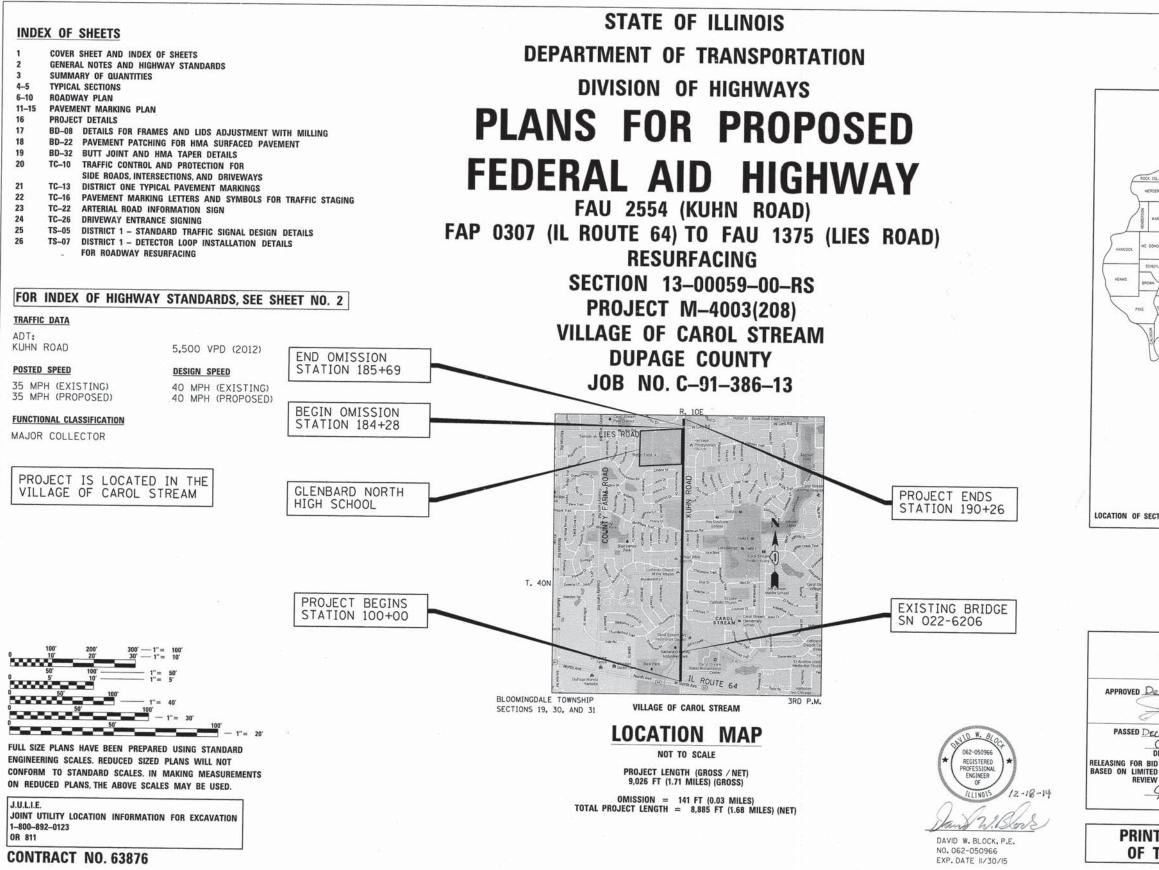
Systems



SECTION COUNTY 2554 13-00059-00-RS DUPAGE

CONTRACT NO. 63876



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

APPROVED De comber 18,2014

VILLAGE OF CAROL STREAM, VILLAGE ENGINEER

PASSED DECEMBER 29 ,2014

CHRISTOPHER HOLT

DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

REVIEW DECEMBER 30, 2014

OLE Kataran DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.
- 2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- 3. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR SHALL VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT WITH THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS/HER OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE VILLAGE OF CAROL STREAM AT 630-868-2260 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- 6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE
 PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

 2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY
 PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- 7. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING, ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- 8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THEIR AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- 10. HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- 11. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD. PRIOR TO REMOVAL.
- 12. THE CONTRACTOR IS REQUIRED TO USE A PAVER SKI WHEN PLACING BITUMINOUS LIFTS.
- 13. THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.
- 14. THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- 15. THE CONTRACTOR SHALL NOT BE PERMITTED TO STAGE OR OPERATE CONSTRUCTION EQUIPMENT ON THE HMA BIKE PATH.

SIGNING AND STRIPING

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- 2. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

TRAFFIC CONTROL

- 1. SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT TWO-WAY TRAFFIC SHALL REMAIN OPEN AT ALL TIMES.
- 3. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE POSTED ON ALL SIDE STREETS FROM BOTH DIRECTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION PAY ITEMS.

STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF ANY UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- 2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY OWNERS AND THE ENGINEER SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- 5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- 6. ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY, PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.15.
- 7. FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL INCLUDE REPLACEMENT OF EXISTING BROKEN ADJUSTMENT RINGS AND PATCHING INSIDE THE STRUCTURES BETWEEN PIPES AND STRUCTURES WITH HYDRAULIC CEMENT AT LOCATIONS AS DIRECTED BY THE ENGINEER. IF THE STRUCTURE IS A COMBINATION SEWER OR SANITARY MANHOLE THEN CHIMNEY SEALS SHALL BE PROVIDED AND INCLUDED IN THE COST OF FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).
- 8. ALL DRAINAGE STRUCTURE ADJUSTMENTS AND FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL USE PCC. HMA WILL NOT BE ALLOWED. EACH JOINT SHALL BE SEALED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS AS DIRECTED PER ARTICLE 602.02.
- 9. THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.

MISCELLANEOUS

- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE ENGINEER WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
- 2. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REGULARLY SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND PAVING HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL. SWEEPING SHALL BE INCLUDED IN THE PRICE FOR HMA MILLING.
- 4. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, COMMERCIAL PROPERTY OWNERS, AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE ALTERED DUE TO SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER 24 HOURS PRIOR TO THEIR REMOVAL. THESE ITEMS SHALL BE REMOVED AND RECONSTRUCTED HALF AT A TIME SUCH THAT THERE ARE NO DRIVEWAY CLOSURES.
- 5. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.
- 6. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

COMMITMENTS

1. THE CONTRACTOR SHALL NOT BEGIN WORK NORTH OF MUNSON DRIVE BEFORE JUNE 8TH, 2015, AND SHALL COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER BY THE END OF THE DAY ON AUGUST 14TH, 2015. THIS SCHEDULE COMPLIES WITH THE GLENBARD TOWNSHIP HIGH SCHOOL DISTRICT *87 SCHEDULE SO NO WORK OCCURS WHILE SCHOOL IS IN SESSION.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602401-03	MANHOLE, TYPE A
604001-04	FRAME AND LIDS, TYPE 1
604051-04	FRAME & GRATE TYPE 11
604056-04	FRAME & GRATE TYPE 11V
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDG
701301-04	SHORT TIME OPERATIONS
701311-03	MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL TURN LANE
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

SCALE: NTS

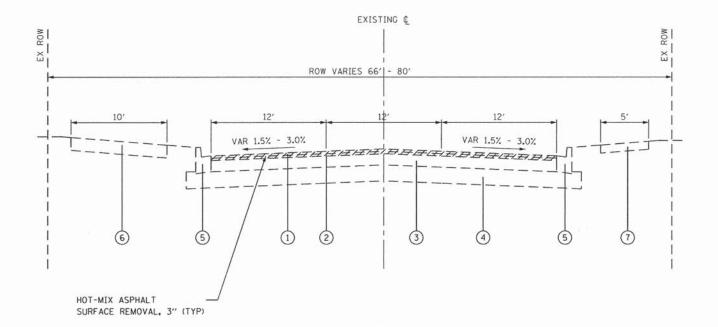
KUHN ROAD RESURFACING	F.A.U SECTION COUNTY TOTAL SHEET NO.
GENERAL NOTES AND HIGHWAY STANDARDS	2554 13-00059-00-RS DUPAGE 26 2
GENERAL NOTES AND HIGHWAT STANDARDS	CONTRACT NO. 63876
SHEET 1 OF 1 SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT

1		SUMMARY OF QUANTITIES			0005 ROADWAY 70% STF
	CODE NO	ITEM	UNIT	TOTAL	30% LA
Ŧ	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	99	99
Ŧ	21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	380	380
Ŧ	25000210	SEEDING, CLASS 2A	ACRE	1	1
ŧ	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	8	8
t	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	8	8
ŧ	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	8	8
ŧ	25100630	EROSION CONTROL BLANKET	SO YD	380	380
t	25200200	SUPPLEMENTAL WATERING	UNIT	10	10
ļ	28000510	INLET FILTERS	EACH	32	32
t	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	56	56
t	31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SO YD	719	719
1	35800100	PREPARATION OF BASE	SQ YD	33,220	33,220
-	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	22,424	22,424
-	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	50	50
ŀ	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,828	1,828
ŀ	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	213	213
	40600990	TEMPORARY RAMP	SO YD	213	213
ŀ	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3,721	3,721
	42001300	PROTECTIVE COAT	SO YD	3,763	3,763
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	3,555	3,555
	42400800	DETECTABLE WARNINGS	SO FT	247	247
	44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SO YD	33,007	33,007
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,140	1,140
	44000600	SIDEWALK REMOVAL	SO FT	3,285	3,285
-	44201725	CLASS D PATCHES, TYPE I, 7 INCH	SO YD	416	416
F	44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	416	416
	44201733	CLASS D PATCHES, TYPE III. 7 INCH	SO YD	416	416
-	44201735	CLASS D PATCHES, TYPE IV. 7 INCH	SO YD	416	416
-	60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID	- EACH	1	1
	60404800	FRAMES AND GRATES, TYPE 11	EACH	6	6
	60404805	FRAMES AND GRATES, TYPE 11V	EACH	21	21
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5
_	60500040	REMOVING MANHOLES	EACH	1	1
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,080	1,080
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	60	60
	67100100	MOBILIZATION	L SUM	1	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
	70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	3	3
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,557	2,557
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	354	354
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	30 F1	334	354

