

CONSTRUCTION PLANS

FOR

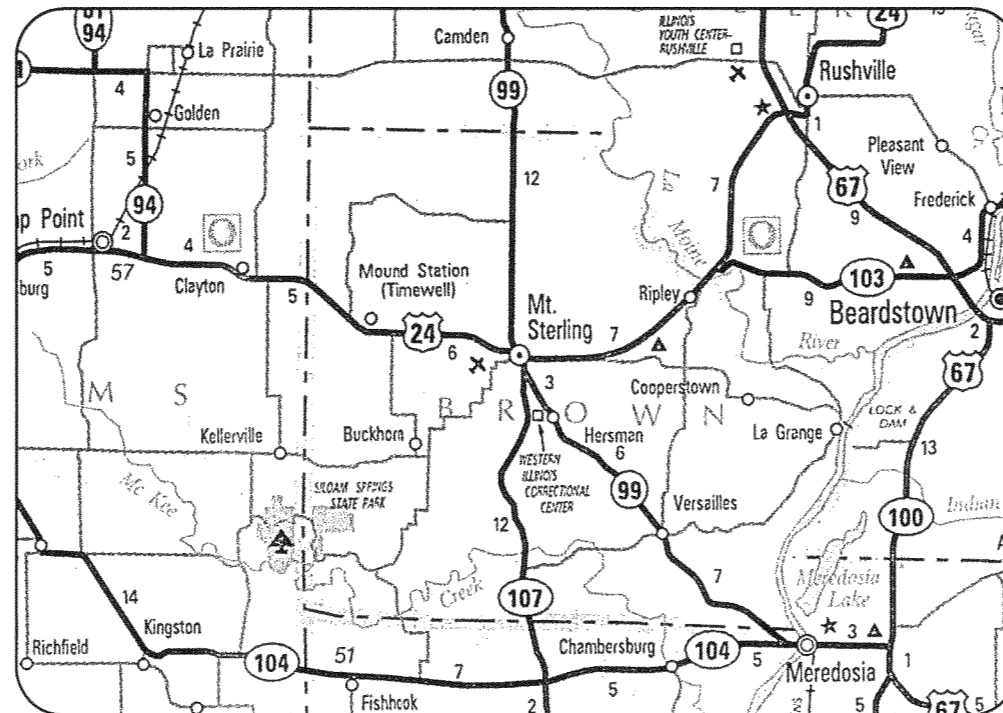
ROBERT F. TRACY MUNICIPAL AIRPORT

MT. STERLING, BROWN COUNTY, ILLINOIS

PFC REHABILITATE RUNWAY 18-36

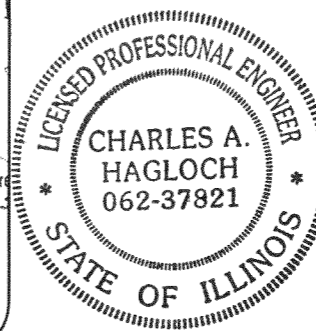
SCOPE OF WORK

THIS PROJECT CONSISTS OF CRACK CLEANING AND SEALING, APPLICATION OF A 0.10' POROUS FRICTION COURSE, PAVEMENT REPAIRS, SHOULDER ADJUSTMENT, SEEDING, MULCHING AND PAVEMENT MARKING OF RUNWAY 18-36 AND CONNECTING TAXIWAY.



LOCATION

ILL. PROJ.: 163-3957
A.I.P. PROJ.: 3-17-0147-B10
LATITUDE: 39° 59' 15"
LONGITUDE: 90° 48' 15"
ELEVATION: 732.0' M.S.L.
DATE: JANUARY 29, 2010



REVISED 03/16/2010

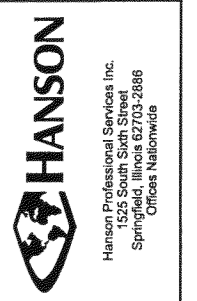
HANSON
Hanson Professional Services Inc.
Submitted by Charles A. Hagloch ENG'R
Date Submitted MARCH 16, 2010
Lic. Exp. Date NOVEMBER 30, 2011

CITY OF MT. STERLING
Approved Billie Brice MAYOR
Date 2-1-10
Approved _____ CHAIRMAN AIRPORT ADVISORY BOARD
Date _____

DATE	REVISION	BY
3/16/10	REVISED PER IDA REVIEW	CAH

ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS
IL PROJ.: 163-3957
A.I.P. PROJ.: 3-17-0147-B10

FILENAME	SCALE	DATE	LAYOUT	DRAWN	REVIEWED
R-001CVR.DWG	NOT TO SCALE	10/28/09	JEO	JEO	JEO



PROPOSED P.F.C.
ON RUNWAY 18-36
COVER SHEET

MS004

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	4,783	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	13,338	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	267	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	141	
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	56,110	
AR603510	BITUMINOUS TACK COAT	GAL.	14,028	
AR625520	PAVEMENT MARKING-WATERBORNE	S.F.	34,682	
AR625525	PAVEMENT MARKING-BLACK BORDER	S.F.	1,225	
AR751943	ADJUST MANHOLE	EA.	11	
AR751949	ADJUST INSPECTION HOLE	EA.	6	
AR901510	SEEDING	AC.	0.99	
AR908520	EXCELSIOR BLANKET	S.Y.	4,783	

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED CONSTRUCTION PLAN STA. 19+95 TO STA. 28+00
5	PROPOSED CONSTRUCTION PLAN STA. 28+00 TO STA. 44+00
6	PROPOSED CONSTRUCTION PLAN STA. 44+00 TO STA. 60+00
7	PROPOSED CONSTRUCTION PLAN STA. 60+00 TO STA. 66+00
8	PROPOSED CONSTRUCTION PLAN STA. 66+00 TO STA. 79+00
9	PROPOSED PAVEMENT PREPARATION PLAN STA. 19+95 TO STA. 28+00
10	PROPOSED PAVEMENT PREPARATION PLAN STA. 28+00 TO STA. 44+00
11	PROPOSED PAVEMENT PREPARATION PLAN STA. 44+00 TO STA. 60+00
12	PROPOSED PAVEMENT PREPARATION PLAN STA. 60+00 TO STA. 66+00
13	PROPOSED PAVEMENT PREPARATION PLAN STA. 66+00 TO STA. 79+00
14	PROPOSED MARKING PLAN STA. 19+95 TO STA. 28+00
15	PROPOSED MARKING PLAN STA. 28+00 TO STA. 44+00
16	PROPOSED MARKING PLAN STA. 44+00 TO STA. 60+00
17	PROPOSED MARKING PLAN STA. 60+00 TO STA. 66+00
18	PROPOSED MARKING PLAN STA. 66+00 TO STA. 79+00

DATE	REVISION	BY
3/16/10	REVISED PER IDA	CAH

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
Filename R-002FLP.DWG	DRAWN	JEO	10/28/09
Scale NOT TO SCALE	REVIEWED	CAH	01/28/10
Date 10/28/09			



**PROPOSED P.F.C.
 ON RUNWAY 18-36**

SUMMARY OF QUANTITIES
 AND
 INDEX TO SHEETS

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UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A DUMP TRUCK.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREA WILL BE 200' X 200'. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTE AND PARKING AREA AS NEEDED TO RESTORE IT TO ITS' ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

AT THE START OF THE CONSTRUCTION PROJECT THAT PORTION OF THE TOWNSHIP ROAD THAT WILL BE USED AS THE HAUL ROUTE WILL BE WALKED BY THE CONTRACTOR, TOWNSHIP ROAD COMMISSIONER AND RESIDENT ENGINEER TO NOTE ANY ROAD DEFICIENCIES. AT THE CONCLUSION OF THE PROJECT THE ROAD WILL BE WALKED AGAIN AND ANY NEW DEFICIENCIES WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

CONTRACTOR RESPONSIBILITIES







THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.

IF THE RUNWAY IS NOT CLOSED, THEN NO TRENCHES OR HOLES WILL REMAIN OPEN OVERNIGHT.

LEGEND

-  EXISTING IMPROVEMENTS
-  PROPOSED IMPROVEMENTS
-  EXISTING BUILDINGS
-  PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
-  PROPOSED BENCHMARK
-  PROPOSED BARRICADES OR TRAFFIC CONES

SCOPE OF WORK

THIS PROJECT CONSISTS OF CRACK CLEANING AND SEALING, APPLICATION OF A 0.10' POROUS FRICTION COURSE, PAVEMENT REPAIRS, SHOULDER ADJUSTMENT, SEEDING, MULCHING AND PAVEMENT MARKING OF RUNWAY 18-36 AND CONNECTING TAXIWAY.

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

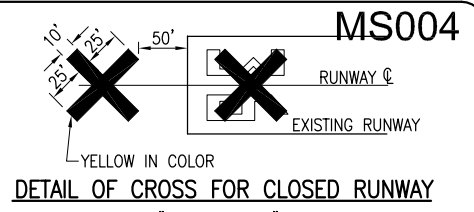
COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

EROSION CONTROL

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

BENCHMARK DATA

NO.	DESCRIPTION	ELEV.
1	IDOT DISK "MT. STERLING AZ 1994"	720.53
2	CB-1 - CHSLD SQUARE ON CONCRETE HEADWALL SOUTHWESTERLY SIDE OF ROAD TO AIRPORT OFFICE BLDG @ 90° SWEEPING CURVE	723.29
3	CB-2 - CHSLD SQUARE NORTH WEST CORNER CONCRETE SLAB FOR GAS PUMP @ SOUTH END OF APRON	729.30
4	CB-3 - CHSLD SQUARE CENTER OF CONCRETE HEADWALL SOUTH SIDE OF TAXIWAY TO RUNWAY 18/36 ±STA. 64+25, 250.9' RT.	726.20
5	CB-4 - CHSLD SQUARE NORTH RIM EDGE DRAIN CLEAN OUT @ NORTHWEST CORNER RUNWAY 18/36 ±STA. 79+00, 40.5' LT.	732.92
6	CB-5 - CHSLD SQUARE CENTER OF CONCRETE HEADWALL, 1ST HEADWALL SOUTH OF TAXIWAY ON EAST SIDE RUNWAY 18/36 ±STA. 58+50, 75' RT.	726.81
7	CB-6 - CHSLD SQUARE CENTER OF CONCRETE HEADWALL, 2ND HEADWALL SOUTH OF TAXIWAY ON EAST SIDE OF RUNWAY 18/36 ±STA. 46+50, 75.2' RT.	722.30
8	CB-7 - CHSLD SQUARE CENTER OF CONCRETE HEADWALL, 3RD HEADWALL SOUTH OF TAXIWAY ON THE WEST SIDE RUNWAY 18/36 ±STA 40+25, 74.9' LT.	721.65



NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OFF THE END OF THE RUNWAY AS SHOWN AND SECURED IN A MANNER APPROVED BY THE MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

J.U.L.I.E. INFORMATION

COUNTY _____ BROWN
 CITY _____ MT. STERLING
 TOWNSHIP _____ LEE
 SECTION NO. _____ 12 & 13
 ADDRESS _____ MT. STERLING MUNICIPAL AIRPORT
 145 W. MAIN STREET
 MT. STERLING, ILLINOIS 62353-1296

CRITICAL POINT DATA

LATITUDE: 39° 59' 18.49"
 LONGITUDE: 90° 48' 20.42"
 ELEVATION: 724.00 M.S.L.

PROPOSED SAFETY PLAN

GENERAL - THE MT. STERLING MUNICIPAL AIRPORT IS COMPRISED OF ONE RUNWAY. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING THE RUNWAY. ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE THE RUNWAY WILL BE CLOSED. PRIOR TO APPLYING THE BITUMINOUS TACK COAT, THE RUNWAY WILL BE CLOSED DURING THE DAY AND REOPENED AT NIGHT AFTER THE CONTRACTOR HAS SMOOTH GRADE ALL AREAS WITH IN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER. AFTER APPLYING THE BITUMINOUS TACK COAT THE RUNWAY WILL BE CLOSED FOR THE DURATION OF THE PROJECT. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

WHEN THE RUNWAY IS CLOSED THE CONTRACTOR WILL DE-ACTIVATE THE RUNWAY LIGHTS, TAXIWAY LIGHTS AND NAVAIDS. WHEN THE RUNWAY IS RE-OPENED THESE CIRCUITS WILL BE RE-ACTIVATED.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3)' FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHZ.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE MT. STERLING MUNICIPAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

150-ENGINEER'S FIELD OFFICE NOTES

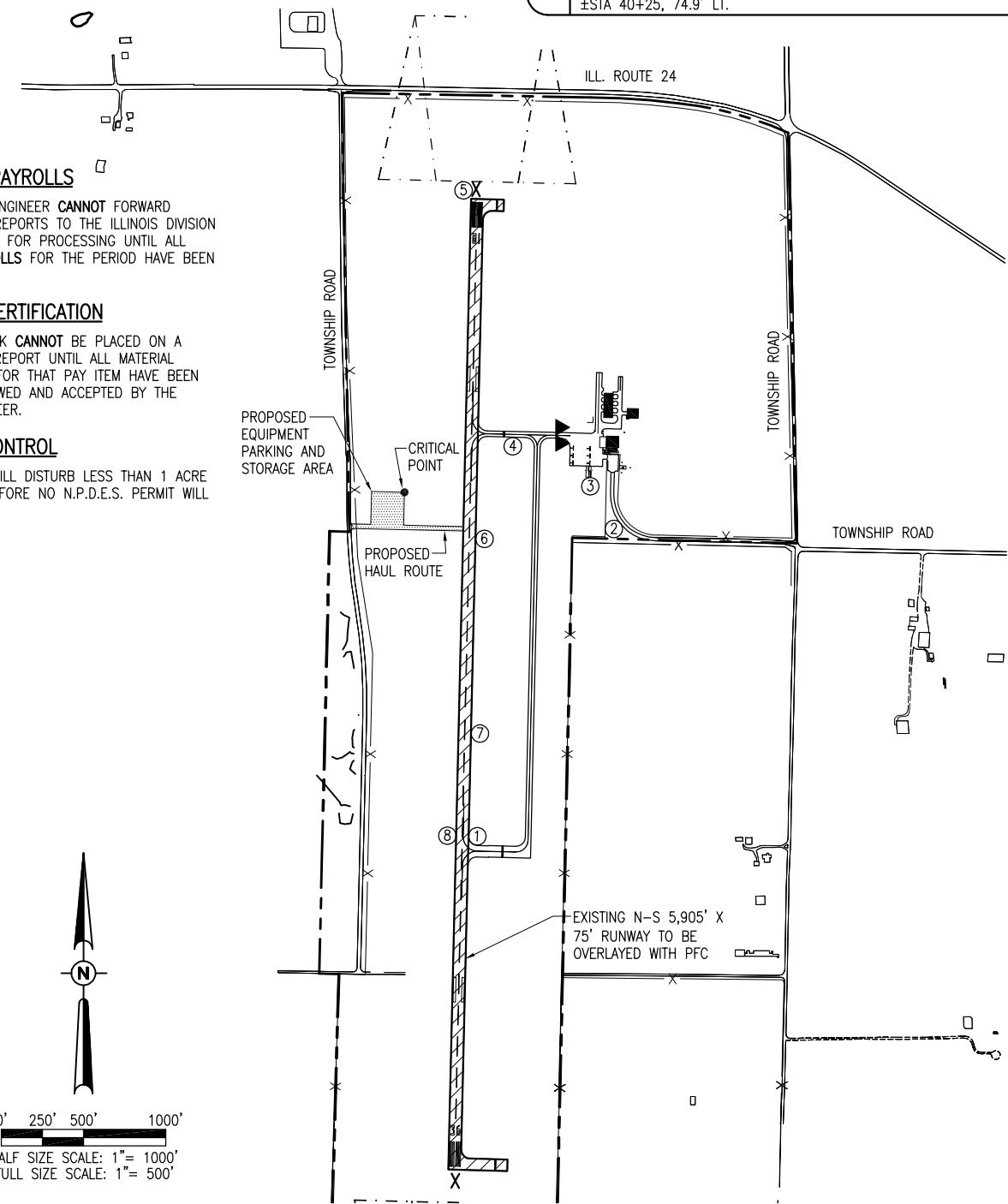
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 49 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.


THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A CELL PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

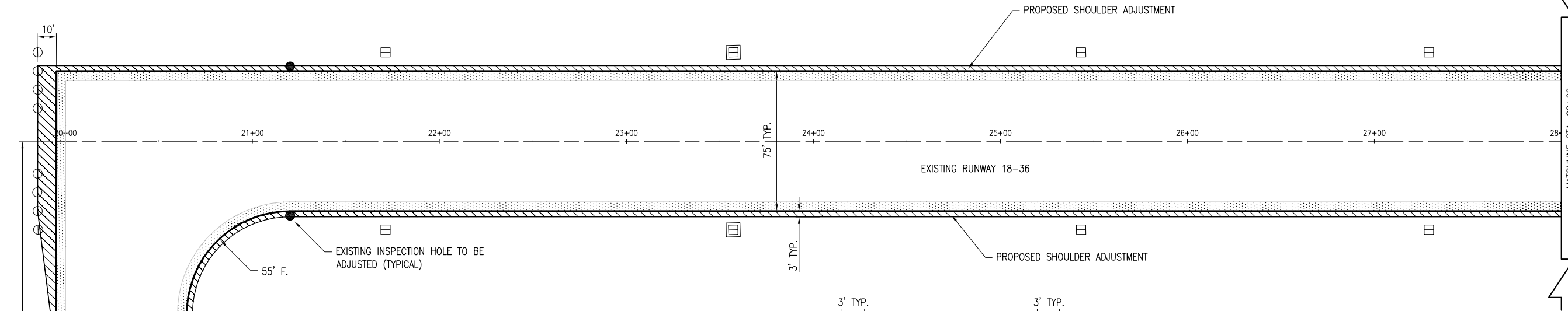
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE _____ 1 L.S.



BY: CAH REVISION PER: DA REVIEW DATE: 3/16/10	PROJECT: 3-17-0147-B10 ILL. PROJ.: 163-3957	ROBERT F. TRACY MUNICIPAL AIRPORT MT. STERLING, ILLINOIS
Hanson Project No. 09A0127D_0800 Filename: R-003SFY.DWG Scale: 1" = 500' Date: 10/30/09		
LAYOUT	JED	10/30/09
DRAWN	BAK	10/30/09
REVIEWED	CAH	01/28/10
 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide		
PROPOSED P.F.C. ON RUNWAY 18-36	PROPOSED SAFETY PLAN	
3		
3 of 18 sheets		

MS004

NOTE: THE SHOULDER ADJUSTMENT WILL BE FLUSH WITH THE PROPOSED POROUS FRICTION COURSE ON EACH END OF THE RUNWAY AND THE END OF THE TURNAROUND.



THRESHOLD LIGHTS NOTE

SIX OF THE EIGHT EXISTING THRESHOLD LIGHTS ON RUNWAY END 18 AND 36 WILL BE REMOVED PRIOR TO PAVING TO PROVIDE CLEARANCE FOR THE PAVING TRAIN. THE CONTRACTOR WILL STORE THESE LIGHTS UNTIL PAVING OPERATIONS ARE COMPLETE. ONCE PAVING IS COMPLETE, THE CONTRACTOR WILL INSTALL THE THRESHOLD LIGHTS TO THEIR ORIGINAL POSITIONS. THE REMOVAL, STORAGE AND RE-INSTALLATION OF THESE THRESHOLD LIGHTS WILL BE CONSIDERED PART OF THE POROUS FRICTION COURSE INSTALLATION AND NO OTHER COMPENSATION WILL BE ALLOWED.

SHOULDER ADJUSTMENT

1. SHOULDER ADJUSTMENT WILL BE PERFORMED ADJACENT TO ALL PAVEMENT OVERLAY AREAS TO PROVIDE A 1-1/2" DROP-OFF. PRIOR TO PLACING THE SHOULDER MATERIAL, THE LIMITS WILL BE MOWED AND PULVERIZED, DISKED, OR TILLED TO THE SATISFACTION OF THE RESIDENT ENGINEER. THE PROPOSED GRADING, SEEDING AND MULCHING LIMITS ARE SHOWN ON THE CONSTRUCTION PLANS AS PROPOSED SHOULDER ADJUSTMENT. A 1-1/2 INCH DROP SHALL BE MAINTAINED FROM THE PAVEMENT EDGE TO THE EARTH SHOULDER UNLESS OTHERWISE NOTED. THE EARTH FILLETS WILL NOT REQUIRE COMPACTING OR GRADING, OTHER THAN LIGHT ROLLING AND SHAPING. THE MATERIAL FOR THE PROPOSED EARTH FILLETS WILL BE OBTAINED FROM OFF-SITE.

2. THE MATERIAL FOR THE PROPOSED SHOULDER ADJUSTMENT WILL BE PAID FOR UNDER: ITEM AR152480 "SHOULDER ADJUSTMENT"

3. QUANTITY OF "SHOULDER ADJUSTMENT" _____ 4,692 SQ. YDS.

SEEDING LIMITS

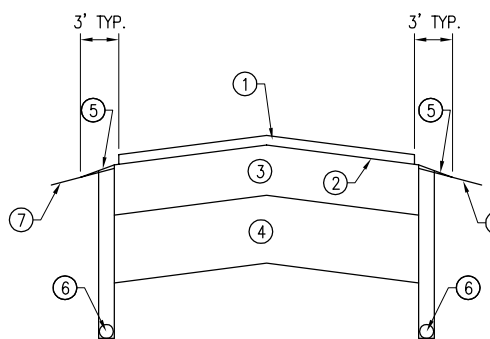
ALL AREAS WHICH ARE DISTURBED BY THE CONTRACTOR, OUTSIDE OF THE PROPOSED SHOULDER ADJUSTMENT LIMITS, WILL BE LIMED, FERTILIZED, AND SEEDED IN ACCORDANCE WITH THE REQUIREMENTS OF ITEM AR901510 AT NO ADDITIONAL COST TO THE CONTRACT.

THE EXISTING SOIL ADJACENT TO THE PAVEMENT SHALL BE THOROUGHLY LOOSENED TO A DEPTH NOT LESS THAN 4 INCHES PRIOR TO PLACEMENT OF THE SHOULDER ADJUSTMENT.

THE SEED BED SHALL BE SMOOTH AND TO GRADE UPON COMPLETION OF THE SEEDING OPERATION. THE CONTRACTOR WILL USE DRAG BARS, HARROWS OR OTHER EQUIPMENT NECESSARY TO OBTAIN THE SMOOTH GRADE TO THE SATISFACTION OF THE RESIDENT ENGINEER.

ALL ROCK, ASPHALT OR CONCRETE DEBRIS LEFT FROM THE PAVING OPERATION WILL BE REMOVED FROM THE AIRPORT SITE.

THE PRIME CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATION BETWEEN ALL SUB-CONTRACTORS AS TO THEIR RESPONSIBILITIES PERTAINING TO THE SEEDING OPERATION.



TYPICAL SECTION STA. 19+95 TO STA. 79+00
"NOT TO SCALE"

LEGEND FOR TYPICAL SECTIONS

- ① 402 - PROPOSED POROUS FRICTION COURSE, 0.10'
- ② 603 - PROPOSED BIT. TACK COAT (0.25 GAL. S.Y.)
- ③ 201/401 - EXISTING BIT. SURFACE/BASE COURSE (4" DEPTH)
- ④ 209 - AGGREGATE BASE COURSE (6" DEPTH)
- ⑤ 152 - SHOULDER ADJUSTMENT
- ⑥ 705 - EXISTING UNDERDRAIN
- ⑦ EXISTING GRADE

POROUS FRICTION COURSE NOTES

THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF ONE TENTH OF A FOOT (0.10') IN ACCORDANCE WITH ITEM AR402622 AND THE SPECIAL PROVISIONS.

THE POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER. THE CONTRACTOR WILL BE REQUIRED TO REMOVE/BLADE ALL SOD THAT HAS GROWN OVER THE EXISTING PAVEMENT EDGES PRIOR TO PLACEMENT OF THE BITUMINOUS TACK COAT. THIS WORK WILL BE CONSIDERED PART OF THE POROUS FRICTION COURSE INSTALLATION.

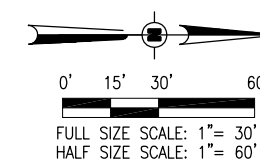
THE SURFACES TO BE OVERLAYED WILL BE SPRAYED WITH AN APPLICATION OF A BITUMINOUS TACK COAT. AN APPLICATION RATE OF 0.25 GAL/S.Y. (DILUTED) WAS USED FOR CALCULATING THE QUANTITY OF TACK FOR THIS APPLICATION. THE EXACT RATE OF APPLICATION WILL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

THE PROPOSED POROUS FRICTION COURSE WILL BE PAID FOR UNDER ITEM AR402622 - POROUS FRICTION COURSE, 0.10' = 56,110 S.Y.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM AR603510 - BITUMINOUS TACK COAT = 14,028 GAL.

LEGEND

- EXISTING PAVEMENT
- PROPOSED PFC OVERLAY
- PROPOSED SHOULDER ADJUSTMENT
- EXISTING THRESHOLD LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING INSPECTION HOLE TO BE ADJUSTED

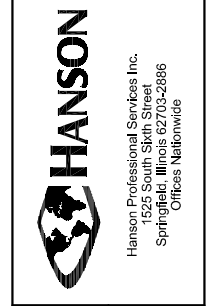


DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

AL.P. PROJ.: 3-17-0147-B10
IL. PROJ.: 163-3957

Hanson Project No. 09A01270_0800	FILENAME R-121CON.DWG	SCALE: 1" = 30'	DATE 01/07/10
LAYOUT	JEO	10/28/09	
DRAWN	BAK	10/28/09	
REVIEWED	CAH	01/28/10	

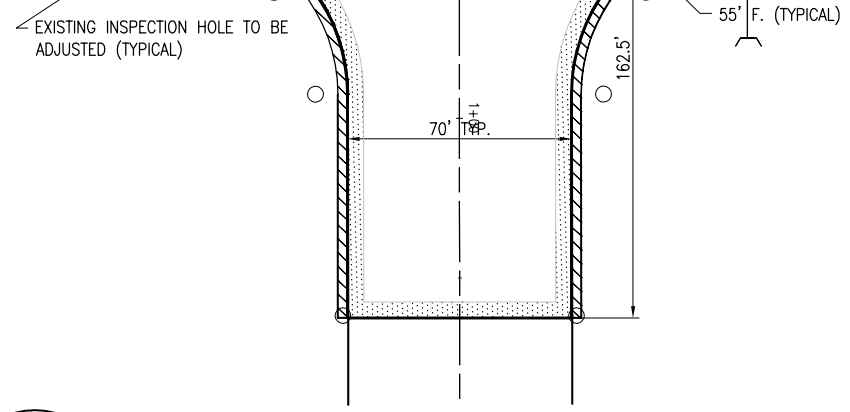
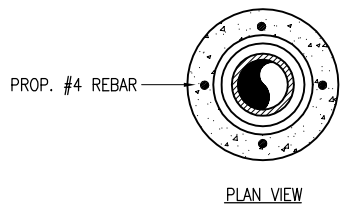
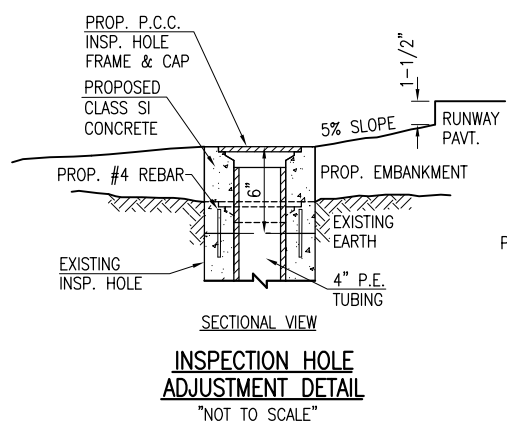
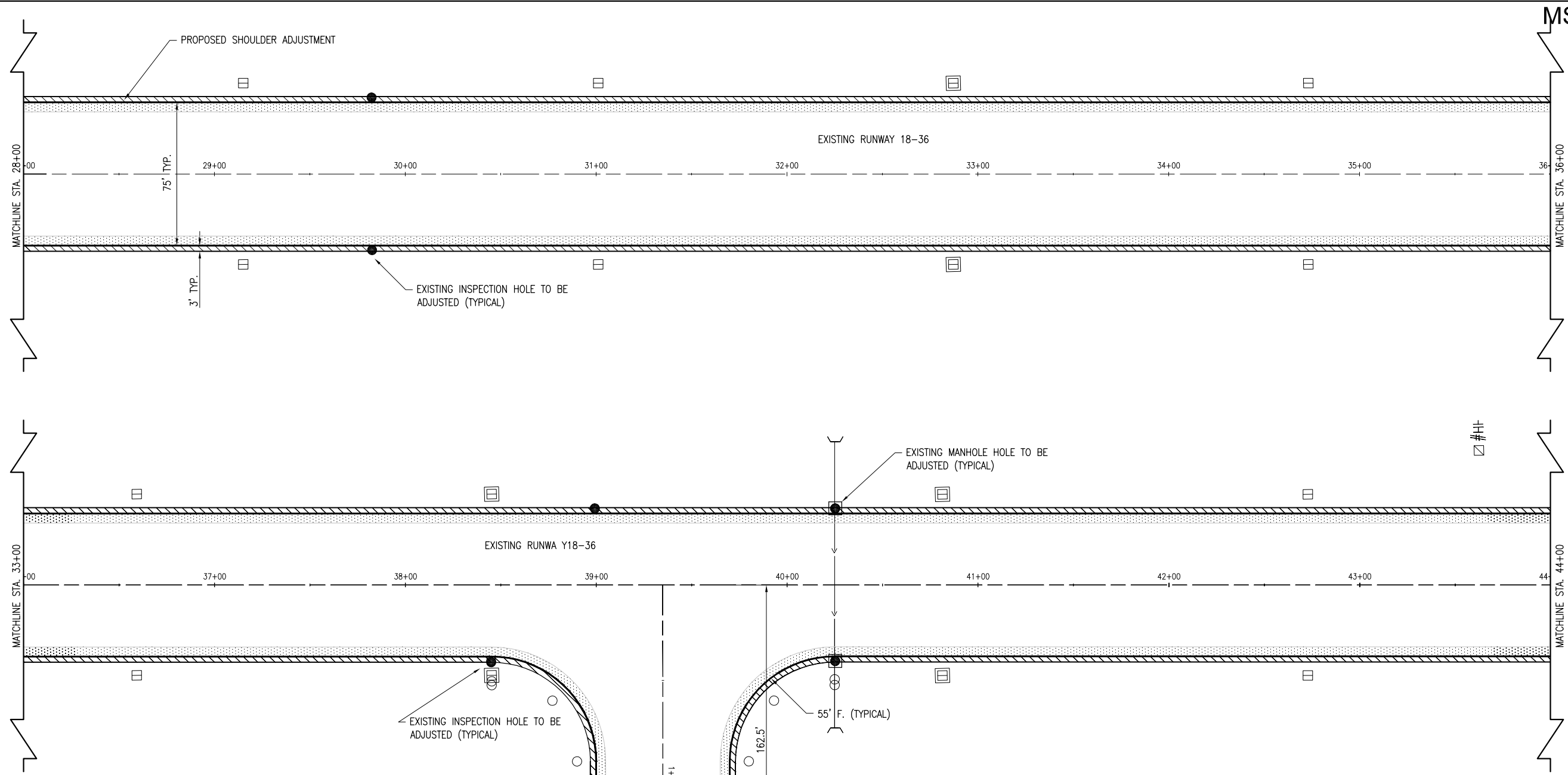


