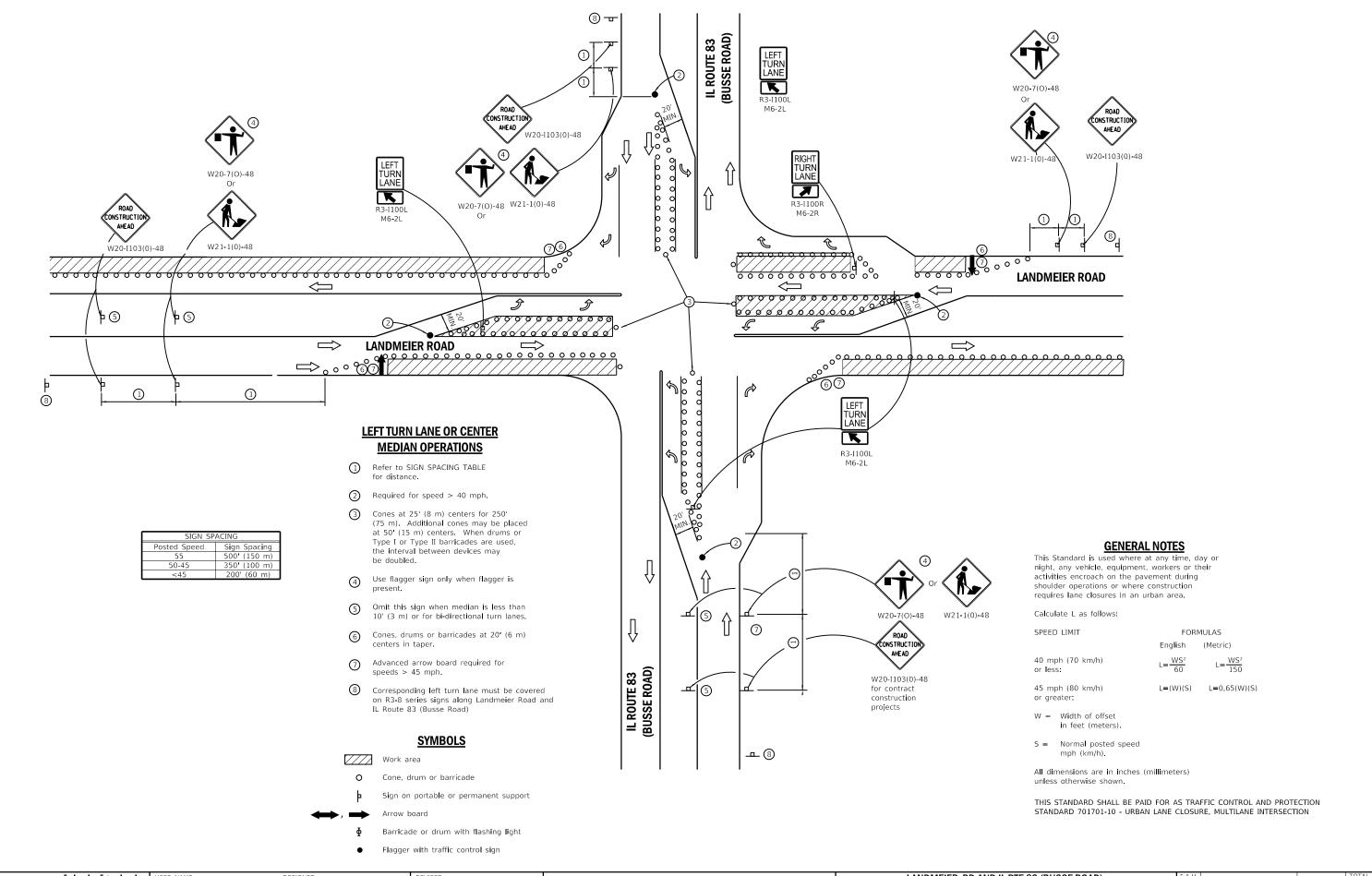












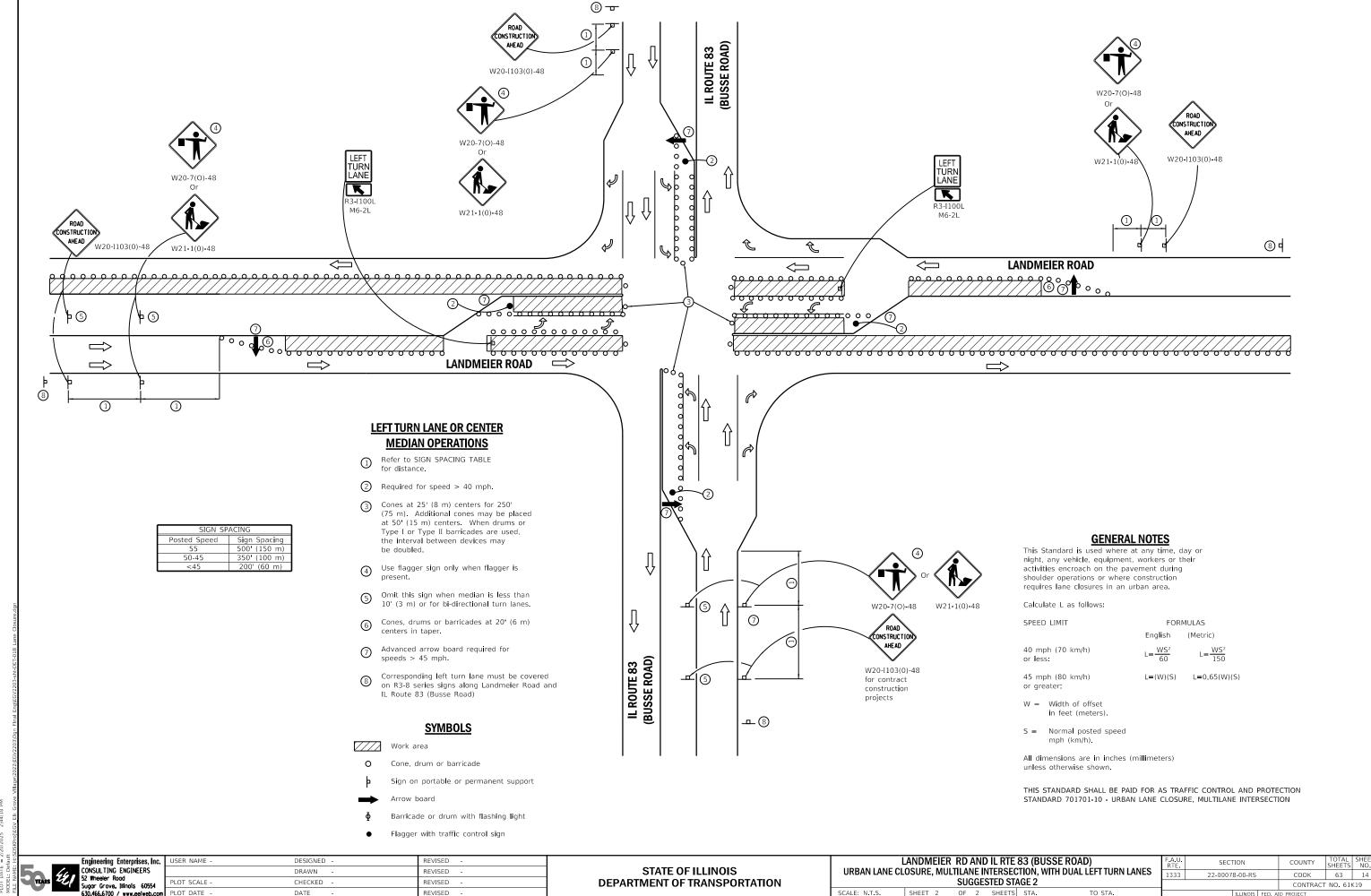
Engineering Enterprises, Inc. ONSULTING ENGINEERS

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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) URBAN LANE CLOSURE, MULTILANE INTERSECTION, WITH DUAL LEFT TURN LANES SUGGESTED STAGE 1 OF 2 SHEETS STA.

SECTION COUNTY 22-00078-00-RS COOK 63 13 CONTRACT NO. 61K19



REVISED DATE

COOK 63 14

STORM STRUCTURE PROTECTION SCHEDULE STORM STRUCTURE PROTECTION SCHEDULE STRUCTURE NO. STATION OFFSET STRUCTURE NO. STATION OFFSET 1 604+73.3 31.9' RT 2 605+73.4 32.2' RT 3 606+97.1 42.3' RT CATCH BASIN 18 614+11.2 57.8 RT **LEGEND** ATCH BASIN 19 614+13.5 85.8 LT INLET FILTER ATCH BASIN 20 615+65.9 77.1 LT CATCH BASIN 21 615+77.0 104.1 RT CATCH BASIN 22 616+24.9 50.6 LT CATCH BASIN 23 616+27.5 50.7 LT CATCH BASIN 24 616+54.3 12.2 RT 4 612+53.2 44.1 RT PERMITER EROSION BARRIER 5 615+59.6 119.3 RT 6 614+22.1 68.3 RT TEMPORARY EROSION CONTROL SEEDING 7 617+04.1 43.4 RT 8 617+76.7 43.1 RT ATCH BASIN 25 616+95.2 50.6' RT INLET 9 622+45.7 34.6' RT
INLET 10 623+53.7 63.9' LT CATCH BASIN 26 617+04.6 ← EXISTING DITCH FLOW CATCH BASIN 27 617+06.4 48.8' RT CATCH BASIN 28 617+22.9 50.9' RT PR1 614+06.6 → EXISTING SWALE FLOW ATCH BASIN 29 617+23.4 12.3 RT CATCH BASIN 1 606+01.8 37.9' RT CATCH BASIN 30 617+78.1 47.2 RT CATCH BASIN 31 618+25.7 12.3 RT CATCH BASIN 2 607+08.5 PROPOSED SWALE FLOW CATCH BASIN 3 607+24.7 26.8 LT CATCH BASIN 4 608+51.3 CATCH BASIN 32 619+08.9 41.9' RT NOTES: CATCH BASIN 33 619+39.3 3.6 RT CATCH BASIN 34 619+54.5 54.5 RT CATCH BASIN 35 619+57.1 7.0 RT CATCH BASIN 36 620+56.4 6.2 LT CATCH BASIN 5 608+65.2 37.3 RT CATCH BASIN 6 609+77.5 39.7' RT ALL STATION AND OFFSETS ARE 7.0 RT CATCH BASIN 7 609+94.0 CATCH BASIN 8 611+21.6 33.1 LT REFERENCED TO THE LANDMEIER ROAD CENTERLINE ALIGNMENT 36.1 LT ATCH BASIN 37 622+33.2 5.4 LT CATCH BASIN 38 623+03.5 42.8 LT CATCH BASIN 9 611+32.2 40.8 RT CATCH BASIN 10 611+90.2 58.3 RT CATCH BASIN 39 623+25.7 4.0 LT CATCH BASIN 40 623+26.2 4.1 LT CATCH BASIN 11 612+44.1 6.8' LT CATCH BASIN 12 612+75.4 38.1 LT CATCH BASIN 13 613+39.6 44.1 RT CATCH BASIN 41 623+35.8 32.8 RT CATCH BASIN 42 623+43.7 30.6 RT CATCH BASIN 14 613+51.2 6.7' LT CATCH BASIN 15 613+52.7 42.2' LT CATCH BASIN 43 623+78.8 32.0' RT CATCH BASIN 44 624+91.8 26.9' LT +33.53 41.9 LT CATCH BASIN 16 613+85.2 5.9 LT CATCH BASIN 17 613+86.3 47.5 LT 29.2 LT CATCH BASIN PR1 615+95.8 54.1' LT +45.90 33.0 LT EX ROW ® \$\partial 46.22 \text{ } +29.06 32.5 LT 42.5 LT 42.2 LT æ======= 608+). 16 RESURFACING LANDMEIER ROAD 0 LIMITS STA A 60 NO MATCHLINE STA LIMITS STA 604+59 ======== +88.58 39.1 RT 32.6 RT +59.20 +88.39

46.8 RT

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64.	CONSULTING ENGINEERS	
YEARS 25	52 Wheeler Road Sugar Grove, Illinois 60554	Ρl
	630.466.6700 / www.eelweb.com	Ρl

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44.9 RT

STATE OF ILLINOIS	
DEPARTMENT OF TRANSPORTATION	

l	LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) EROSION CONTROL PLAN SHEET 1 OF 4 SHEETS STA. 604+59 TO STA. 608+00								
	SHEET	1	OF	4	SHEETS	STA. 604+59	TO STA.	608+00	

SCALE: 1" = 20'

+60.92 37.8 RT

45.6 RT

+65.26 53.4 RT

F.A.U. RTE	SEC	COUNTY	TOTAL SHEETS	SHE		
1333	22-0007	'8-00-RS		соок	63	15
				CONTRACT	NO. 61	<19
		ILLINOIS	FED. A	ID PROJECT		

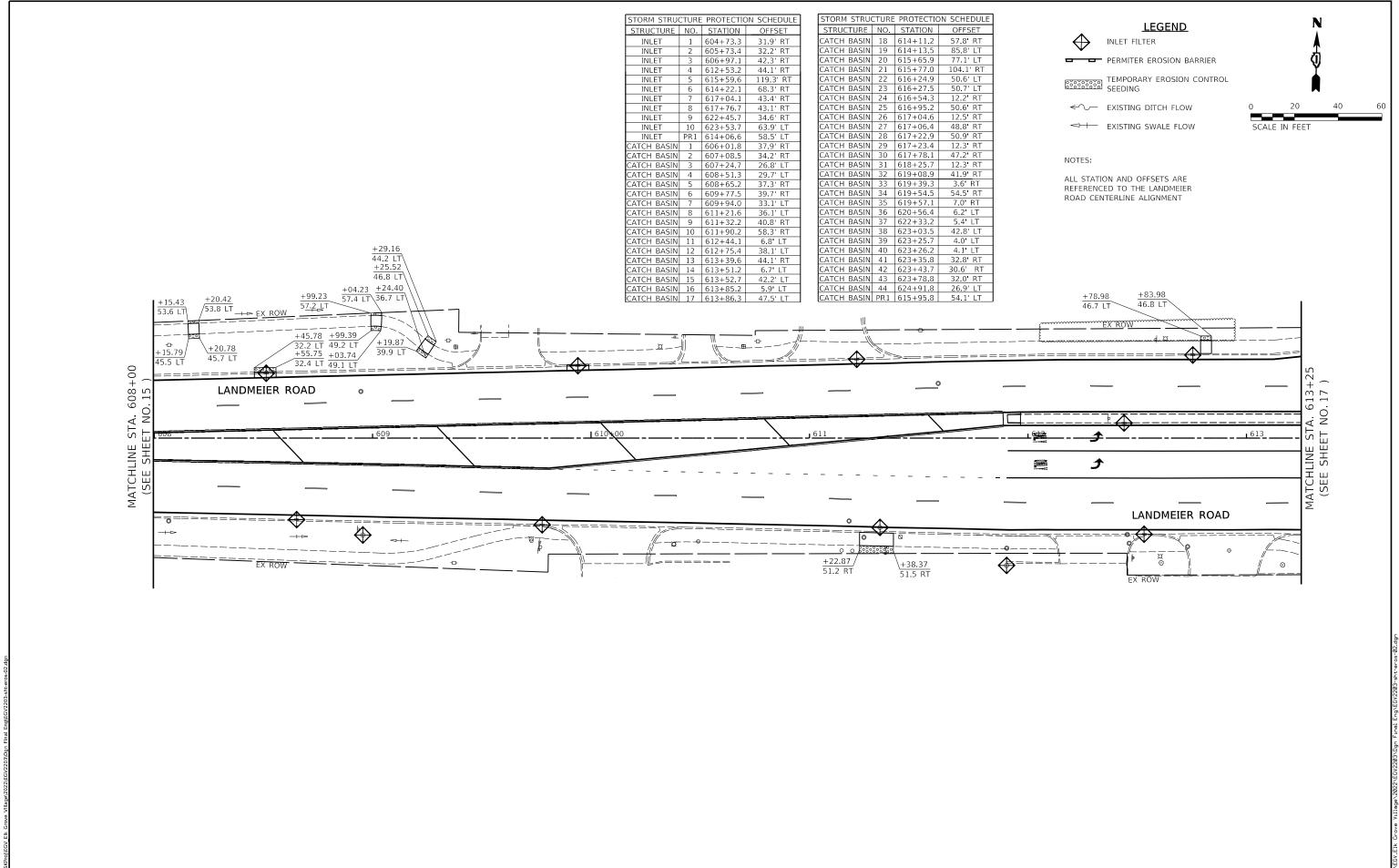
38.2 RT

46.2 RT

\+82.22

20

SCALE IN FEET



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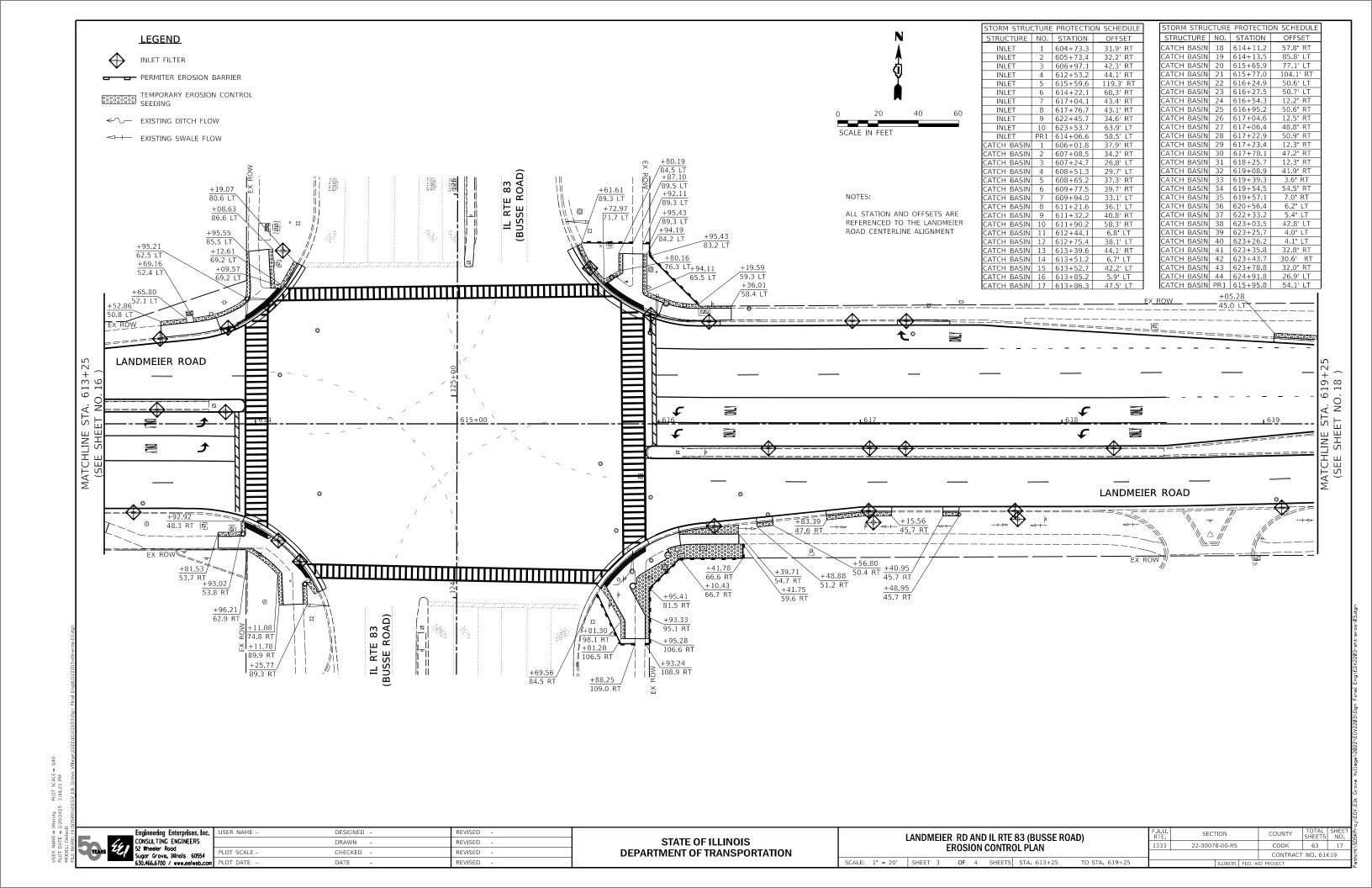
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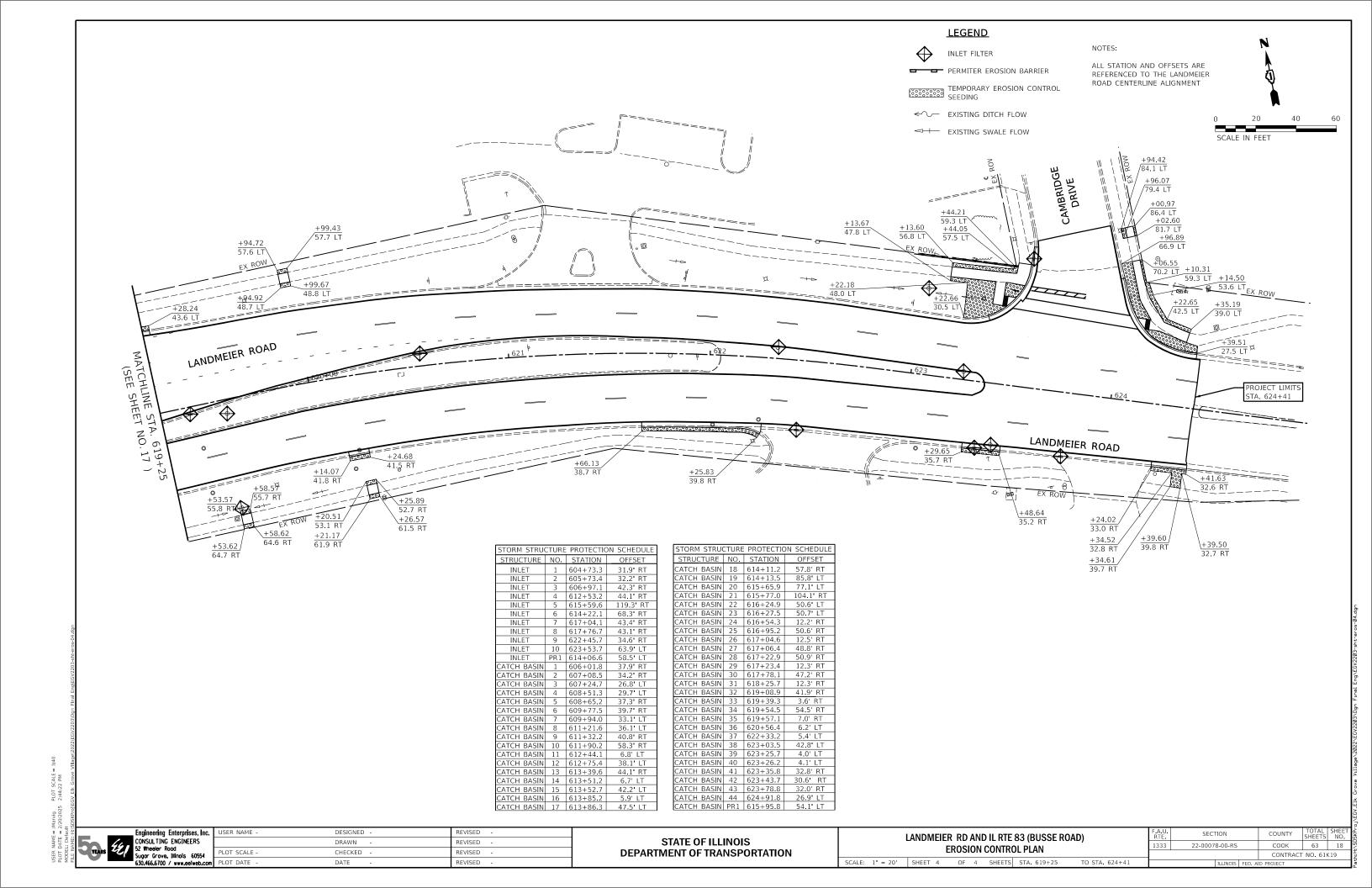
DEPARTMENT OF TRANSPORTATION

