

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-RS-1	COOK	23 *	1
		ILLINOIS	CONTRACT NO. 62G21	

\*23 + 7 = 30 TOTAL SHEETS

D-91-287-18



LOCATION OF SECTION INDICATED THUS: —

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE CITY OF CHICAGO.

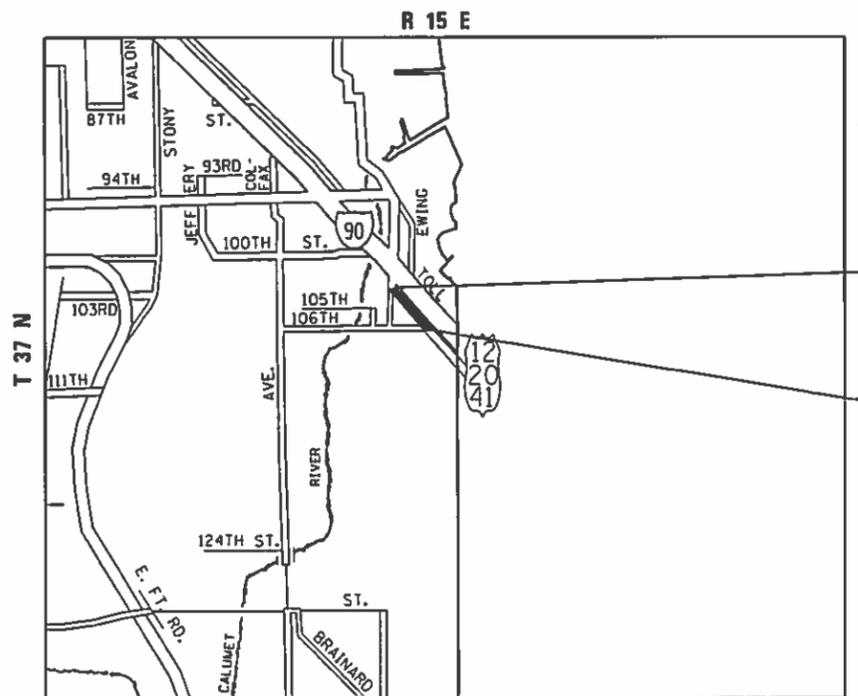
**TRAFFIC DATA**

ADT (2017) = 12,000  
POSTED SPEED LIMIT = 30 MPH

# PROPOSED HIGHWAY PLANS

F.A.P. ROUTE 029: US 12 /20 /41 (INDIANAPOLIS AVE.)  
EWING AVE. TO 106TH ST.  
SECTION: 2018-007-RS-1  
PROJECT: NHPP-URNE(679)  
RESURFACING; PEDESTRIAN RAMPS  
COOK COUNTY

C-91-185-18

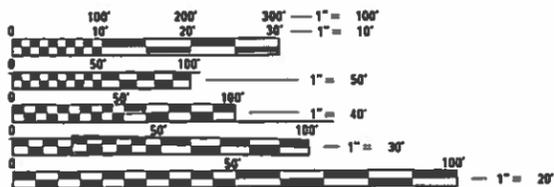


PROJECT  
BEGINS: STA. 13 + 62.7

PROJECT  
ENDS: STA. 59 + 47.8

HYDE PARK TOWNSHIP

GROSS & NET LENGTH = 4585.1 FT. = 0.868 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N  
CHICAGO UTILITY ALERT NETWORK  
1-312-744-7000

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3056  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62G21

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED DECEMBER 4, 2018

*Anthony A. Wraggley*  
PERSONAL ENGINEER

*Feb 19 2019*

*So E. ETC*  
ENGINEER OF DESIGN AND ENVIRONMENT

*Feb 19 2019*

*Paul P. Ch...*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

**STATE STANDARDS**

SHEET NO.	DESCRIPTION
1.	TITLE SHEET
2.	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-5.	SUMMARY OF QUANTITIES
6.	TYPICAL SECTIONS
7-8.	ROADWAY & PAVEMENT MARKING PLANS
8a-8g.	PROPOSED SIDEWALK & DETECTABLE WARNINGS
9-10.	DETECTOR LOOP REPLACEMENT PLANS
11.	ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
12.	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
13.	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
13a.	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL. (BD-34)
13b.	CITY OF CHICAGO CATCH BASIN, INLET AND MANHOLE DETAILS (BD-47)
14.	CITY OF CHICAGO DETECTABLE WARNINGS (BD-58)
15.	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
16.	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
17.	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
18.	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
19.	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
20.	ARTERIAL ROAD INFORMATION SIGNING (TC-22)
21.	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)
22.	DRIVEWAY ENTRANCE SIGNING (TC-26)
23.	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STANDARD NO	DESCRIPTION
424001-11	PERPENDICULAR CURB RAMPS
424006-04	DIAGONAL CURB RAMPS
442201-03	CLASS C AND D PATCHES
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40MPH
701501-06	URBAN LANE CLOSURE, 2L, 2L, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND REVISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING ANY FORESTRY WORK FOR LAYOUT.
- THE CONTRACTOR WILL BE REQUIRED TO OBTAIN ALL NECESSARY PERMITS REQUIRED BY THE CITY OF CHICAGO FOR ANY TYPE OF FORESTRY WORK.
- THE CONTRACTOR SHALL COORDINATE WITH JIM NEESON FROM CHICAGO DEPARTMENT OF TRANSPORTATION, DIVISION OF ELELCTRICAL OPERATION BEFORE INSTALLING DETECTOR LOOPS.

**GENERAL NOTES:**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL C.U.A.N. (CHICAGO UTILITY ALERT NETWORK) AT (312)744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.
- THE LICENSED SEWER CONTRACTOR/SUB-CONTRACTOR MUST SUBMIT TWO (2) SETS OF PLANS APPROVED BY THE DEPARTMENT OF SEWERS FOR THE ISSUE OF THE SEWER PERMIT IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF SEWERS.
- PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.
- OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 11\*2INCHES (40MM) WHERE THE SPEED LIMIT IS 40 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b>	<b>INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI28718\Design\DI28718-sht-plen.dgn		CHECKED -	REVISED -			029	2018-007-RS-1	COOK	/	/	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			<b>CONTRACT NO. 62G21</b>					
PLOT DATE = 1/15/2019						SHEET OF SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0005				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005				
20101300	TREE PRUNING ( 1 TO 10 INCH DIAMETER)	EACH	35	35				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	16	16				
20200100	EARTH EXCAVATION	CU YD	14	14				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	43	43				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1.2	1.2				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1.2	1.2				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1.2	1.2				
25200110	SODDING, SALT TOLERANT	SQ YD	43	43				
25200200	SUPPLEMENTAL WATERING	UNIT	1.0	1.0				
40500001	FIBER-MODIFIED ASPHALT CRACK SEALING	FOOT	240	240				
40600290	BITUMINOUS MATERIALS ( TACK COAT)	POUND	25346	25346				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	55	55				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1569	1569				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	165	165				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0005				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005				
40600990	TEMPORARY RAMP	SQ YD	132	132				
40603365	HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	3160	3160				
42101300	PROTECTIVE COAT	SQ YD	350	350				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1388	1388				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	36037	36037				
44000600	SIDEWALK REMOVAL	SQ FT	1388	1388				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	12611	12611				
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	1120	1120				
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SQ YD	152	152				
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SQ YD	574	574				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	2	2				
60406530	FRAMES AND LIDS, CLOSED LID (CITY OF CHICAGO)	EACH	4	4				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	22	22				

FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US RTE 12 /20 /41 (INDIANAPOLIS AVE.) EWING AVE. - 106TH ST. SUMMARY OF QUANTITIES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -			CONTRACT NO. 62G21					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT					

\*= SPECIALTY ITEM

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0005					SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0005					
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005						CODE NO	ITEM		UNIT	80% FED 20% STATE 0005				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1														
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1														
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	20	20														
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1														
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6														
67100100	MOBILIZATION	L SUM	1	1														
70100350	TRAFFIC CONTROL AND PROTECTION, STANDARD 701101	EACH	1	1														
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1														
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1														
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1														
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1														
70300100	SHORT TERM PAVEMENT MARKING	FOOT	17016	17016														
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	5672	5672														
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	285	285														
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	15330	15330														
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	495	495														
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	920	920														
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1437	1437														
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	247	247														
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	4254	4254														
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	285	285														
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15330	15330														
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	495	495														
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	920	920														
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1437	1437														

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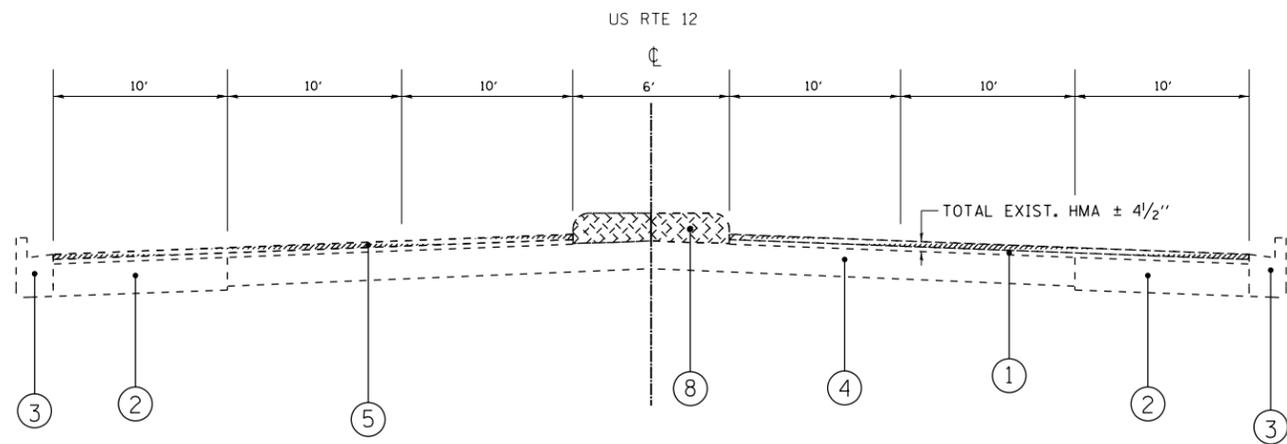
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US RTE 12 /20 /41 (INDIANAPOLIS AVE.) EWING AVE. - 106TH ST.  
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-RS-1	COOK	23	4
CONTRACT NO. 62G21				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\*= SPECIALTY ITEM

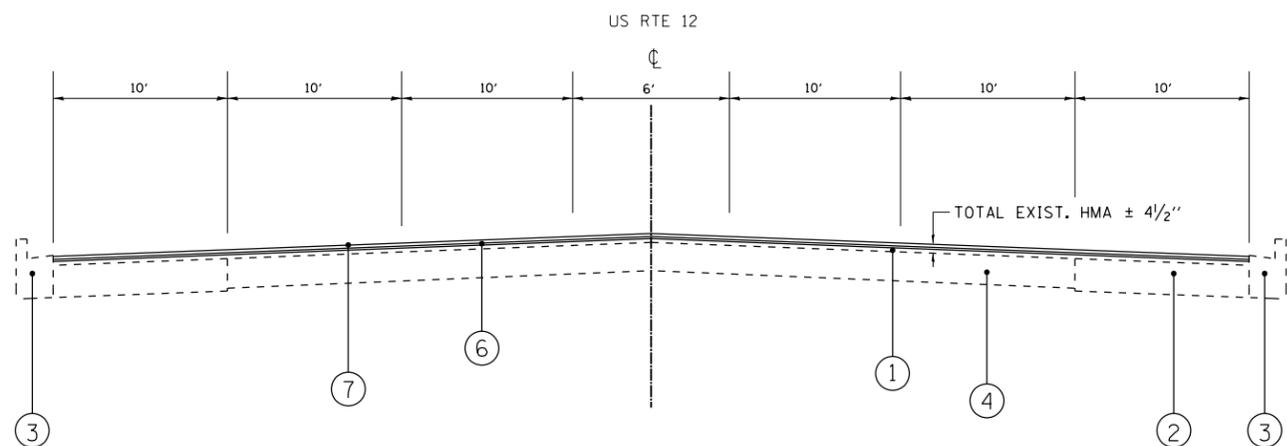




**EXISTING TYPICAL SECTION**

STA. 13+62.7 TO 59+47.8

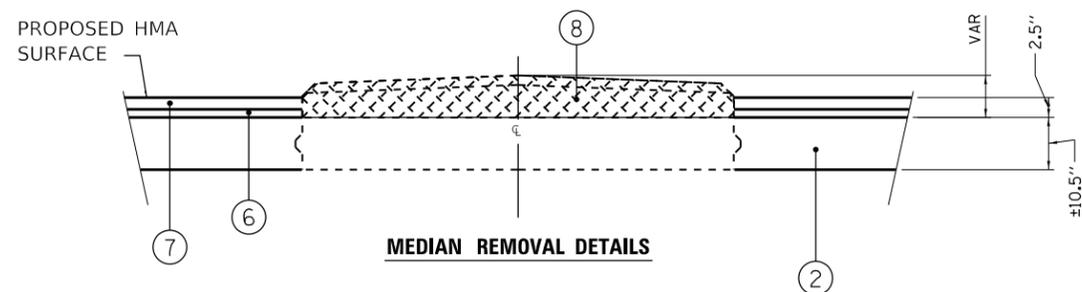
• MEDIAN REMOVAL ONLY APPLIES TO CORRUGATED MEDIANS FROM STA. 18+92.4 TO STA. 41+34.4



