Sheet List Table		
Sheet Number Sheet Title		
GI001	COVER SHEET	
GI101	AIRPORT SITE PLAN	
GC001	CONSTRUCTION SAFETY AND PHASING NOTES	
GC002	CONSTRUCTION SAFETY AND PHASING DETAILS	
GC100	CONSTRUCTION ACTIVITY PLAN OVERVIEW	
GC101	CONSTRUCTION ACTIVITY PLAN 1	
GC102	CONSTRUCTION ACTIVITY PLAN 2	
CD101	EXISTING CONDITIONS AND REMOVALS 1	
CD102	EXISTING CONDITIONS AND REMOVALS 2	
CD103	EXISTING CONDITIONS AND REMOVALS 3	
CP100	PROJECT CONTROL AND ALIGNMENT DATA	
CP101	PROPOSED IMPROVEMENTS 1	
CP102	PROPOSED IMPROVEMENTS 2	
CP103	PROPOSED IMPROVEMENTS 3	
CP301	TYPICAL SECTIONS	
CP501	PAVING DETAILS	
CP502	PAVING DETAILS 2	
CP503	JOINTING DETAILS	
CJ101	JOINTING PLAN 1	
CJ102	JOINTING PLAN 2	
CS101	STAKING PLAN 1	
CS102	STAKING PLAN 2	
CS103	STAKING PLAN 3	
CM101	MARKING AND TIE DOWN PLAN 1	
CM102	MARKING AND TIE DOWN PLAN 2	
CM501	MARKING AND TIE DOWN DETAILS	

CONSTRUCTION PLANS FOR WILLARD AIRPORT

UNIVERSITY OF ILLINOIS SAVOY, IL

IL. PROJ. NO: CMI-5043

REHABILITATE GENERAL AVIATION APRONS

MARCH 07, 2025







(ChampaignAp/21001968-00_CMIRehabGAApron ILE: 210001968-GIO01.dwg IPDATE BY: Max Kron

TOTAL SHEETS: 26 UN065

	SUMMARY OF QUANTITIES			
BASE BID)			
TEM NO.	DESCRIPTION	UNIT	QUANTITY	
R150520	MOBILIZATION	L SUM	1	
R401610	BITUMINOUS SURFACE COURSE	TON	190	
AR401650	BITUMINOUS PAVMENT MILLING	SQ YD	1,275	
R401655	BUTT JOINT CONSTRUCTION	SQ YD	350	
R501509	9" PCC PAVEMENT	SQ YD	1,800	
R501510	10" PCC PAVEMENT	SQ YD	625	
R501530	PCC TEST BATCH	EACH	1	
R501900	REMOVE PCC PAVEMENT	SQ YD	2,425	
R501922	PCC SPALL REPAIR	SQ FT	60	
R510901	REMOVE TIE DOWN - TYPE A	EACH	9	
AR603510	BITUMINOUS TACK COAT	GAL	250	
R620520	PAVEMENT MARKING - WATERBORNE	SQ FT	2,375	

ADDITIVE ALTERNATE #1 - SELECT PCC PANEL REPAIR

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AS150520	MOBILIZATION	LSUM	1
AS501510	10" PCC PAVEMENT	SQ YD	400
AS501900	REMOVE PCC PAVEMENT	SQ YD	400
ADDITIVE	ALTERNATE #2 - SELECT PCC PANI	EL REPA	IR 2
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AT150520	MOBILIZATION	LSUM	1
AT501510	10" PCC PAVEMENT	SQ YD	150
AT501900	REMOVE PCC PAVEMENT	SQ YD	150
ADDITIVE	ALTERNATE #3 - MISC. SPALL REPA	NR .	
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AU501922	PCC SPALL REPAIR	SQ FT	160
AU510511	TIE DOWN - TYPE A	EACH	9
AU605510	JOINT SEALING FILLER	FOOT	11.800