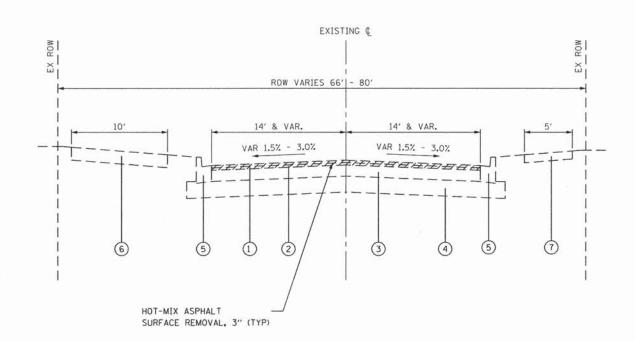
		SUMMARY OF QUANTITIES			0005 ROADWAY				
	CODE NO	ITEM	UNIT	TOTAL QUANTITY	70% STP 30% LA				
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,881	1,881				
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,496	1,496				
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	221	221				
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1,056	1,056				
	72000100	SIGN PANEL - TYPE I	SQ FT	75	75				
	72900100	METAL POST - TYPE A	FOOT	65	65				
	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	354	354				
	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	21,972	21,972				
	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,881	1,881				
	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,496	1,496				
	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	221	221				
	88600600	DETECTOR LOOP REPLACEMENT	FOOT	164	164				
	X0327036	BIKE PATH REMOVAL	SQ YD	35	35				
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH						
	Z0013798	CONSTRUCTION LAYOUT		18	18				
			L SUM	1	1				
	Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	45	45				
	Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	3	3				
	Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	214	214				
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SPECIALTY ITEM

FILE NAME :	USER NAME = _USER_	DESIGNED	- BSH	REVISED -		WILLIAM DOAD DECLIDE ACING		F.A.U	SECTION	COUNTY	TOTAL SHEE	
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	PLOT SCALE = 50.000 1/ in.	CHECKED	- DWB	REVISED -	DEPARTMENT OF TRANSPORTATION		SUMMARY OF QUANTITIES		2554	13-00059-00-RS	DUPAGE	26 3
AODELNAME® F	PLOT DATE = 12/17/2014	DATE	- 10/17/2014	REVISED -		SCALE: NTS	SHEET 1 OF 1 SHEETS STA.	TO STA.	_	It I that's sea	CONTRAC	T NO. 63876



EXISTING TYPICAL SECTION
STA. 100+00 TO STA. 123+97
KUHN ROAD



EXISTING TYPICAL SECTION

STA. 123+97 TO STA. 184+28

OMISSION STA. 184+28 TO STA. 185+69

STA. 185+69 TO STA. 190+25

KUHN ROAD

	USER NAME = _USER_	DESIGNED - BSH	REVISED -	
G:\CH09\0109\Road\Sheets\0109-TYF	SECTIONS-1.dgn	DRAWN - BSH	REVISED -	
200-25 CONTROL STATE OF THE STA	PLOT SCALE = 100.000 '/ in.	CHECKED - DWB	REVISED -	
\$MODELNAME\$	PLOT DATE = 12/22/2014	DATE - 10/17/2014	REVISED -	

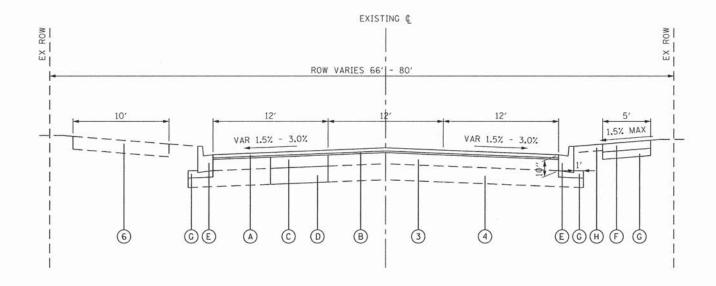
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: NTS

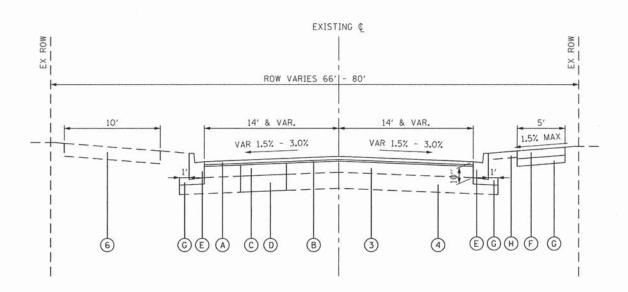
KUHN ROAD RESURFACING		F.A.U RTE.	SECTION	COUNTY	TOTAL						
TYPICAL SECTIONS				2554	13-00059-00-RS	DUPAGE	26	4			
	TYPICAL SECTIONS				IONS				CONTRAC	T NO. 6	3876
SHEET	1	OF	2	SHEETS	STA.	TO STA.		ILLINOIS FED.			

LEGEND

- 1) EXISTING HMA SURFACE COURSE, 1.5"
- 2 EXISTING HMA BINDER COURSE, 1.5"
- 3 EXISTING HMA SUBBASE, 7"
- 4 EXISTING AGGREGATE SUBGRADE, 8"
- (5) EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- 6) EXISTING MULTI-USE PATH, 3" HMA OVER 9" AGGREGATE BASE COURSE
- 7 EXISTING PCC SIDEWALK, 5"
- A HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- B) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- C CLASS D PATCHES, 7" (AS DIRECTED BY ENGINEER)
- D REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AGGREGATE SUBGRADE IMPROVEMENT (AT PATCHING LOCATIONS AS DIRECTED BY ENGINEER)
- E COMBINATION CURB AND GUTTER REMOVAL
 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 OR TYPE B-6.24
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- F) SIDEWALK REMOVAL
 PCC SIDEWALK, 5"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- G SUBBASE GRANULAR MATERIAL, TYPE B 4"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- (H) SEEDING, CLASS 2A
 EROSION CONTROL BLANKET
 TOPSOIL FURNISH AND PLACE, 4"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)



PROPOSED TYPICAL SECTION STA. 100+00 TO STA. 123+97 KUHN ROAD



PROPOSED TYPICAL SECTION
STA. 123+97 TO STA. 184+28
OMISSION STA. 184+28 TO STA. 185+69
STA. 185+69 TO STA. 190+25
KUHN ROAD

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- F SIDEWALK REMOVAL PCC SIDEWALK, 5"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
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 EROSION CONTROL BLANKET
 TOPSOIL FURNISH AND PLACE, 4"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)

NOTES

 PROPOSED ROADWAY PROFILE AND CROSS-SLOPES AT PEDESTRIAN CROSSINGS SHALL BE ADA COMPLIANT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