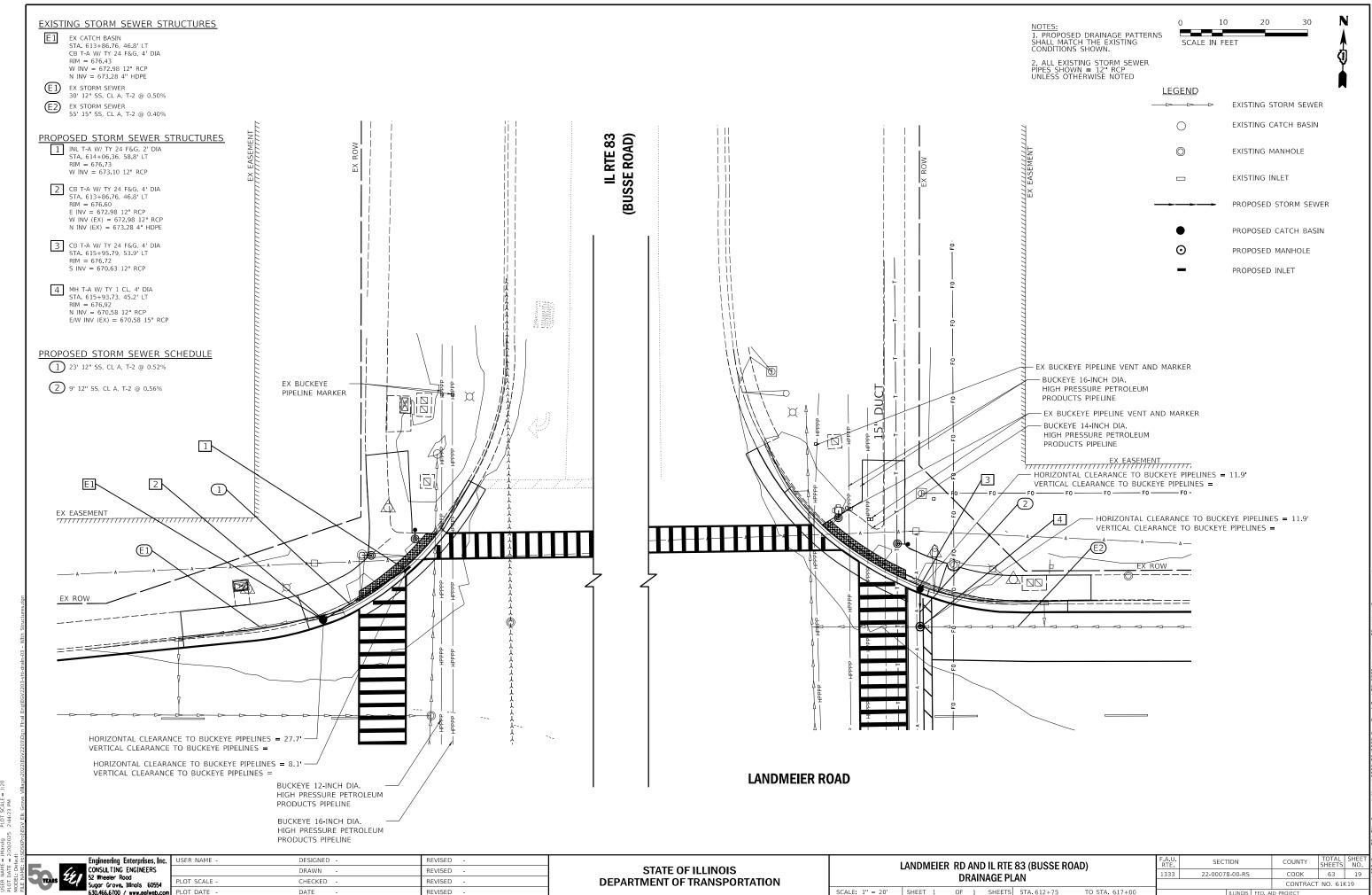
LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) **EROSION CONTROL PLAN** SCALE: 1" = 20' SHEET 2 OF 4 SHEETS STA. 608+00 TO STA. 613+25

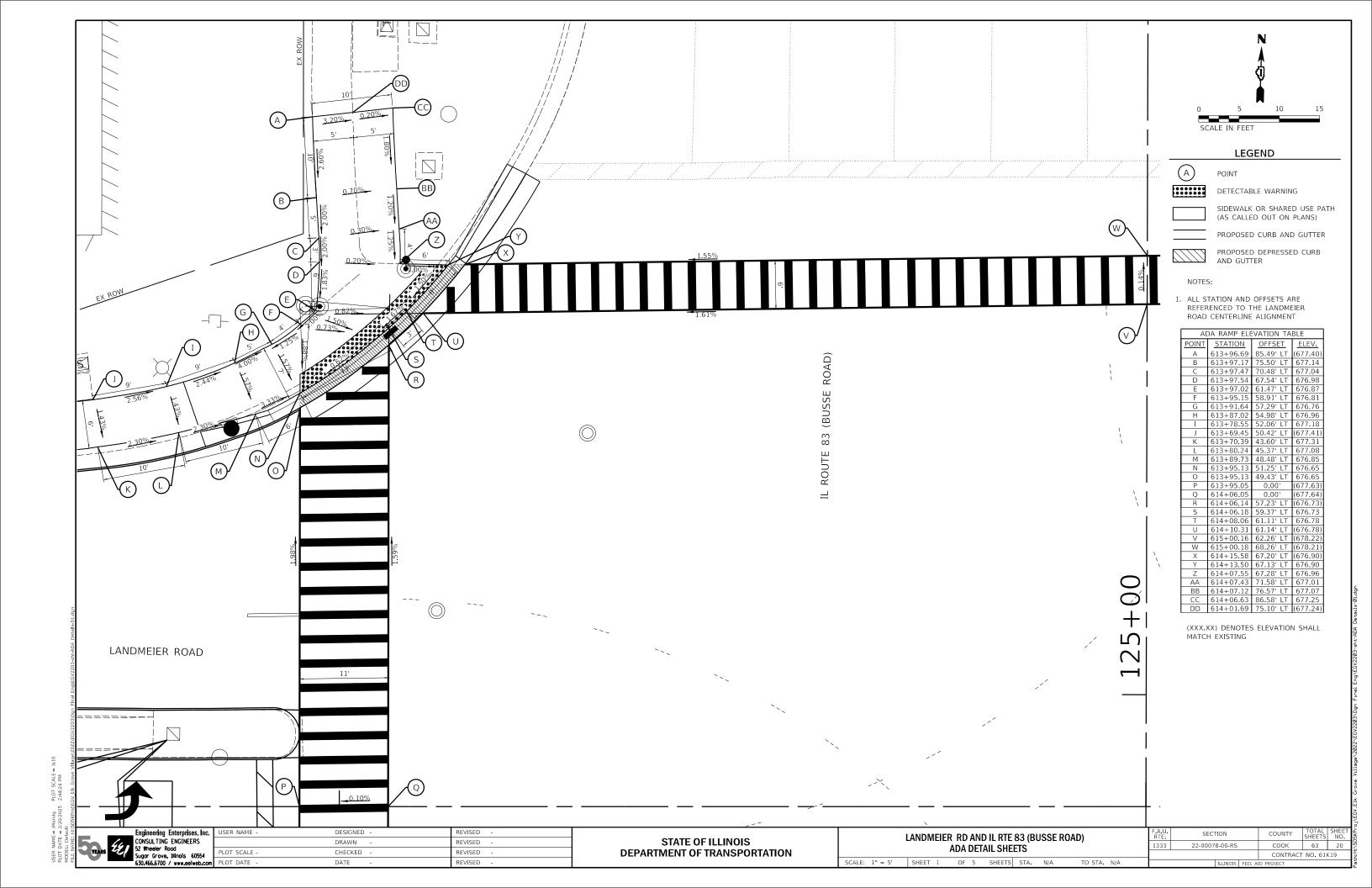
SECTION COUNTY 22-00078-00-RS COOK 63 16 CONTRACT NO. 61K19