	-
	NCMT
STAGING AREA 1	License No. 184-000613 CONSULTANTS
STAGING AREA 2	R
RUNWAY SAFETY AREA	
— TAXIWAY SAFETY AREA	
CONTRACTOR ACCESS ROUTE	0 400' 800'
	THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).
TAXIWAY OBJECT FREE AREA	
CTOR'S ACCESS SHALL BE AS FOLLOWS:	
ONTRACTOR'S ACCESS TO THE WORK SHALL BE AS IN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY 3.	
IG ADVERSE WEATHER, THE CONTRACTOR SHALL PROVISIONS FOR ACCESS TO THE WORK SITE AT NO IONAL COST TO THE CONTRACT. NO EXTENSION OF RACT TIME WILL BE CONSIDERED FOR DELAYS DUE CK OF ADEQUATE ACCESS TO THE WORK.	
ONTRACTOR SHALL INSTALL AND MAINTAIN A Y-DUTY PADLOCK ON HIS ACCESS GATES. HE SHALL DE KEYS FOR EACH PADLOCK TO THE RPR, AIRPORT RITY, AND MAINTENANCE SUPERVISOR. NO IONAL KEYS ARE TO BE DISTRIBUTED UNLESS DRIZED BY THE AIRPORT MANAGER.	100%
ONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING	MARCH 07, 2025
DNSTRUCTION ACCESS GATES CLOSED & SECURED L TIMES INCLUDING WORK HOURS. IF THE RACTOR CHOOSES TO LEAVE THE GATE OPEN IG HAULING OPERATIONS, HE SHALL POST A ETENT, FULL TIME SECURITY GUARD TO PREVENT THORIZED ENTRIES. THE CONTRACTOR SHALL ICCE ANY UNSATISFACTORY SECURITY GUARDS IF SO TED BY THE AIRPORT MANAGER OR ENGINEER.	REHABILITATE GENERAL AVIATION APRONS
ONTRACTOR SHALL CLOSE AND LOCK THE ACCESS S UPON LEAVING THE SITE.	OWNER
UGHOUT THE DURATION OF THE CONTRACT, ANY GE TO THE ACCESS GATES OR FENCING ADJACENT E PROJECT SHALL BE REPAIRED BY THE RACTOR TO THE SATISFACTION OF THE AIRPORT GER.	CHAMPAIGN
OSTS RELATING TO CONTRACTOR'S ACCESS AND RITY SHALL BE THE RESPONSIBILITY OF THE RACTOR.	Ŭ R B A N A
DYEE PERSONAL VEHICLES SHALL NOT BE ALLOWED ND THE CONTRACTOR'S PARKING AREA. RACTOR PERSONNEL SHALL PARK IN THE RACTOR'S STAGING & STORAGE AREA. PERSONNEL BE TRANSPORTED TO THE WORK SITE BY COMPANY ID VEHICLES.	UNIVERSITY OF ILLINOIS WILLARD AIRPORT SAVOY, IL
ONTRACTOR SHALL HAVE A VACUUM TYPE SWEEPER ABLE AT ALL TIMES.	
AUTHORIZED PERSONNEL PERMITTED TO USE THE ALL BE PROVIDED BY THE CONTRACTOR TO THE RPR.	
TRACTOR SHALL PROVIDE A SIGN AT THE ACCESS YING "AUTHORIZED PERSONNEL & CONSTRUCTION ONLY".	IL, PROJ. NO: CMI-5043 CMT PROJECT NO: 21001968 CAD DWG FILE: 21001968-GI101.DWG
TRACTOR PERSONNEL AND SECURITY GUARDS ON IELD WILL BE REQUIRED TO SUBMIT A TEN YEAR DUND CHECK TO AIRPORT SECURITY AND WILL BE D TO BE TRAINED BY THE AIRPORT TO GAIN ACCESS VORK AREA. CONTRACTOR SHALL COORDINATE THE I PROCESS A MINIMUM OF 30 DAYS PRIOR TO TING ACCESS TO THE FIELD.	DESIGNED BY: MRK DRAWN BY: MRK CHECKED BY: MJD APPROVED BY: CBG COPYRIGHT: SHEET TITLE
L NOTES	AIRPORT SITE PI AN
DNSTRUCTION ACTIVITY PLANS FOR CONTRACTOR IS BY PHASE. IG AREA 2 SHALL NOT BE USED FOR PERSONAL	
LES OR STUCKPILES.	GI101
	SHEET 2 OF 26