**PROPOSED TYPICAL SECTION**

STA. 13+62.7 TO 59+47.8

• MEDIAN REMOVAL ONLY APPLIES TO CORRUGATED MEDIANS FROM STA. 18+92.4 TO STA. 41+34.4



**MEDIAN REMOVAL DETAILS**

**LEGEND**

- ① EXISTING REMAINING HMA AFTER MILLING ±2"
- ② EXISTING P.C.C. PAVEMENT (±10 1/2")
- ③ EXISTING COMB. CURB & GUTTER, TYPE B-6.12
- ④ EXISTING BRICK OR P.C.C. PAVEMENT (±5 1/2" TO ±9 3/4")
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑥ PROPOSED POLY. LEVELING BINDER, MM, IL-4.75, N50, 3/4"
- ⑦ PROPOSED POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- ⑧ PROP. PARTIAL CORRUGATED PCC MEDIAN REMOVAL (SEE DETAIL)

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED LEVELING BINDER WHERE THE SURFACE JOINT WILL BE LOCATED

**HMA MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
POLY HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5 mm)	4% @ 70 GYR	QCP (NOTE 1)
POLYMERIZED LEVELING BINDER, (MM), IL 4.75, N50	3.5% @ 50 GYR	QCP (NOTE 1)
PAVEMENT PATCHING		
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

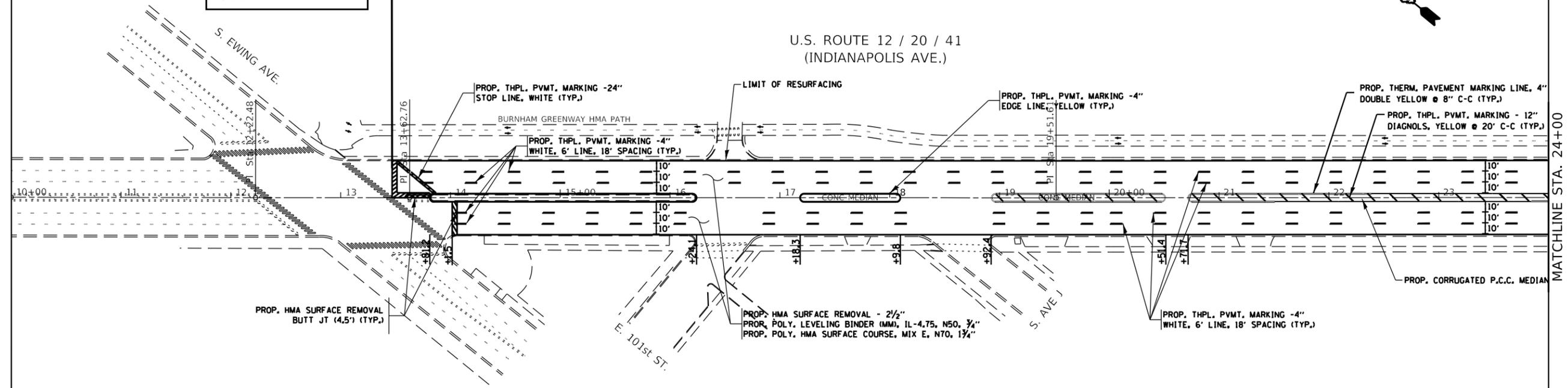
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

NOTE 1: THE DENSITY FOR THIS MIX WILL BE ASSIGNED 100 PERCENT PAY FACTOR, BE TESTED RANDOMLY AT THE FREQUENCY REQUIRED IN THE HMA-QUALITY CONTROL FOR PERFORMANCE SPECIAL PROVISION, AND SHALL MEET MINIMUM QC/QA DENSITY REQUIREMENTS IN ACCORDANCE WITH ART. 1030 OF THE STANDARD SPECIFICATIONS.

**NOTE:**  
**CONTRACTOR SHALL MILL FIRST BEFORE PATCHING**

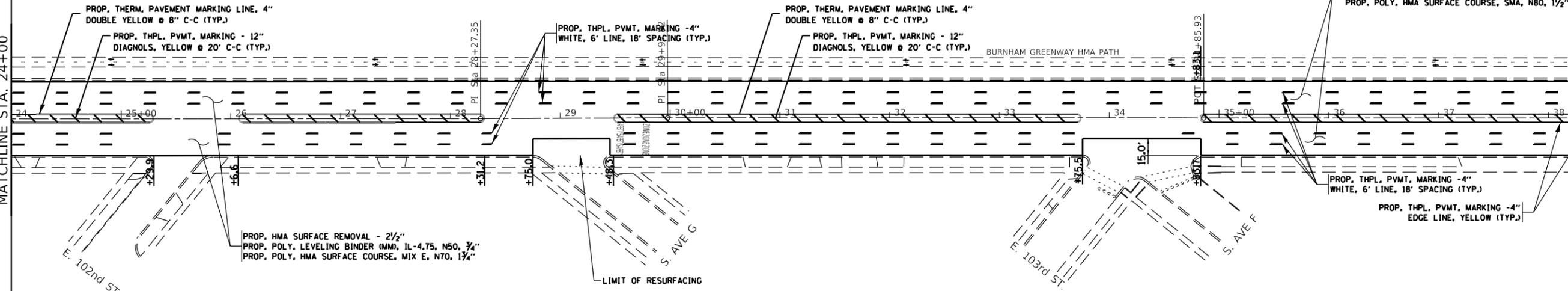
**IMPROVEMENT BEGINS  
STA. 13+46.8**

U.S. ROUTE 12 / 20 / 41  
(INDIANAPOLIS AVE.)



MATCHLINE STA. 24+00

U.S. ROUTE 12 / 20 / 41  
(INDIANAPOLIS AVE.)



MATCHLINE STA. 24+00

MATCHLINE STA. 39+00

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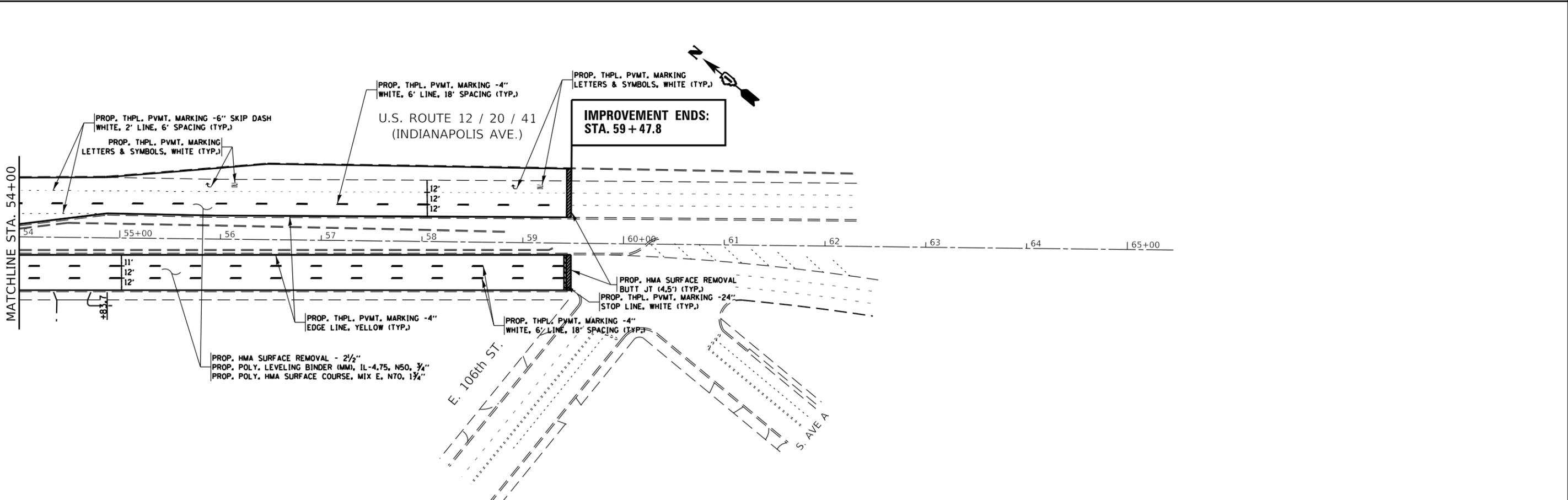
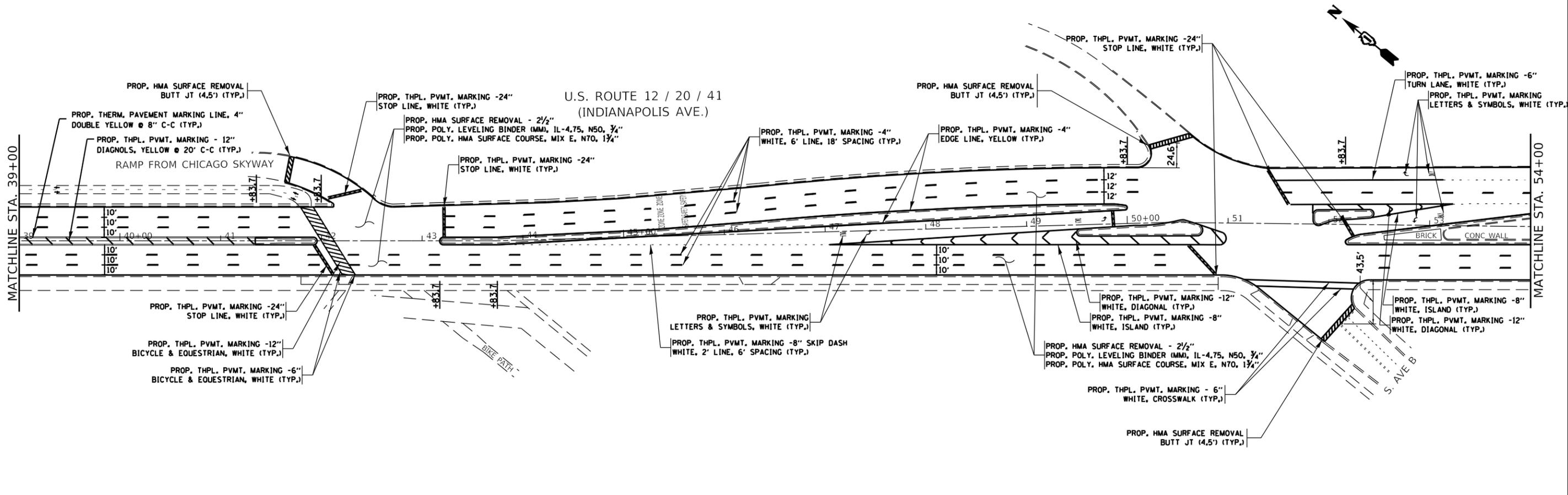
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PLOT DATE = 12/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
U.S. ROUTE 12 (INDIANAPOLIS AVE) EWING AVE. -106th ST.**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-RS-1	COOK	1	1
CONTRACT NO. 62G21				
ILLINOIS		FED. AID PROJECT		



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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/14/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN**  
**U.S. ROUTE 12 (INDIANAPOLIS AVE) EWING AVE. -106th ST.**

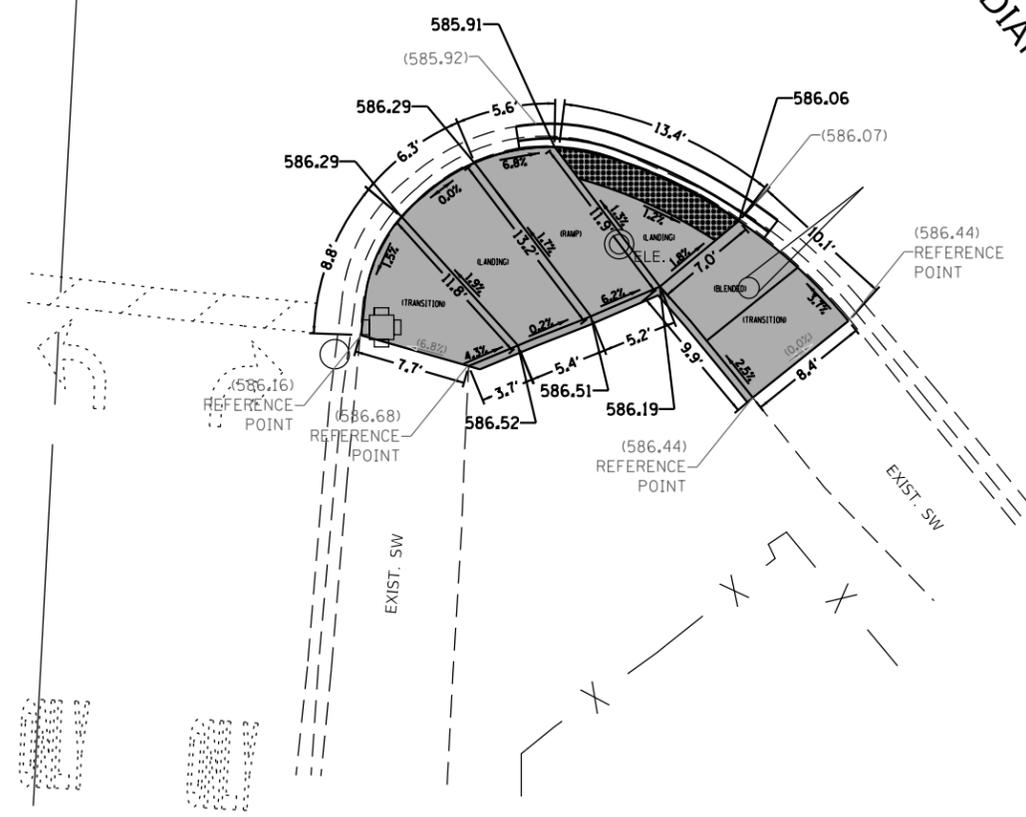
SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE. 029	SECTION 2018-007-RS-1	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 8
CONTRACT NO. 62G21				ILLINOIS FED. AID PROJECT