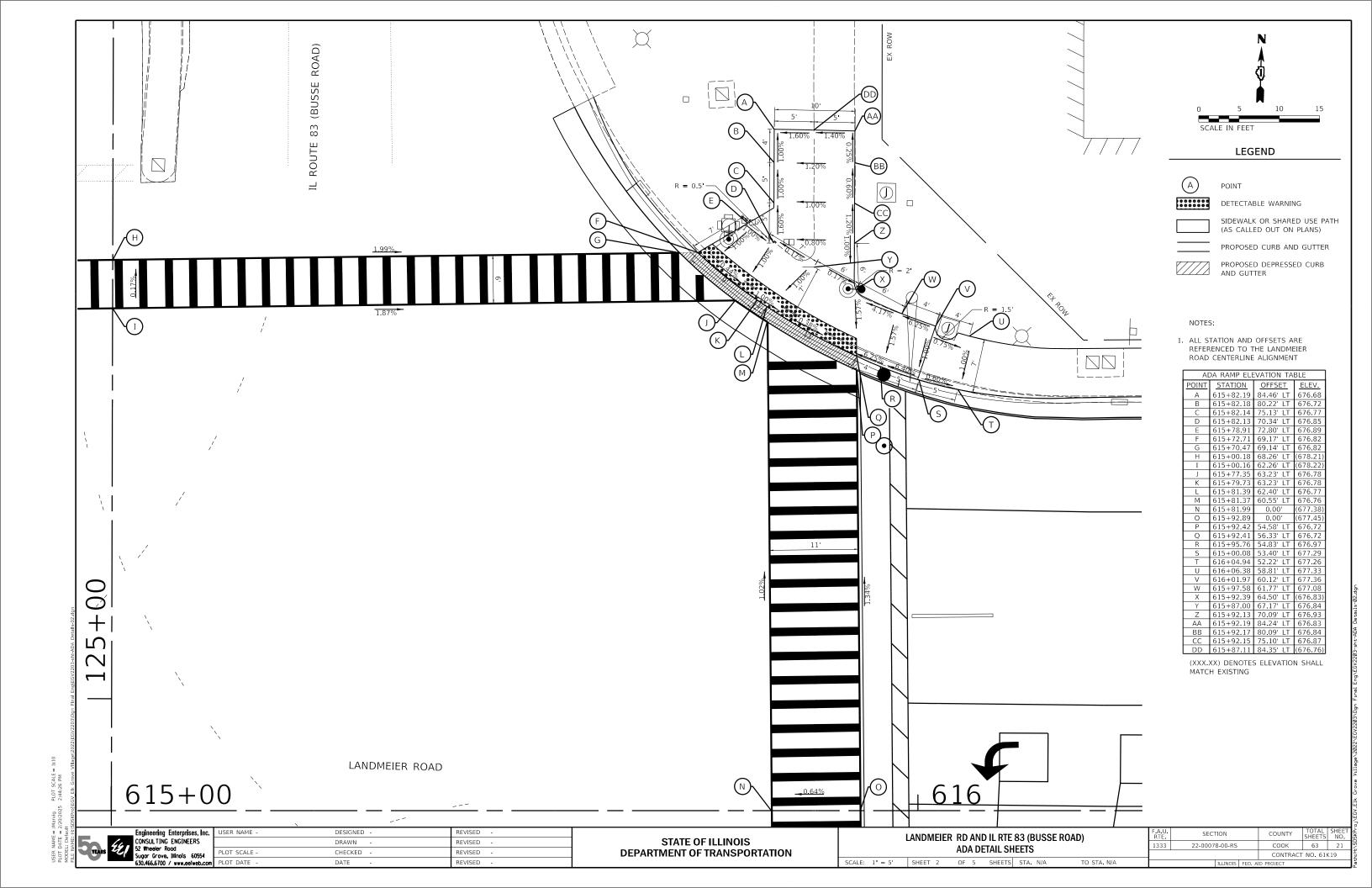
STATE OF ILLINOIS

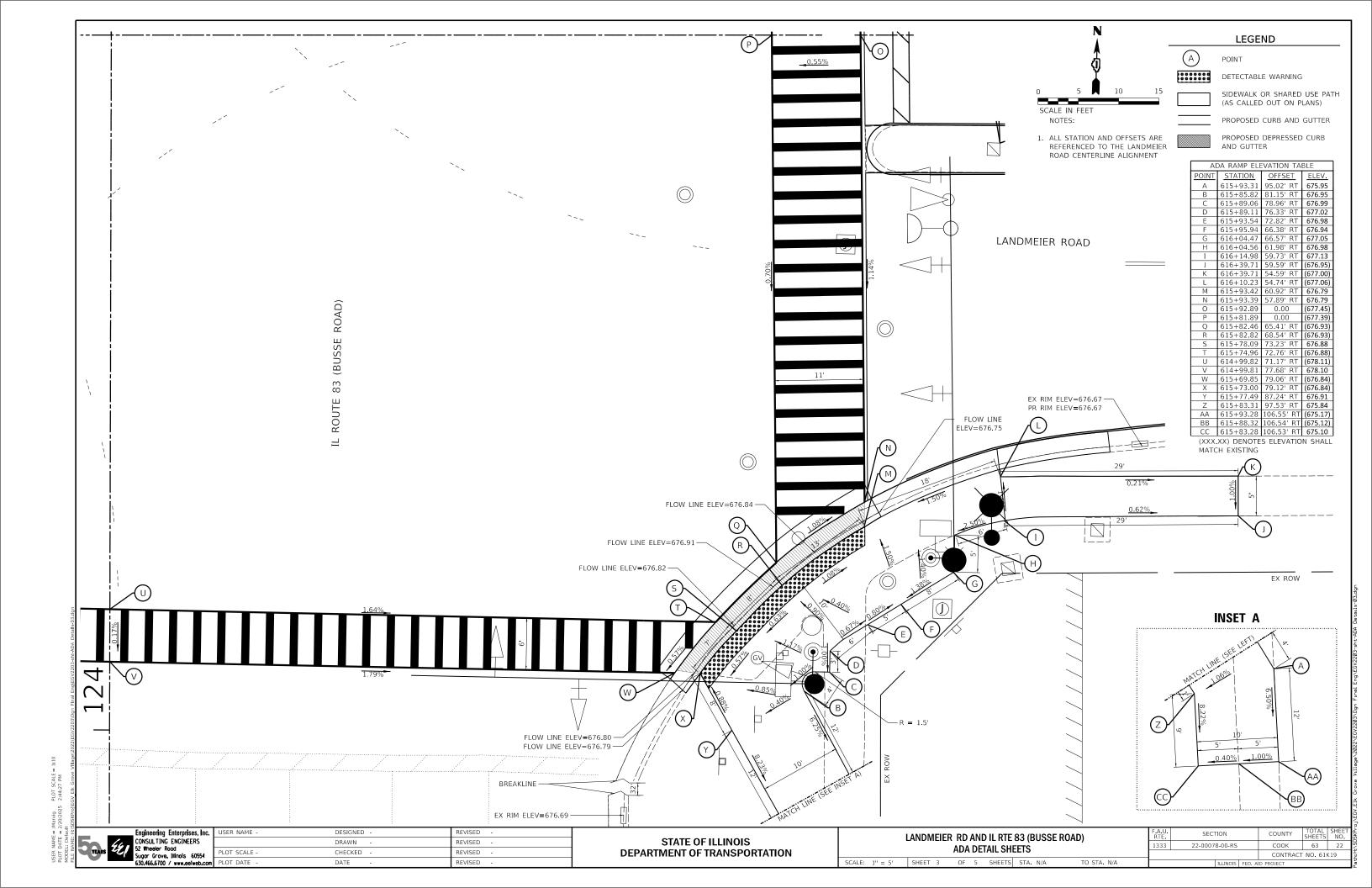


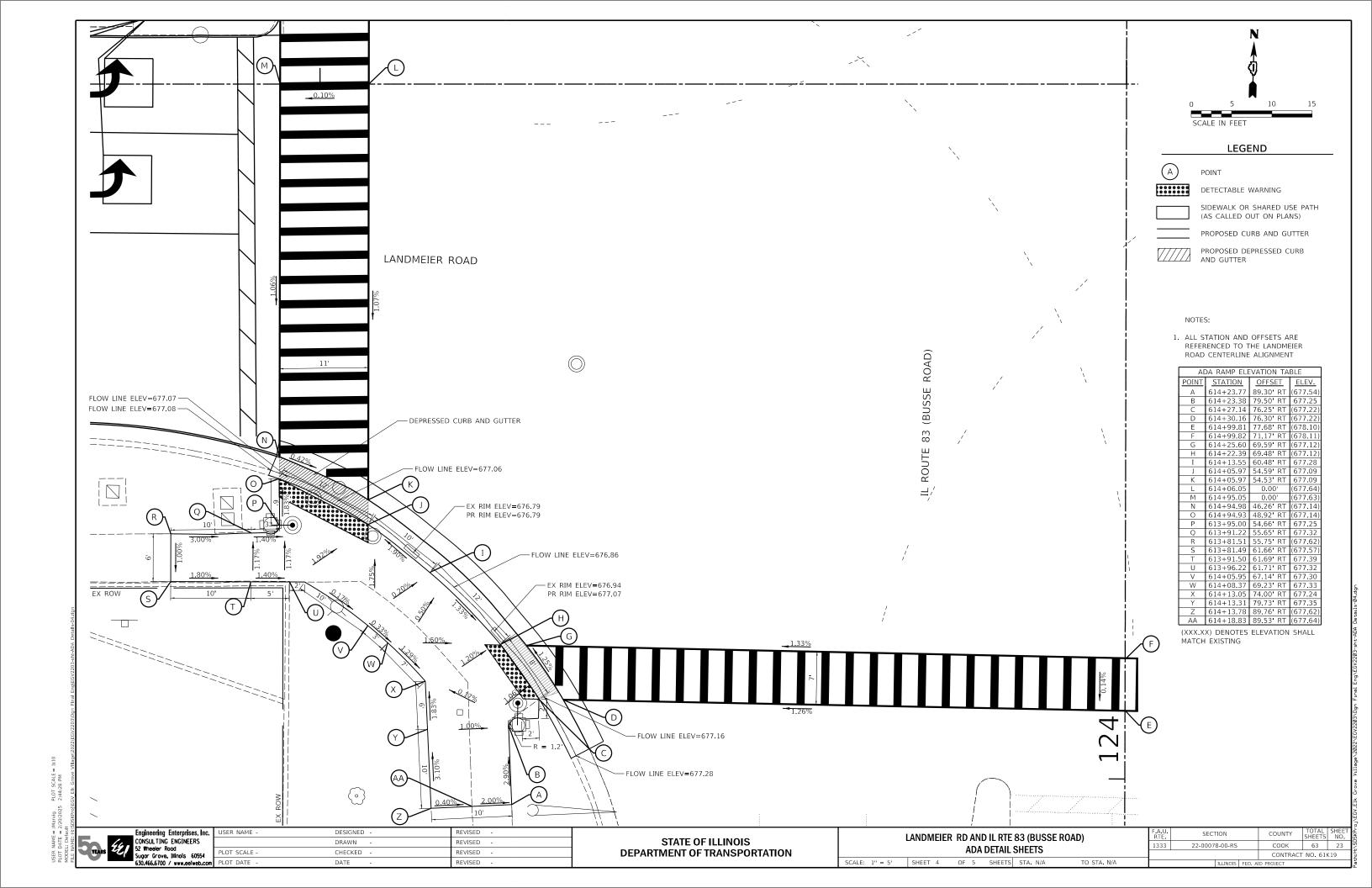


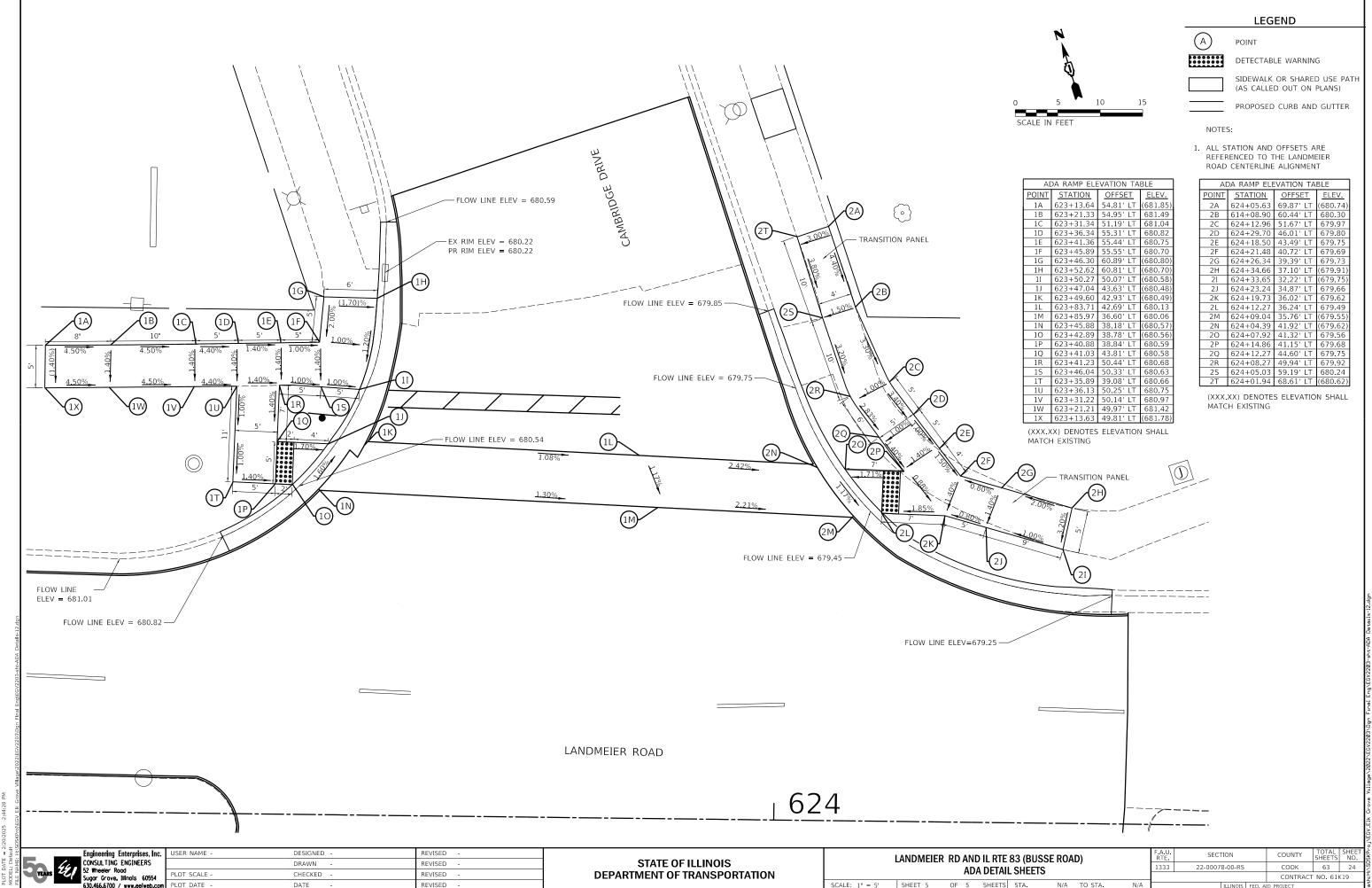




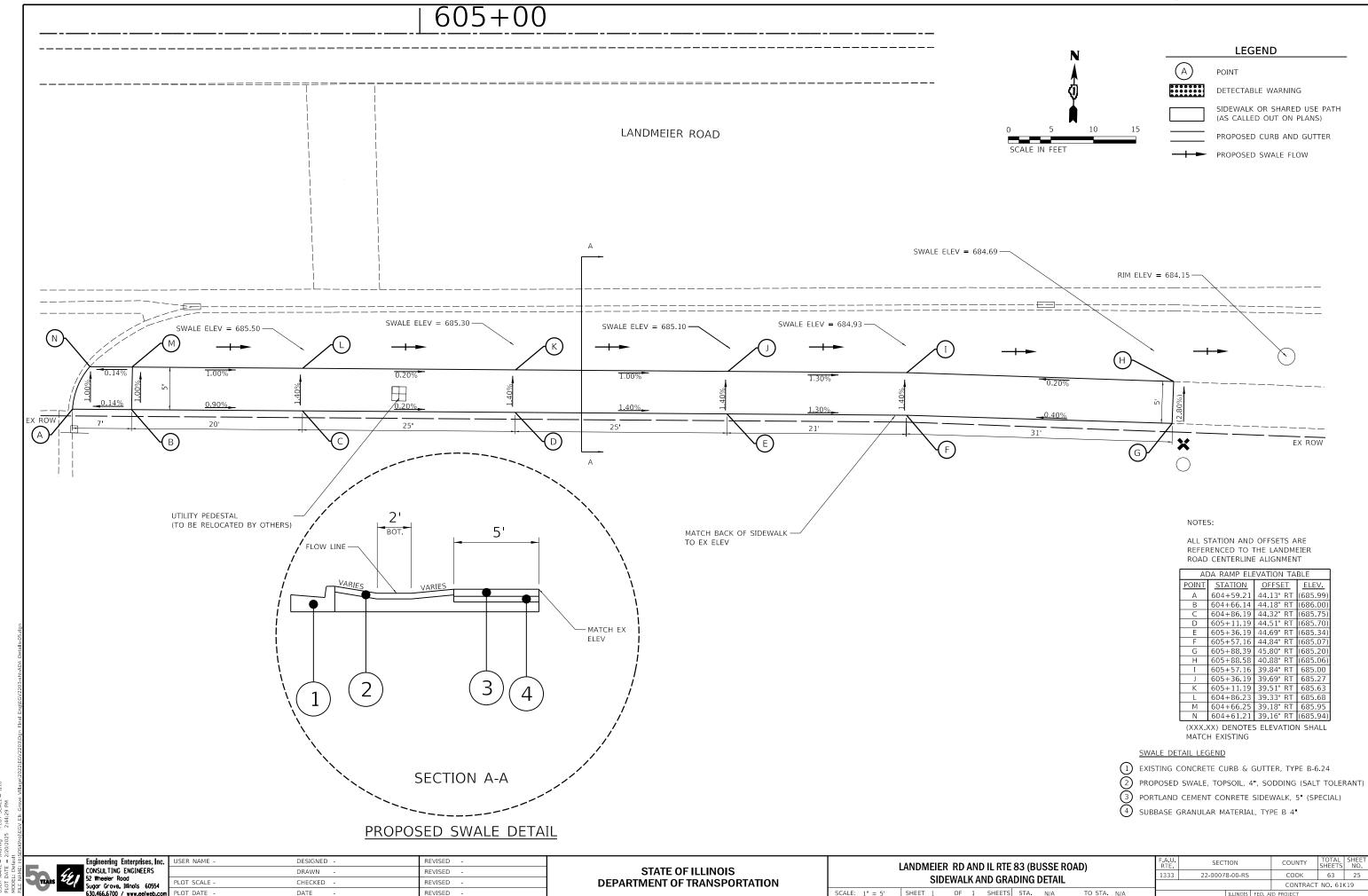








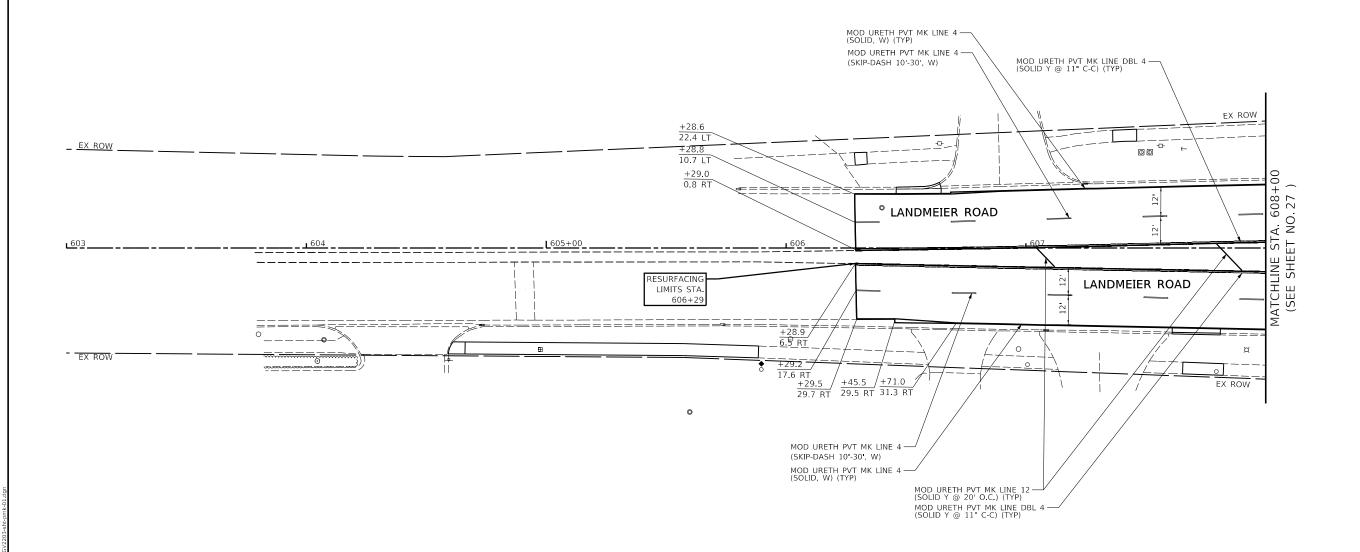
OF 5 SHEETS STA. N/A TO STA. CONTRACT NO. 61K19



NOTES:

1. ALL STATION AND OFFSETS ARE REFERENCED TO THE LANDMEIER ROAD CENTERLINE ALIGNMENT

2. ALL PAVEMENT MARKINGS ALONG COOK COUNTY MAINTAINED ROADS SHALL BE MODIFIED URETHANE.



Trans

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SQUART Crow, Illinois 60554
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

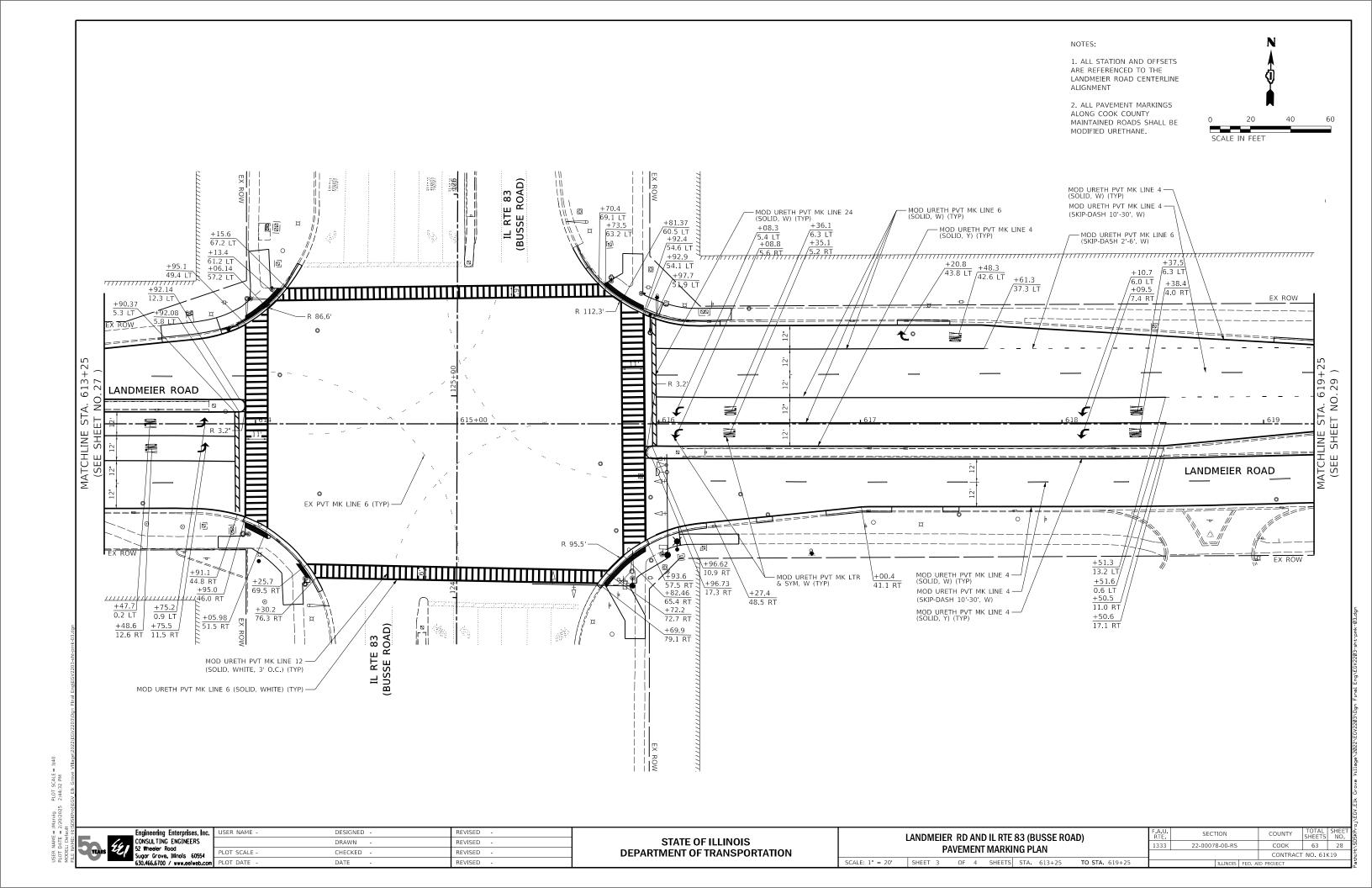
l	LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) PAVEMENT MARKING PLAN SHEET 1 OF 4 SHEETS STA. 606+29 TO STA. 608+00							
	SHEET	1	OF	4	SHEETS	STA.	606+29	TO STA. 608+00

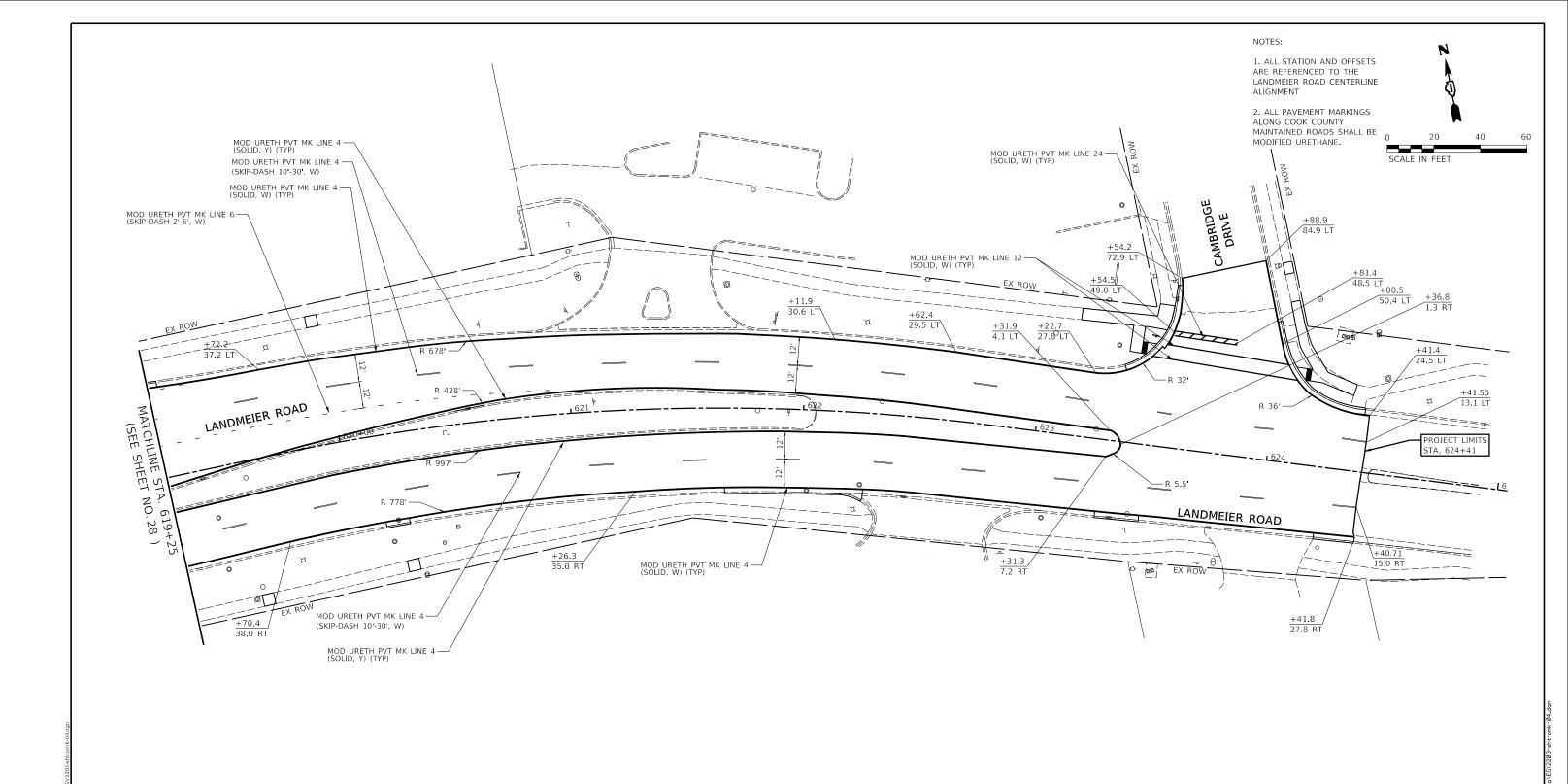
SCALE: 1" = 20'

I	F.A.U. RTE				COUNTY	TOTAL SHEETS	SHE
ſ	1333	22-000	78-00-RS	соок	63	2	
I			CONTRACT	NO. 61	<19		
I			ILLINOIS	FED. A	ID PROJECT		
	1333	22-000		FED. A	CONTRACT		11

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A.U. RTE	SECTION		COUNTY	TOTAL SHEETS	SHEE
1333	22-00078-00-RS		соок	63	27
			CONTRACT	NO. 61	(19
	ILLINOIS	FED. Al	ID PROJECT		





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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) PAVEMENT MARKING PLAN SCALE: 1" = 20' SHEET 4 OF 4 SHEETS STA. 619+25 TO STA. 624+41

SECTION 22-00078-00-RS COOK 63 29 CONTRACT NO. 61K19

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USER	MOD		Sugar Grove, Illinois 60554	L
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DEPARTMENT OF TRANSPORTATION	l

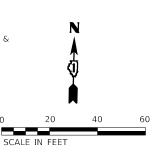
LANDMEIER RD AND IL RTE 83 (BUSSE ROAD)											
LANDSCAPING PLAN											
SCALE: 1" = 20'	SHEET 1	OF	4	SHEETS	STA.	604+59	TO STA.	608+00			