0. GENERAL	5. CONTRACTOR ACCESS	8. WILDLIFE MANAGEMENT	13. PENALTIES
1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST, AND ALL	1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE	1. THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE RPR IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.	1. NONCOMPLIANCE BY T REGULATIONS OR FAIL APPROVED CSPP AND
AIRPORT SAFETY AND SECURITY REQUIREMENTS. THE CSPP CONSISTS OF THIS SHEET AND SHEETS GC001- GC101.	RESPONSIBILITY OF THE CONTRACTOR. 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.	2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.	
 PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR STALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/05370-2G OR 	THE CONTRACTOR SHALL BE RESPONSIBLE FOR REEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE	3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.	1. ADJACENT CONSTRUCT
LATEST. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.	AS DIRECTED.	4. THE CONTRACTOR SHALL MAINTAIN THE STIE TO LIMIT STANDING WATER AND TALL GRASS TO REDUCE THEIR ATTRACTION AND DISRUPTION TO WILDLIFE HABITAT.	CONTRACTOR. CONTR CONTRACTOR(S) TO P AREA AND ALLOW FOR
BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.	3. CONTRACTOR SHALL OBTAIN ALL NECESSART PERMITS AND SHALL OBTAIN ALL NECESSART PERMITS AND SHALL CASEN OF A SHALL ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SOLUTION AND SHALL ACCEMPTORY CONTROL SOLUTION.	9. NOTIFICATION OF CONSTRUCTION ACTIVITIES	15 BUNWAY & TA
 A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS 	TOWNSHIP, OR I.D.O.T.	1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.	1. ALL RUNWAYS, TAXIW AIRPORT TRAFFIC DU
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.	THE CONTRACTOR WITHIN THE AIR OPERATIONS AREA (AOA) SHALL ATTEND THE APPROPRIATE DRIVERS TRAINING PROGRAM ADMINISTERED BY THE AIRPORT. ONLY THOSE INDIVIDUALS WHO RECEIVE TRAINING AND	2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS/ARFF PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.	2. IF ANY RUNWAY OR TA
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE	DRIVING PRIVILEGES WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.	3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF FOURPMENT TOTAL HEIGHT AND LOCATION	CONTRACTOR AND AP SHALL USE MARKING, REQUIREMENTS OF FA
CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.	5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE OR ATTAIN	WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A	16. MARKING & S
1. COORDINATION	DRIVER'S PHIVICEGES BUT SHALL SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE	DETERMINATION FROM FAA IS RECEIVED. 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.	1. BARRICADES AND SIG ACCESS ROUTE AS DE ACTIVITY PLAN SHEET
ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF DEPENDING FOR AND ATTENDING THE DEFCONSTRUCTION	SOBSTREE WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL WHO IS BADGED AND HAS OBTAINED DRIVERS PRIVILEGES STATED IN #4 ABOVE. CONTRACTOR	5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.	17, HAZARD MAR
CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.	PRIOR TO INITIATING ESCORT PROCEDURES.	FIRE MARSHAL JOHN CUMBEE - DIRECTOR OF PUBLIC SAFETY OFFICE (217) 244-8764 CELL (217) 202-8213	1. THE CONTRACTOR SH AND ASSOCIATED LIGI
CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED	AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS	AIRPORT OPERATIONS ANDREW SMITH - ASSISTANT DIRECTOR OF OPERATIONS AND MAINTENANCE	2. ALL CONSTRUCTION E IN ACCORDANCE WITH
ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.	PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED CMI/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.	OFFICE (217) 333-8623 CELL (217) 238-1714	150/5210-5D OR LATES AIRPORT PROPERTY.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF, LOCAL FAA ATO AND RPR. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING CANAL DE INCIDENTAL TO THE CONTRACT	 WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA. 	ENGINEER CHRIS GROTH P.E PROJECT ENGINEER (217) 787-8050 BPB TO BE DETERMINED	3. BARRICADES SHALL BI CONSTRUCTION ACTIV ENGINEER.
