

F.A.P. ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	1
ILLINOIS PROJECT C-91-810-09			M-ARA-9003 (421)	
CONTRACT NO. 63351				

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
ST. CHARLES ROAD (FAU 1397)
IL ROUTE 53 (COLUMBINE AVENUE) TO GRACE STREET / RESURFACING**

**PROJECT NO.: M-ARA-9003 (421)
SECTION NO.: 09-00153-00-RS
JOB NO.: C-91-810-09
VILLAGE OF LOMBARD, ILLINOIS
DuPAGE COUNTY**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF LOMBARD

TRAFFIC DATA
ST.CHARLES ROAD

POSTED AND DESIGN SPEED

WESTBOUND

IL ROUTE 53 TO WEST ROAD = 35 MPH
CRESCENT BOULEVARD TO GARFIELD STREET = 25 MPH
GARFIELD STREET TO MARTHA STREET = 30 MPH

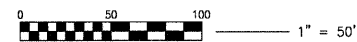
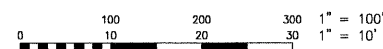
EASTBOUND

IL 53 TO CRESCENT BOULEVARD = 35 MPH
CRESCENT BOULEVARD TO GARFIELD STREET = 25 MPH
GARFIELD STREET TO MARTHA STREET = 30 MPH

2008 ADT = 16,000 VPD

DESIGN DESIGNATION
COLLECTOR

BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-001121
EXPIRES 4/30/2011



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.

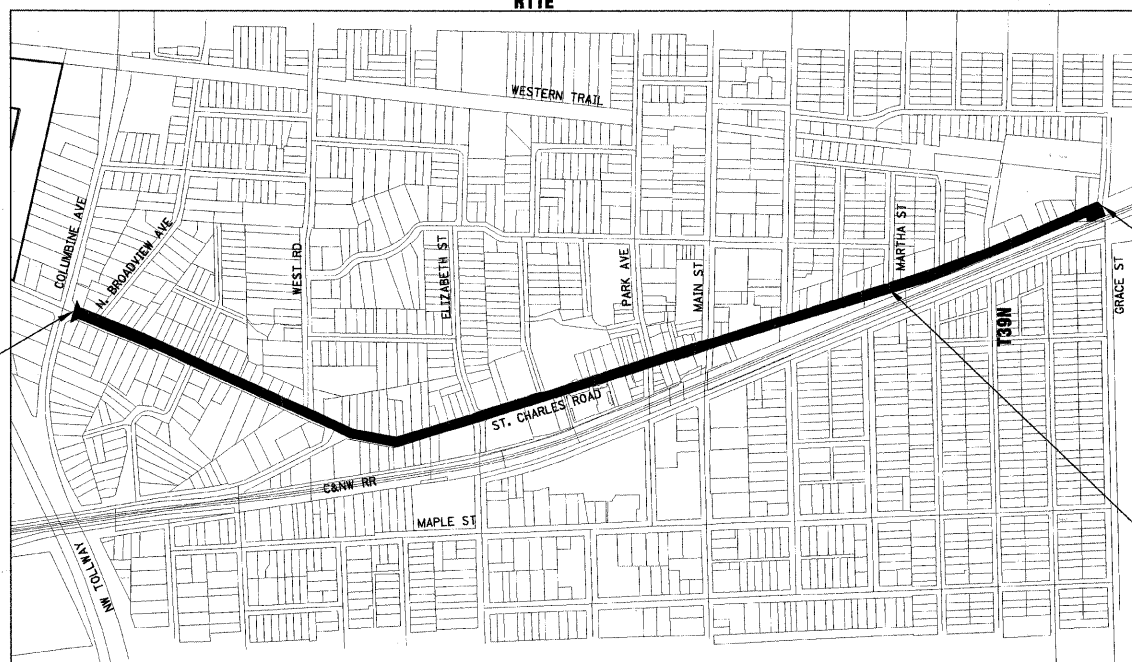
J.U.L.I.E. DESIGN STAGE REQUEST
D.G. No. A1552018



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:

COUNTY = DuPAGE
CITY-TOWNSHIP = LOMBARD-YORK TOWNSHIP
SEC. & 1/4 SEC. NO. = 07-08
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 63351



R11E 3rd PRIME MERIDIAN
YORK TOWNSHIP
LOCATION MAP
NOT TO SCALE

**GROSS LENGTH OF IMPROVEMENT = 7303 LIN.FT. OR 1.383 MILES
NET LENGTH OF IMPROVEMENT = 5825 LIN. FT. OR 1.103 MILES**



Madison, Wisconsin
608.241.5481

Burlington, Wisconsin
262.763.7834

Crystal Lake, Illinois
815.459.1260

Grayslake, Illinois
847.223.5088

Plainfield, Illinois
815.609.7425

Mokena, Illinois
708.478.2090

DeKalb, Illinois
815.787.3111

Chicago, Illinois
312.578.0500

Itasca, Illinois
630.773.1870



OMISSION ENDS
PROJECT ENDS
STA 74+02

OMISSION BEGINS
STA. 59+25



LOCATION OF SECTION INDICATED THUS: [Symbol]

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED November 11, 2009

William J. Mueller
VILLAGE PRESIDENT LOMBARD

PASSED JANUARY 29, 2010

Chris DeStefano
DISTRICT 1 ENGINEER OF
LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW FEBRUARY 3, 2010

Diane M. O'Keefe
DEPUTY DIRECTOR OF
HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
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PROJECT NO: 090317.40 DATE: 06-26-09

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 I.D.O.T. FIELD ENGINEER: MARLIN SOLOMON, P.E. 847-705-4407
 SCHAUMBURG, IL.

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602401-02	MANHOLE, TYPE A
602701-02	MANHOLE STEPS
604001-03	FRAMES AND LIDS, TYPE 1
604006-04	FRAME AND GRATE, TYPE 3
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701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
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SANITARY 1	SANITARY MANHOLE

SUMMARY OF QUANTITIES

PAY ITEM NO.	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY 1000
20101100	TREE TRUNK PROTECTION	EACH	75
** 20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	18
** 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	45
25200200	SUPPLEMENTAL WATERING	UNIT	2
** 25200700	SODDING, SPECIAL	SQ YD	45
28000510	INLET FILTERS	EACH	55
** 31101100	SUB-BASE GRANULAR MATERIAL, TYPE B	CU YD	18
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	30
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3,168
40600300	AGGREGATE (PRIME COAT)	TON	64
** 40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	5
** 40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75 N50	TON	1,049
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	209
40603340	HMA SURFACE COURSE, MIX "D", N70	TON	3,154
42001300	PROTECTIVE COAT	SQ YD	1,699
** 42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	8,259
** 42400800	DETECTABLE WARNINGS	SQ FT	422
44000155	HMA SURFACE REMOVAL, 1 1/2"	SQ YD	11,706
44000162	HMA SURFACE REMOVAL, 3 1/4"	SQ YD	19,977
44000600	SIDEWALK REMOVAL	SQ FT	8,259
** 44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,507
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	208
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	795
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	2,106
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	7,000
** 56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	9
60250200	CATCH BASINS TO BE ADJUSTED	EACH	6
** 60250300	CATCH BASINS TO BE ADJUSTED (SPECIAL)	EACH	4
60255500	MANHOLES TO BE ADJUSTED	EACH	9
** 60255600	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH	7
60260100	INLETS TO BE ADJUSTED	EACH	12
** 60260200	INLETS TO BE ADJUSTED (SPECIAL)	EACH	25
60262700	INLETS TO BE RECONSTRUCTED	EACH	2
** 60265800	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	EACH	23
** 60266610	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	5
** 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	42
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	50
60500040	REMOVING MANHOLES	EACH	1
67100100	MOBILIZATION	L SUM	1
** 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
** 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
** 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
* ** 78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	250
* ** 78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	14,043
* ** 78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	3,013
* ** 78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	FOOT	643
* ** 78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"	FOOT	454
* ** 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4
* ** 88600600	DETECTOR LOOP REPLACEMENT	FOOT	75
* 89502200	MODIFY EXISTING CONTROLLER	EACH	4
** 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2,599
** X0321556	SANITARY MANHOLES TO BE ADJUSTED	EACH	15
** X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51
** XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	160
** XX003668	PRECONSTRUCTION VIDEO TAPING	L SUM	1
* ** XX005723	VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	4
** XX005972	STAMPED ASPHALT CROSSWALK	SQ YD	234
* ** XX007246	ELECTRIC CABLE IN CONDUIT NO. 18 3C	FOOT	2,327
** XX007509	REMOVE AND REPLACE SANITARY MANHOLES, 4' DIAMETER, 0-8 FEET DEEP	EACH	10
** XX007510	REMOVE AND REPLACE SANITARY MANHOLES, 4' DIAMETER, 8-12 FEET DEEP	EACH	9
** XX008264	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SPECIAL)	FOOT	1,285

* DENOTE SPECIALTY ITEM

** INDICATES ITEM COVERED BY SPECIAL PROVISIONS

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DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED - PER IDOT FINAL COMMENTS 01-18-10

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS, SUMMARY OF QUANTITIES

SCALE: PROJECT NO: 090317 STA. TO STA.

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	2
CONTRACT NO. 63351				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

1. ALL REFERENCES TO STANDARD SPECIFICATIONS IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007 AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2010.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS, IDOT STANDARD DRAWINGS, AND VILLAGE OF LOMBARD STANDARD DRAWINGS" INCLUDED IN THE CONTRACT DOCUMENTS.
3. IN THE GENERAL NOTES, ALL REFERENCES TO ENGINEER SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.

STAKING

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

1. ALL TREES ARE DESIGNATED TO BE SAVED UNLESS OTHERWISE NOTED ON THE PLANS, AND SHALL BE PROTECTED IN ACCORDANCE WITH THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.

UTILITIES

1. PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE OF LOMBARD DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER, AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

3. THE VILLAGE OF LOMBARD WILL LOCATE EXISTING SANITARY AND WATER SERVICES TO THE INDIVIDUAL RESIDENCES WITHIN THE PROJECT'S LIMITS. THESE SERVICES WILL BE LOCATED ONE TIME ONLY. FROM THEN ON THE CONTRACTOR SHALL DOCUMENT AND BE RESPONSIBLE FOR MAINTAINING KNOWLEDGE OF THE LOCATIONS OF THESE SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AS-BUILT KNOWLEDGE OF THE LOCATION (INCLUDING ELEVATIONS) OF THE NEWLY INSTALLED SERVICES AND MAINS. THE VILLAGE WILL NOT BE RESPONSIBLE FOR LOCATING NEW MAINS OR SERVICES FOR THE CONTRACTOR. THIS IS IN EFFECT UNTIL FINAL PAYMENT. FOR THE DURATION OF THE CONTRACT, THE CONTRACTOR SHALL MAKE THE AS-BUILT UNDERGROUND UTILITY INFORMATION AVAILABLE TO THE ENGINEER WHENEVER REQUESTED. IN THE EVENT OF A "JULIE" CALL WITHIN THE PROJECT LIMITS, THE ENGINEER WILL NOTIFY THE CONTRACTOR TO MARK ANY UTILITIES STILL UNDER THE CONTRACTORS RESPONSIBILITY.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ALL UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE OF LOMBARD. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

5. THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE OF LOMBARD IN ALL UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

6. ALL MANHOLES, CATCH BASINS, AND VALVE VAULTS SHALL BE MORTARED WITH HYDRAULIC CEMENT AS SHOWN ON THE DETAILS ON THE INSIDE AND OUTSIDE AT ALL STRUCTURE JOINTS BETWEEN BARREL, CONE, AND FLAT TOP SECTIONS. ADJUSTING RINGS SHALL BE MORTARED ON THE OUTSIDE AROUND JOINTS TO PREVENT INFILTRATION PROVIDING THEY ARE OUTSIDE OF PAVED AREAS. ONLY NON-PREFORMED MASTIC SHALL BE USED BETWEEN THE FRAME, RINGS, AND TOP OF CONE OR FLAT TOP. (PREFORMED MASTIC --- EZ STICK, RUB-R-NECK, ETC IS NOT ALLOWED ABOVE THE CONE OR FLAT TOP)

7. GRADATION OF TRENCH BACKFILL MATERIAL SHALL BE CA-6, AND SHALL BE PLACED IN UNIFORM LAYERS NOT EXCEEDING 12 INCHES (LOOSE MEASURE) AND COMPACTED WITH MECHANICAL EQUIPMENT TO 95% OF STANDARD PROCTOR DENSITY. PIPE BEDDING SHALL BE A MINIMUM OF 4 INCHES THICK.

8. ALL FRAMES WITH SELF SEALING CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF MANHOLES, CATCH BASIN, INLET, VALVE VAULT, OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM". ALL LIDS TO BE USED ON SANITARY SEWER STRUCTURES SHALL BEAR THE WORD "SANITARY". ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURES SHALL BEAR THE WORD "WATER". ALL CURB BOXES SHALL SAY "DUMP NO WASTE!" OR "DRAINS TO RIVERS" OR SIMILAR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FRAME AND GRATE OR FRAME AND CLOSED LID PROVIDED.

9. WHENEVER DURING CONSTRUCTION OPERATIONS LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.

10. ALL AUXILIARY VALVES, FRAMES, GRATES, LIDS AND WATER SERVICE BOXES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ALL HYDRANTS TO BE REMOVED SHALL BE REMOVED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL CONTACT THE VILLAGE OF LOMBARD TO DETERMINE IF THE VILLAGE WISHES TO SALVAGE THE HYDRANTS, FRAMES, OR LIDS. THE CONTRACTOR WILL BE RESPONSIBLE TO EITHER RETURN THE HYDRANTS, FRAMES, OR LIDS TO PUBLIC WORKS OR DISPOSE OF THEM.

11. ALL EXISTING AND PROPOSED SEWER DAMAGED BY THE CONSTRUCTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.

12. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.

13. ALL PROPOSED UTILITY STRUCTURES SHALL INCLUDE AN EXTERNAL CHIMNEY SEAL UNLESS THEY FALL IN THE PAVEMENT AND ARE ADJUSTED TO FINAL GRADE WITH CLASS SI CONCRETE OR HOT-MIX ASPHALT SURFACE OR BINDER FULLY COVERING THE RINGS AND CASTING. THE COST OF THE CHIMNEY SEAL SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UTILITY STRUCTURE BEING INSTALLED.

PAVING AND CURB & GUTTER

1. THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
2. BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
3. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOP SOIL, SOD PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
4. LONGITUDINAL PAVING JOINTS SHALL BE PLACED TO PROVIDE FOR A MINIMUM 6" OFFSET FROM ANY PROPOSED LANE LINE MARKINGS.

STORM & SANITARY SEWER

1. THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
2. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
3. ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.
4. ALL TYPE 3 FRAME AND GRATES FOR CATCH BASINS AND INLETS SHALL BE NEENAH R-3279-1 OR EAST JORDAN IRON WORKS 7220 OR APPROVED EQUAL.

SIGNING, STRIPING & LANDSCAPING

1. THOSE SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL SIGNS WHICH ARE DAMAGED DURING CONSTRUCTION OPERATIONS BEYOND REPAIR SHALL BE REPLACED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.
2. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
3. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR, SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

MISCELLANEOUS

1. ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION, EXCEPT FOR PERIODS OF SHORT DURATION (4 HOURS OR LESS). THE COST TO PROVIDE AND MAINTAIN ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM "AGGREGATE FOR TEMPORARY ACCESS."
2. ALL DRIVEWAY APRONS SHALL BE REPLACED WITH MATERIAL OF THE SAME KIND AS THE EXISTING APRON, EXCEPT FOR EXISTING AGGREGATE DRIVEWAY APRONS WHICH SHALL BE REPLACED WITH HOT-MIX ASPHALT DRIVEWAY APRONS.
3. DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
4. ALL WASTE MATERIAL SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY AT THE CONTRACTOR'S EXPENSE.
5. THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING NO. 701801 WHEN CLOSING SIDEWALKS.
6. ESTIMATED LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
7. IF, IN THE ENGINEER'S OPINION, THE WORK SHOWN ON THE PLANS IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR II BARRICADE USED. (ONE (1) WEIGHTED BAG ACROSS EACH BOTTOM RAIL.)
9. THE FOLLOWING RATES OF APPLICATION HAVE BEEN ASSUMED IN CALCULATING PLAN QUANTITIES:

BITUMINOUS MATERIALS (PRIME COAT)	0.1 GAL/SQ YD
HOT-MIX ASPHALT SURFACE COURSE	112 LBS/SQ YD/INCH
POLYMERIZED LEVELING BINDER (MACHINE METHOD)	105 LBS/SQ YD/INCH
10. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/8" (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1" (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3" (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 3:1 (H:V).

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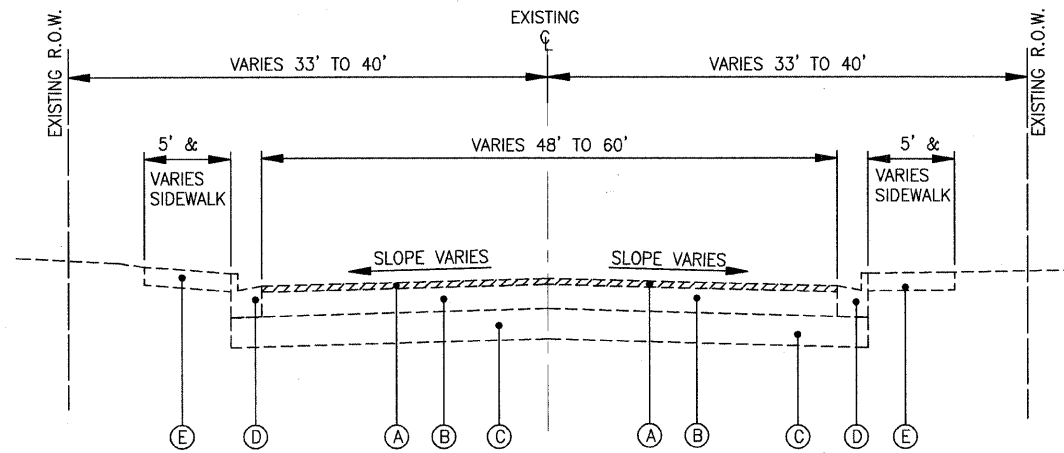
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DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
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DATE - 06-26-09	REVISED -

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DEPARTMENT OF TRANSPORTATION**

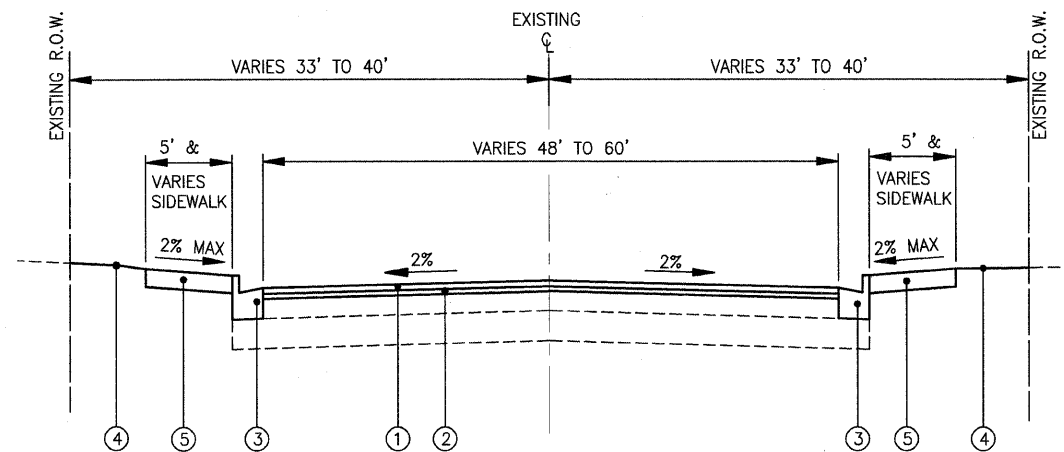
GENERAL NOTES

SCALE: PROJECT NO: 090317 STA. TO STA.

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DUPAGE	36	3
CONTRACT NO. 63351				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-RA-9003(421)				



EXISTING TYPICAL SECTION
STA. 1+00 TO STA. 36+00
ST. CHARLES ROAD
 (NOT TO SCALE)



PROPOSED TYPICAL SECTION
STA. 1+00 TO STA. 36+00
ST. CHARLES ROAD
 (NOT TO SCALE)

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE REMOVAL - 3 1/2"
- (B) EXISTING HOT-MIX ASPHALT PAVEMENT, VARIES 7.5" TO 12"
- (C) EXISTING AGGREGATE BASE COURSE
- (D) EXISTING CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING SIDEWALK (PCC OR BRICK PAVERS)
- (F) EXISTING HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- (G) EXISTING PCC PAVEMENT, VARIES 6" TO 11"
- (H) CONCRETE PAVERS, 6CM
- (I) SAND CUSHION, 1"
- (J) SUB BASE GRANULAR MATERIAL, TYPE B, 6"
- (K) CONCRETE EDGING, 12" WIDE
- (L) SUB BASE GRANULAR MATERIAL, TYPE B, 4"
- (M) GEOTECHNICAL STABILIZATION FABRIC

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/4"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12 (AS DETERMINED BY THE ENGINEER)
- (4) TOPSOIL FURNISH AND PLACE, 4" (AS DETERMINED BY THE ENGINEER) SODDING, SPECIAL (AS DETERMINED BY THE ENGINEER)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK, 5" , SPECIAL OR BRICK SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- (7) STRIP REFLECTIVE CRACK CONTROL

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

MIX TYPE	AIR VOIDS @ Ndes
ROADWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 (IL-9.5mm)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES, 12 INCH, (HMA BINDER, IL-19mm)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LB/SY-IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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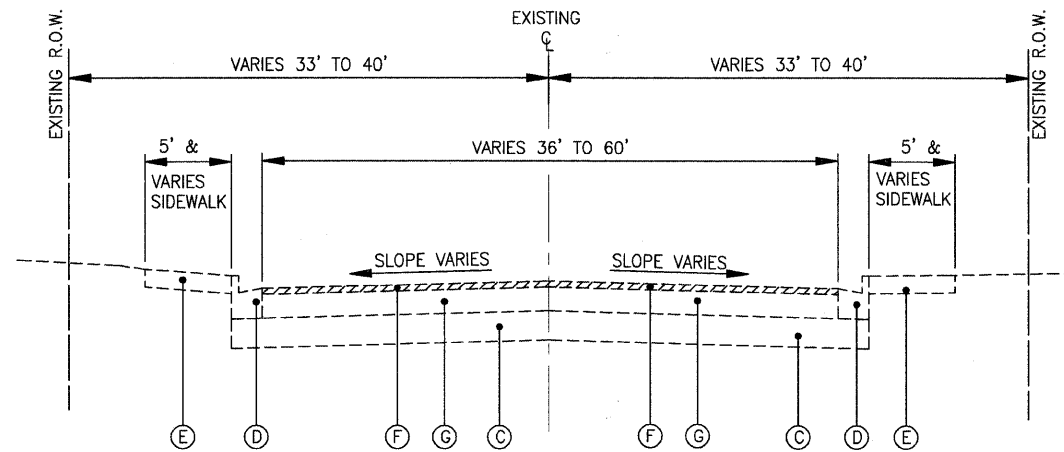


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DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
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DATE - 06-26-09	REVISED - PER IDOT FINAL COMMENTS 01-18-10

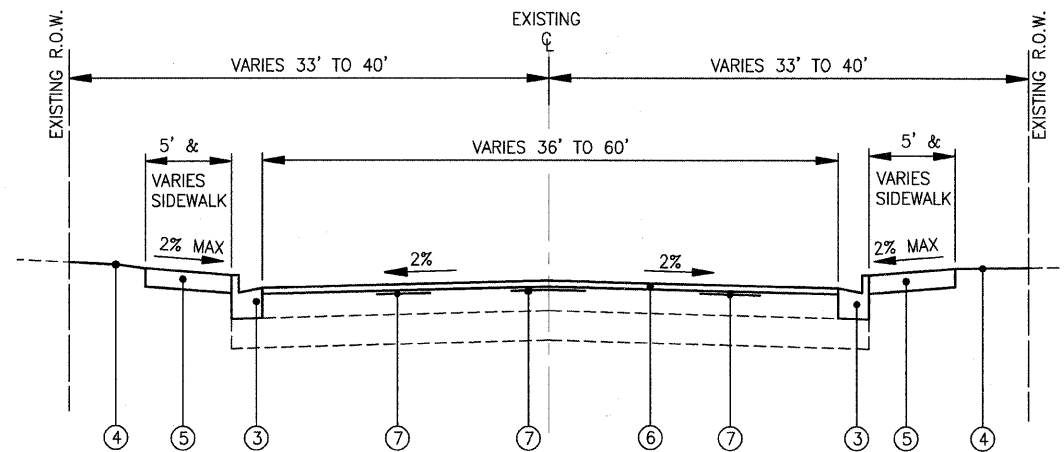
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DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD
EXISTING AND PROPOSED TYPICAL SECTIONS