**PROPOSED P.F.C.
ON RUNWAY 18-36**

PROPOSED
CONSTRUCTION PLAN
STA. 19+95 TO STA. 28+00

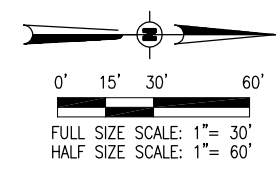
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LEGEND

	EXISTING PAVEMENT
	PROPOSED PFC OVERLAY
	PROPOSED SHOULDER ADJUSTMENT
	EXISTING STAKE MOUNTED RUNWAY LIGHT
	EXISTING BASE MOUNTED RUNWAY LIGHT
	EXISTING INSPECTION HOLE TO BE ADJUSTED
	EXISTING MANHOLE TO BE ADJUSTED
	EXISTING TAXIWAY LIGHT TO REMAIN IN PLACE



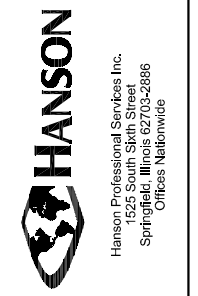
DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800
 Filename: R-121CON.DWG
 Scale: 1" = 30'
 Date: 01/07/10

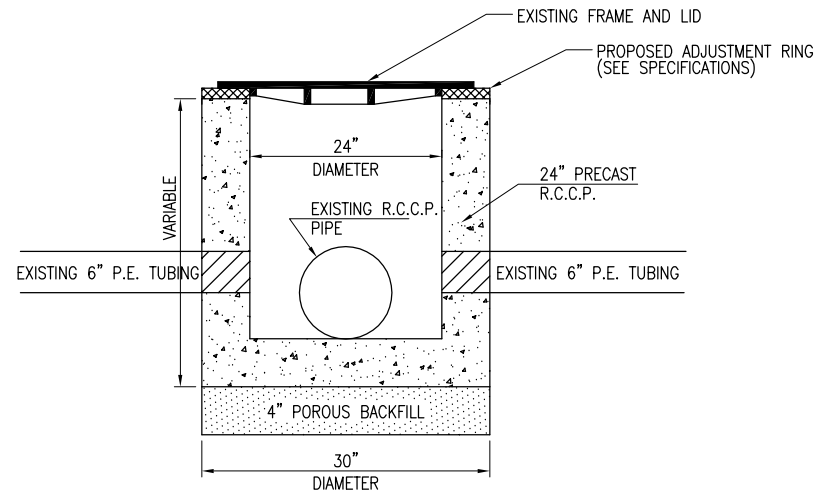
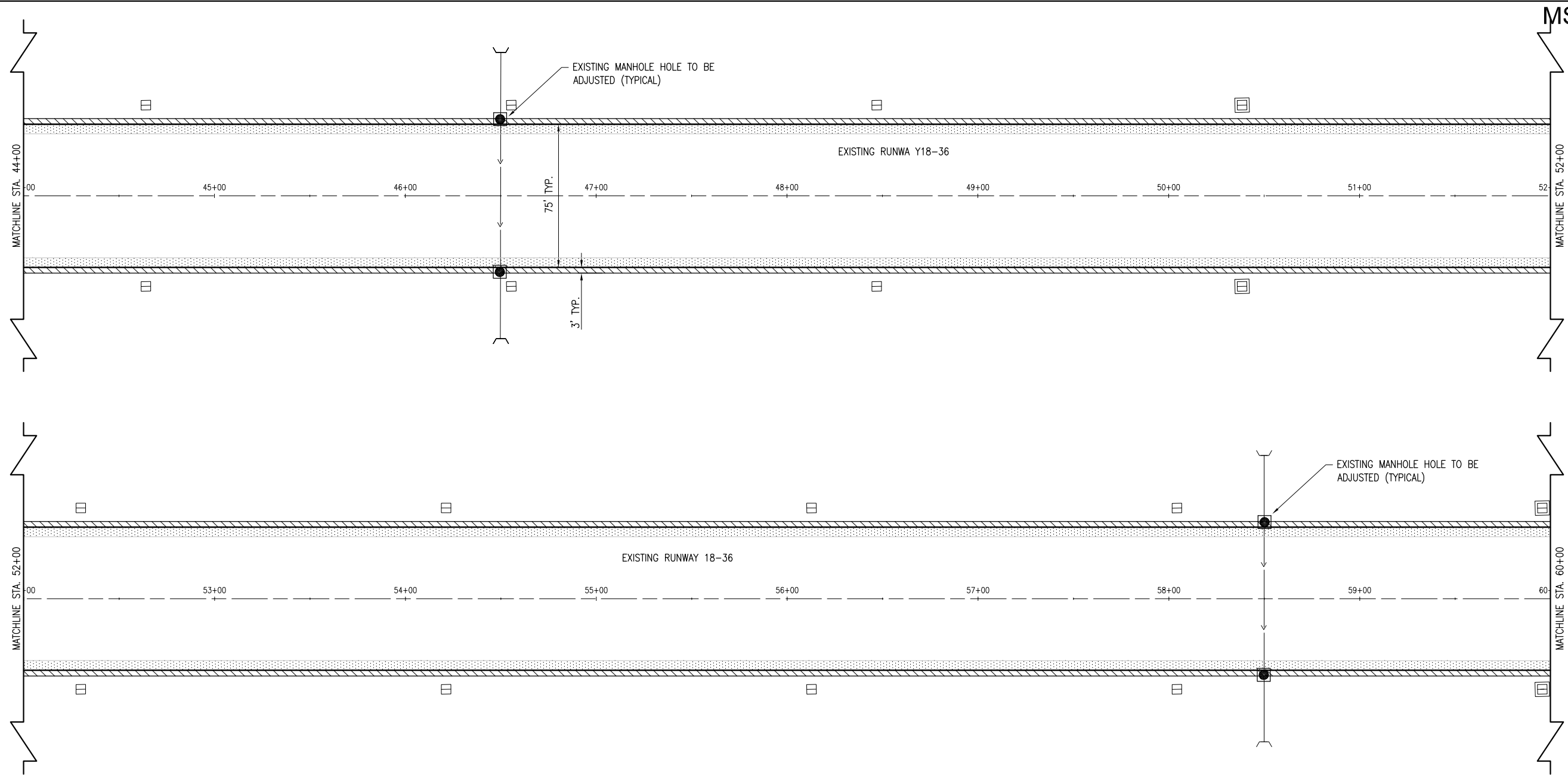
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DRAWN	BAK	10/28/09
REVIEWED	CAH	01/28/10



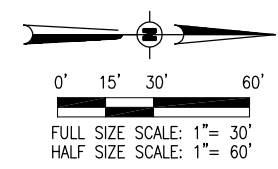
**PROPOSED P.F.C.
 ON RUNWAY 18-36**

PROPOSED
 CONSTRUCTION PLAN
 STA. 28+00 TO STA. 44+00

MAR 16, 2010 8:10 AM HAGL000382
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PROPOSED MANHOLE ADJUSTMENT
 "NOT TO SCALE"



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - PROPOSED SHOULDER ADJUSTMENT
 - EXISTING STAKE MOUNTED RUNWAY LIGHT
 - EXISTING BASE MOUNTED RUNWAY LIGHT
 - EXISTING MANHOLE TO BE ADJUSTED

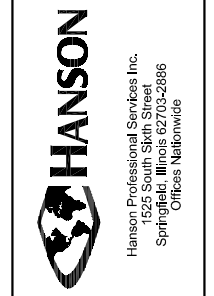
MS004

DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

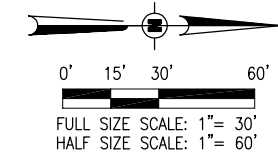
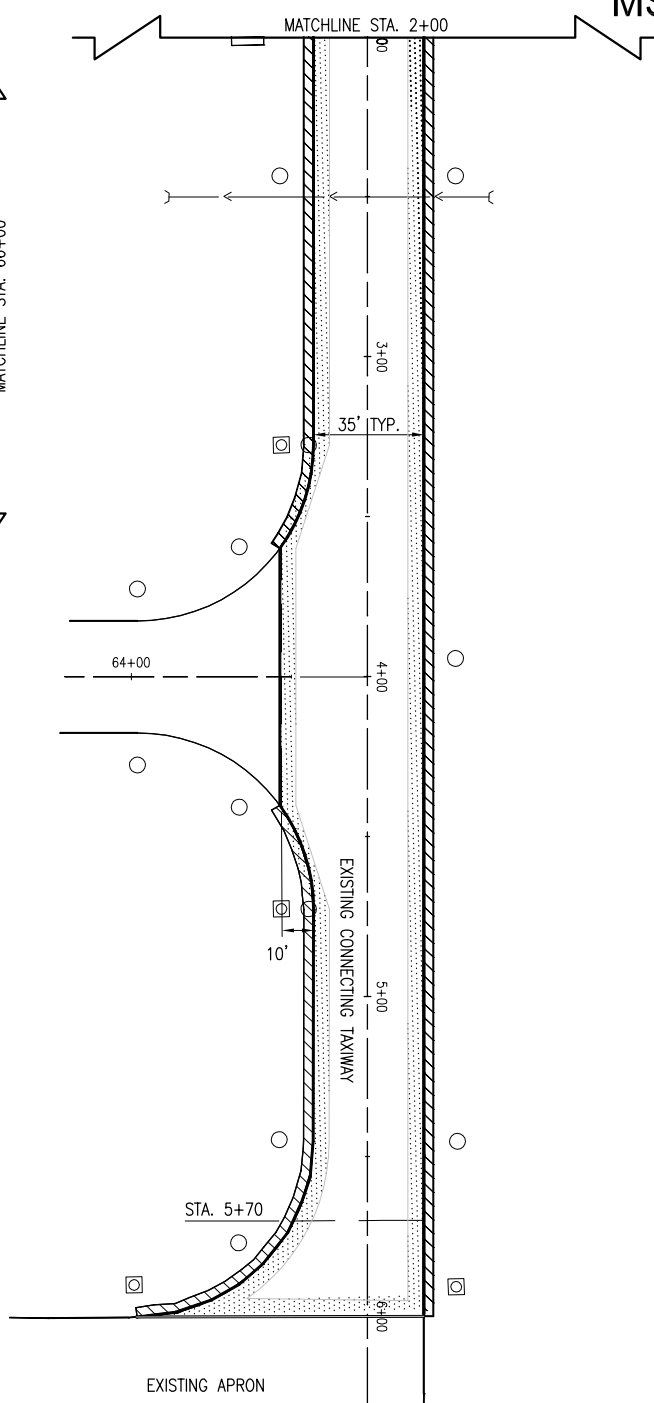
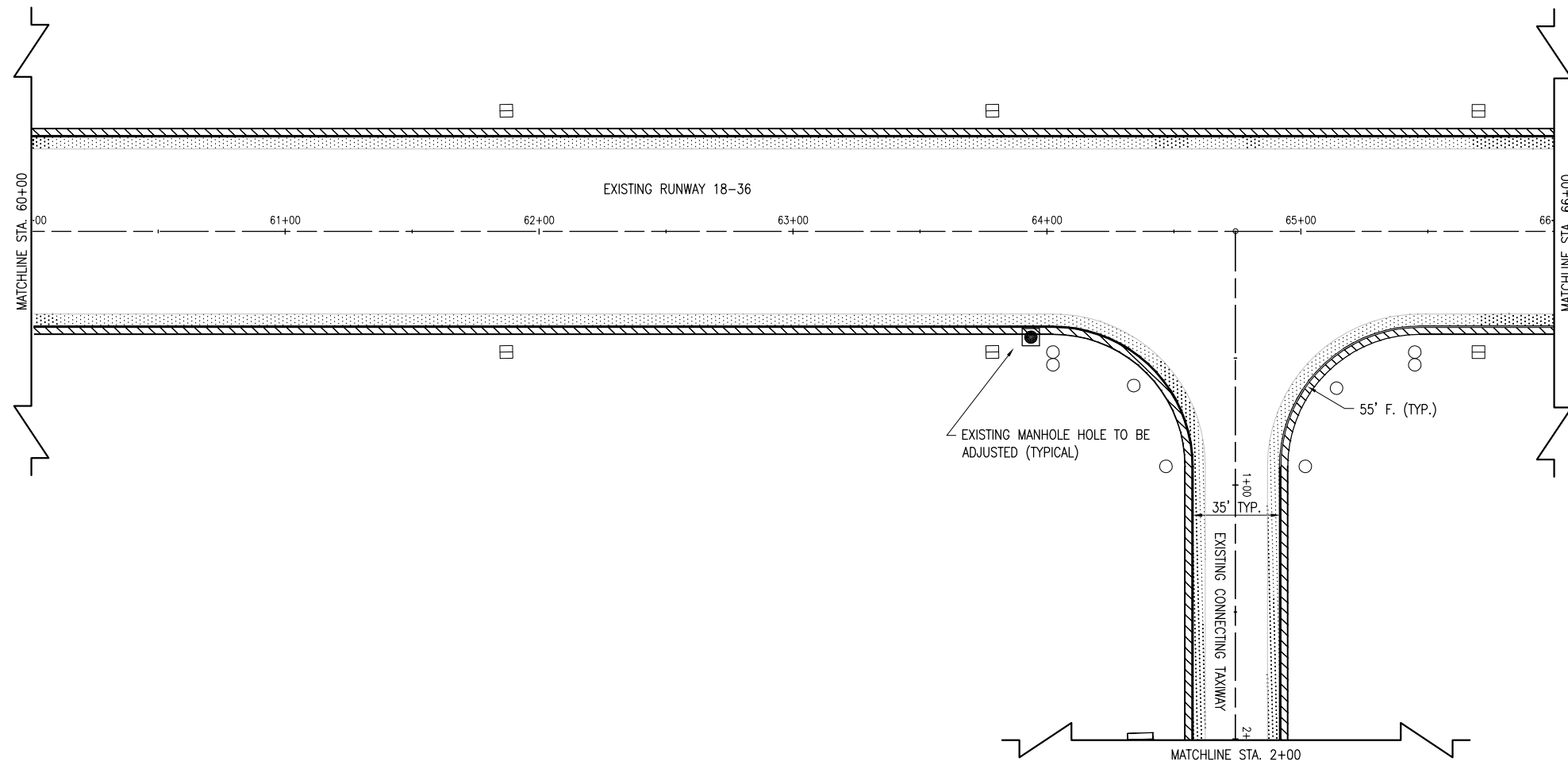
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Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
Filename R-121CON.DWG	DRAWN	BAK	10/28/09
Scale SCALE: 1" = 30'	REVIEWED	CAH	01/28/10
Date 01/07/10			



**PROPOSED P.F.C.
 ON RUNWAY 18-36**

PROPOSED
 CONSTRUCTION PLAN
 STA. 44+00 TO STA. 60+00



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - PROPOSED SHOULDER ADJUSTMENT
 - EXISTING STAKE MOUNTED RUNWAY LIGHT
 - EXISTING BASE MOUNTED RUNWAY LIGHT
 - EXISTING MANHOLE