 THE CSPP AS WRITTEN HAS BEEN APPROVED BY THE AIRPORT AND THE EAA DROPOSED CHANCES TO THE WORK LIMITS SHALL BE 	 THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY 	OFFICE (217) 787-8050 10. INSPECTION REQUIREMENTS	4. THE CONTRACTOR SH EACH WORK DAY TO IN
COORDINATED THROUGH THE FAA FOR AIRSPACE ANALYSIS AND WILL REQUIRE A MINIMUM OF 30 DAYS TO REVIEW.	 F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, 	THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE ONDUANCE WITH THE CORD. THE CHECKLIST FOUND IN ADDENING 2	OPERATION OF THE R PLACEMENT.
	TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.	OF FAA AC 150/5370-2G OR LATEST MAY BE USED TO AID IN THE INSPECTIONS.	18. PROTECTION
2. PHASING	10. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HALL ROLLES WHICH WILL BE OPENED TO ALL TRAFFIC TO	2. THE CONTRACTOR SHALL REQUEST FINAL OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED.	1. ALL WORK REQUIRED
1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.	THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION	PUBLIC SAFETY WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.	CENTERLINE OF 50' TA TAXIWAYS, WILL REQU CONTRACTOR SHALL (
3. CONSTRUCTION ACTIVITY AREAS		11. UNDERGROUND UTILITIES	72 HOURS PRIOR TO T
 ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN. 	11. ALL VEHICLE AND EQUIPMENT OPERATORS UTILIZED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR. 12. THE CONTRACTOR SHALL NOTICY THE AIBCRAFT RESCUE AND FIRE	IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF	2. ALL WORK REGURED SAFETY AREA, WHICH PAVEMENT, WILL REQ THE CONTRACTOR SH
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY, SHALL TAKE PRECEDENCE AND SHALL COVERN. ENAL	FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.	OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION	OF 72 HOURS PRIOR T
AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.	6. FOD MANAGEMENT	ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS	19. OTHER LIMITA
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.	 THE CONTRACTOR SHALL DISCARD ANY FOREIGN OBJECT DEBRIS (FOD) ON THE AIRFIELD PAVEMENTS. 	ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST BEIMBURSED TO THE UTILITY BY THE CONTRACTOR AREIED	1. IF, DURING CONSTRUC AIRPORT, THE CONTR PAVEMENT OF ALL VE
4. WORK ZONE LIGHTING FOR NIGHTWORK	2. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN	LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR	2. BROKEN CONCRETE, E OTHER MISCELLANEO
 WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION. 	THEY WERE PRIOR TO BEGINNING CONSTRUCTION. 3. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION	BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALLULUE, AND	PROPERTY, UNLESS O 3. THE CONTRACTOR WI
LIGHTS SHALL CONSIST OF VEHICLE OR MOVEABLE POLE-MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED	AREAS AND HAUL ROUTES AT THE END OF EACH WORKING DAY, REGARDLESS OF THE WORK AREA BEING OPEN OR CLOSED TO AIR TRAFFIC.	CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-04 & 70-05 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.	AIRSPACE FOR THE CO THAT SPECIFIED ON T TAKE UP TO 12 WEEKS
ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS OR ATCT CONTROLLER SIGHT LINES. ANY WORK BEING PERFORMED UNDER INSUFFICIENT	7. PROTECTION OF NAVAIDS	12. HAZARDOUS MATERIALS MANAGAMENT	4. THE CONTRACTOR SH EXISTING CIRCUITS PF
UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION & TESTING CAN BE MADE.	1. THE CONTRACTOR SHALL MAINTAIN A 100' DISTANCE BETWEEN HIS OPERATIONS AND ANY FAA-OWNED NAVAID (TYPICALLY ORANGE). CONTRACTOR SHALL CONTACT ATCT PRIOR TO ENTERING AN ILS CRITICAL AREA AS SHOWN ON THE SITE PLAN.	1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT & RESPONSE PLAN AND KEEP COPIES ON THE OBSITE OF MATERIAL SAFETY DATA SHEETS FOR ALL MATERIALS HANDLED ON THE JOBSITE.	
 ARTIFICIAL LIGHTING SHALL NOT BE AIMED AT THE ATCT OR THE APPROACH ENDS OF AN ACTIVE RUNWAY. 	2. ANY WORK WHICH AFFECTS A NAVAID WILL BE COORDINATED WITH FAA ATO THROUGH THE AIRPORT. WORK IN A NAVAID CRITICAL AREA IS RESTRICTED AND SUBJECT TO AVAILABILITY BASED ON RUNWAY	2. FUELING OPERATIONS SHALL NOT OCCUR IN ANY ACTIVE OBJECT FREE AREAS.	
	CONFIGURATION AND WEATHER CONDITIONS AND MAY BE POSTPONED BY THE AIRPORT AT ANY TIME.		