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						CONTRACT NO. 63351	
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)	



EXISTING TYPICAL SECTION
STA. 36+00 TO STA. 59+25
ST. CHARLES ROAD
 (NOT TO SCALE)



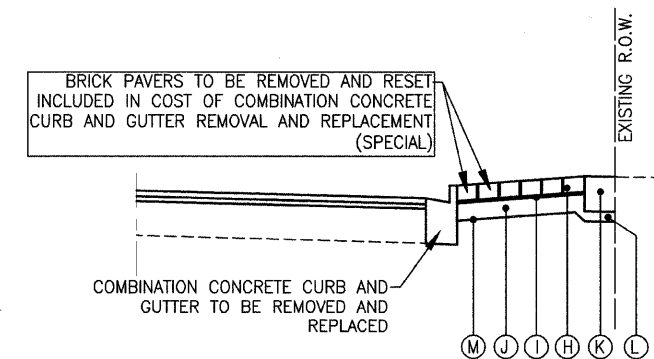
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STA. 36+00 TO STA. 59+25
ST. CHARLES ROAD
 (NOT TO SCALE)

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE REMOVAL - 3 1/4"
- (B) EXISTING HOT-MIX ASPHALT PAVEMENT, VARIES 7.5" TO 12"
- (C) EXISTING AGGREGATE BASE COURSE
- (D) EXISTING CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING SIDEWALK (PCC OR BRICK PAVERS)
- (F) EXISTING HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- (G) EXISTING PCC PAVEMENT, VARIES 6" TO 11"
- (H) CONCRETE PAVERS, 6CM
- (I) SAND CUSHION, 1"
- (J) SUB BASE GRANULAR MATERIAL, TYPE B, 6"
- (K) CONCRETE EDGING, 12" WIDE
- (L) SUB BASE GRANULAR MATERIAL, TYPE B, 4"
- (M) GEOTECHNICAL STABILIZATION FABRIC

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/4"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12 (AS DETERMINED BY THE ENGINEER)
- (4) TOPSOIL FURNISH AND PLACE, 4" (AS DETERMINED BY THE ENGINEER) SODDING, SPECIAL (AS DETERMINED BY THE ENGINEER)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK, 5" , SPECIAL OR BRICK SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- (7) STRIP REFLECTIVE CRACK CONTROL



COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT (SPECIAL)
 (NOT TO SCALE)

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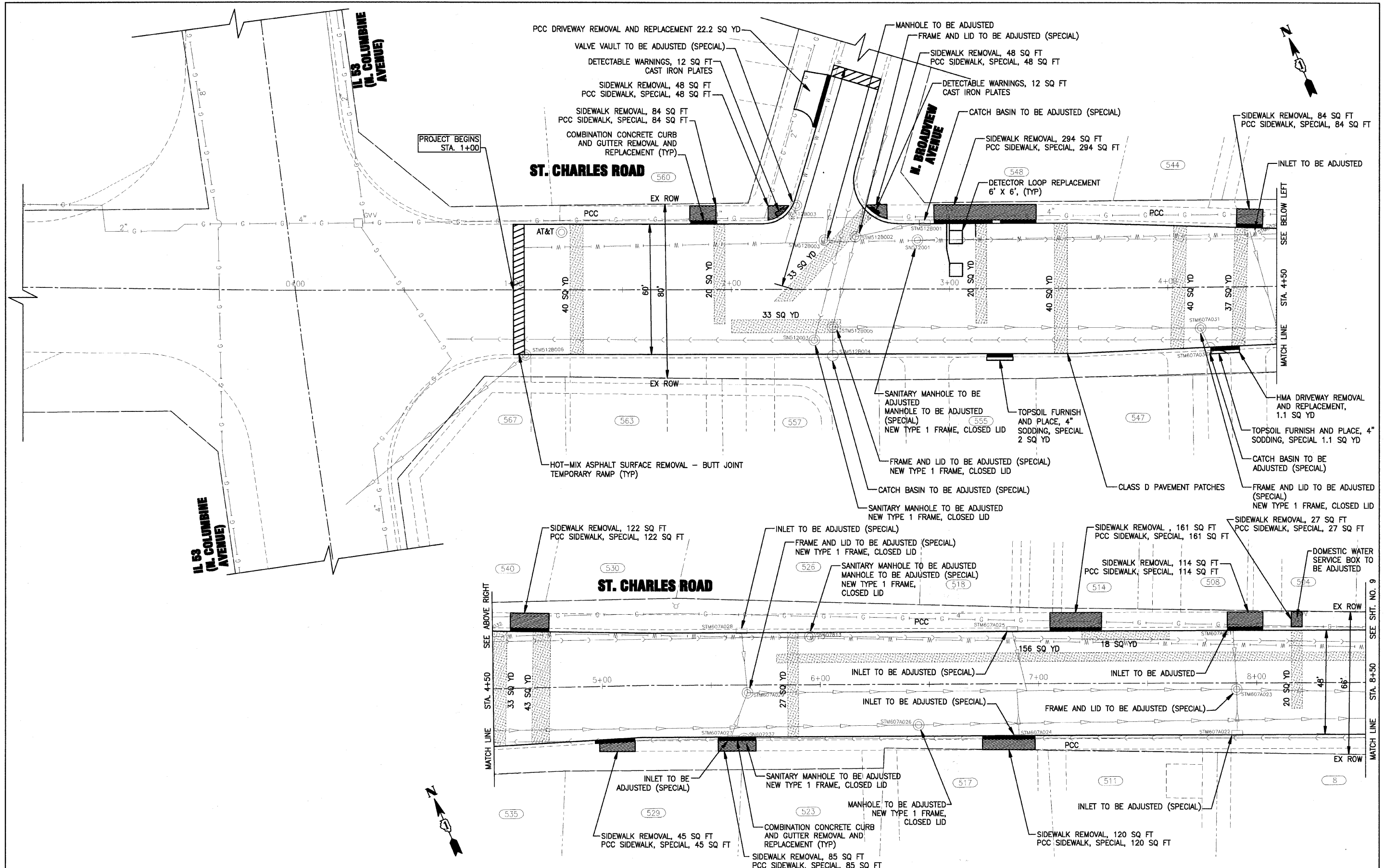
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD
EXISTING AND PROPOSED TYPICAL SECTIONS

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63351				
SCALE: PROJECT NO: 090317 STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				

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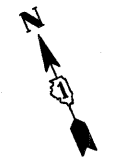
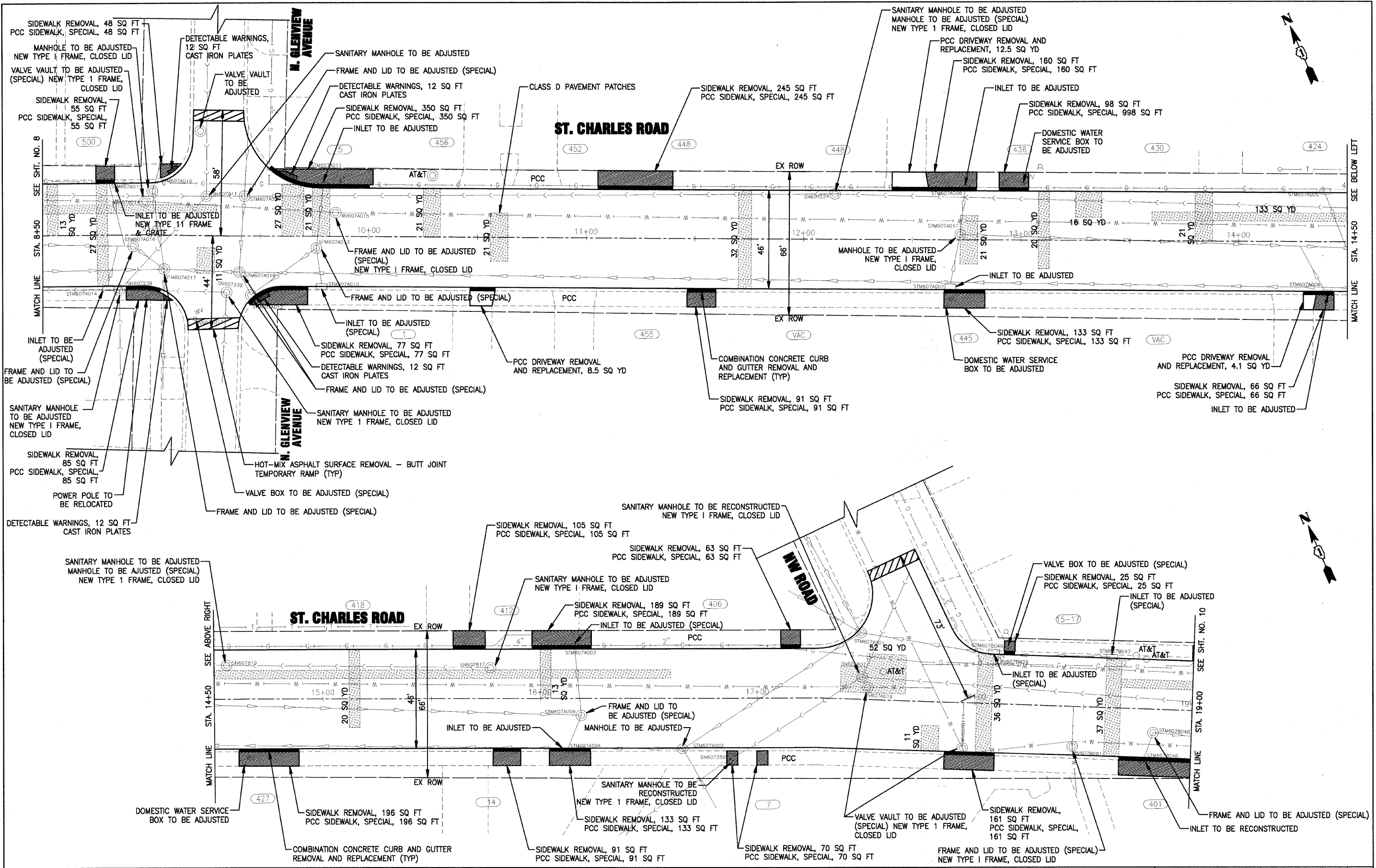
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ST. CHARLES ROAD PLAN

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CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED - PER IDOT FINAL REVIEW 01-18-10

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 0+00 TO STA. 8+50

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 8
CONTRACT NO. 63351			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-8003(421)	



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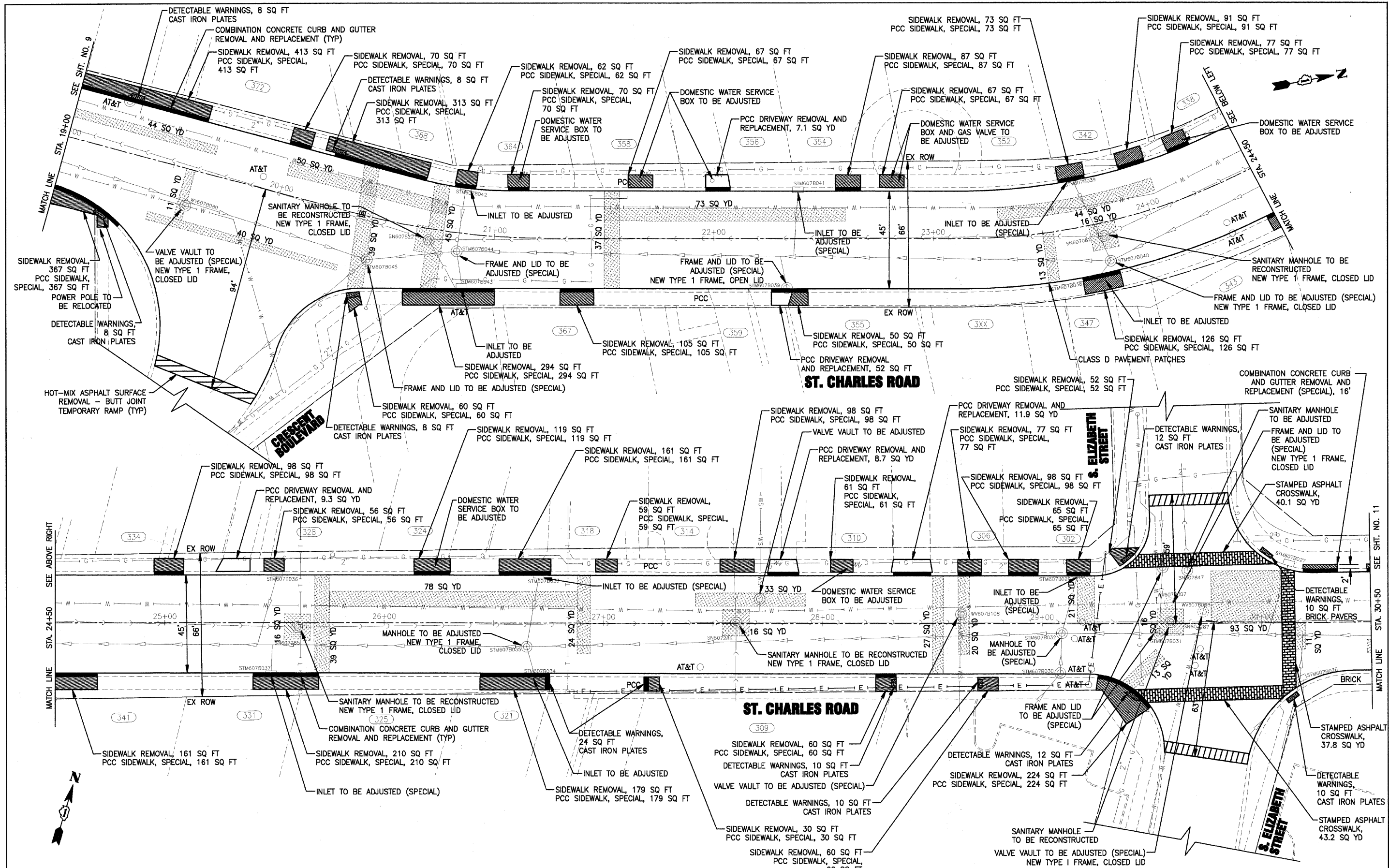
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ST. CHARLES ROAD PLAN

SCALE: HORIZ. 1"=20'	PROJECT NO: 090317	STA. 8+50	TO STA. 19+00	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT W-ARA-9003(421)
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CONTRACT NO. 63351				



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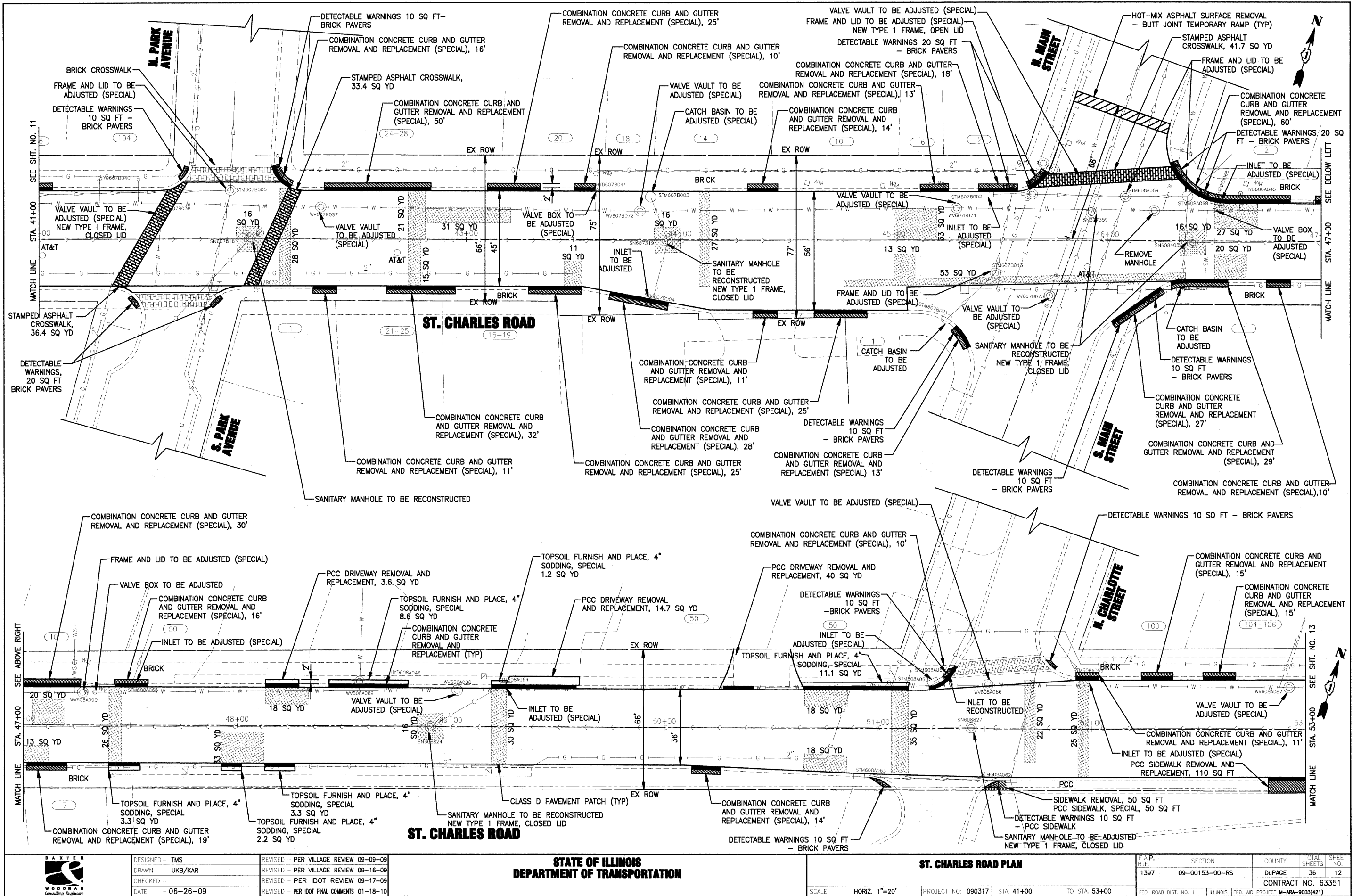
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STATE OF ILLINOIS
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ST. CHARLES ROAD PLAN

SCALE: HORIZ. 1"=20'
 PROJECT NO: 090317 STA. 19+00 TO STA. 30+50

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DUPAGE	TOTAL SHEETS 36	SHEET NO. 10
CONTRACT NO. 63351				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				



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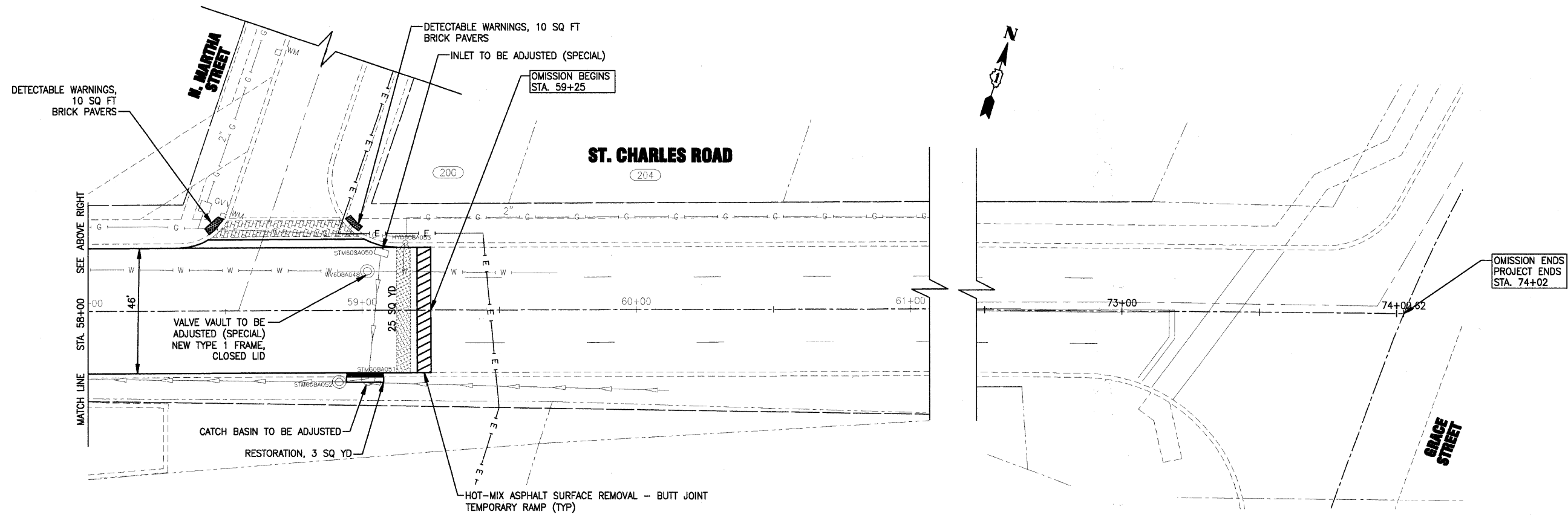
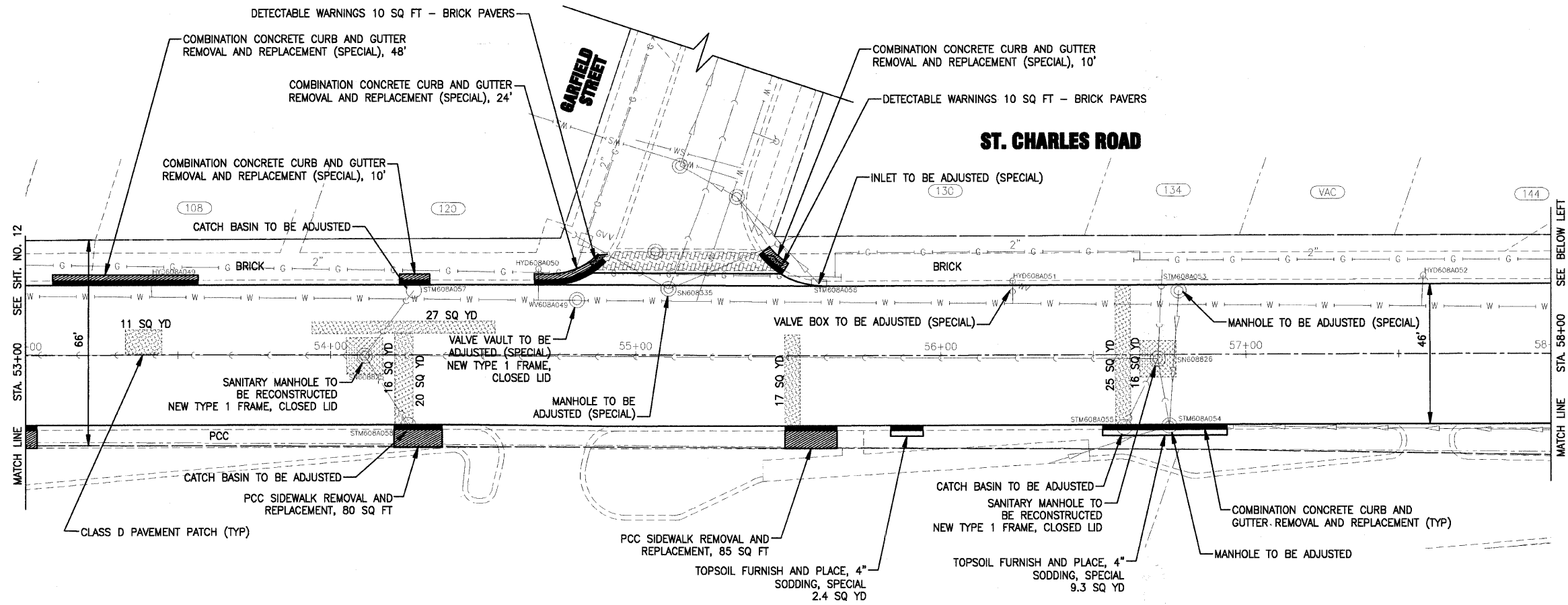
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ST. CHARLES ROAD PLAN

