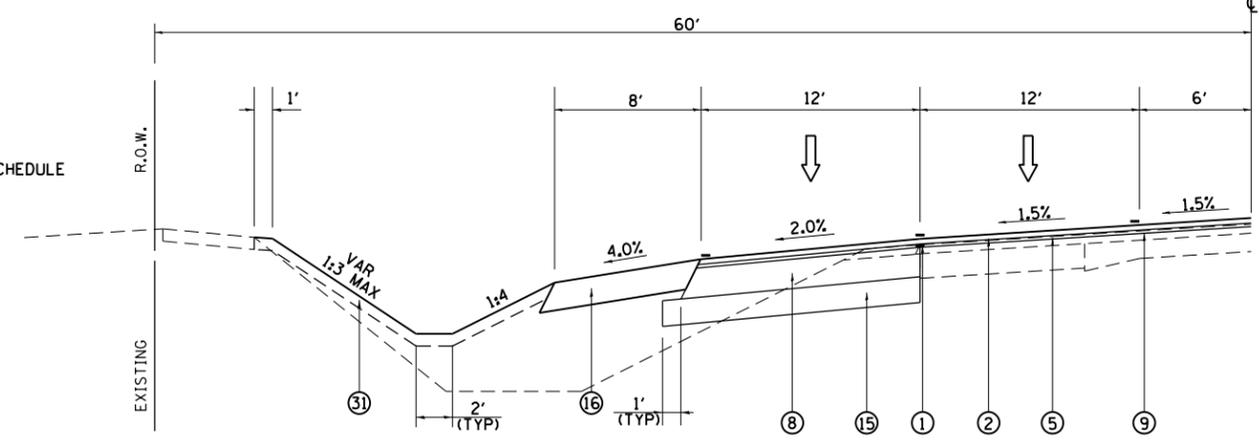


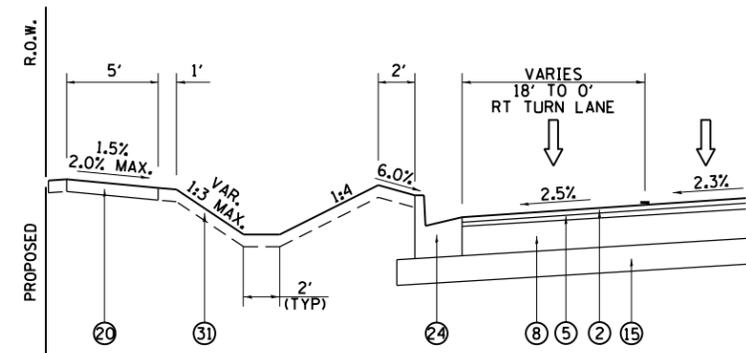
- ① PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ② PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- ③ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1" & VARIABLE
- ④ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1/4"
- ⑤ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1/2" & VARIABLE
- ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1/2"
- ⑦ PROPOSED HOT-MIX ASPHALT BASE COURSE, 7"
- ⑧ PROPOSED HOT-MIX ASPHALT BASE COURSE 8 1/2"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3/4" OR 2" & VARIABLE. SEE MILLING SCHEDULE
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1/4" & VARIABLE
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2" & VARIABLE
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2" TO 4 1/4" & VARIABLE
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2" & VARIABLE
- ⑮ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE A, 8"
- ⑯ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
- ⑰ PROPOSED AGGREGATE SHOULDER, TYPE B, 6"
- ⑱ PROPOSED CONCRETE MEDIAN SURFACE, 4"
- ⑲ PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- ⑳ PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 4"
- ㉑ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06
- ㉒ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ㉓ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- ㉔ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ㉕ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- ㉖ PROPOSED PAVEMENT REMOVAL
- ㉗ PROPOSED SHOULDER REMOVAL
- ㉘ PROPOSED CONCRETE MEDIAN REMOVAL
- ㉙ PROPOSED GUTTER/CURB AND GUTTER REMOVAL
- ㉚ PROPOSED ROCKFILL (SPECIAL)
- ㉛ PROPOSED TOPSOIL FURNISH AND PLACE, 4"

PAVING NOTE:
 AT LOCATIONS WHERE RESURFACING THICKNESS EXCEEDS
 5 3/4" TO PROVIDE PROFILE GRADE ADJUSTMENT, HOT-MIX
 ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE INCLUDED
 BELOW THE 1/2" POLYMERIZED LEVELING BINDER.