THE CONTRACTOR WITH AIRPORT RULES AND ILURE TO COMPLY WITH THE AIRPORT'S D THE CONTRACTOR'S APPROVED SPCD MAY ALLOWED BY LAW.	
NDITIONS	License No. 184-000613
ICTION MAY IMPACT THE OPERATIONS OF THE TRACTOR SHALL COORDINATE WITH ADJACENT PROVIDE UNHINDERED ACCESS TO EACH WORK IN THE TIMELY PROSECUTION AND PROGRESS OF EING PERFORMED AT THE AIRPORT.	CONSULTANTS
AXIWAY VISUAL AIDS	THIS BAB IS FOLIAL TO 2" AT FULL SCALE (34322)
VAYS, AND APRONS SHALL BE KEPT OPEN TO JRING CONSTRUCTION EXCEPT AS NOTED IN THE IVITY PLAN.	THIS DAN IS EQUAL TO 2 AT FOLL SCALE (34A22).
AXIWAY CLOSURES ARE REQUESTED BY THE PPROVED BY THE AIRPORT, THE CONTRACTOR , LIGHTING AND SIGNS THAT FOLLOWING THE AA AC 150/5370-2G OR LATEST EDITION.	
GIGNS FOR ACCESS ROUTES	
SNS SHALL BE USED ALONG THE CONTRACTOR'S IETAILED ON THIS SHEET AND THE CONSTRUCTION T.	
KING & LIGHTING	
HALL FURNISH, ERECT, AND MAINTAIN MARKINGS HTING OF OPEN TRENCHES, EXCAVATIONS, PILES, AND HIS/HER CONSTRUCTION EQUIPMENT.	
EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED H FAA ADVISORY CIRCULAR 150/5370-2G AND ST EDITIONS AT ALL TIMES WHILE OPERATING ON . THE MAXIMUM EQUIPMENT HEIGHT IS 25°.	100% MARCH 07, 2025
BE PLACED AT THE LOCATIONS SHOWN ON THE IVITY PLAN SHEET OR AS DIRECTED BY THE	REHABILITATE GENERAL AVIATION APRONS
HALL INSPECT THE BARRICADES ONCE DURING INSURE PROPER PLACEMENT AND PROPER RED AND/OR YELLOW LIGHTS AND FLAG	
O ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE EE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY 'AXIWAYS AND 130' FROM THE CENTERLINE OF 75' UIRE THE TAXIWAY TO BE CLOSED. THE . COORDINATE WITH THE AIRPORT A MINIMUM OF THE REQUESTED CLOSURE TIME.	CHAMPAIGN U R B A N A
O ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE H EXTENDS 70' FROM THE APRON'S EDGE OF QUIRE A PORTION OF THAT APRON TO BE CLOSED. HALL COORDINATE WITH THE AIRPORT A MINIMUM TO THE REQUESTED CLOSURE TIME.	UNIVERSITY OF ILLINOIS WILLARD AIRPORT SAVOY, IL
ATIONS ON CONSTRUCTION	
CTION, AN EMERGENCY IS DECLARED BY THE RACTOR SHALL IMMEDIATELY CLEAR THE EHICLES, PERSONNEL AND EQUIPMENT.	MARK DATE DESCRIPTION
BROKEN ASPHALT, RUBBISH FROM DEMO, AND DUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT OTHERWISE SPECIFIED.	AIP PROJ. NO: IL. PROJ. NO: CMI-5043 CMT PROJECT NO: 21001968
VILL BE RESPONSIBLE FOR COORDINATING THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THE PLANS WITH THE FAA. THIS PROCESS MAY (S TO COMPLETE.	CAD DWG FILE: 21001968-GC000.DWG DESIGNED BY: MRK DRAWN BY: MRK CHECKED BY: MJD
HALL BE RESPONSIBLE FOR MEGGAR TESTING ALL PRIOR TO CONSTRUCTION AND FOLLOWING PECIFIED IN THE CONTRACT DOCUMENTS.	APPROVED BY: CBG COPYRIGHT:
	NOTES
	GC001
	sheet 3 ог 26