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 41+00 TO STA. 53+00

F.A.P. R.T.E. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 12
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD PLAN

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 53+00 TO STA. 61+00

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 13
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-AM-9003(421)				

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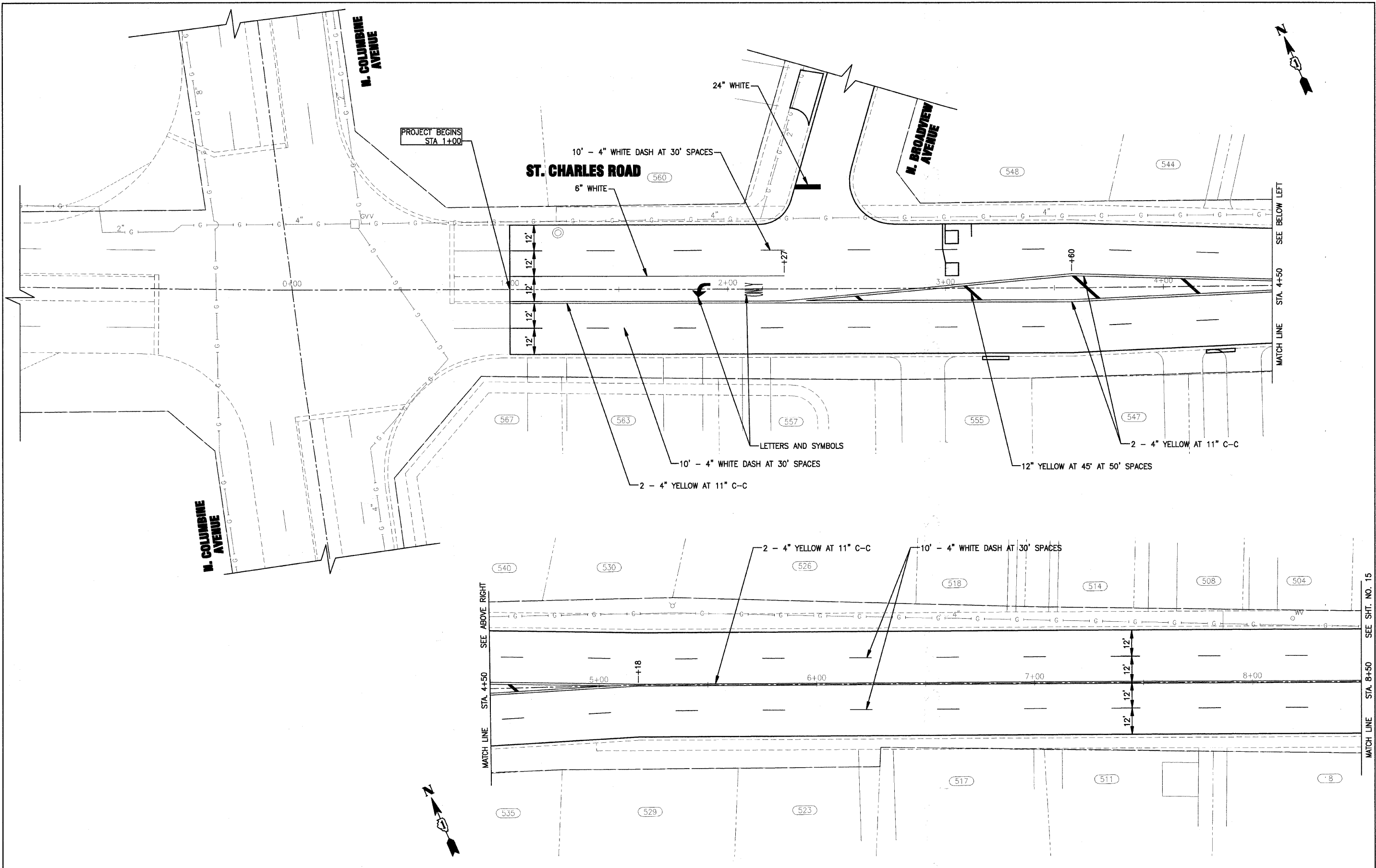
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DATE - 06-26-09	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ST. CHARLES ROAD
PAVEMENT MARKING**

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 0+00 TO STA. 8+50

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	14
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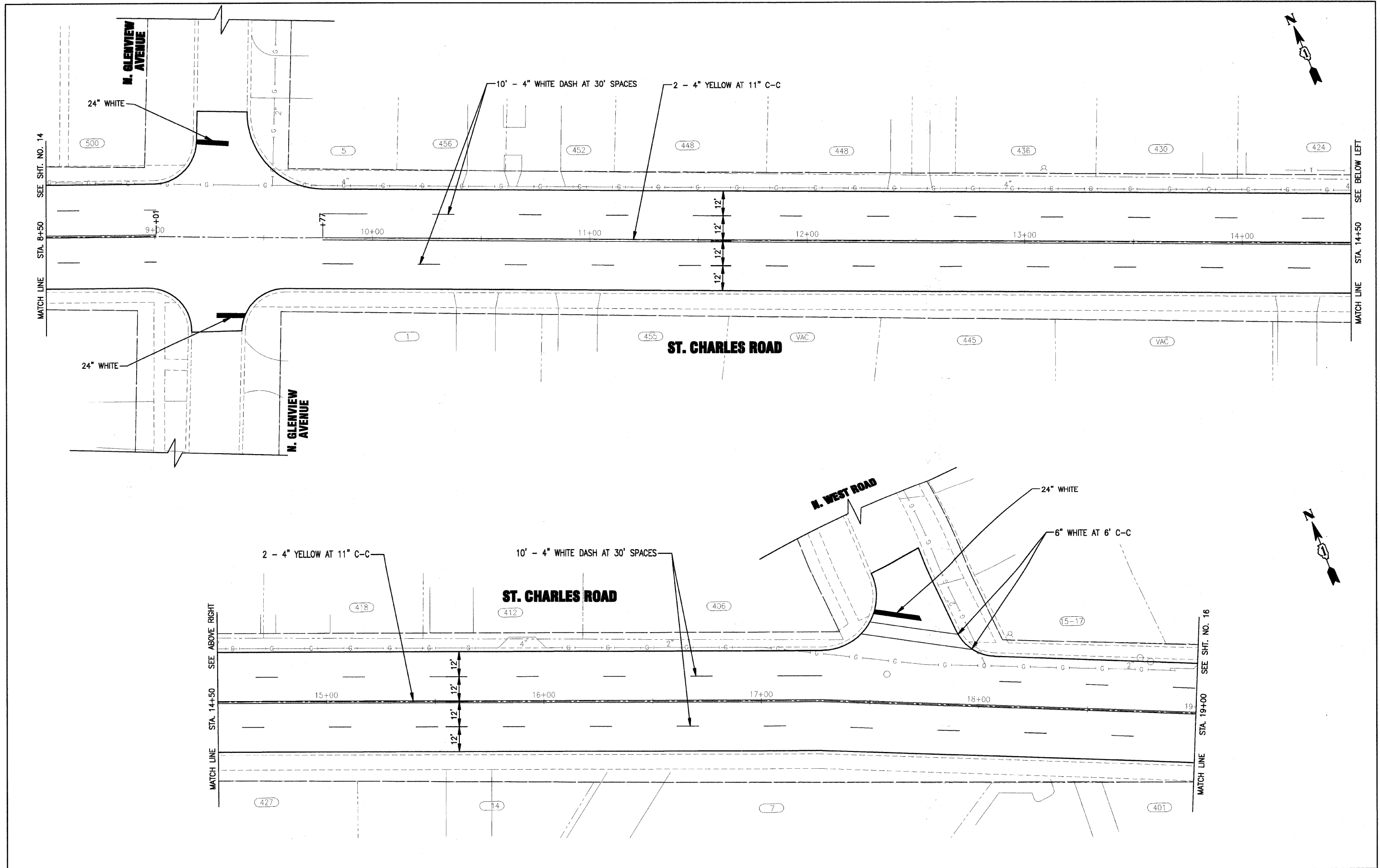


MATCH LINE STA. 4+50 SEE ABOVE RIGHT

MATCH LINE STA. 4+50 SEE BELOW LEFT

MATCH LINE STA. 8+50 SEE SHT. NO. 15

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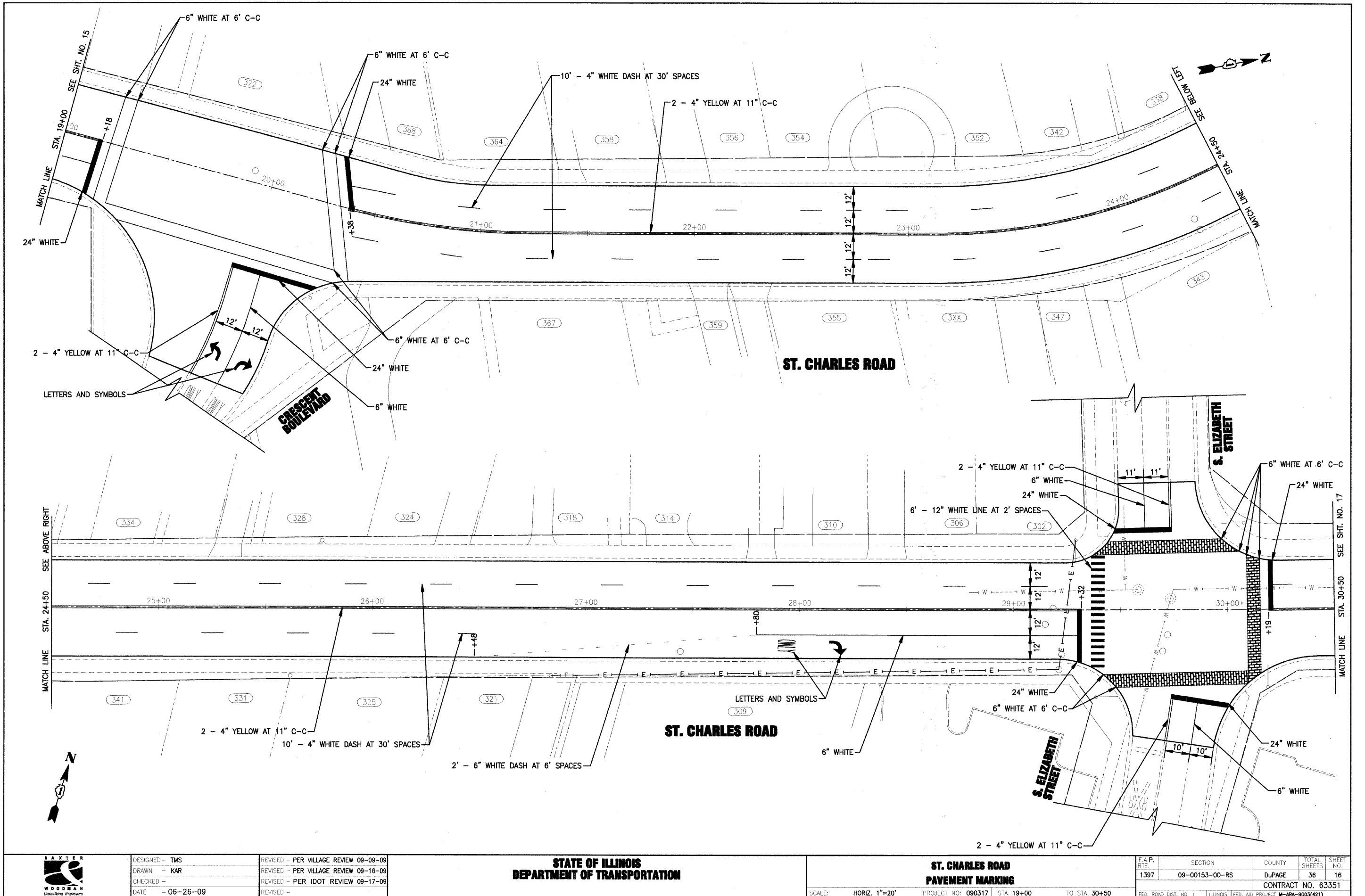
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DATE - 06-26-09	REVISED -

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ST. CHARLES ROAD PAVEMENT MARKING	
SCALE: HORIZ. 1"=20'	PROJECT NO: 090317 STA. 8+50 TO STA. 19+00

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 15
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-ARA-9003(421)			CONTRACT NO. 63351	

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MATCH LINE STA. 19+00 SEE SHT. NO. 15

MATCH LINE STA. 30+50 SEE SHT. NO. 17

24" WHITE

SEE BEYOND LEFT STA. 24+50

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DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED -

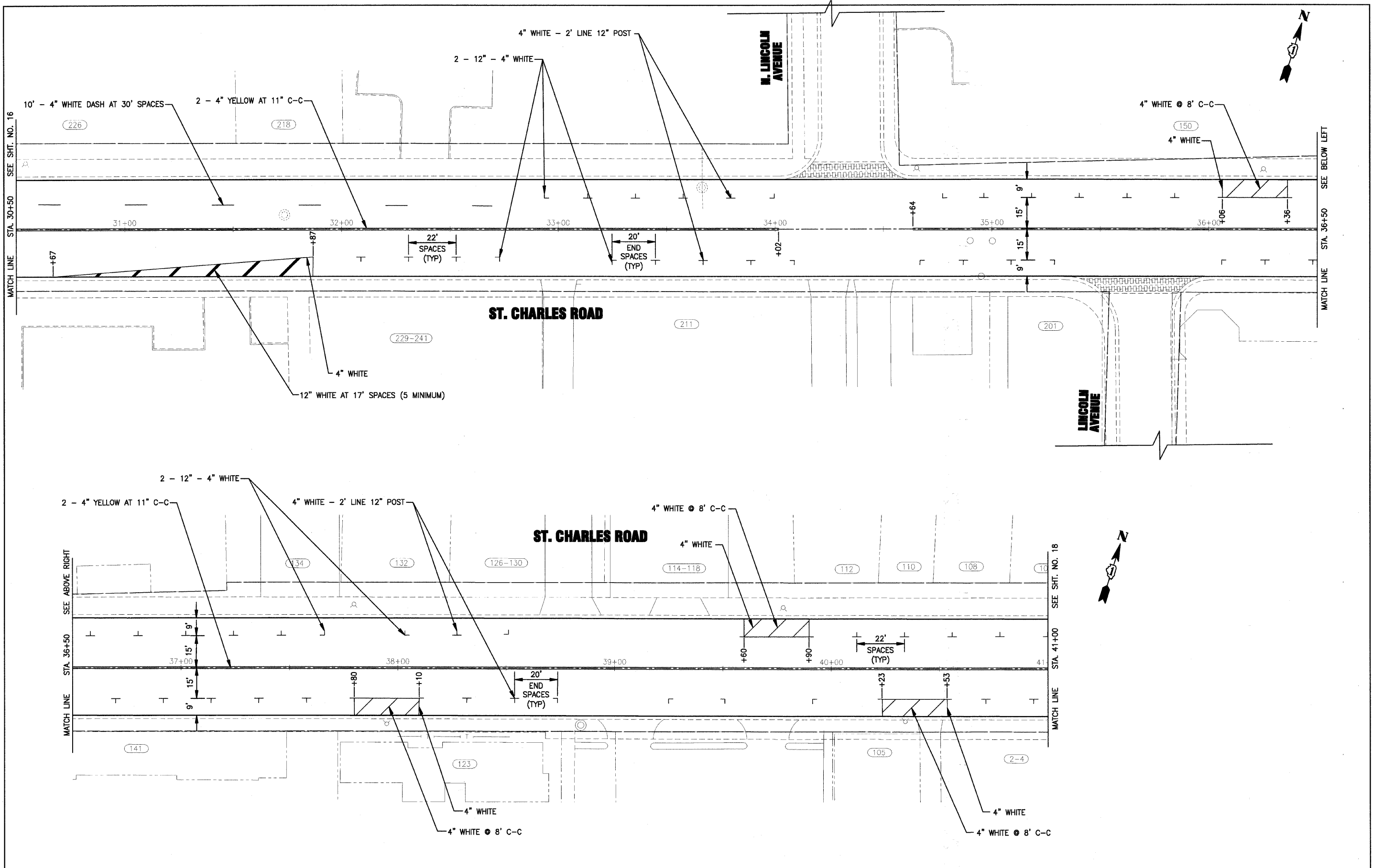
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DEPARTMENT OF TRANSPORTATION**

**ST. CHARLES ROAD
PAVEMENT MARKING**

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA 19+00 TO STA 30+50

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DU PAGE	TOTAL SHEETS 36	SHEET NO. 16
CONTRACT NO. 63351				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				

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CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED - PER IDOT FINAL COMMENTS 01-18-10

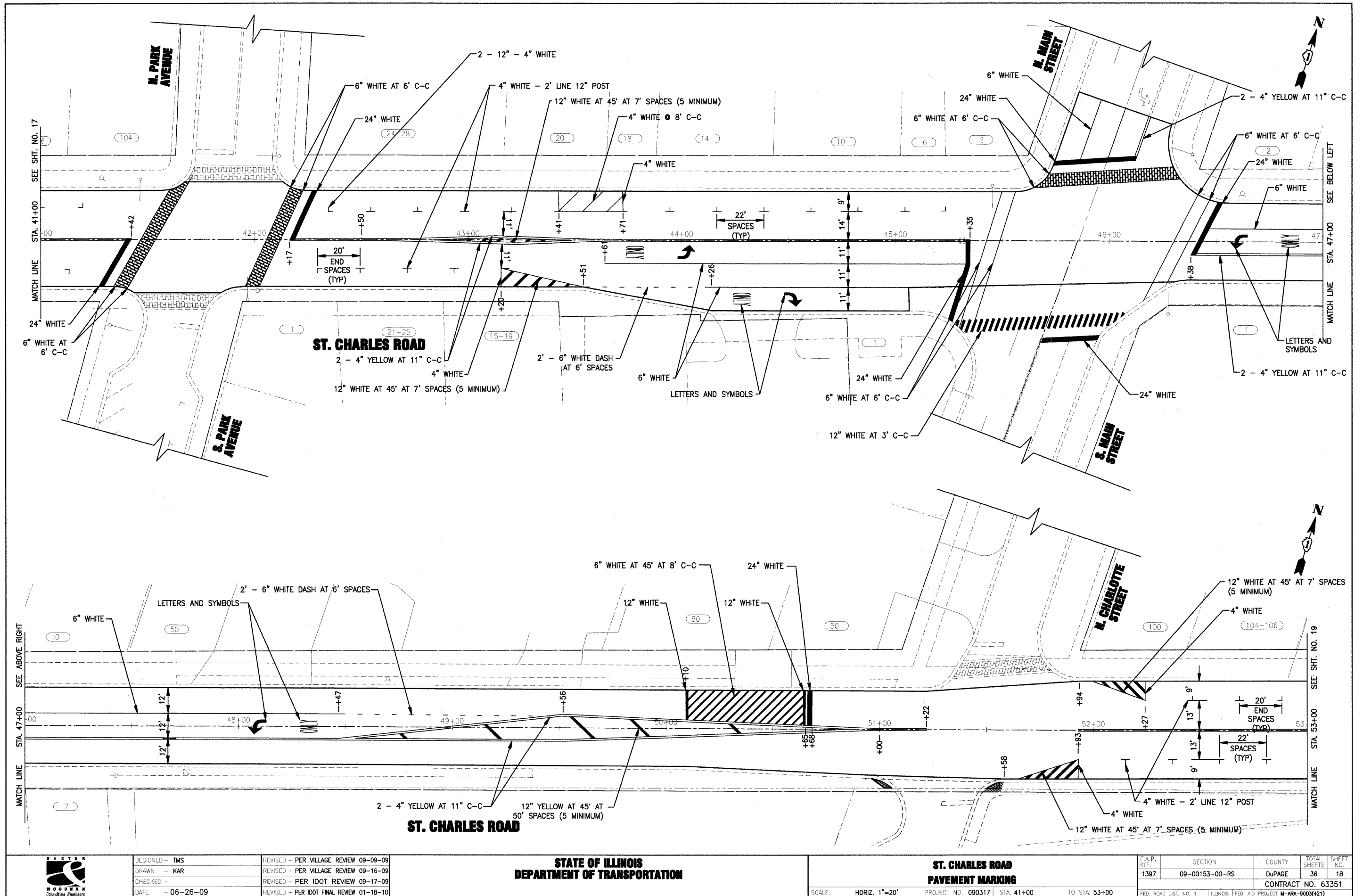
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**ST. CHARLES ROAD
PAVEMENT MARKING**

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 30+50 TO STA. 41+00

F.A.P. RE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 17
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63351	
FED. AID PROJECT M-ARA-9003(421)				

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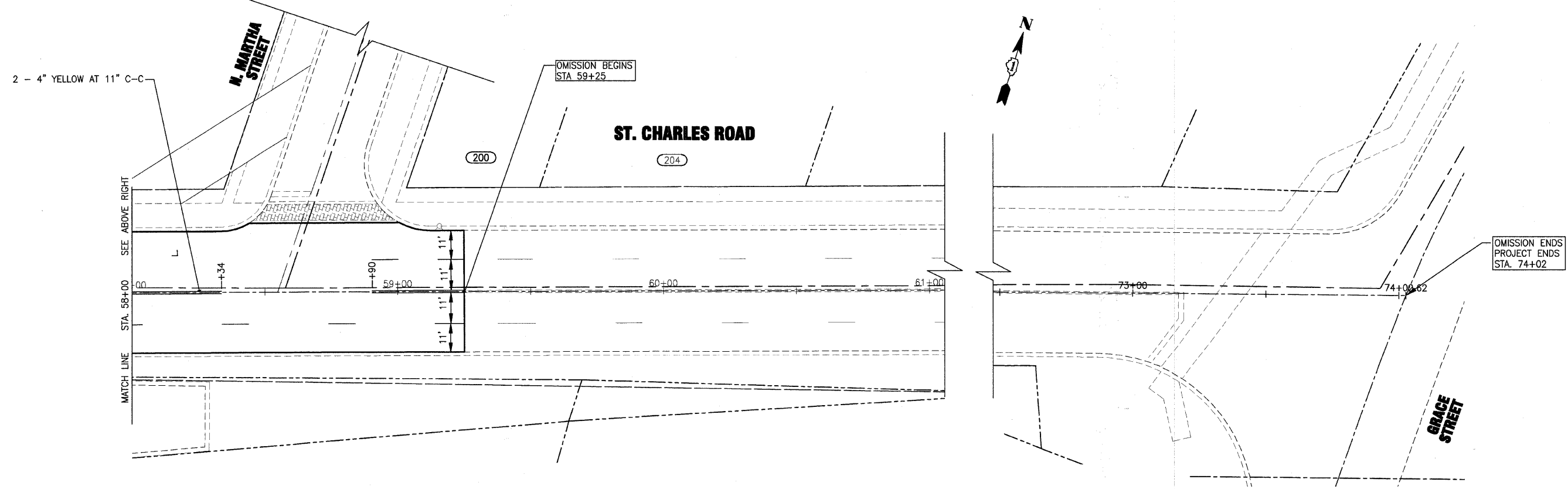
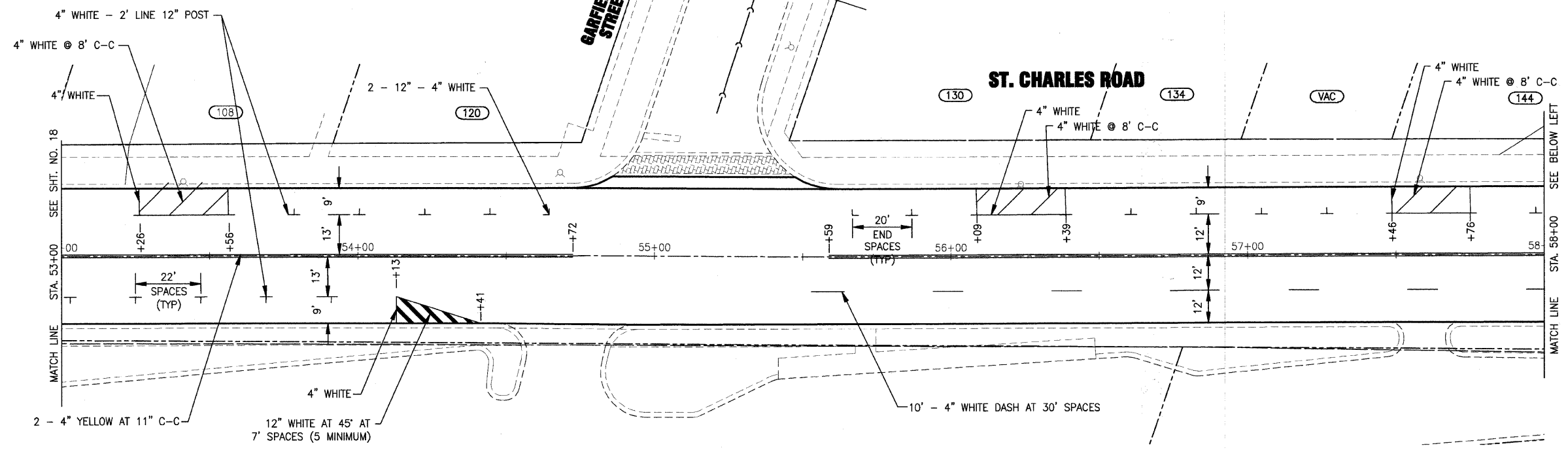