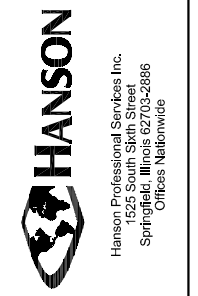
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DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

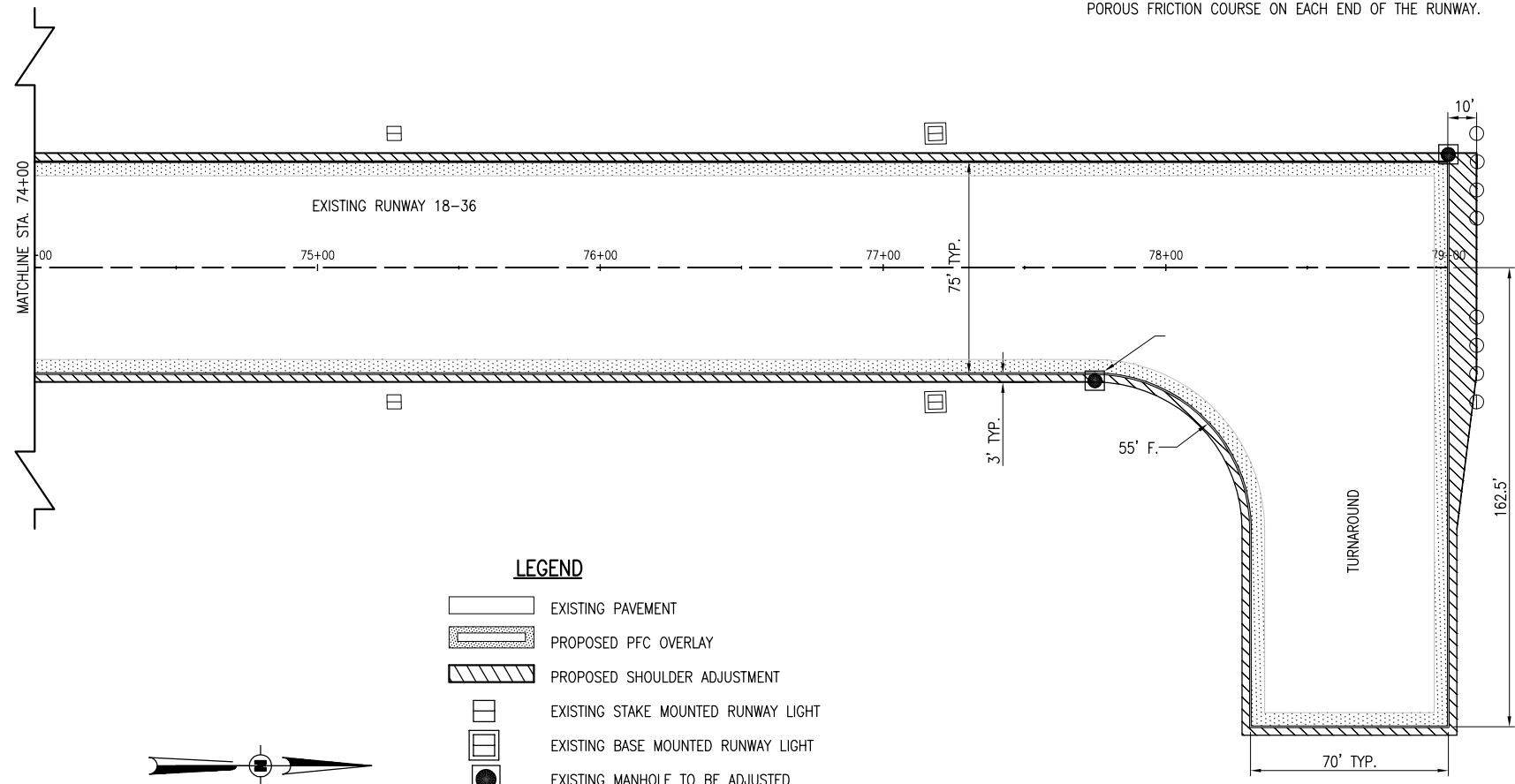
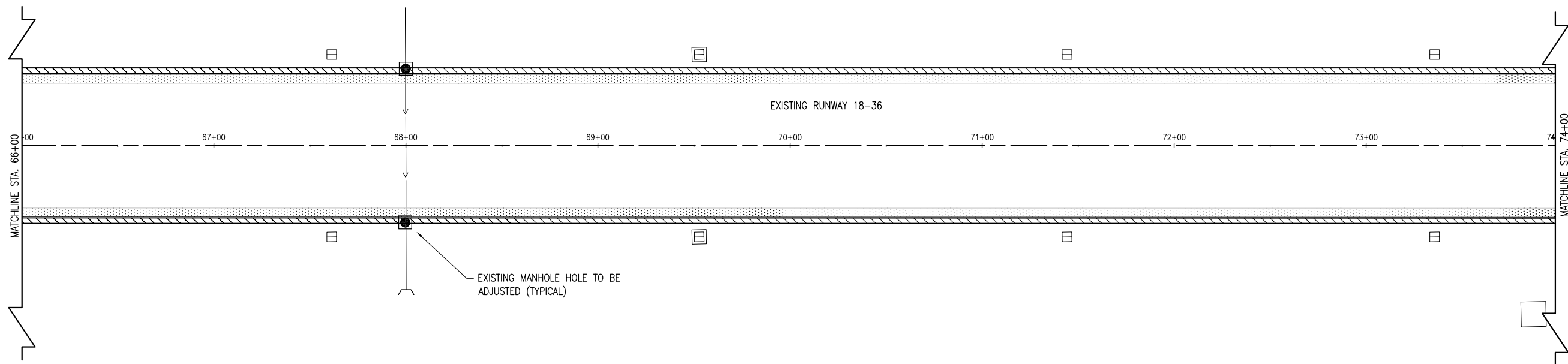
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Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
Filename: R-121CON.DWG	DRAWN	BAK	10/28/09
Scale: 1" = 30'	REVIEWED	CAH	01/28/10
Date: 01/07/10			



**PROPOSED P.F.C.
 ON RUNWAY 18-36**

PROPOSED
 CONSTRUCTION PLAN
 STA. 60+00 TO STA. 66+00



NOTE: THE SHOULDER ADJUSTMENT WILL BE FLUSH WITH THE PROPOSED POROUS FRICTION COURSE ON EACH END OF THE RUNWAY.

THRESHOLD LIGHTS NOTE

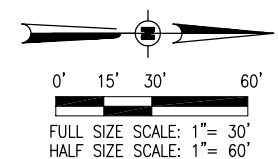
SIX OF THE EIGHT EXISTING THRESHOLD LIGHTS ON RUNWAY END 18 AND 36 WILL BE REMOVED PRIOR TO PAVING TO PROVIDE CLEARANCE FOR THE PAVING TRAIN. THE CONTRACTOR WILL STORE THESE LIGHTS UNTIL PAVING OPERATIONS ARE COMPLETE. ONCE PAVING IS COMPLETE, THE CONTRACTOR WILL INSTALL THE THRESHOLD LIGHTS TO THEIR ORIGINAL POSITIONS. THE REMOVAL, STORAGE AND RE-INSTALLATION OF THESE THRESHOLD LIGHTS WILL BE CONSIDERED PART OF THE POROUS FRICTION COURSE INSTALLATION AND NO OTHER COMPENSATION WILL BE ALLOWED.

EXCELSIOR BLANKET NOTES

AN EROSION CONTROL MATERIAL (EXCELSIOR BLANKET) WILL BE INSTALLED OVER THE SHOULDER ADJUSTMENT LOCATIONS SHOWN ON THESE CONSTRUCTION PLAN SHEETS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR908520 "EXCELSIOR BLANKET" 4,783 S.Y.

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - PROPOSED SHOULDER ADJUSTMENT
 - EXISTING STAKE MOUNTED RUNWAY LIGHT
 - EXISTING BASE MOUNTED RUNWAY LIGHT
 - EXISTING MANHOLE TO BE ADJUSTED



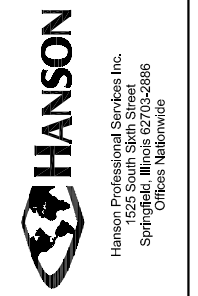
SUMMARY OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	TOTAL QUANTITIES
AR152480	SHOULDER ADJUSTMENT	S.Y.	4,783
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	13,338
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	267
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	141
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	56,110
AR603510	BITUMINOUS TACK COAT	GAL.	14,028
AR751943	ADJUST MANHOLE	EA.	6
AR751949	ADJUST INSPECTION HOLE	EA.	11
AR901510	SEEDING	ACRE	0.99
AR908520	EXCELSIOR BLANKET	S.Y.	4,783

DATE	REVISION	BY
03/16/10	REVISED PER IDA REVIEW	CAH

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
Filename: R-121CON.DWG	DRAWN	BAK	10/28/09
Scale: 1" = 30'	REVIEWED	CAH	01/28/10
Date: 01/07/10			

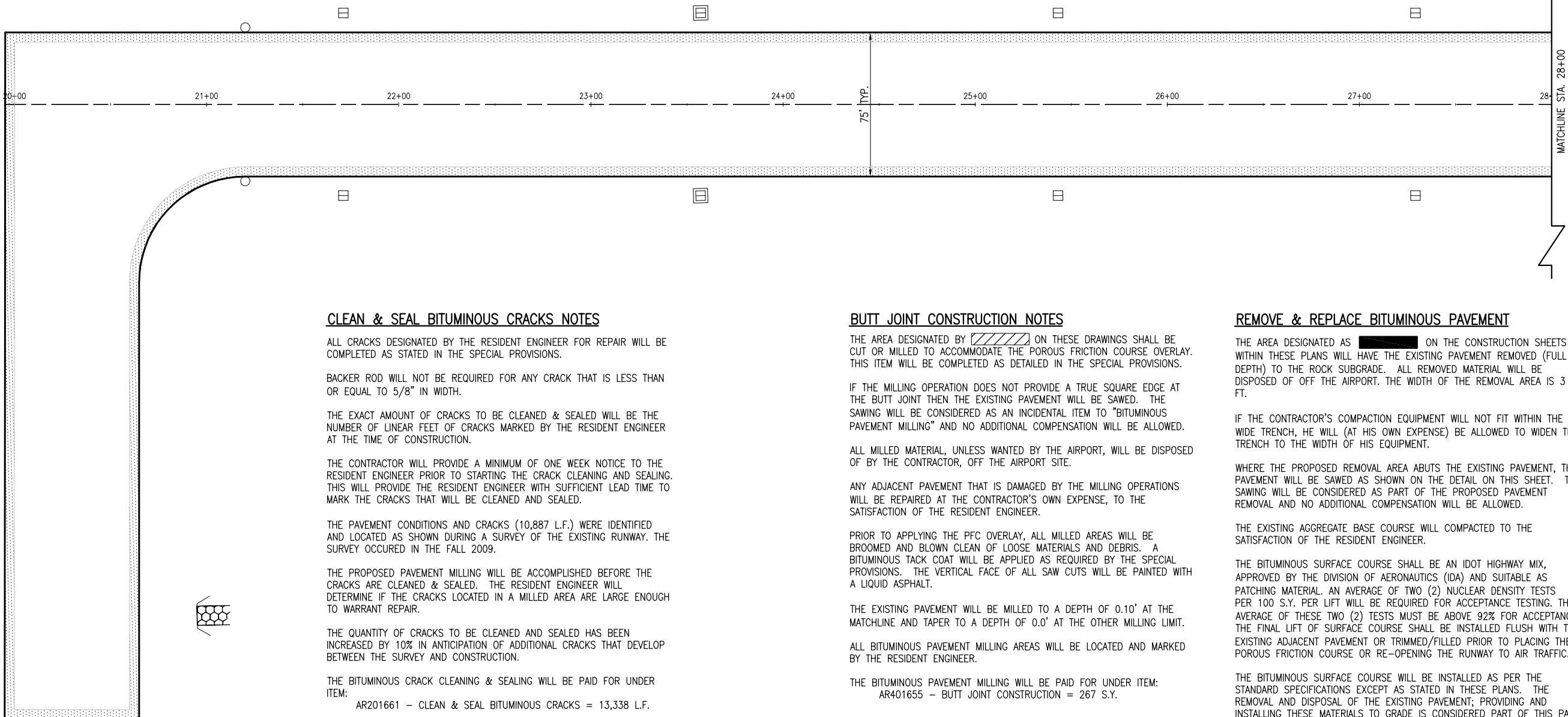


**PROPOSED P.F.C.
ON RUNWAY 18-36**

PROPOSED
CONSTRUCTION PLAN
STA. 66+00 TO STA. 79+00

MAR 16, 2010 8:15 AM HAGL000382 I:\AIRPORTS\MSTERLING\09A0127\CADD\AIRPORT\SHEET\R-121CON.DWG - STA. 76+00 TO STA. 79+00

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CLEAN & SEAL BITUMINOUS CRACKS NOTES

ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR REPAIR WILL BE COMPLETED AS STATED IN THE SPECIAL PROVISIONS.

BACKER ROD WILL NOT BE REQUIRED FOR ANY CRACK THAT IS LESS THAN OR EQUAL TO 5/8" IN WIDTH.

THE EXACT AMOUNT OF CRACKS TO BE CLEANED & SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

THE CONTRACTOR WILL PROVIDE A MINIMUM OF ONE WEEK NOTICE TO THE RESIDENT ENGINEER PRIOR TO STARTING THE CRACK CLEANING AND SEALING. THIS WILL PROVIDE THE RESIDENT ENGINEER WITH SUFFICIENT LEAD TIME TO MARK THE CRACKS THAT WILL BE CLEANED AND SEALED.

THE PAVEMENT CONDITIONS AND CRACKS (10,887 L.F.) WERE IDENTIFIED AND LOCATED AS SHOWN DURING A SURVEY OF THE EXISTING RUNWAY. THE SURVEY OCCURED IN THE FALL 2009.

THE PROPOSED PAVEMENT MILLING WILL BE ACCOMPLISHED BEFORE THE CRACKS ARE CLEANED & SEALED. THE RESIDENT ENGINEER WILL DETERMINE IF THE CRACKS LOCATED IN A MILLED AREA ARE LARGE ENOUGH TO WARRANT REPAIR.

THE QUANTITY OF CRACKS TO BE CLEANED AND SEALED HAS BEEN INCREASED BY 10% IN ANTICIPATION OF ADDITIONAL CRACKS THAT DEVELOP BETWEEN THE SURVEY AND CONSTRUCTION.

THE BITUMINOUS CRACK CLEANING & SEALING WILL BE PAID FOR UNDER ITEM:
AR201661 - CLEAN & SEAL BITUMINOUS CRACKS = 13,338 L.F.



BUTT JOINT CONSTRUCTION NOTES

THE AREA DESIGNATED BY [Hatched Box] ON THESE DRAWINGS SHALL BE CUT OR MILLED TO ACCOMMODATE THE POROUS FRICTION COURSE OVERLAY. THIS ITEM WILL BE COMPLETED AS DETAILED IN THE SPECIAL PROVISIONS.

IF THE MILLING OPERATION DOES NOT PROVIDE A TRUE SQUARE EDGE AT THE BUTT JOINT THEN THE EXISTING PAVEMENT WILL BE SAWED. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO "BITUMINOUS PAVEMENT MILLING" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL, UNLESS WANTED BY THE AIRPORT, WILL BE DISPOSED OF BY THE CONTRACTOR, OFF THE AIRPORT SITE.

ANY ADJACENT PAVEMENT THAT IS DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE, TO THE SATISFACTION OF THE RESIDENT ENGINEER.