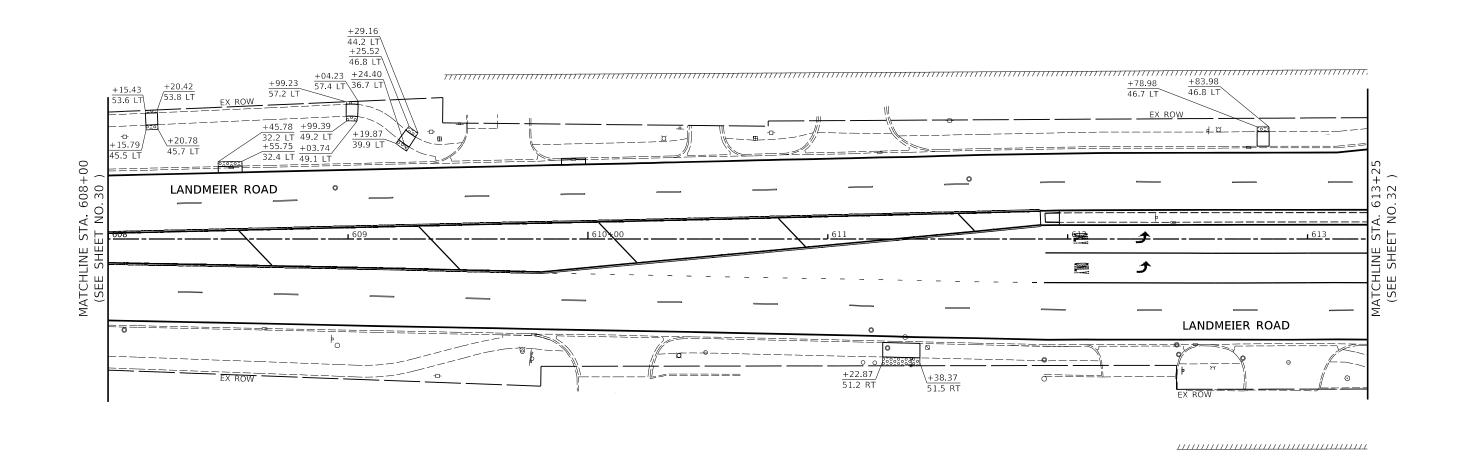
F.A.U. RTE	SECTION			COUNTY	TOTAL SHEETS	SHEE NO.
1333	22-00078-00-RS			соок	63	30
				CONTRACT	NO. 61	<19
ILLINOIS FED. A				ID PROJECT		

TOPSOIL, FURNISH & PLACE, 4" & SALT TOLERANT SOD

NOTES:

ALL STATION AND OFFSETS ARE REFERENCED TO THE LANDMEIER ROAD CENTERLINE ALIGNMENT



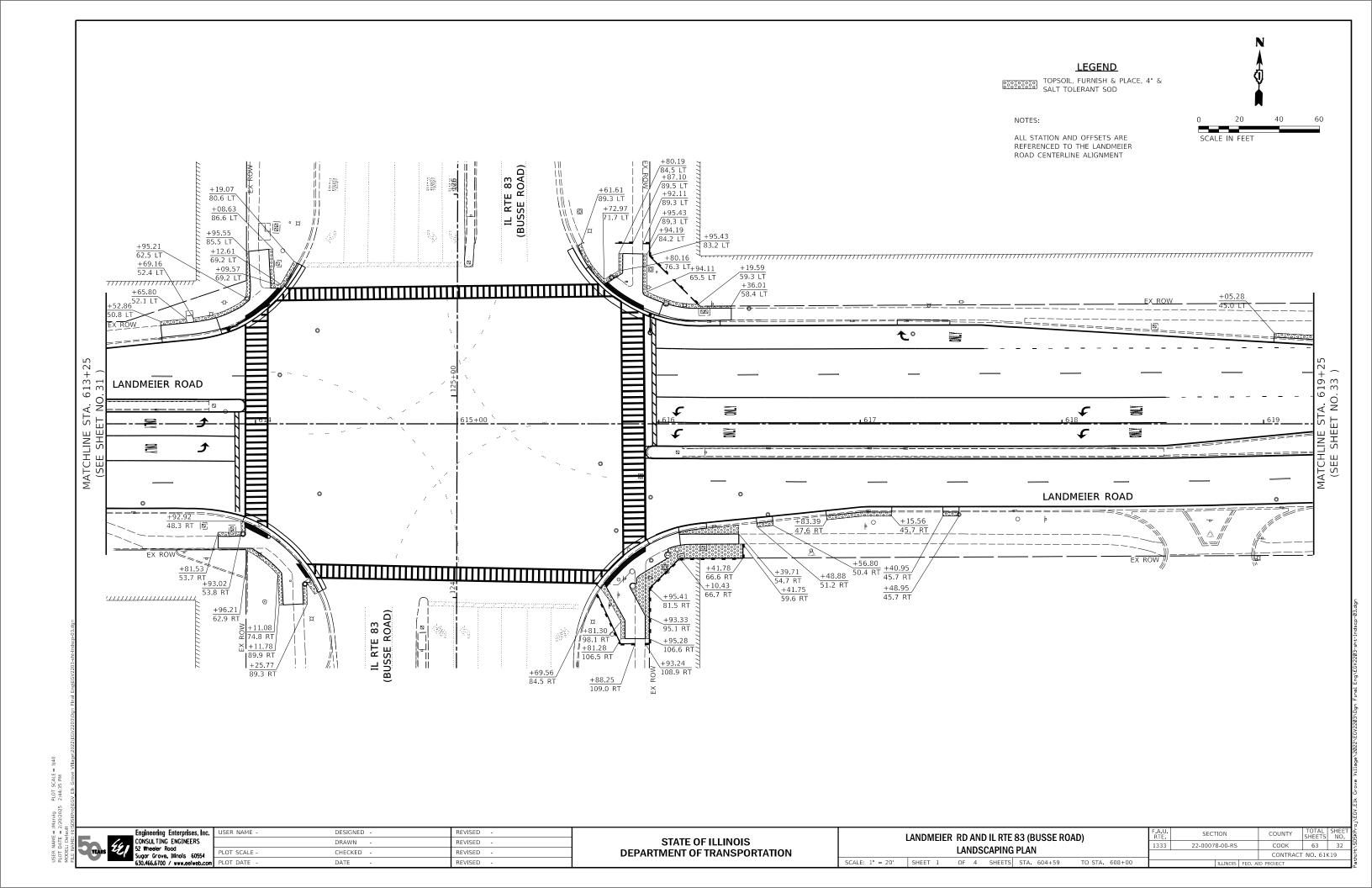


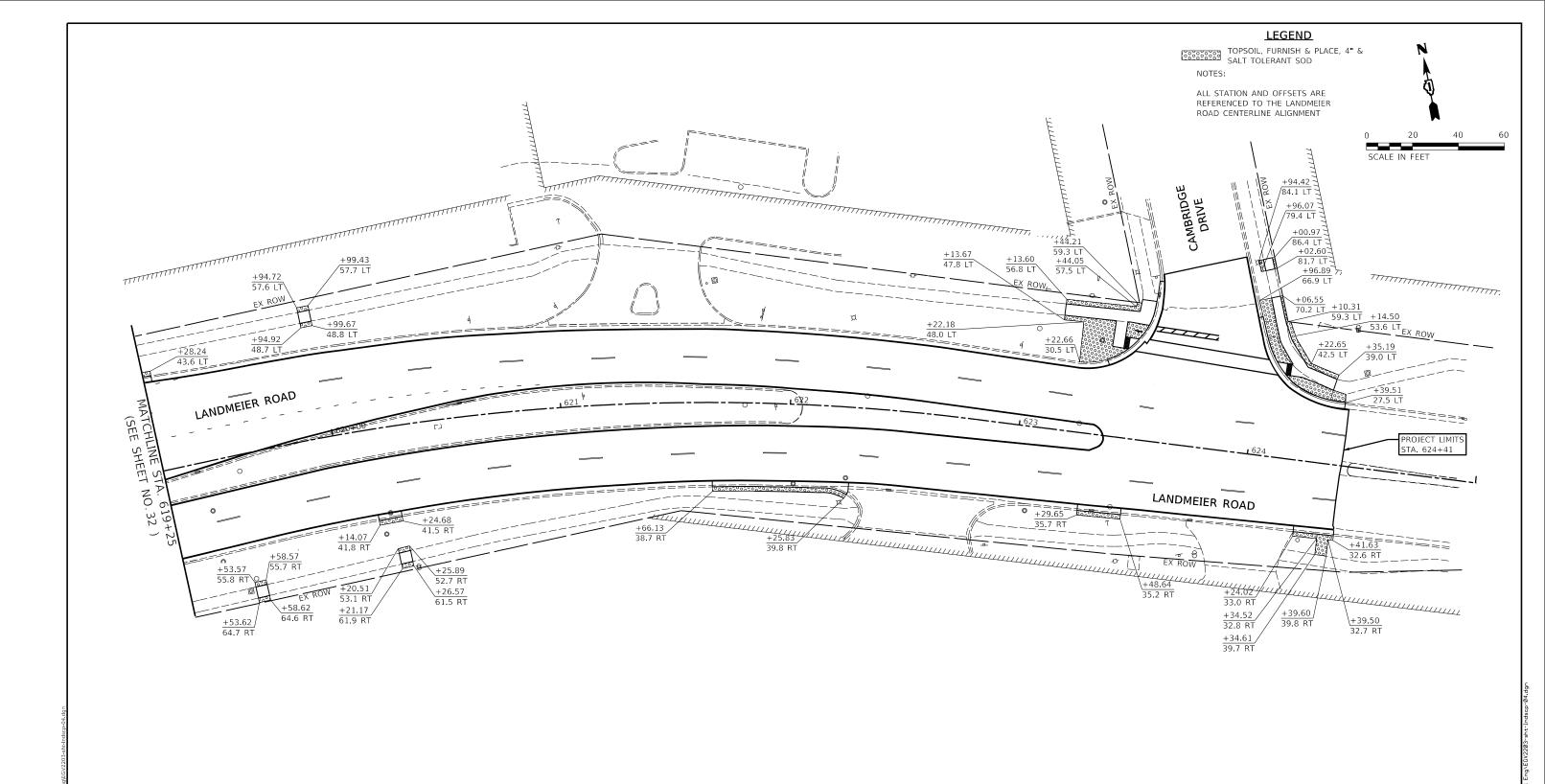
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SCALE: 1" = 20"	SHEET 2	2	OF	4	SHEETS	STA.	608+00	TO STA	۸.

F.A.U. RTE	SECTION			COUNTY	TOTAL SHEETS	SHEE NO.
1333	22-00078-00-RS			соок	63	31
				CONTRACT	NO. 61	<19
ILLINOIS FED. A				ID PROJECT		





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Sugar Grove, Illinois 60554
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

LANDMEIER RD AND IL RTE 83 (BUSSE ROAD)
LANDSCAPING PLAN

SCALE: 1" = 20' SHEET 4 OF 4 SHEETS STA. 619+25 TO STA. 624+41

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	<u>EXISTING</u>	PROPOSED	ITEM	<u>EXISTING</u>	<u>PROPOSED</u>	ITEM	<u>EXISTING</u>	PROPOSED
CONTROLLER CABINET	\boxtimes		HANDHOLE -SQUARE			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		R R Y
COMMUNICATION CABINET	ECC	CC	-ROUND				(*)<\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\	6 6
MASTER CONTROLLER	EMC	MC	HEAVY DUTY HANDHOLE -SQUARE -ROUND	H (H)	H 19		P	4 Y 4 Y 4 G P
MASTER MASTER CONTROLLER	EMMC	ммс	DOUBLE HANDHOLE			SIGNAL HEAD WITH BACKPLATE	(a) (a) (a)	RRR
UNINTERRUPTABLE POWER SUPPLY	4	•	JUNCTION BOX		0	-(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
SERVICE INSTALLATION -(P) POLE MOUNTED	P	- ■ -P	RAILROAD CANTILEVER MAST ARM	X = X = X	IOI I			R Y Y G G G G G G G G G G G G G G G G G
			RAILROAD FLASHING SIGNAL	∑O ∑	X⊕X		P RB	P RB
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	$\boxtimes^{G}\boxtimes^{GM}$	$\mathbf{Z}^{G}\mathbf{Z}^{GM}$	RAILROAD CROSSING GATE	∑⊖∑ >	X•X-	DEDECTRIAN CICAIAL HEAD		(4.)
TELEPHONE CONNECTION	ET	Т	RAILROAD CROSSBUCK	₹	*	PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		**
STEEL MAST ARM ASSEMBLY AND POLE	·	•	RAILROAD CONTROLLER CABINET		₽ -€	PEDESTRIAN SIGNAL HEAD	() C	₽ c x D
ALUMINUM MAST ARM ASSEMBLY AND POLE	0	•	UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			WITH COUNTDOWN TIMER		[* D
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	0 - X	•*	TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY	0	 ◆ BM 	SYSTEM ITEM	S	SP	NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE.		
(SM) BARREE MOORTES TERM OVART			INTERSECTION ITEM	I	IP	ALL DETECTOR LOOP CABLE TO BE SHIELDED	\sim	
WOOD POLE	⊗ .	•	REMOVE ITEM		R	GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		<u> </u>
GUY WIRE	>	-	RELOCATE ITEM		RL	ELECTRIC CABLE IN CONDUIT, TRACER	(1)	<u>—(1)</u> —
SIGNAL HEAD		-	ABANDON ITEM		Α	NO. 14 1/C	, -	•
SIGNAL HEAD WITH BACKPLATE	+t>> P P	+ > P P	CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF	COAXIAL CABLE	<u> </u>	— <u>c</u> —
SIGNAL HEAD OPTICALLY PROGRAMMED	> ^P +->	→ P + → P	MAST ARM POLE AND		RMF	VENDOR CABLE		
FLASHER INSTALLATION -(FS) SOLAR POWERED	0+> ^F 0+> ^{FS}	•► FS	FOUNDATION TO BE REMOVED		KIVIF	COPPER INTERCONNECT CABLE,	,	(a)
	□+> F □+> FS	■→ ^F ■→ ^{FS}	SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF	NO. 18, 3 PAIR TWISTED, SHIELDED	(6#18)	(6#18)
PEDESTRIAN SIGNAL HEAD	-0	-1	DETECTOR LOOP, TYPE I			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F		——————————————————————————————————————
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			PREFORMED DETECTOR LOOP	P P	P P	-NO. 62.5/125, MM12F SM24F		
RADAR DETECTION SENSOR	R	R	SAMPLING (SYSTEM) DETECTOR	5 5	5 (5)			— <u>36</u> F—
VIDEO DETECTION CAMERA	V	V	INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	IS (IS)	IS (IS)			
RADAR/VIDEO DETECTION ZONE			QUEUE AND SAMPLING (SYSTEM) DETECTOR	QS QS	QS QS	GROUND ROD -(C) CONTROLLER -(M) MAST ARM	$\stackrel{\underline{\dot{=}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{-}}}{\overset{\underline{\dot{-}}}{\overset{\underline{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{\dot{-}}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{-}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}}{\overset{\underline{\dot{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}}{\overset{\underline{-}}}}}{\overset{\underline{-}}}{\overset{\underline{-}}}{\overset{\underline{-}}}}{\overset{\underline{-}}}}{$	$\stackrel{=}{\overset{C}{\downarrow}}^{C} \stackrel{\stackrel{=}{\downarrow}^{M}}{\overset{=}{\downarrow}}^{P} \stackrel{\stackrel{=}{\downarrow}^{S}}{\overset{=}{\downarrow}}^{S}$
PAN, TILT, ZOOM (PTZ) CAMERA	PTZ	PTZ	WIRELESS DETECTOR SENSOR	®	®	-(P) POST -(S) SERVICE		
EMERGENCY VEHICLE LIGHT DETECTOR	\bowtie	~	WIRELESS ACCESS POINT					
CONFIMATION BEACON	0-()	+1						
WIRELESS INTERCONNECT	○ +++ -	•++++						
WIRELESS INTERCONNECT RADIO REPEATER	ERR	RR						

_	Engineering Enterprises, Inc.	Г
S	CONSULTING ENGINEERS	Г
YEARS	52 Wheeler Road Sugar Grove, Illinois 60554	Γ
	630,466,6700 / www.eeiweb.com	Г

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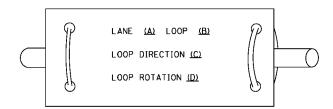
STAT	E OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

	DISTRICT ONE											
:	STANDARD	TRAFFIC	SIGNA	. DESIGN	DETAILS							
SCALE: NONE	SHEET 1	OF 7	SHEETS	STA.	TO STA.							

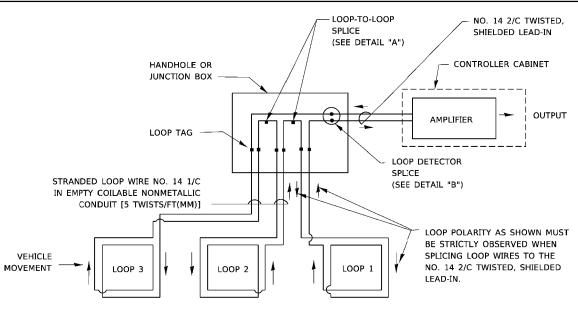
F.A. RTE.	SEC	ION		COUNTY	TOTAL SHEETS	SHEET NO.
1333	22-00078-00-RS			СООК	63	34
	TS-05		CONTRACT NO	61	(19	
		ILLINOIS	FED. A	D PROJECT		

- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER
 DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS.
 SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

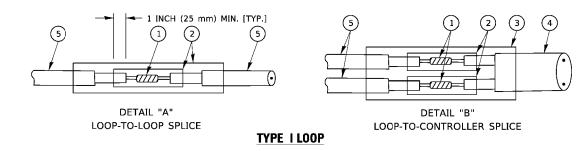


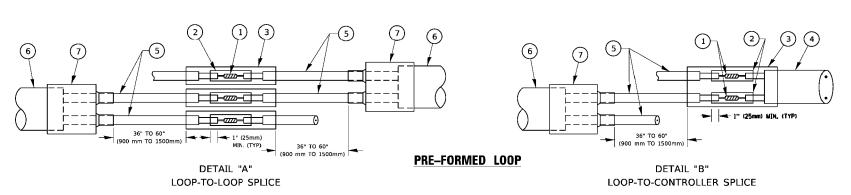
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
 SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE





LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

SCALE: NONE

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE PRE-FORMED LOOP
- (6) XL POLYOLEFIN 2 CONDUCTOR
- (7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SHEET 2 OF 7 SHEETS STA. TO ST.

 F.A. RTE.
 SECTION
 COUNTY SHEETS
 TOTAL SHEETS
 SHEET NO.

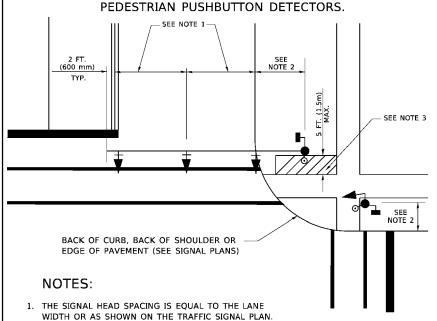
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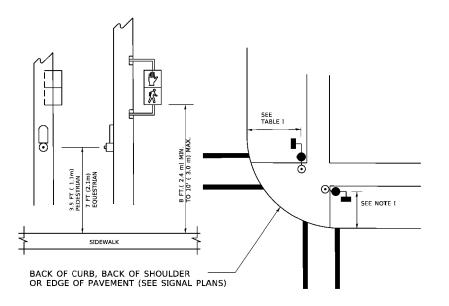
TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND



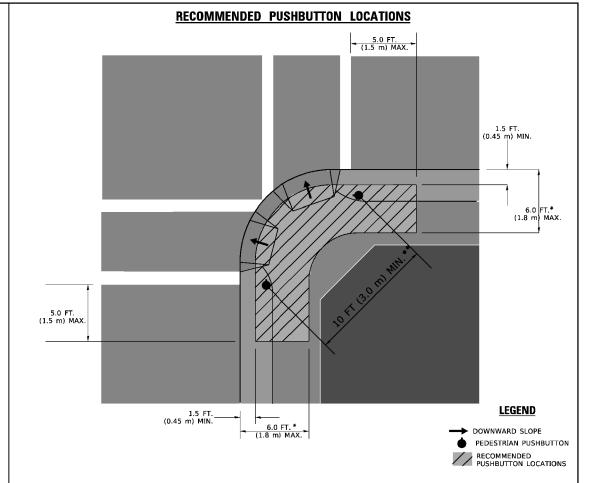
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- . THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