AVENUE B

U.S. ROUTE 12  
(INDIANAPOLIS AVENUE)



**LEGEND**

- XX.XX' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 588.69 FT  
 BENCHMARK : TOP SPINDLE OF FIRE HYDRANT  
 LOCATION : AT S. WEST CORNER OF E. AVE B AND US 12/20/41

MODEL: Default  
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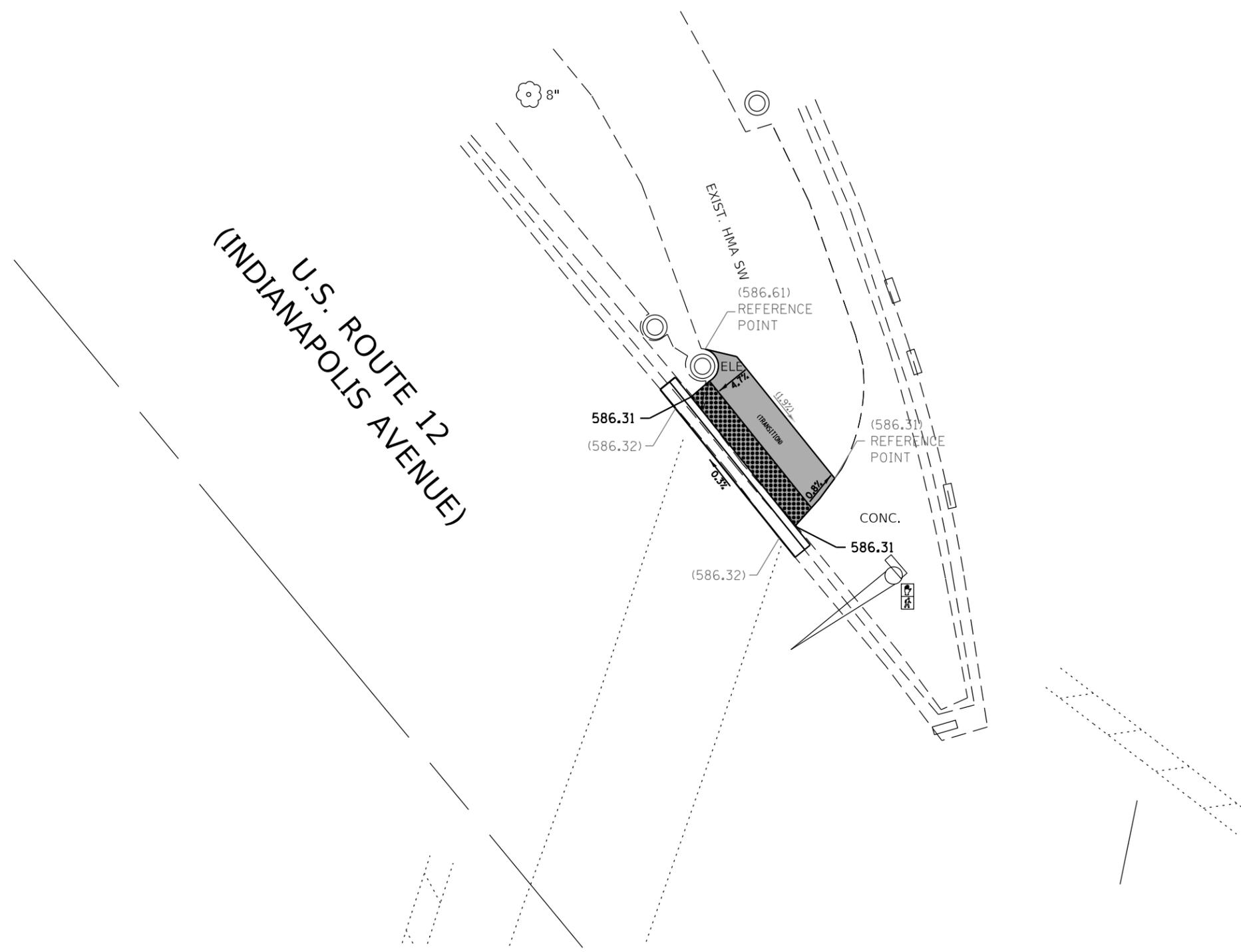
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED SIDEWALK PLAN  
US 12 /20 /41 AT E. AVE B, SOUTH WEST CORNER**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-R5-1	COOK	23	8a
CONTRACT NO. 62G21				
ILLINOIS FED. AID PROJECT				



U.S. ROUTE 12  
(INDIANAPOLIS AVENUE)

**LEGEND**

- XX.XX' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 589.24 FT  
BENCHMARK : TOP SPINDLE OF FIRE HYDRANT  
LOCATION : AT S. EAST CORNER OF BURNHAM GREENWAY AND US 12/20/41

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PLOT DATE = 12/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

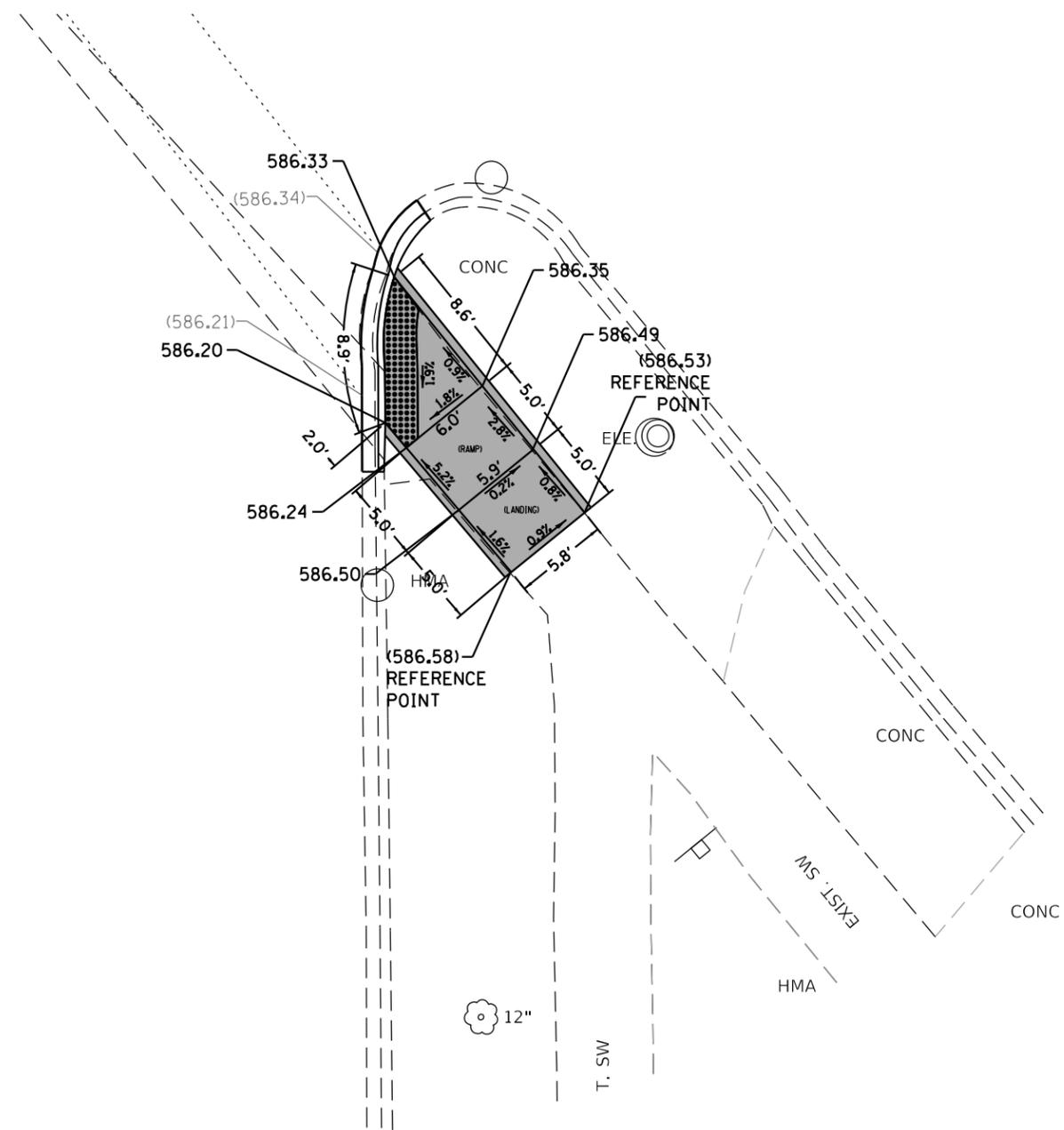
**PROPOSED SIDEWALK PLAN  
US 12 /20 /41 AT BURNHAM GREENWAY, NORTH EAST CORNER**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-R5-1	COOK	23	8b
CONTRACT NO. 62G21				
ILLINOIS FED. AID PROJECT				







**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 587.980 FT  
 BENCHMARK : S. EAST BOLT ON FIRE HYDRANT  
 LOCATION : AT S. EAST CORNER OF E. AVE J AND US 12/20/41

MODEL: Default  
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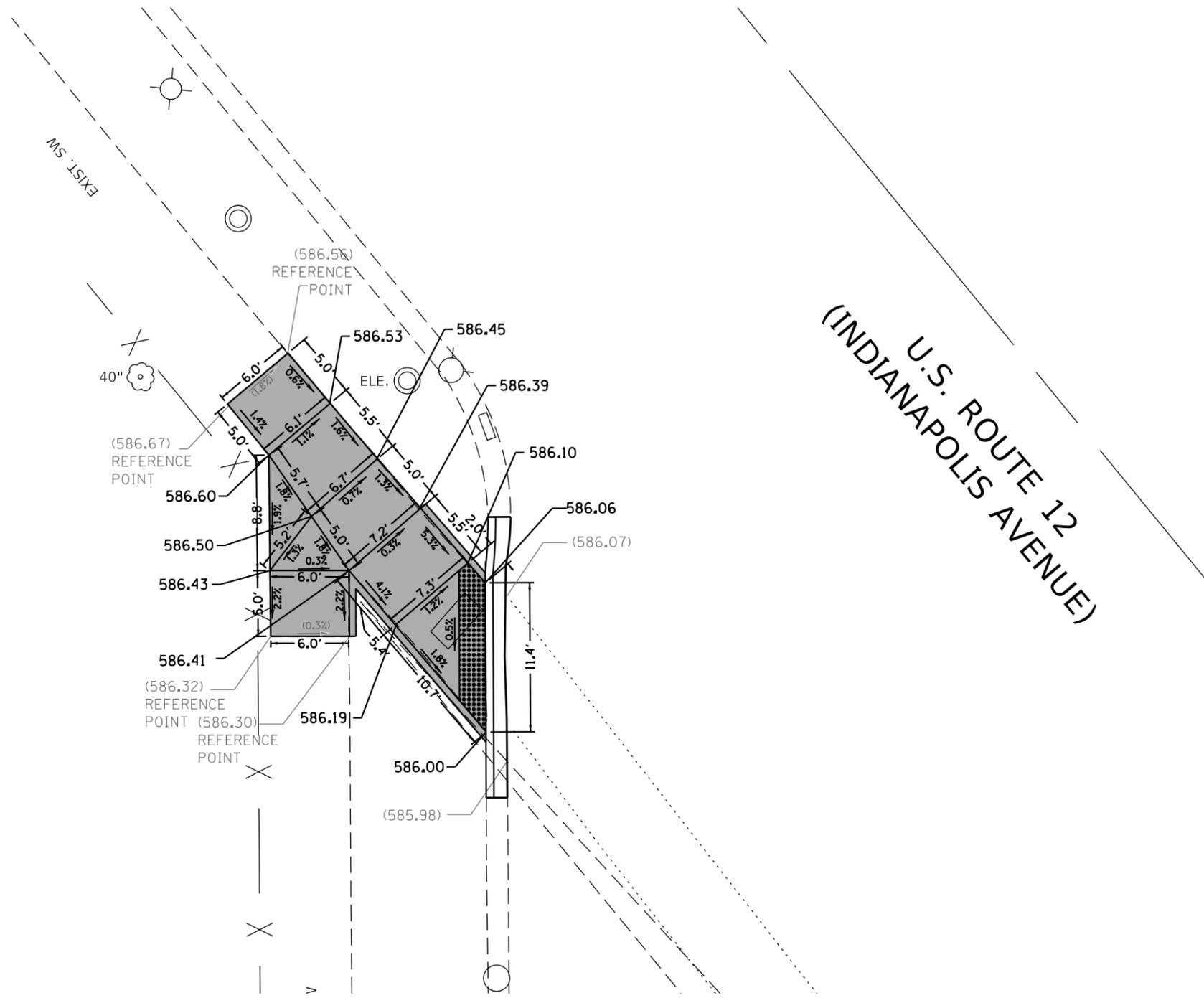
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED SIDEWALK PLAN  
US 12 /20 /41 AT E. AVE J, SOUTH WEST CORNER**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-R5-1	COOK	23	8e
CONTRACT NO. 62G21				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- XX.XX' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 587.980 FT  
 BENCHMARK : S. EAST BOLT AT FIRE HYDRANT  
 LOCATION : AT S. EAST CORNER OF E. 101ST ST AND US 12/20/41