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD PAVEMENT MARKING		
SCALE: HORIZ. 1"=20'	PROJECT NO: 090317	STA. 41+00 TO STA. 53+00

F.A.P. RILEY	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 18
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-RA-9003(421)			CONTRACT NO. 63351	



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DATE - 06-26-09	REVISED - PER IDOT FINAL REVIEW 01-18-10

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ST. CHARLES ROAD
PAVEMENT MARKING**

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. 53+00 TO STA. 61+00

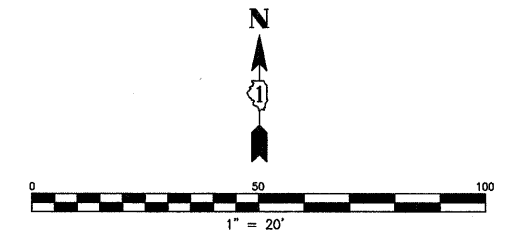
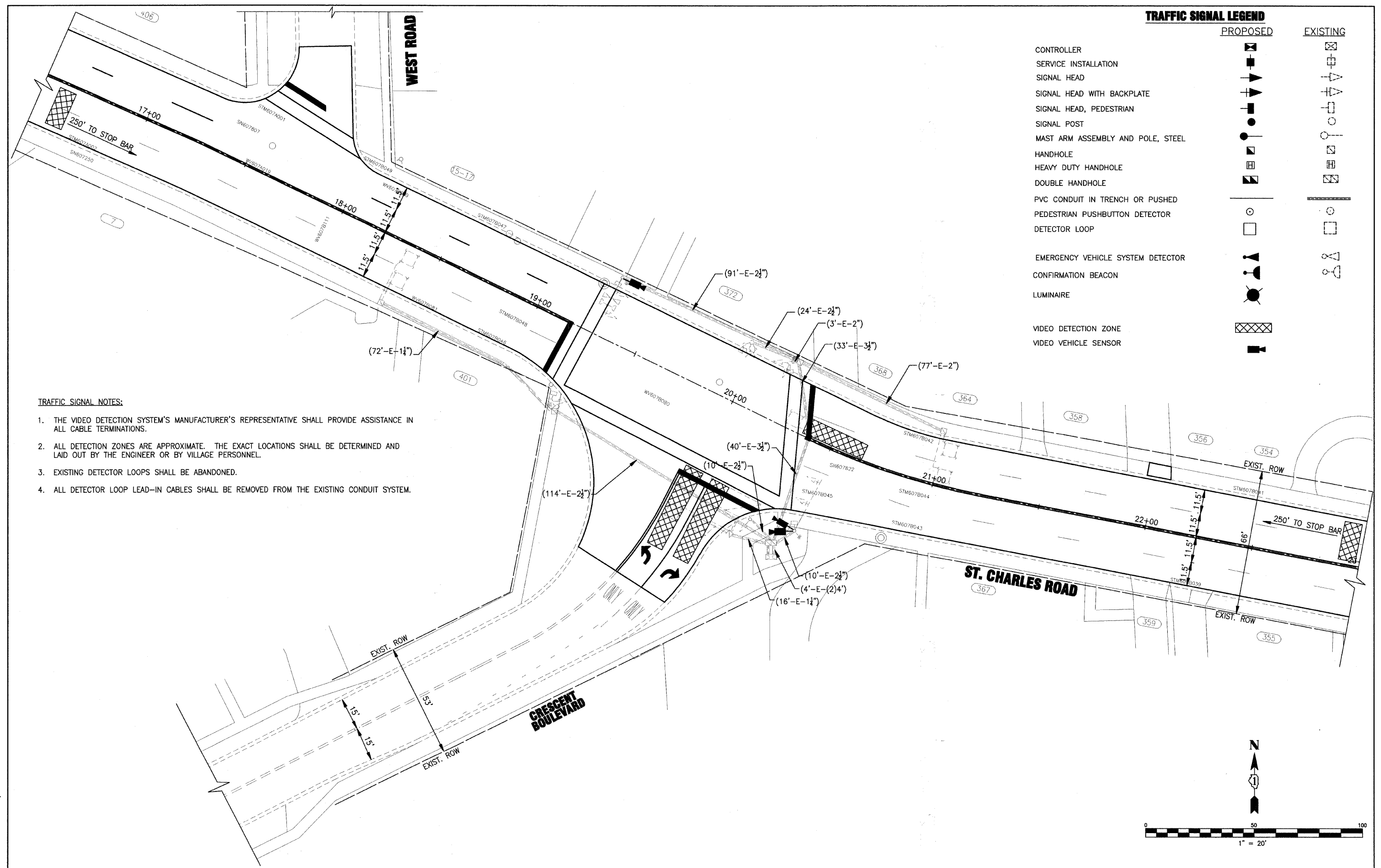
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	19
CONTRACT NO. 63351			ILLINOIS FED. AID PROJECT M-ARA-9003(421)	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
PVC CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
LUMINAIRE		
VIDEO DETECTION ZONE		
VIDEO VEHICLE SENSOR		

TRAFFIC SIGNAL NOTES:

1. THE VIDEO DETECTION SYSTEM'S MANUFACTURER'S REPRESENTATIVE SHALL PROVIDE ASSISTANCE IN ALL CABLE TERMINATIONS.
2. ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT BY THE ENGINEER OR BY VILLAGE PERSONNEL.
3. EXISTING DETECTOR LOOPS SHALL BE ABANDONED.
4. ALL DETECTOR LOOP LEAD-IN CABLES SHALL BE REMOVED FROM THE EXISTING CONDUIT SYSTEM.



I:\LMBD\090317-ST CHAR LAPP\CADD-SURVEY\DRAWINGS\DWGS\090317-SIG-PLAN.dwg -- ST CHARLES-CRSOINT
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DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
CHECKED - JJF	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

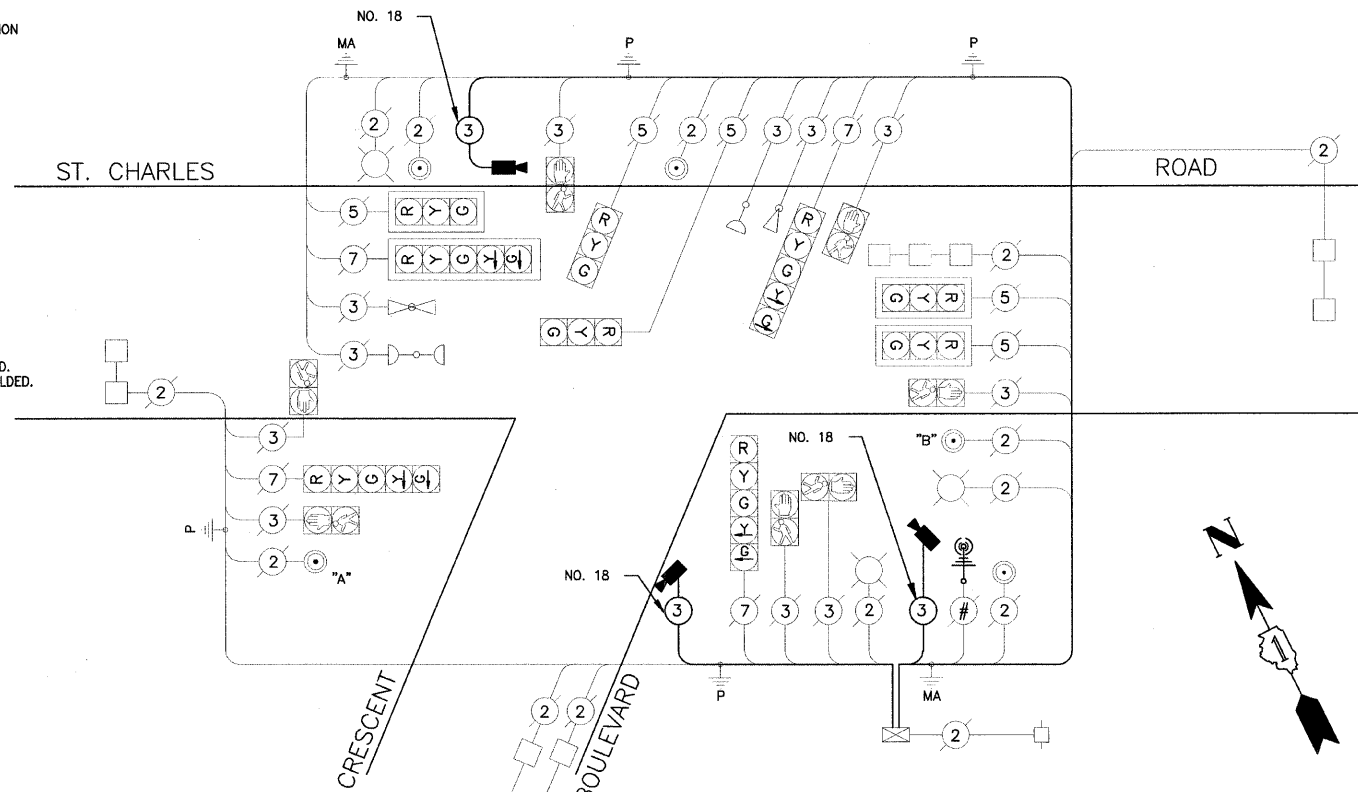
**TRAFFIC SIGNAL MODIFICATION PLAN
ST. CHARLES ROAD**

SCALE: HORIZ. 1"=20' PROJECT NO: 090317 STA. TO STA.

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 20
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-ARA-9003(421)			CONTRACT NO. 63351	

CABLE PLAN LEGEND

- | | | |
|-----------------|-----------------|---|
| EXISTING | PROPOSED | |
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | IMAGE SENSOR |
| | | LUMINAIRE |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN "NO LEFT TURN" |
| | | ILLUMINATED SIGN "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | | RADIO INTERCONNECT ANTENNA |
| | | COAXIAL CABLE |



CABLE DIAGRAM

NOTE: PUSH BUTTON "A" PLACES A CALL TO PHASES 4 AND 6
PUSH BUTTON "B" PLACES A CALL TO PHASES 8 AND 6

SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	472
VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 18 3C	FOOT	388
VIDEO TRANSMISSION SYSTEM	EACH	1

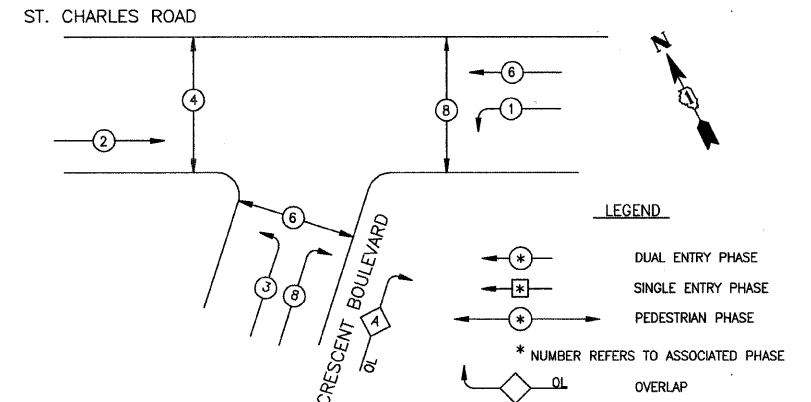
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO LAMPS	LED WATTAGE	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	9	17	0.50	76.5
(YELLOW)	9	25	0.25	56.25
(GREEN)	9	15	0.25	33.75
ARROW	8	12	0.10	9.6
PED. SIGNAL	6	25	1.00	150
CONTROLLER	1	100	1.00	100
ILLUM. SIGN	-	40	0.50	-
STREET LIGHTS	3	250	0.50	375
DET. CAMERA	3	25	1.0	75
TOTAL =				801.1

ENERGY COSTS BILLED TO:
(ADDRESS) VILLAGE OF LOMBARD
255 E. WILSON AVE.
LOMBARD, IL 60148-3931

ENERGY SUPPLY CONTACT:
PHONE:
COMPANY: COMMONWEALTH EDISON

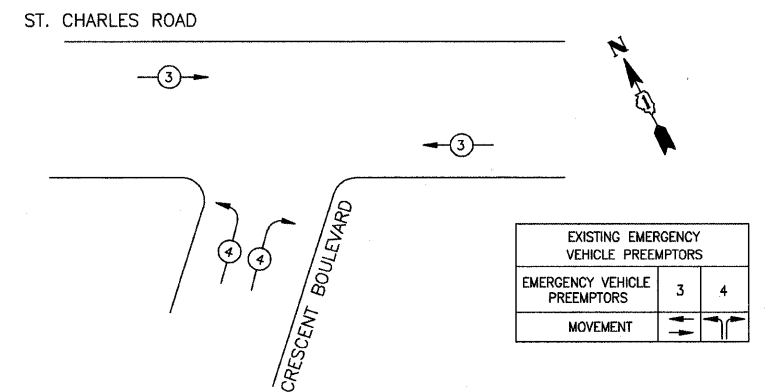
FOUNDATION (DEPTH)	(FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4	HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4	DOUBLE HANDHOLE	13	MAST ARM (L) POLE	20'+L-2=
E - M. ARM POLE	-	SIGNAL POST	2	BRACKET MOUNTED	13
24"	10"	CONTROLLER CAB.	6	PED. PUSHBUTTON	4
30"	15	FIBER OPTIC	13	ELECTRIC SERVICE	13.5
		ELECTRIC SERVICE	1	SERVICE TO GROUND	13.5
		GROUND CABLE	1	POST MOUNTED	6

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	=	8 + 1



EMERGENCY VEHICLE PREEMPTION SEQUENCE

I:\L\OMB\090317-ST CHARLES CADD-SURVEY\DRAWINGS\DWGS\090317-SIG-CABLE.dwg - ST.CHARLES-CRSONT
 Project 2-08-10 By: [unclear] 4/24/10
 State of Illinois - Professional Design Firm
 License No. - 184-001121 - Expires 4-30-09



DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-16-09
DRAWN - KAR	REVISED - PER IDOT REVIEW 09-17-0
CHECKED - JJF	REVISED -
DATE - 06-26-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

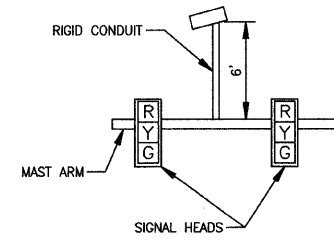
**TRAFFIC SIGNAL MODIFICATION CABLE PLAN
ST. CHARLES ROAD**

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 21
CONTRACT NO. 63351			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)	

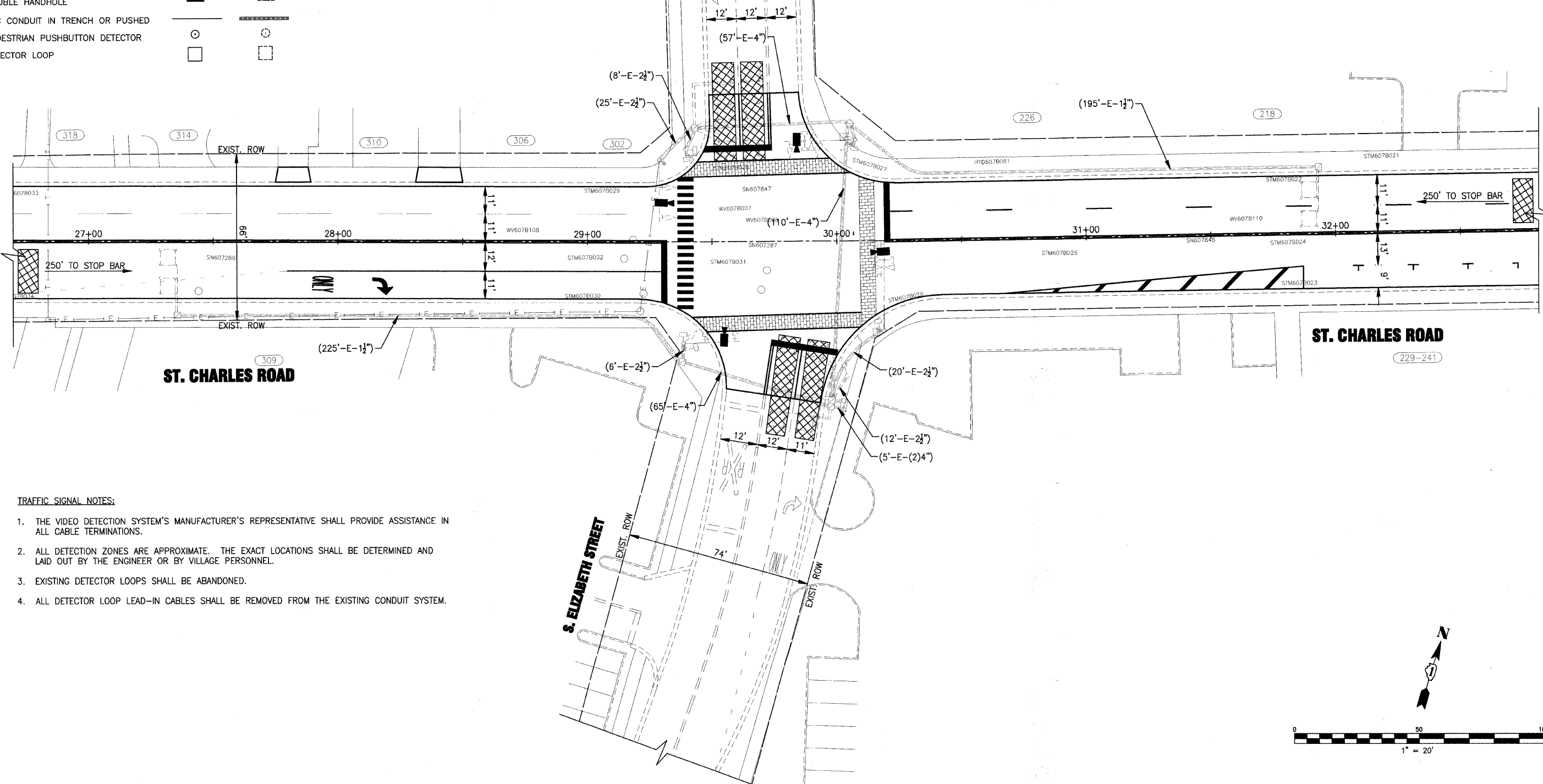
SCALE: NONE PROJECT NO: 090317 STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER			EMERGENCY VEHICLE SYSTEM DETECTOR		
SERVICE INSTALLATION			CONFIRMATION BEACON		
SIGNAL HEAD			LUMINAIRE		
SIGNAL HEAD WITH BACKPLATE			VIDEO DETECTION ZONE		
SIGNAL HEAD, PEDESTRIAN			VIDEO VEHICLE SENSOR		
SIGNAL POST					
MAST ARM ASSEMBLY AND POLE, STEEL					
HANDHOLE					
HEAVY DUTY HANDHOLE					
DOUBLE HANDHOLE					
PVC CONDUIT IN TRENCH OR PUSHED					
PEDESTRIAN PUSHBUTTON DETECTOR					
DETECTOR LOOP					

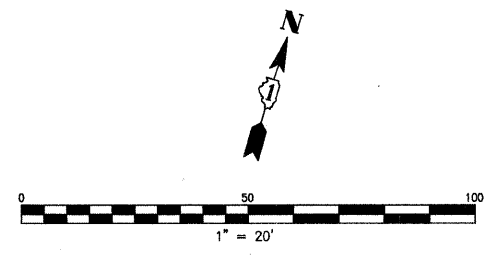


VIDEO DETECTION MOUNTING DETAIL.



TRAFFIC SIGNAL NOTES:

1. THE VIDEO DETECTION SYSTEM'S MANUFACTURER'S REPRESENTATIVE SHALL PROVIDE ASSISTANCE IN ALL CABLE TERMINATIONS.
2. ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT BY THE ENGINEER OR BY VILLAGE PERSONNEL.
3. EXISTING DETECTOR LOOPS SHALL BE ABANDONED.
4. ALL DETECTOR LOOP LEAD-IN CABLES SHALL BE REMOVED FROM THE EXISTING CONDUIT SYSTEM.

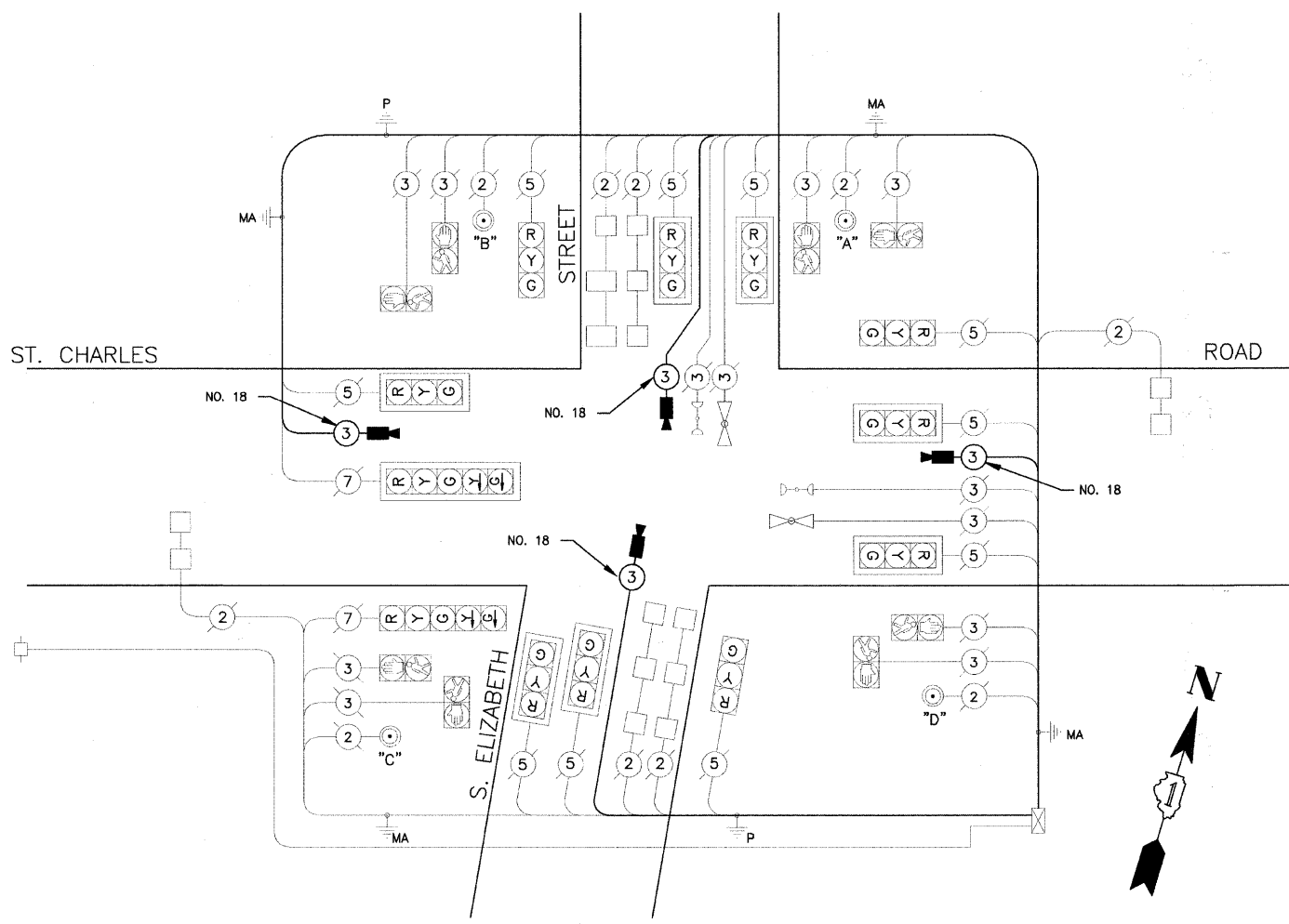


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	DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09				CONTRACT NO. 63351				
CHECKED - JUF	REVISED - PER IDOT REVIEW 09-17-09		SCALE: HORIZ. 1"=20'	PROJECT NO: 090317	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				
DATE - 06-26-09	REVISED -									