SCALE: NONE

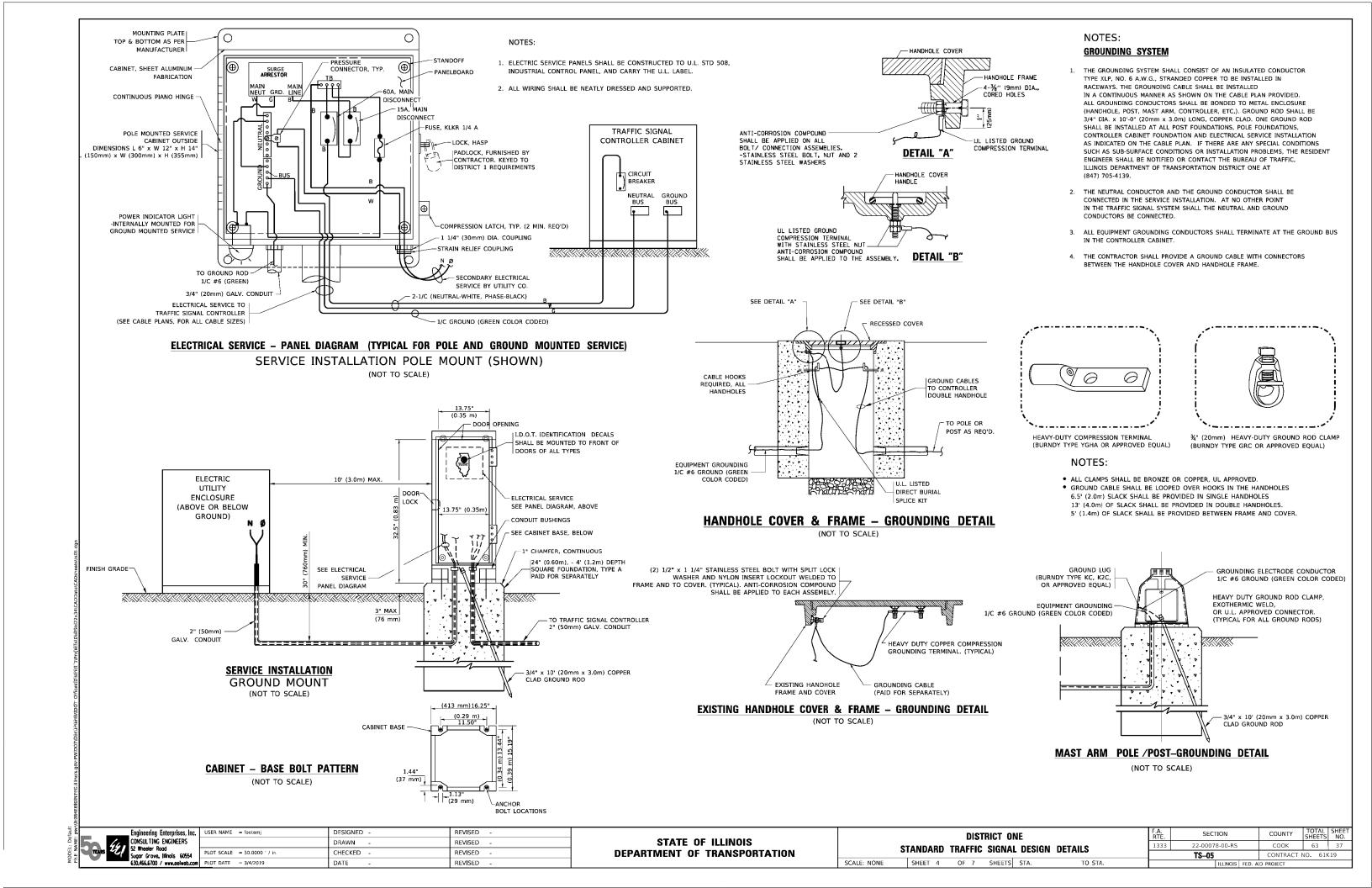
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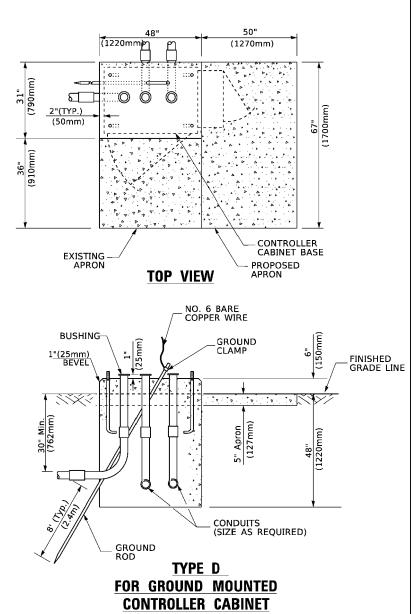
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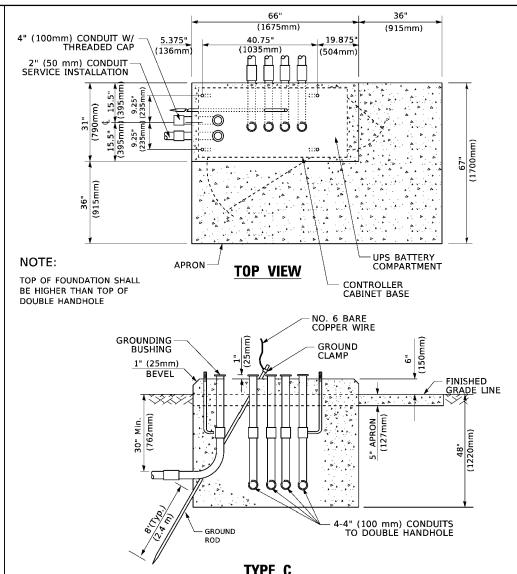
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
		DETAILS	1333	22-00078-00-RS	соок	63	36			
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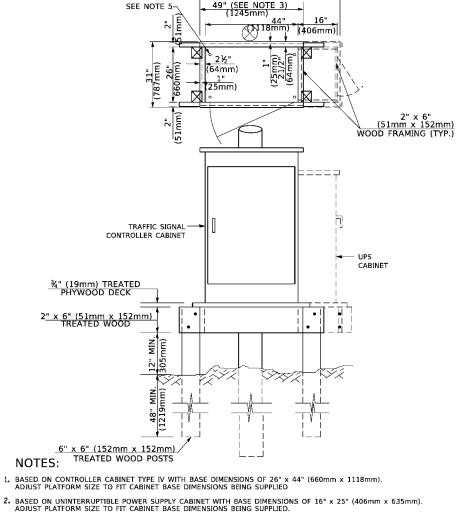








TYPE C FOR GROUND MOUNTED SUPER P (TYPE IV) AND SUPER R (TYPE V) **CONTROLLER CABINETS**



65" (SEE NOTE 4) (1651mm)

- 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET
- 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER **WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH		1
CADLE SLACK LENGTH	FEET	METE
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

CABLE SLACK

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

SCALE: NONE

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30′ (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
30' (9.1 m) and less than 40' (12.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0'' (4.6 m)	36'' (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along
 the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa).
 This strength shall be verified by boring data prior to construction or with testing by the Engineer
 during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised
 design if other conditions are encountered.
- 2. Combination most arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
- 4. For most arm assemblies with dual arms refer to state standard 878001...

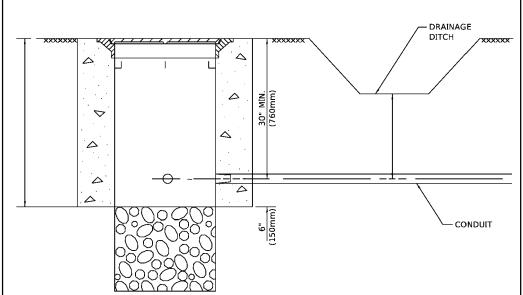
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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CONSULTING EN	GINEERS
YEARS Sugar Grove, Illin	ois 60554 P
630,466,6700 / w	

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STATI	: OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

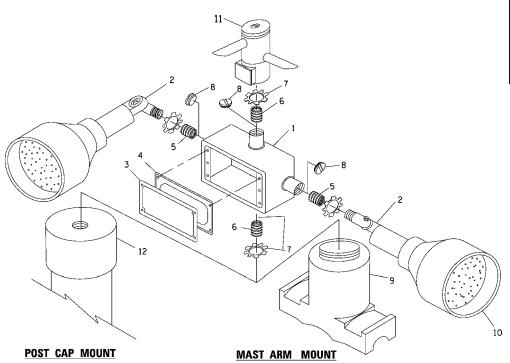
DISTRICT ONE	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	1333	22-00078-00-RS	COOK	63	38
STANDARD HALLIC SIGNAL DESIGN DETAILS		T\$-05	CONTRAC	T NO. 61	K19
SHEET 5 OF 7 SHEETS STA TO STA		ILLINOIS FED A	D PROJECT		



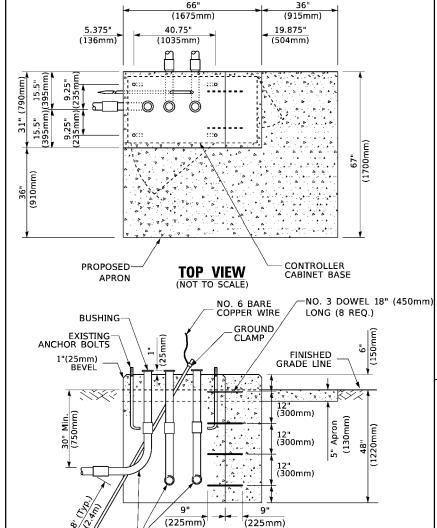
NOTES:

- 1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- 2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- 3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

EXISTING CONDUITS

EXISTING GROUND ROD

(NOT TO SCALE)

(12mm) 1.18"-(30mm) -0.25" (6mm 0.25" **MATERIAL** └─0.31"(8mm) - ASTM A36 STEEL - ASTM A-123 HOT DIPPED GALVANIZED - 0.20"(5mm) HEIGHT WEIGHT 9.5"(241mm 19"(483mn 7" (178mm) - 12" (300mm 53 lbs (24kg) VARIES 7" (178mm) - 12" (300m 68 lbs (31 kg)

SHROUD

7" (178mm) - 12" (300mm

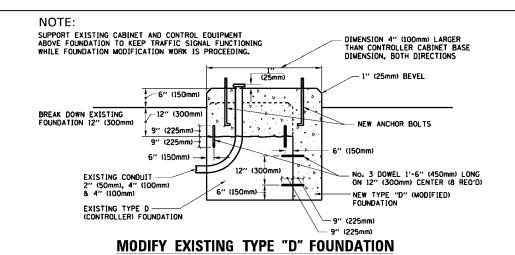
7" (178mm) - 12" (300mn

81 lbs (37 kg) 126 lbs (57 kg)

VARIES

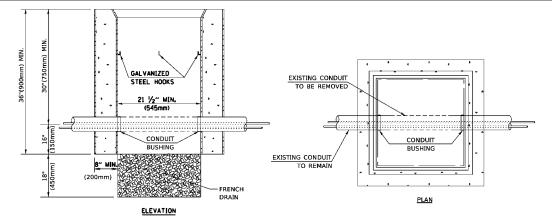
VARIES

- 1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- 2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS
- 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE



OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M) LAMP HOLDER AND COVER 3 OUTLET BOX COVER 4 RUBBER COVER GASKET 5 REDUCING BUSHING 6 ¾"(19 mm) CLOSE NIPPLE ¾"(19 mm) LOCKNUT SADDLE BRACKET - GAL 6 WATT PAR 38 LED FLOOD LAMP DETECTOR UNIT 12 POST CAP [18 FT. (5.4 m) POST MIN.]

- 1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR
- 2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- 3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



- 1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

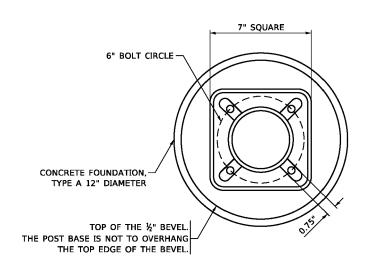
DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS SHEET 6 OF 7 SHEETS STA.

COUNTY SHEETS NO. соок CONTRACT NO. 61K19

ngineering Enterprises, Inc. | USER NAME = footem ONSULTING ENGINEERS

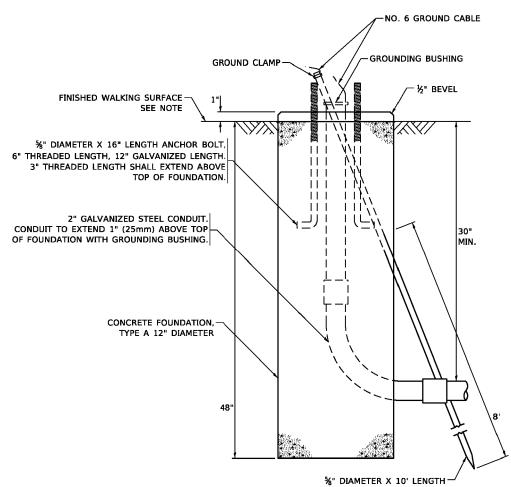
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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**



BOLT PATTERN

1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



TYPE A 12-INCH DIAMETER

PEDESTRIAN SIGNAL POST, 10 FT.

PEDESTRIAN SIGNAL POST, 5 FT.







<u>R10-3b</u>

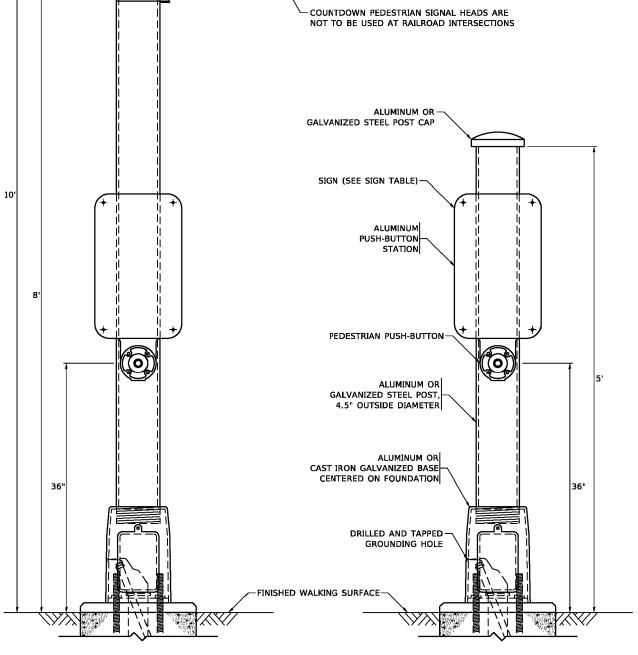
R10-3d

<u>R10–3e</u>

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

- 1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
- 2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
- 3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.



PEDESTRIAN SIGNAL HEAD

CONCRETE FOUNDATION,

GROUND ROD

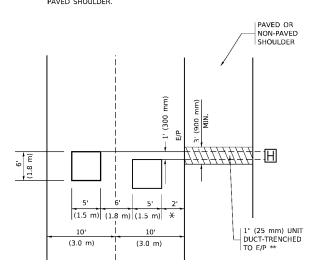
		Engineering Enterprises, Inc.	US
50	66.	CONSULTING ENGINEERS	
YEARS	4	52 Wheeler Rood Sugar Grove, Illinois 60554	PLO
		630,466,6700 / www.eeiweb.com	PLO

JSER NAME = gaglianobt REVISED -10-15-2020 DESIGNED - IP DRAWN REVISED OT SCALE = 100.0000 ' / in. CHECKED -REVISED OT DATE = 11/23/2020 10-15-2018 REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS SHEET 7 OF 7 SHEETS STA.

22-00078-00-RS COOK 63 CONTRACT NO. 61K19



* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS

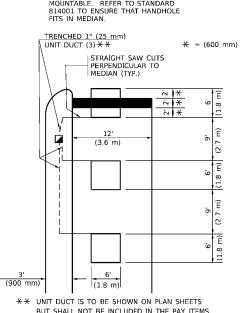
+ = (600 mm)

VOLUME DENSITY ("FAR OUT" DETECTION)

ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

LEFT TURN LANES WITH MEDIANS

NAME OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE

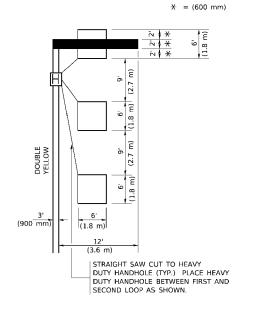


PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

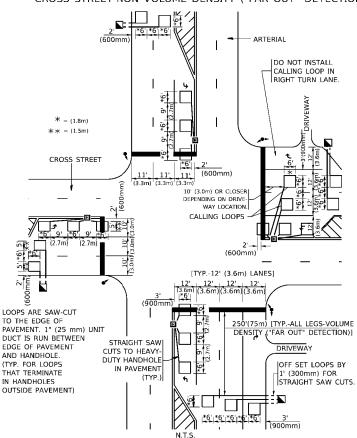
(PROTECTED / PERMITTED LEFT TURN PHASING)



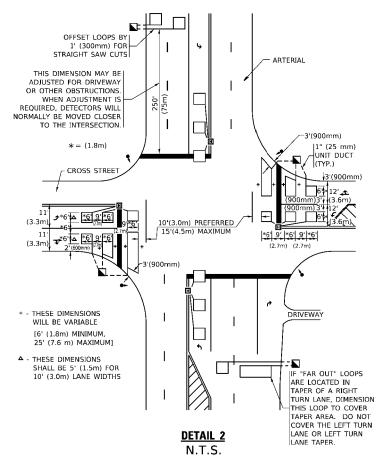
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIFL DED
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN, WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

Engineering Enterprises, Inc DISULTING ENGINEERS

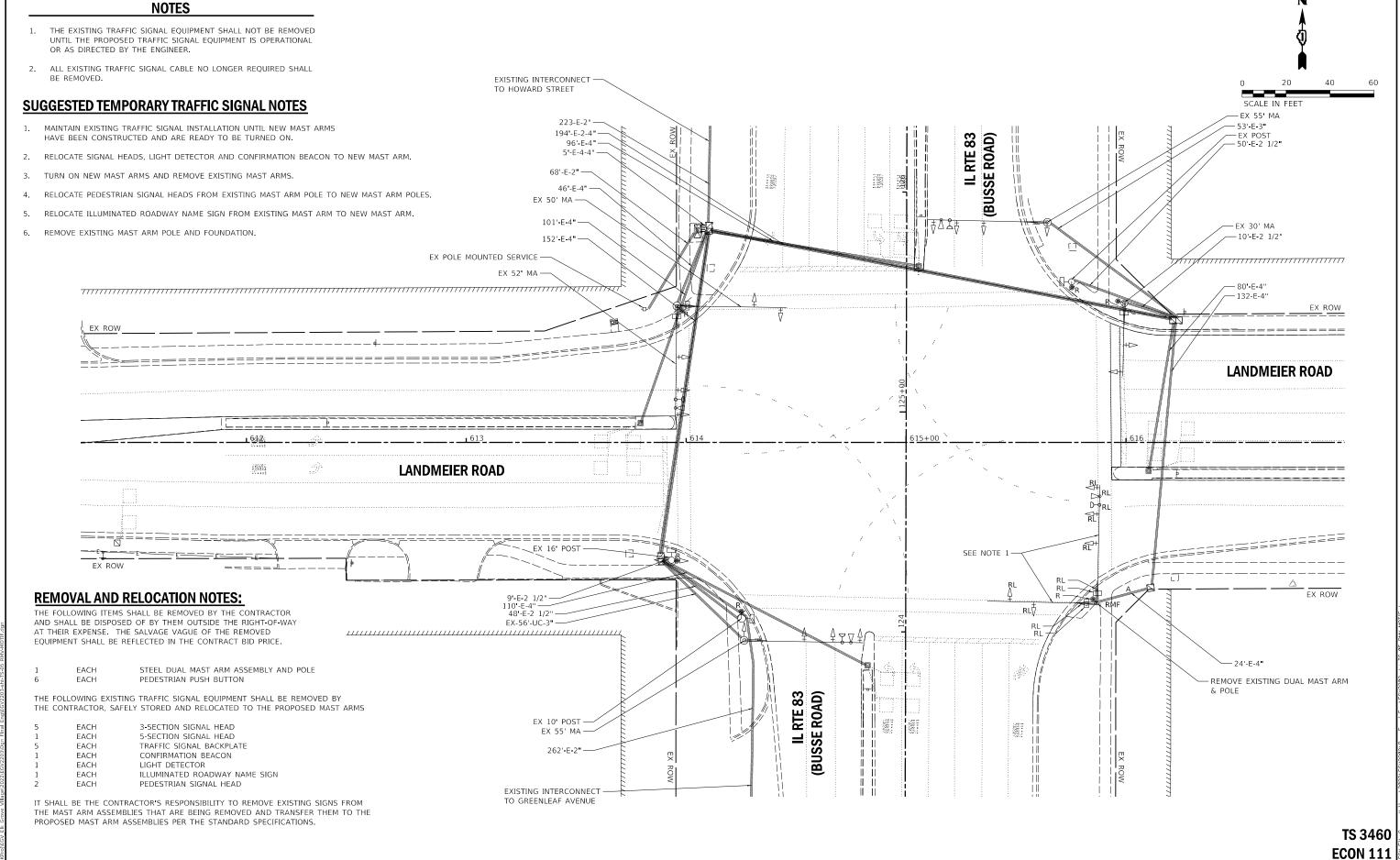
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DETAIL 1

N.T.S.

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** DISTRICT 1 - DETECTOR LOOP INSTALLATION **DETAILS FOR ROADWAY RESURFACING** SHEET 1 OF 1 SHEETS STA. TO STA.