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	DRAWN -	REVISED -
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PLOT DATE = 12/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

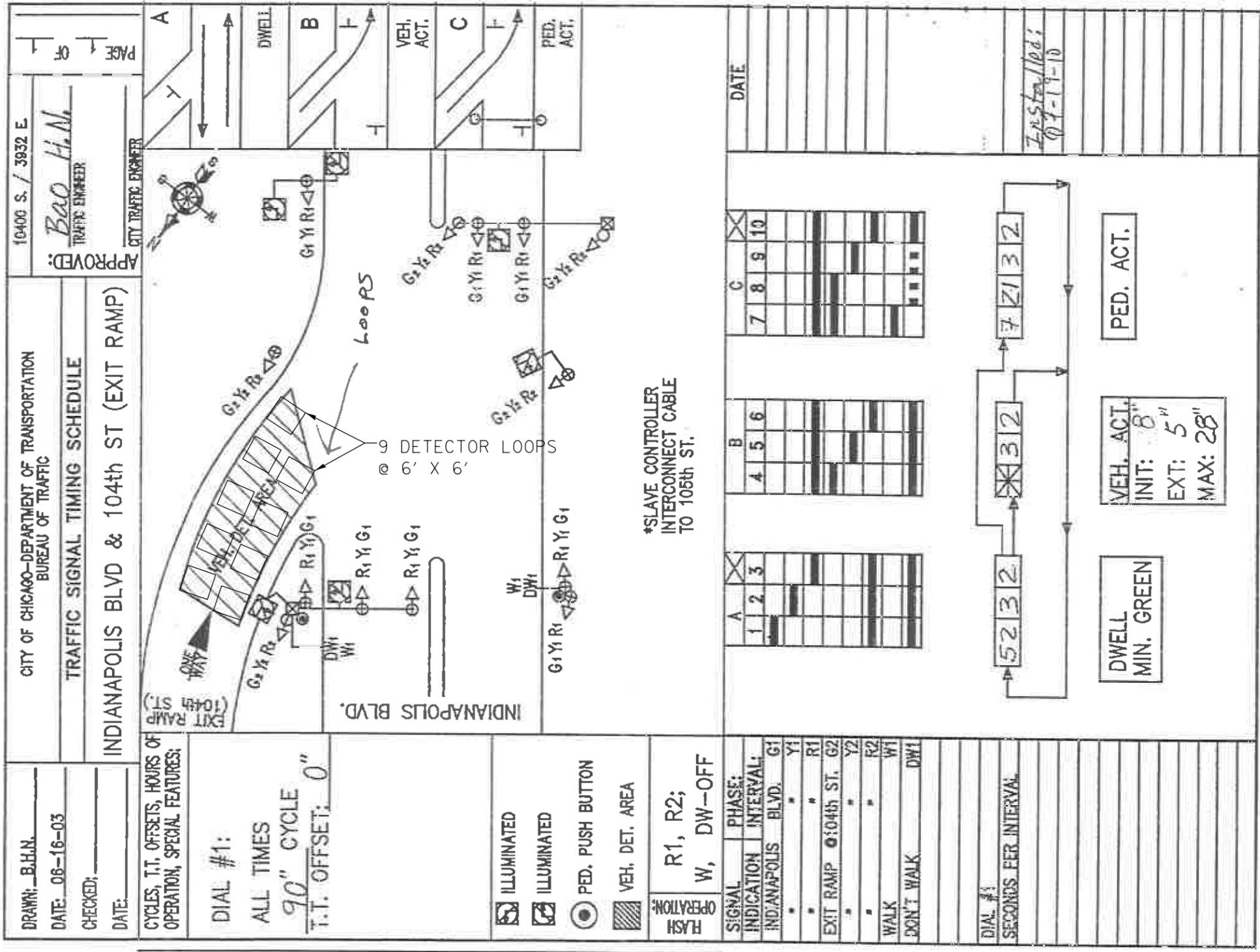
**PROPOSED SIDEWALK PLAN  
US 12 /20 /41 AT E. AVE J, NORTH WEST CORNER**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
029	2018-007-R5-1	COOK	23	8f
CONTRACT NO. 62G21				
ILLINOIS FED. AID PROJECT				



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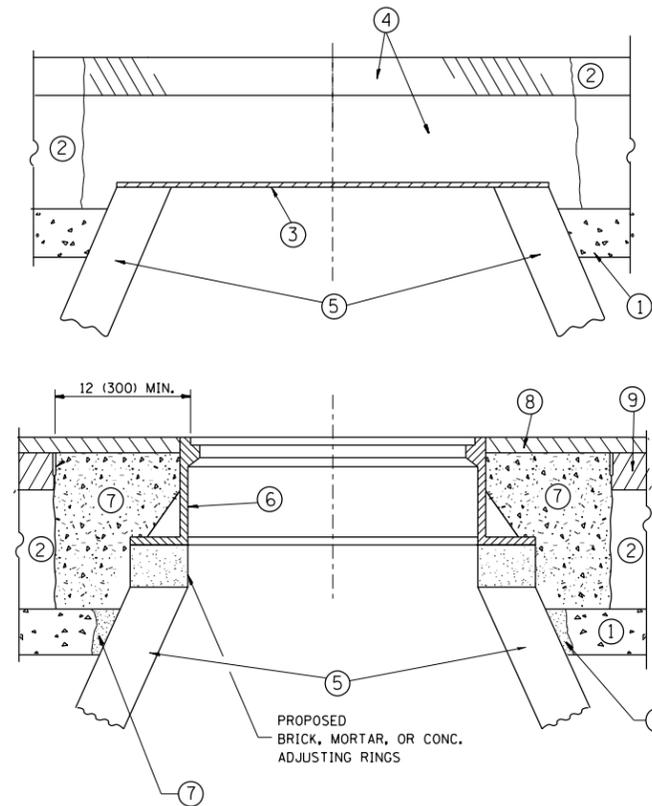
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DATE = 06-16-03	DRAWN -	REVISED -
CHECKED:	CHECKED -	REVISED -
DATE:	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO DETECTOR LOOP  
 U.S. ROUTE 12 / 20 / 41 (INDIANAPOLIS AVE) EWING AVE. -106th ST.  
 SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 029	SECTION 2018-007-RS-1	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 9
CONTRACT NO. 62G21				
ILLINOIS FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

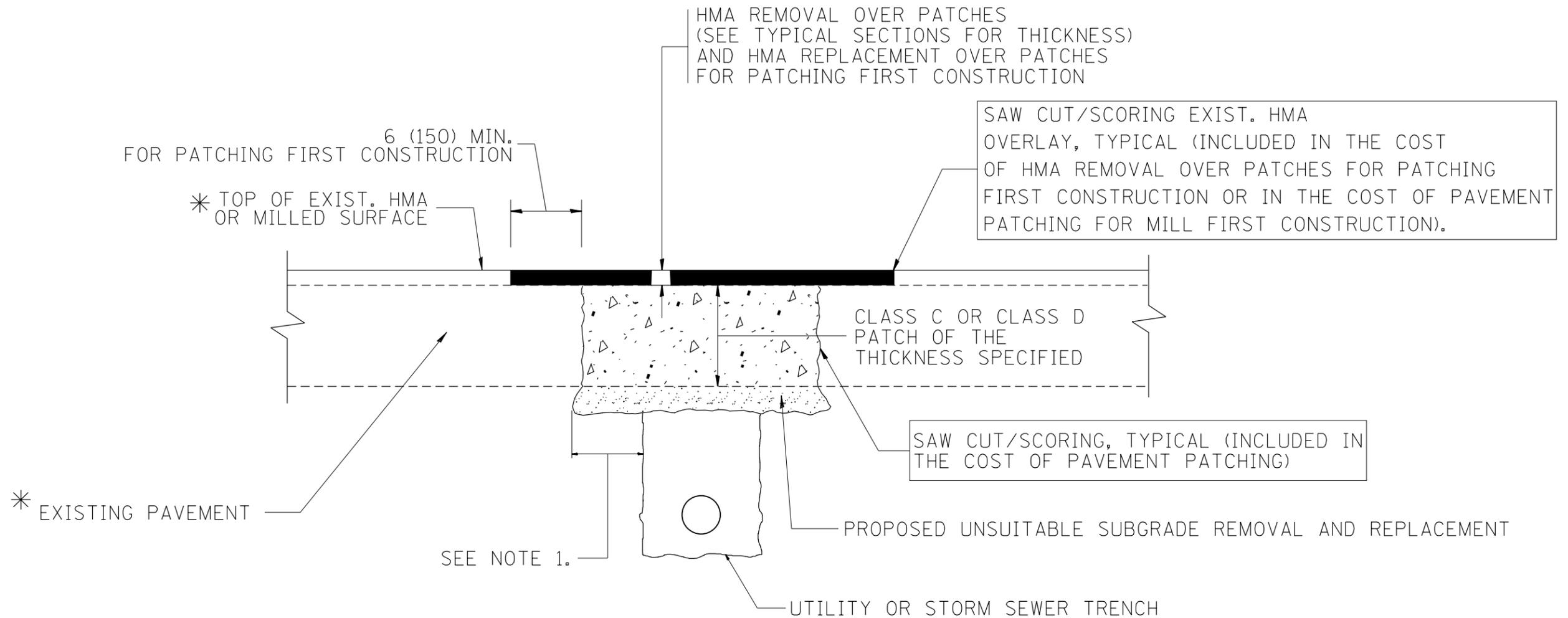
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/14/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			23	//
BD600-03 (BD-8)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

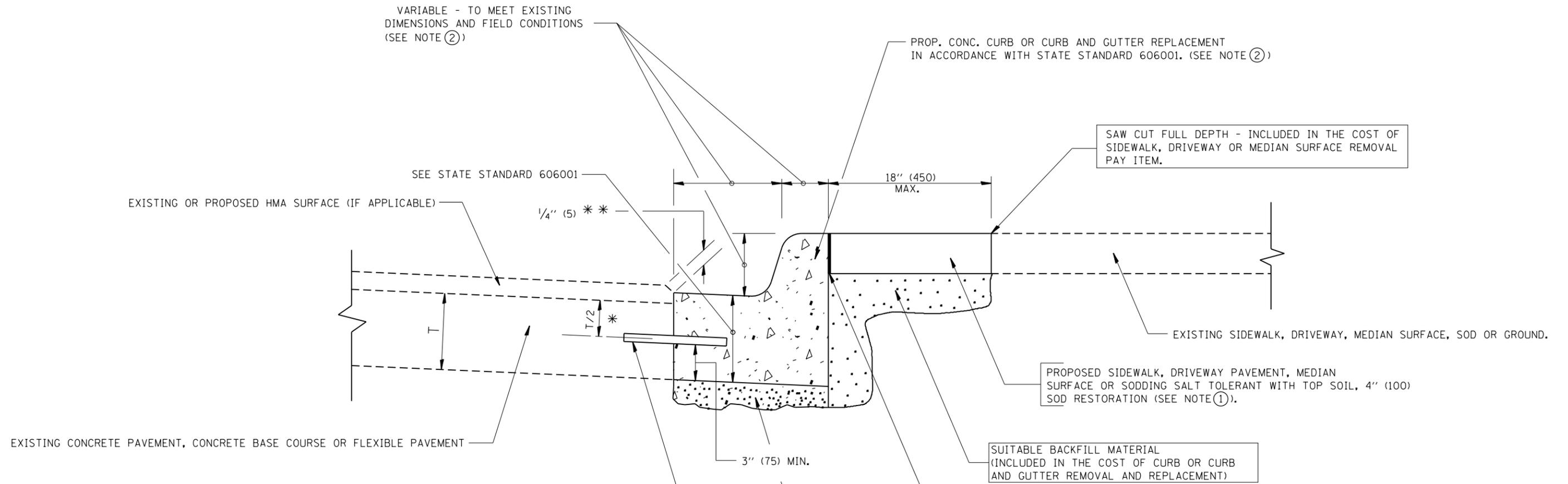
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hemj	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI28718\Drawings\Design\DI28718-sht-plen.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					BD400-04 (BD-22)	CONTRACT NO.	23	12	
	PLOT DATE = 12/14/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT		
			REVISED - K. ENG 10-27-08									