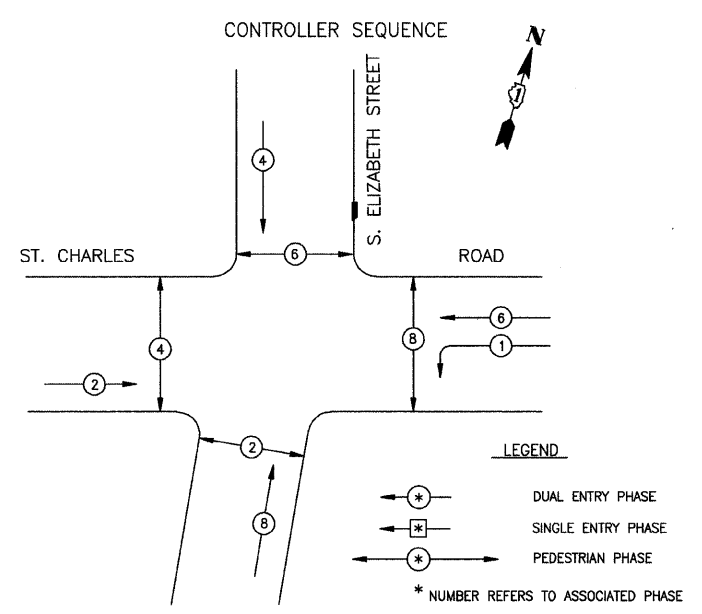
CABLE PLAN LEGEND

EXISTING	PROPOSED	
		8" (200mm) TRAFFIC SIGNAL SECTION
		12" (300mm) TRAFFIC SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER
		CONTROLLER CABINET
		SERVICE INSTALLATION
		TELEPHONE INSTALLATION
		VEHICLE DETECTOR, INDUCTION LOOP
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSHBUTTON DETECTOR
		DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
		FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F
		SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD.
		IMAGE SENSOR
		LUMINAIRE
		RAILROAD CONTROL CABINET
		ILLUMINATED SIGN "NO LEFT TURN"
		ILLUMINATED SIGN "NO RIGHT TURN"
		GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C)
		GROUND ROD AT POST (P) OR MAST ARM POLE (MA)
		GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		RADIO INTERCONNECT ANTENNA
		COAXIAL CABLE

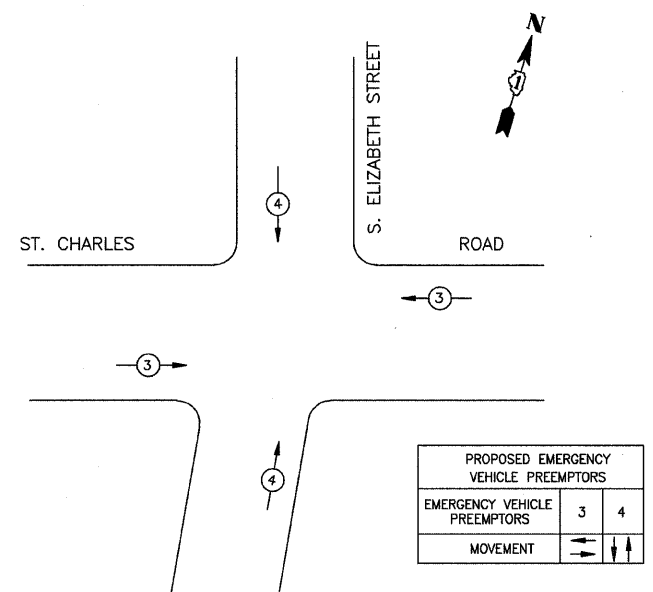


CABLE DIAGRAM

NOTE: PUSH BUTTON "A" PLACES A CALL TO PHASES 6 AND 8
 PUSH BUTTON "B" PLACES A CALL TO PHASES 6 AND 4
 PUSH BUTTON "C" PLACES A CALL TO PHASES 4 AND 2
 PUSH BUTTON "D" PLACES A CALL TO PHASES 2 AND 8



PHASE DESIGNATION DIAGRAM



EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	876
VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 18 3C	FOOT	687
VIDEO TRANSMISSION SYSTEM	EACH	1

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO LAMPS	WATTAGE	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	17	0.50	102
(YELLOW)	12	25	0.25	75
(GREEN)	12	15	0.25	45
ARROW	4	12	0.10	4.8
PED. SIGNAL	8	25	1.00	200
CONTROLLER	1	100	1.00	100
ILLUM. SIGN	-	40	0.50	-
STREET LIGHTS	-	250	0.50	-
FLASHER	-	-	0.50	-
DET. CAMERA	4	25	1.0	100
TOTAL =				326.8

FOUNDATION (DEPTH)	(FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4	HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4	DOUBLE HANDHOLE	13	MAST ARM (L) POLE	20'+L-2=
E - M. ARM POLE	-	SIGNAL POST	2	BRACKET MOUNTED	13
24"	10"	CONTROLLER CAB.	6	PED. PUSHBUTTON	4
30"	15	FIBER OPTIC	13	ELECTRIC SERVICE	13.5
		ELECTRIC SERVICE	1	SERVICE TO GROUND	13.5
		GROUND CABLE	1	POST MOUNTED	6

ENERGY COSTS BILLED TO:
 (ADDRESS) VILLAGE OF LOMBARD
 255 E. WILSON AVE.
 LOMBARD, IL 60148-3931

ENERGY SUPPLY CONTACT:
 PHONE:
 COMPANY: COMMONWEALTH EDISON

DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-16-09
DRAWN - KAR	REVISED - PER IDOT REVIEW 09-17-0
CHECKED - JJF	REVISED -
DATE - 06-26-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODIFICATION CABLE PLAN
ST. CHARLES ROAD

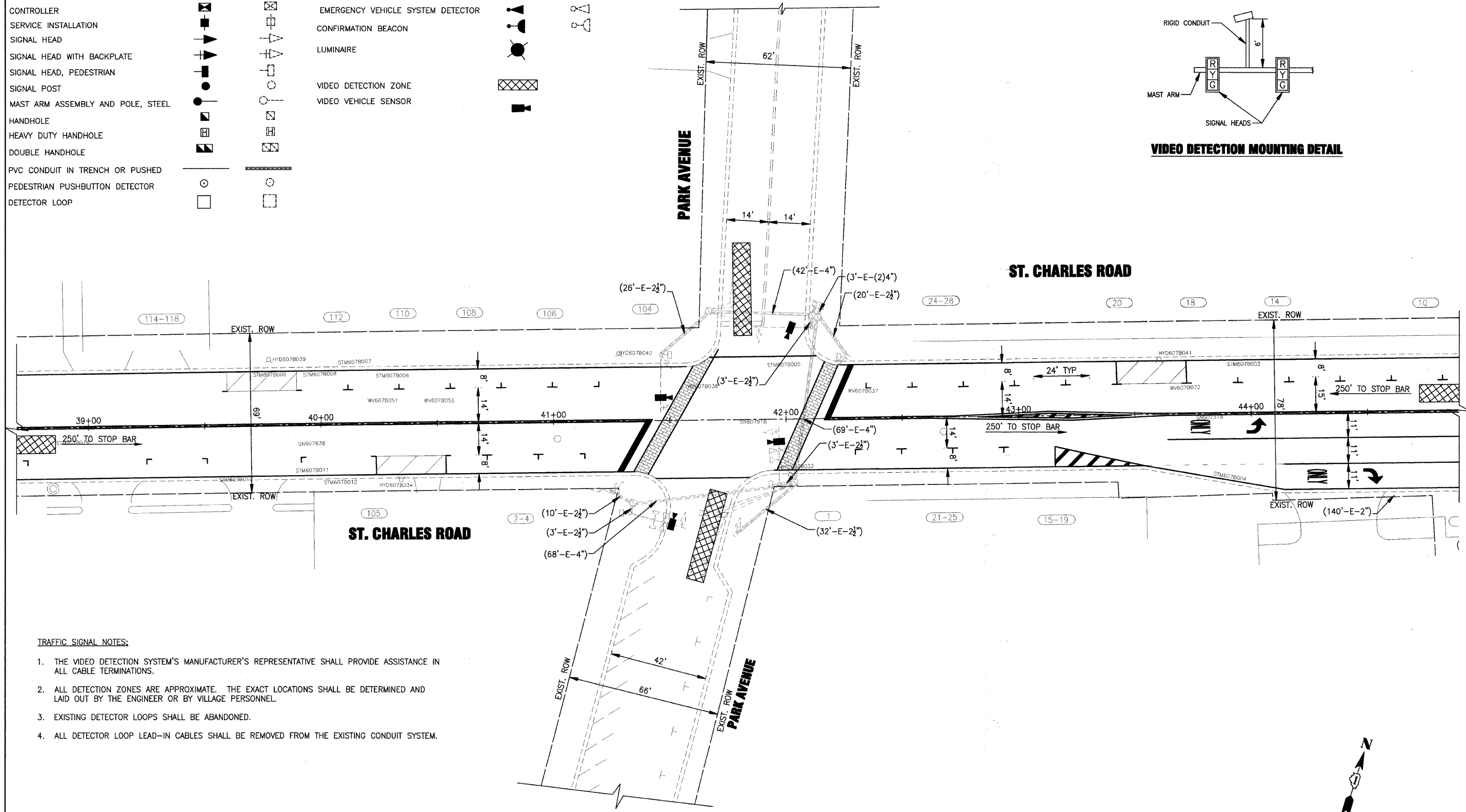
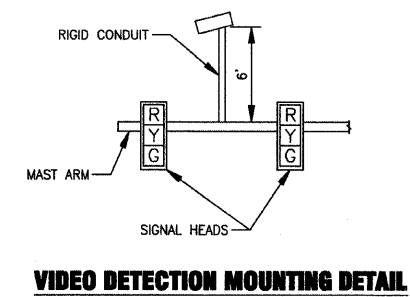
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SCALE: NONE			CONTRACT NO. 63351	
PROJECT NO: 090317 STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)		

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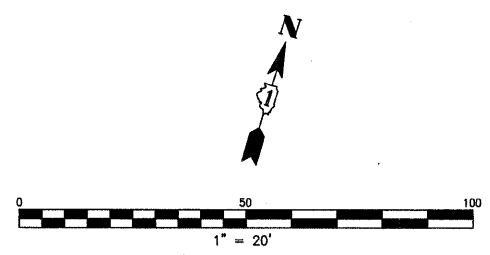
TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER			EMERGENCY VEHICLE SYSTEM DETECTOR		
SERVICE INSTALLATION			CONFIRMATION BEACON		
SIGNAL HEAD			LUMINAIRE		
SIGNAL HEAD WITH BACKPLATE			VIDEO DETECTION ZONE		
SIGNAL HEAD, PEDESTRIAN			VIDEO VEHICLE SENSOR		
SIGNAL POST					
MAST ARM ASSEMBLY AND POLE, STEEL					
HANDHOLE					
HEAVY DUTY HANDHOLE					
DOUBLE HANDHOLE					
PVC CONDUIT IN TRENCH OR PUSHED					
PEDESTRIAN PUSHBUTTON DETECTOR					
DETECTOR LOOP					



TRAFFIC SIGNAL NOTES:

1. THE VIDEO DETECTION SYSTEM'S MANUFACTURER'S REPRESENTATIVE SHALL PROVIDE ASSISTANCE IN ALL CABLE TERMINATIONS.
2. ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT BY THE ENGINEER OR BY VILLAGE PERSONNEL.
3. EXISTING DETECTOR LOOPS SHALL BE ABANDONED.
4. ALL DETECTOR LOOP LEAD-IN CABLES SHALL BE REMOVED FROM THE EXISTING CONDUIT SYSTEM.

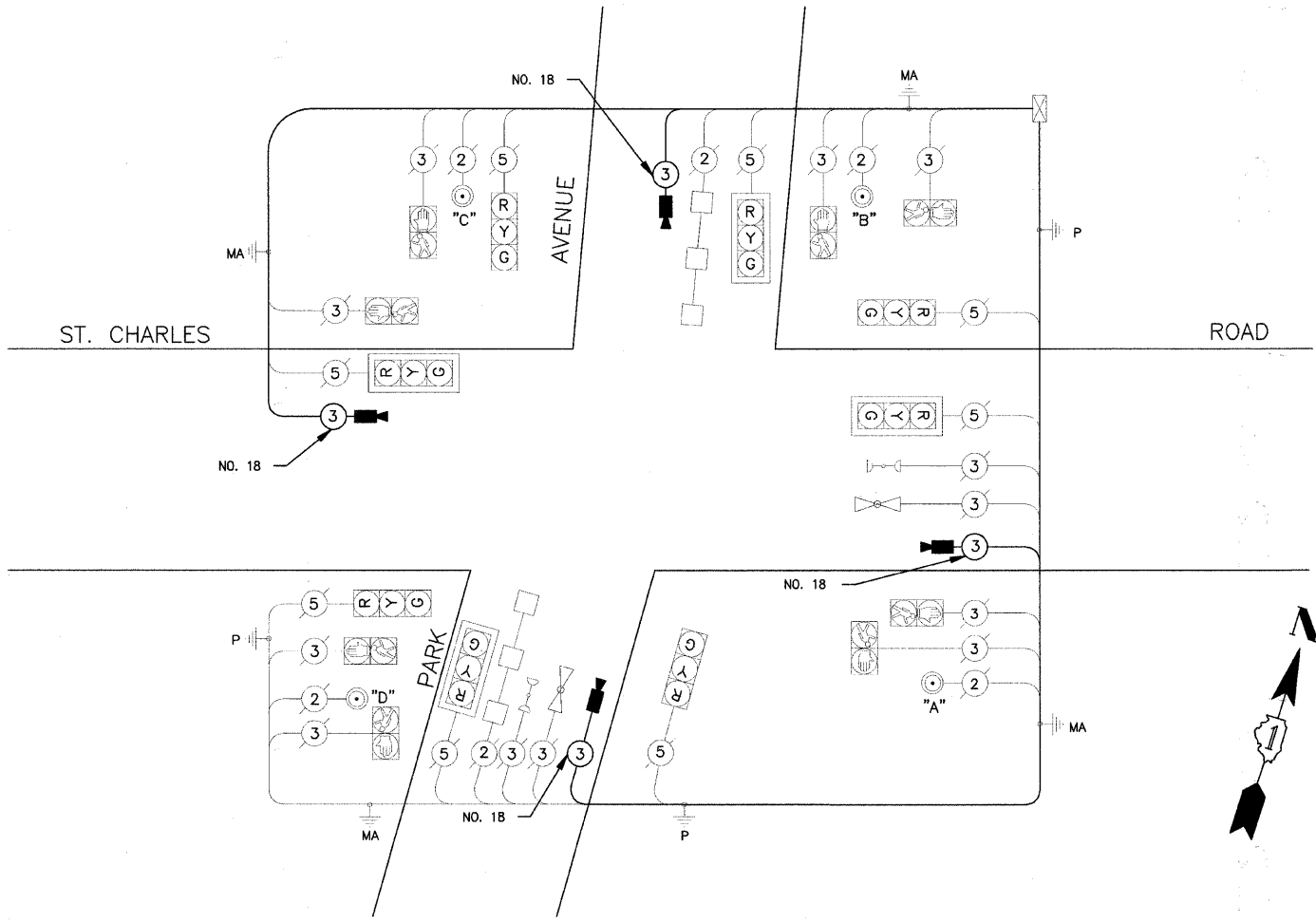


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	DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODIFICATION PLAN ST. CHARLES ROAD		F.A.P. R/E	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09		SCALE: HORIZ. 1"=20'	PROJECT NO: 090317	STA. TO STA.	1397	09-00153-00-RS	DuPAGE	36
	CHECKED - JJF	REVISED - PER IDOT REVIEW 09-17-09				CONTRACT NO. 63351				
	DATE - 06-26-09	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				

CABLE PLAN LEGEND

- | | | |
|-----------------|-----------------|---|
| EXISTING | PROPOSED | |
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | IMAGE SENSOR |
| | | LUMINAIRE |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN "NO LEFT TURN" |
| | | ILLUMINATED SIGN "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | | RADIO INTERCONNECT ANTENNA |
| | | COAXIAL CABLE |



CABLE DIAGRAM

NOTE: PUSH BUTTON "A" PLACES A CALL TO PHASES 2 AND 8
 PUSH BUTTON "B" PLACES A CALL TO PHASES 8 AND 6
 PUSH BUTTON "C" PLACES A CALL TO PHASES 6 AND 4
 PUSH BUTTON "D" PLACES A CALL TO PHASES 4 AND 8

SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	183
VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
ELECTRIC CABLE IN CONDUIT, NO. 18 3C	FOOT	556
VIDEO TRANSMISSION SYSTEM	EACH	1

FOUNDATION (DEPTH)	(FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4	HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4	DOUBLE HANDHOLE	13	MAST ARM (L) POLE	20'+L-2'
E - M. ARM POLE	-	SIGNAL POST	2	BRACKET MOUNTED	13
24"	10"	CONTROLLER CAB.	6	PED. PUSHBUTTON	4
30"	15	FIBER OPTIC	13	ELECTRIC SERVICE	13.5
		ELECTRIC SERVICE	1	SERVICE TO GROUND	13.5
		GROUND CABLE	1	POST MOUNTED	6

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	8	17	0.50	68
(YELLOW)	8	25	0.25	50
(GREEN)	8	15	0.25	30
ARROW	-	-	-	-
PED. SIGNAL	8	25	1.00	200
CONTROLLER	1	100	1.00	100
ILLUM. SIGN	-	40	0.50	-
STREET LIGHTS	-	250	0.50	-
FLASHER	-	-	0.50	-
DET. CAMERA	4	25	1.0	100
TOTAL =				548.0

ENERGY COSTS BILLED TO:
 (ADDRESS) VILLAGE OF LOMBARD
 255 E. WILSON AVE.
 LOMBARD, IL 60148-3931

ENERGY SUPPLY CONTACT:
 PHONE:
 COMPANY: COMMONWEALTH EDISON



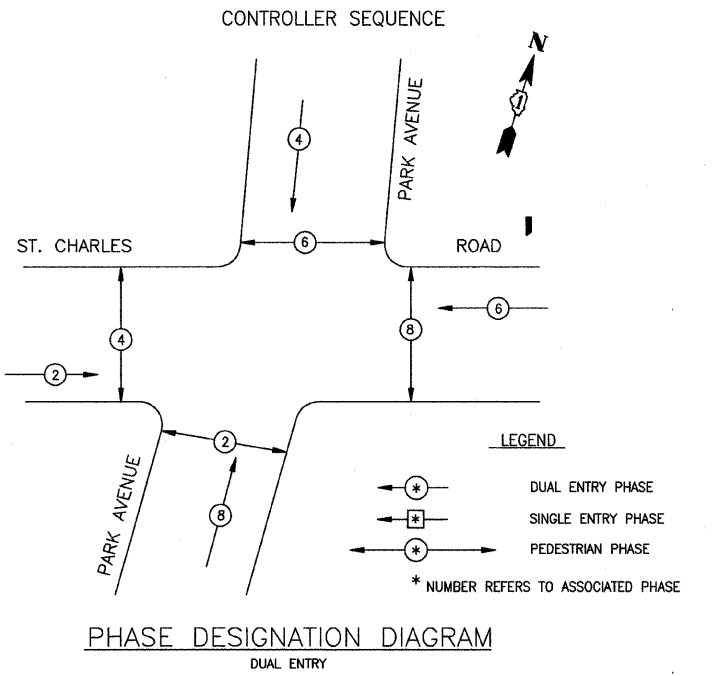
DESIGNED - TMS
 DRAWN - KAR
 CHECKED - JJF
 DATE - 06-26-09

REVISED - PER VILLAGE REVIEW 09-16-09
 REVISED - PER IDOT REVIEW 09-17-0

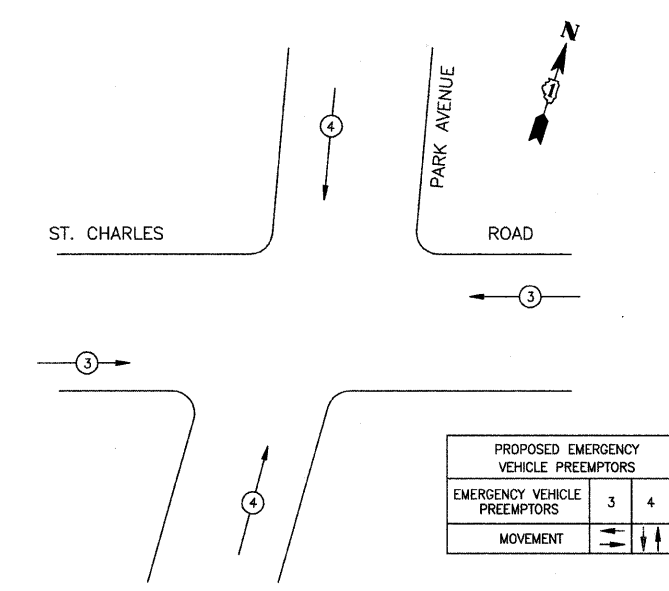
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODIFICATION CABLE PLAN
 ST. CHARLES ROAD**

SCALE: NONE PROJECT NO: 090317 STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-8003(421)



PHASE DESIGNATION DIAGRAM



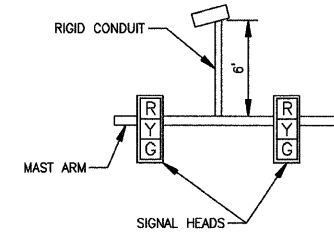
EMERGENCY VEHICLE PREEMPTION SEQUENCE

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTORS	3	4
MOVEMENT	←	→

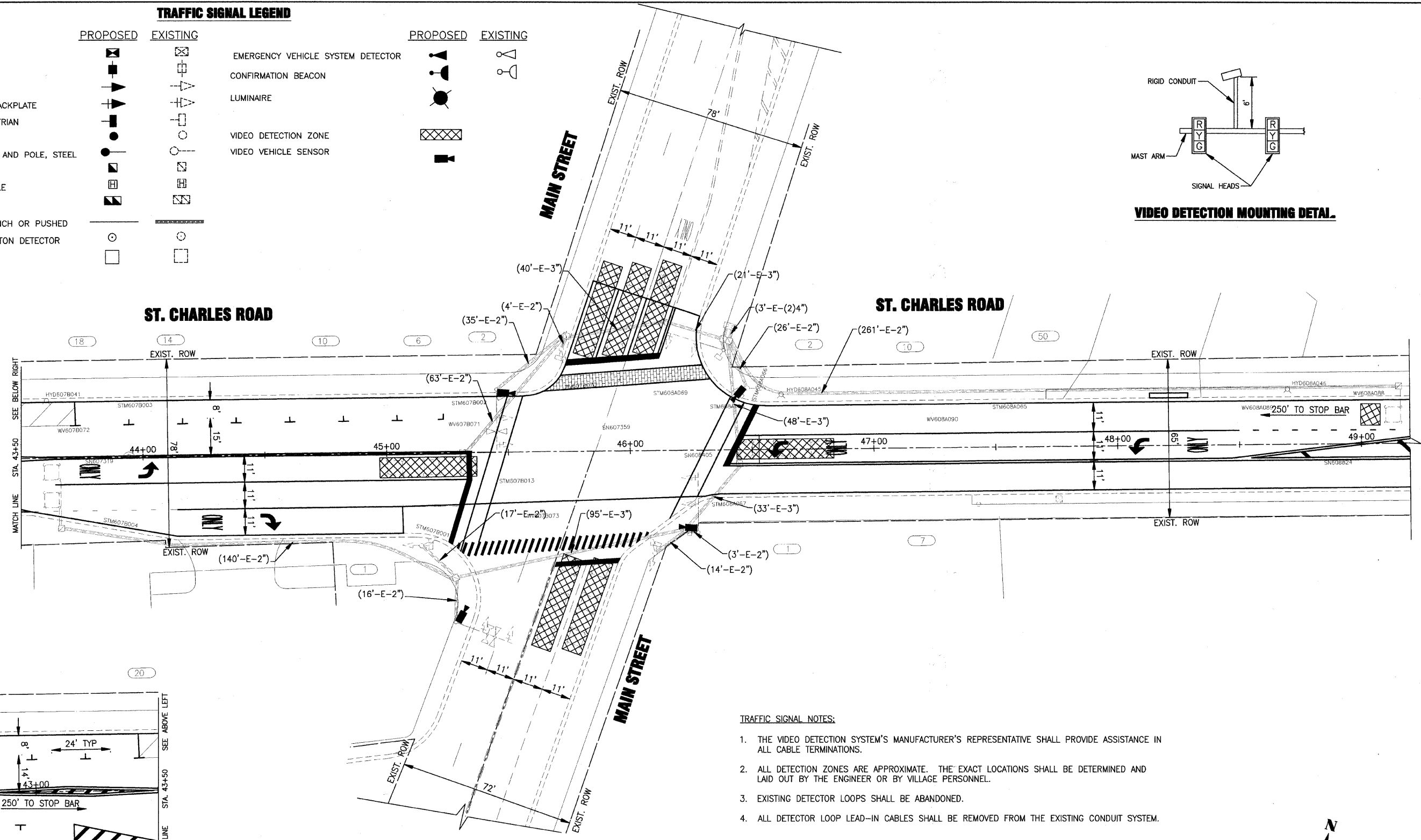
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 License No. - 184-001121 - Expires 4-30-09

TRAFFIC SIGNAL LEGEND

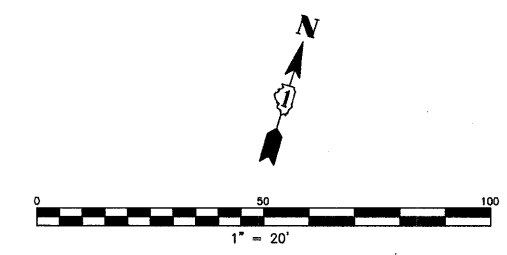
	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER			EMERGENCY VEHICLE SYSTEM DETECTOR		
SERVICE INSTALLATION			CONFIRMATION BEACON		
SIGNAL HEAD			LUMINAIRE		
SIGNAL HEAD WITH BACKPLATE			VIDEO DETECTION ZONE		
SIGNAL HEAD, PEDESTRIAN			VIDEO VEHICLE SENSOR		
SIGNAL POST					
MAST ARM ASSEMBLY AND POLE, STEEL					
HANDHOLE					
HEAVY DUTY HANDHOLE					
DOUBLE HANDHOLE					
PVC CONDUIT IN TRENCH OR PUSHED					
PEDESTRIAN PUSHBUTTON DETECTOR					
DETECTOR LOOP					



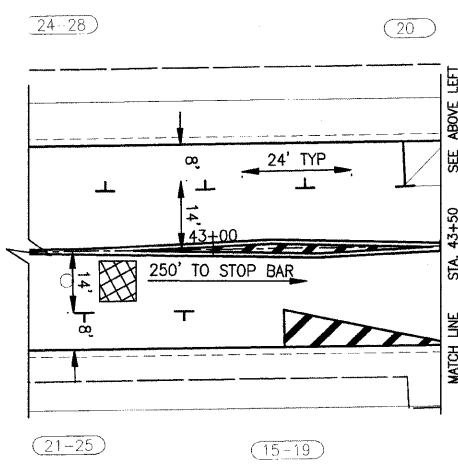
VIDEO DETECTION MOUNTING DETAIL.