SECTION COUNTY HEETS 22-00078-00-RS COOK 63 41 TS-07 CONTRACT NO. 61K19



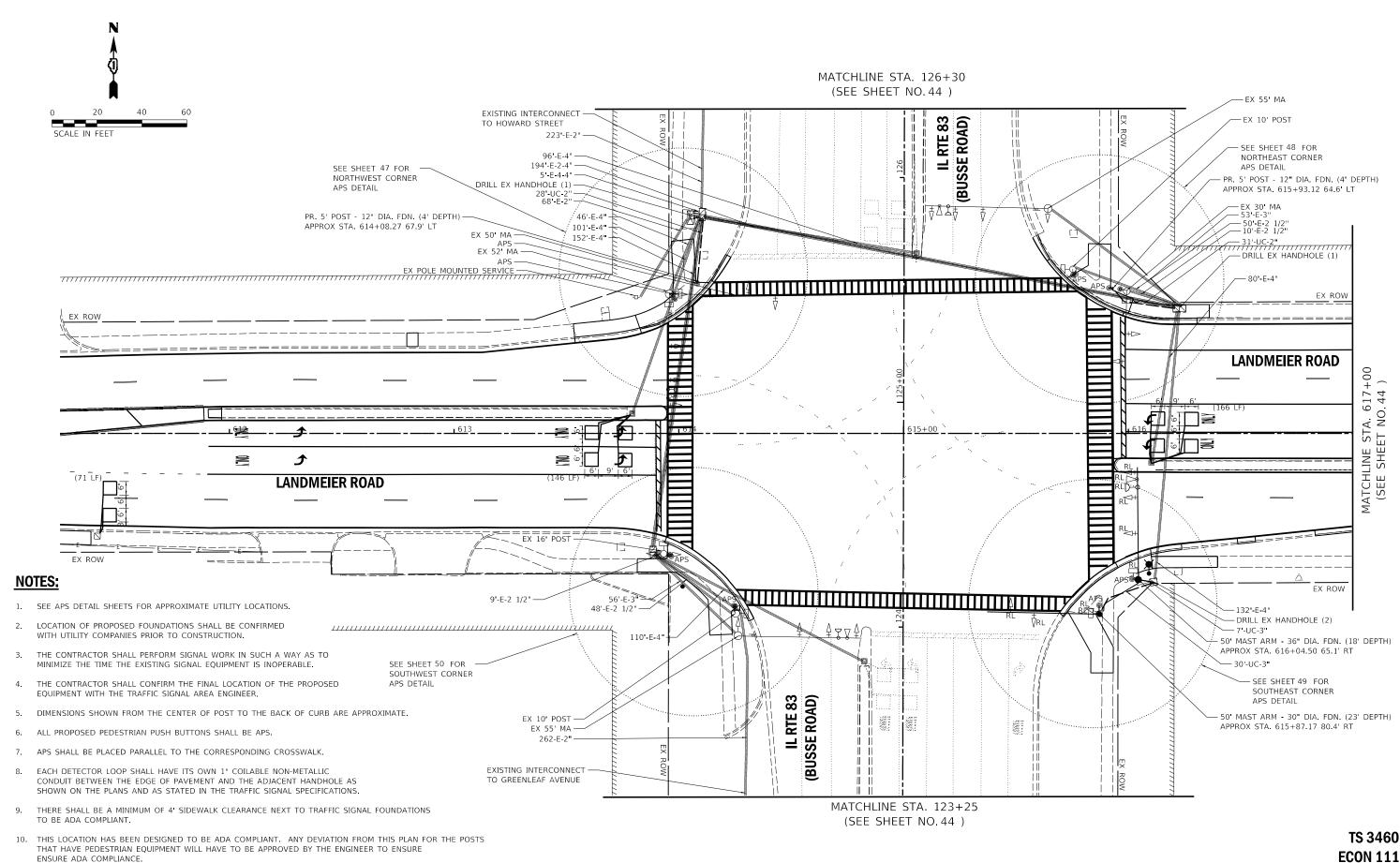
Engineering Enterprises, Inc. ONSULTING ENGINEERS

DESIGNED DRAWN REVISED CHECKED REVISED DATE

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL REMOVAL PLAN LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) SCALE: 1" = 20' SHEET 1 OF 3 SHEETS STA.

SECTION COUNTY 22-00078-00-RS COOK 63 42 CONTRACT NO. 61K19



USER NAME = JMarvig PLOT SCALE = 1:40
PLOT DATE = 2/20/2025 2:44:59 PM

5 TEARS EX

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eelweb.com

 USER NAME DESIGNED REVISED

 DRAWN REVISED

 PLOT SCALE CHECKED REVISED

 PLOT DATE DATE REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LANDMEIER RD AND IL RTE 83 (BUSSE ROAD)
TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 1 OF 2)

1 20' SHEET 1 OF 2 SHEETS STA TO STA

IL ROUTE 83 (BUSSE ROAD)

128+00

ONLY ONLY

ECON 111 COUNTY TOTAL SHEETS NO.
COOK 63 44 COUNTY 22-00078-00-RS CONTRACT NO. 61K19

TS 3460

250' TO STOP BAR -

617 + 00



DATE

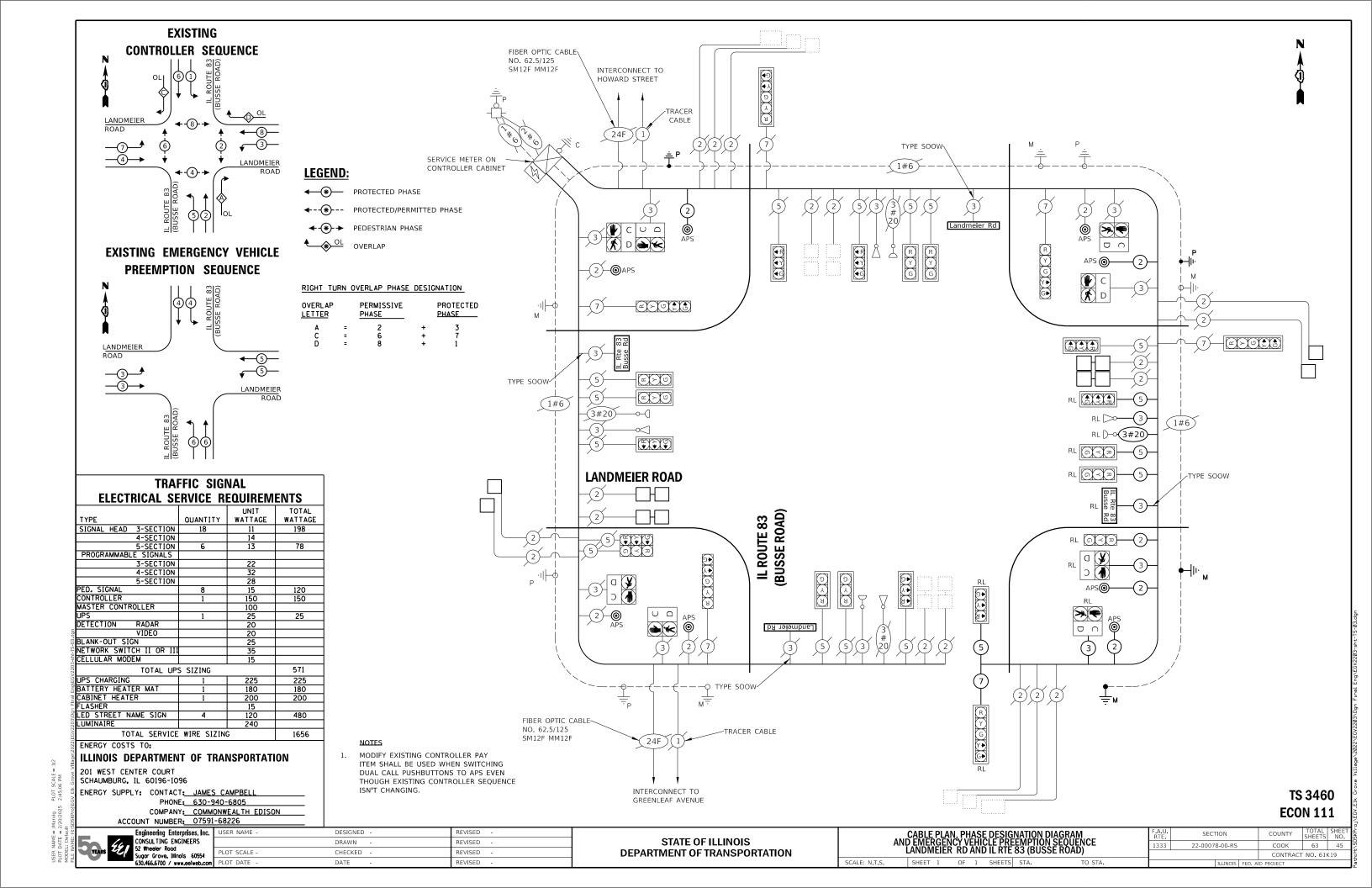
INTERCONNECT CONDUIT -TO HOWARD STREET

INTERSECTION AND SAMPLING (SYSTEM) DETECTORS

BAR

TO STOP

250'



Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Rood Sugar Crove, Illinois 60554 630.466.6700 / www.eelweb.com PLOT DATE -

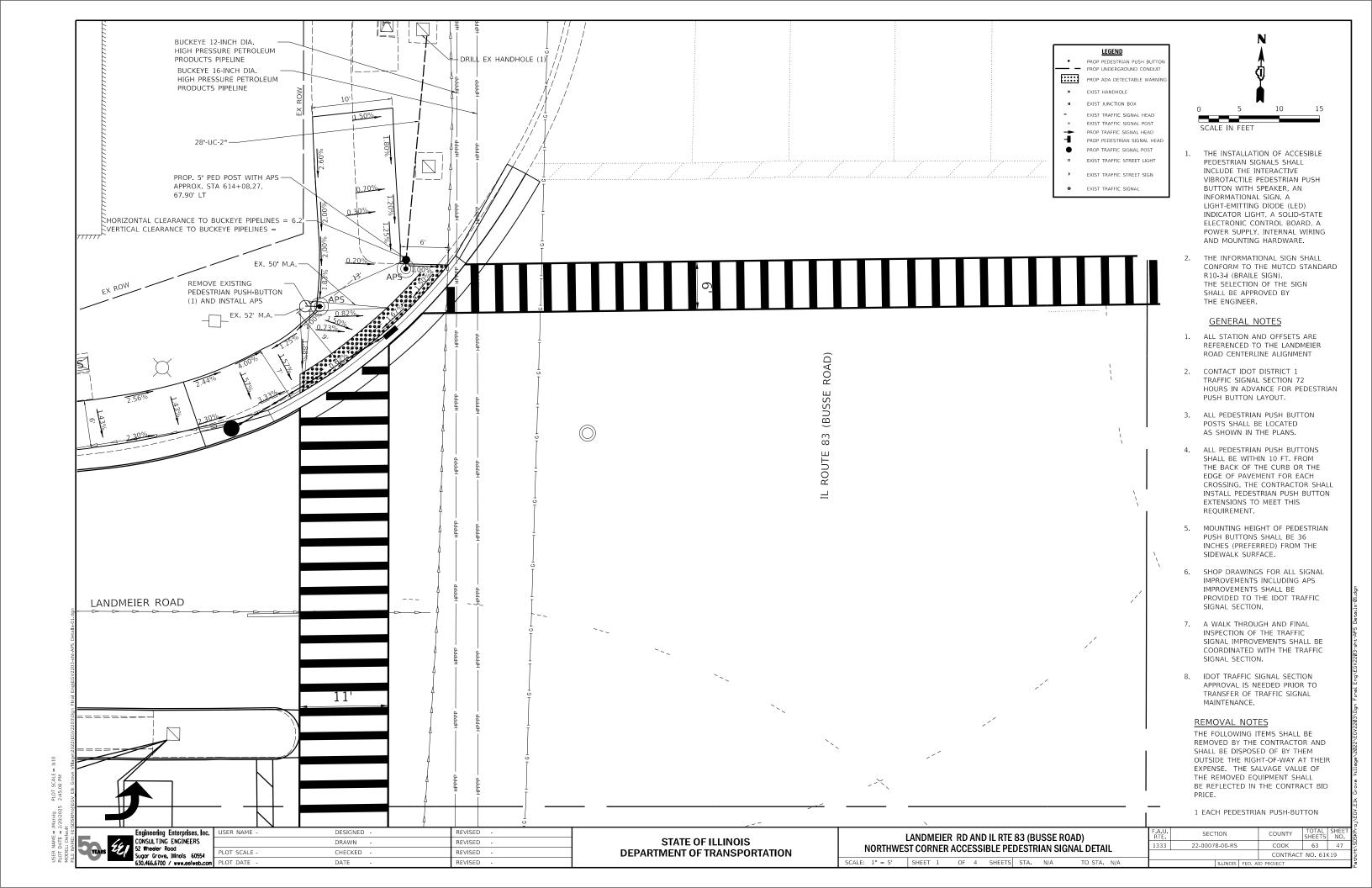
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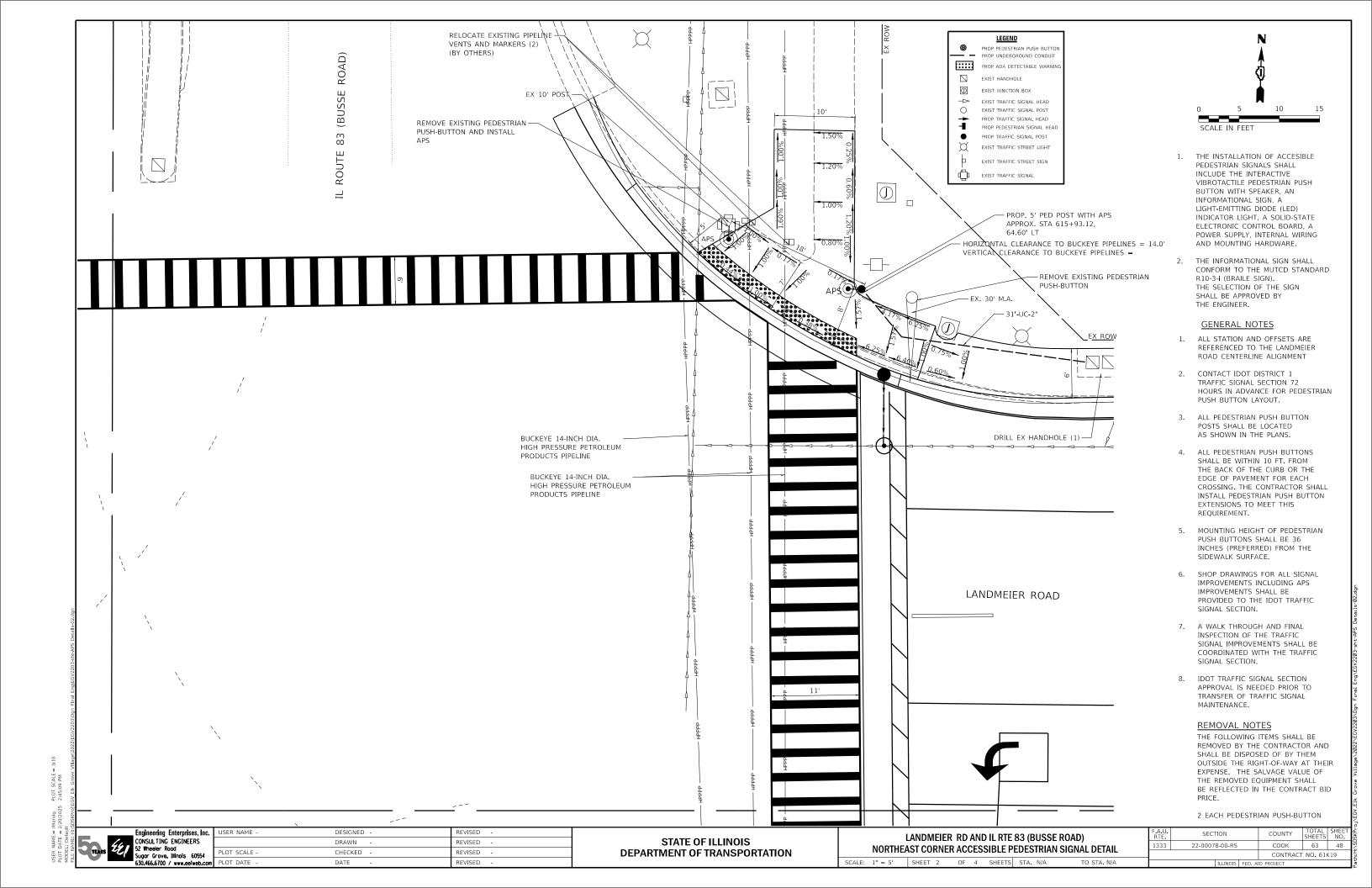
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

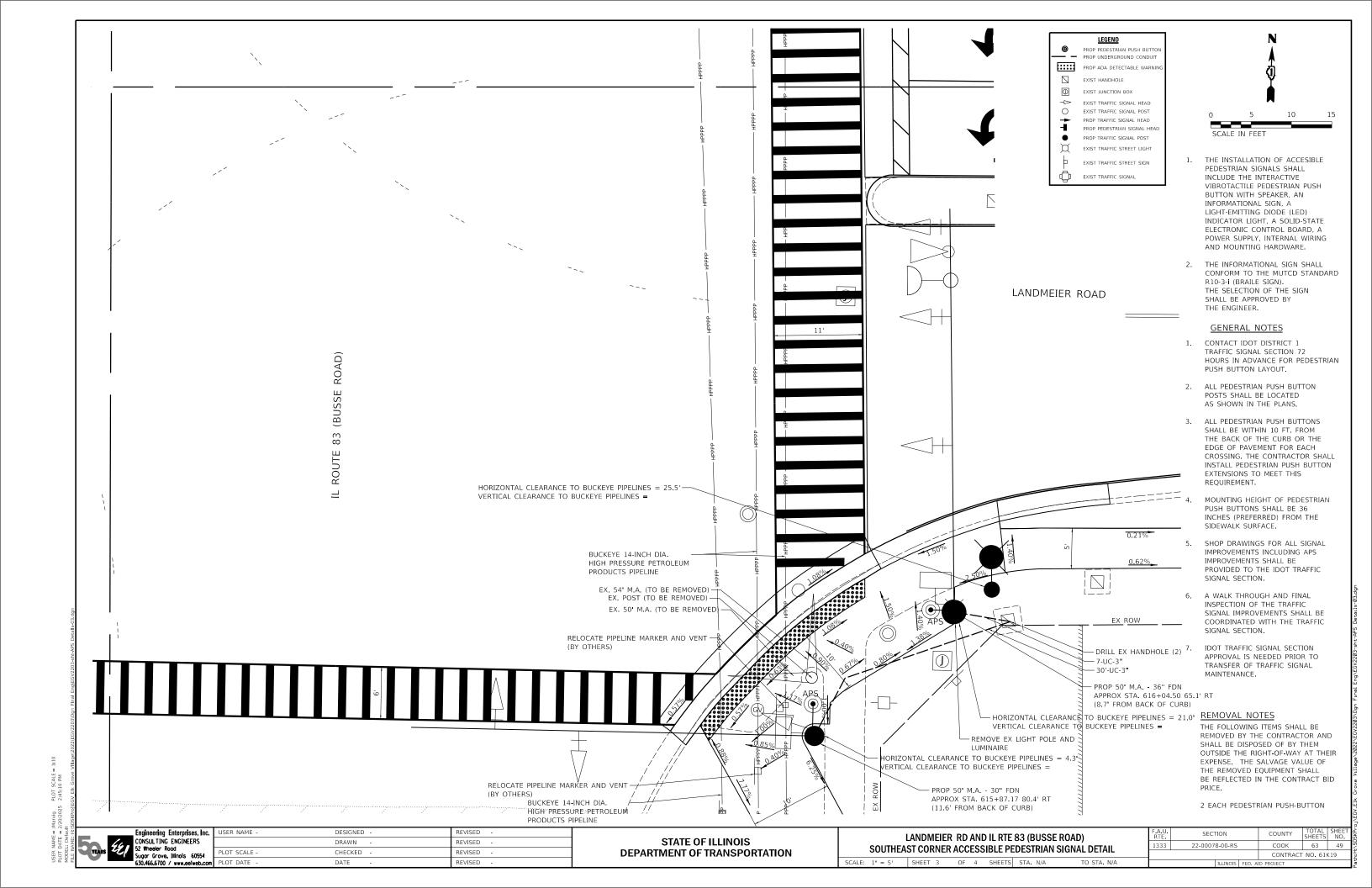
				TS 34 CON 1	
F.A.U. RTE	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
1333	22-00078-00-RS		соок	63	46
			CONTRAC	T NO. 611	<19
	ILLINOIS	FED. A	ID PROJECT		

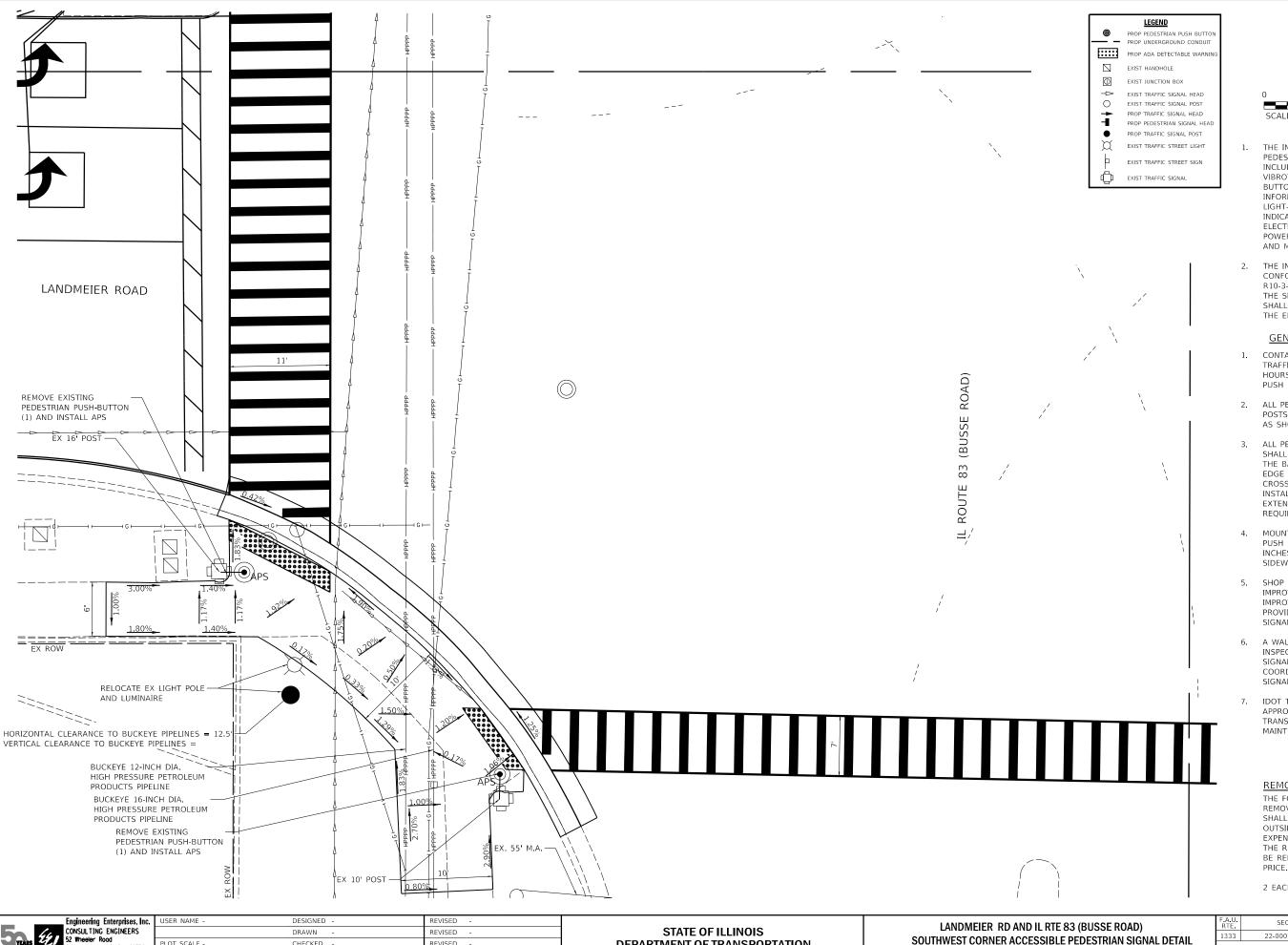
SCHEDULE OF QUANTITIES TEM DESCRIPTION

ITEM DESCRIPTION	UNIT	TOTAL QTY.
RELOCATE SIGN PANEL - TYPE 1	SQ FT	18
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	59
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	37
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,147
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,652
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,163
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	450
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1,116
STEEL MAST ARM ASSEMBLY AND POLE, 50 FT	EACH	2
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	23
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	18
DRILL EXISTING HANDHOLE	EACH	4
DETECTOR LOOP, TYPE I	EACH	470
RELOCATE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	6,674
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C	FOOT	441
PEDESTRIAN SIGNAL POST, 5 FT	EACH	2
ELECTRIC CABLE IN CONDUIT, STREET NAME SIGN, NO. 14 3C, TYPE SOOW	FOOT	403
ACCESSIBLE PEDSESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1









SCALE IN FEET

- THE INSTALLATION OF ACCESIBLE PEDESTRIAN SIGNALS SHALL INCLUDE THE INTERACTIVE VIBROTACTILE PEDESTRIAN PUSH BUTTON WITH SPEAKER, AN INFORMATIONAL SIGN, A LIGHT-EMITTING DIODE (LED) INDICATOR LIGHT, A SOLID-STATE ELECTRONIC CONTROL BOARD, A POWER SUPPLY, INTERNAL WIRING AND MOUNTING HARDWARE.
- THE INFORMATIONAL SIGN SHALL CONFORM TO THE MUTCD STANDARD R10-3-i (BRAILE SIGN). THE SELECTION OF THE SIGN SHALL BE APPROVED BY THE ENGINEER.