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

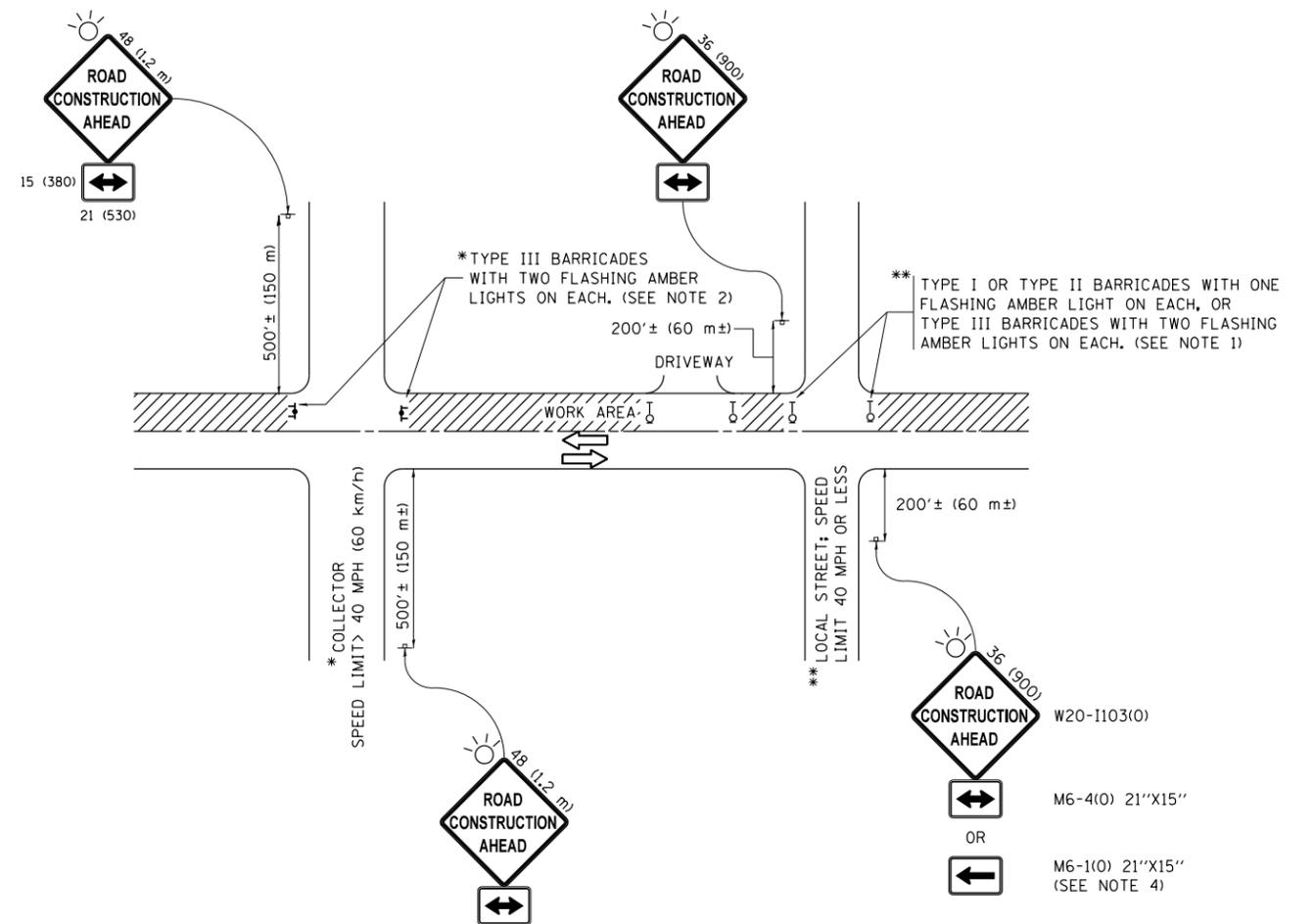
**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hemj	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI28718\Design\DI28718-shr-plen.dgn	PLLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - A. ABBAS 03-21-97								
PLLOT DATE = 12/14/2018	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		
		REVISED - R. BORO 12-15-09									
						BD600-06 (BD-24)		CONTRACT NO.			
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					





**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

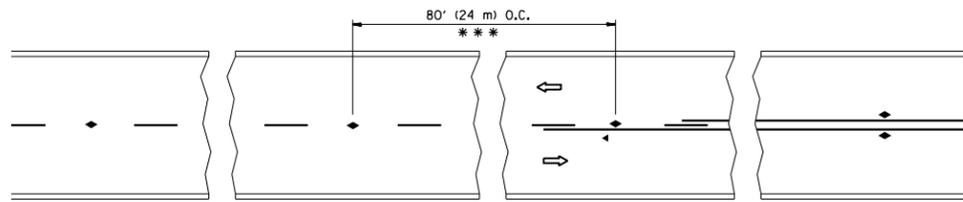
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	PLOT DATE = 12/14/2018		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

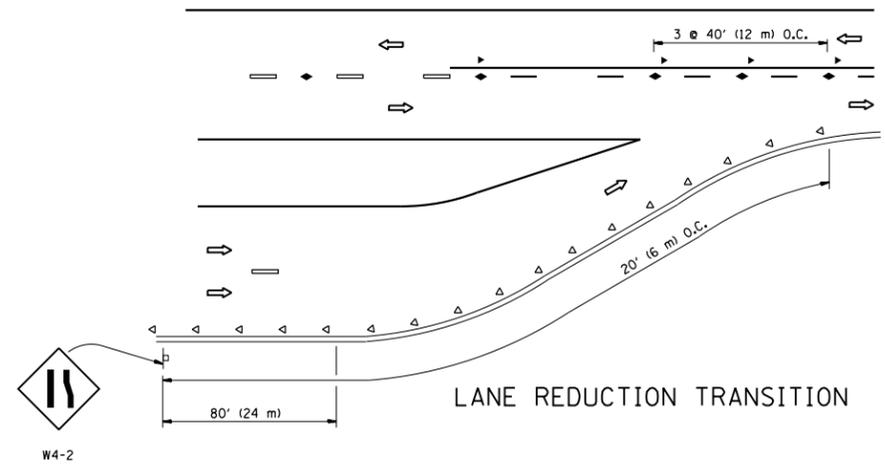
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			23	15
<b>TC-10</b>			<b>CONTRACT NO.</b>	
ILLINOIS FED. AID PROJECT				

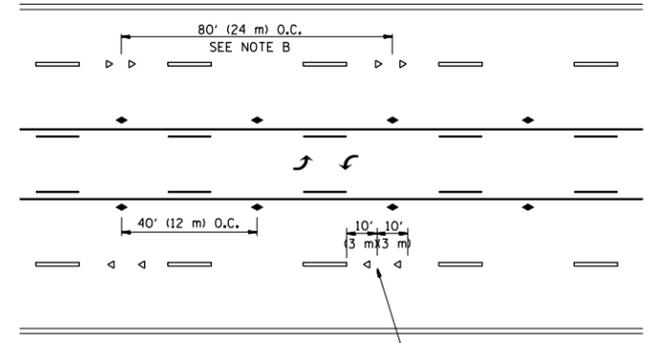


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

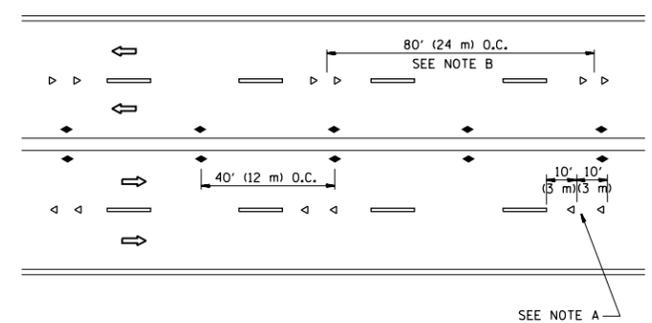
TWO-LANE/TWO-WAY



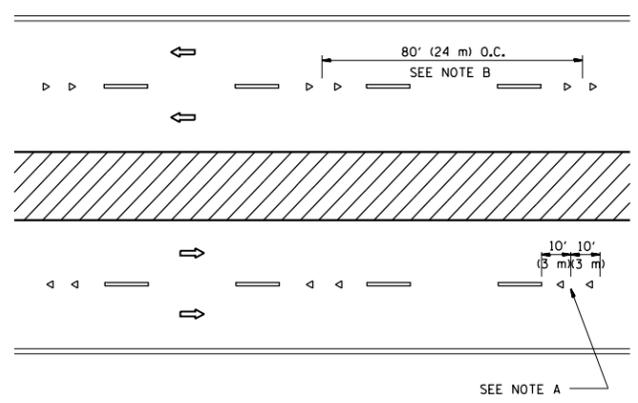
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

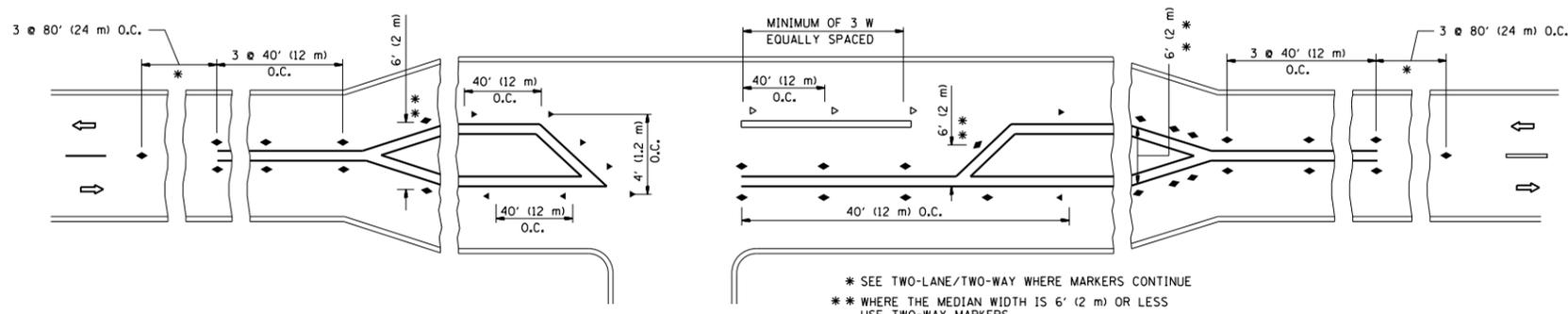
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



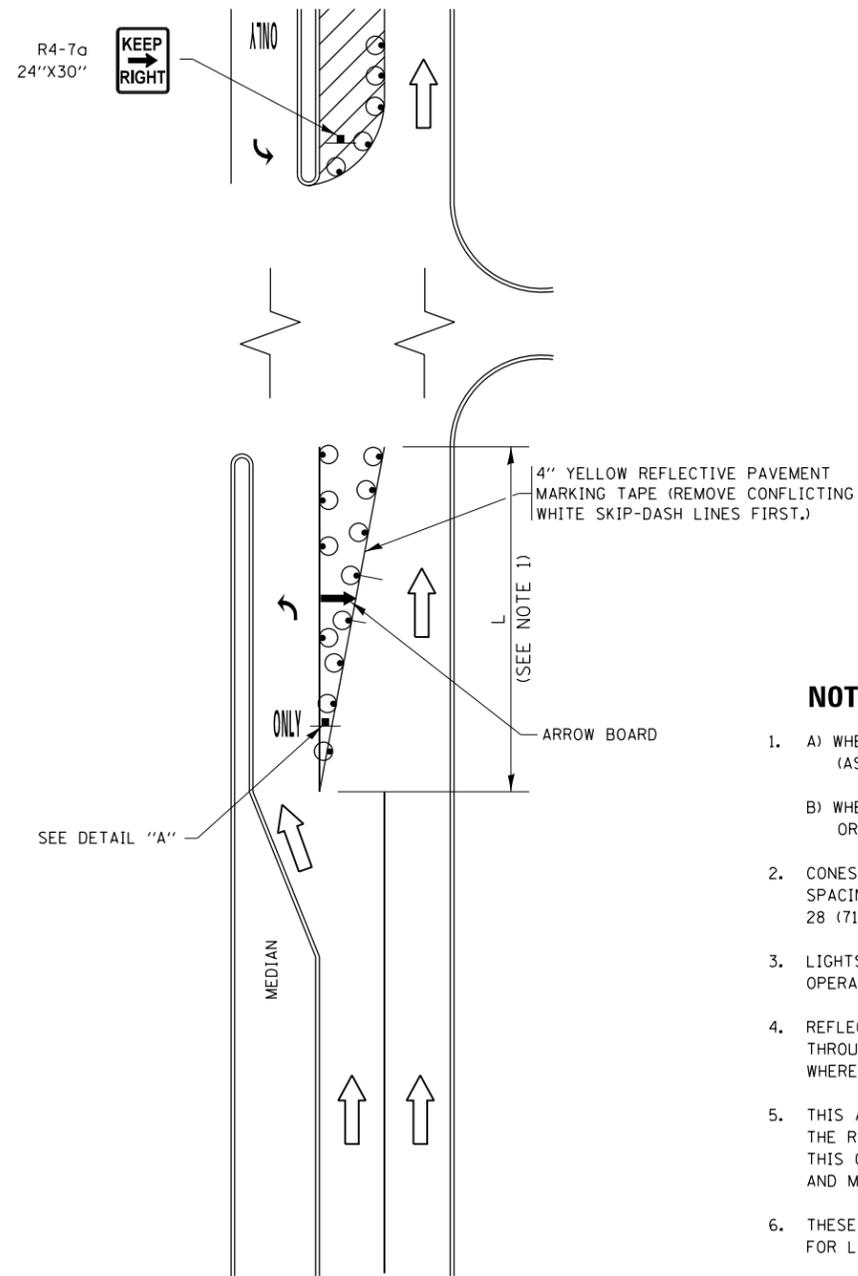
LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