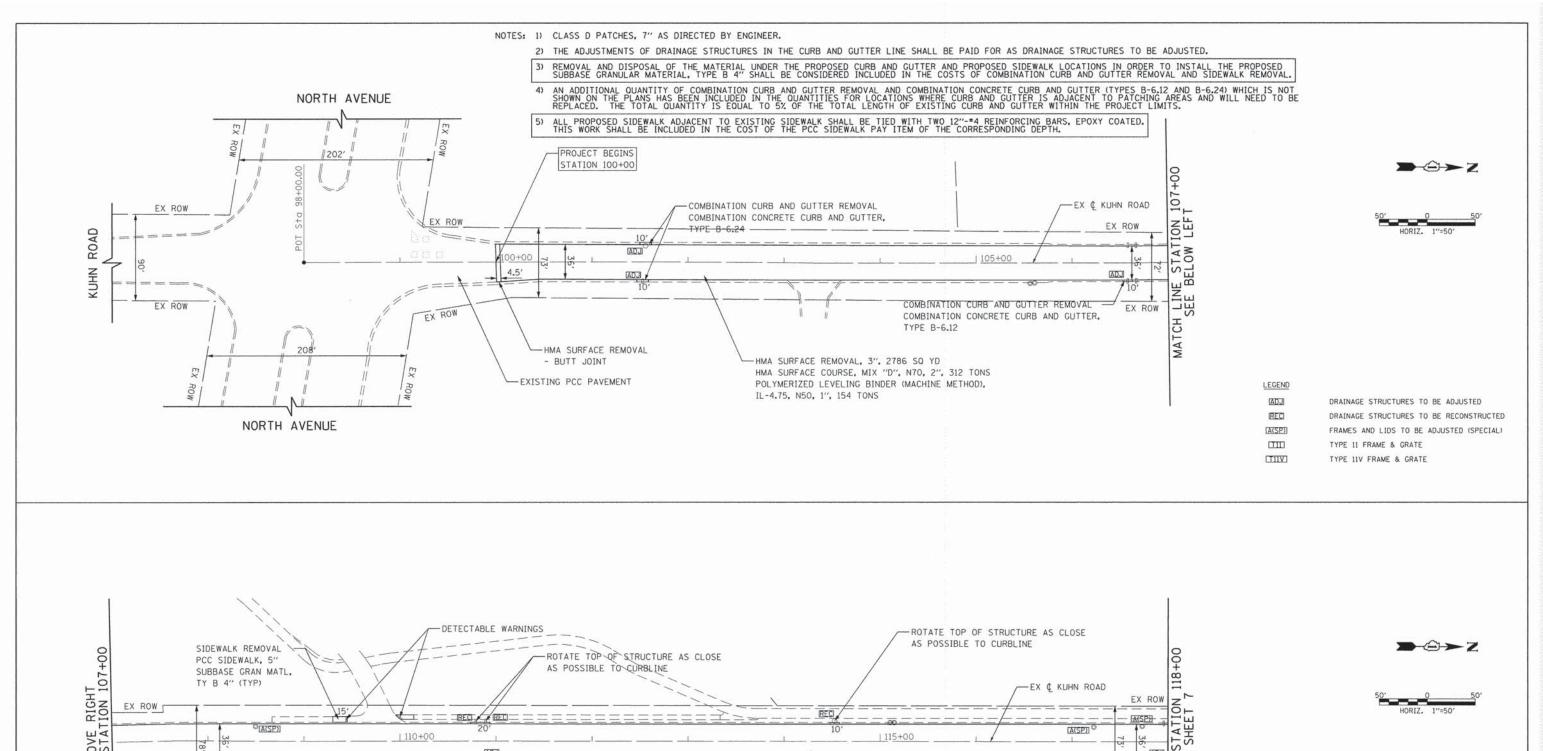
MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% € 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM), N70, 7" (IN 2 LIFTS)	4% € 70 GYRATIONS

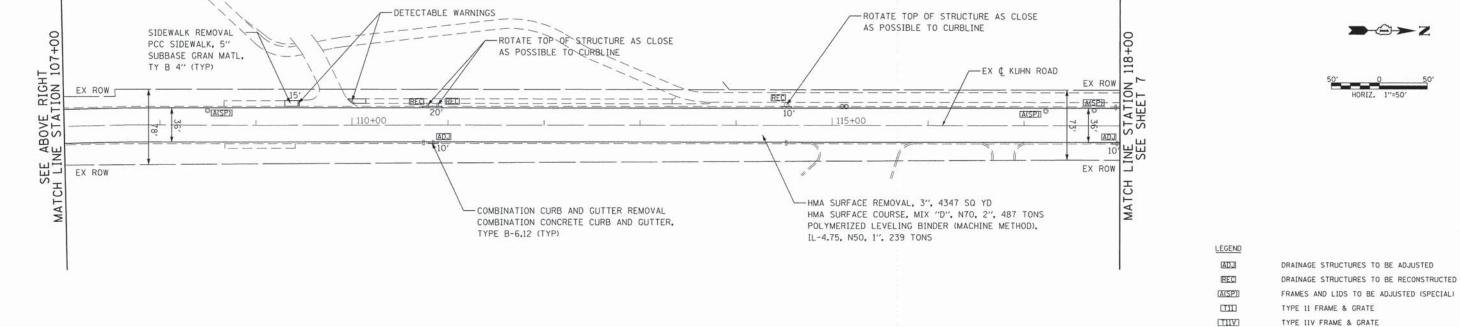
- NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 - 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE THE SPECIAL PROVISIONS.
 - 3) THE CONTRACTOR SHALL MILL BEFORE PATCHING.

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G:\CH09\0109\Road\Sheet	ta\0109-TYPSECTIONS-2.dgn	DRAWN - BSH	REVISED -	STATE 0
	PLOT SCALE = 100.000 ' / in.	CHECKED - DWB	REVISED -	DEPARTMENT OF
\$MODELNAME\$	PLOT DATE = 12/22/2014	DATE - 10/17/2014	REVISED -	

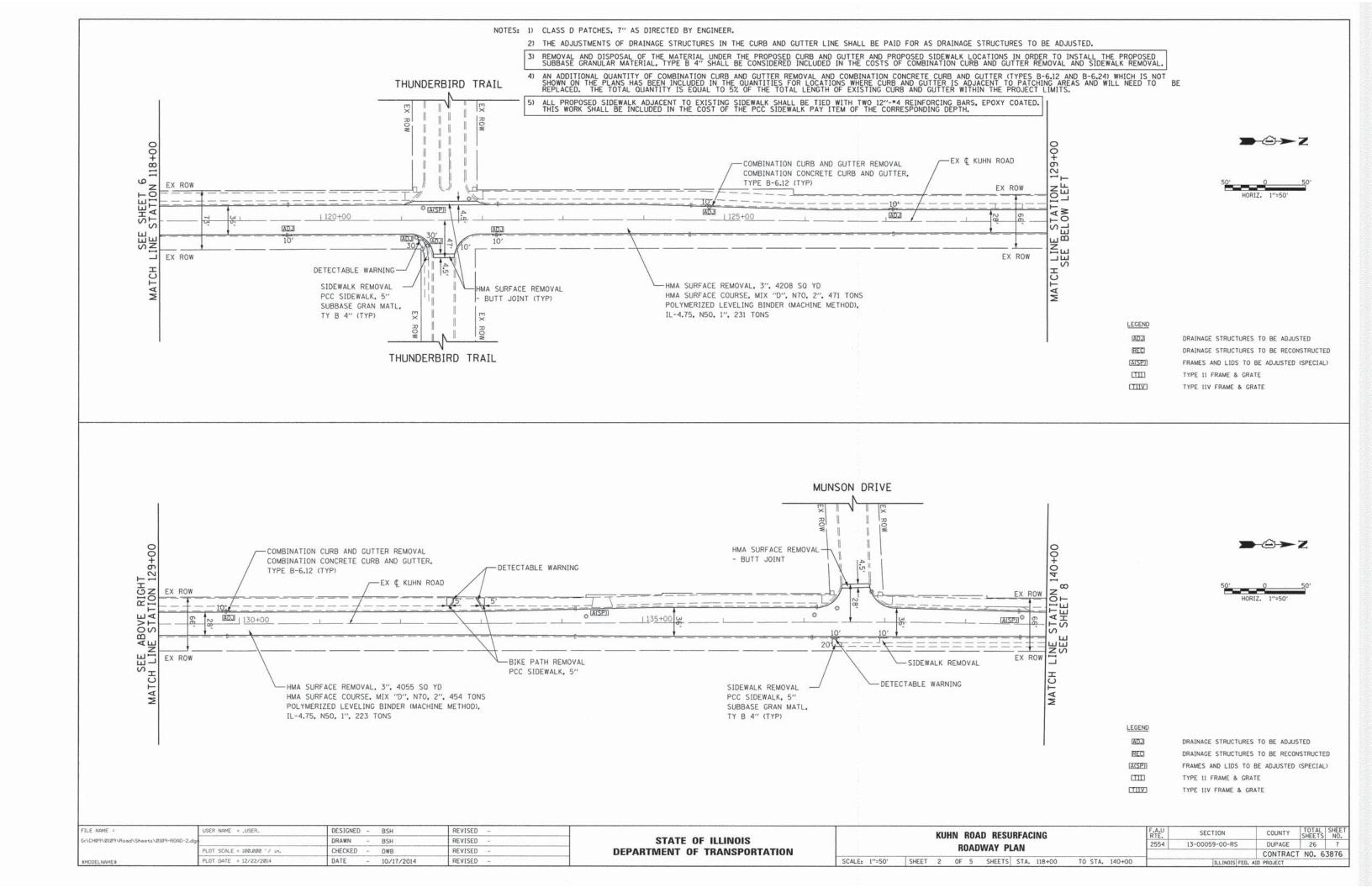
STATI	E OI	FILLINOIS
DEPARTMENT	OF	TRANSPORTATION