PRIOR TO APPLYING THE PFC OVERLAY, ALL MILLED AREAS WILL BE BROOMED AND BLOWN CLEAN OF LOOSE MATERIALS AND DEBRIS. A BITUMINOUS TACK COAT WILL BE APPLIED AS REQUIRED BY THE SPECIAL PROVISIONS. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

THE EXISTING PAVEMENT WILL BE MILLED TO A DEPTH OF 0.10' AT THE MATCHLINE AND TAPER TO A DEPTH OF 0.0' AT THE OTHER MILLING LIMIT.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE BITUMINOUS PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM:
AR401655 - BUTT JOINT CONSTRUCTION = 267 S.Y.

REMOVE & REPLACE BITUMINOUS PAVEMENT

THE AREA DESIGNATED AS [Solid Black Box] ON THE CONSTRUCTION SHEETS WITHIN THESE PLANS WILL HAVE THE EXISTING PAVEMENT REMOVED (FULL DEPTH) TO THE ROCK SUBGRADE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT. THE WIDTH OF THE REMOVAL AREA IS 3 FT.

IF THE CONTRACTOR'S COMPACTION EQUIPMENT WILL NOT FIT WITHIN THE 3' WIDE TRENCH, HE WILL (AT HIS OWN EXPENSE) BE ALLOWED TO WIDEN THE TRENCH TO THE WIDTH OF HIS EQUIPMENT.

WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWED AS SHOWN ON THE DETAIL ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS PART OF THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE EXISTING AGGREGATE BASE COURSE WILL COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER.

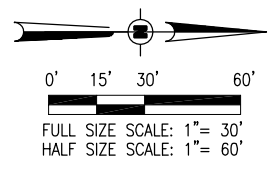
THE BITUMINOUS SURFACE COURSE SHALL BE AN IDOT HIGHWAY MIX, APPROVED BY THE DIVISION OF AERONAUTICS (IDA) AND SUITABLE AS PATCHING MATERIAL. AN AVERAGE OF TWO (2) NUCLEAR DENSITY TESTS PER 100 S.Y. PER LIFT WILL BE REQUIRED FOR ACCEPTANCE TESTING. THE AVERAGE OF THESE TWO (2) TESTS MUST BE ABOVE 92% FOR ACCEPTANCE. THE FINAL LIFT OF SURFACE COURSE SHALL BE INSTALLED FLUSH WITH THE EXISTING ADJACENT PAVEMENT OR TRIMMED/FILLED PRIOR TO PLACING THE POROUS FRICTION COURSE OR RE-OPENING THE RUNWAY TO AIR TRAFFIC.

THE BITUMINOUS SURFACE COURSE WILL BE INSTALLED AS PER THE STANDARD SPECIFICATIONS EXCEPT AS STATED IN THESE PLANS. THE REMOVAL AND DISPOSAL OF THE EXISTING PAVEMENT; PROVIDING AND INSTALLING THESE MATERIALS TO GRADE IS CONSIDERED PART OF THIS PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.

THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED DURING A SURVEY IN THE FALL 2009.

THE QUANTITY OF REMOVAL AND REPLACEMENT OF BITUMINOUS PAVEMENT HAS BEEN INCREASED BY 10% IN ANTICIPATION OF ADDITIONAL AREAS FALLING BETWEEN THE SURVEY AND CONSTRUCTION.

THE REMOVAL AND REPLACEMENT OF BITUMINOUS PAVEMENT WILL BE PAID FOR UNDER ITEM:
AR401910 "REMOVE & REPLACE BITUMINOUS PAVEMENT"----141 SQ. YDS.



LEGEND

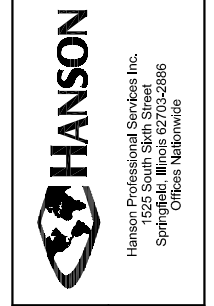
[Dotted Pattern]	EXISTING PAVEMENT
[Hatched Pattern]	PROPOSED PFC OVERLAY
[Solid Black]	REMOVE & REPLACE BIT. PAVEMENT

DATE	REVISION	BY
3/16/10	REVISED PER IDA REVIEW	CAH

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

IL. PROJ.: 163-3957
A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800	FILENAME R-111PRP.DWG	SCALE: 1" = 30'	DATE 01/07/10
LAYOUT	JEO	10/28/09	
DRAWN	BAK	10/28/09	
REVIEWED	CAH	01/28/10	

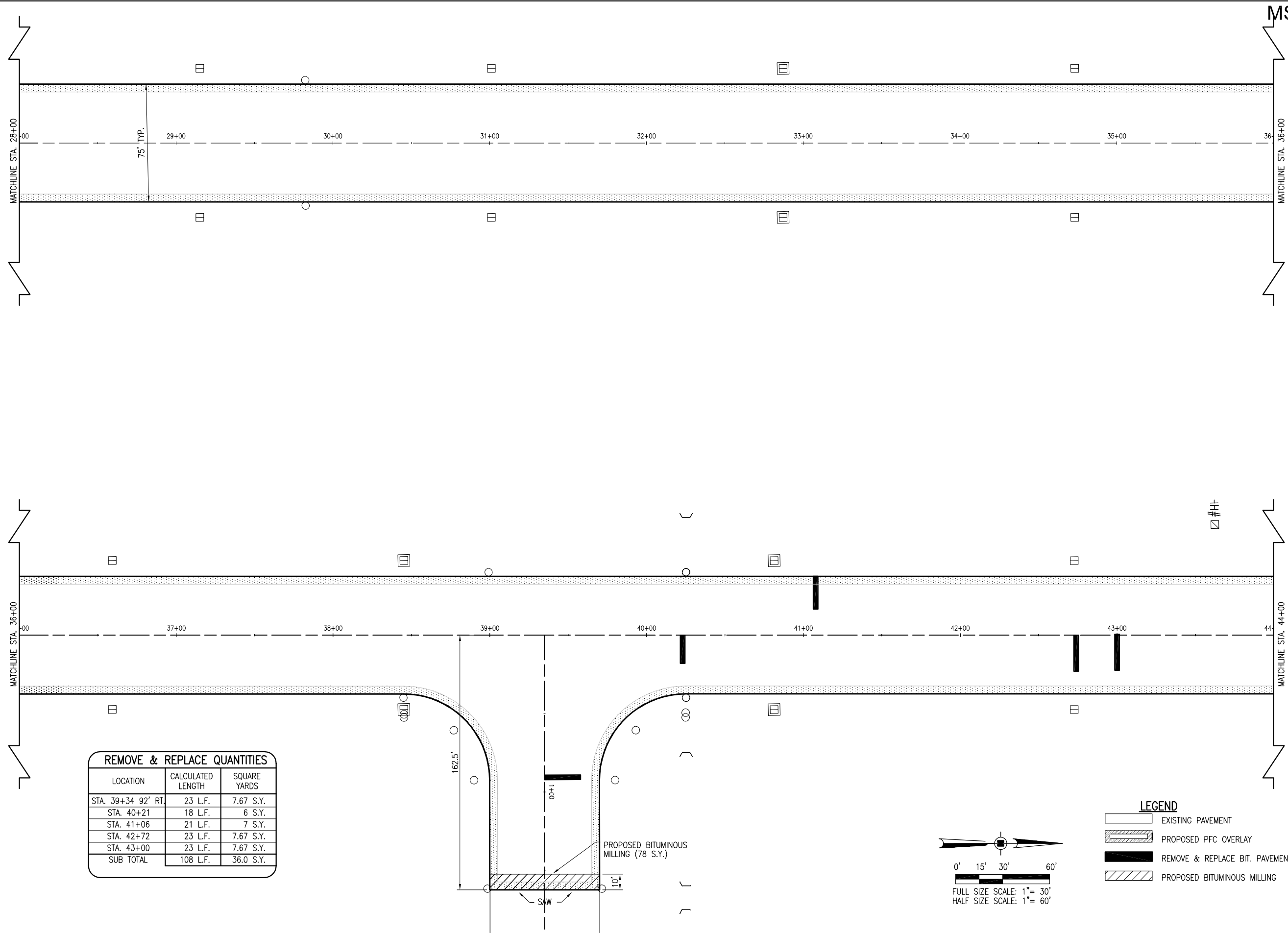


**PROPOSED P.F.C.
ON RUNWAY 18-36**

PROPOSED PAVEMENT
PREPARATION PLAN
STA. 19+95 TO STA. 28+00

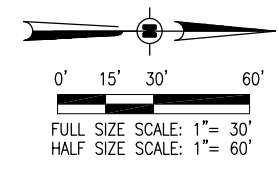
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REMOVE & REPLACE QUANTITIES		
LOCATION	CALCULATED LENGTH	SQUARE YARDS
STA. 39+34 92' RT.	23 L.F.	7.67 S.Y.
STA. 40+21	18 L.F.	6 S.Y.
STA. 41+06	21 L.F.	7 S.Y.
STA. 42+72	23 L.F.	7.67 S.Y.
STA. 43+00	23 L.F.	7.67 S.Y.
SUB TOTAL	108 L.F.	36.0 S.Y.

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - REMOVE & REPLACE BIT. PAVEMENT
 - PROPOSED BITUMINOUS MILLING



MS004

DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

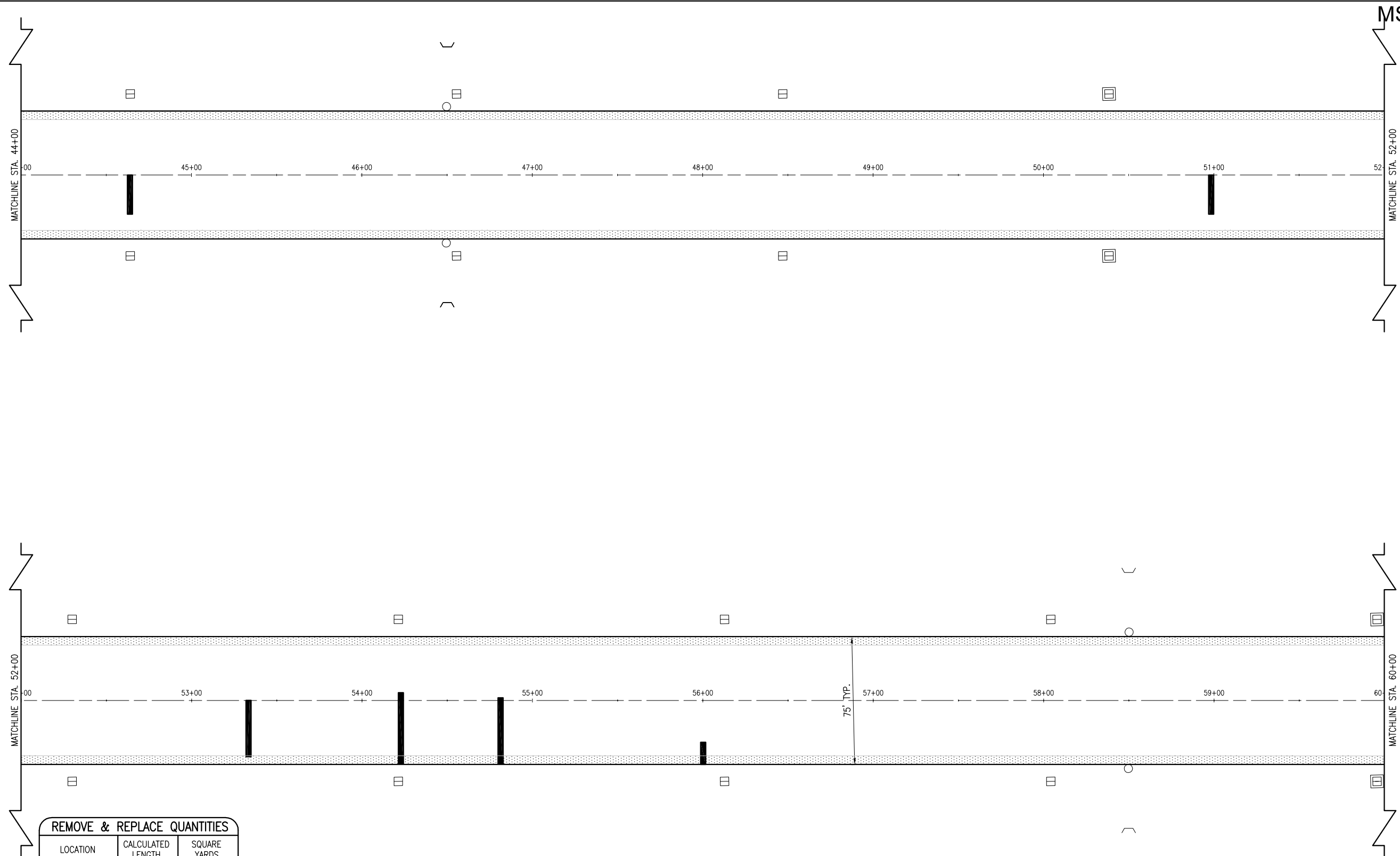
IL PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
Filename: R-111PRP.DWG	DRAWN	BAK	10/28/09
Scale: SCALE: 1" = 30'	REVIEWED	CAH	01/28/10
Date: 01/07/10			

HANSON
 Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Offices Nationwide

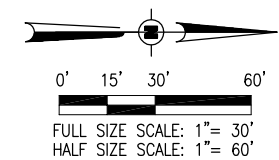
**PROPOSED P.F.C.
 ON RUNWAY 18-36**

PROPOSED PAVEMENT
 PREPARATION PLAN
 STA. 28+00 TO STA. 44+00



MS004

REMOVE & REPLACE QUANTITIES		
LOCATION	CALCULATED LENGTH	SQUARE YARDS
STA. 44+65	23 L.F.	7.67 S.Y.
STA. 51+00	23 L.F.	7.67 S.Y.
STA. 53+32	33 L.F.	11 S.Y.
STA. 54+21	42 L.F.	14 S.Y.
STA. 54+80	39 L.F.	13 S.Y.
STA. 56+00	13 L.F.	4.33 S.Y.
SUB TOTAL	173 L.F.	57.7 S.Y.