GENERAL NOTES

- CONTACT IDOT DISTRICT 1 TRAFFIC SIGNAL SECTION 72 HOURS IN ADVANCE FOR PEDESTRIAN PUSH BUTTON LAYOUT.
- ALL PEDESTRIAN PUSH BUTTON POSTS SHALL BE LOCATED AS SHOWN IN THE PLANS.
- ALL PEDESTRIAN PUSH BUTTONS SHALL BE WITHIN 10 FT. FROM THE BACK OF THE CURB OR THE EDGE OF PAVEMENT FOR EACH CROSSING. THE CONTRACTOR SHALL INSTALL PEDESTRIAN PUSH BUTTON EXTENSIONS TO MEET THIS REQUIREMENT.
- MOUNTING HEIGHT OF PEDESTRIAN PUSH BUTTONS SHALL BE 36 INCHES (PREFERRED) FROM THE SIDEWALK SURFACE.
- SHOP DRAWINGS FOR ALL SIGNAL IMPROVEMENTS INCLUDING APS IMPROVEMENTS SHALL BE PROVIDED TO THE IDOT TRAFFIC SIGNAL SECTION.
 - A WALK THROUGH AND FINAL INSPECTION OF THE TRAFFIC SIGNAL IMPROVEMENTS SHALL BE COORDINATED WITH THE TRAFFIC SIGNAL SECTION.
- IDOT TRAFFIC SIGNAL SECTION APPROVAL IS NEEDED PRIOR TO TRANSFER OF TRAFFIC SIGNAL MAINTENANCE.

REMOVAL NOTES

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID

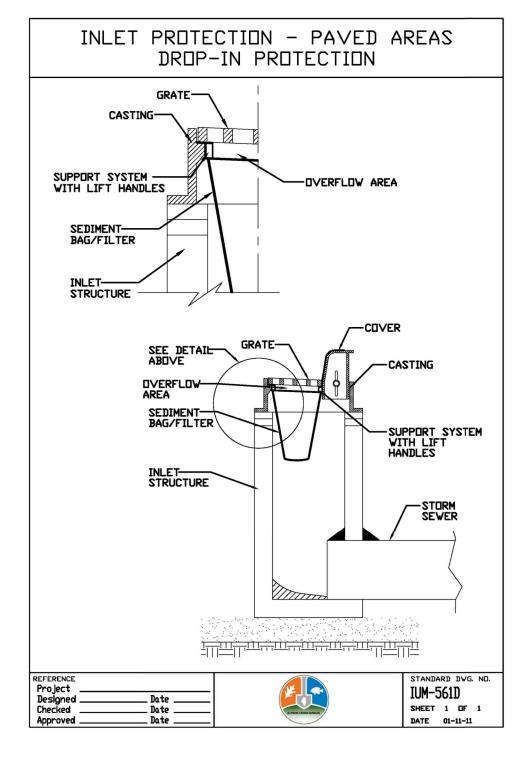
2 EACH PEDESTRIAN PUSH-BUTTON

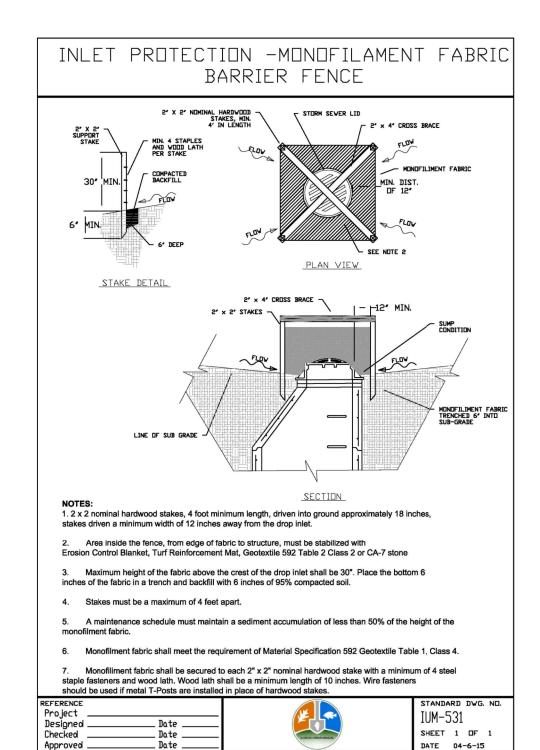
CHECKED REVISED REVISED DATE

DEPARTMENT OF TRANSPORTATION

SOUTHWEST CORNER ACCESSIBLE PEDESTRIAN SIGNAL DETAIL SCALE: 1" = 5' SHEET 4 OF 4 SHEETS STA. N/A

SECTION 22-00078-00-RS COOK 63 50 CONTRACT NO. 61K19





Engineering Enterprises, Inc. ONSULTING ENGINEERS

DESIGNED -REVISED DRAWN REVISED CHECKED REVISED REVISED DATE

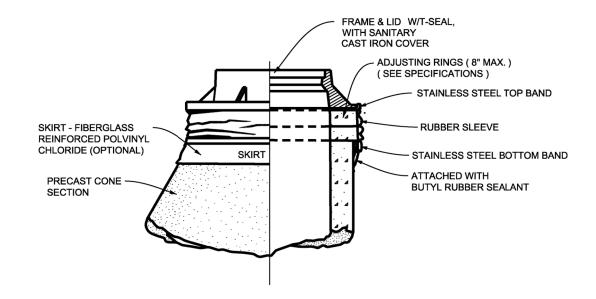
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) **EROSION CONTROL SYSTEM DETAILS** SHEET 1 OF 1 SHEETS STA.

SECTION 22-00078-00-RS COOK 63 51 CONTRACT NO. 61K19

NOTE:

- PROVIDE 1/2" PREFORMED EXPANSION JOINT (FULL DEPTH) AT 50' INTERVALS AND ALONG ALL ABUTTING CONCRETE WORK.
- ALL CURB SIDEWALK RAMPS SHALL MEET THE APPLICABLE IDOT STANDARDS 424001, 424006, 424011, 424016, OR 424021.
- 3. DETECTABLE WARNING PANELS SHALL BE ONE OF THE PRODUCTS SPECIFIED IN THE SPECIAL PROVISIONS AND SHALL BE INSTALLED PER THE APPLICABLE IDOT STANDARDS 424001, 424006, 424011, 424016, OR 424021.
- 4. SEE THE SPECIFICATIONS FOR LANDSCAPE AND PAVEMENT RESTORATION.
- SIDEWALK THROUGH DRIVE ENTRANCES SHALL HAVE CONTRACTION
 JOINTS ON 4-FOOT CENTERS UNLESS OTHERWISE DIRECTED BY ENGINEER.

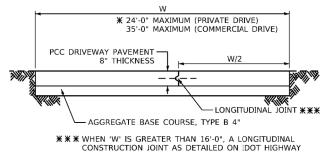
PCC SIDEWALK



CHIMNEY SEAL

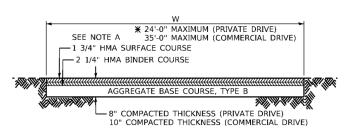
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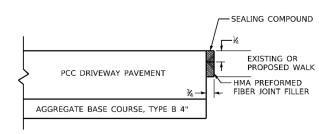


** * WHEN 'W' IS GREATER THAN 16'-0", A LONGITUDINAL CONSTRUCTION JOINT AS DETAILED ON IDOT HIGHWAY STANDARD 420001 SHALL BE USED AS SHOWN.

SECTION A-A PCC DRIVEWAY

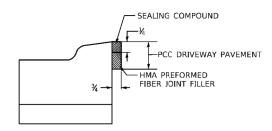


SECTION A-A HMA DRIVEWAY



SECTION B-B

THE EXPANSION JOINT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK OR PER SQUARE YARD FOR PORTLAND CEMENT CONCRETE DRIVEWAY



SECTION C-C

GUIDE FOR DRIVEWAY LENGTH

- MEET EXISTING WALK (USE JOINT FOR PCC PAVEMENT SECTION B-B)
- IF THERE IS NO EXISTING OR PROPOSED WALK, EXTEND TO RIGHT OF WAY LINE
 IF THERE IS NO EXISTING WALK BUT A PROPOSED WALK IS REQUIRED, CONSTRUCT 8
- INCH WALK FOR FULL DRIVEWAY WIDTH (SECTION B-B)
 IF THERE IS A WALK ADJUSTMENT, CONSTRUCT 8" WALK FOR FULL DRIVEWAY WIDTH AND 5" WALK BEYOND.

QUANTITIES SHALL BE COMPUTED ON THE BASIS OF THE DRIVES EXTENDING TO SIDEWALK OR RIGHT OF WAY LINE AS SHOWN ON THE PLANS. HOWEVER, IT SHALL BE THE RESPONSIBILITY OF THE FIELD ENGINEER TO DETERMINE THE ACTUAL LIMITS OF

CHART

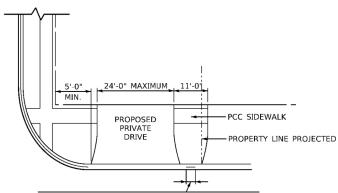
FOR DRIVEWAY WIDTH - W ** (FT)	X (FT)	Y (FT)
12	6.0	4.2
14 - 16	4.9	3.4
17 - 18	4.0	2.9
OVER 18	3.2	2.3

* PRIVATE DRIVES: 12'-0" MINIMUM, 24'-0" MAXIMUM

COMMERCIAL DRIVES: 12'-0" MINIMUM, 24'-0" MAXIMUM FOR 1-WAY OPERATION 24'-0" MINIMUM, 35'-0" MAXIMUM FOR 2-WAY OPERATION

MAXIMUM DRIVEWAY SLOPE

	PROPERTY	URBAN	RURAL	RURAL (PERMIT WORK ONLY)
	COMMERCIAL	6%	6%	10%
ſ	NON-COMMERCIAL	8%	8%	12%



DESIRABLE DISTANCE OF FULL CURB HEIGHT BETWEEN ADJACENT DRIVES SHALL BE 6'-0".
IT IS DESIRABLE THAT THE DRIVE RETURNS TERMINATE WITHIN THE PROPERTY LINE AS PROJECTED.

LIMITATIONS UPON PRIVATE DRIVES

AT STREET INTERSECTIONS MAXIMUM WIDTH & MINIMUM DISTANCE BETWEEN DRIVES

NOTE A

FOR HMA DRIVEWAYS: AS AN ALTERNATE TO THE 1 3/4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-12.5 OR 9.5, N70, AND THE 2 1/4" HOT-MIX ASPHALT CONCRETE BINDER COURSE, IL-19, N70 4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-12.5 OR 9.5 N70 MAY BE USED. IN THE EVENT THE CONTRACTOR ELECTS TO USE THIS ALTERNATE, THE ADDITIONAL 2 1/4" OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-12.5 OR 9.5 N70 SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT BINDER COURSE, IL-19,

GENERAL NOTE:

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.

TO STA. N/A

MUNICIPALITY, IL	
MUNICIPALITY, IL	ľ
NAME TOWNSHIP	ŀ

COUNTY HIGHWAY: XXX FISCAL YEAR: XXXX SECTION: XX-XXXXX-XX-XX

COUNTY OF COOK DEPARTMENT OF TRANSPORTATION AND HIGHWAYS CHECKED:

- EXISTING DRIVEWAY

C&G TRANS

SQ YD PCC SQ YD HMA "W" DRIVEWAY DRIVEWAY

PAVEMENT PAVEMENT

C◀

DEPRESSED CURB

€ PAVEMENT

STATION

DETAIL OF PRIVATE DRIVE

SUMMARY OF DRIVEWAY PAVEMENT

SECTION WITH

CURB & GUTTER -

Α

SIDEWALK ADJUSTMENT WHEN

- EXISTING WALK OR PROPOSED

5 IN SIDEWALK

REQUIRED FOR EXISTING WALK

W (VARIES)

24'-0" MAXIMUM

(PRIVATE DRIVE) FXISTING WALK OR PROPOSED

8 IN SIDEMATK

— EXISTING WALK OR PROPOSED (

* * WHEN CRUSHED STONE SHOULDER

IS USED, "L" IS THE MEASURED AND

CONSTRUCTED LENGTH OF THE DRIVE TO THE EDGE OF PAVEMENT.

> EDGE OF SHOULDER

> > STATION

- EDGE OF PAVEMENT

SECTION WITHOUT CURB & GUTTER

SQ YD PCC SQ YD HMA

PAVEMENT

PAVEMENT

5 IN SIDEWALK

EXPANSION JOINT -

XXX REVISIONS
GENERAL COMPUTED: DRAWN: XXX XXX

PRIVATE AND COMMERCIAL DRIVE DETAIL COOK COUNTY DEPARTMENT OF TRANSPORTATION AND HIGHWAYS STANDARD C-002

C-XXX

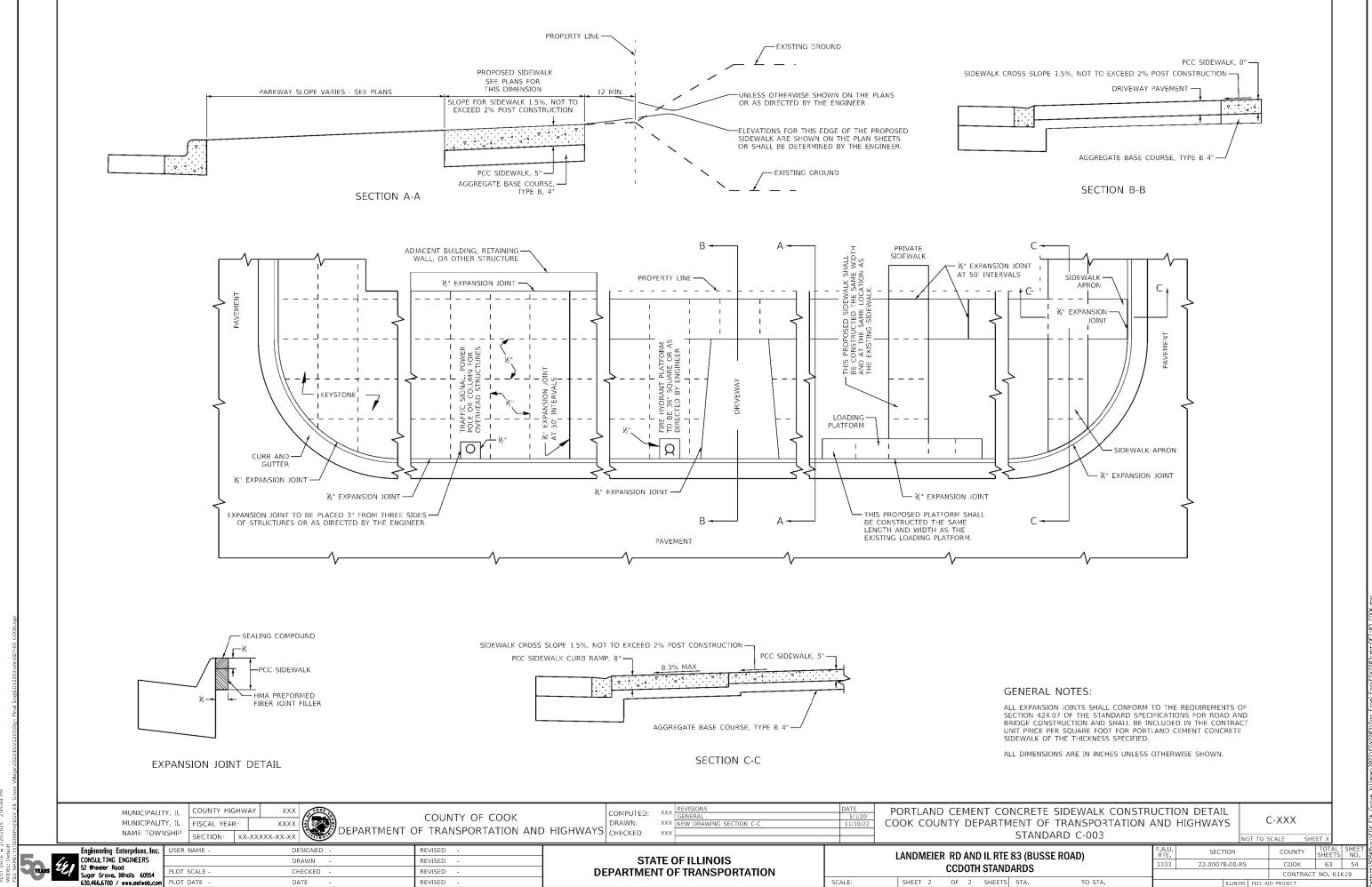
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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** LANDMEIER RD AND IL RTE 83 (BUSSE ROAD) **CCDOTH STANDARDS**

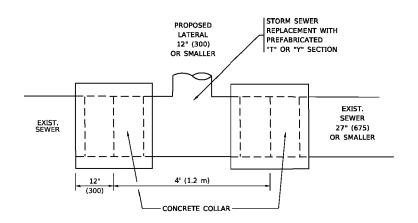
OF 2 SHEETS STA. N/A

SECTION COUNTY COOK 63 53 CONTRACT NO. 61K19

Engineering Enterprises, Inc. ONSULTING ENGINEERS

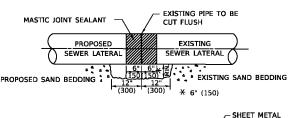


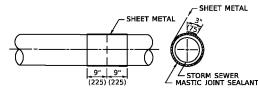
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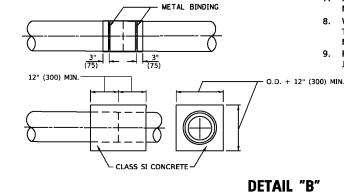


DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER







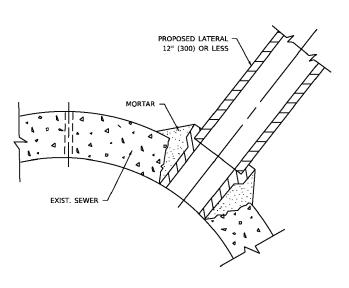
CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- 1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN
- 2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- 3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- 4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERANCE OF THE PIPE PLUS 3" (75) LONG
- 5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- 6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- 7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.