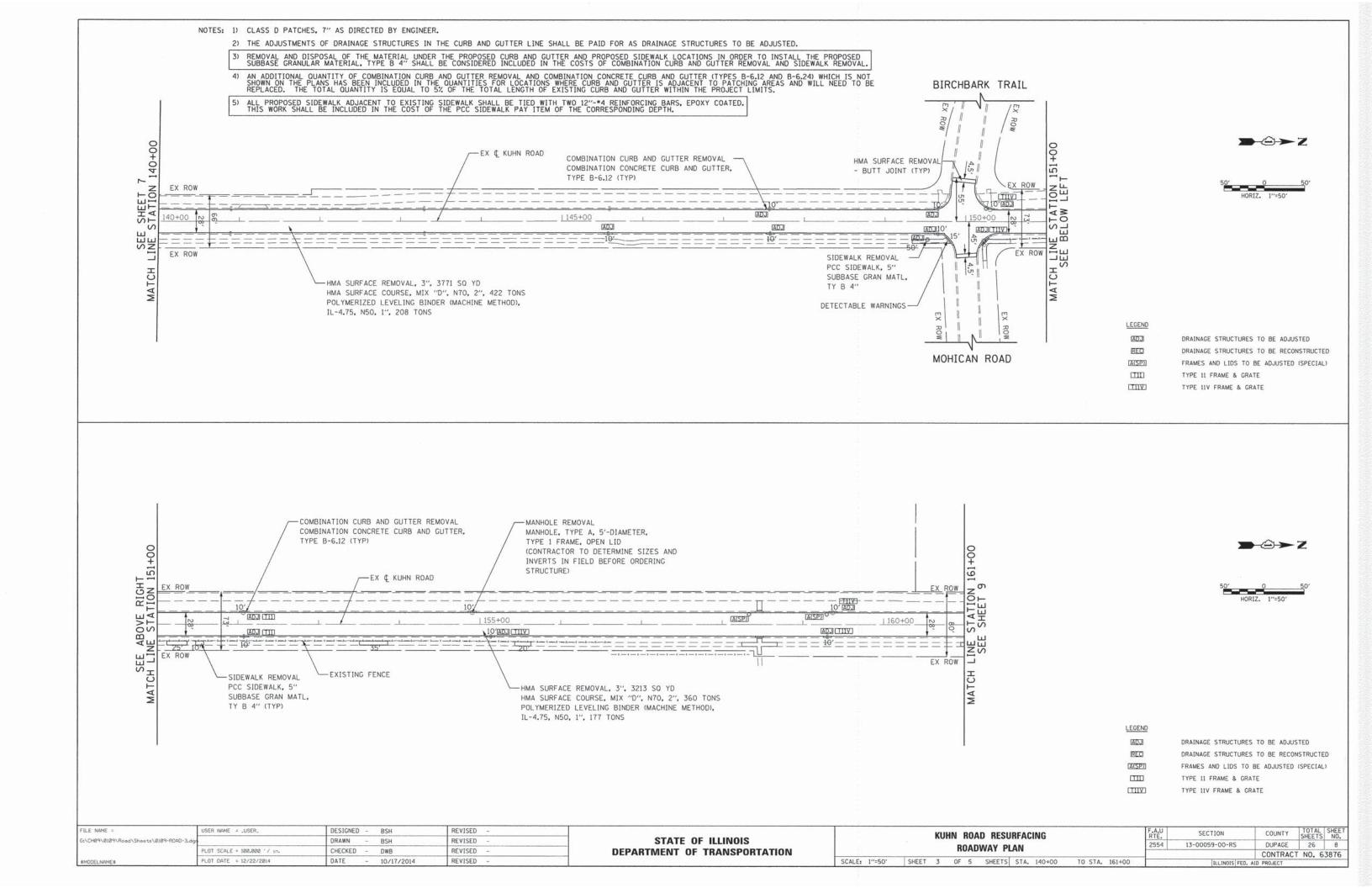
		KU	HN	ROA	D RESU	RFACING		F.A.U RTE.	SECTION	COUNTY	TOTAL	SHEE NO.
TYPICAL SECTIONS						ONS		2554	13-00059-00-RS	DUPAGE	26	5
			- ' '	I IUF	AL SECT	IOIVO				CONTRAC	T NO. 6	3876
SCALE: NTS	SHEET	2	OF	2	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

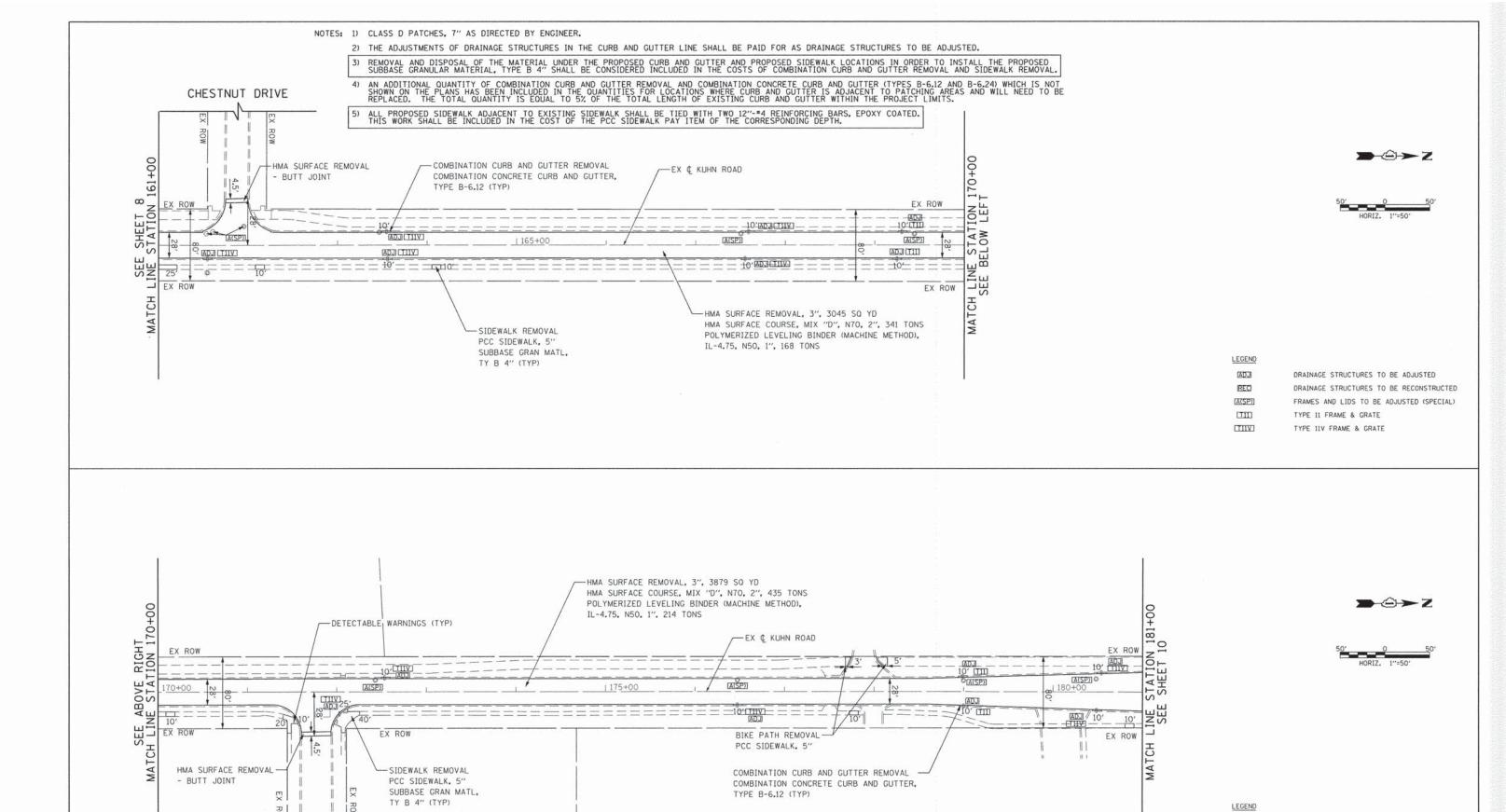




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G:\CH09\0109\Road\Sheets\0109-ROAD-1.dgr		DRAWN - BSH	REVISED -	STATE OF ILLINOIS		ROADWAY PLAN						2554	13-00059-00-RS	DUPAGE	26	6		
	PLOT SCALE = 100.000 * / in.	CHECKED - DWB	REVISED -	DEPARTMENT OF TRANSPORTATION						RUADWAY PLAN						CONTRAC	CT NO.	63876
\$MODELNAME\$	OT DATE = 12/22/2014	DATE - 10/17/2014	REVISED -		SCALE: 1"=50"	SHEET	1	OF 5	SHE	ETS ST	A. 100+00	TO STA. 118+00		ILLINOIS FED.	AID PROJECT	3		







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SMODELNAMES	PLOT DATE = 12/22/2014	DATE - 10/17/2014	REVISED -		SCALE: 1"=50"	SHEET	4 OF	5 SHEET	TS STA. 161+00	TO STA. 181+00		ILLINOIS FED.	AID PROJECT	1 110: 050	210