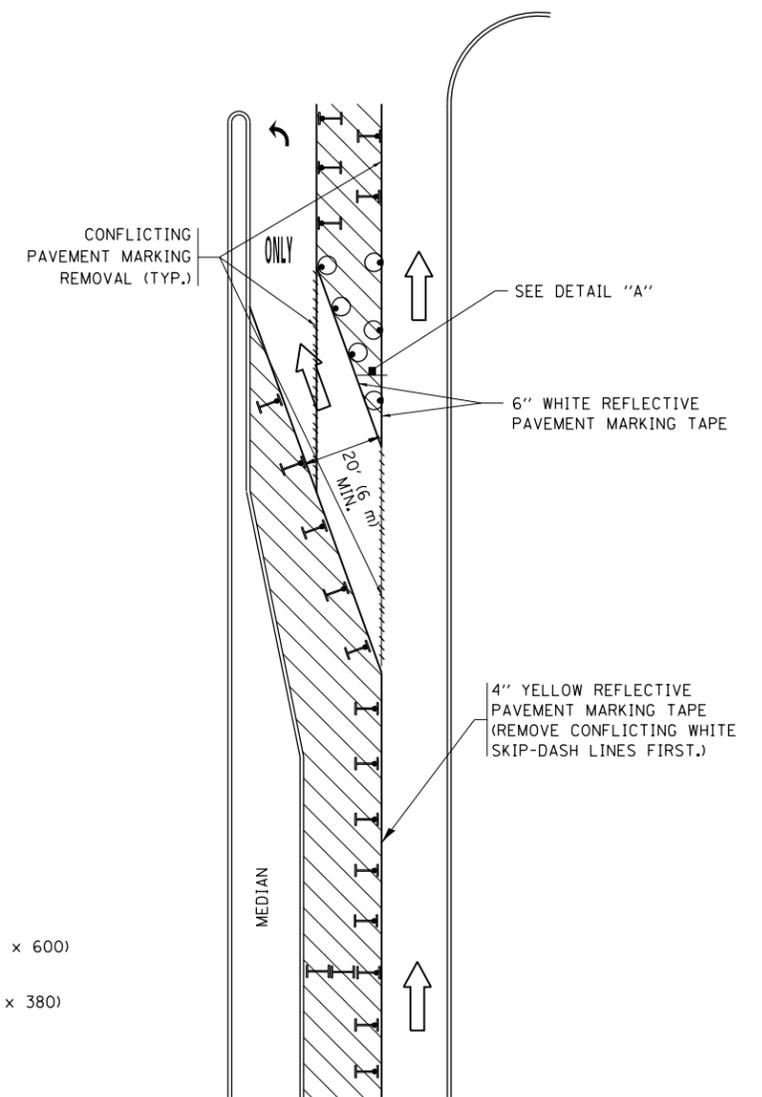
FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI28718\Design\DI28718-sht-plen.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					<b>TC-11</b>			CONTRACT NO.	
	PLOT DATE = 12/14/2018	DATE -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	23	16	
			REVISED - C. JUCIUS 09-09-09									

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



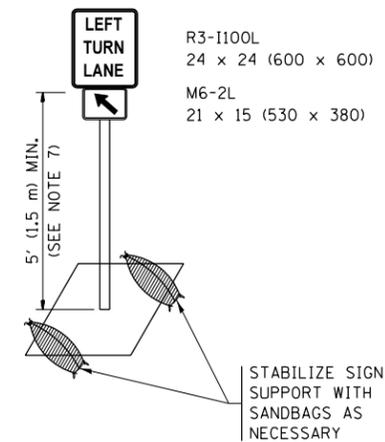
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

**NOTES:**

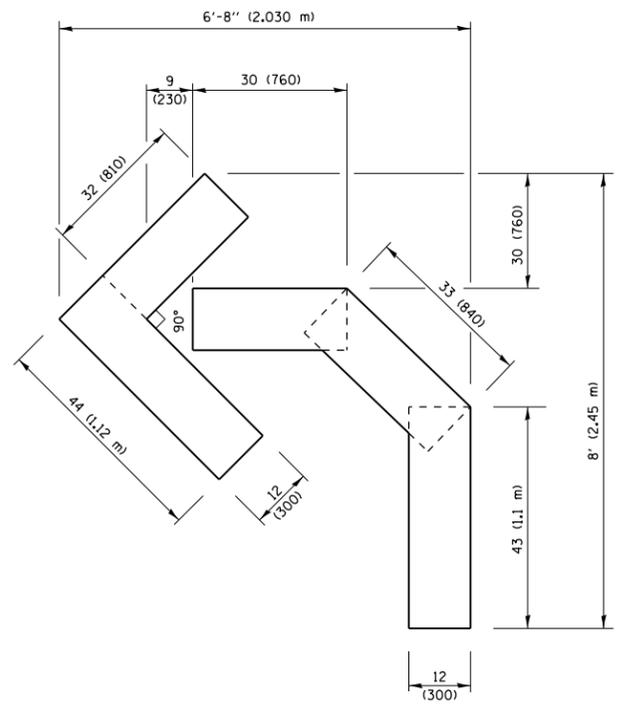
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



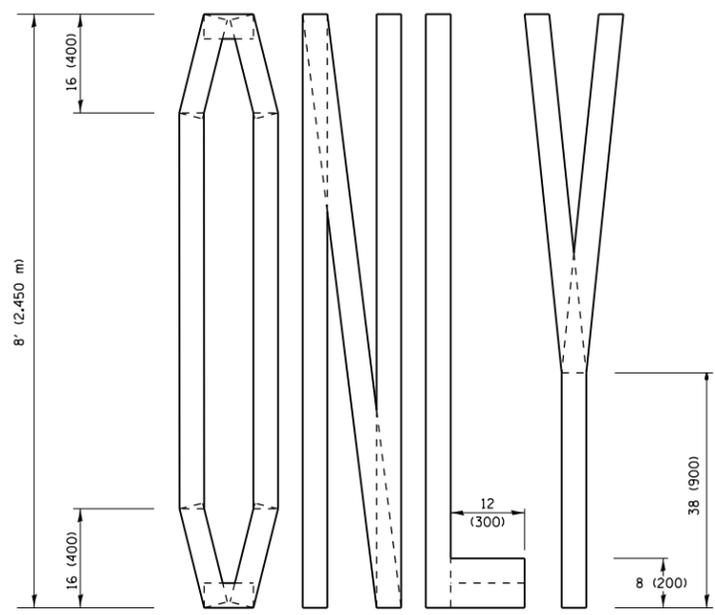
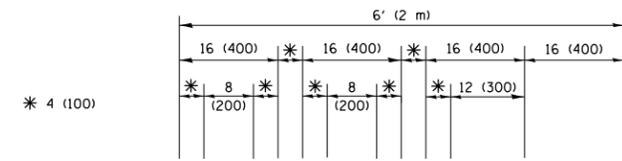
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

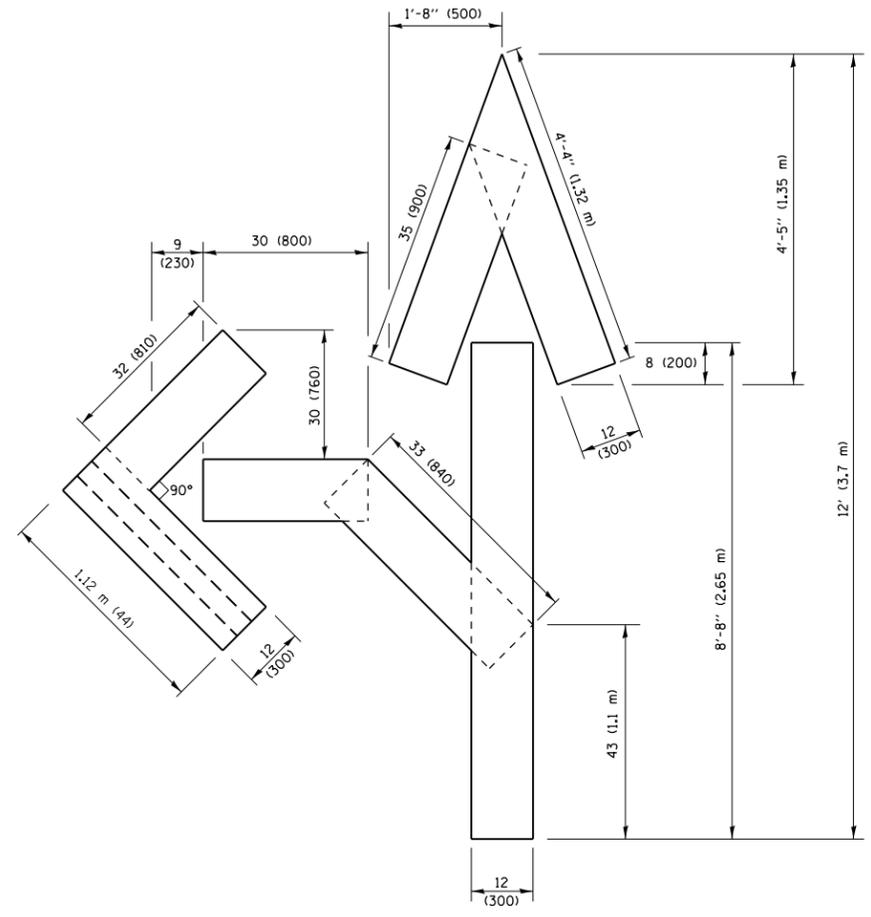
FILE NAME =	USER NAME = hemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT DATE = 12/14/2018	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16					<b>TC-14</b>			<b>CONTRACT NO.</b>	
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

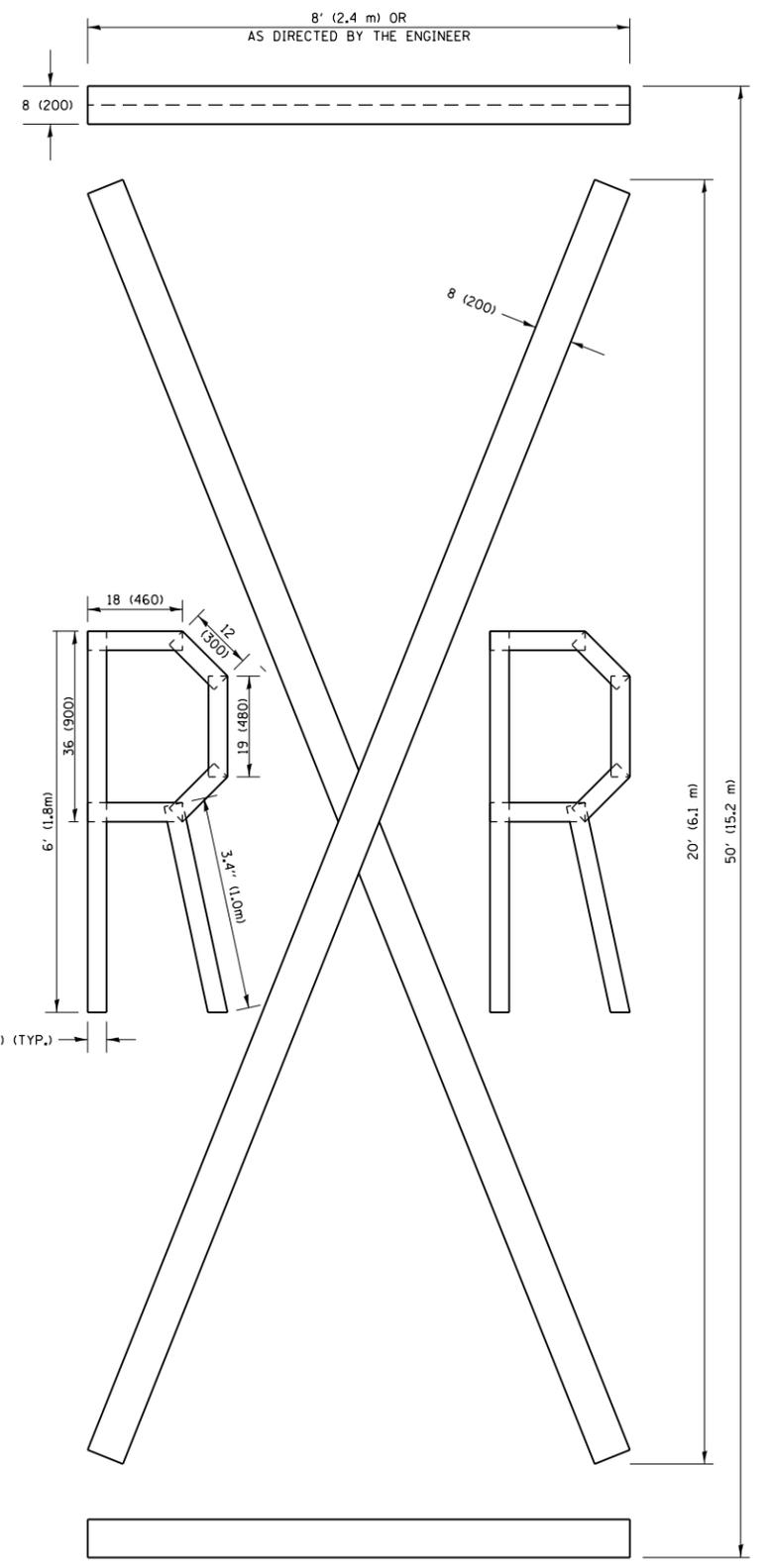


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

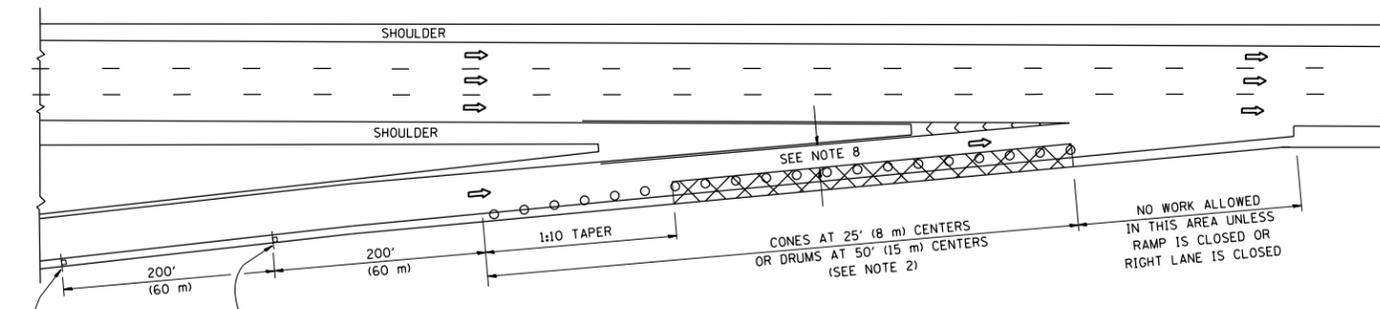
FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI28718\Design\DI28718-sht-plen.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