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - REMOVE & REPLACE BIT. PAVEMENT

DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

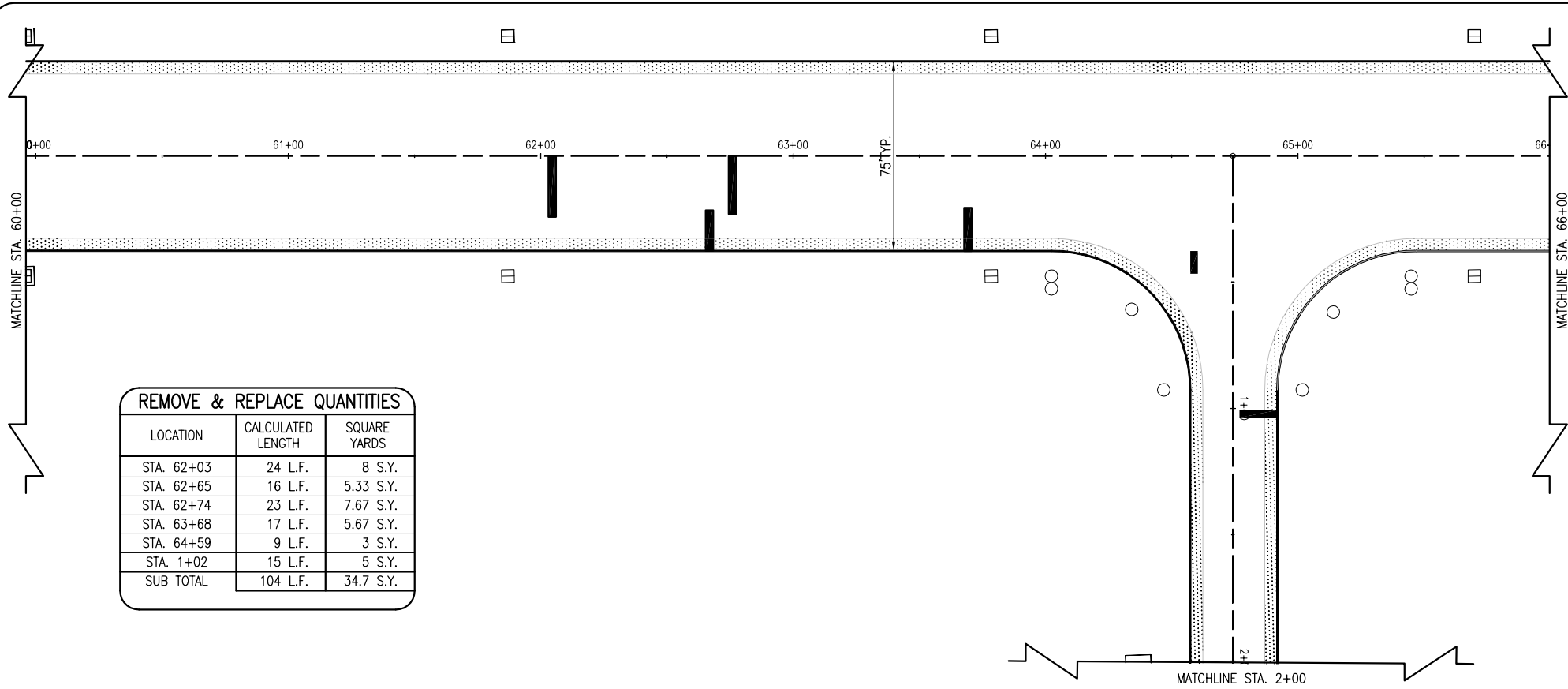
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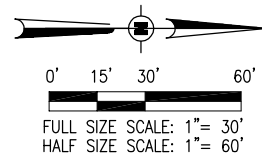


**PROPOSED P.F.C.
 ON RUNWAY 18-36**

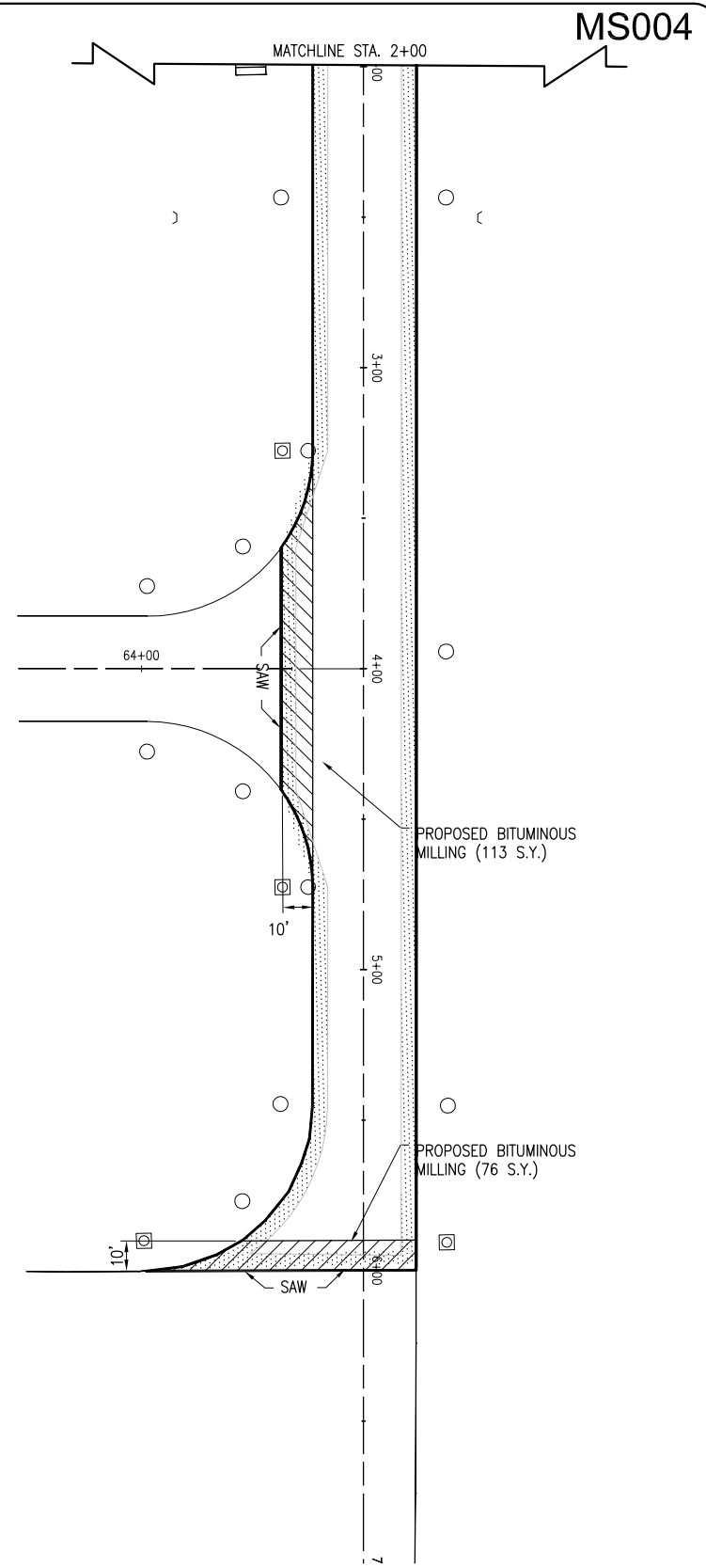
PROPOSED PAVEMENT
 PREPARATION PLAN
 STA. 44+00 TO STA. 60+00



REMOVE & REPLACE QUANTITIES		
LOCATION	CALCULATED LENGTH	SQUARE YARDS
STA. 62+03	24 L.F.	8 S.Y.
STA. 62+65	16 L.F.	5.33 S.Y.
STA. 62+74	23 L.F.	7.67 S.Y.
STA. 63+68	17 L.F.	5.67 S.Y.
STA. 64+59	9 L.F.	3 S.Y.
STA. 1+02	15 L.F.	5 S.Y.
SUB TOTAL	104 L.F.	34.7 S.Y.

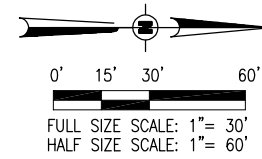
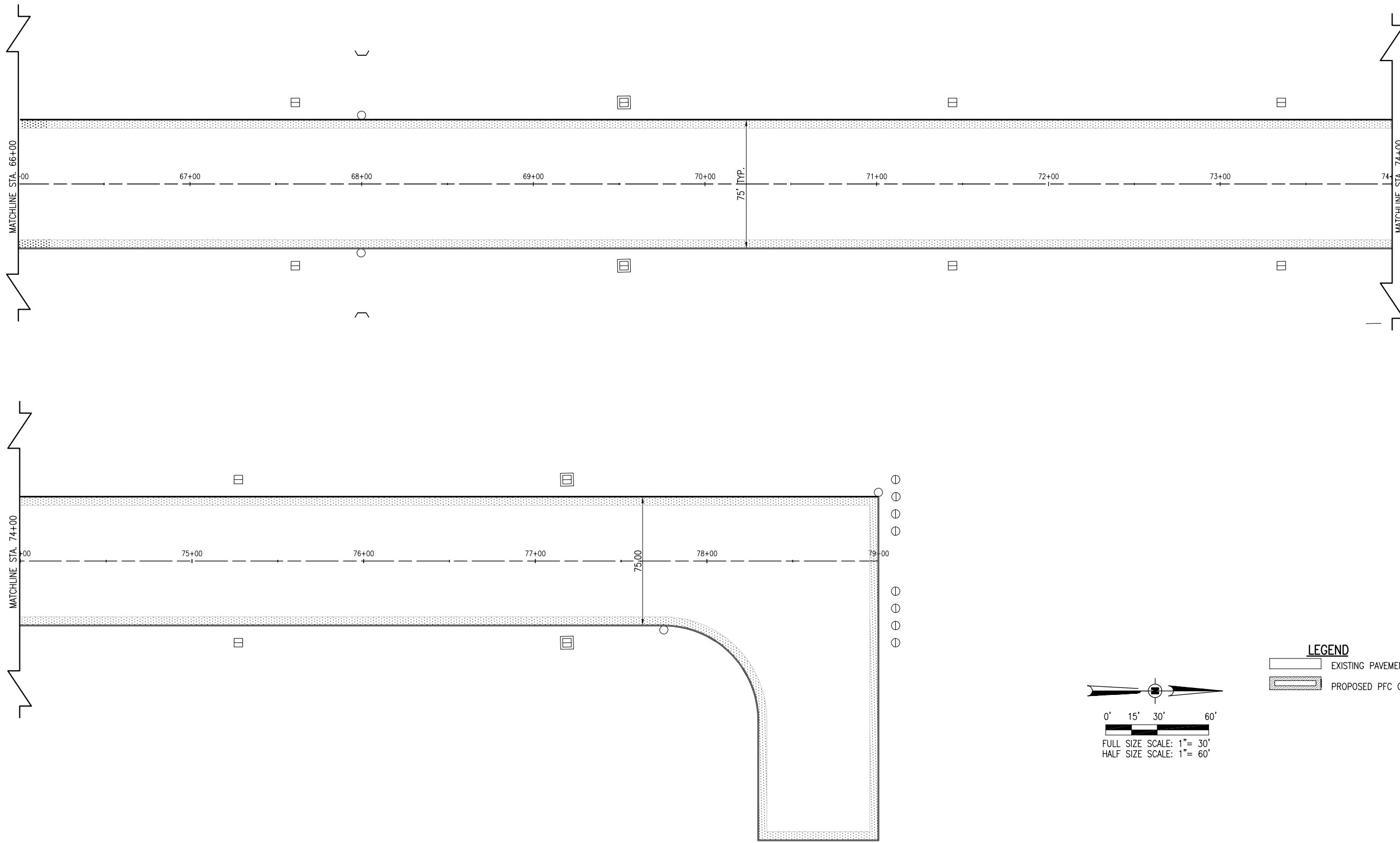


- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PFC OVERLAY
 - REMOVE & REPLACE BIT. PAVEMENT
 - PROPOSED BITUMINOUS MILLING



PROPOSED P.F.C. ON RUNWAY 18-36	PROPOSED PAVEMENT PREPARATION PLAN	12	IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10	REVISION DATE	BY
ROBERT F. TRACY MUNICIPAL AIRPORT MT. STERLING, ILLINOIS		Hanson Project No. 09A0127D_0800 Filename: R-111PRP.DWG Scale: 1" = 30' Date: 01/07/10		LAYOUT: JEO 10/28/09 DRAWN: BAK 10/28/09 REVIEWED: CAH 01/28/10	
		Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide		STA. 60+00 TO STA. 66+00	
12 of 18 sheets					

MS004



LEGEND
 EXISTING PAVEMENT
 PROPOSED PFC OVERLAY

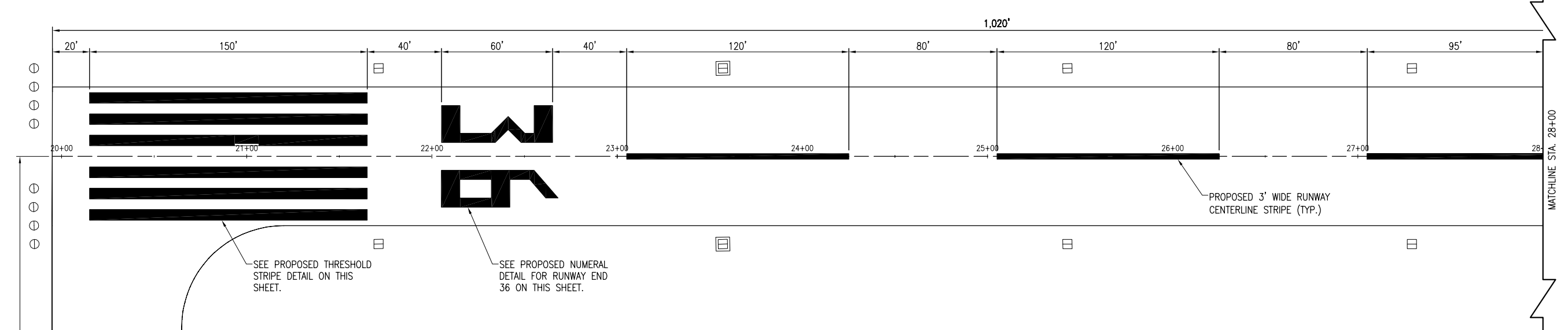
DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**
 I.L. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Project No.	File Name	Scale	Date	LAYOUT	DATE
09A0127D_0800	R-111PRP.DWG	1" = 30'	01/07/10	JEO	10/28/09
				BAK	10/28/09
				CAH	01/28/10

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 Springfield, Illinois 62703-2886
 Offices Nationwide

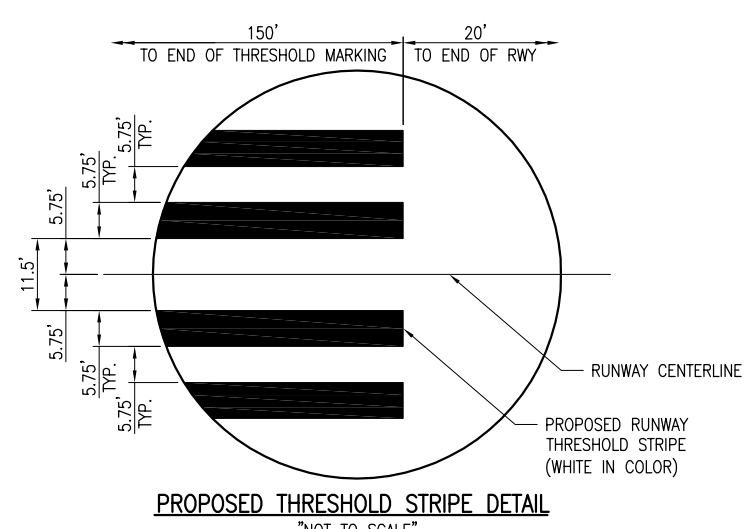
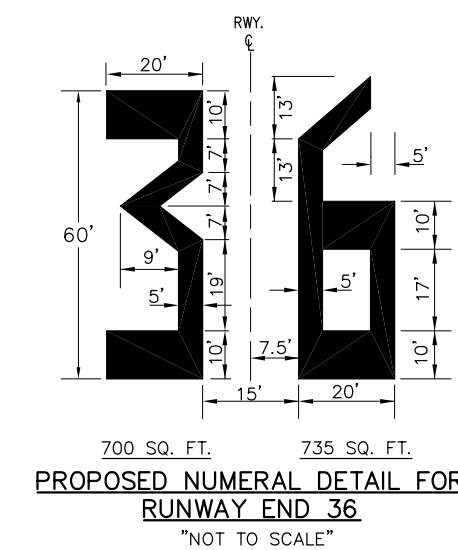
**PROPOSED P.F.C.
 ON RUNWAY 18-36**
 PROPOSED
 PAVEMENT PREPARATION PLAN
 STA. 66+00 TO STA. 79+00



SEE PROPOSED THRESHOLD STRIPE DETAIL ON THIS SHEET.

SEE PROPOSED NUMERAL DETAIL FOR RUNWAY END 36 ON THIS SHEET.