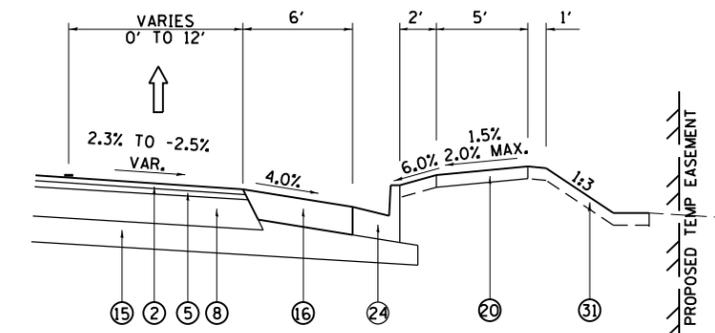
PROFILE GRADE LINE
 B EXISTING WABASH AVENUE
 C PROPOSED WABASH AVENUE



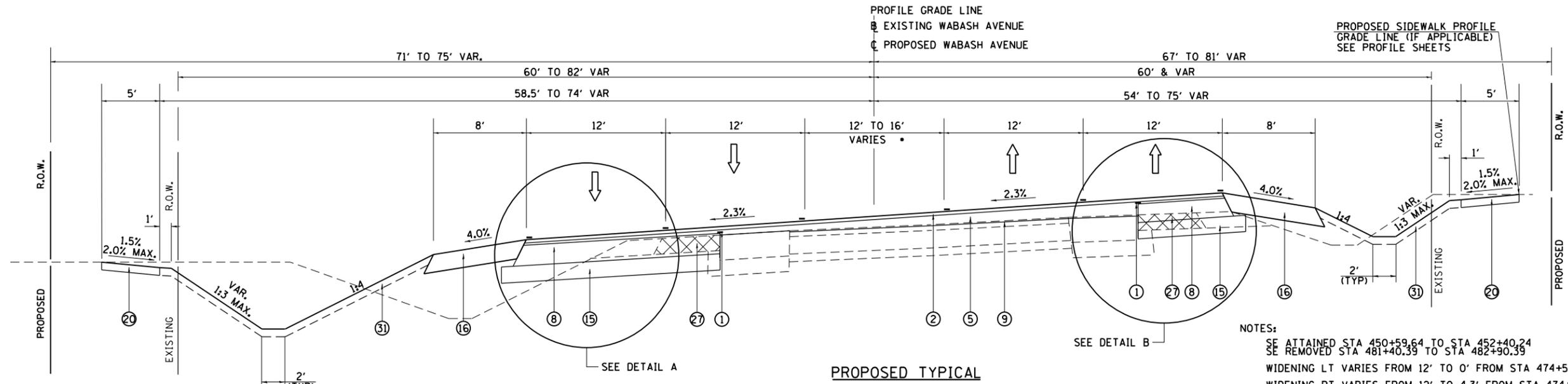
PROPOSED TYPICAL
 STA. 444+72.92 TO STA. 446+42.64 LT
 STA. 446+42.64 TO STA. 449+42.64 LT



DETAIL A
 STA 453+41.48 TO STA 460+00.14
 STA 465+19.28 TO STA 471+25.62



DETAIL B
 STA 477+96.19 TO STA 479+96.04
 STA 479+96.04 TO STA 482+36.19 **



PROPOSED TYPICAL
 STA. 453+41.48 TO STA. 482+90.62 (SEE NOTES)

NOTES:
 SE ATTAINED STA 450+59.64 TO STA 452+40.24
 SE REMOVED STA 481+40.39 TO STA 482+90.39
 WIDENING LT VARIES FROM 12' TO 0' FROM STA 474+50 TO STA 478+45.32.
 WIDENING RT VARIES FROM 12' TO 4.3' FROM STA 474+50 TO STA 477+96.19.
 • WIDTH TRANSITION FROM 12' TO 16' FROM STA 478+45.32 TO STA 480+70.39.
 ** TRANSITION FROM 2.3% SE TO -2.5%

FILE NAME =	USER NAME = sparksgw	DESIGNED - JAW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WABASH AVENUE PROPOSED TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwwork\pwwork\sparksgw\10373645\10672890-sh-typical103.dgn	DRAWN - JCW	REVISED -	8012					21(W-3, TS-1, RS-7)	SANGAMON	679	53	
PLOT SCALE = 10.0000' / in.	CHECKED - AWM	REVISED -	CONTRACT NO. 72890									
PLOT DATE = Feb-04-2014 01:29:29PM	DATE - 12/04/2013	REVISED -	ILLINOIS FED. AID PROJECT									
				SCALE: N/A	SHEET NO.	OF SHEETS	STA.	TO STA.				