rath: K:/ChampaignAp/21001968-00_CMIRehabGAApron/Draw/Sheets/21001968-GC100.dwg







EYMAP		
	NORTH	
		License No. 184-000613
		THIS BAR IS EQUAL TO 2° AT FULL SCALE (34X22).
GEND		
	BITUMINOUS PAVEMENT MILLING	
\boxtimes	NEW BUTT JOINT MILLING	
	EXISTING DUCT BANK	
R	TO BE REMOVED	
	EXISTING RUNWAY OR TAXIWAY EDGE LIGHT	
	EXISTING GUIDANCE SIGN	
Н	EXISTING HANDHOLE	
•	EXISTING SPLICE CAN	
E	EXISTING ELECTRICAL MANHOLE	100%
D	EXISTING STORM SEWER MANHOLE	MARCH 07, 2025
СВ	EXISTING STORM SEWER INLET	REHABILITATE GENERAL
\triangle	SURVEY CONTROL POINT	AVIATION APRONS
· · · <u> </u>	EXISTING ELECTRICAL CIRCUITS	
	EXISTING VASI CIRCUIT	
— PAPI——	EXISTING PAPI CIRCUIT	OWNER
W	EXISTING WATER LINE	
G	EXISTING GAS LINE	
SS	EXISTING SANITARY SEWER LINE	
FAA	EXISTING FAA POWER / CONTROL CABL	E CHAMPAIGN
> ST	EXISTING STORM SEWER LINE	URBANA
—> — UD	EXISTING UNDERDRAIN	
LOC	EXISTING LOCALIZER CIRCUIT	
θ	EXISTING TIE DOWN	SAVOY, IL
WWV	EXISTING WATER VALVE	
0	EXISTING STORM SEWER MANHOLE	
СВ	EXISTING STORM SEWER INLET	
0	EXISTING CLEAN OUT	
$\overline{\odot}$	EXISTING FIRE HYDRANT	IL. PROJ. NO: CMI-5043
-X÷	EXISTING OVERHEAD LIGHT	CMT PROJECT NO: 21001968
DTES		DESIGNED BY: MRK
PAVEMENT MIL EXISTING HMA TYPICAL SECTI	LING SHALL INCLUDE MILLING INTO + PCC PAVEMENT AS SHOWN IN ONS.	DRAWN BY: MRK CHECKED BY: MJD APPROVED BY: CBG COPYRIGHT:
PAVEMENT STF FROM RECORD REPRESENTAT PAVEMENT SEC THE THICKNES THERE WILL BE CONTRACTOR QUANTITY OF E	AUCTURES SHOWN WERE TAKEN DRAWINGS AND ARE CONSIDERED IVE OF THE "AS CONSTRUCTED" CTION WITH SOME VARIABILITY FROM S INDICATED TO BE EXPECTED. E NO ADDITIONAL PAYMENT TO THE DUE TO VARIATIONS IN SIZE OR EXISTING FEATURES.	SHEET TITLE EXISTING CONDITIONS AND REMOVALS 1
EXISTING TIE D ACCORDING TO	OWNS ON THIS SHEET ARE "TYPE A" D RECORD DRAWINGS.	CD101 sheet 8 of 26



UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED. THAT THE LOCATIONS, SIZE. AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY. THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

LEGEND

X	X	X	

REMOVE 9"-10" PCC PANEL - BB REMOVE 9"-10" PCC PANEL - AA1 REMOVE 9"-10" PCC PANEL - AA2

	EXISTING DUCT BANK
R	TO BE REMOVED
	EXISTING RUNWAY OR TAXIWAY EDGE LIGHT
	EXISTING GUIDANCE SIGN
Н	EXISTING HANDHOLE
9	EXISTING SPLICE CAN
E	EXISTING ELECTRICAL MANHOLE
D	EXISTING STORM SEWER MANHOLE
СВ	EXISTING STORM SEWER INLET
\bigtriangleup	SURVEY CONTROL POINT
	EXISTING ELECTRICAL CIRCUITS
- VASI	EXISTING VASI CIRCUIT
– PAPI––––	EXISTING PAPI CIRCUIT
— W —	EXISTING WATER LINE
— G ——	EXISTING GAS LINE
— SS ——	EXISTING SANITARY SEWER LINE
-FAA	EXISTING FAA POWER / CONTROL CABLE
⊳ — ST —	EXISTING STORM SEWER LINE
⊳ — UD—	EXISTING UNDERDRAIN
- LOC	EXISTING LOCALIZER CIRCUIT
6	EXISTING TIE DOWN
⋈ _{wv}	EXISTING WATER VALVE
D	EXISTING STORM SEWER MANHOLE
СВ	EXISTING STORM SEWER INLET
60	EXISTING CLEAN OUT
3	EXISTING FIRE HYDRANT

EXISTING OVERHEAD LIGHT



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EYMAP		
THIS		License No. 184-000613 CONSULTANTS
		THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).
GEND		
XXX	REMOVE 9"-10" PCC PANEL- BB	
	REMOVE 9"-10" PCC PANEL - AA1	
	REMOVE 9"-10" PCC PANEL - AA2	
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Н	EXISTING HANDHOLE	100%
9	EXISTING SPLICE CAN	MARCH 07, 2025
E	EXISTING ELECTRICAL MANHOLE	REHABILITATE GENERAL
D	EXISTING STORM SEWER MANHOLE	AVIATION APRONS
СВ	EXISTING STORM SEWER INLET	
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	EXISTING VASI CIRCUIT	
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W	EXISTING WATER LINE	
G	EXISTING GAS LINE	CHAMPAIGN
SS	EXISTING SANITARY SEWER LINE	URBANA
FAA	EXISTING FAA POWER / CONTROL CABLE	
—⊳ — ST —	EXISTING STORM SEWER LINE	WILLARD AIRPORT
—⊳ — UD —	EXISTING UNDERDRAIN	SAVOY, IL
LOC	EXISTING LOCALIZER CIRCUIT	
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WWV	EXISTING WATER VALVE	MARK DATE DESCRIPTION
D	EXISTING STORM SEWER MANHOLE	AIP PROJ. NO:
СВ	EXISTING STORM SEWER INLET	IL. PROJ. NO: CMI-5043
0	EXISTING CLEAN OUT	CAD DWG FILE: 21001968-CD100.DWG
$\overline{\bigcirc}$	EXISTING FIRE HYDRANT	DESIGNED BY: MRK DRAWN BY: MRK
¥.	EXISTING OVERHEAD LIGHT	CHECKED BY: MJD APPROVED BY: CBG COPYRIGHT:
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I. THE REMOV BE MODIFIE REMOVAL A CONTRACTO THE MINIMU	/AL / CONSTRUCTION LIMITS MAY D TO FACILITATE MORE UNIFORM ND REPLACEMENT. RE AND OR WILL DETERMINE IN THE FIELD JM LIMITS OF REMOVAL.	EXISTING CONDITIONS AND REMOVALS 3 CD103
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