SEE PROPOSED HOLDING POSITION DETAIL ON SHEET 18.



620--PAVEMENT MARKING--WATERBORNE NOTES

THE PAVEMENT MARKING--WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 277 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM SHALL CONSIST OF RUNWAY AND HOLDING LINE MARKING IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. ALL RUNWAY MARKING WILL BE WHITE IN COLOR AND THE HOLDING LINE MARKING WILL BE YELLOW IN COLOR WITH A BLACK OUTLINE. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL CURING COMPOUND WILL BE CLEANED FROM CONCRETE PAVEMENT PRIOR TO APPLYING PAINT. NO EXCEPTIONS.

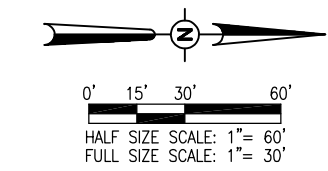
ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:
 AR620520 PAVEMENT MARKING--WATERBORNE ___ PER S.F.

MARKING QUANTITIES			
DESCRIPTION	UNIT AREA	NO. REQUIRED	TOTAL AREA
RUNWAY CENTERLINE STRIPE (SOLID)	360	27	9,720
NUMBER 1 (SOLID)	320	1	320
NUMBER 3 (SOLID)	700	1	700
NUMBER 6 (SOLID)	735	1	735
NUMBER 8 (SOLID)	900	1	900
THRESHOLD STRIPE (SOLID)	862.5	12	10,350
AIMING POINT (SOLID)	2,250	4	9,000
TOTAL WHITE			31,725
HOLDING LINE	735	1	735
TAXIWAY CENTERLINE	2,222	1	2,222
TOTAL YELLOW			2,957
HOLDING LINE (BLACK OUTLINE)	1,225	1	1,225
TOTAL BLACK			1,225
TOTAL MARKING			35,907



LEGEND

	EXISTING PAVEMENT
	PROPOSED MARKING

DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

AL.P. PROJ.: 3-17-0147-B10
 I.L. PROJ.: 163-3957

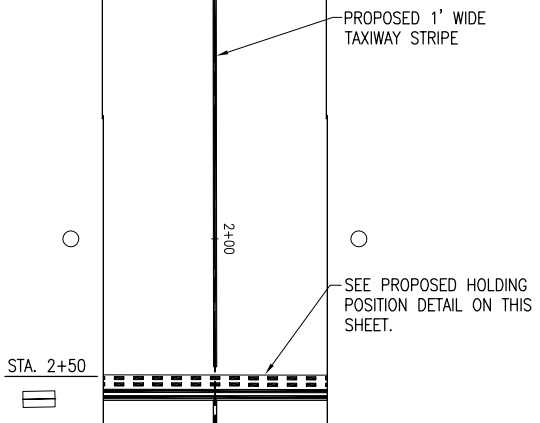
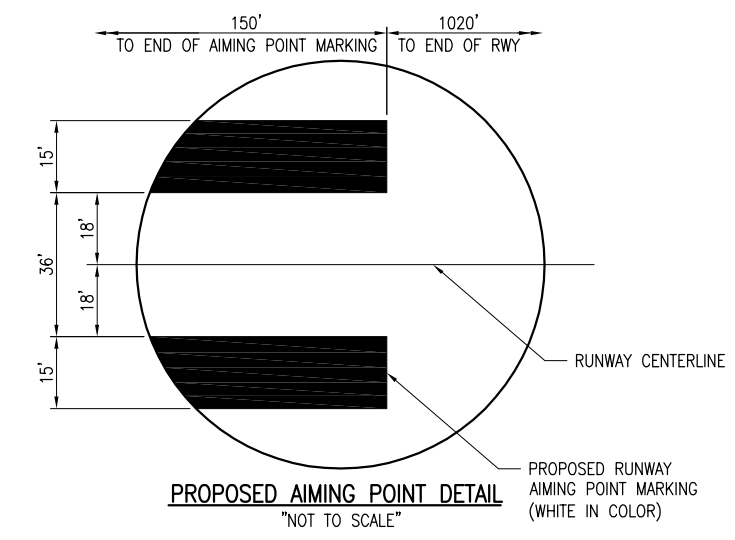
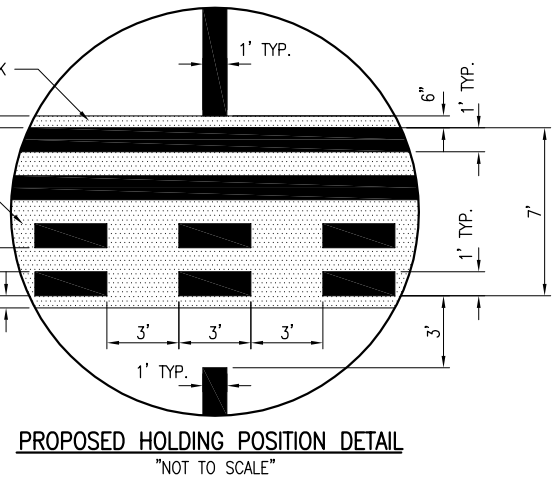
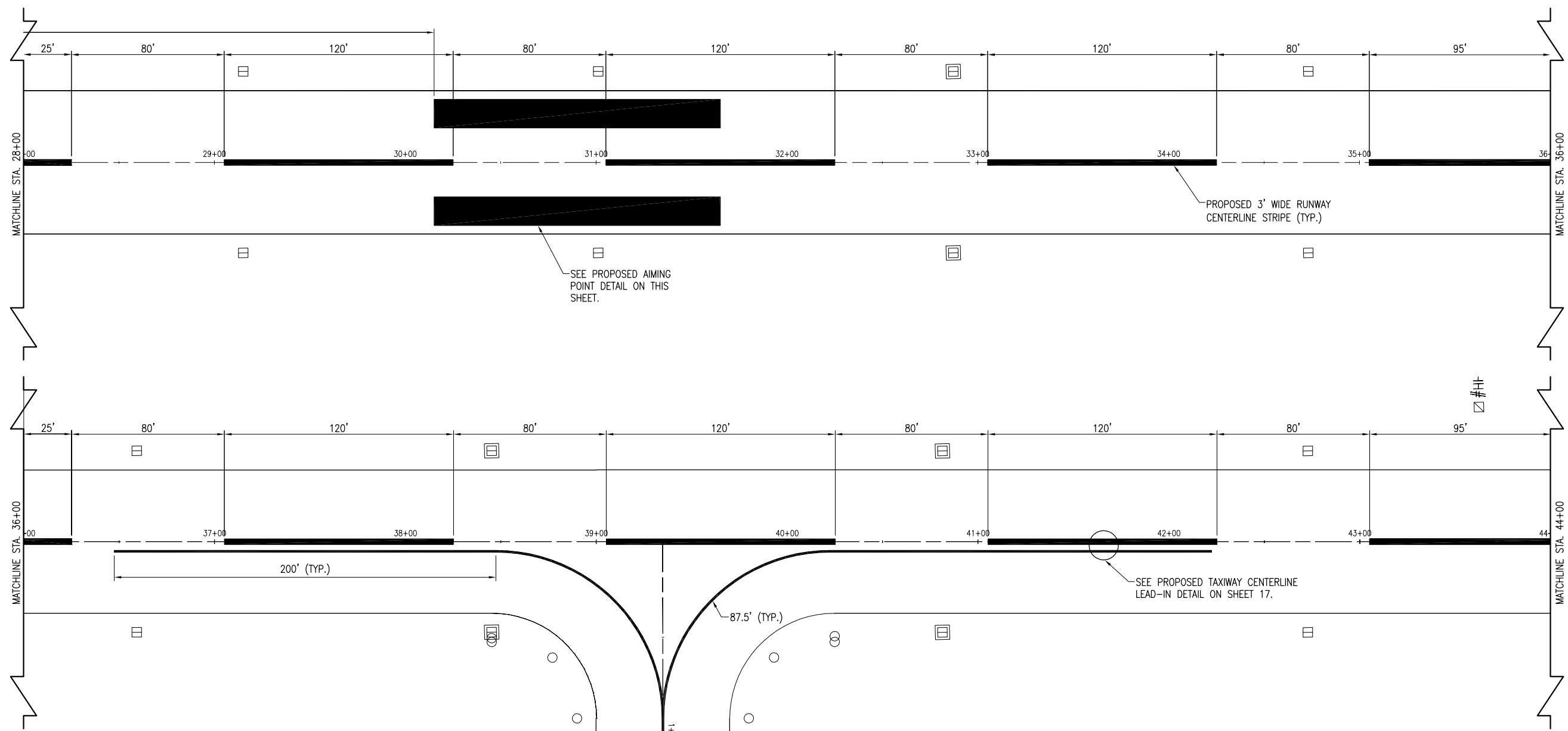
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Filename R-151MRK.DWG	DRAWN	BAK	10/28/09
Scale SCALE: 1" = 30'	REVIEWED	CAH	xx/xx/xx
Date 01/07/10			

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 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Offices Nationwide

**PROPOSED P.F.C.
 ON RUNWAY 18-36**

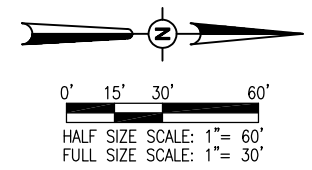
PROPOSED
 MARKING PLAN

STA. 19+95 TO STA. 28+00



PROPOSED 3' WIDE RUNWAY CENTERLINE STRIPE (TYP.)

SEE PROPOSED TAXIWAY CENTERLINE LEAD-IN DETAIL ON SHEET 17.



LEGEND

	EXISTING PAVEMENT
	EXISTING MARKING
	PROPOSED MARKING

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DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

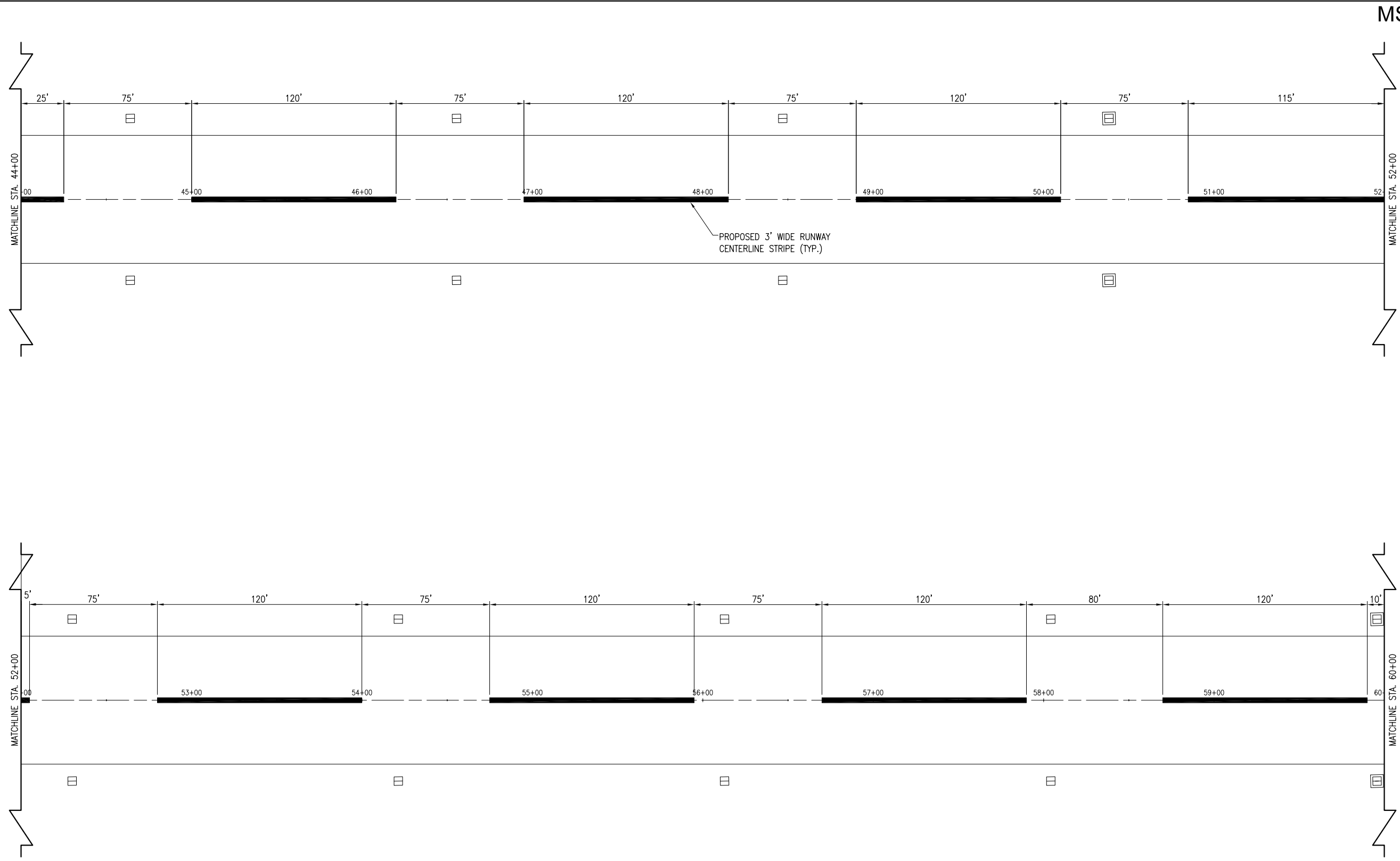
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Filename R-151MRK.DWG	DRAWN	BAK	10/28/09
Scale SCALE: 1" = 30'	REVIEWED	CAH	xx/xx/xx
Date 01/07/10			

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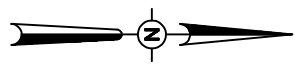
**PROPOSED P.F.C.
ON RUNWAY 18-36**

PROPOSED MARKING PLAN
STA. 28+00 TO STA. 44+00

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MS004



0' 15' 30' 60'
 HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'