- TRAFFIC SIGNAL NOTES:**
1. THE VIDEO DETECTION SYSTEM'S MANUFACTURER'S REPRESENTATIVE SHALL PROVIDE ASSISTANCE IN ALL CABLE TERMINATIONS.
 2. ALL DETECTION ZONES ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT BY THE ENGINEER OR BY VILLAGE PERSONNEL.
 3. EXISTING DETECTOR LOOPS SHALL BE ABANDONED.
 4. ALL DETECTOR LOOP LEAD-IN CABLES SHALL BE REMOVED FROM THE EXISTING CONDUIT SYSTEM.



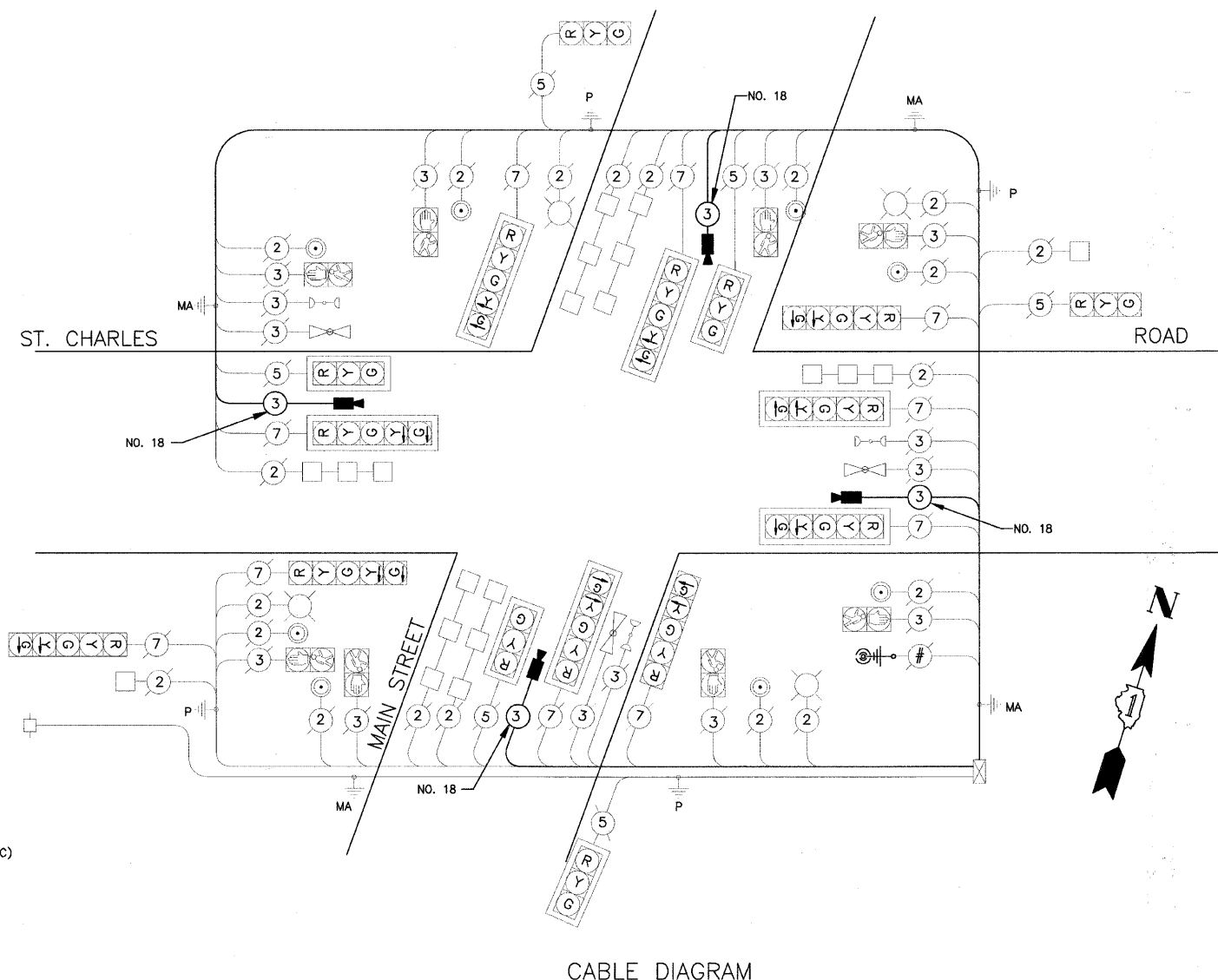
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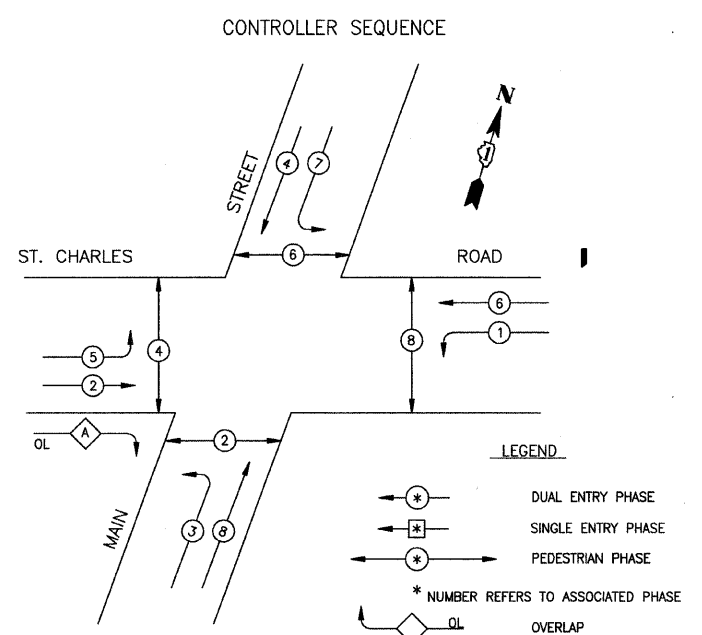
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	DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-07		SCALE: HORIZ. 1"=20'	PROJECT NO: 090317	STA. TO STA.	1397	09-00153-00-RS	DuPAGE	36
	CHECKED - JJF	REVISED - PER IDOT REVIEW 09-17-09				CONTRACT NO. 63351				
	DATE - 06-26-09	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				

CABLE PLAN LEGEND

- | | | |
|-----------------|-----------------|---|
| EXISTING | PROPOSED | |
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | IMAGE SENSOR |
| | | LUMINAIRE |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN "NO LEFT TURN" |
| | | ILLUMINATED SIGN "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | | RADIO INTERCONNECT ANTENNA |
| | | COAXIAL CABLE |

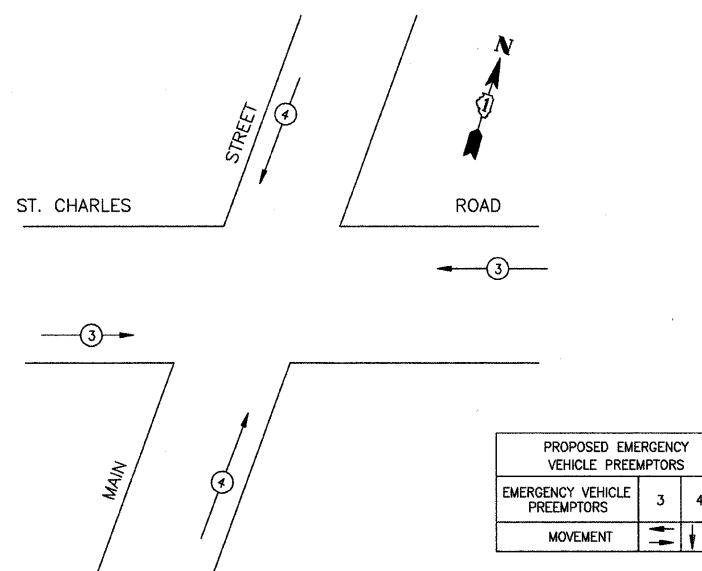


CABLE DIAGRAM



PHASE DESIGNATION DIAGRAM

OVERLAP LETTER	DUAL ENTRY PERMISSIVE PHASE	PROTECTED PHASE
A	= 2	+ 3



EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1068
VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
ELECTRIC CABLE IN CONDUIT, NO. 18 3C	FOOT	696
VIDEO TRANSMISSION SYSTEM	EACH	1

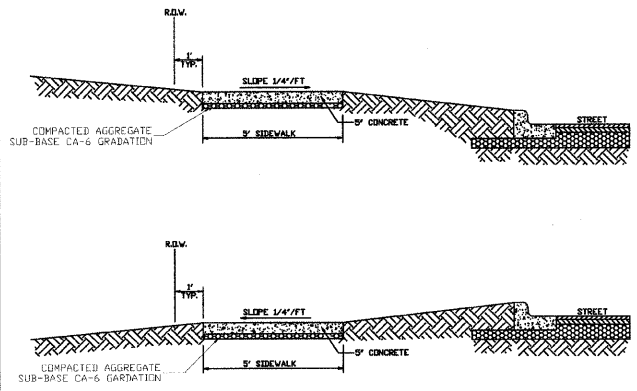
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	15	17	0.50	127.5
(YELLOW)	15	25	0.25	93.75
(GREEN)	15	15	0.25	56.25
ARROW	18	12	0.10	21.6
PED. SIGNAL	8	25	1.00	200
CONTROLLER	1	100	1.00	100
ILLUM. SIGN	-	40	0.50	-
STREET LIGHTS	-	250	0.50	-
FLASHER	-	-	0.50	-
DET. CAMERA	4	25	1.0	100
TOTAL =				699.1

FOUNDATION (DEPTH)	(FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4	HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4	DOUBLE HANDHOLE	13	MAST ARM (L) POLE	20'+L-2'
E - M. ARM POLE	-	SIGNAL POST	2	BRACKET MOUNTED	13
24"	10"	CONTROLLER CAB.	6	PED. PUSHBUTTON	4
30"	15	FIBER OPTIC	13	ELECTRIC SERVICE	13.5
		ELECTRIC SERVICE	1	SERVICE TO GROUND	13.5
		GROUND CABLE	1	POST MOUNTED	6

ENERGY COSTS BILLED TO:
(ADDRESS) VILLAGE OF LOMBARD
255 W. WILSON AVE.
LOMBARD, IL 60148-3931

ENERGY SUPPLY CONTACT:
PHONE:
COMPANY: COMMONWEALTH EDISON

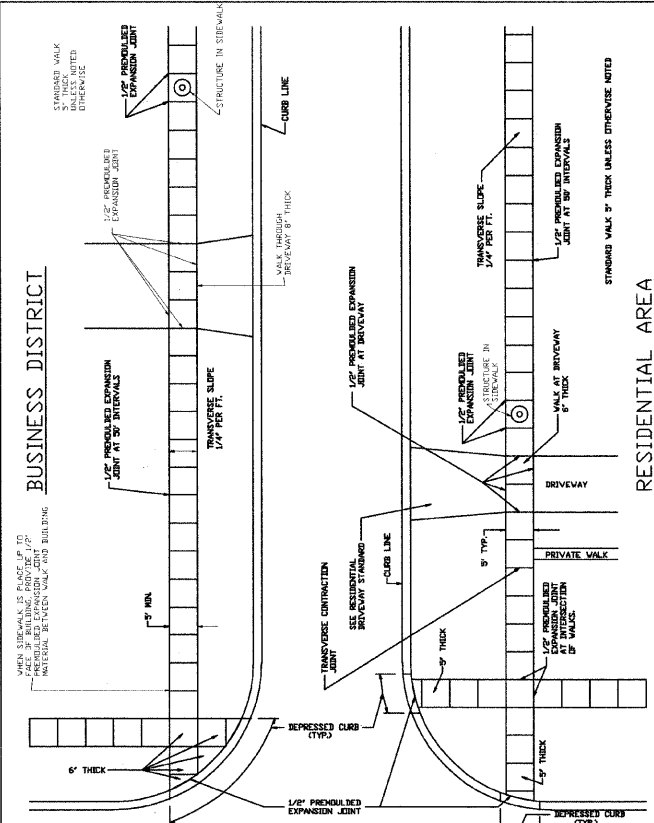
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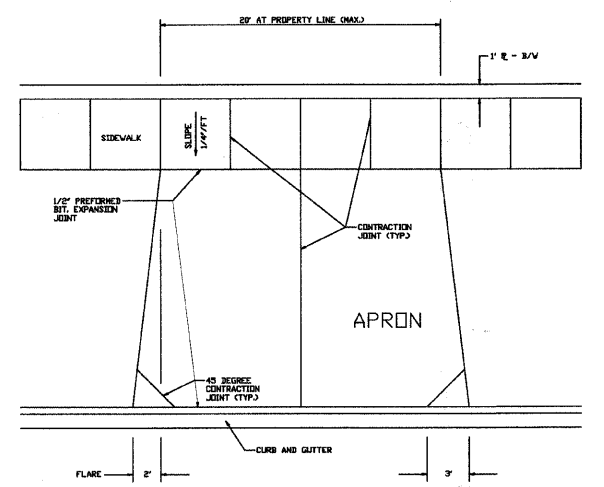
GENERAL NOTES:

1. CONCRETE SHALL BE CLASS SI.
2. MINIMUM SIDEWALK THICKNESS SHALL BE FIVE INCHES (5").
3. SIDEWALK THICKNESS ACROSS DRIVEWAYS SHALL BE SIX INCHES (6") MINIMUM FOR RESIDENTIAL DRIVEWAYS, AND EIGHT INCHES (8") MINIMUM FOR NON-RESIDENTIAL DRIVEWAYS.
4. MAXIMUM LONGITUDINAL SLOPE SHALL NOT EXCEED 6% (1/16").
5. MINIMUM TRANSVERSE SLOPE SHALL BE 1/4" PER FT. (2%) TYPICAL. MAXIMUM TRANSVERSE SLOPE SHALL BE NO GREATER THAN 1/2" PER FT. (4%) TYPICAL.
6. A TWO INCH (2") MINIMUM AGGREGATE SUB-BASE (CA-6 GRADATION) SHALL BE PROVIDED (FOUR INCHES (4") MINIMUM) THROUGH NON-RESIDENTIAL DRIVEWAYS.
7. AGGREGATE SUB-BASE COURSE SHALL BE MECHANICALLY COMPACTED.
8. ALL SIDEWALK SHALL BE PROMPTLY BACKFILLED AND PROTECTED FROM DAMAGE.

REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 2



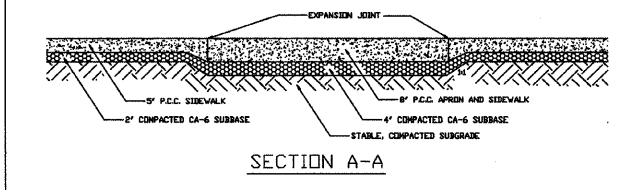
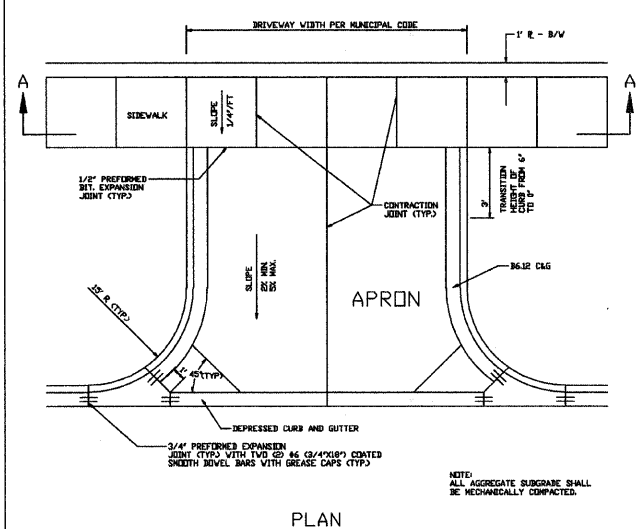
REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 4



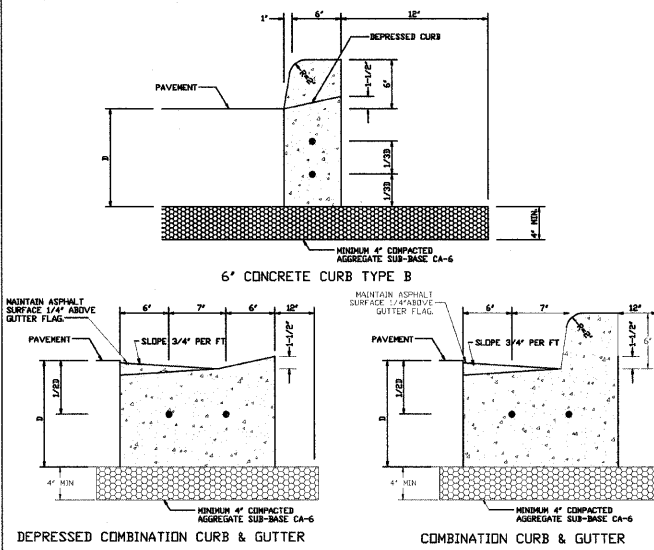
GENERAL NOTES:

1. APRONS SHALL NOT EXCEED 20 FEET IN WIDTH MEASURED AT THE RIGHT-OF-WAY LINE.
2. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED.
3. MINIMUM THICKNESS FOR APRONS: 6" P.C. CONCRETE ON 2" COMPACTED AGGREGATE SUB-BASE (CA-6 GRADATION), OR 3" BITUMINOUS SURFACE ON 6" COMPACTED AGGREGATE SUB-BASE (CA-6 GRADATION).
4. SIDEWALK SHALL EXTEND THROUGH THE DRIVEWAY.
5. DRIVEWAYS SHALL HAVE A MINIMUM SLOPE OF 2% AND A MAXIMUM SLOPE OF 8%.
6. DRIVEWAY APRONS SHALL HAVE A MINIMUM SLOPE OF 2% AND A MAXIMUM SLOPE OF 5%.
7. PATCHES ARE NOT ALLOWED IN NEW APRONS.

REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 6



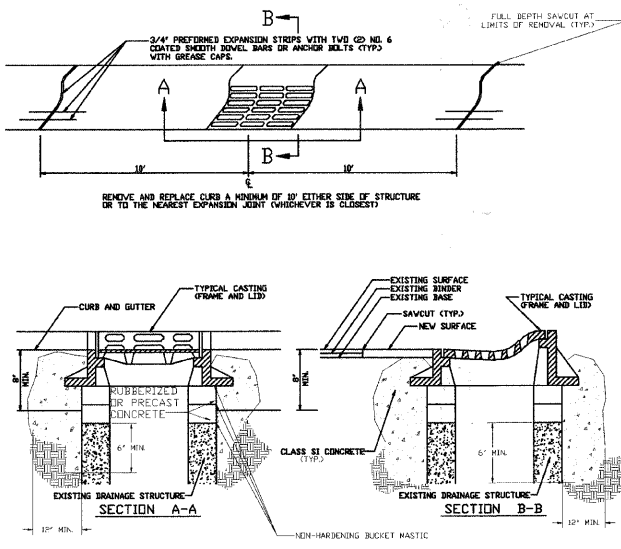
REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 7



GENERAL NOTES:

1. 3/4" PREFORMED BITUMINOUS EXPANSION JOINT MATERIAL WITH TWO #6 COATED SMOOTH DOVEL BARS (3/4" DIAMETER X 18") WITH GREASED CAPS SHALL BE PLACED EVERY 45 FEET. THEY SHALL ALSO BE PLACED AT 10' EITHER SIDE OF DRAINAGE STRUCTURES, P.C.'S, RADIUS POINTS, AND BACK OF CUL-DE-SACS. WHEN EXPANSION JOINTS ARE CONSTRUCTED ADJACENT TO EXISTING CURB AND GUTTER, THE EXISTING CURB SHALL BE DRILLED, AND TWO #6 COATED SMOOTH DOVEL BARS (3/4" DIAMETER X 18") SHALL BE GRouted IN PLACE. GREASE CAPS SHALL BE PLACED ON THE SIDE OF THE NEW CURB AND GUTTER AND SHALL HAVE A PINCHED STOP THAT WILL PROVIDE A MINIMUM 1" EXPANSION.
2. TODDLED CONTROL JOINTS OR SAWCUTS SHALL BE MADE EVERY 15 FEET.
3. SAWCUTS SHALL BE MADE WITHIN TWENTY-FOUR (24) HOURS AND SEALED WITH A VILLAGE APPROVED JOINT SEALANT. JOINTS SHALL BE CLEAN AND DRY PRIOR TO APPLICATION OF SEALANT.
4. TWO (2) #4 REBARS SHALL BE PLACED CONTINUOUS THROUGHOUT THE CURB AND GUTTER.

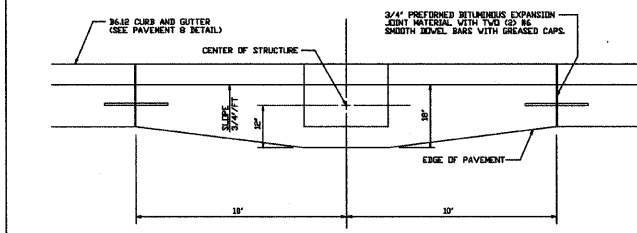
REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 8



GENERAL NOTES:

1. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MINIMUM OF TWO TAPERED RUBBERIZED ADJUSTING RINGS SHALL BE USED FOR FINAL ADJUSTMENT. ONE CONCRETE RING NOT LESS THAN TWO INCHES THICK MAY ALSO BE USED. A MAXIMUM OF THREE ADJUSTING RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. THE RINGS AND FRAME SHALL BE SET ON A BED OF NON-HARDENING BUCKET MASTIC.
2. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT AND SHALL HAVE A MINIMUM THICKNESS OF TWO (2) INCHES.
3. MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS.
4. ALL REMOVABLE CASTINGS SHALL BE ORIENTED SO THE OPENING IN THE GRATE PROVIDES THE MAXIMUM HYDRAULIC EFFICIENCY.

REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	STORM 8



GENERAL NOTES:

1. STORM SEWER CASTING SHALL BE MEEHAW R-3278-1, EAST JORDAN 7221, OR EQUIVALENT AS APPROVED BY THE VILLAGE ENGINEER.

REV. 1	REV. 1	REV. 1	VILLAGE OF LOMBARD
DATE: 2-16-98	DATE: 2-16-98	DATE: 2-16-98	PAVEMENT 9

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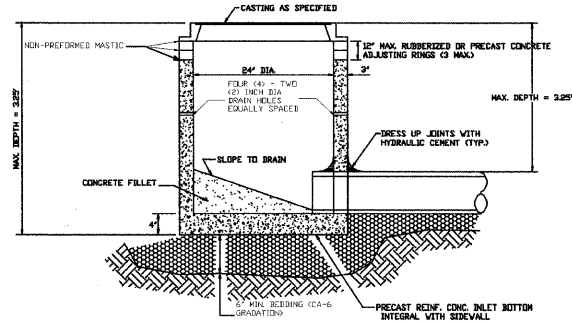
DESIGNED - TMS	REVISED -
DRAWN - KAR	REVISED -
CHECKED - JWF	REVISED -
DATE - 06-26-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

VILLAGE OF LOMBARD STANDARD DETAILS

SCALE: HORIZ. 1"=20' / VERT. 1"=5' PROJECT NO: 090317 STA. TO STA.

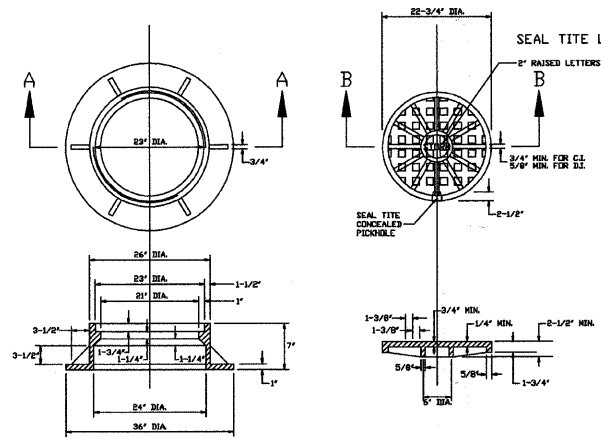
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	28
CONTRACT NO. 63351				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-8003(421)				



GENERAL NOTES:

1. PROVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION. CONCRETE BLOCK CONSTRUCTION IS NOT PERMITTED.
2. PROVIDE GRANULAR BACKFILL AROUND INLET TO SUBGRADE ELEVATION IN PAVED AREAS. MATERIAL SHALL MEET THE REQUIREMENTS OF IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COARSE AGGREGATE (CA-6 GRADATION).
3. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MINIMUM OF TWO TAPERED RUBBERIZED ADJUSTING RINGS SHALL BE USED FOR FINAL ADJUSTMENT. ONE CONCRETE RING NOT LESS THAN TWO INCHES THICK MAY ALSO BE USED. A MAXIMUM OF THREE ADJUSTING RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. EACH RING AND THE FRAME SHALL BE SET IN A BED OF NON-PREFORMED MASTIC.
4. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT AND SHALL HAVE A MINIMUM THICKNESS OF TWO INCHES.
5. WITHIN NON-PAVED AREAS, MORTAR SHALL ONLY BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME ON THE EXTERIOR OF THE STRUCTURE. MORTAR IS NOT PERMITTED ON THE INSIDE OF THE RINGS AND/OR FRAME.
6. IN PAVED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE COVERED WITH FILTER FABRIC. FILTER FABRIC SHALL BE SECURED TO THE OUTSIDE OF STRUCTURE PRIOR TO BACKFILL.
7. IN GRASSED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE PLUGGED WITH HYDRAULIC CEMENT.
8. IF AN IDOT TYPE B GRATE CASTING IS CALLED OUT, THE FRAME SHALL BE SET IN A BED OF PREFORMED MASTIC.

REV. 1	ESH	REV. 01-06-09	INLET TYPE A	VILLAGE OF LOMBARD
REV. 2	ESH	REV. 2-14-09		
DRAWN BY: V.JG	DATE: 2-16-08			STORM 2



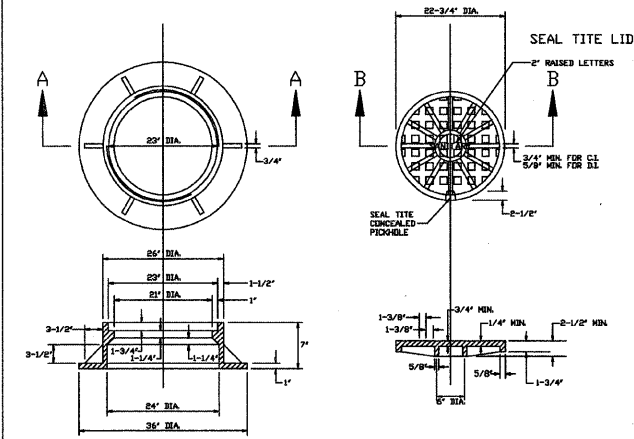
SECTION A-A
CAST FRAME

SECTION B-B
CAST CLOSED LID
SEAL TITE CONCEALED PICKHOLE

GENERAL NOTES:

1. DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.
2. ALL FRAMES AND COVERS SHALL HAVE A MACHINED HORIZONTAL AND VERTICAL BEARING SURFACES. PICK HOLES IN THE COVER SHALL NOT BE OPEN.
3. THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.
4. DIMENSIONS FOR CASTINGS ARE COMPARABLE TO EAST JORDAN IRON WORKS, INC. 1022-3 OR NEENAH FOUNDRY 1772-C FURNISHED WITH TYPE F CONCEALED PICK HOLES.
5. WATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INUNDATION. (NEENAH R-1916-F, EAST JORDAN 1022-3 WT WITH TYPE 5 CLOSED PICK HOLES OR APPROVED EQUAL).

REV. 1	ESH	REV. 09-09-09	STORM MANHOLE	VILLAGE OF LOMBARD
REV. 2	ESH	REV. 2-16-08	FRAME AND COVER	
DRAWN BY: V.JG	DATE: 2-16-08			STORM 3B



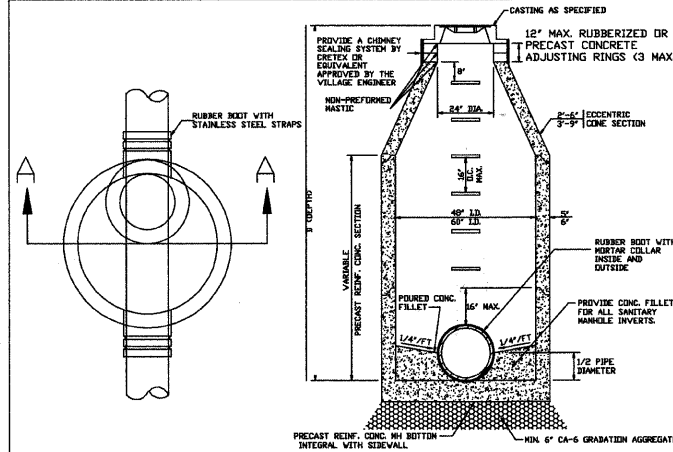
SECTION A-A
CAST FRAME

SECTION B-B
CAST CLOSED LID
SEAL TITE CONCEALED PICKHOLE

GENERAL NOTES:

1. DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.
2. ALL FRAMES AND COVERS SHALL HAVE A MACHINED HORIZONTAL AND VERTICAL BEARING SURFACES. PICK HOLES IN THE COVER SHALL NOT BE OPEN.
3. THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.
4. DIMENSIONS FOR CASTINGS ARE COMPARABLE TO EAST JORDAN IRON WORKS, INC. 1022-3 OR NEENAH FOUNDRY 1772-C FURNISHED WITH TYPE F CONCEALED PICK HOLES.
5. WATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INUNDATION. (NEENAH R-1916-F, EAST JORDAN 1022-3 WT WITH TYPE 5 CLOSED PICK HOLES OR APPROVED EQUAL).

REV. 1	ESH	REV. 09-09-09	SANITARY MANHOLE	VILLAGE OF LOMBARD
REV. 2	ESH	REV. 2-16-08	FRAME AND COVER	
DRAWN BY: V.JG	DATE: 2-16-08			SANITARY 3



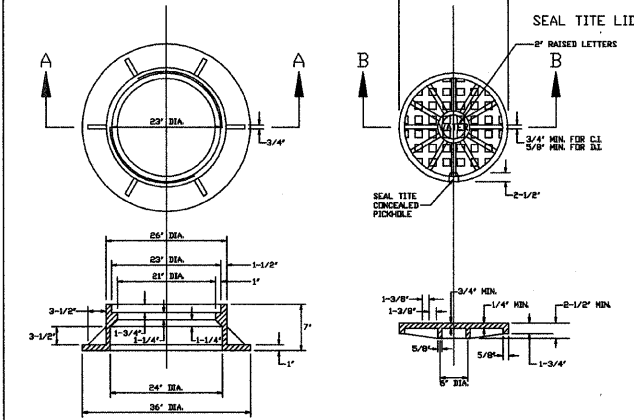
PLAN

SECTION A-A

GENERAL NOTES:

1. PROVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION. CONCRETE BLOCK CONSTRUCTION IS NOT PERMITTED.
2. PROVIDE SELECT GRANULAR BACKFILL AROUND MANHOLE TO SUBGRADE ELEVATION IN PAVED AREAS. MATERIAL SHALL MEET THE REQUIREMENTS OF IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" COARSE AGGREGATE CA-6 GRADATION, OR AS OTHERWISE DIRECTED BY THE VILLAGE ENGINEER.
3. APPLY A CONTINUOUS LAYER OF NON-HARDENING PREFORMED BITUMINOUS MASTIC MATERIAL CRUB-R-NEK OR EZ STICK TO EACH JOINT BELOW THE BOTTOM OF CONE OR FLATTOP TO PREVENT INFLOW.
4. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MINIMUM OF TWO TAPERED RUBBERIZED ADJUSTING RINGS SHALL BE USED FOR FINAL ADJUSTMENT. ONE CONCRETE RING NOT LESS THAN TWO INCHES THICK MAY ALSO BE USED. A MAXIMUM OF THREE ADJUSTING RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. EACH RING AND THE FRAME SHALL BE SET ON A BED OF NON-PREFORMED MASTIC.
5. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT.
6. MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME.
7. ONLY PLASTIC POLYMER STEPS SHALL BE USED.
8. WHEN MANHOLE DEPTH IS OVER 12 FEET, THE THICKNESS OF THE BASE SHALL BE A MINIMUM OF 10 INCHES. WHEN MANHOLE DEPTH IS LESS THAN 12 FEET, THE THICKNESS SHALL BE A MINIMUM OF 8 INCHES.
9. DRESS UP INTERIOR JOINTS WITH HYDRAULIC CEMENT.
10. SANITARY MANHOLES SHALL BE CONSTRUCTED WITH A CHIMNEY SEALING SYSTEM BY CRETIX OR AN EQUAL APPROVED BY THE VILLAGE. CHIMNEY SEALS SHALL BE REQUIRED UNLESS THE MANHOLE IS ADJUSTED TO FINAL GRADE IN ACCORDANCE WITH VILLAGE DETAIL STORM 7 - CASTING ADJUSTMENTS FOR STRUCTURES IN PAVED AREAS.

REV. 1	ESH	REV. 01-06-09	SANITARY MANHOLE	VILLAGE OF LOMBARD
REV. 2	ESH	REV. 7-14-09		
DRAWN BY: V.JG	DATE: 2-16-08			SANITARY 1



SECTION A-A
CAST FRAME

SECTION B-B
CAST CLOSED LID
SEAL TITE CONCEALED PICKHOLE

GENERAL NOTES:

1. DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.
2. ALL FRAMES AND COVERS SHALL HAVE A MACHINED HORIZONTAL AND VERTICAL BEARING SURFACES. PICK HOLES IN THE COVER SHALL NOT BE OPEN.
3. THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.
4. DIMENSIONS FOR CASTINGS ARE COMPARABLE TO EAST JORDAN 1022-3 OR NEENAH FOUNDRY 1772-C.
5. WATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INUNDATION. (NEENAH R-1916-F, EAST JORDAN 1022-3 WT OR APPROVED EQUAL).

REV. 1	ESH	REV. 09-09-09	VALVE VAULT	VILLAGE OF LOMBARD
REV. 2	ESH	REV. 2-16-08	FRAME AND COVER	
DRAWN BY: V.JG	DATE: 2-16-08			WATER 5

I:\LDM\090317-St. Chor LAPP\CADD-SURVEY\DRAWINGS\DWGS\090317-VILLAGE-DET.dwg - 02
 Plot: 2-09-10 @ 1:41pm By: 560kar
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DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED - PER VILLAGE REVIEW 01-18-10
CHECKED - JWF	REVISED -
DATE - 06-26-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

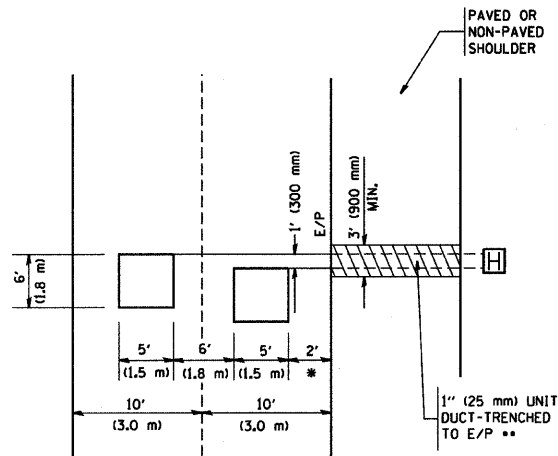
VILLAGE OF LOMBARD STANDARD DETAILS

SCALE: HORIZ. 1"=20' / VERT. 1"=5' PROJECT NO. 090317 STA. TO STA.

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	29
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AD. PROJECT M-ARA-9003(421)				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

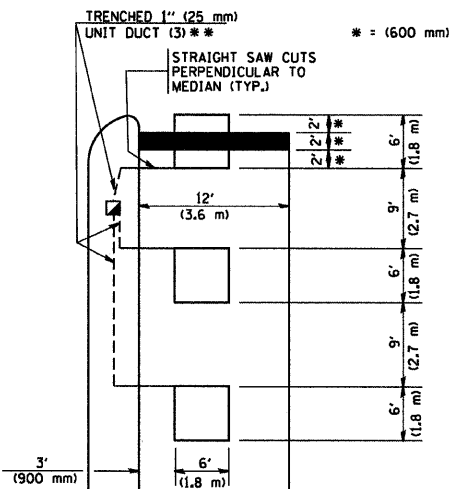


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

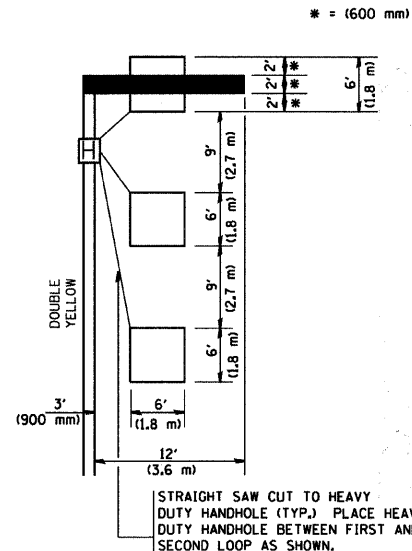
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

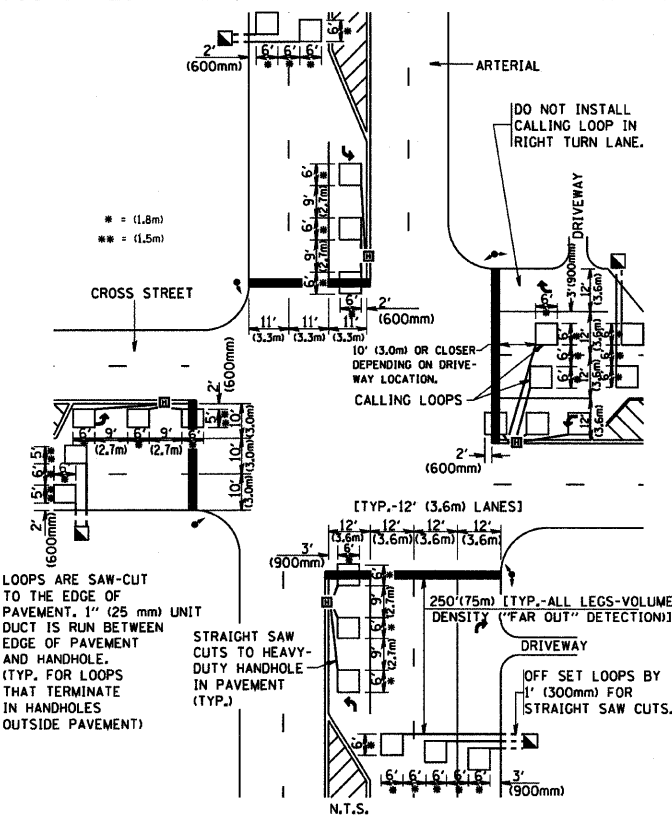
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



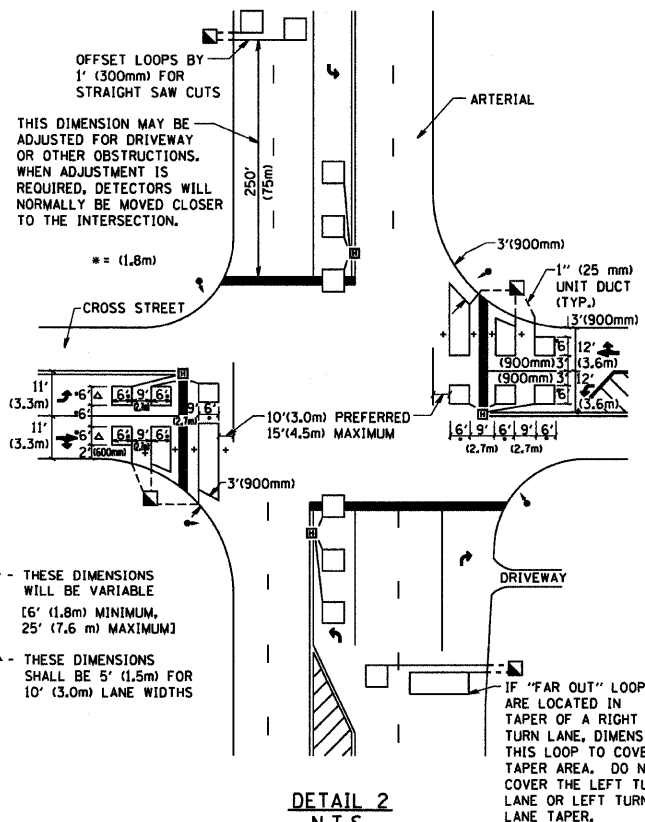
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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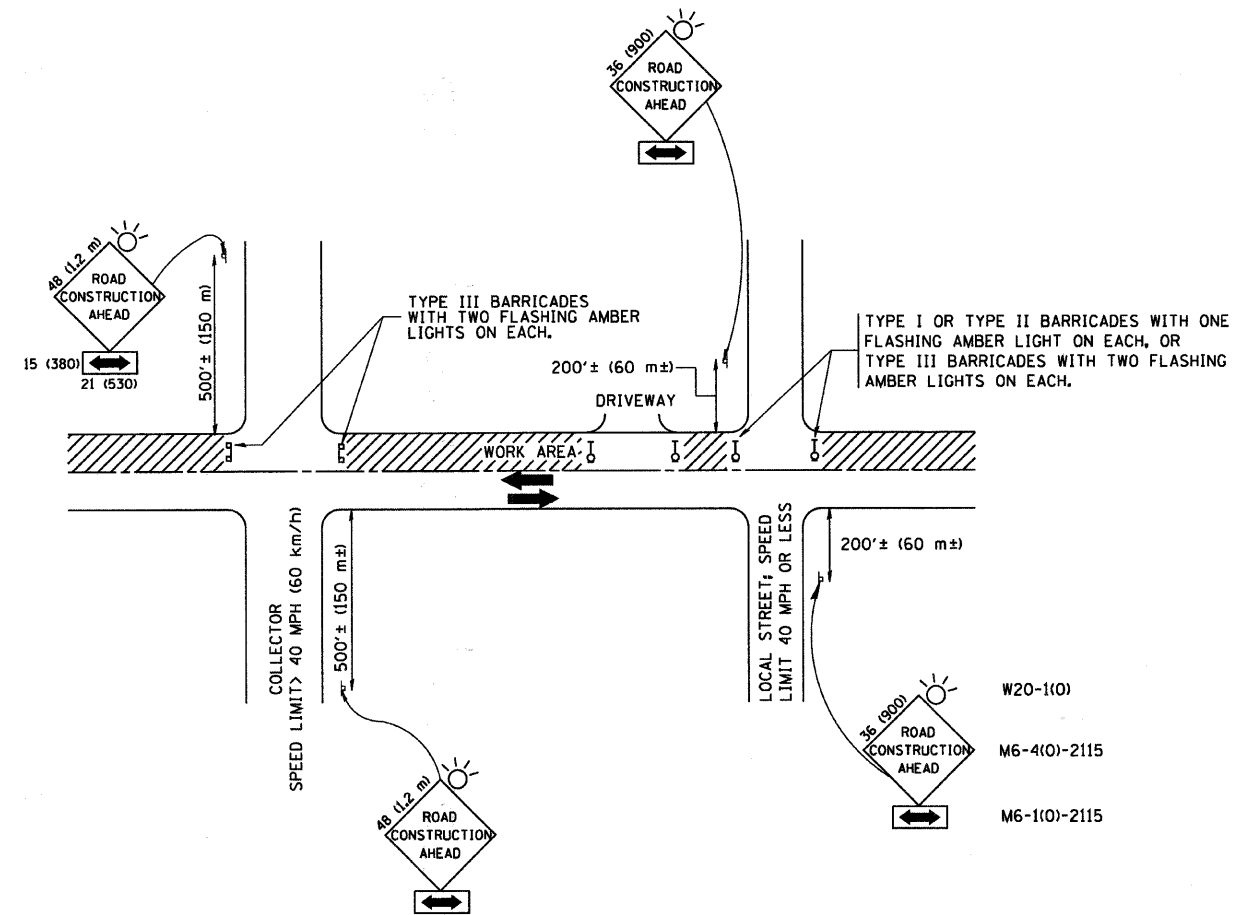
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	30
TS-07			CONTRACT NO. 63351	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-ARA-9003(421)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