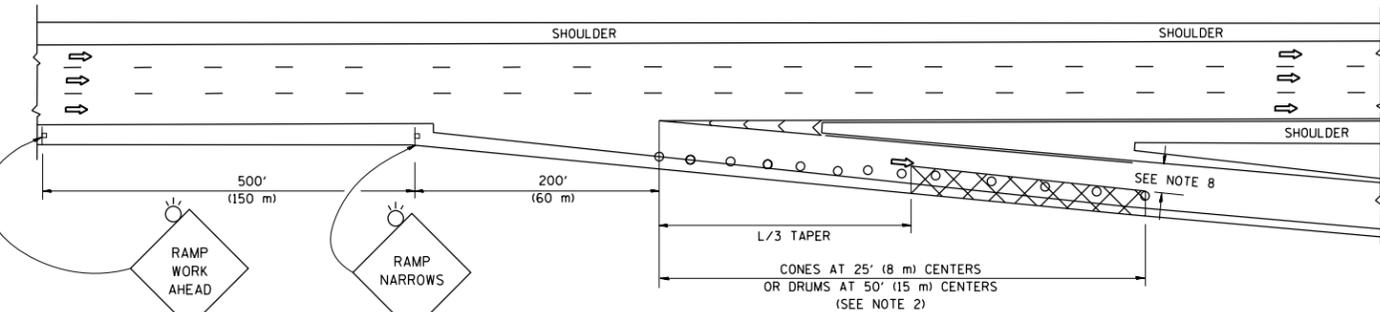
<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-16		23	18
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO.	

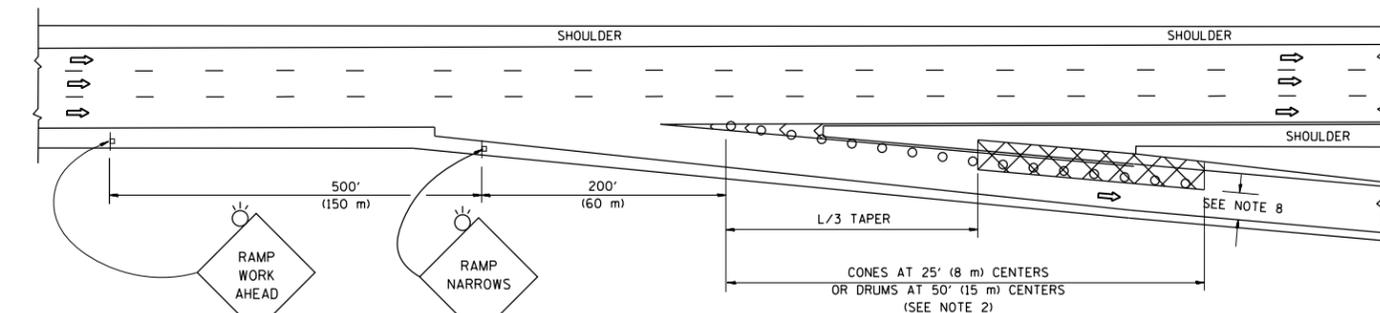
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

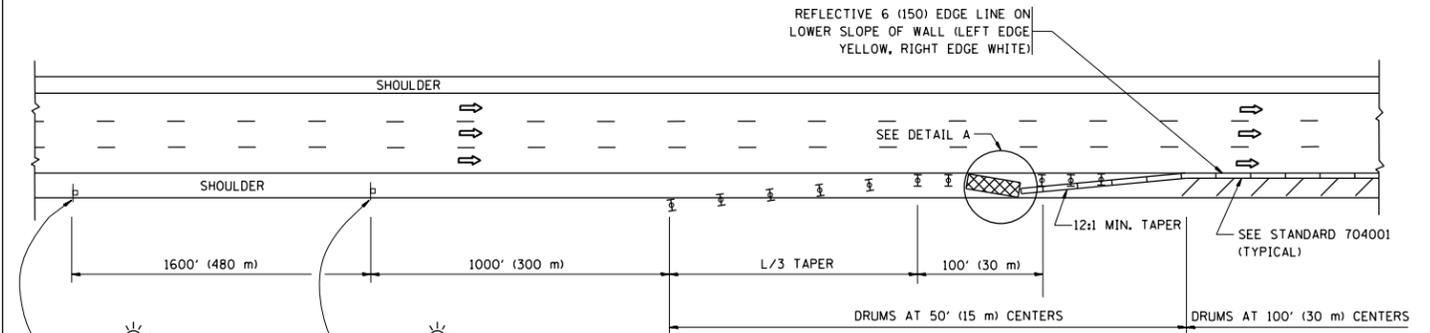
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:  

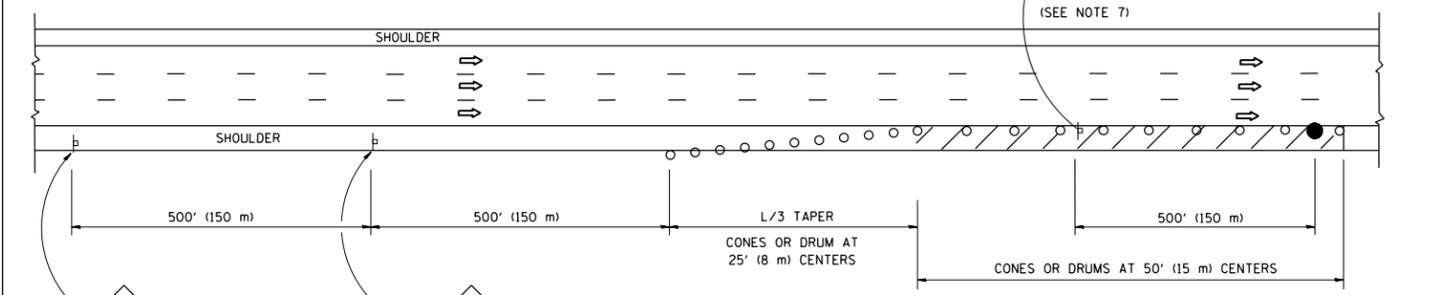
SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$

W = WIDTH OF OFFSET IN FEET (METERS)  
 S = NORMAL POSTED SPEED MPH (KM/H)
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

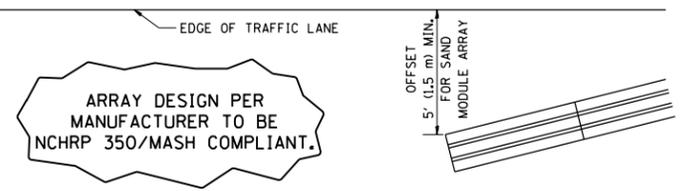


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

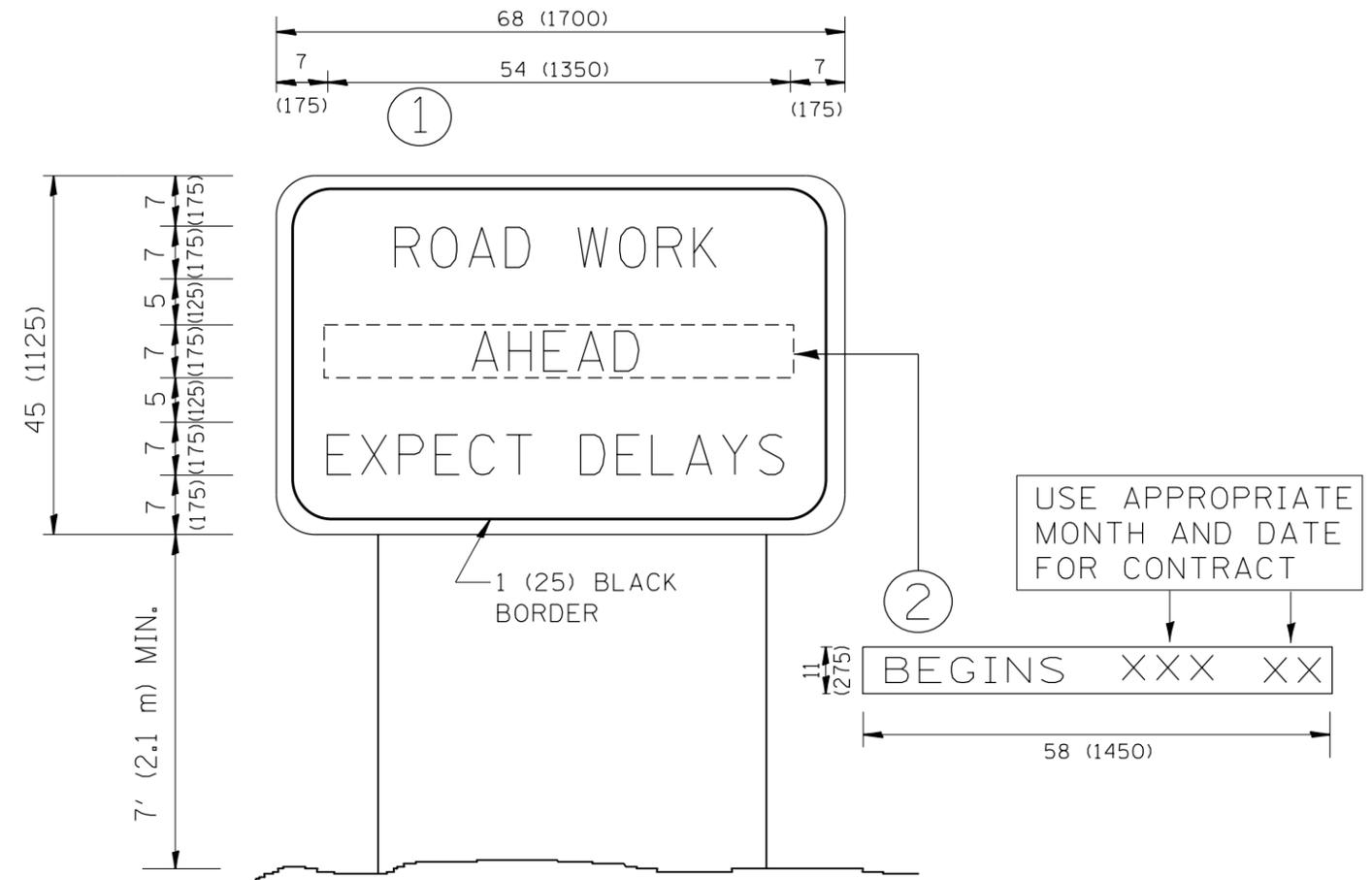


DETAIL "A"  
 IMPACT ATTENUATOR, TEMPORARY  
 (SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCRACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
 16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED - S.P.B. 01-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI287\Drawings\Design\DWG\18-sht-plen.dgn	PLLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - S.P.B. 12-09			TC-17	23	19		
Default	PLLOT DATE = 12/14/2018	DATE - 11-96	REVISED - M.D. 06-13			CONTRACT NO.				
			REVISED - M.D. 01-18			ILLINOIS FED. AID PROJECT				

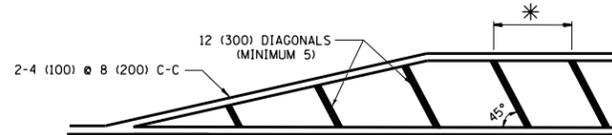
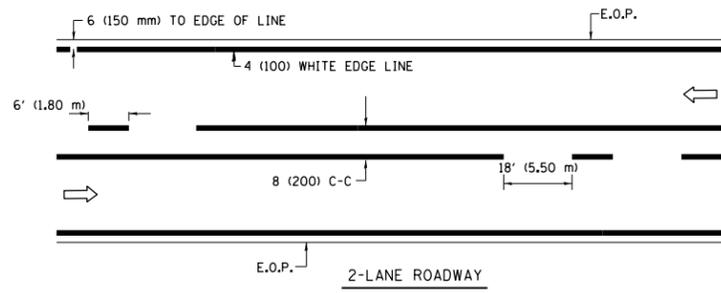