LEGEND

	EXISTING PAVEMENT
	PROPOSED MARKING

DATE	REVISION	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

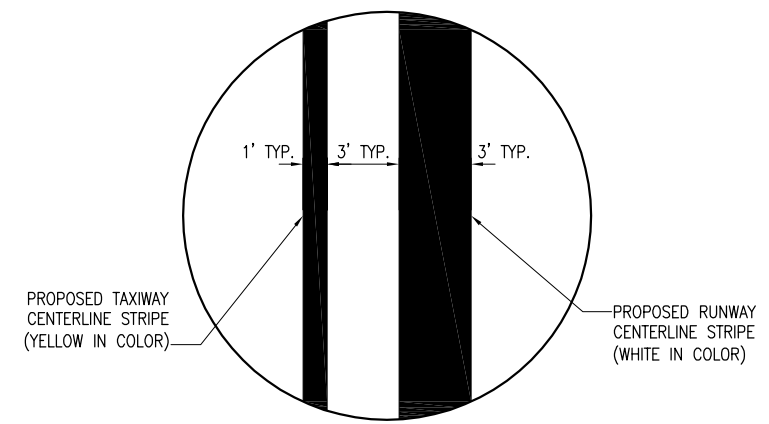
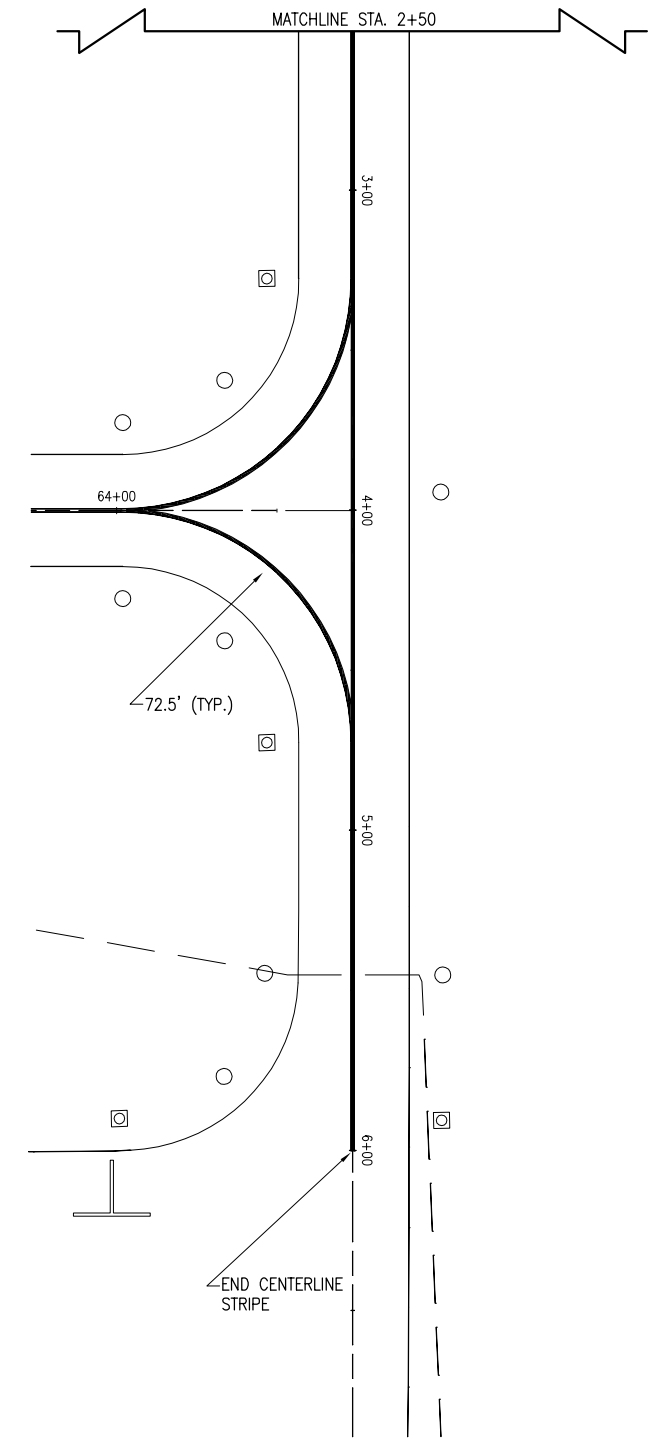
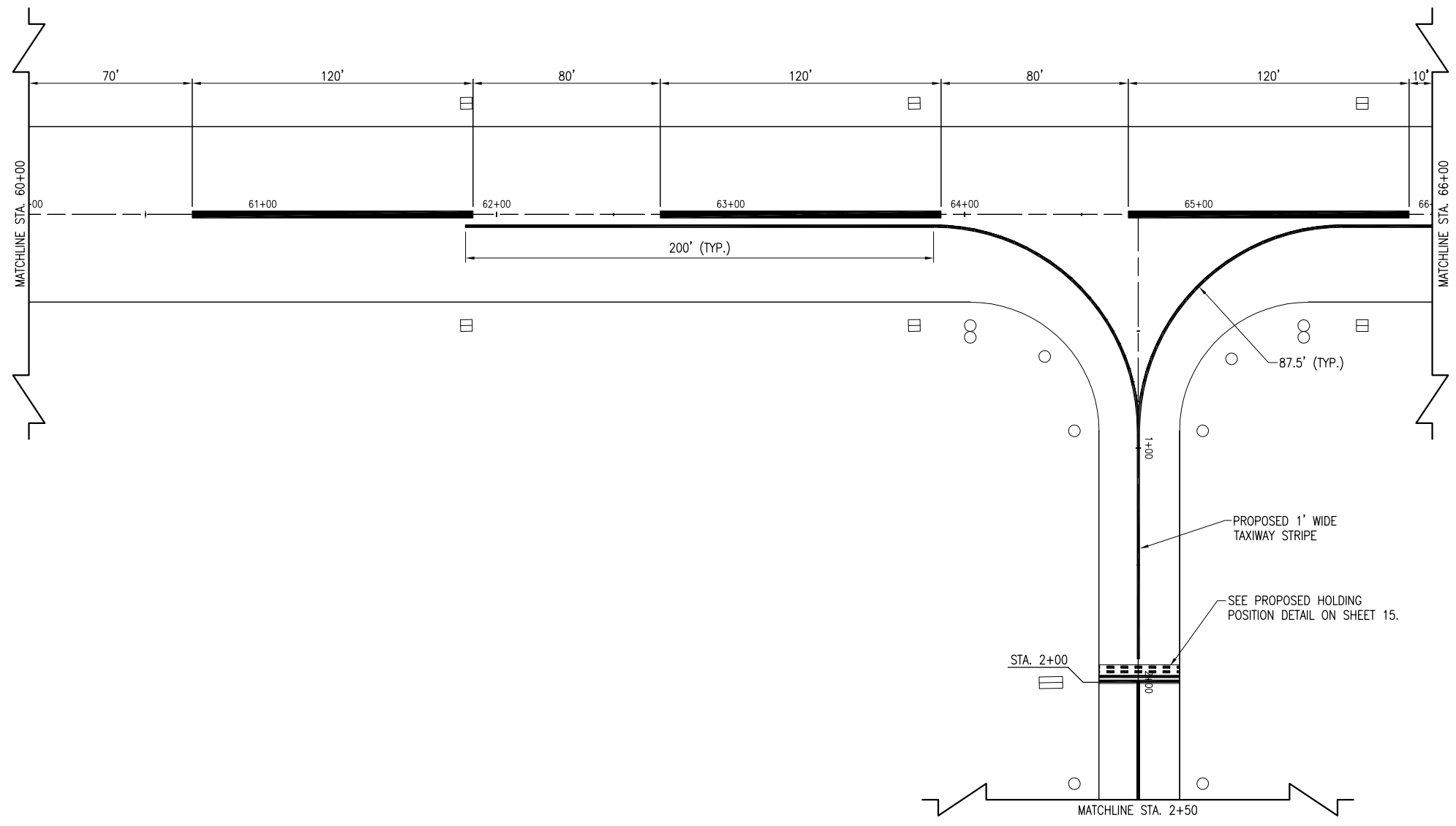
IL. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10

Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
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Scale SCALE: 1" = 30'	REVIEWED	CAH	xx/xx/xx
Date 01/07/10			

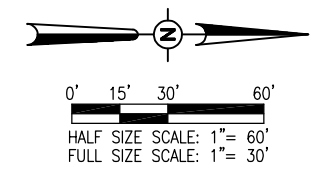
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**PROPOSED P.F.C.
 ON RUNWAY 18-36**

PROPOSED
 MARKING PLAN
 STA. 44+00 TO STA. 60+00



PROPOSED TAXIWAY CENTERLINE LEAD-IN DETAIL
"NOT TO SCALE"

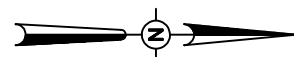
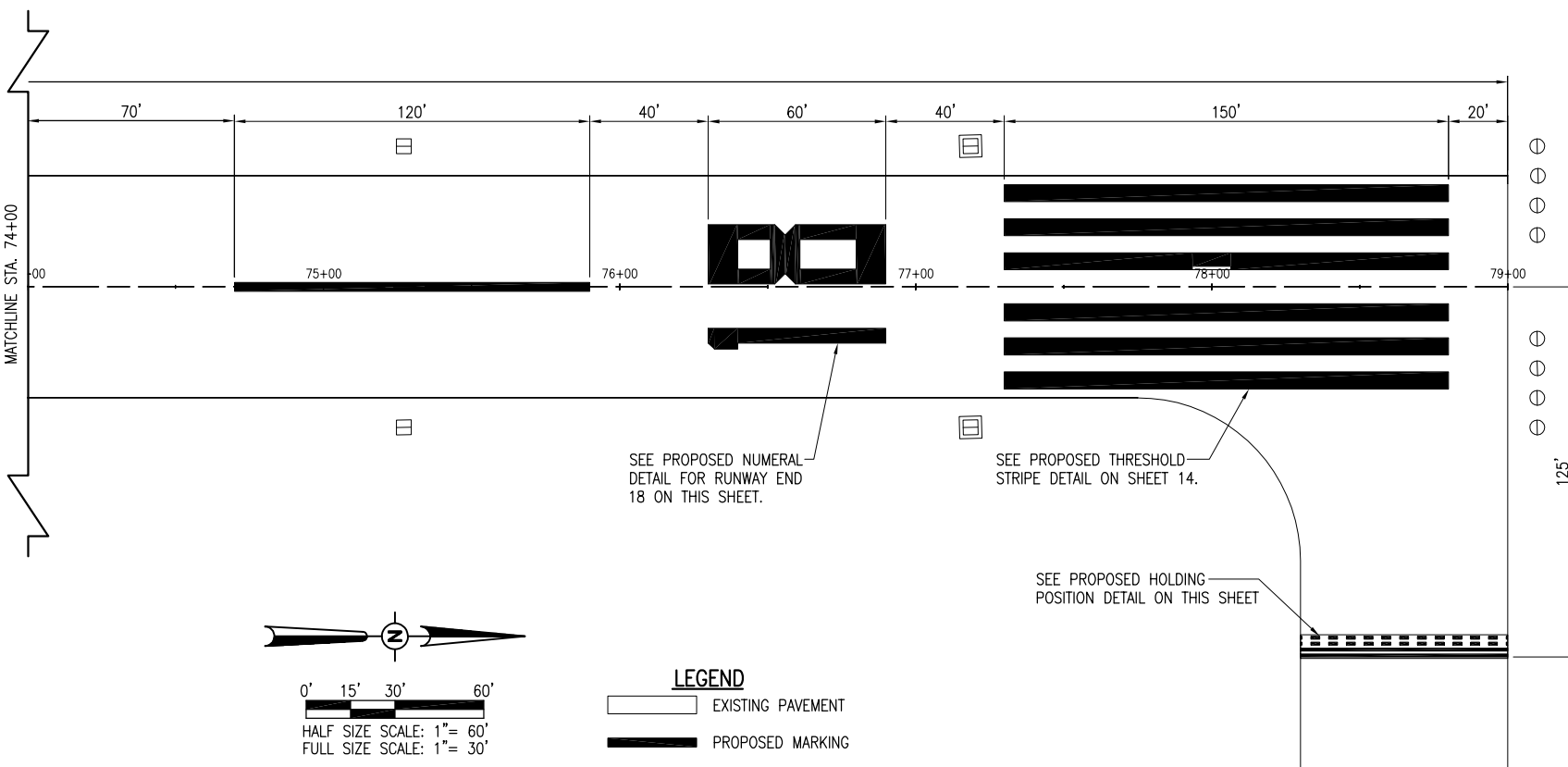
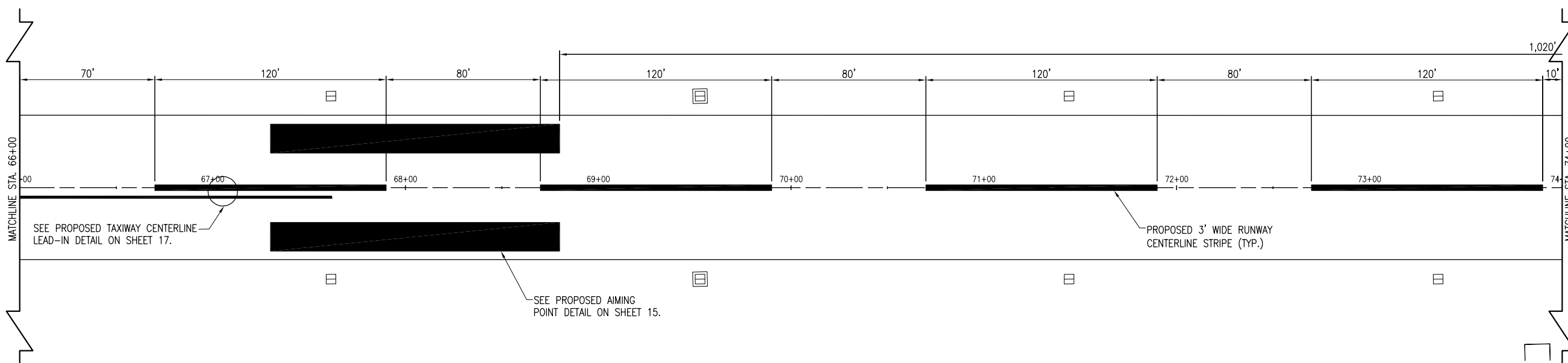


LEGEND

	EXISTING PAVEMENT
	EXISTING MARKING
	PROPOSED MARKING

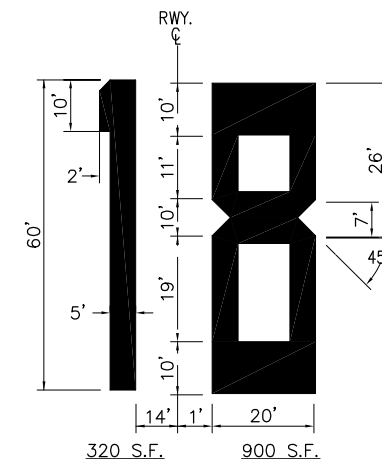
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PROPOSED P.F.C. ON RUNWAY 18-36	PROPOSED MARKING PLAN STA. 60+00 TO STA. 66+00	HANSON Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Chicago Nationwide	Hanson Project No. 09A0127D_0800 Filename R-151MRK.DWG Scale SCALE: 1" = 30' Date 01/07/10	LAYOUT JEO 10/28/09 DRAWN BAK 10/28/09 REVIEWED CAH xx/xx/xx	ROBERT F. TRACY MUNICIPAL AIRPORT MT. STERLING, ILLINOIS I.L. PROJ.: 163-3957 A.I.P. PROJ.: 3-17-0147-B10
17		17 of 18 sheets			

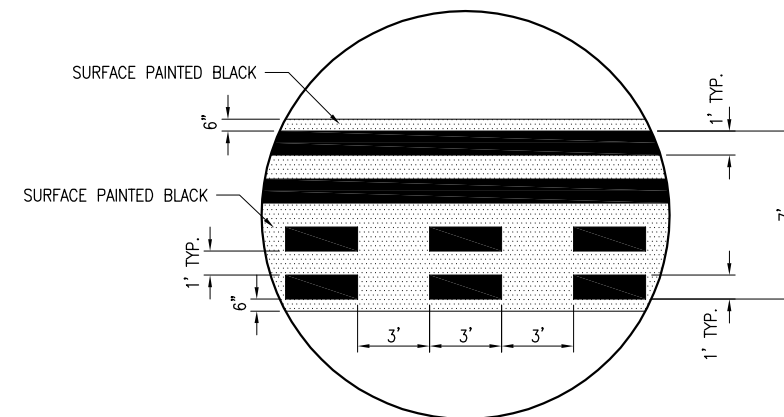


0' 15' 30' 60'
 HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'

LEGEND
 EXISTING PAVEMENT
 PROPOSED MARKING



PROPOSED NUMERAL DETAIL FOR RUNWAY END 18
 "NOT TO SCALE"



PROPOSED HOLDING POSITION DETAIL
 "NOT TO SCALE"

REVISION	DATE	BY

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

Hanson Project No. 09A0127D_0800	LAYOUT	JEO	10/28/09
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**PROPOSED P.F.C.
 ON RUNWAY 18-36**
 PROPOSED MARKING PLAN
 STA. 66+00 TO STA. 79+00

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