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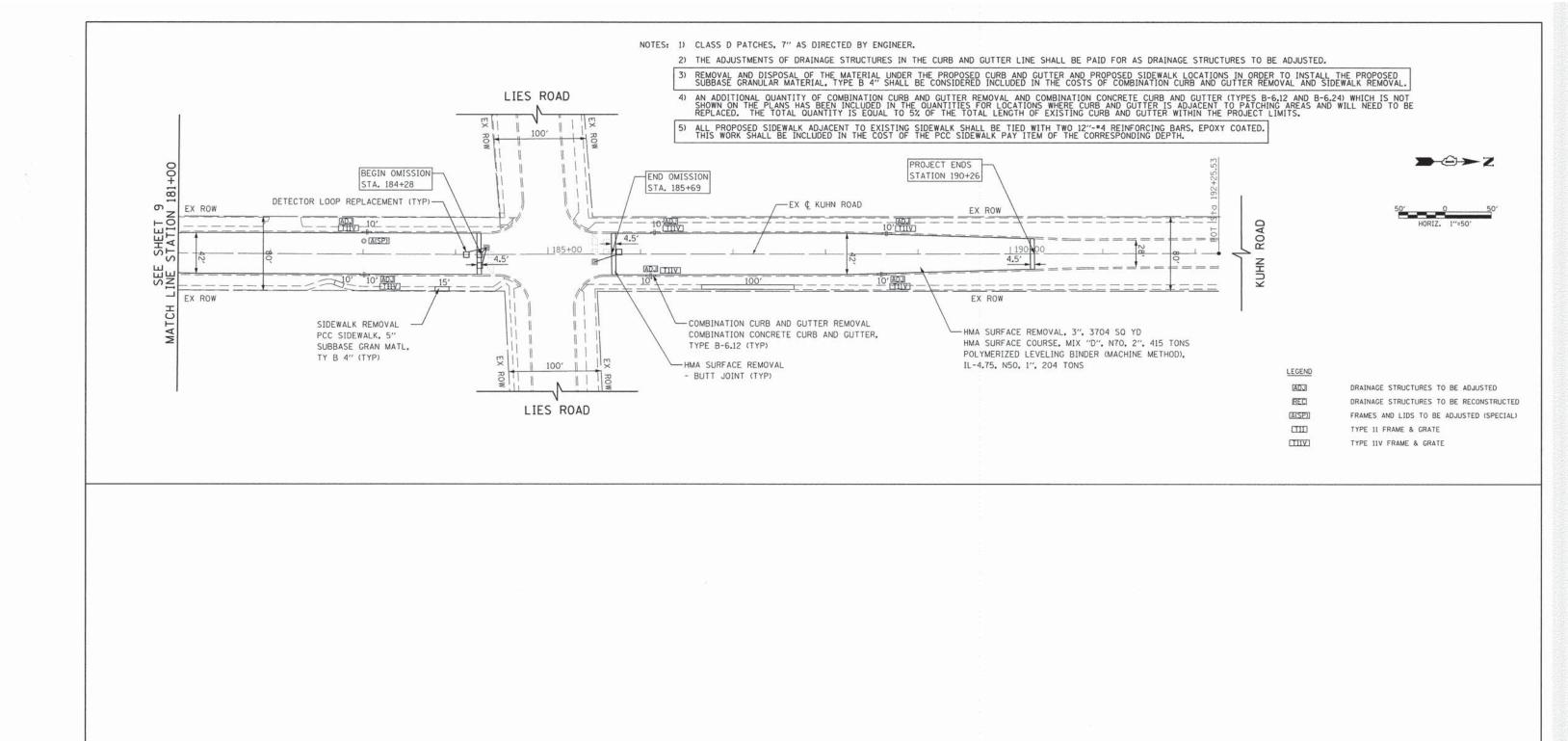
DRAINAGE STRUCTURES TO BE ADJUSTED

TYPE 11 FRAME & GRATE

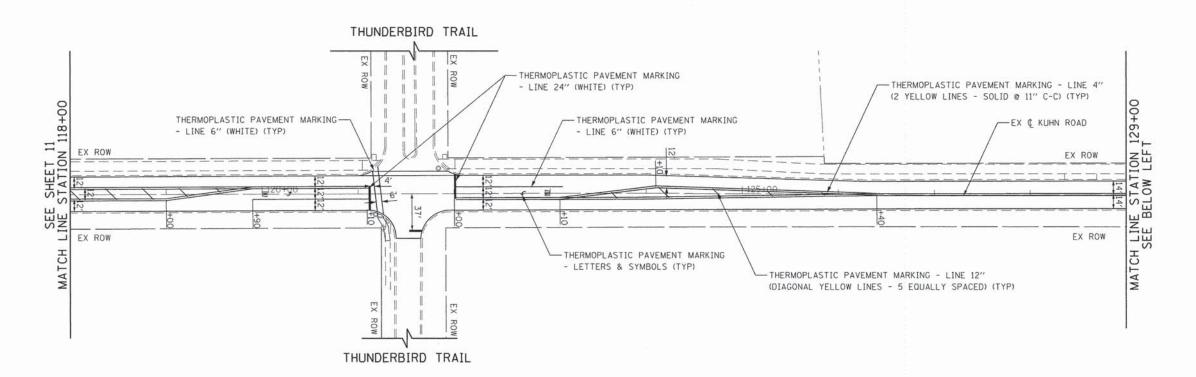
TYPE 11V FRAME & GRATE

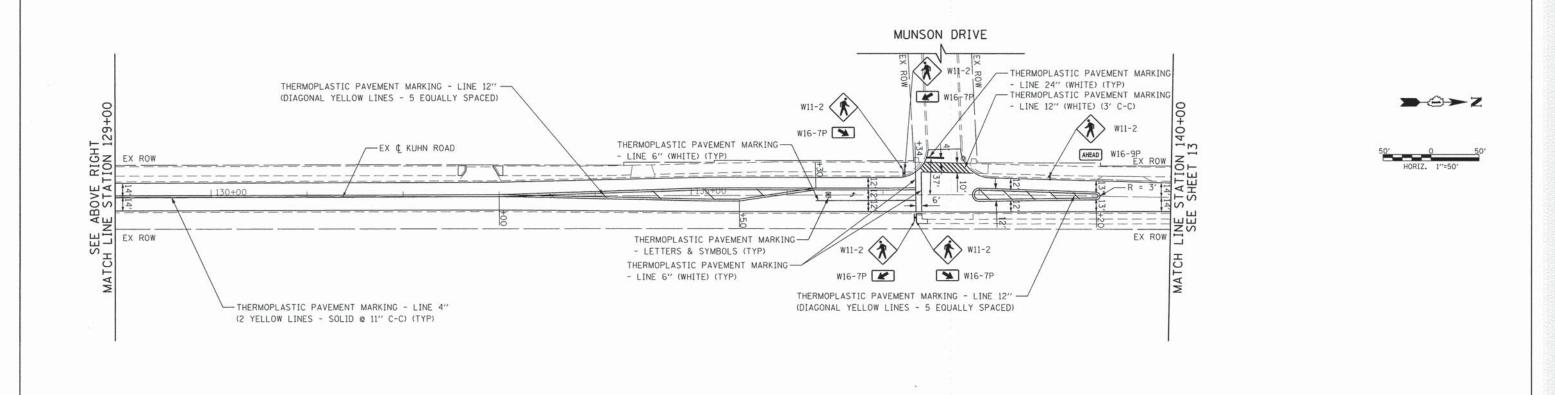
DRAINAGE STRUCTURES TO BE RECONSTRUCTED

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)



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G:\CH09\0109\Road\Sheets\0109-ROAD-5.dgr		DRAWN	- BSH	REVISED -	STATE OF ILLINOIS		2554	13-00059-00-RS	DUPAGE	26 1C
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sMODELNAMEs !	PLOT DATE = 12/22/2014	DATE - 10/17/201	- 10/17/2014	REVISED -		SCALE: 1"=50" SHEET 5 OF 5 SHEETS STA. 181+00 TO STA. 190+26		ILLINOIS FED.	FED. AID PROJECT	





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\$MODELNAME\$	PLOT DATE = 12/22/2014	DATE - 10/17/2014	REVISED -		SCALE: 1"=50" SHEET 2 OF 5 SHEETS STA. 118+00 TO STA. 140+00	1	ILLINOIS FED.	AID PROJECT	1102 05010
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SCALE: 1"=50" SHEET 4 OF 5 SHEETS STA. 161+00

TO STA. 181+00

PLOT DATE = 12/22/2014

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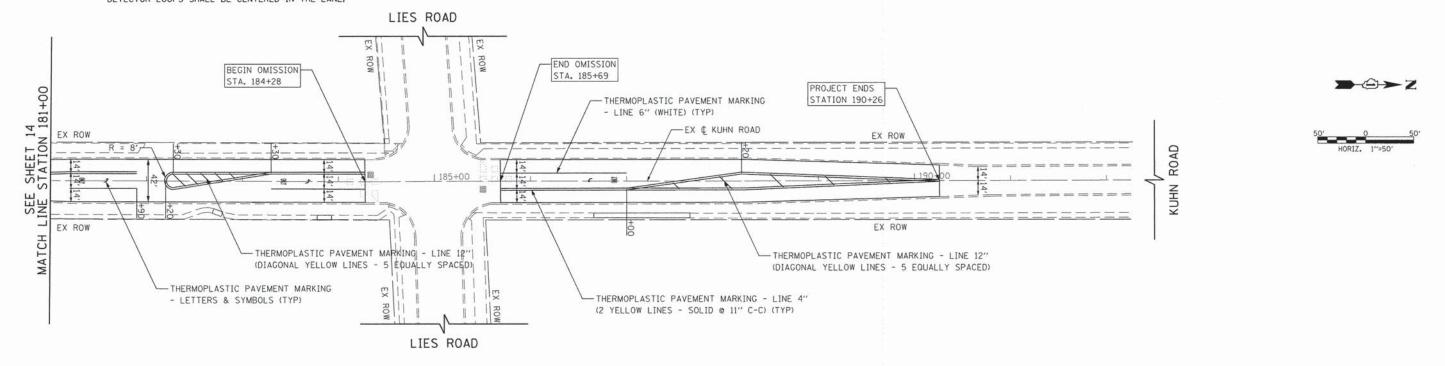
- 10/17/2014

REVISED

CONTRACT NO. 63876



2) ALL DETECTOR LOOPS SHALL BE DIMENSIONED AS 6' BY 6' UNLESS OTHERWISE SHOWN. SEE DISTRICT ONE DETAIL TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE. FOR 14' LANES THE DETECTOR LOOPS SHALL BE CENTERED IN THE LANE.