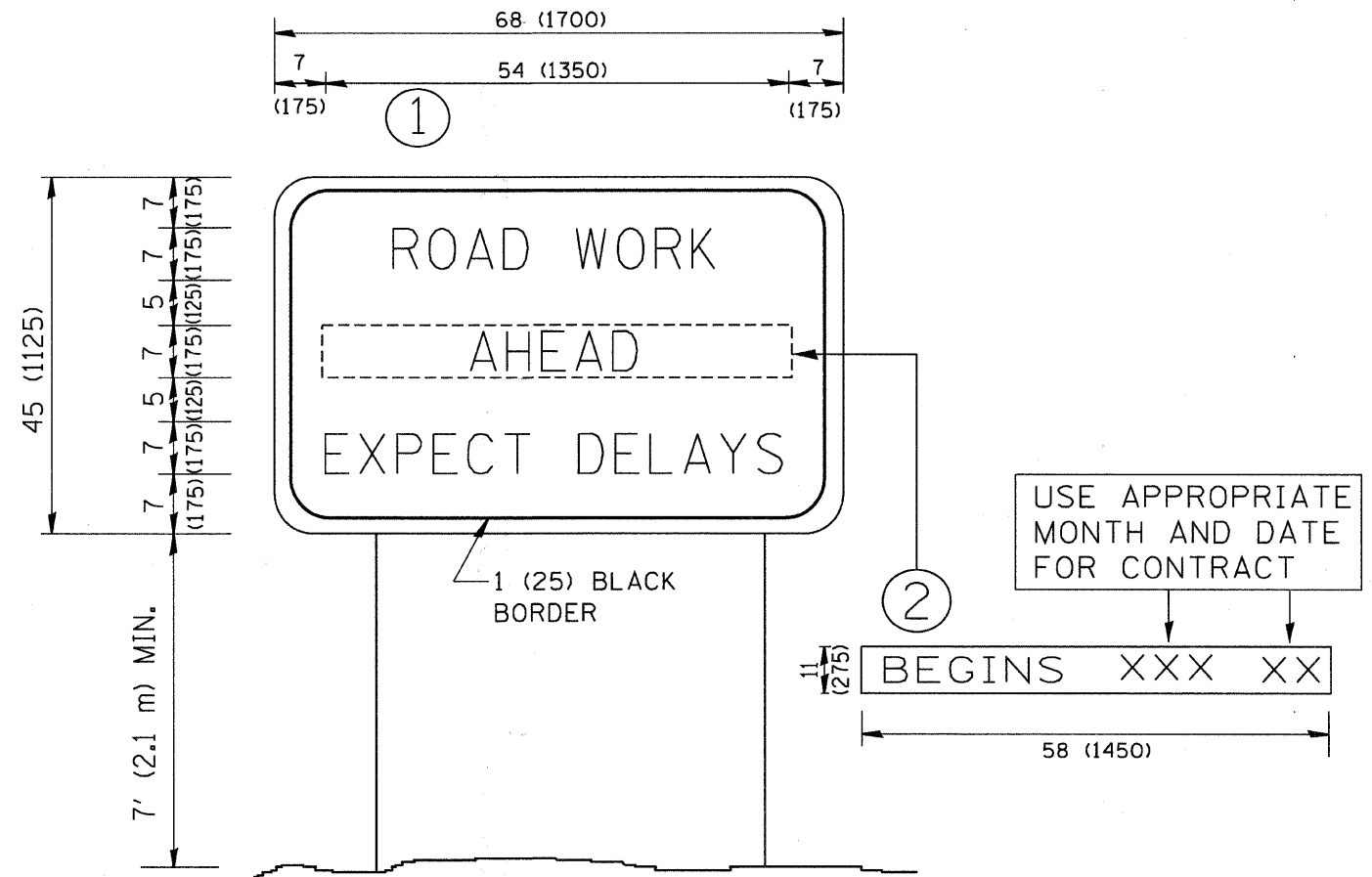
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DuPAGE	36	31
TC-10			CONTRACT NO. 63351	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

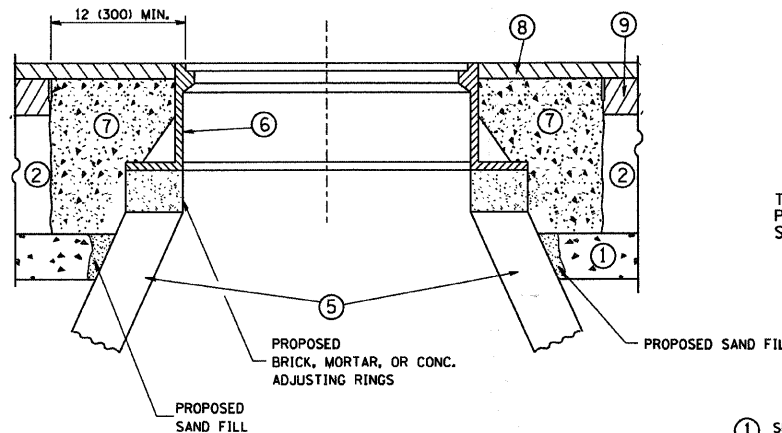
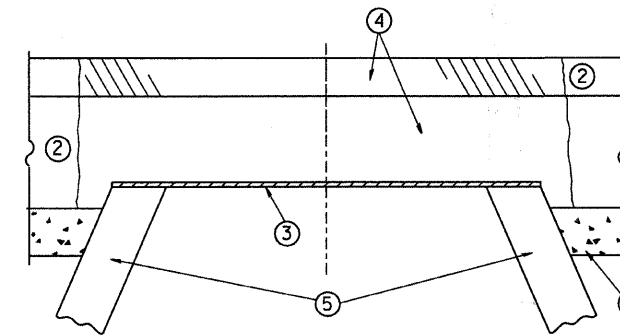
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		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 32
TC-22			CONTRACT NO. 63351	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARRA-9003(421)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

~~CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.~~

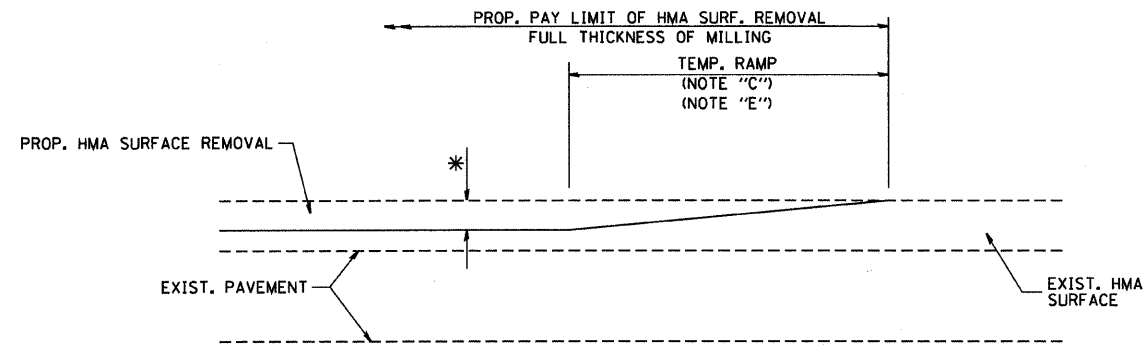
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

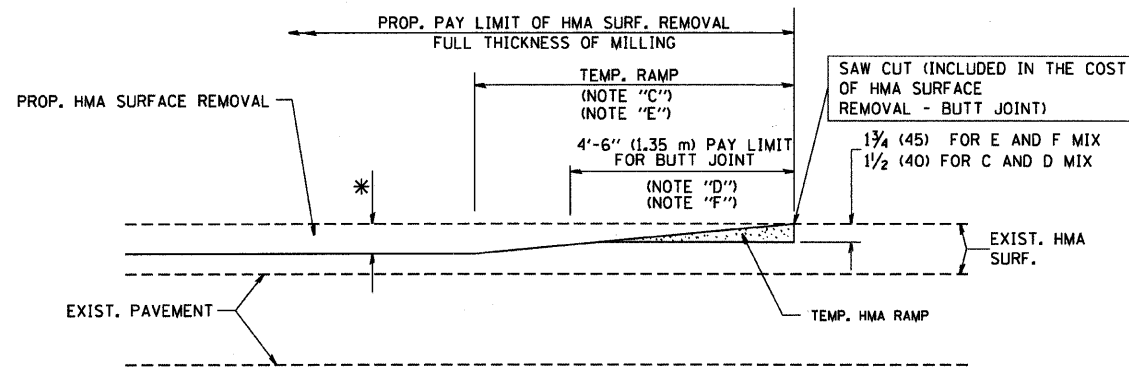
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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	1397	09-00153-00-RS	DuPAGE	36	33
		PLOT SCALE = 50.0000' / IN.	REVISED - R. WIEDEMAN 05-14-04					BD600-03 (BD-8)			CONTRACT NO. 63351	
		PLOT DATE = 1/4/2008	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(421)				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

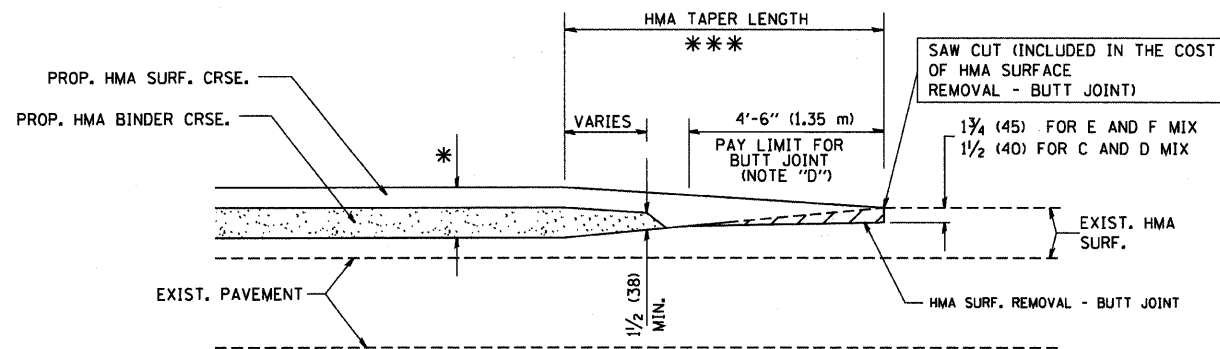
OPTION 1



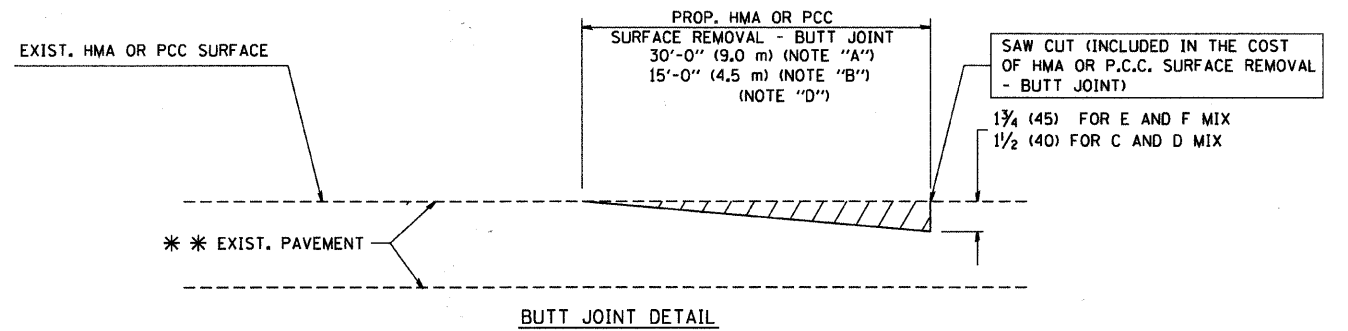
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

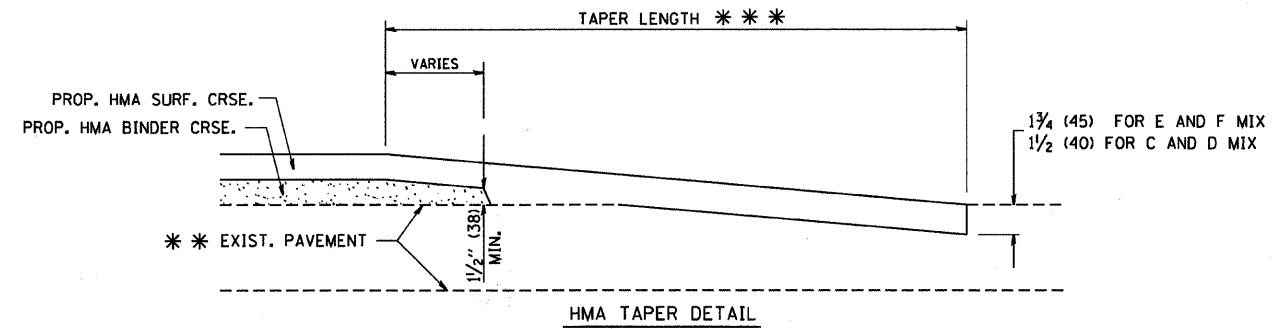
TYPICAL TEMPORARY RAMP



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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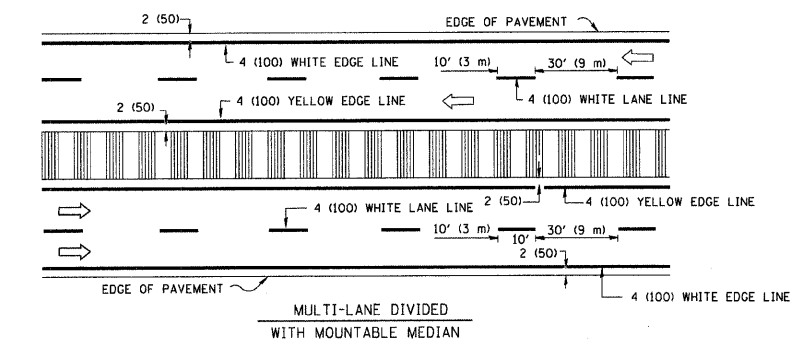
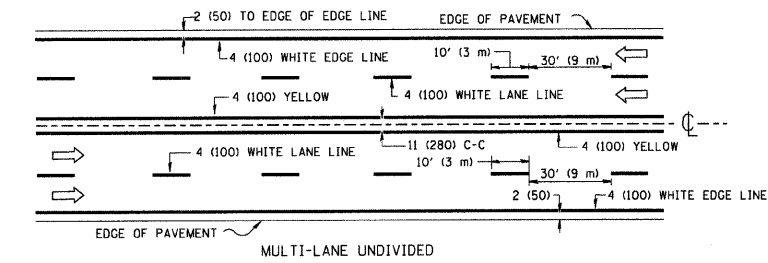
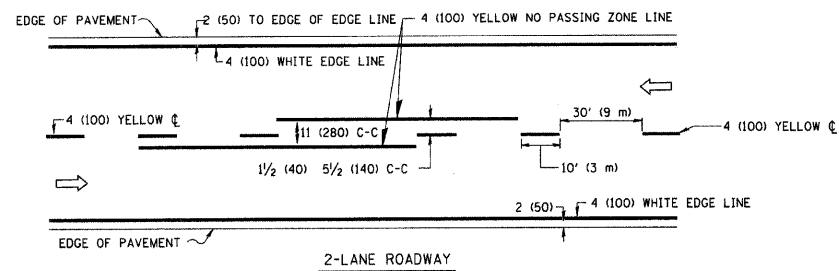
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PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

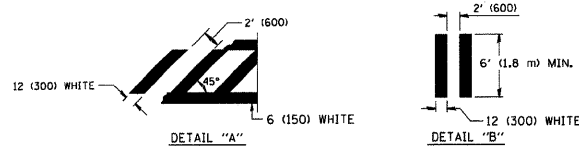
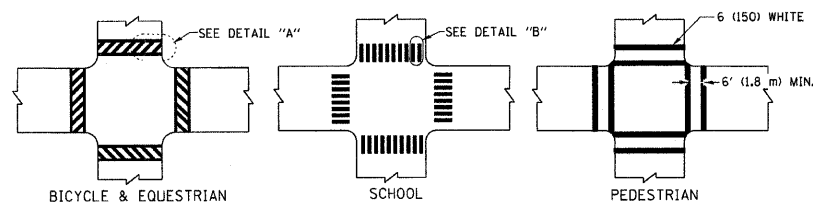
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F.A.P. RTE. 1397	SECTION 09-00153-00-RS	COUNTY DuPAGE	TOTAL SHEETS 36	SHEET NO. 34
BD400-05 BD32			CONTRACT NO. 63351	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003 (421)				

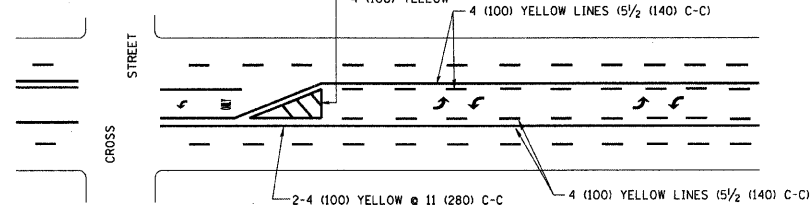
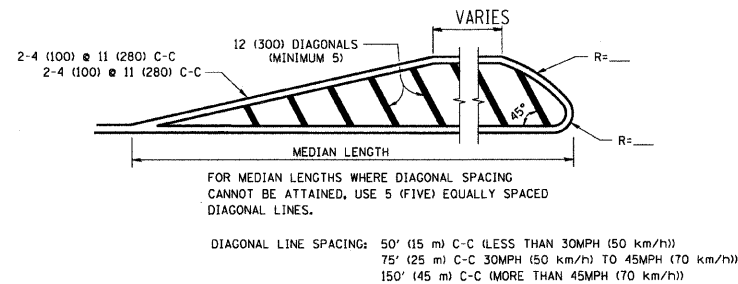
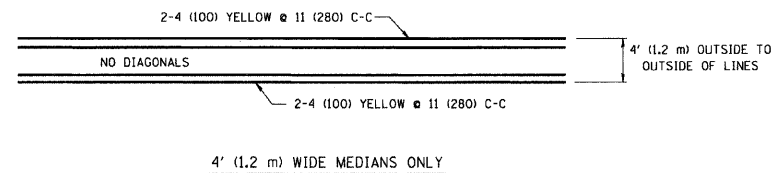


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

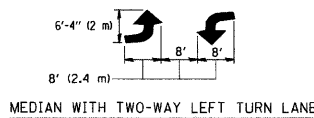
TYPICAL LANE AND EDGE LINE MARKING



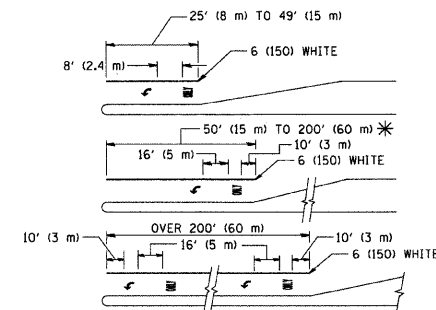
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



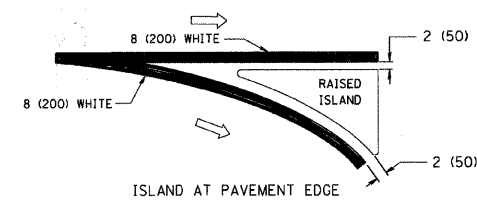
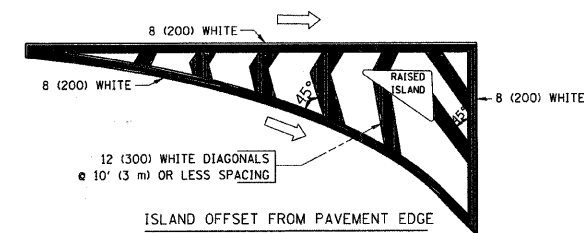
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

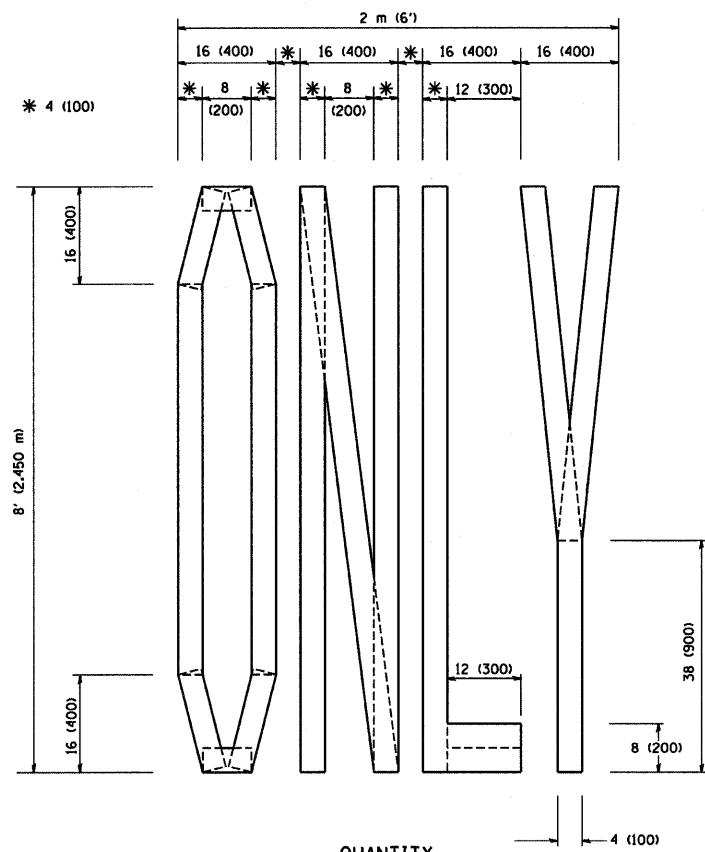


TYPICAL ISLAND MARKING

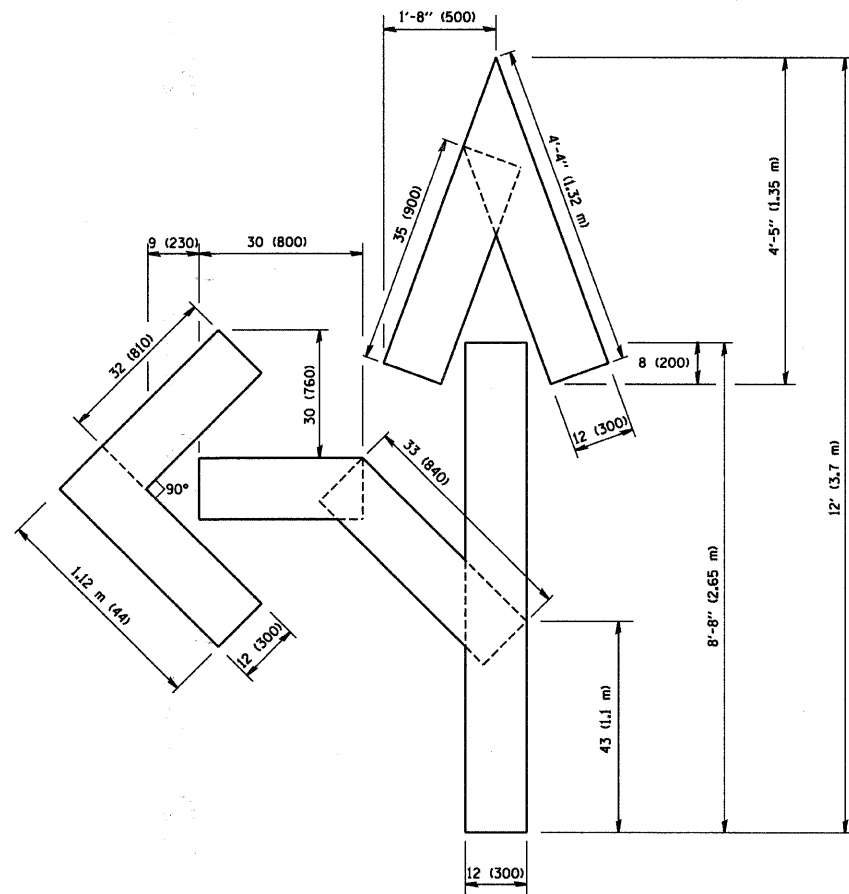
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

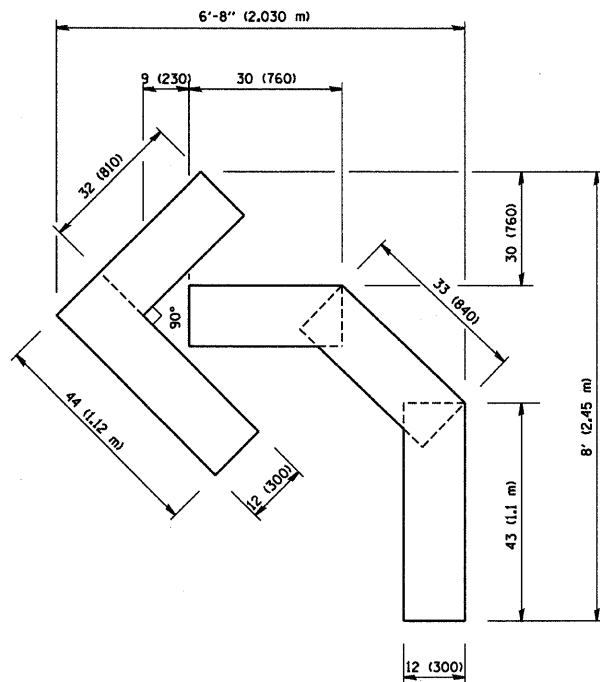
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =
 W:\dist\d\22\34\1016.dgn

USER NAME = gogli.enobt	DESIGNED -	REVISED - T. RAMMACHER 06-05-96
	DRAWN -	REVISED - T. RAMMACHER 11-04-97
PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 03-02-98
PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	09-00153-00-RS	DUPAGE	36	36
TC-16			CONTRACT NO. 63351	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003 (421)				