SCALE: NONE

- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- 9. PLACE CLASS SI CONCRETE AROUND THE



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS: A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

- 1. CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
- 2. CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

- 1. TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.
- 2. REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK
- 3. TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.
- 4. CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

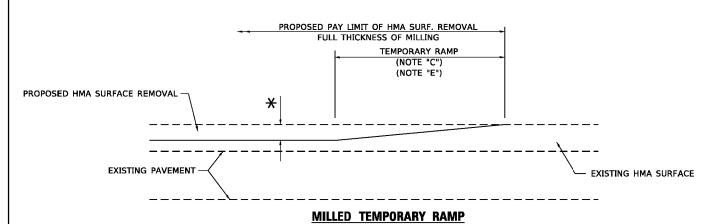


M. DE YONG R. SHAH 09-09-94 Engineering Enterprises, Inc. USER NAME = Lawrence.DeManche DESIGNED -REVISED -DRAWN REVISED R. SHAH 10-25-94 CHECKED REVISED R. SHAH 06-12-96 PLOT SCALE = 100.0000 ' / in. PLOT DATE = 11/18/2022 DATE 07-25-90 REVISED K. SMITH 11-18-22

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

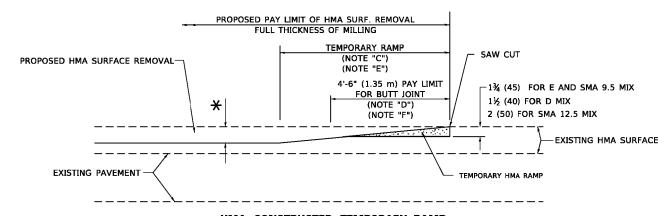
DETAIL OF STORM SEWER **CONNECTION TO EXISTING SEWER** SHEET 1 OF 1 SHEETS STA. TO STA.

COUNTY 63 55 BD500-01 (BD-07) CONTRACT NO.61K19



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

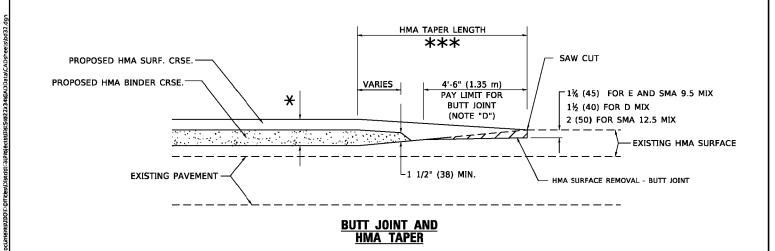


HMA CONSTRUCTED TEMPORARY RAMP

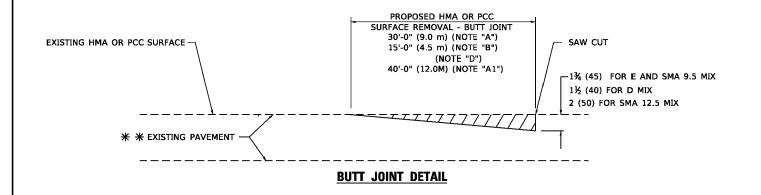
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

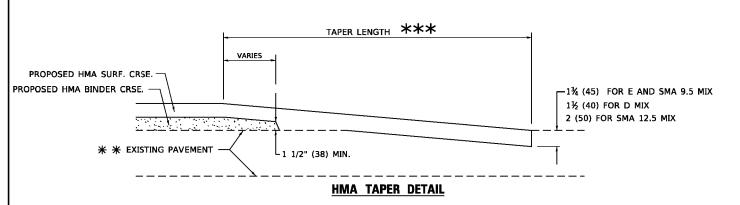
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT"
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

SCALE: NONE

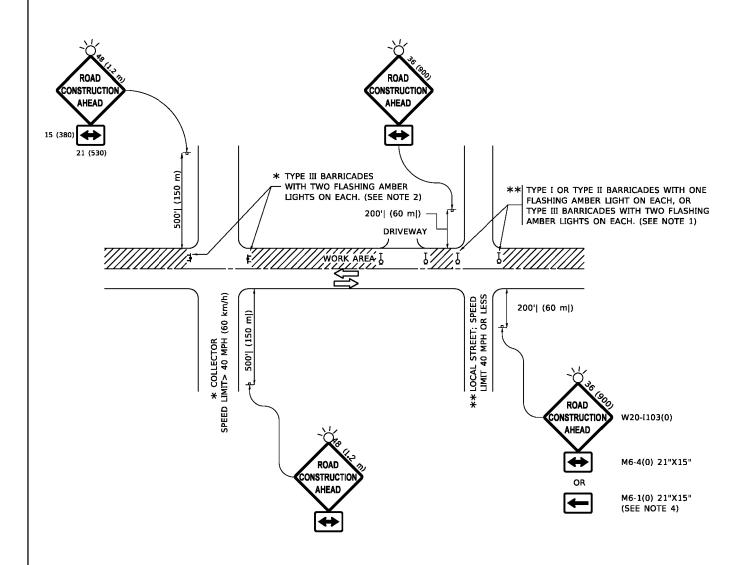
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED -	A. ABBAS 03-21-97
	DRAWN -	REVISED -	M. GOMEZ 04-06-0
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED -	K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS				
		1333	22-00078-00-RS	соок	63	Ξ			
	IIIIA	IAI LII D	LIAILU			BD400-05 BD-32	CONTRACT	NO. 61K	1
	SHEET 1 OF	1 SHEETS	STA	TO STA		THE PLANE SEED A	ID BROJECT		_



NOTES:

- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE
- THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION
 OF THE CLOSED PORTION.
- 3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710)
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE 4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

SCALE: NONE

- 5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- 6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE
- 7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown



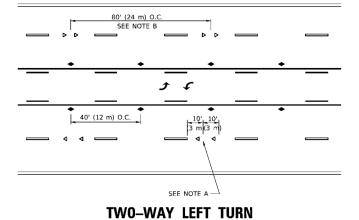
JSER NAME = Lawrence.DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-0
	DRAWN -	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 5/3/2024	DATE - 06-89	REVISED - D. SENDERAK 05-03-24

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEE	
1333	22-00078-00-RS		соок	63	57
	TC-10	CONTRACT	NO. 61	K19	
	ILLINOIS	FED. A	D PROJECT		

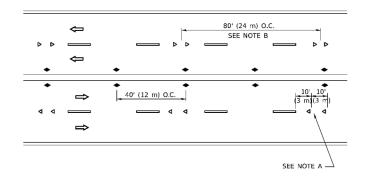
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

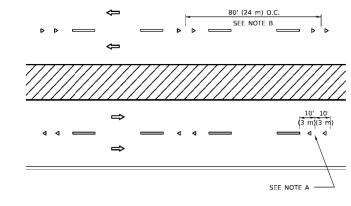
3 @ 40' (12 m) O.C. \Rightarrow LANE REDUCTION TRANSITION

SEE FIGURE 3B-14 MUTCD



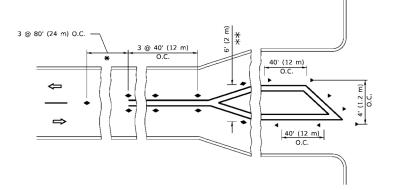
TWO-LANE/TWO-WAY

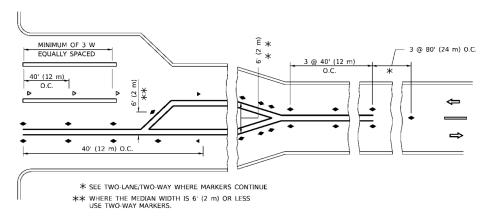




MULTI-LANE/UNDIVIDED







TURN LANES

GENERAL NOTES

- 1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- 4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY FXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY

All dimensions are in inches (millimeters) unless otherwise shown.

Engineering Enterprises, Inc. CONSULTING ENGINEERS

DESIGNED REVISED - T. RAMMACHER 03-12-99 USER NAME = footemj REVISED -T. RAMMACHER 01-06-00 DRAWN CHECKED - C. JUCIUS 09-09-09 PLOT DATE = 3/4/2019 C. JUCIUS 07-01-13 DATE REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) SHEET 1 OF 1 SHEETS STA.

SECTION COUNTY 22-00078-00-RS COOK 63 58 TC-11 CONTRACT NO. 61K19

SYMBOLS

■ ONE-WAY AMBER MARKER

■ ONE-WAY CRYSTAL MARKER (W/O)

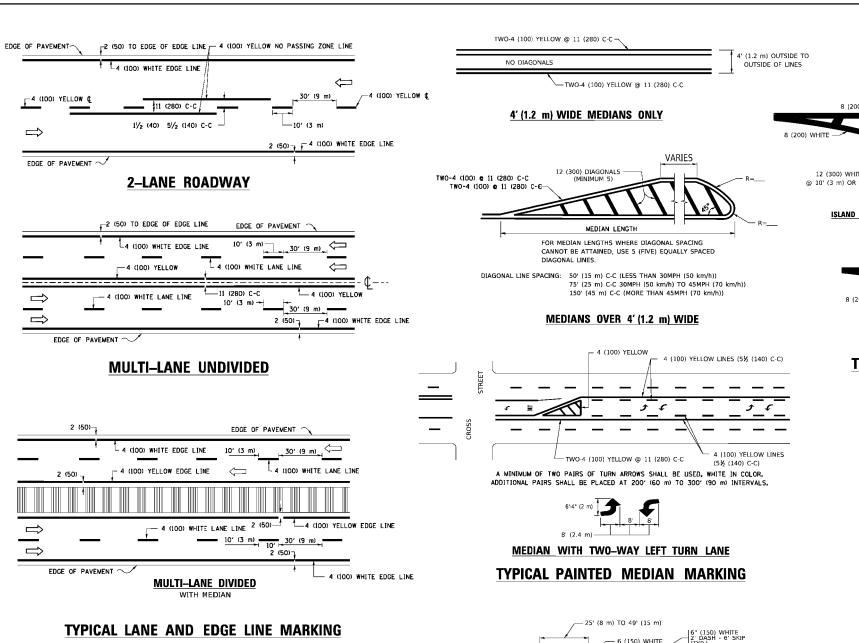
TWO-WAY AMBER MARKER

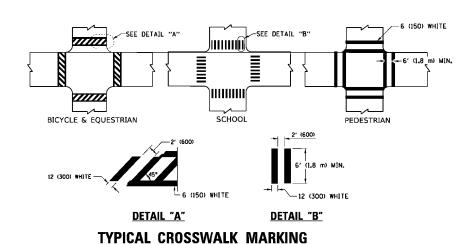
YELLOW STRIPE

WHITE STRIPE

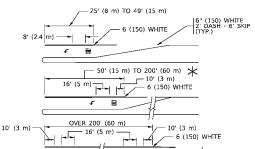
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN

SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE





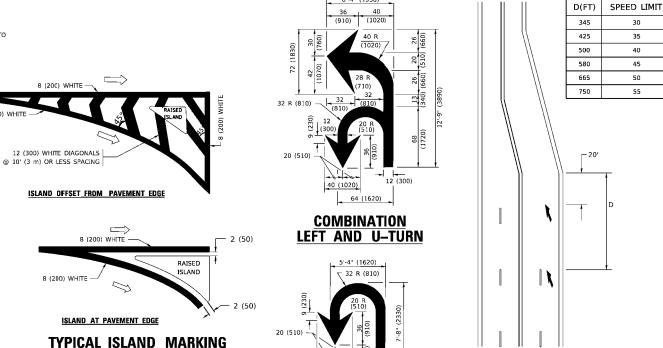
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



 \star Turn lanes in excess of 400' (120 m) in length may have an additional set of arrow - "only" installed midway between the other two sets of

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

LANE REDUCTION TRANSITION

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½, (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT: OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m PEACH "X"=54.0 SQ. FT. (5.0 m PEACH
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RICHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

U-TURN

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

SCALE: NONE

All dimensions are in inches (millimeters) unless otherwise shown.



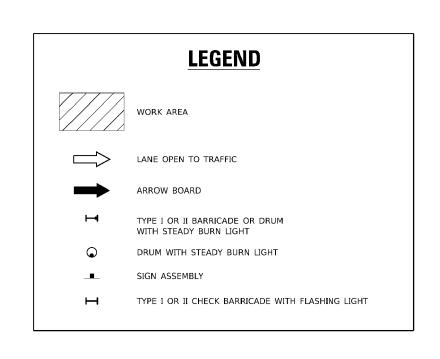
EVERS DESIGNED -DRAWN CHECKED REVISED -PLOT SCALE = 50.0000 ' / in. PLOT DATE = 3/4/2019 DATE 03-19-90 REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION COUNTY DISTRICT ONE 63 59 22-00078-00-RS СООК TYPICAL PAVEMENT MARKINGS CONTRACT NO. 61K19 TC-13 OF 2 SHEETS STA. SHEET 1

TURN BAY ENTRANCE AT START

OF LANE CLOSURE TAPER



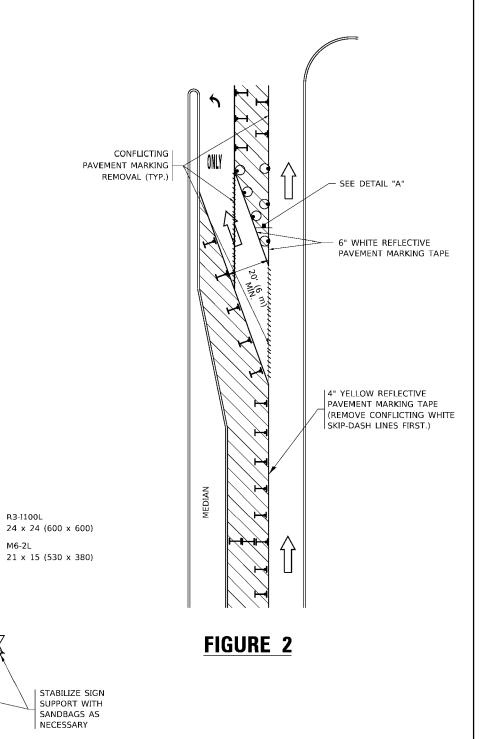
KEEP RIGHT 24"X30" 4" YELLOW REFLECTIVE PAVEMENT MARKING TAPE (REMOVE CONFLICTING WHITE SKIP-DASH LINES FIRST.) - ARROW BOARD SEE DETAIL "A"

FIGURE 1

NOTES:

- 1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 - B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- 2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- 3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- 4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- 5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- 7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
- 8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE



DETAIL A

TURN

LANE

K

M6-2L

All dimensions are in inches (millimeters) unless otherwise shown

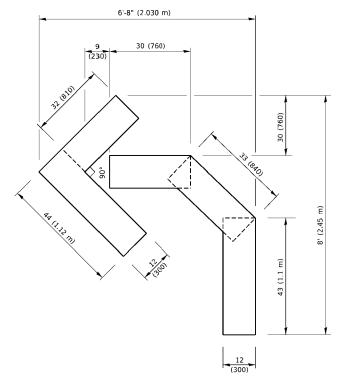
Engineering Enterprises, Inc. | USER NAME = footemj ONSULTING ENGINEERS

DESIGNED -T. RAMMACHER 09-08-94 REVISED - R. BORO 09-14-09 - A. HOUSEH 11-07-95 REVISED - A. SCHUETZE 07-01-13 A. HOUSEH 10-12-96 REVISED - A. SCHUETZE 09-15-16 -T. RAMMACHER 01-06-00 REVISED PLOT DATE = 3/4/2019

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

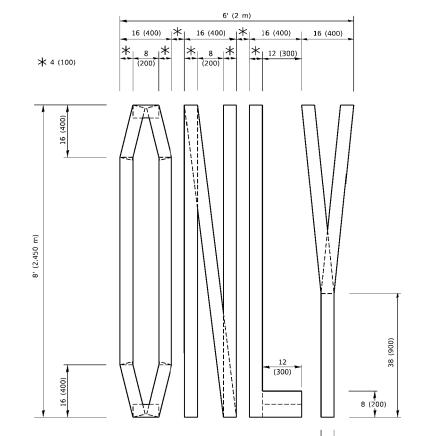
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SCALE: NONE SHEET 1 OF 1 SHEETS STA.

SECTION 22-00078-00-RS COOK 63 60 TC-14 CONTRACT NO. 61K19



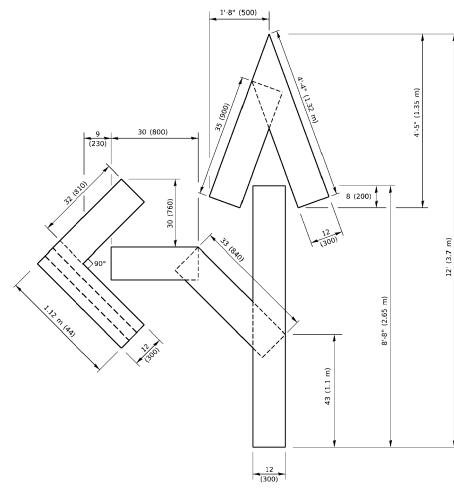
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.41 sq. m)



4 (100) LINE = 64.1 ft. (19.5 m) 21.4 sq. ft. (1.99 sq. m)

QUANTITY

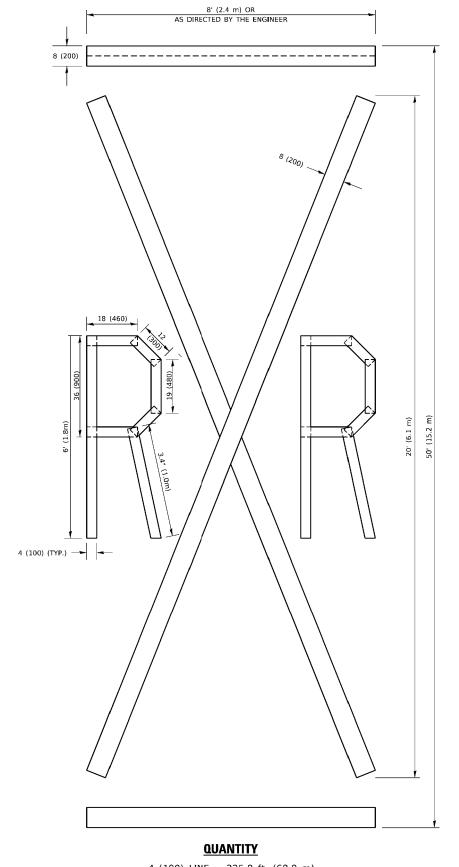


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m) 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



4 (100) LINE = 225.9 ft. (68.9 m) 75.3 sq. ft. (6.99 sq. m)

> All dimensions are in inches (millimeters) unless otherwise shown.



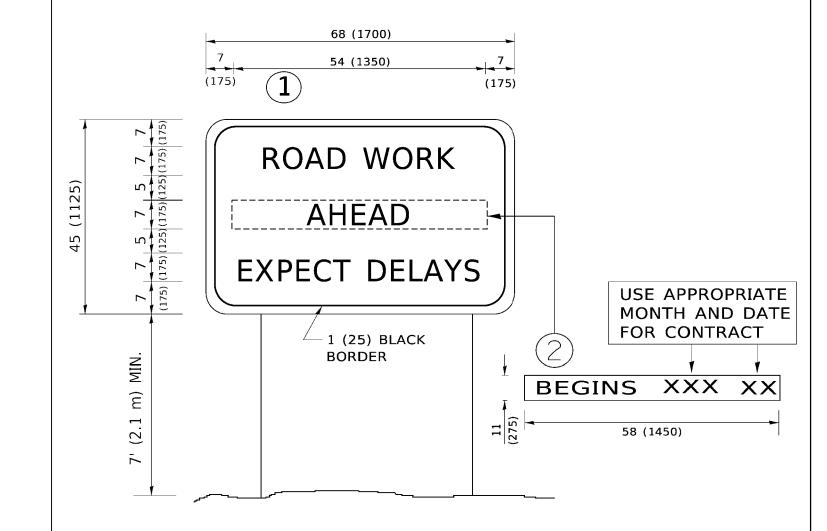
JSER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50.0068 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

4 (100)

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SHORT	TERM	PAV	EMENT	MARKIN	G LETTERS	AND SYMBOLS	;
SCALE: NONE	SHEE	T 1	OF :	L SHEETS	STA.	TO STA.	Т

A. TE.	SECT	TON		COUNTY	TOTAL SHEETS	SHEET NO.
333	22-0007	8-00-RS		соок	63	61
TC-16			CONTRACT I	VO. 61k	(19	
		THIMOTE	D PROJECT			



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN(1)WITH INSTALLED PANEL(2)ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

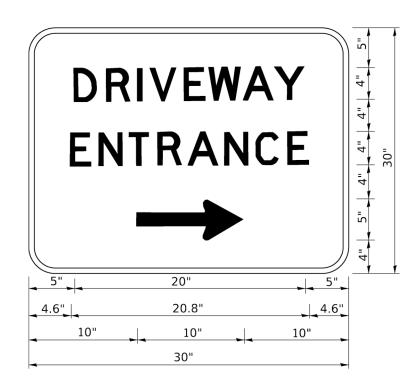
SCALE: NONE

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



	USER NAME = footemj	DESIGNED -	REVISED	- R. MIRS 09-15-97
		DRAWN -	REVISED	- R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED	-T. RAMMACHER 02-02-99
n	PLOT DATE = 3/4/2019	DATE -	REVISED	- C. JUCIUS 01-31-07

	ARTERIAL ROAD							
		INF	OR	MATION	SIGN			
SHEET	1	OF	1	SHEETS	STA			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
- 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
- 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

	Engineering Enterprises, Inc.	USER NAME = leysa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
50 66	CONSULTING ENGINEERS 52 Wheeler Road		DRAWN -	REVISED -
YEARS 4	Sugar Grove, Illinois 60554	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -
	630,466,6700 / www.eeiweb.com		DATE -	REVISED -

STATE OF ILLINOIS
311112 31 12211313
DEPARTMENT OF TRANSPORTATION