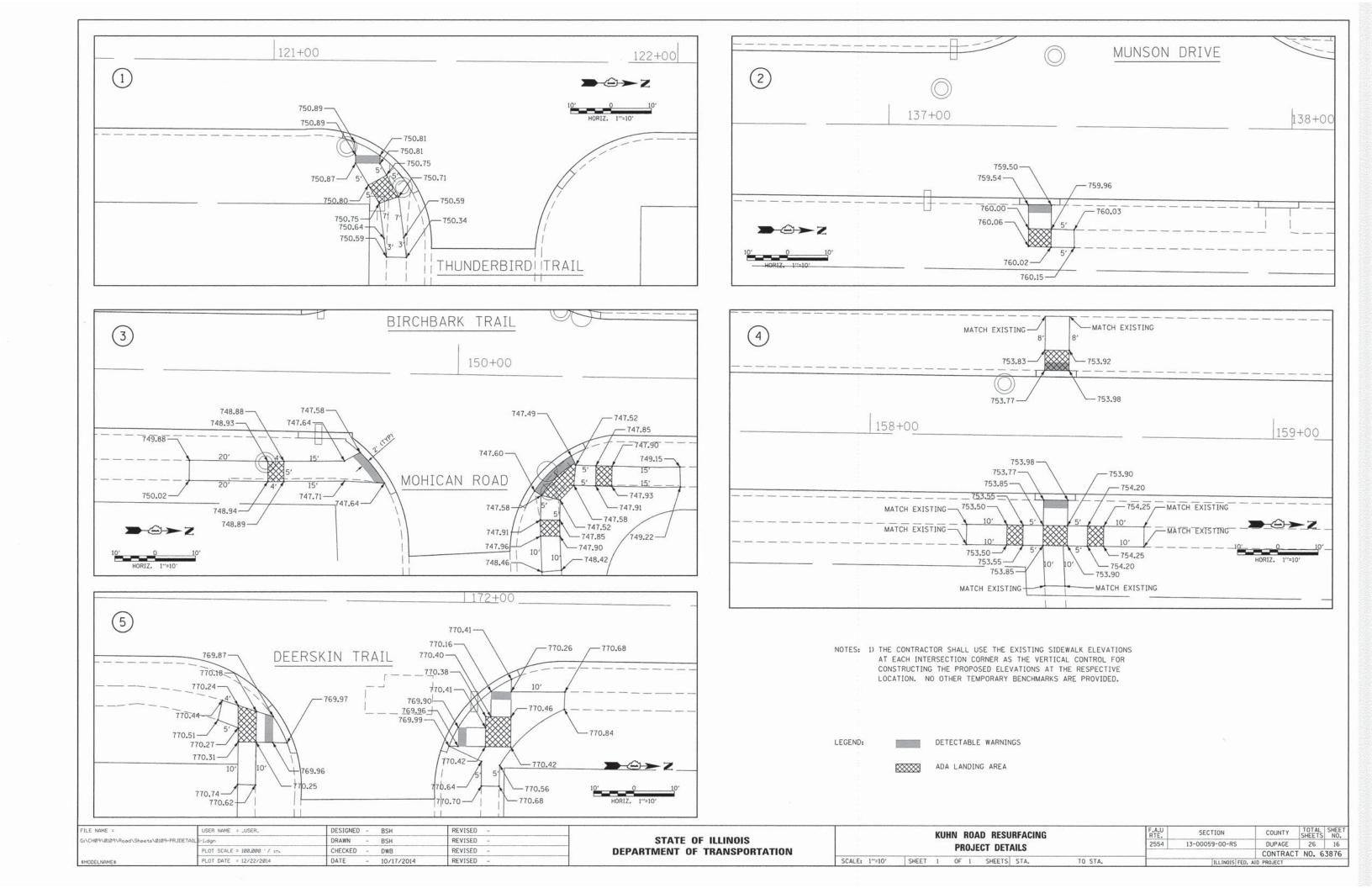


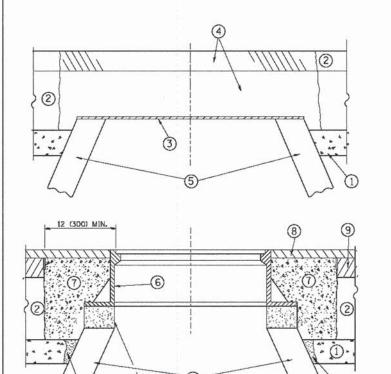
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DEPARTMENT	OF	TRANSPORTATION

SCALE: 1"=50" SHEET

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IA	V LIVI	LIVI	IVIANINI	NG F	LAIV					CONTRACT	NO. 6	3876
5	OF	5	SHEETS	STA.	181+00	TO STA.	192+26		ILLINOIS FED.	AID PROJECT		





NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

PROPOSED

BRICK, MORTAR, OR CONC. ADJUSTING RINGS

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

VILLAGE OF CAROL STREAM CASTINGS ARE THE PROPERTY OF THE VILLAGE AND THE CONTRACTOR SHALL NOTIFY THE VILLAGE FOR REMOVAL AND DISPOSITION OF THE CASTINGS

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED. THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

SCALE: NONE

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- (6) FRAME AND LID (SEE NOTES)
- (2) EXISTING PAVEMENT
- 7 CLASS PP-1* CONCRETE
- 3 36 (900) DIAMETER METAL PLATE
- (8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX

 (5) EXISTING STRUCTURE
- 9 PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. LIPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

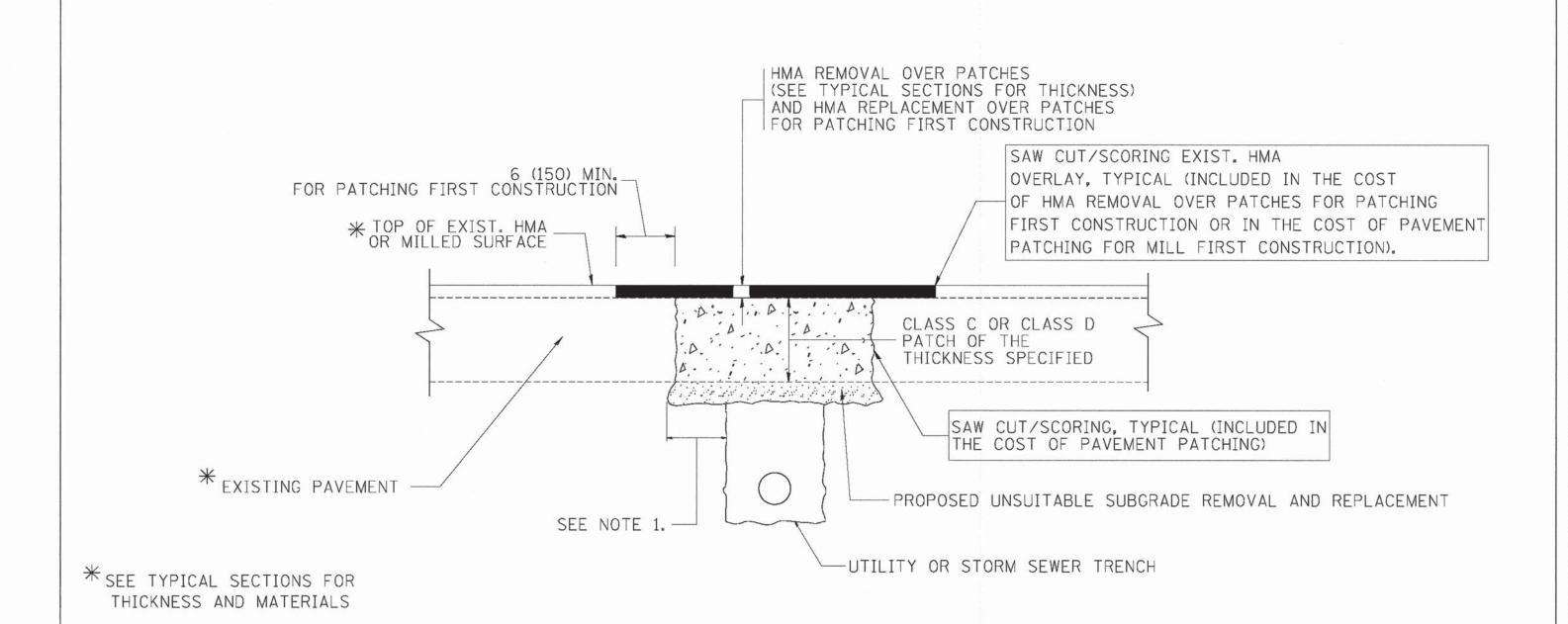
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CONTRACT NO. 63876

FILE NAME =	USER NAME = bouerd1	DESIGNED -	R. SHAH	REVISED	- R	. WIEDEMAN 05-14-04
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	PLDT SCALE = 1968.5000 '/ m	CHECKED -		REVISED	- R	BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE -	10-25-94	REVISED	- R	80R0 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR		F.A.U RTE.	SECTION	COUNTY
	HIMC	2554	13-00059-00-RS	DUPAGE
FRAMES AND LIDS ADJUSTMENT WITH M	ILLING	В	D600-03 (BD-8)	CONTRA
SHEET NO. 1 OF 1 SHEETS STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED.	UD PROJECT



NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

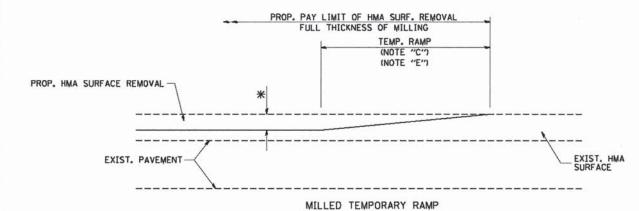
- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

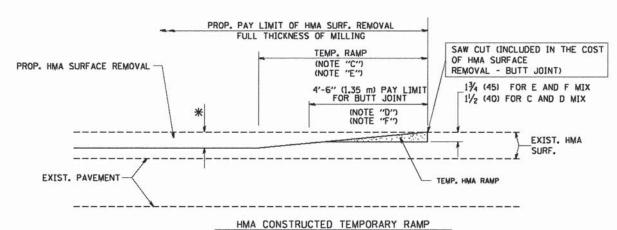
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = bouerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98		PAVEMENT PATCHING FOR	F.A.U SECTION	COUNTY TOTAL SHEET
c:\projects\diststd22x34\bd22.dgn		DRAWN -	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS		2554 13-00059-00-RS	DUPAGE 26 18
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	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST, NO. 1 ILLINDIS FED. AL	



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

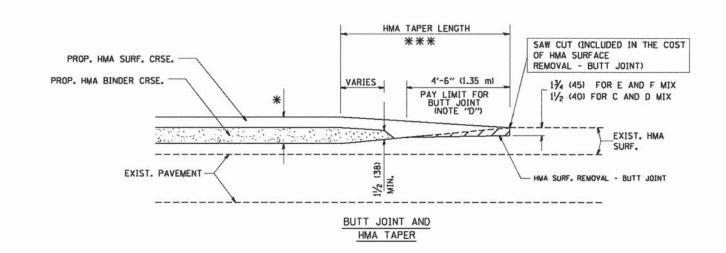
OPTION 1



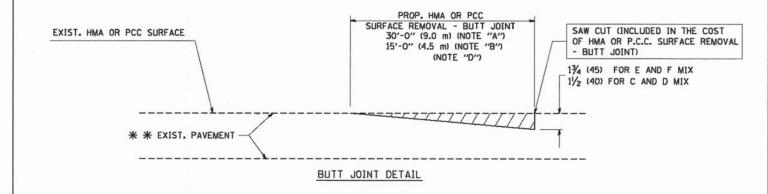
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

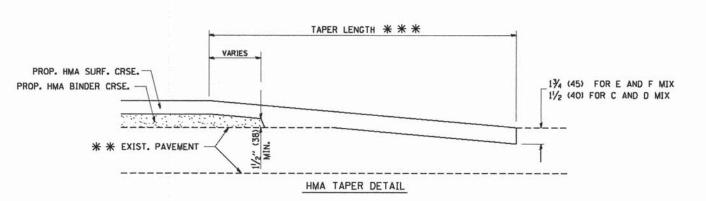
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE. HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-O" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER)
 FOR "HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

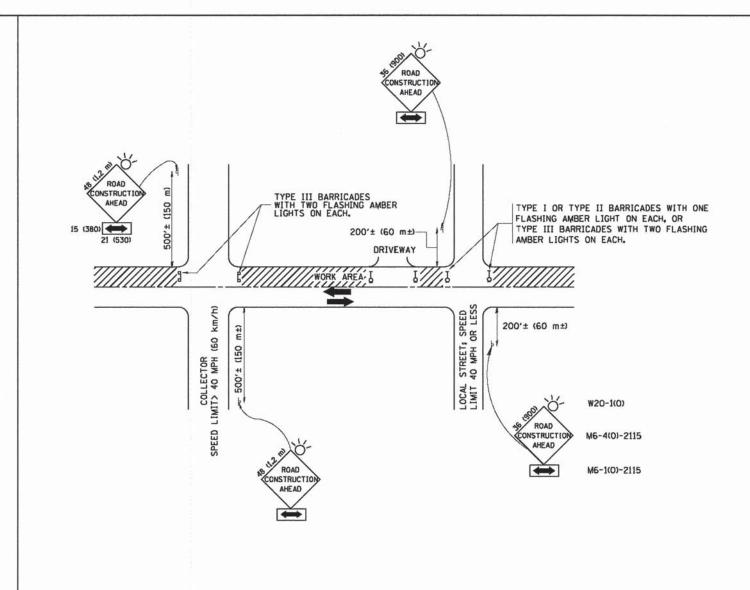
SCALE: NONE

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATI	E OF	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

BUTT JOINT AND	F.A.U RTÉ.	COUNTY	TOTAL	SHEET NO.	
HMA TAPER DETAILS	2554	13-00059-00-RS	DUPAGE	26	19
HIVIA TAPER DETAILS		CONTRACT	53876		
SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FFD. BOAL	DIST NO 1 DI INDIS EED	AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 36 × 36 (900×900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN POLICE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE 1, TYPE 11 OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

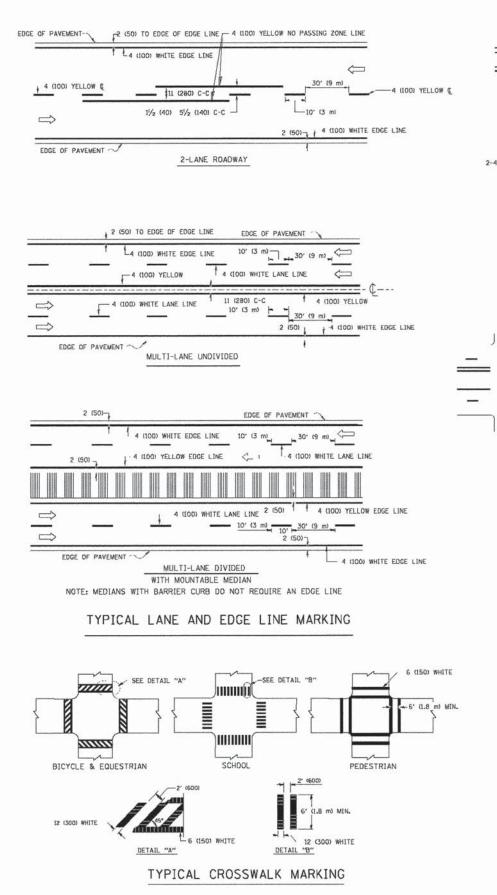
B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY LINLESS DTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

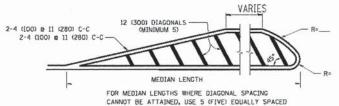
All dimensions are in millimeters (inches) unless otherwise shown.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION





4' (1.2 m) WIDE MEDIANS ONLY



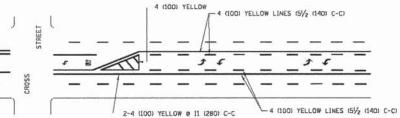
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))

75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))

150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINES.

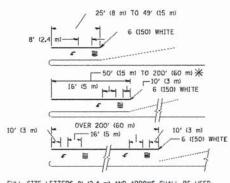


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR, ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

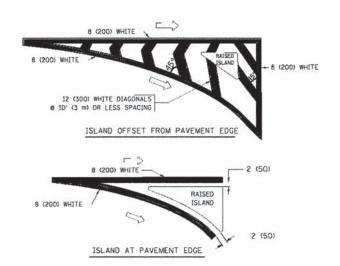


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P_1 AREA = 15.6 SO. FT. (1.5 m²) \P_1 AREA = 20.8 SO. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

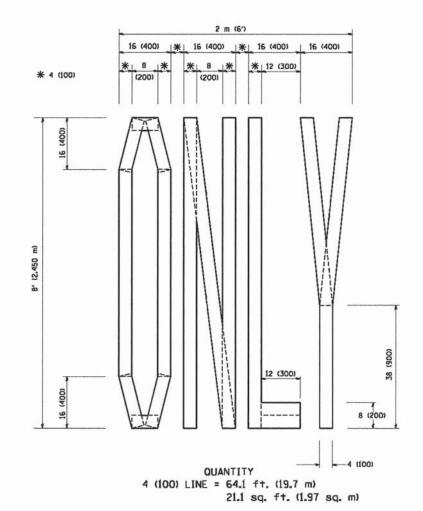
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 8 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 m 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 m 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 51/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 % 6 (150) 12 (300) % 45° 12 (300) % 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (500) APART 2' (500) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO GROSSMALK, IF PRESENT. OTHERWISE, PLACE AT USERIED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 m 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA 0F: '78'-3.6 SO, FT. (0.33 m²) EACH '78'-54,0 SQ, FT, (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (0VER 45MPH (70 km/h))

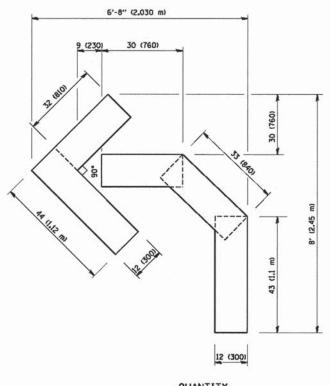
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters)