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

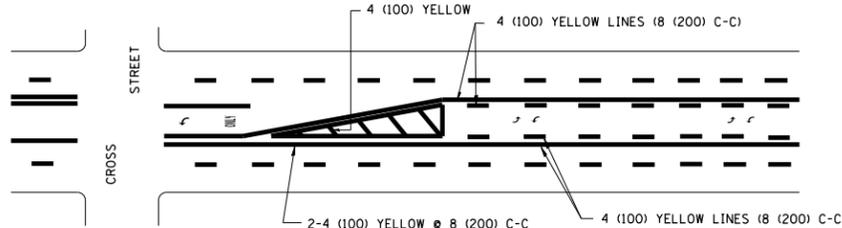
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p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI28718\Drawings\Design\DI28718-sht-plen.dgn		CHECKED -	REVISED - R. MIRS 12-11-97								23	20
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99		<b>TC-22</b>			<b>CONTRACT NO.</b>				
		PLOT DATE = 12/14/2018	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



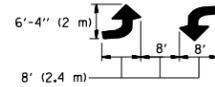
\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

\* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

**PAINTED MEDIANS**

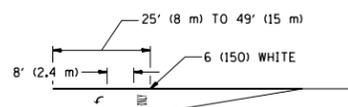
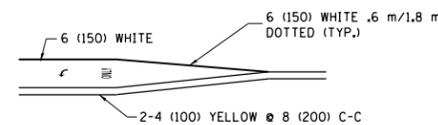


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

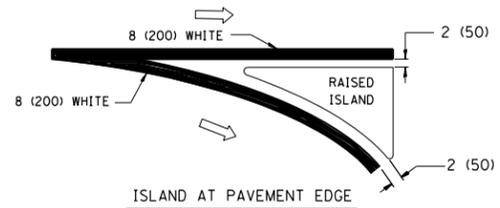
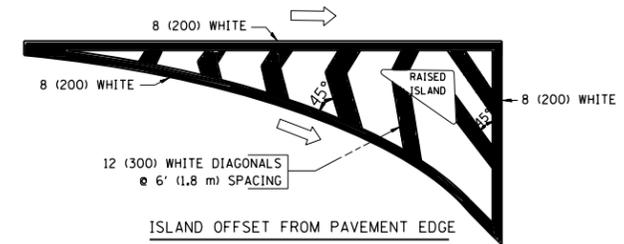
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

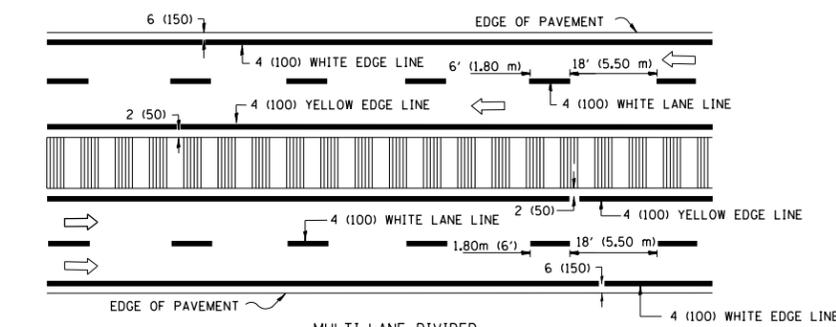
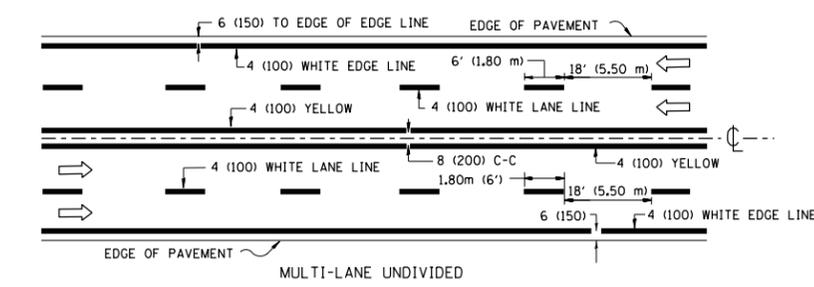


**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

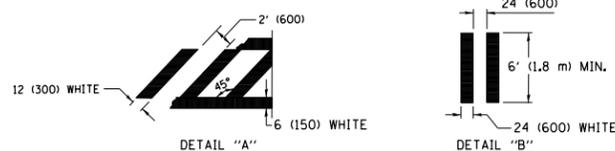
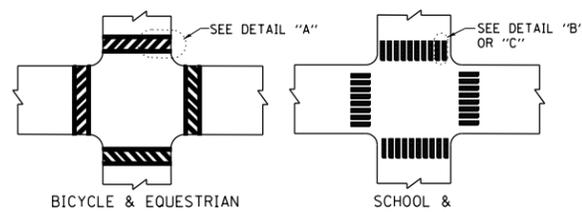
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

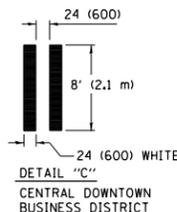


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**



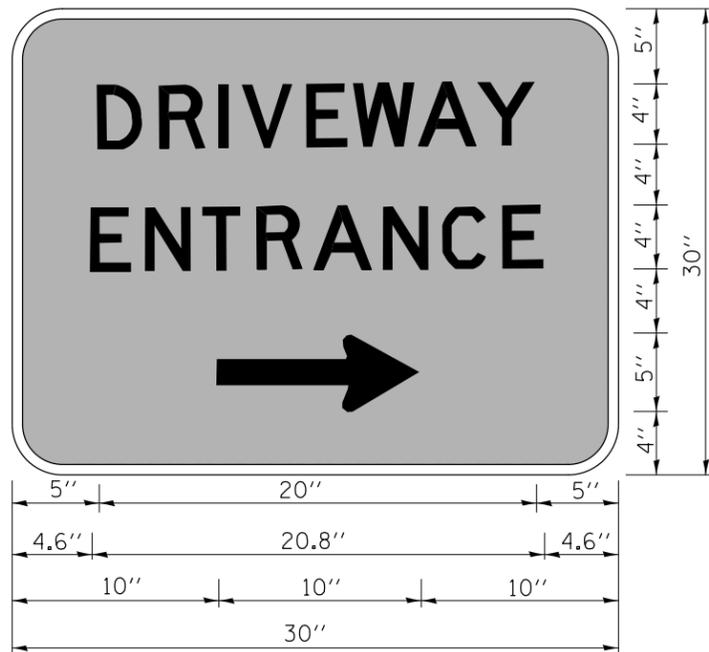
DETAIL "B"  
CENTRAL DOWNTOWN BUSINESS DISTRICT

FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI28718\Design\DI28718-sht-plen.dgn		CHECKED -	REVISED - K. ENG 02-28-12
		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA. TO STA.	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-24		23	21
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI28718\Drawings\Design\DI28718-sht-plen.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

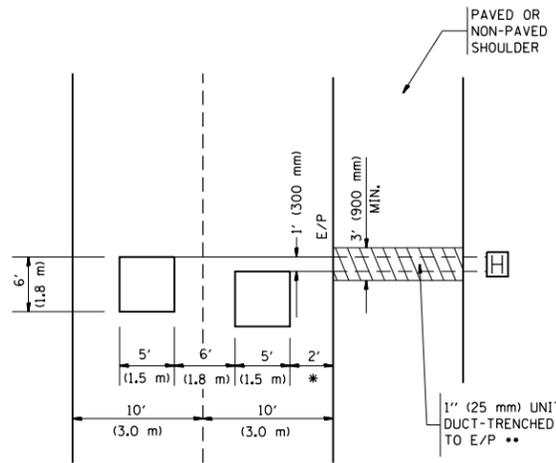
**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			23	22
TC-26			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



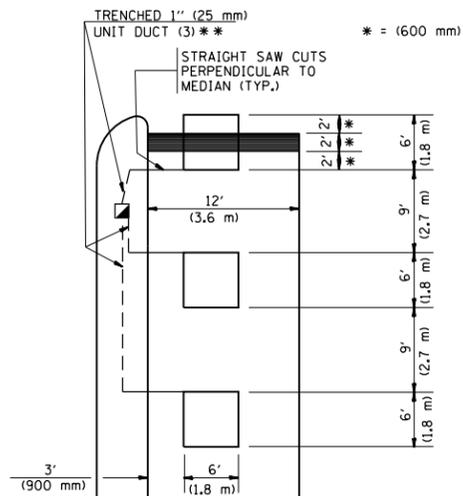
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

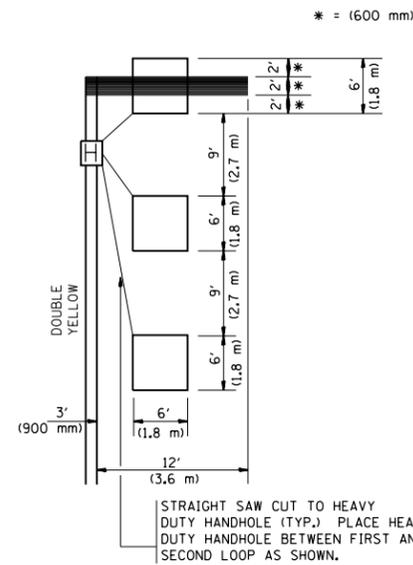


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

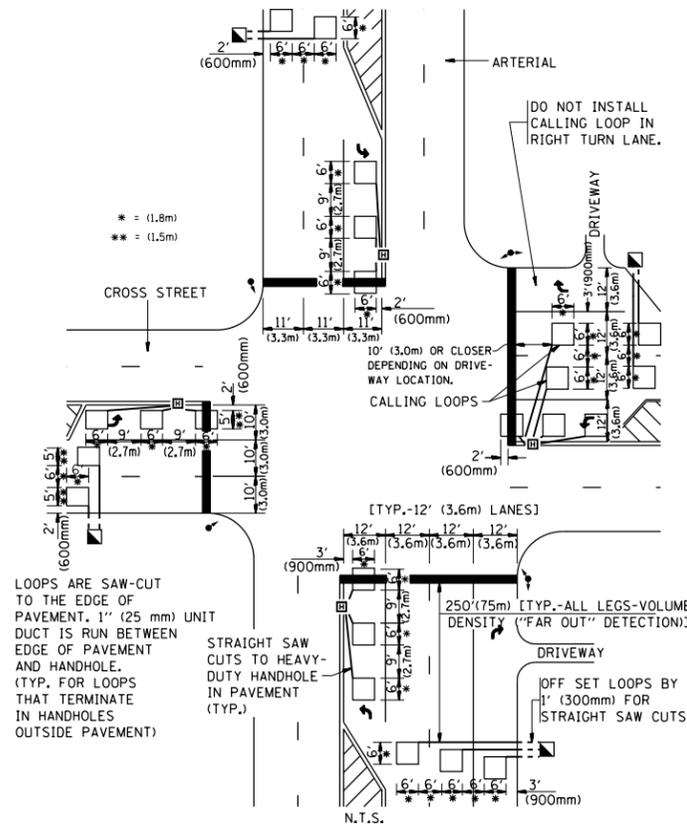
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



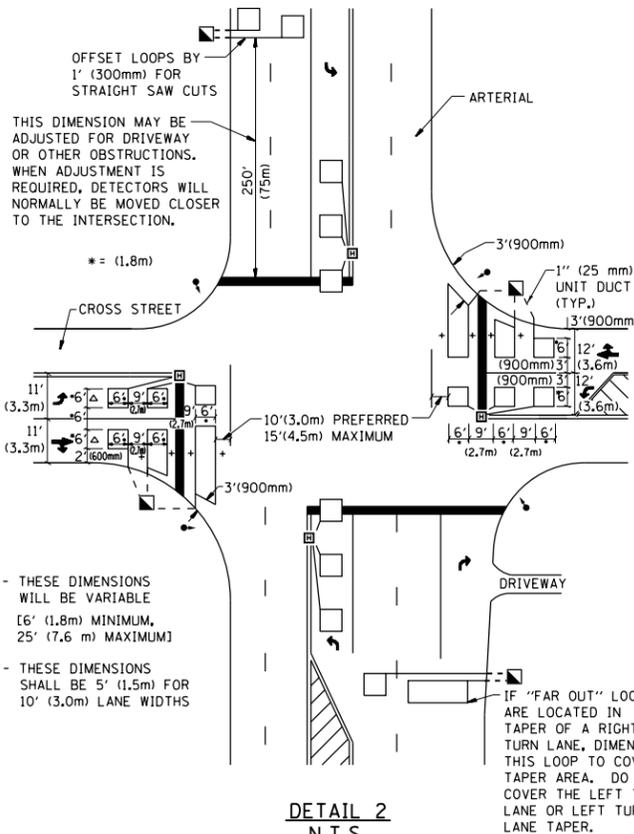
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = hemj	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\11inois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI28718\Design\DI28718-sht-plen.dgn		CHECKED - R.K.F.	REVISED -					TS-07			23	23
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -					CONTRACT NO.				
PLOT DATE = 12/14/2018								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					