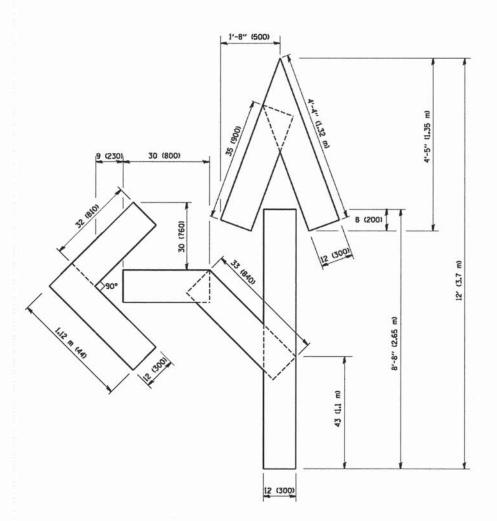
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-			

FILE NAME	USER NAME dravakovajii	DESIGNED - EVERS	REVISED -T. RAMMAC	HER 10-27-94			DISTRICT ONE		F.A.U RTE.	SECTION	COUNTY	TOTAL SH	ET O.
u:\pw_work\pwidot\drivakoog\d8180315\to	3.dqn	DRAWN -	REVISED -C. JUCIUS	09-09-09	STATE OF ILLINOIS		TYPICAL PAVEMENT MARKINGS		2554	13-00059-00-RS	DUPAGE	26	21
	PLOT SCALE 50.000 '/ IN.	CHECKED -	REVISED -		DEPARTMENT OF TRANSPORTATION					TC-13	CONTRACT	NO. 6387	16
	PLOT DATE 9/9/2009	DATE - 03-19-90	REVISED -			SCALE: NONE	SHEET NO. [OF 1 SHEETS STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED.	AID PROJECT		





OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)



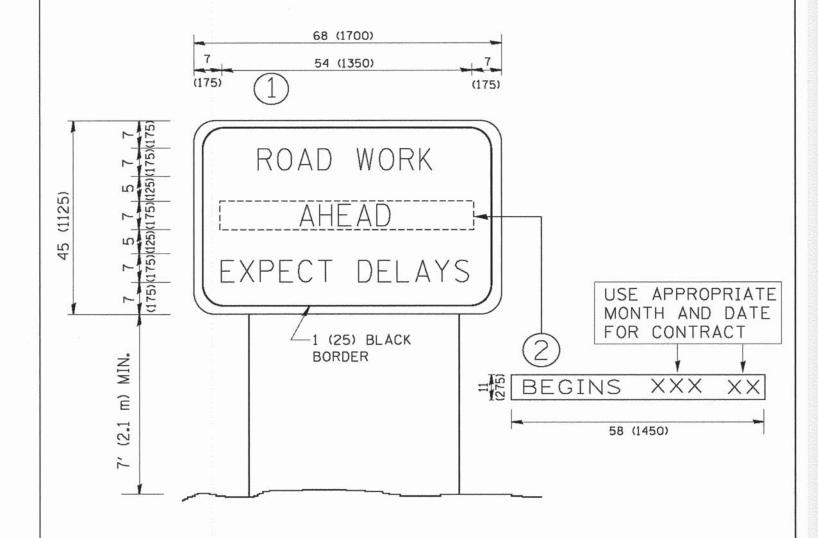
QUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = geglienobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
W:\diststd\22x34\tc16.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
The state of the s	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

	PAVEMENT MARKING LETTE	RS AND SY	MBOLS	F.A.U RTE.	SECTION	COUNTY	TOTAL	SHEET NO.
	FOR TRAFFIC STAGING				13-00059-00-RS	DUPAGE	26	22
	ron marrie si	Adiiva			TC-16	CONTRAC	T NO. 6	3876
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED.	AID PROJECT		



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN () WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO	O STA.	FED. ROA	D DIST. NO. 1 ILLINOIS FED.	ALD PROJECT		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99	DEPARTMENT OF TRANSPORTATION	INFORMATION SIGN TC-22 CG		CONTRAC	CONTRACT NO. 63876					
Wi\diststd\22x34\to22.dgn		DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS	2554 13-00059-00-RS		DUPAGE	26	23				
FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97			ARTERIAL RO	ΔD		F.A.U RTE.	SECTION	COUNTY	SHEETS	NO.



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

USER NAME = confluencht

- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
- 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.

DESIGNED -

3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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	Wi\diststd\22x34\ta26.dgn		DRAWN -	REVISED -	STATE OF ILLINOIS		DHIVEVVA	ENTRANC	E SIGNING	
		PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION					
		PLOT DATE = 1/4/2008	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.

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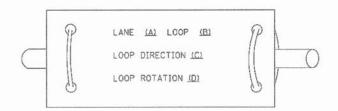
CONTRACT NO. 63876

REVISED - C. JUCIUS 02-(5-07

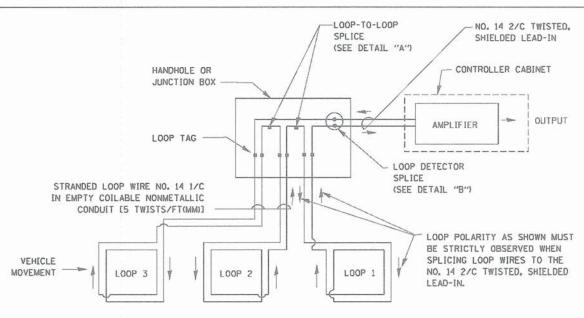
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER.
 ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT
 FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE
 DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

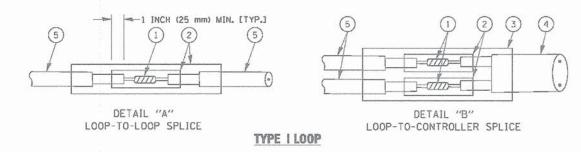


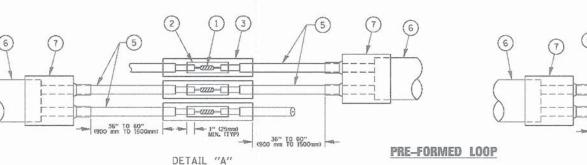
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

- " LOOPS SHALL BE SPLICED IN SERIES.
- " SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- " LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.





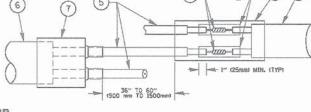
LOOP DETECTOR SPLICE

- (1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

SCALE: NONE

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

- (5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- (6) PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

26 25

ACT NO. 63876

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U RTE.	SECTION	COUNTY
PTANDARD TRAFFIC CICNAL DECICN	2554	13-00059-00-RS	DUPAGE	
STANDARD TRAFFIC SIGNAL DESIGN	DETAILS		CONTRA	
HEET NO. 2 OF 7 SHEETS STA.	TO STA.	FEO. ROAD	DIST. NO. 1 ILLINOIS FED.	AID PROJECT

LOOPS NEXT TO SHOULDERS PROVIDE A PAVEMENT REPLACEMENT PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER 900 MIN (1.5 m) (1.8 m) (1.5 m) 1" (25 mm) UNIT DUCT-TRENCHED (3,0 m) (3.0 m)

* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS

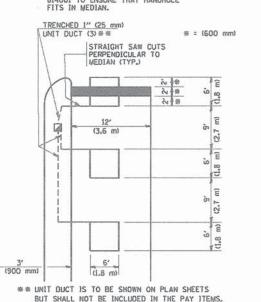
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

= (600 mm)

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING) HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS

AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE



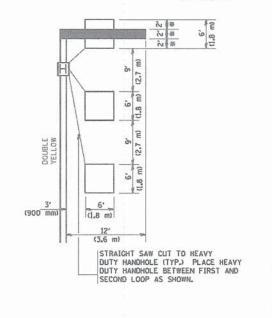
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

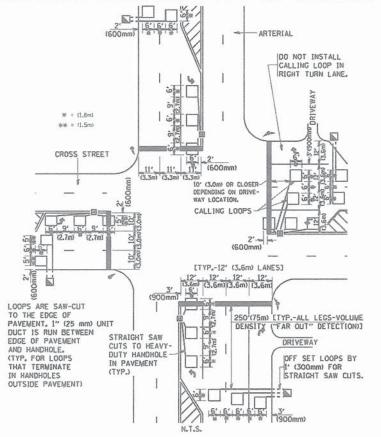
% = (600 mm)

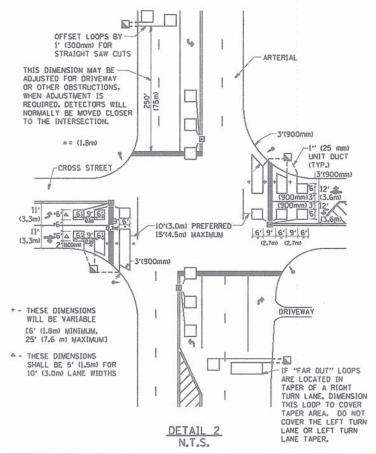


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX, EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (I.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN, WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE). USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

TOTAL SHEE SHEETS NO.

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\diatatd\22x34\ta87.dgn		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

DETAIL

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SECTION COUNTY DISTRICT 1 - DETECTOR LOOP INSTALLATION 2554 13-00059-00-RS DUPAGE 26 DETAILS FOR ROADWAY RESURFACING CONTRACT NO. 63876 TS-07 SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT