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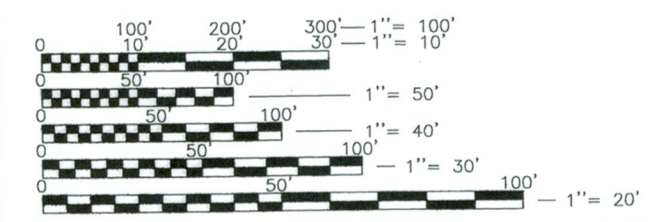
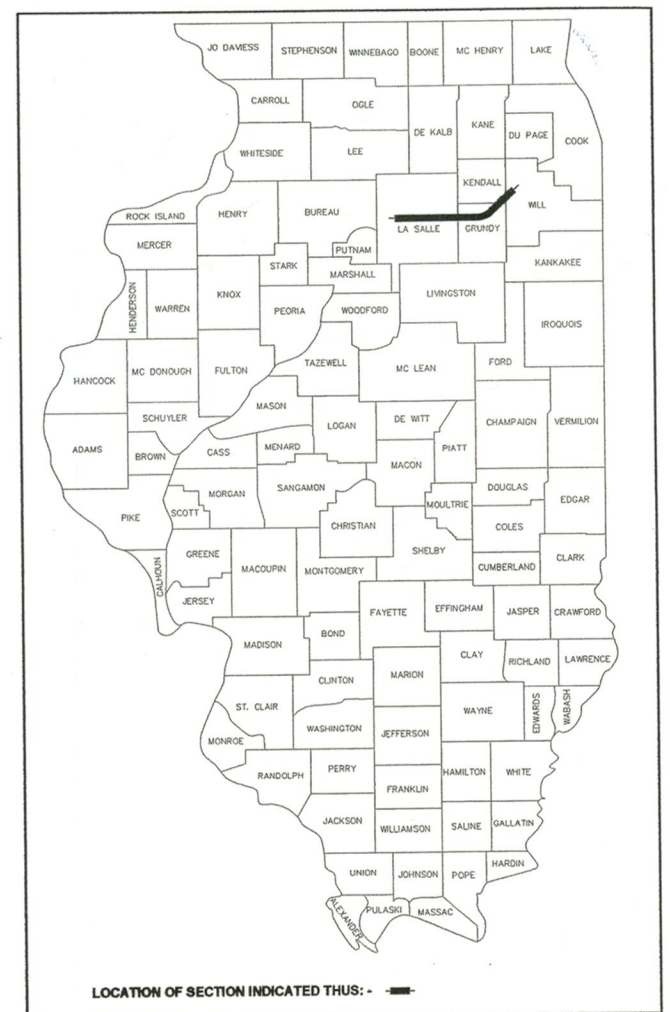
PROPOSED
LOCAL AGENCY IMPROVEMENT
ILLINOIS TRANSPORTATION ENHANCEMENT PROGRAM
I & M CANAL NATIONAL HERITAGE
CORRIDOR HISTORIC MARKERS &
WAYFINDING SIGNAGE PROJECT - PHASE 1
SECTION 10-00143-00-SG
PROJECT TE-00D3(083)
INSTALL SIGNAGE
C-93-016-14

HIGHWAY STANDARDS

- 701001-02 OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
- 701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701201-04 LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEEDS ≥ 45 MPH
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701502-06 URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
- 701901-03 TRAFFIC CONTROL DEVICES

DESIGN GUIDELINES

- BUREAU OF LOCAL ROADS AND STREETS MANUAL
- RECONSTRUCTION POLICY OF THE BUREAU OF LOCAL ROADS AND STREETS MANUAL
- TRAFFIC CONTROL DEVICES, BUREAU OF LOCAL ROADS AND STREETS MANUAL



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 87575

GROSS LENGTH OF IMPROVEMENT: 60 MILES
NET LENGTH OF IMPROVEMENT: 60 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 11/14/2013 Craig Cassem
GRUNDY COUNTY ENGINEER

PASSED 12/2/2013 Daniel R. [Signature]
DISTRICT 3 LOCAL ROADS AND STREETS ENGINEER

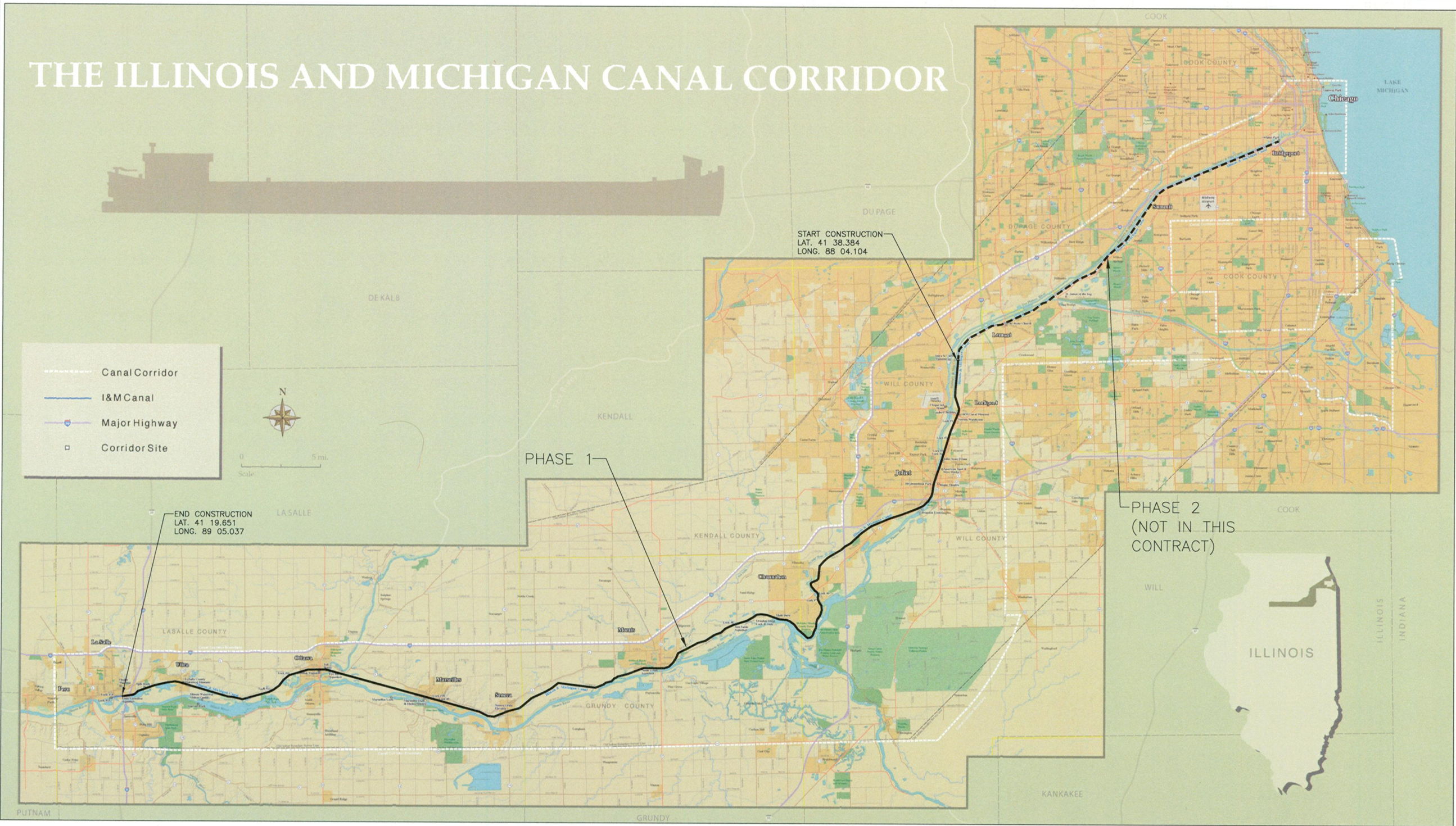
RELEASED FOR BID 12/2/2013
ON LIMITED REVIEW Paul D. [Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION 2 ENGINEER

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OF THE STATE OF ILLINOIS



CHAMLIN & ASSOCIATES, INC.
PERU MORRIS
ILLINOIS

THE ILLINOIS AND MICHIGAN CANAL CORRIDOR



SUMMARY OF QUANTITIES

PAY ITEM NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	WILL COUNTY	GRUNDY COUNTY	LASALLE COUNTY
67100100	MOBILIZATION	L.SUM	1	1	0	0
XX008922	SIGN PANEL - MILEMARKER	EACH	65	16	16	33
XX008923	SIGN PANEL - SILHOUETTE	EACH	29	14	3	12
XX008924	SIGN PANEL - CANTILEVER	EACH	17	3	6	8
XX008925	SIGN PANEL - KIOSK	EACH	18	6	5	7
XX008926	SIGN PANEL - STRAIGHT COR-TEN	EACH	7	6	1	0
XX008927	SIGN PANEL - MILEMARKER DELIVERED	EACH	1	0	0	1
XX008928	SIGN PANEL - STRAIGHT COR-TEN DELIVERED	EACH	1	0	0	1

CONTRACT NO. 87575

GROSS LENGTH OF IMPROVEMENT: 60 MILES
NET LENGTH OF IMPROVEMENT: 60 MILES

CHAMLIN & ASSOCIATES, INC. PERU MORRIS ILLINOIS

Sign Schedule - Sorted by Sign Type

Cantilever Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
43.07	41 31.536	088 05.279	Life in the Rocks	Cantilever	Joliet	Joliet Park District	Installed
48.17	41 25.264	088 13.716	Meeting of the Waters	Cantilever	Channahon	Will Co FP	Installed
48.18	41 23.097	088 14.399	Orientation sign (McKinley woods	Cantilever	Channahon	Will Co FP	Installed
51.04	41 23.746	088 19.922	Aux Sable Village	Cantilever	Morris	IDNR	Installed
51.05	41 23.754	088 19.892	Lock 8 and Aux Aqueduct	Cantilever	Morris	IDNR	Installed
51.06	41 24.126	088 16.816	Dresden Barn	Cantilever	Morris	IDNR	Installed
51.07	41 21.370	088 26.320	Yesterdays Canal	Cantilever	Morris	IDNR	Installed
51.08	41 21.377	088 25.424	Welcome to Morris a Historic I&M Canal Town	Cantilever	Morris	City of Morris	Installed
51.13	41 21.379	088 25.419	Welcome to Morris a Historic I&M Canal Town	Cantilever	Morris	City of Morris	Installed
52.03	41 18.900	088 36.676	Old Grain Elevator	Cantilever	Seneca	IDNR	Installed
53.05	41 19.701	088 42.720	Old Swing Bridge	Cantilever	Marseilles	IDNR	Installed
54.01	41 19.782	088 54.876	Stagecoach Canal and Rail Travel	Cantilever	Ottawa	IDNR	Installed
58.05	41 20.101	089 03.338	Transportation at Split Rock	Cantilever	LaSalle	IDNR	Installed
58.06	41 20.100	089 03.339	Split Rock Geology	Cantilever	LaSalle	IDNR	Installed
58.16	41 19.582	089 05.674	North Meets South	Cantilever	LaSalle	IDNR	Installed
58.17	41 19.579	089 05.690	Rough and Ready	Cantilever	LaSalle	IDNR	Installed
58.20	41 19.651	89 5.037	Little Vermillion Aqueduct	Cantilever	LaSalle	IDNR	Installed



Kiosk Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
43.25	41 30.246	088 06.341	Brandon Road Kiosk	Kiosk	Joliet	IDNR	Installed
48.01	41 25.168	088 13.691	Bridge Street Kiosk	Kiosk	Channahon	IDNR	Installed
48.02	41 28.413	088 11.230	Channahon Park District kiosk	Kiosk	Channahon	IDNR	Installed
48.03	41 25.425	088 13.670	Channahon State Park Kiosk (parking lot)	Kiosk	Channahon	IDNR	Installed
48.04	41 25.382	088 13.727	Channahon State Park Kiosk (trail)	Kiosk	Channahon	IDNR	Installed
48.05	41 29.173	088 09.924	Rock Run Kiosk	Kiosk	Channahon	IDNR	Installed
51.01AB	41 21.412	088 26.353	Gebhard Woods Kiosk/ credit panel	Kiosk	Morris	IDNR	Installed
51.11AB	41 24.281	088 17.395	Dresden Kiosk/ credit	Kiosk	Morris	IDNR	Installed
51.19	41 21.370	088 25.236	Straton Kiosk	Kiosk	Morris	IDNR	Installed
51.26	41 23.727	088 19.954	Aux Sable Kiosk	Kiosk	Morris	IDNR	Installed
51.30	41 21.930	088 24.281	Cemetery Road Kiosk	Kiosk	Morris	IDNR	Installed
52.04A	41 18.898	088 36.612	Seneca kiosk/ credit panel	Kiosk	Seneca	IDNR	Installed
54.02	41 19.785	088 54.851	Buffalo Rock kiosk	Kiosk	Ottawa	IDNR	Installed
54.06	41 21.030	088 51.964	Ottawa Kiosk	Kiosk	Ottawa	IDNR	Installed
56.04	41 20.449	089 00.745	Utica Kiosk	Kiosk	Utica	IDNR	Installed
58.19	41 20.100	089 03.338	Split Rock Kiosk	Kiosk	LaSalle	IDNR	Installed
58.02AB	41 19.586	089 05.615	Lock 14 Kiosk/ credit panel	Kiosk	LaSalle	IDNR	Installed
54.09AB	41 21.151	088 49.763	Ottawa Kiosk (aqueduct)	Kiosk	Ottawa	IDNR	Installed



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Sign Schedule - Sorted by Sign Type

Mile Marker Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
48.11	41 25.420	088 13.731	Milemarker 44	Milemarker	Channahon	IDNR	Installed
43.18	41 29.183	088 09.902	Milemarker 38	Milemarker	Joliet	IDNR	Installed
43.19	41 29.562	088 08.870	Milemarker 37	Milemarker	Joliet	IDNR	Installed
43.20	41 29.836	088 07.766	Milemarker 36	Milemarker	Joliet	IDNR	Installed
43.21	41 30.147	088 06.686	Milemarker 35	Milemarker	Joliet	IDNR	Installed
43.28	41 32.238	088 04.785	Milemarker 32	Milemarker	Joliet	IDNR	Installed
48.06	41 28.682	088 10.833	Milemarker 39	Milemarker	Channahon	IDNR	Installed
48.07	41 28.144	088 11.743	Milemarker 40	Milemarker	Channahon	IDNR	Installed
48.08	41 27.615	088 12.646	Milemarker 41	Milemarker	Channahon	IDNR	Installed
48.09	41 27.052	088 13.495	Milemarker 42	Milemarker	Channahon	IDNR	Installed
48.10	41 26.217	088 13.741	Milemarker 43	Milemarker	Channahon	IDNR	Installed
48.12	41 23.813	088 13.858	Milemarker 46	Milemarker	Channahon	IDNR	Installed
48.13	41 23.063	088 14.325	Milemarker 47	Milemarker	Channahon	IDNR	Installed
48.14	41 24.277	088 18.322	Milemarker 51	Milemarker	Channahon	IDNR	Installed
48.15	41 23.892	088 19.349	Milemarker 52	Milemarker	Channahon	IDNR	Installed
48.16	41 24.610	088 13.459	Milemarker 45	Milemarker	Channahon	IDNR	Installed
51.14	41 20.290	088 30.588	Milemarker 63	Milemarker	Morris	IDNR	Installed
51.15	41 20.476	088 29.506	Milemarker 62	Milemarker	Morris	IDNR	Installed
51.16	41 20.883	088 28.515	Milemarker 61	Milemarker	Morris	IDNR	Installed
51.17	41 21.134	088 27.414	Milemarker 60	Milemarker	Morris	IDNR	Installed
51.18	41 21.370	088 26.320	Milemarker 59	Milemarker	Morris	IDNR	Installed
51.20	41 21.339	088 25.172	Milemarker 58	Milemarker	Morris	IDNR	Installed
51.21	41 21.930	088 24.208	Milemarker 57	Milemarker	Morris	IDNR	Installed
51.22	41 22.403	088 23.486	Milemarker 56	Milemarker	Morris	IDNR	Installed
51.23	41 22.771	088 22.439	Milemarker 55	Milemarker	Morris	IDNR	Installed
51.24	41 23.164	088 21.420	Milemarker 54	Milemarker	Morris	IDNR	Installed
51.25	41 23.604	088 20.441	Milemarker 53	Milemarker	Morris	IDNR	Installed
51.27	41 24.233	088 17.218	Milemarker 50	Milemarker	Morris	IDNR	Installed
51.28	41 24.056	088 16.105	Milemarker 49	Milemarker	Morris	IDNR	Installed
51.29	41 23.532	088 15.214	Milemarker 48	Milemarker	Morris	IDNR	Installed
52.01AB	41 18.945	088 39.038	Milemarker 71	Milemarker	Seneca	IDNR	Installed
52.02AB	41 20.406	088 31.733	Milemarker 64	Milemarker	Seneca	IDNR	Installed
52.05AB	41 19.099	088 35.788	Milemarker 68	Milemarker	Seneca	IDNR	Installed
52.06AB	41 19.623	088 34.874	Milemarker 67	Milemarker	Seneca	IDNR	Installed
52.07AB	41 20.009	088 33.840	Milemarker 66	Milemarker	Seneca	IDNR	Installed
52.08AB	41 20.484	088 32.875	Milemarker 65	Milemarker	Seneca	IDNR	Installed



Sign Schedule - Sorted by Sign Type

Mile Marker Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
52.09AB	in storage in Seneca		Milemarker 70	Milemarker	Seneca	IDNR	Installed
52.10AB	missing	missing	Milemarker 69	Milemarker	Seneca	IDNR	Installed
53.01AB	41 20.305	088 45.624	Milemarker 77	Milemarker	Marseilles	IDNR	Installed
53.02AB	41 20.083	088 44.550	Milemarker 76	Milemarker	Marseilles	IDNR	Installed
53.04AB	41 19.873	088 43.458	Milemarker 75	Milemarker	Marseilles	IDNR	Installed
53.06AB	41 19.631	088 42.349	Milemarker 74	Milemarker	Marseilles	IDNR	Installed
53.07AB	41 19.395	088 41.236	Milemarker 73	Milemarker	Marseilles	IDNR	Installed
53.08AB	41 19.174	088 40.152	Milemarker 72	Milemarker	Marseilles	IDNR	Installed
54.03AB	41 19.655	088 54.349	Milemarker 85	Milemarker	Ottawa	IDNR	Installed
54.04AB	41 20.345	088 53.378	Milemarker 84	Milemarker	Ottawa	IDNR	Installed
54.05AB	41 20.892	088 52.516	Milemarker 83	Milemarker	Ottawa	IDNR	Installed
54.07AB	41 21.162	088 51.426	Milemarker 82	Milemarker	Ottawa	IDNR	Installed
54.08AB	41 21.151	088 50.045	Milemarker 81	Milemarker	Ottawa	IDNR	Installed
54.10AB	41 20.978	088 48.928	Milemarker 80	Milemarker	Ottawa	IDNR	Installed
54.11AB	41 20.771	088 47.818	Milemarker 79	Milemarker	Ottawa	IDNR	Installed
54.12AB	41 20.546	088 46.713	Milemarker 78	Milemarker	Ottawa	IDNR	Installed
56.01AB	41 20.123	089 03.258	Milemarker 93	Milemarker	Utica	IDNR	Installed
56.02AB	41 20.370	089 02.151	Milemarker 92	Milemarker	Utica	IDNR	Installed
56.03AB	41 20.482	089 01.013	Milemarker 91	Milemarker	Utica	IDNR	Installed
56.04AB	41 20.262	088 59.908	Milemarker 90	Milemarker	Utica	IDNR	Installed
56.05AB	41 20.032	088 58.799	Milemarker 89	Milemarker	Utica	IDNR	Installed
56.06AB	41 19.792	088 57.690	Milemarker 88	Milemarker	Utica	IDNR	Installed
56.07AB	41 19.587	088 56.575	Milemarker 87	Milemarker	Utica	IDNR	Installed
56.08AB	41 19.655	088 55.459	Milemarker 86	Milemarker	Utica	IDNR	Installed
58.01AB	41 19.389	089 06.506	Milemarker 96	Milemarker	LaSalle	IDNR	Installed
58.03AB	41 19.618	089 05.410	Milemarker 95	Milemarker	LaSalle	IDNR	Installed
58.04AB	41 19.744	089 04.274	Milemarker 94	Milemarker	LaSalle	IDNR	Installed
54.17	tollhouse	tollhouse	Orientation sign (tollhouse)	Milemarker	Ottawa	IDNR	Delivered
51.09AB	41 21.345	088 25.415	Downtown Morris	Milemarker	Morris	City of Morris	Installed
51.12	41 21.379	088 25.418	Canalport credit panels	Milemarker	Morris	City of Morris	Installed



Sign Schedule - Sorted by Sign Type

Silhouette Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
38.01AB	41 38.384	088 04.104	Pierre Moreau	Silhouette	Romeoville	Will Co FP	Installed
38.02AB	41 38.386	088 04.100	Chachagwessiou	Silhouette	Romeoville	Will Co FP	Installed
43.01AB	41 31.537	088 05.283	Lester Frank Ward/ Rare Finds	Silhouette	Joliet	Joliet Park Dist	Installed
43.02AB	41 31.321	088 05.307	Natividad Guterrez	Silhouette	Joliet	Joliet Park Dist	Installed
43.03AB	41 31.538	088 05.288	Peter Baum John Stender/ Joliet Stone	Silhouette	Joliet	Joliet Park Dist	Installed
43.05AB	41 31.533	088 05.282	Francis Hoffman/ Historic Buildings	Silhouette	Joliet	Joliet Park Dist	Installed
43.08AB	41 31.536	088 05.283	Peter Baum John Stender/ credit panel	Silhouette	Joliet	Joliet Park Dist	Installed
43.10AB	41 31.536	088 05.282	The Demmonds	Silhouette	Joliet	Joliet Park Dist	Installed
43.11AB	41 31.607	088 04.900	The Marx Brothers (north)	Silhouette	Joliet	Joliet Park Dist	Installed
43.13AB	41 31.307	088 06.057	Patrick Smith	Silhouette	Joliet	Joliet Park Dist	Installed
43.14AB	41 30.642	088 06.314	Walter Olin	Silhouette	Joliet	Joliet Park Dist	Installed
43.15AB	41 31.942	088 04.991	Mary Sentina	Silhouette	Joliet	Joliet Park Dist	Installed
43.16AB	41 31.488	088 05.263	James Bruce	Silhouette	Joliet	Joliet Park Dist	Installed
43.17AB	41 32.181	088 04.817	George Kiser	Silhouette	Joliet	Joliet Park Dist	Installed
51.02AB	41 21.362	088 25.505	John Sullivan	Silhouette	Morris	City of Morris	Installed
51.10AB	41 21.379	088 25.420	Elise Armstrong 1	Silhouette	Morris	City of Morris	Installed
51.31 AB	41 21.379	088 25.425	Elsie Armstrong 2	Silhouette	Morris	City of Morris	Installed
53.09AB	41 19.747	088 42.498	Sgt. James Sandford	Silhouette	Marseilles	Village of Marseilles	Installed
56.09AB	41 20.427	089 00.592	James Clark	Silhouette	Utica	LaSalle Co Hist Society	Installed
58.07AB	41 19.545	089 05.686	Shabbona	Silhouette	LaSalle	City of LaSalle	Installed
58.08AB	41 19.545	089 05.688	Father John O'Reilly	Silhouette	LaSalle	City of LaSalle	Installed
58.09AB	41 19.573	089 05.686	John Means	Silhouette	LaSalle	City of LaSalle	Installed
58.10AB	41 19.583	089 05.661	John W. Connett	Silhouette	LaSalle	City of LaSalle	Installed
58.11AB	41 19.510	89 05.688	Grenville M. Dodge	Silhouette	LaSalle	City of LaSalle	Installed
58.12AB	41 19.545	089 05.686	Abraham Lincoln	Silhouette	LaSalle	City of LaSalle	Installed
58.13AB	41 19.543	089 05.696	Gurdon S. Hubbard	Silhouette	LaSalle	City of LaSalle	Installed
58.14AB	41 19.556	089 05.700	Clara and Lydia Matteson	Silhouette	LaSalle	IDNR	Installed
58.15AB	41 19.568	089 05.705	Wild Bill Hickok	Silhouette	LaSalle	IDNR	Installed
58.18AB	41 19.675	089 05.646	Rene-Robert Cavelier de La Salle	Silhouette	LaSalle	City of LaSalle	Installed



Straight Cor-Ten Signs

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
43.06	41 31.537	088 05.283	Joliet Buildings	Straight Cor-Ten	Joliet	Joliet Park Dist	Installed
43.09	41 31.536	088 05.282	Quarries are us	Straight Cor-Ten	Joliet	Joliet Park Dist	Installed
43.24AB	41 31.478	088 05.268	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.23AB	41 30.658	088 06.309	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.22AB	41 31.617	088 04.911	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.26	41 32.182	088 04.820	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
53.10	missing	missing	Welcome to the I&M Canal Passage (Marseilles)	Straight Cor-Ten	Marseilles	IDNR	Delivered
51.03AB	41 21.383	088 25.509	Welcome to Canalport/ credit	Straight Cor-Ten	Morris	City of Morris	Installed



11.08.13
Phase 3.0
1201-20

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Sign Schedule - Sorted by Location Coordinates

Sorted from East to West

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
38.01AB	41 38.384	088 04.104	Pierre Moreau	Silhouette	Romeoville	Will Co FP	Installed
38.02AB	41 38.386	088 04.100	Chachagwessiou	Silhouette	Romeoville	Will Co FP	Installed
43.01AB	41 31.537	088 05.283	Lester Frank Ward/ Rare Finds	Silhouette	Joliet	Joliet Park Dist	Installed
43.02AB	41 31.321	088 05.307	Natividad Guterrez	Silhouette	Joliet	Joliet Park Dist	Installed
43.03AB	41 31.538	088 05.288	Peter Baum John Stender/ Joliet Stone	Silhouette	Joliet	Joliet Park Dist	Installed
43.05AB	41 31.533	088 05.282	Francis Hoffman/ Historic Buildings	Silhouette	Joliet	Joliet Park Dist	Installed
43.06	41 31.537	088 05.283	Joliet Buildings	Straight Cor-Ten	Joliet	Joliet Park Dist	Installed
43.07	41 31.536	088 05.279	Life in the Rocks	Cantilever	Joliet	Joliet Park District	Installed
43.08AB	41 31.536	088 05.283	Peter Baum John Stender/ credit panel	Silhouette	Joliet	Joliet Park Dist	Installed
43.09	41 31.536	088 05.282	Quarries are us	Straight Cor-Ten	Joliet	Joliet Park Dist	Installed
43.10AB	41 31.536	088 05.282	The Demmonds	Silhouette	Joliet	Joliet Park Dist	Installed
43.11AB	41 31.607	088 04.900	The Marx Brothers (north)	Silhouette	Joliet	Joliet Park Dist	Installed
43.13AB	41 31.307	088 06.057	Patrick Smith	Silhouette	Joliet	Joliet Park Dist	Installed
43.14AB	41 30.642	088 06.314	Walter Olin	Silhouette	Joliet	Joliet Park Dist	Installed
43.15AB	41 31.942	088 04.991	Mary Sentina	Silhouette	Joliet	Joliet Park Dist	Installed
43.16AB	41 31.488	088 05.263	James Bruce	Silhouette	Joliet	Joliet Park Dist	Installed
43.17AB	41 32.181	088 04.817	George Kiser	Silhouette	Joliet	Joliet Park Dist	Installed
43.18	41 29.183	088 09.902	Milemarker 38	Milemarker	Joliet	IDNR	Installed
43.19	41 29.562	088 08.870	Milemarker 37	Milemarker	Joliet	IDNR	Installed
43.20	41 29.836	088 07.766	Milemarker 36	Milemarker	Joliet	IDNR	Installed
43.21	41 30.147	088 06.686	Milemarker 35	Milemarker	Joliet	IDNR	Installed
43.22AB	41 31.617	088 04.911	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.23AB	41 30.658	088 06.309	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.24AB	41 31.478	088 05.268	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.25	41 30.246	088 06.341	Brandon Road Kiosk	Kiosk	Joliet	IDNR	Installed
43.26	41 32.182	088 04.820	Joliet Bicycle Map	Straight Cor-Ten	Joliet	City of Joliet	Installed
43.28	41 32.238	088 04.785	Milemarker 32	Milemarker	Joliet	IDNR	Installed
48.01	41 25.168	088 13.691	Bridge Street Kiosk	Kiosk	Channahon	IDNR	Installed
48.02	41 28.413	088 11.230	Channahon Park District kiosk	Kiosk	Channahon	IDNR	Installed
48.03	41 25.425	088 13.670	Channahon State Park Kiosk (parking lot)	Kiosk	Channahon	IDNR	Installed
48.04	41 25.382	088 13.727	Channahon State Park Kiosk (trail)	Kiosk	Channahon	IDNR	Installed
48.05	41 29.173	088 09.924	Rock Run Kiosk	Kiosk	Channahon	IDNR	Installed
48.06	41 28.682	088 10.833	Milemarker 39	Milemarker	Channahon	IDNR	Installed
48.07	41 28.144	088 11.743	Milemarker 40	Milemarker	Channahon	IDNR	Installed
48.08	41 27.615	088 12.646	Milemarker 41	Milemarker	Channahon	IDNR	Installed
48.09	41 27.052	088 13.495	Milemarker 42	Milemarker	Channahon	IDNR	Installed



Sign Schedule - Sorted by Location Coordinates

Sorted from East to West

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
48.10	41 26.217	088 13.741	Milemarker 43	Milemarker	Channahon	IDNR	Installed
48.11	41 25.420	088 13.731	Milemarker 44	Milemaker	Channahon	IDNR	Installed
48.12	41 23.813	088 13.858	Milemarker 46	Milemarker	Channahon	IDNR	Installed
48.13	41 23.063	088 14.325	Milemarker 47	Milemarker	Channahon	IDNR	Installed
48.14	41 24.277	088 18.322	Milemarker 51	Milemarker	Channahon	IDNR	Installed
48.15	41 23.892	088 19.349	Milemarker 52	Milemarker	Channahon	IDNR	Installed
48.16	41 24.610	088 13.459	Milemarker 45	Milemarker	Channahon	IDNR	Installed
48.17	41 25.264	088 13.716	Meeting of the Waters	Cantilever	Channahon	Will Co FP	Installed
48.18	41 23.097	088 14.399	Orientation sign (McKinley woods	Cantilever	Channahon	Will Co FP	Installed
51.01AB	41 21.412	088 26.353	Gebhard Woods Kiosk/ credit panel	Kiosk	Morris	IDNR	Installed
51.02AB	41 21.362	088 25.505	John Sullivan	Silhouette	Morris	City of Morris	Installed
51.03AB	41 21.383	088 25.509	Welcome to Canalport/ credit	Straight Cor-Ten	Morris	City of Morris	Installed
51.04	41 23.746	088 19.922	Aux Sable Village	Cantilever	Morris	IDNR	Installed
51.05	41 23.754	088 19.892	Lock 8 and Aux Aqueduct	Cantilever	Morris	IDNR	Installed
51.06	41 24.126	088 16.816	Dresden Barn	Cantilever	Morris	IDNR	Installed
51.07	41 21.370	088 26.320	Yesterdays Canal	Cantilever	Morris	IDNR	Installed
51.08	41 21.377	088 25.424	Welcome to Morris a Historic I&M Canal Town	Cantilever	Morris	City of Morris	Installed
51.09AB	41 21.345	088 25.415	Downtown Morris	Milemarker	Morris	City of Morris	Installed
51.10AB	41 21.379	088 25.420	Elise Armstrong 1	Silhouette	Morris	City of Morris	Installed
51.11AB	41 24.281	088 17.395	Dresden Kiosk/ credit	Kiosk	Morris	IDNR	Installed
51.12	41 21.379	088 25.418	Canalport credit panels	Milemarker	Morris	City of Morris	Installed
51.13	41 21.379	088 25.419	Welcome to Morris a Historic I&M Canal Town	Cantilever	Morris	City of Morris	Installed
51.14	41 20.290	088 30.588	Milemarker 63	Milemarker	Morris	IDNR	Installed
51.15	41 20.476	088 29.506	Milemarker 62	Milemarker	Morris	IDNR	Installed
51.16	41 20.883	088 28.515	Milemarker 61	Milemarker	Morris	IDNR	Installed
51.17	41 21.134	088 27.414	Milemarker 60	Milemarker	Morris	IDNR	Installed
51.18	41 21.370	088 26.320	Milemarker 59	Milemarker	Morris	IDNR	Installed
51.19	41 21.370	088 25.236	Straton Kiosk	Kiosk	Morris	IDNR	Installed
51.20	41 21.339	088 25.172	Milemarker 58	Milemarker	Morris	IDNR	Installed
51.21	41 21.930	088 24.208	Milemarker 57	Milemarker	Morris	IDNR	Installed
51.22	41 22.403	088 23.486	Milemarker 56	Milemarker	Morris	IDNR	Installed
51.23	41 22.771	088 22.439	Milemarker 55	Milemarker	Morris	IDNR	Installed
51.24	41 23.164	088 21.420	Milemarker 54	Milemarker	Morris	IDNR	Installed
51.25	41 23.604	088 20.441	Milemarker 53	Milemarker	Morris	IDNR	Installed
51.26	41 23.727	088 19.954	Aux Sable Kiosk	Kiosk	Morris	IDNR	Installed
51.27	41 24.233	088 17.218	Milemarker 50	Milemarker	Morris	IDNR	Installed



Sign Schedule - Sorted by Location Coordinates

Sorted from East to West

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
51.28	41 24.056	088 16.105	Milemarker 49	Milemarker	Morris	IDNR	Installed
51.29	41 23.532	088 15.214	Milemarker 48	Milemarker	Morris	IDNR	Installed
51.30	41 21.930	088 24.281	Cemetery Road Kiosk	Kiosk	Morris	IDNR	Installed
51.31 AB	41 21.379	088 25.425	Elsie Armstrong 2	Silhouette	Morris	City of Morris	Installed
52.01AB	41 18.945	088 39.038	Milemarker 71	Milemarker	Seneca	IDNR	Installed
52.02AB	41 20.406	088 31.733	Milemarker 64	Milemarker	Seneca	IDNR	Installed
52.03	41 18.900	088 36.676	Old Grain Elevator	Cantilever	Seneca	IDNR	Installed
52.04A	41 18.898	088 36.612	Seneca kiosk/ credit panel	Kiosk	Seneca	IDNR	Installed
52.05AB	41 19.099	088 35.788	Milemarker 68	Milemarker	Seneca	IDNR	Installed
52.06AB	41 19.623	088 34.874	Milemarker 67	Milemarker	Seneca	IDNR	Installed
52.07AB	41 20.009	088 33.840	Milemarker 66	Milemarker	Seneca	IDNR	Installed
52.08AB	41 20.484	088 32.875	Milemarker 65	Milemarker	Seneca	IDNR	Installed
52.09AB	in storage in Seneca		Milemarker 70	Milemarker	Seneca	IDNR	Installed
52.10AB	missing	missing	Milemarker 69	Milemarker	Seneca	IDNR	Installed
53.01AB	41 20.305	088 45.624	Milemarker 77	Milemarker	Marseilles	IDNR	Installed
53.02AB	41 20.083	088 44.550	Milemarker 76	Milemarker	Marseilles	IDNR	Installed
53.04AB	41 19.873	088 43.458	Milemarker 75	Milemarker	Marseilles	IDNR	Installed
53.05	41 19.701	088 42.720	Old Swing Bridge	Cantilever	Marseilles	IDNR	Installed
53.06AB	41 19.631	088 42.349	Milemarker 74	Milemarker	Marseilles	IDNR	Installed
53.07AB	41 19.395	088 41.236	Milemarker 73	Milemarker	Marseilles	IDNR	Installed
53.08AB	41 19.174	088 40.152	Milemarker 72	Milemarker	Marseilles	IDNR	Installed
53.09AB	41 19.747	088 42.498	Sgt. James Sandford	Silhouette	Marseilles	Village of Marseilles	Installed
53.10	missing	missing	Welcome to the I&M Canal Passage (Marseilles)	Straight Cor-Ten	Marseilles	IDNR	Delivered
54.01	41 19.782	088 54.876	Stagecoach Canal and Rail Travel	Cantilever	Ottawa	IDNR	Installed
54.02	41 19.785	088 54.851	Buffalo Rock kiosk	Kiosk	Ottawa	IDNR	Installed
54.03AB	41 19.655	088 54.349	Milemarker 85	Milemarker	Ottawa	IDNR	Installed
54.04AB	41 20.345	088 53.378	Milemarker 84	Milemarker	Ottawa	IDNR	Installed
54.05AB	41 20.892	088 52.516	Milemarker 83	Milemarker	Ottawa	IDNR	Installed
54.06	41 21.030	088 51.964	Ottawa Kiosk	Kiosk	Ottawa	IDNR	Installed
54.07AB	41 21.162	088 51.426	Milemarker 82	Milemarker	Ottawa	IDNR	Installed
54.08AB	41 21.151	088 50.045	Milemarker 81	Milemarker	Ottawa	IDNR	Installed
54.09AB	41 21.151	088 49.763	Ottawa Kiosk (aqueduct)	Kiosk	Ottawa	IDNR	Installed
54.10AB	41 20.978	088 48.928	Milemarker 80	Milemarker	Ottawa	IDNR	Installed
54.11AB	41 20.771	088 47.818	Milemarker 79	Milemarker	Ottawa	IDNR	Installed
54.12AB	41 20.546	088 46.713	Milemarker 78	Milemarker	Ottawa	IDNR	Installed
54.17	tollhouse	tollhouse	Orientation sign (tollhouse)	Milemarker	Ottawa	IDNR	Delivered



Sign Schedule - Sorted by Location Coordinates

Sorted from East to West

Sign Number	Latitude	Longitude	Panel Title	Sign Type	City	Sign Owner	Installed / Delivered
56.01AB	41 20.123	089 03.258	Milemarker 93	Milemarker	Utica	IDNR	Installed
56.02AB	41 20.370	089 02.151	Milemarker 92	Milemarker	Utica	IDNR	Installed
56.03AB	41 20.482	089 01.013	Milemarker 91	Milemarker	Utica	IDNR	Installed
56.04	41 20.449	089 00.745	Utica Kiosk	Kiosk	Utica	IDNR	Installed
56.04AB	41 20.262	088 59.908	Milemarker 90	Milemarker	Utica	IDNR	Installed
56.05AB	41 20.032	088 58.799	Milemarker 89	Milemarker	Utica	IDNR	Installed
56.06AB	41 19.792	088 57.690	Milemarker 88	Milemarker	Utica	IDNR	Installed
56.07AB	41 19.587	088 56.575	Milemarker 87	Milemarker	Utica	IDNR	Installed
56.08AB	41 19.655	088 55.459	Milemarker 86	Milemarker	Utica	IDNR	Installed
56.09AB	41 20.427	089 00.592	James Clark	Silhouette	Utica	LaSalle Co Hist Society	Installed
58.01AB	41 19.389	089 06.506	Milemarker 96	Milemarker	LaSalle	IDNR	Installed
58.02AB	41 19.586	089 05.615	Lock 14 Kiosk/ credit panel	Kiosk	LaSalle	IDNR	Installed
58.03AB	41 19.618	089 05.410	Milemarker 95	Milemarker	LaSalle	IDNR	Installed
58.04AB	41 19.744	089 04.274	Milemarker 94	Milemarker	LaSalle	IDNR	Installed
58.05	41 20.101	089 03.338	Transportation at Split Rock	Cantilever	LaSalle	IDNR	Installed
58.06	41 20.100	089 03.339	Split Rock Geology	Cantilever	LaSalle	IDNR	Installed
58.07AB	41 19.545	089 05.686	Shabbona	Silhouette	LaSalle	City of LaSalle	Installed
58.08AB	41 19.545	089 05.688	Father John O'Reilly	Silhouette	LaSalle	City of LaSalle	Installed
58.09AB	41 19.573	089 05.686	John Means	Silhouette	LaSalle	City of LaSalle	Installed
58.10AB	41 19.583	089 05.661	John W. Connett	Silhouette	LaSalle	City of LaSalle	Installed
58.11AB	41 19.510	89 05.688	Grenville M. Dodge	Silhouette	LaSalle	City of LaSalle	Installed
58.12AB	41 19.545	089 05.686	Abraham Lincoln	Silhouette	LaSalle	City of LaSalle	Installed
58.13AB	41 19.543	089 05.696	Gurdon S. Hubbard	Silhouette	LaSalle	City of LaSalle	Installed
58.14AB	41 19.556	089 05.700	Clara and Lydia Matteson	Silhouette	LaSalle	IDNR	Installed
58.15AB	41 19.568	089 05.705	Wild Bill Hickok	Silhouette	LaSalle	IDNR	Installed
58.16	41 19.582	089 05.674	North Meets South	Cantilever	LaSalle	IDNR	Installed
58.17	41 19.579	089 05.690	Rough and Ready	Cantilever	LaSalle	IDNR	Installed
58.18AB	41 19.675	089 05.646	Rene-Robert Cavalier de La Salle	Silhouette	LaSalle	City of LaSalle	Installed
58.19	41 20.100	089 03.338	Split Rock Kiosk	Kiosk	LaSalle	IDNR	Installed
58.20	41 19.651	89 5.037	Little Vermillion Aqueduct	Cantilever	LaSalle	IDNR	Installed



Silhouette / Mile Markers

Sign Type Drawing

Mile Markers | Silhouette Interpretative Panels

TYPICAL #1

- ① **PROPOSED Sign Panel**
The graphic panel will be 1/2" thick phenolic resin graphic panel

Colors are CMYK 4 color process to match:
PMS 123C
PMS 655 C
White
Gradients of PMS 655C and PMS 646C

The panel will have a minimum 10 year warranty for covering product failures.

The panel will be mounted using non-rusting stainless steel attachments. Sign panels will be installed with tamper resistant nuts. Four (4) machined holes in back of panel to accept threaded inserts for custom high pressure laminates materials to be McMaster-Carr PN. 91732A212 or approved equal.

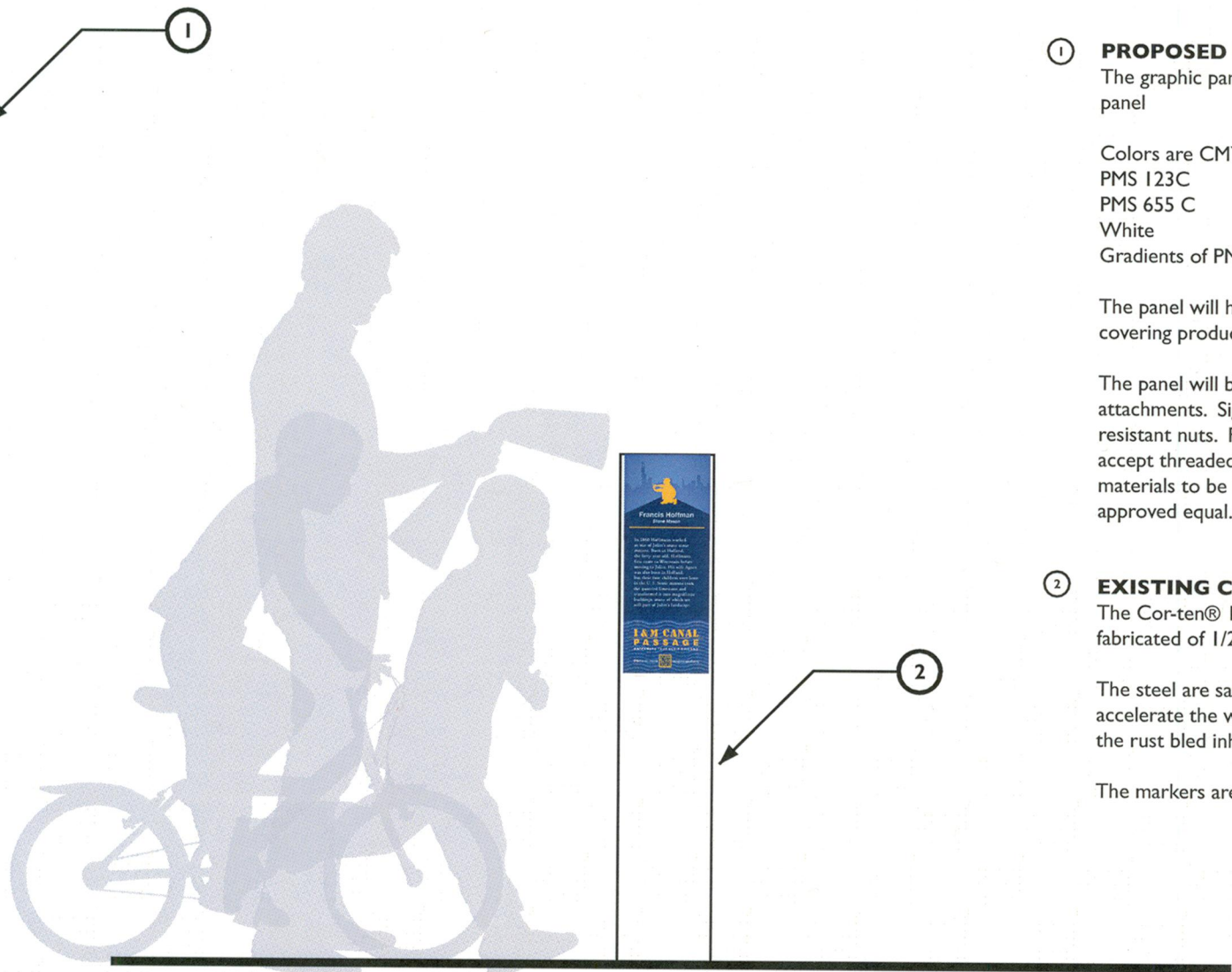
- ② **EXISTING Cor-ten Sign Post**
The Cor-ten® 10"w x42"h V shaped markers are fabricated of 1/2" Cor-ten® steel.

The steel are sandblasted and pre-oxidized with acid to accelerate the weathering of the steel in order to reduce the rust bled inherent with weathering steel.

The markers are pre-drilled to accommodate signage.



Panel Elevation
Scale: 3"=1'-0"



Elevation
Scale: 3/4"=1'-0"



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LaSalle, Illinois 61301

Grundy County
1320 Union Street
Morris, IL 60450

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11.08.13
Phase 3.0
1201-20

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Cantilever
Interpretive Panels

TYPICAL #2

- ① **PROPOSED Sign Panel**
The graphic panel will be 1/4" thick phenolic resin graphic panel

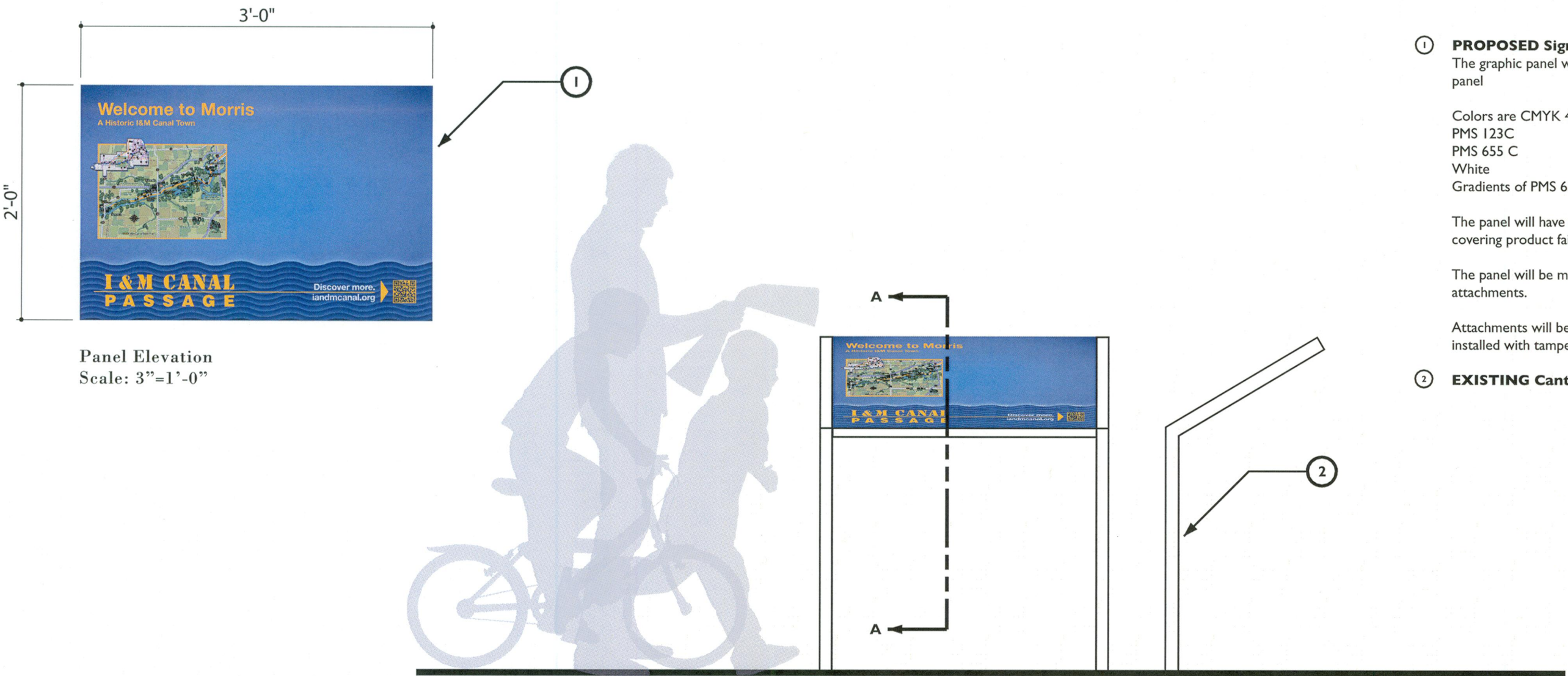
Colors are CMYK 4 color process to match:
PMS 123C
PMS 655 C
White
Gradients of PMS 655C and PMS 646C

The panel will have a minimum 10 year warranty for covering product failures.

The panel will be mounted using non-rusting stainless steel attachments.

Attachments will be non-exposed. Sign panels will be installed with tamper resistant nuts.

- ② **EXISTING Cantilever Sign Post**



Panel Elevation
Scale: 3"=1'-0"

Elevation
Scale: 3/4"=1'-0"

Section AA
Scale: 3/4"=1'-0"



Interpretive Panels - Kiosk panels

Sign Type Drawing

Kiosk Interpretive Panels

TYPICAL #3

① **PROPOSED Sign Panel**
The graphic panel will be 1/2" thick phenolic resin graphic panel

Colors are CMYK 4 color process to match:
PMS 123C
PMS 655 C
White
Gradients of PMS 655C and PMS 646C

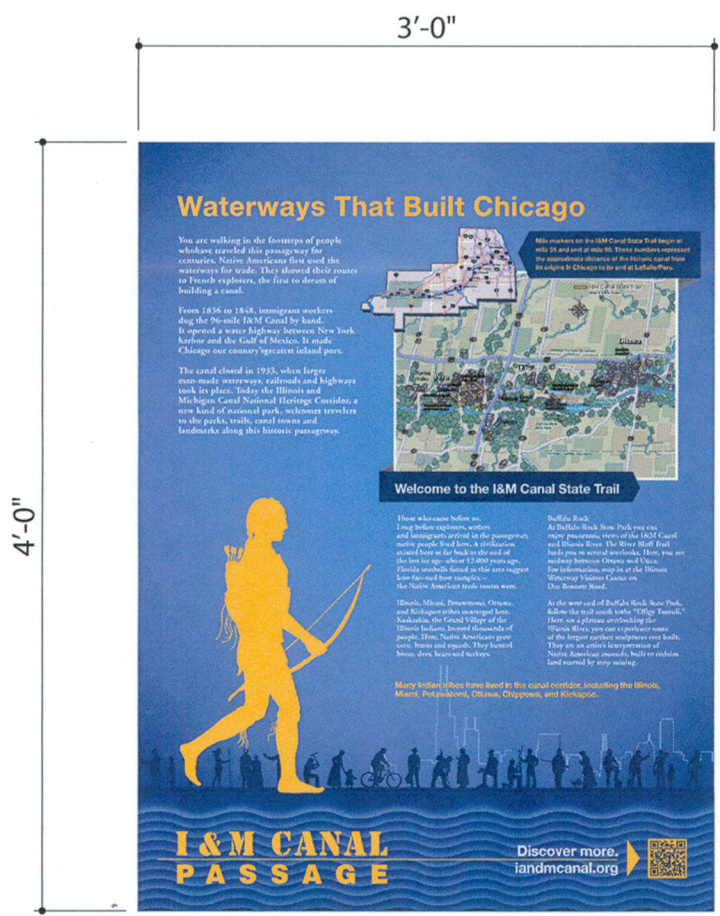
The panel will have a minimum 10 year warranty for covering product failures.

The panel will be mounted using non-rusting stainless steel attachments.

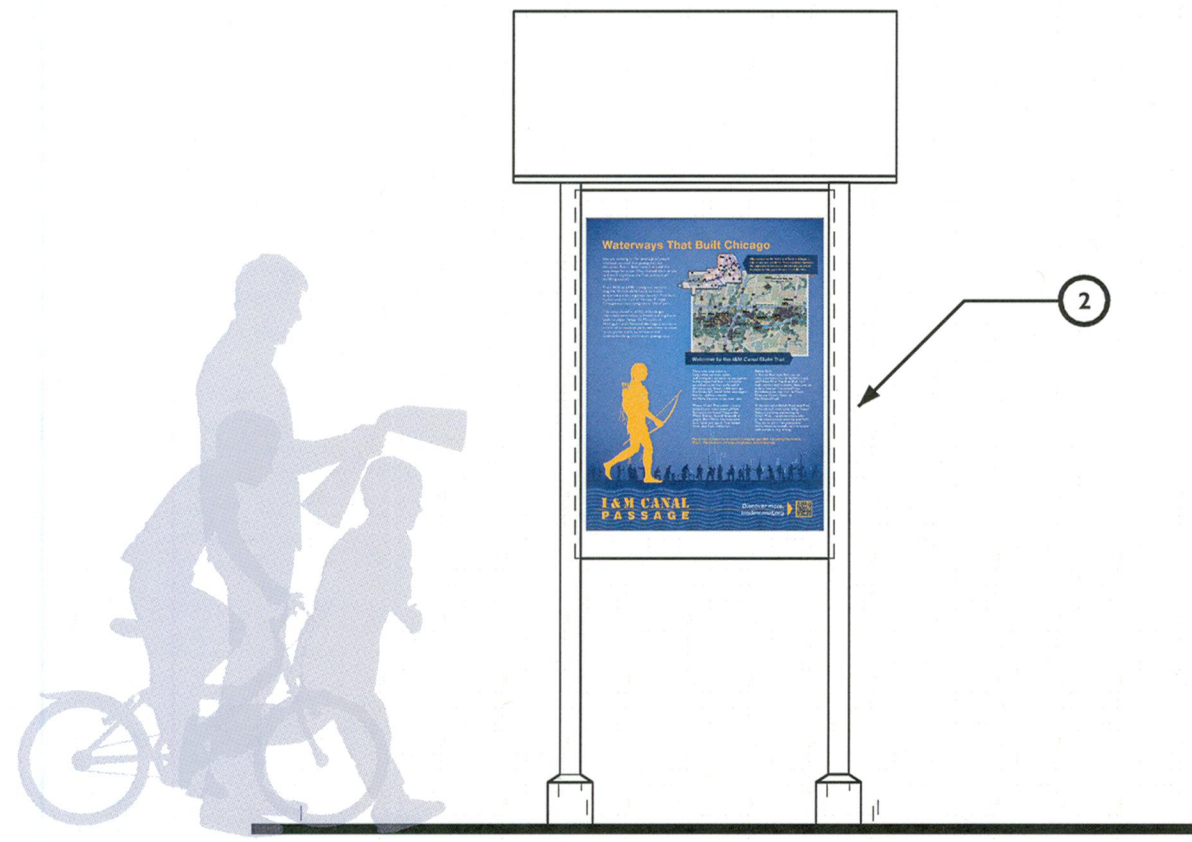
Sign panels will be installed with tamper resistant nuts. Through holes shall be drilled oversized for bolts. Bolts should be slightly countersunk into the face side of a laminate-clad substrate. This will allow for expansion and contraction. Location of each through hole to be coordinated with existing installed signs.

② **EXISTING Sign Post**

DNR Kiosk



Elevation
Scale: 1"=1'-0"



Elevation
Scale: 1/2"=1'-0"



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Interpretive Panels – Straight Cor-Ten

Sign Type Drawing – sizes vary

Straight Cor-Ten Interpretive Panels

TYPICAL #4

- ① **PROPOSED Sign Panel**
The graphic panel will be 1/2" thick phenolic resin graphic panel

Colors are CMYK 4 color process to match:
PMS 123C
PMS 655 C
White
Gradients of PMS 655C and PMS 646C

The panel will have a minimum 10 year warranty for covering product failures.

The panel will be mounted using non-rusting stainless steel attachments.

Sign panels will be installed with tamper resistant nuts. Through holes shall be drilled oversized for bolts. Bolts should be slightly countersunk into the face side of a laminate-clad substrate. This will allow for expansion and contraction. Location of each through hole to be coordinated with existing installed signs.
- ② **EXISTING Sign Post**

Straight Cor-Ten Panel



Elevation
Scale: 1"=1'-0"

Elevation
Scale: 3/4"=1'-0"



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Location - Trailwide

You are at mile
32
Gaylord Building
is 3.9 miles ahead

Tough Breaks.
The steel mills provided work for immigrants, but many were injured or killed on the job. A 1907 study was called *Making Steel and Killing Men*.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.28a

You are at mile
32
To I&M Canal State Trail
is 4.3 miles ahead

Convict Labor.
Convicts built the State Penitentiary in Joliet from local limestone. W.W. Boyington, architect of Chicago's historic Water Tower, designed the prison.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.28b

You are at mile
35
Rock Run Access
is 3.1 miles ahead

Birth of a city.
Joliet owes its existence to the I&M Canal. But it boomed as a result of the railroads that arrived in 1852, making it the largest canal town after Chicago.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.21a

You are at mile
35
Downtown Joliet
is 2.5 miles ahead

Over the river.
The canal crossed the Des Plaines River near here. When the Illinois Waterway opened in 1933, the Joliet portion of the canal including lock 5 was submerged.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.21b

You are at mile
36
Rock Run Access
is 2.1 miles ahead

City of stone and steel.
Joliet was rich in limestone and quarries surrounded the town in the mid-1800s. It later became well-known for its iron and steel works.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.20a

You are at mile
36
Brandon Road Access
is 1.3 miles ahead

Warm enough for you?
400-million years ago, a tropical inland ocean covered the region. Local limestone contains the fossilized remains of marine animals from that ocean.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.20b

You are at mile
37
Rock Run Access
is 1.1 miles ahead

Worldly connections.
When the I&M Canal opened in 1848, a Joliet newspaper called it "a line of improvement that connects us with the broad [Atlantic] ocean and the world..."

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.19a

You are at mile
37
Brandon Road Access
is 2.3 miles ahead

The great passageway.
The I&M Canal was built on an ancient passageway. You are walking in the footsteps of Native Americans, French fur traders, canal workers, and settlers.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.19b

You are at mile
38
Locks 6 & 7
is 6.1 miles ahead

A big connection.
When it opened in 1848, the I&M Canal connected the Great Lakes and the Mississippi River, creating a water highway between New York and the Gulf of Mexico.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.18a

You are at mile
38
Brandon Road Access
is 3.3 miles ahead

What a deal!
The I&M Canal cost only \$6.4 million to build. It was the last of the great American canals, built at a time when waterways were the nation's major highways.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 43.18b

You are at mile
39
Locks 6 & 7
is 5.1 miles ahead

Water or rail?
Completed in 1848, the canal soon had to compete with railroads. The canal was better for shipping bulk items such as limestone, but railroads were faster.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 48.06a

You are at mile
39
Rock Run Access
is 1 mile ahead

The melting pot canal.
Although many ethnic groups helped to dig the canal - Norwegians, French Canadians and Germans - the Irish played the lead role in this backbreaking work.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 48.06b

You are at mile
40
Locks 6 & 7
is 4.1 miles ahead

Waterway to roadway.
In Chicago, 8 miles of the Stevenson Expressway (I-55) was built over the I&M Canal. During planning it was called the I&M Canal Expressway.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 48.07a

You are at mile
40
Rock Run Access
is 2 miles ahead

The American dream.
In 1827 the U. S. gave Illinois 285,000 acres to help finance construction of the canal. Land auctions attracted a frenzy of speculation.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO


Discover more  landmcanal.org

Location: 48.07b

You are at mile
41
Locks 6 & 7
is 3.1 miles ahead

Teenage mule drivers.
Mule drivers led the mules that pulled the canal boats. Many were teenage boys who were known to swear, fight and gamble.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org

Location: 48.08a

☐ APPROVED
☐ APPROVED AS NO TED

SIGNATURE _____ DATE _____



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Mile Marker Panels


Location - Trailwide




You are at mile
41
Rock Run Access
is 3 miles ahead

Horn of plenty.
The canal reshaped agriculture in Illinois by providing farmers with a means of getting their goods to market quickly and cheaply.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.08b




You are at mile
42
Locks 6 & 7
is 2.1 miles ahead

Psst, this way.
Native Americans showed French explorers a shortcut through the prairies between the Illinois River and Lake Michigan, which later was used for the I&M Canal.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.09a




You are at mile
42
Rock Run Access
is 4 miles ahead

Don't shoot 'til you see the whites of their eyes.
Wildlife was plentiful along the canal. Canal boat travelers sometimes shot deer and grouse right from the deck.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.09b




You are at mile
43
Locks 6 & 7
is 1.1 miles ahead

Small but efficient.
The I&M Canal was only 6-feet deep, and 60-feet wide. Several widewaters allowed canal boats to pass each other.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.10a




You are at mile
43
Rock Run Access
is 5 miles ahead

Kaboom!
As a result of the canal's opening, Chicago's population and exports nearly quadrupled between 1848 and 1854. Canal towns became centers of employment and trade.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.10b




You are at mile
44
McKinley Woods
is 3.1 miles ahead

Open the gates!
Locktenders operated the locks that acted as "water ladders" to help boats climb the 140-ft. change in water level from one end of the canal to the other.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.11a




You are at mile
44
Rock Run Access
is 6 miles ahead

How locks work.
A canal boat enters a lock. The locktender closes the gates and fills or drains the lock to raise or lower the boat. The boat leaves at a new water level.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 48.11b




You are at mile
45
McKinley Woods
is 2.1 miles ahead

Water, water everywhere.
At Channahon, the Des Plaines and DuPage Rivers merge. Channahon means "meeting of the waters" according to Native Americans.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.16a




You are at mile
45
Locks 6 & 7
is 1 mile ahead

Altered states.
The Illinois River that you see today looks different than it did 100 years ago. It was deepened by 1933 to accommodate modern barge traffic.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 48.16b




You are at mile
46
McKinley Woods
is 1.1 miles ahead

Fertile farmland.
The farmland you see on either side of the canal is some of the world's most fertile. Corn and soybeans are still the region's major agricultural products.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.12a




You are at mile
46
Locks 6 & 7
is 2 miles ahead

People and the land.
This rural landscape is largely engineered by man. Waterways have been built and dammed, fields drained for planting, stone quarried, and coal mined.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.12b




You are at mile
47
McKinley Woods
is 0.1 mile ahead

Water traffic.
Hundreds of canal boats shipped thousands of tons of corn, stone, coal and lumber on the I&M Canal. Over a hundred years ago, this was a noisy, bustling place.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.13a




You are at mile
47
Locks 6 & 7
is 3 miles ahead

People passage.
Until 1854, packet boats carried people on the canal. Often crowded and uncomfortable, the boats took 22-26 hours to travel the full length of the canal.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 48.13b




You are at mile
48
Dresden Mule Barn
is 1.7 miles ahead

Family affair.
Canal boat captains' families were often aboard. Wives served as cooks for the crew while young children were tethered to the deck to keep from falling in.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.29a




You are at mile
48
McKinley Woods
is 0.9 miles ahead

Home sweet home.
Locktenders had to be available day and night to open the gates. Only two of the locktender's houses remain - you can see them at Channahon and Aux Sable.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 51.29b



You are at mile
49
Dresden Mule Barn
is 0.7 miles ahead

Mule condos.
Mule barns once stood along the canal every 10 to 15 miles. Mule teams were attached to the boats by long lines and led on the towpath by mule drivers.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 51.28a

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DATE _____	



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Grundy County
1320 Union Street
Morris, IL 60450

Cardosi Kiper Design Group
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Chicago, Illinois 60608

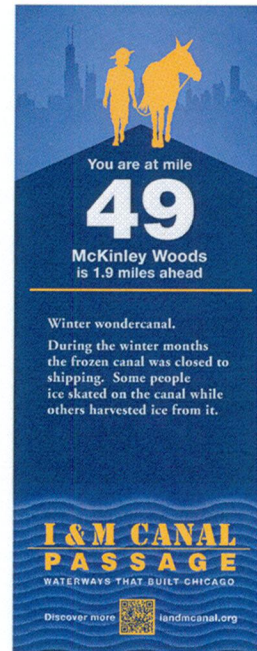
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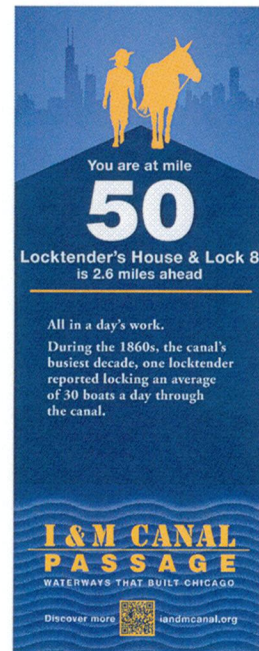
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Mile Marker Panels

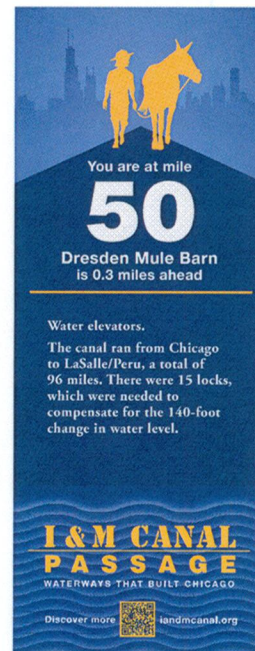
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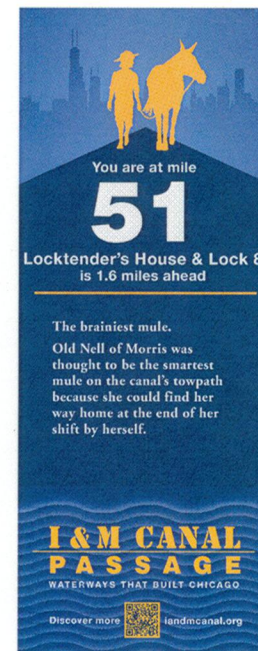
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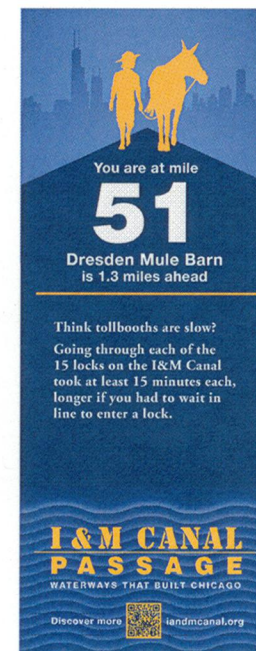
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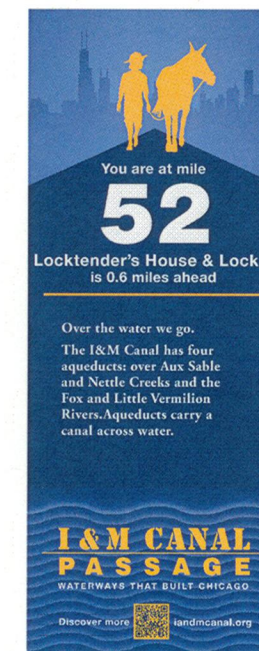
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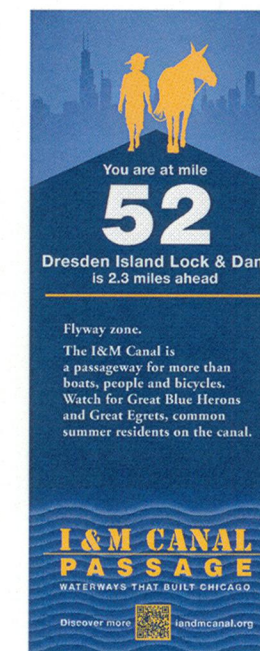
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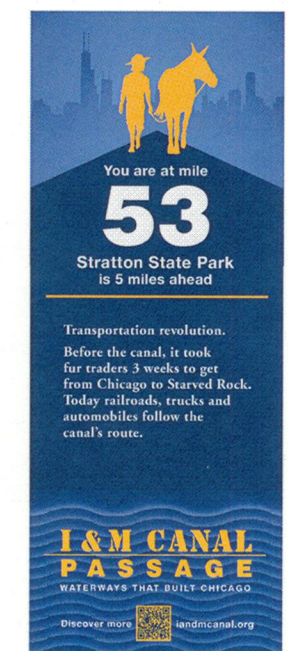
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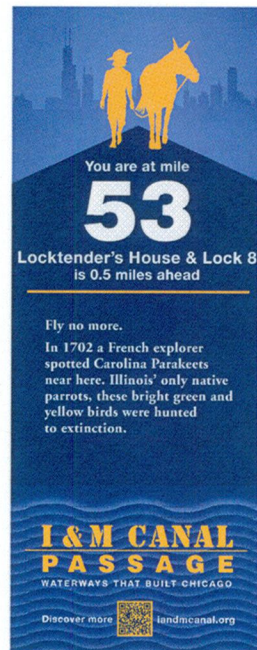
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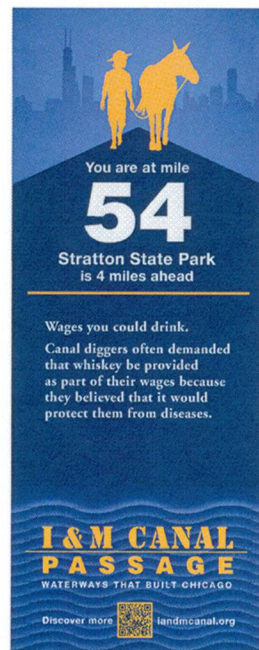
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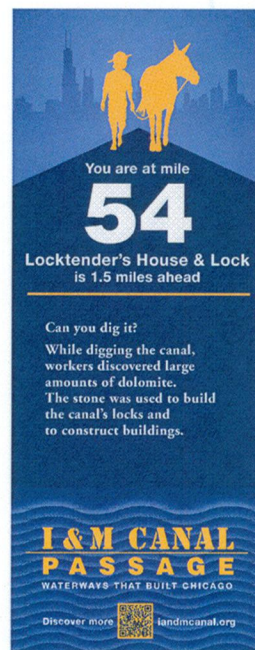
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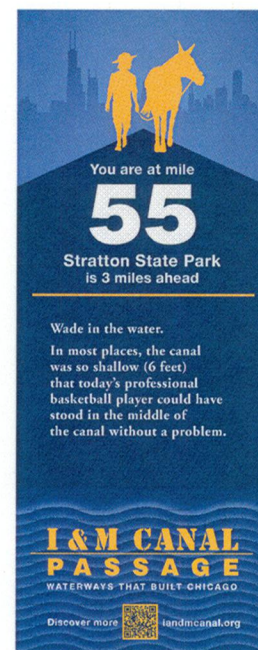
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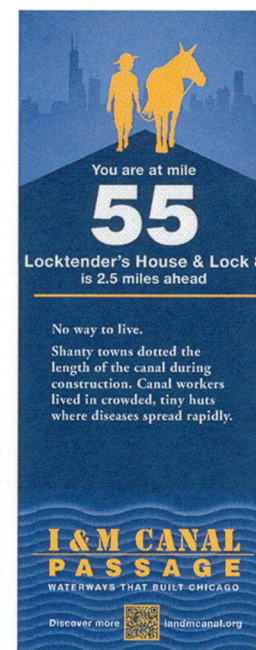
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Location: 51.24b



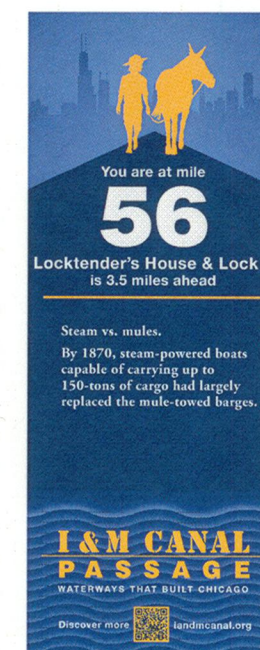
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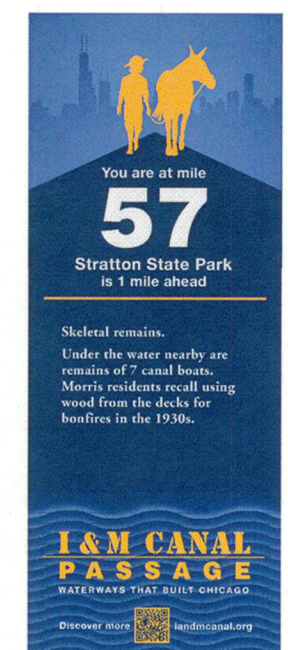
Location: 51.23b



Location: 51.22a



Location: 51.22b



Location: 51.21

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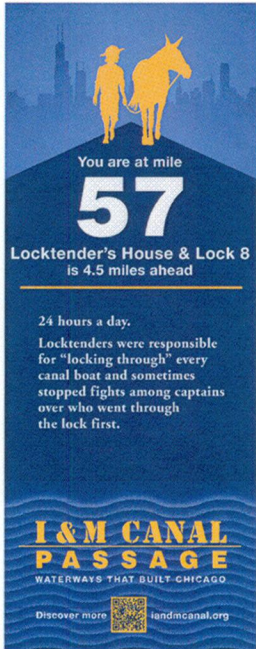
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Sheet
19

Mile Marker Panels

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Location - Trailwide



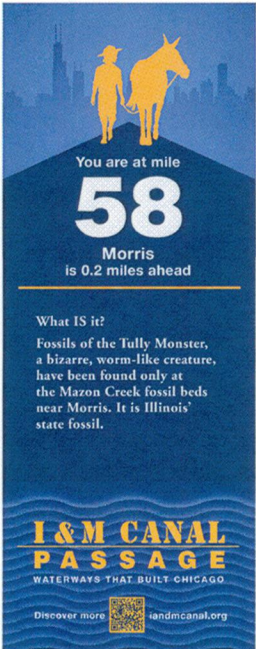
You are at mile
57
Locktender's House & Lock 8
is 4.5 miles ahead

24 hours a day.
Locktenders were responsible for "locking through" every canal boat and sometimes stopped fights among captains over who went through the lock first.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.21b



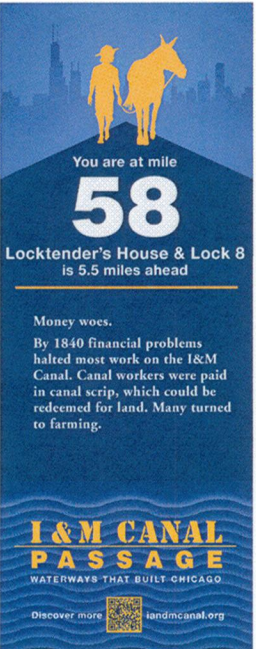
You are at mile
58
Morris
is 0.2 miles ahead

What IS it?
Fossils of the Tully Monster, a bizarre, worm-like creature, have been found only at the Mazon Creek fossil beds near Morris. It is Illinois' state fossil.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.20a



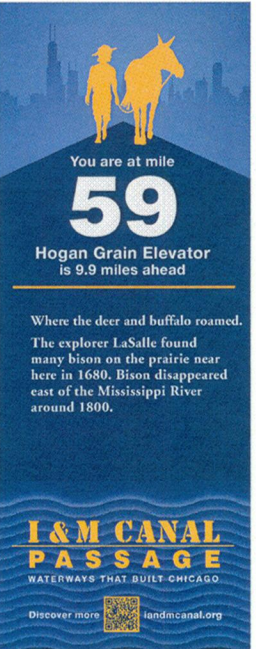
You are at mile
58
Locktender's House & Lock 8
is 5.5 miles ahead

Money woes.
By 1840 financial problems halted most work on the I&M Canal. Canal workers were paid in canal scrip, which could be redeemed for land. Many turned to farming.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.20b



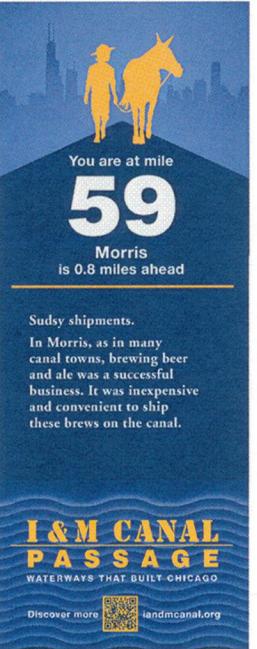
You are at mile
59
Hogan Grain Elevator
is 9.9 miles ahead

Where the deer and buffalo roamed.
The explorer LaSalle found many bison on the prairie near here in 1680. Bison disappeared east of the Mississippi River around 1800.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.18a



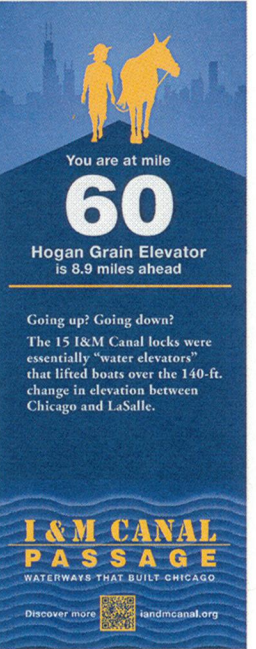
You are at mile
59
Morris
is 0.8 miles ahead

Sudsy shipments.
In Morris, as in many canal towns, brewing beer and ale was a successful business. It was inexpensive and convenient to ship these brews on the canal.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.18b



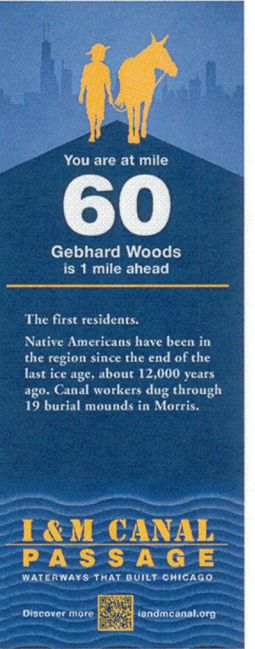
You are at mile
60
Hogan Grain Elevator
is 8.9 miles ahead

Going up? Going down?
The 15 I&M Canal locks were essentially "water elevators" that lifted boats over the 140-ft. change in elevation between Chicago and LaSalle.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.17a



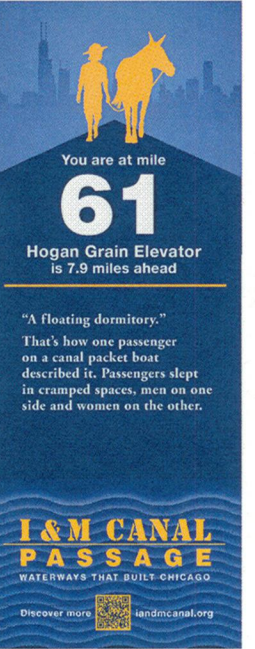
You are at mile
60
Gebhard Woods
is 1 mile ahead

The first residents.
Native Americans have been in the region since the end of the last ice age, about 12,000 years ago. Canal workers dug through 19 burial mounds in Morris.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.17b



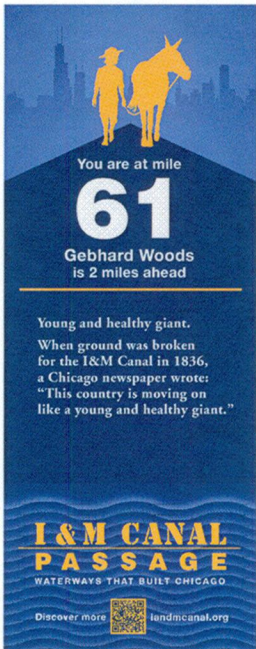
You are at mile
61
Hogan Grain Elevator
is 7.9 miles ahead

"A floating dormitory."
That's how one passenger on a canal packet boat described it. Passengers slept in cramped spaces, men on one side and women on the other.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 51.16a



You are at mile
61
Gebhard Woods
is 2 miles ahead

Young and healthy giant.
When ground was broken for the I&M Canal in 1836, a Chicago newspaper wrote: "This country is moving on like a young and healthy giant."

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 51.16b



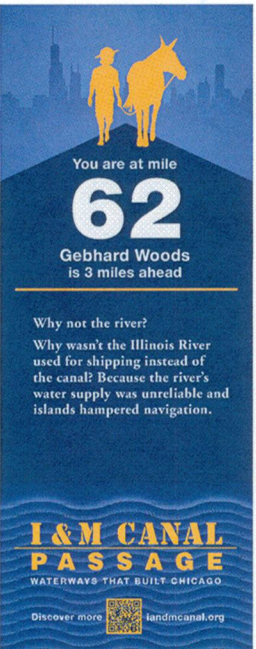
You are at mile
62
Hogan Grain Elevator
is 6.9 miles ahead

Growing trade.
The canal produced profound changes in Illinois. Trade with other states grew and homemade furniture, tools and clothes soon disappeared from local markets.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.15a



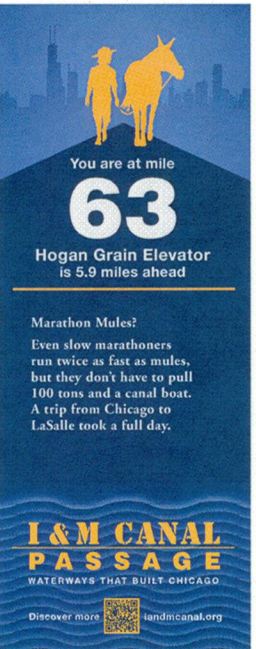
You are at mile
62
Gebhard Woods
is 3 miles ahead

Why not the river?
Why wasn't the Illinois River used for shipping instead of the canal? Because the river's water supply was unreliable and islands hampered navigation.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.15b



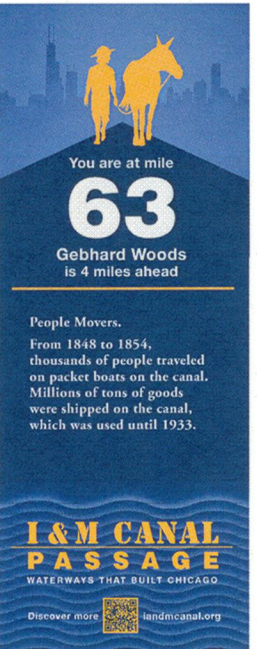
You are at mile
63
Hogan Grain Elevator
is 5.9 miles ahead

Marathon Mules?
Even slow marathoners run twice as fast as mules, but they don't have to pull 100 tons and a canal boat. A trip from Chicago to LaSalle took a full day.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 51.14a



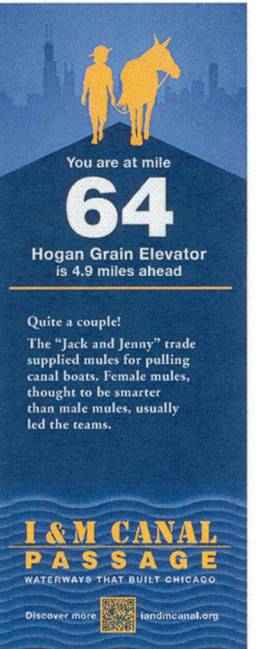
You are at mile
63
Gebhard Woods
is 4 miles ahead

People Movers.
From 1848 to 1854, thousands of people traveled on packet boats on the canal. Millions of tons of goods were shipped on the canal, which was used until 1933.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 51.14b



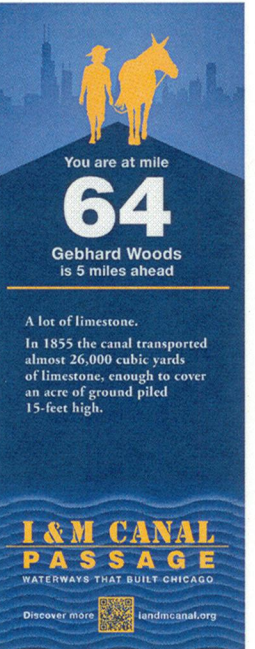
You are at mile
64
Hogan Grain Elevator
is 4.9 miles ahead

Quite a couple!
The "Jack and Jenny" trade supplied mules for pulling canal boats. Female mules, thought to be smarter than male mules, usually led the teams.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 52.02a



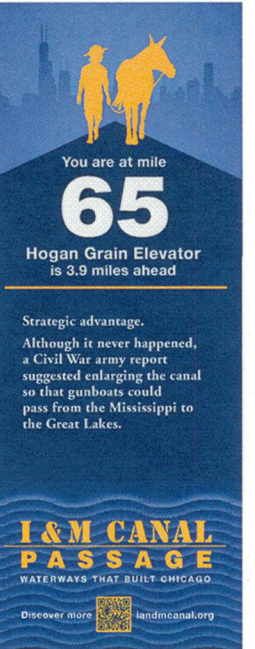
You are at mile
64
Gebhard Woods
is 5 miles ahead

A lot of limestone.
In 1855 the canal transported almost 26,000 cubic yards of limestone, enough to cover an acre of ground piled 15-feet high.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 52.02b



You are at mile
65
Hogan Grain Elevator
is 3.9 miles ahead

Strategic advantage.
Although it never happened, a Civil War army report suggested enlarging the canal so that gunboats could pass from the Mississippi to the Great Lakes.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 52.08a

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Mile Marker Panels


Location - Trailwide




You are at mile
65
Gebhard Woods
is 6 miles ahead

Wagons to water.
Before the canal, wagons hauled farm produce to Chicago. After it opened in 1848, canal towns became trade centers and products poured into Chicago.

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PASSAGE**
WATERWAYS THAT BUILT CHICAGO

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Location: 52.08b




You are at mile
66
Hogan Grain Elevator
is 2.9 miles ahead

What's that smell?
In 1871 the I&M Canal was deepened and the Chicago River's water flow was reversed, allowing Lake Michigan water to flush Chicago sewage downstream.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.07a




You are at mile
66
Gebhard Woods
is 7 miles ahead

Cramped and crowded.
Entire families often lived on canal boats. One boat captain remembered "crawling like a muskrat" in order to get to the cramped quarters below deck.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.07b




You are at mile
67
Hogan Grain Elevator
is 1.9 miles ahead

Where did it go?
It's hard to imagine that this stretch of the canal, now hidden by trees and covered with silt, once shipped hundreds of tons of cargo.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.06a




You are at mile
67
Gebhard Woods
is 8 miles ahead

Transformed landscape.
The I&M Canal transformed the landscape. It replaced the valley's prairies and wetlands with cities, towns, industry, and agriculture.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.06b




You are at mile
68
Hogan Grain Elevator
is 0.9 miles ahead

Bursting with bushels.
The Hogan Grain Elevator is the last surviving 19th-century elevator on the I&M Canal. In 1876, it shipped 900,000 bushels of grain.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.05a




You are at mile
68
Gebhard Woods
is 9 miles ahead

French explorers.
In the 1600s, the French explored the Illinois Valley in search of adventure, furs, and mineral wealth. They called the Illinois River La Divine.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.05b




You are at mile
69
Locks 9 & 10
is 4.6 miles ahead

World's bread basket.
When the canal opened in 1848, farmers gained direct access to Chicago, which soon became the largest and most efficient grain market in the world.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.10a




You are at mile
69
Hogan Grain Elevator
is 0.1 miles ahead

Oldest elevator.
The 1861 Hogan Grain Elevator operated into the 1980s. It was one of dozens of grain elevators that once punctuated the skyline along the I&M Canal.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.10b




You are at mile
70
Locks 9 & 10
is 3.6 miles ahead

A wealth of waterways.
The I&M Canal was one of hundreds of U.S. canals built in the 1800s. When it opened in 1848, many other canals were already competing with railroads.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.09a




You are at mile
70
Hogan Grain Elevator
is 1.1 miles ahead

Low slow targets.
The town of Seneca played a leading role in World War II. Thousands of landing ship tanks (LSTs), dubbed "low slow targets," were made here.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.09b




You are at mile
71
Locks 9 & 10
is 2.6 miles ahead

Undiscovered treasures.
The Illinois River Valley contains numerous Native American sites. Thousands of Native Americans lived in this area until the 1830s.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.01a




You are at mile
71
Hogan Grain Elevator
is 2.1 miles ahead

Get your cereal here!
In June 1855, 100,000 bushels of grain were shipped in one day, enough to fill thousands of bowls of cereal.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 52.01b




You are at mile
72
Locks 9 & 10
is 1.6 miles ahead

A year to remember.
1848 was a pivotal year in northern Illinois' history. In addition to the canal opening, the first railroad and telegraph connections entered Chicago.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 53.08a




You are at mile
72
Hogan Grain Elevator
is 3.1 miles ahead

Watermelon people.
The Potawatomi Indians, who once lived here, were called the watermelon people by some other tribes, probably because they liked to grow Indian squash.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org


Location: 53.08b



You are at mile
73
Locks 9 & 10
is 0.6 miles ahead

Think highways are slow!
Canal boats traveled less than 5 miles per hour. A trip down the full length of the canal took 22 to 26 hours. Today, a car takes only 2 hours.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more  landmcanal.org

Location: 53.07a

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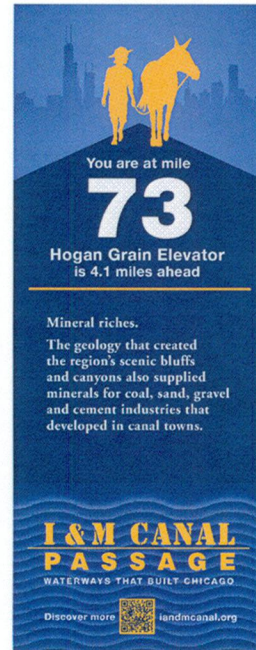
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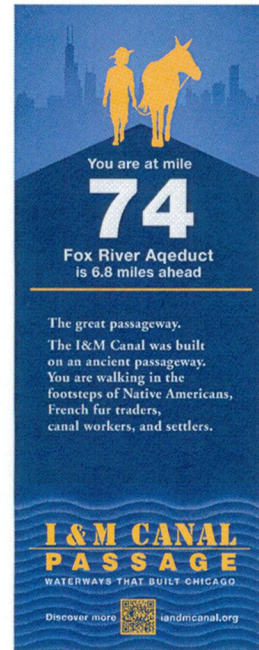
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Mile Marker Panels

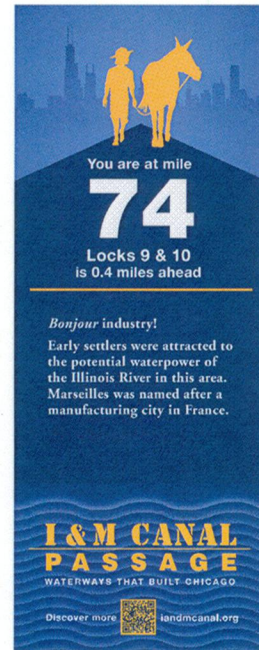
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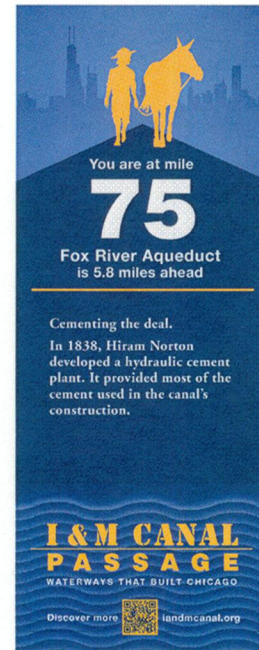
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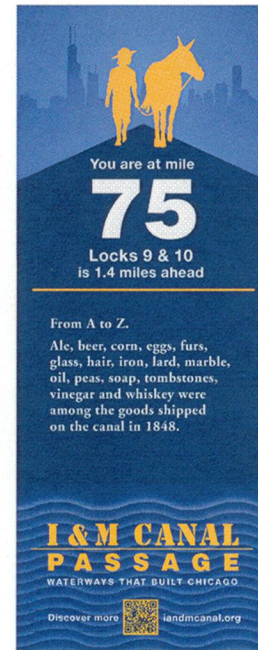
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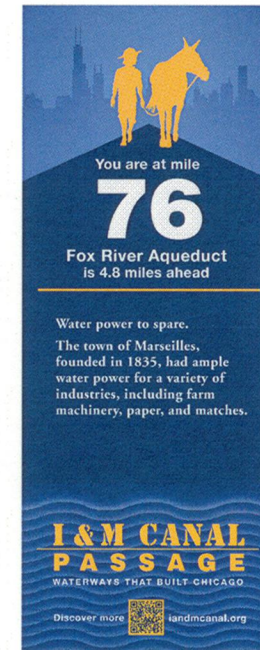
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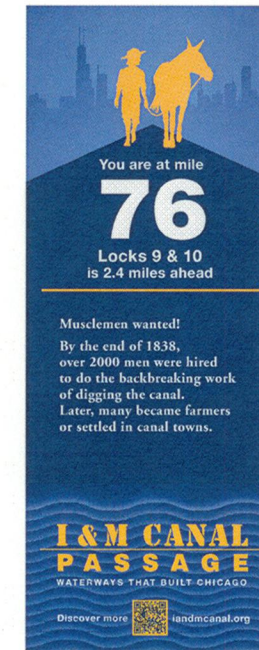
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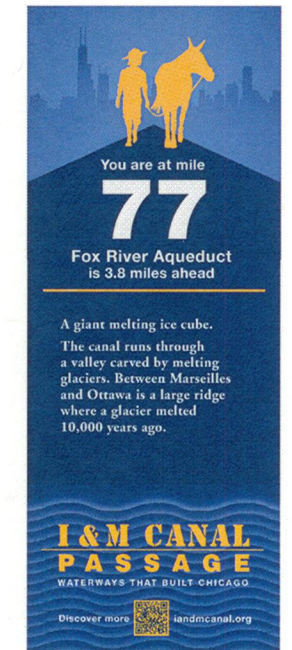
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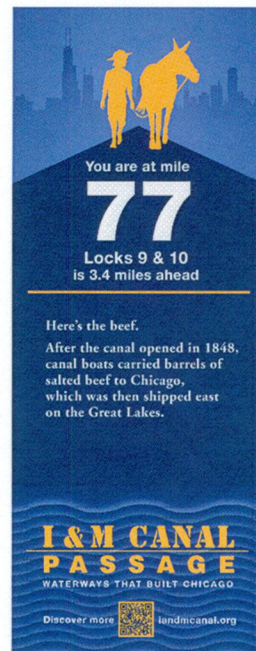
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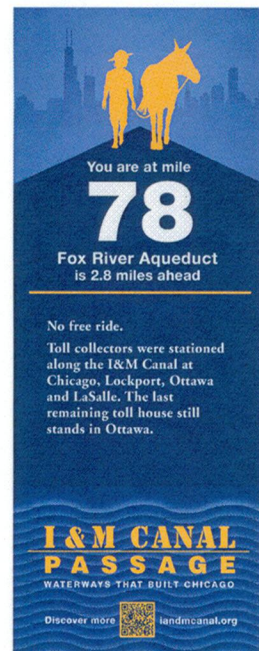
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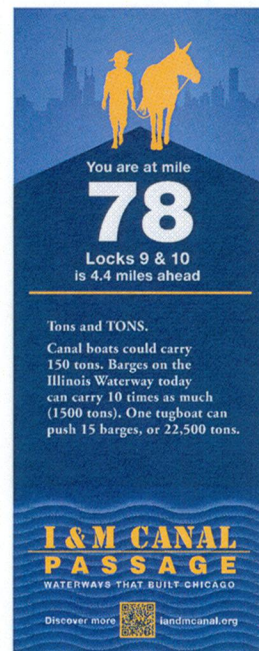
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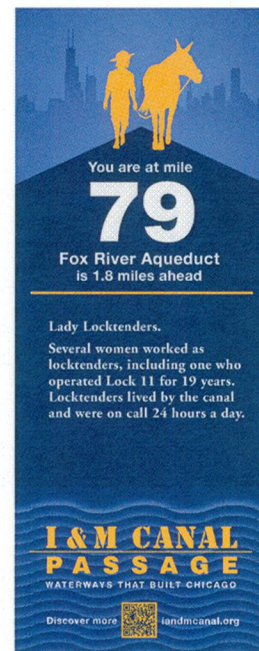
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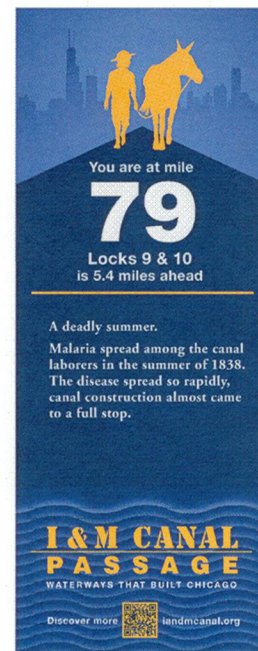
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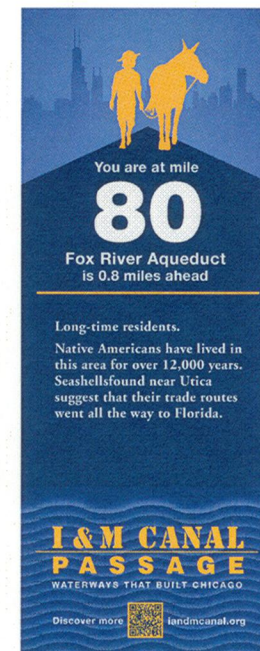
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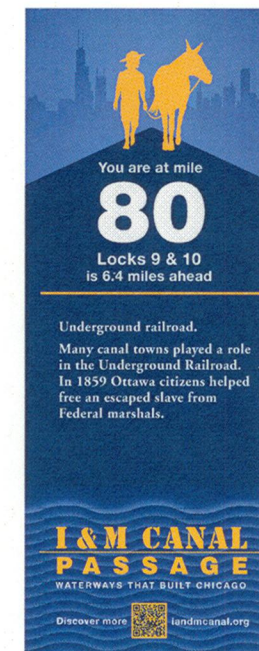
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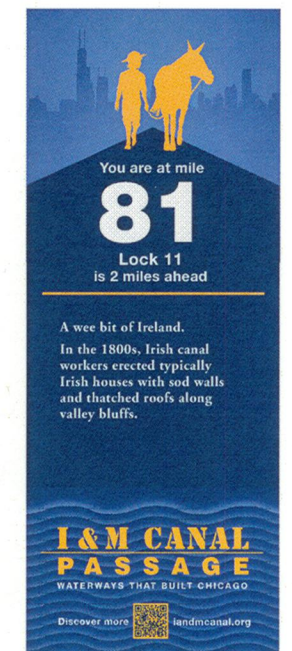
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Location: 54.10a



Location: 54.10b



Location: 54.08a

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Location - Trailwide

You are at mile
81
Fox River Aqueduct
is 0.2 miles ahead

Wild, wild Midwest.
In 1838, brief but violent riots erupted among different groups of Irish laborers. Locals formed a posse and killed several of the rioters.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.08b

You are at mile
82
Lock 11
is 1 mile ahead

Ottawa's most famous event.
In 1858, Abraham Lincoln debated Stephen A. Douglas over the issue of slavery in Washington Square in Ottawa. Nearly 10,000 people attended.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.07a

You are at mile
82
Fox River Aqueduct
is 1.2 miles ahead

From towpath to trail.
In the 1930s, during the Great Depression, the Civilian Conservation Corps (CCC) turned the canal towpath into a recreational trail.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.07b

You are at mile
83
Lock 12
is 1.4 miles ahead

Dirty work.
Before the canal could be dug, the land had to be "grubbed" and cleared - trees had to be cut down, brush cleared away and boulders removed.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.05a

You are at mile
83
Fox River Aqueduct
is 2.2 miles ahead

Water Power.
The canal served purposes other than just hauling freight. Water power was developed to fuel paper, flour and other mills at Lockport, Ottawa and Joliet.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.05b

You are at mile
84
Lock 12
is 0.4 miles ahead

Scenic bluffs.
The Fox River is the most popular canoe route in the state. Its scenic bluffs - vertical walls of St. Peter's sandstone - range from 85 to 150-feet high.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.04a

You are at mile
84
Lock 11
is 1 mile ahead

Why Illinois & Michigan?
The Illinois and Michigan Canal was so named because it connected the Illinois River with the water of Lake Michigan via the Chicago River.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.04b

You are at mile
85
Buffalo Rock
is 0.5 miles ahead

A big connection.
When it opened in 1848, the I&M Canal connected the Great Lakes and the Mississippi River, creating a water highway between New York and the Gulf of Mexico.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.03a

You are at mile
85
Lock 12
is 0.6 miles ahead

Opening the floodgate.
The I&M Canal was the first major public works project in the State of Illinois. The canal brought new commodities, new people and new ideas.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 54.03b

You are at mile
86
Utica
is 4.6 miles ahead

First the canal, then rails.
Many believe that railroads caused Chicago and Illinois' phenomenal growth in the 19th century, but it was the I&M Canal that started it.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.08a

You are at mile
86
Buffalo Rock
is 0.5 miles ahead

From sand to glass.
Before it became a state park in 1911, Starved Rock's beautiful canyons were threatened by the heavy use of silica sand in the glass industry.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.08b

You are at mile
87
Utica
is 3.6 miles ahead

Home, home on the Rock.
Buffalo Rock was once home to the Miami tribe. Later it became a summer resort. In the 1930s, the Civilian Conservation Corps (CCC) built a camp here.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.07a

You are at mile
87
Buffalo Rock
is 1.5 miles ahead

Crown Jewel of the Canal Corridor.
Starved Rock State Park offers scenic vistas, Native American history, interesting geology, and an abundance of wildlife.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.07b

You are at mile
88
Utica
is 2.6 miles ahead

Parlez-vous Français?
French explorers recorded most of what we know about the Illinois Indians. Today, their descendants are known as the Peoria Tribe of Oklahoma.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.06a

You are at mile
88
Buffalo Rock State Park
is 2.5 miles ahead

Many tribes.
Illinois, Miami, Potawatomi, Ottawa and Kickapoo tribes once lived here. Men hunted bison and deer, women tended crops. They grew corn, beans, squash and melons.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.06b

You are at mile
89
Utica
is 1.6 miles ahead

A Grand Village.
Thousands of Native Americans once lived near here in a village on the Illinois River. Father Jacques Marquette said mass for them in 1675.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO

Discover more landmcanal.org

Location: 56.05a

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Mile Marker Panels

Location - Trailwide

You are at mile
89
Buffalo Rock State Park
is 3.5 miles ahead

Where did it go?
The I&M Canal closed in 1933.
The canal walls still remain
beneath the trees and silt
that now hide it from view.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.05b

You are at mile
90
Utica
is 0.6 miles ahead

That's a BIG bowl of cereal!
In June 1855, 100,000 bushels
of grain were shipped along
the canal in just one day.
Grain elevators dotted
the landscape of all
the canal towns.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.04a

You are at mile
90
Buffalo Rock State Park
is 4.5 miles ahead

Canal Cornucopia.
Items shipped on the canal
included ale, beeswax, ice,
horns, hemp, and animal skins.
Corn, wheat, stone and timber
were the primary goods.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.04b

You are at mile
91
Split Rock
is 2.2 miles ahead

Where are the warehouses?
Utica has one of only 2
remaining canal warehouses
along the I&M Canal. It now
houses the LaSalle County
Historical Society.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.03a

You are at mile
91
Utica
is 0.4 miles ahead

Think highways are slow?
Canal boats traveled less than
5 miles per hour. A trip down
the 96-mile canal took almost
26 hours. By car today it takes
only 2 hours.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.03b

You are at mile
92
Split Rock
is 1.2 miles ahead

Marathon Mules?
Even slow marathoners run
twice as fast mules, but they
don't have to pull 100 tons
and a canal boat. A canal trip
from Chicago to LaSalle
took a full day.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.02a

You are at mile
92
Utica
is 1.4 miles ahead

Keep 'em moving.
Most mule drivers were
teenage boys, 14 to 17.
They were known to swear,
fight, and gamble. One of
the most famous moved west
and became known as
"Wild Bill" Hickok.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.02b

You are at mile
93
Split Rock
is 0.2 miles ahead

Slicing through Sandstone.
During construction of the
canal, canal workers blasted
through a sandstone exposure
to create Split Rock, located
between Utica and LaSalle.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.01a

You are at mile
93
Utica
is 2.4 miles ahead

Musclemen wanted.
Irish and other immigrants
came to Illinois to do the
arduous work of digging the
canal. Much of the canal
was dug by hand using only
picks and shovels.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 56.01b

You are at mile
94
LaSalle
is 1.9 miles ahead

Living in squalor.
For canal workers, life could
be brutal. Shanty towns dotted
the length of the canal where
entire families were crowded
into dirty, tiny huts.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.04a

You are at mile
94
Split Rock
is 0.8 miles ahead

Mud, mosquitoes and malaria.
Working on the canal
was no picnic. Canal workers
died of cholera, typhoid and
dysentery. In marshy areas,
leeches covered their skin.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.04b

You are at mile
95
Lock 14
is 0.4 miles ahead

Fishing anyone?
Many Great Blue Herons,
Great Egrets, and other water
birds find the fishing good
on the I&M Canal. Fishermen
catch channel catfish, carp
and bluegill.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.03a

You are at mile
95
Little Vermilion Aqueduct
is 0.3 miles ahead

People Movers.
From 1848 to 1854,
thousands of people traveled
on canal packet boats between
LaSalle and Chicago.
Millions of tons of goods
were shipped until 1933.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.03b

You are at mile
96
End of Trail
is 400 feet ahead

Steamboats a' comin'.
Mississippi River steamboats
often met canal boats at this
mile-long steamboat basin
where passengers and goods
could be exchanged.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.01a

You are at mile
96
Lock 14
is 0.8 miles ahead

Tons and TONS.
Canal boats could carry
150 tons. Barges on the
Illinois Waterway today
can carry 10 times as much
(1500 tons). One tugboat
can push 15 barges, or
22,500 tons.

**I&M CANAL
PASSAGE**
WATERWAYS THAT BUILT CHICAGO
Discover more landmcanal.org

Location: 58.01b



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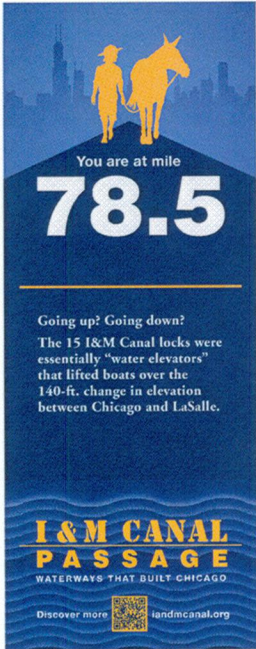
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Mile Marker Panels

Location - Trailwide



Location: 54.17

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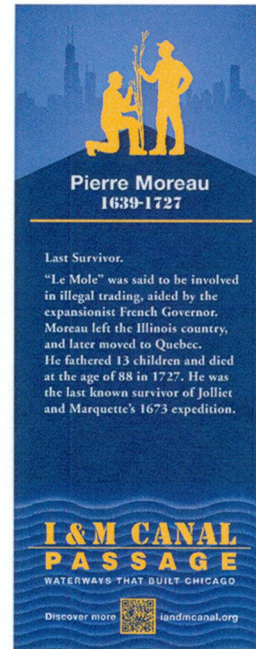
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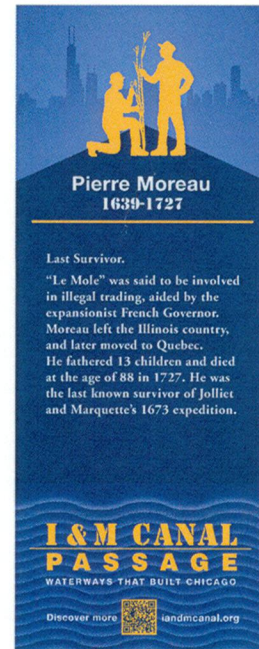
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Silhouette Panels

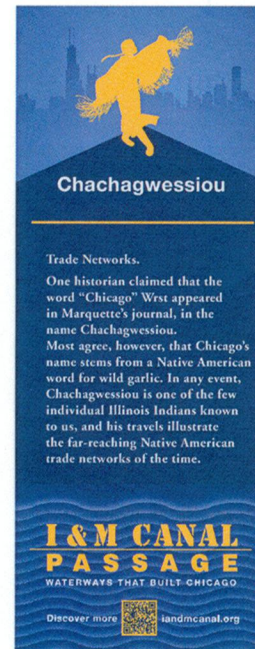
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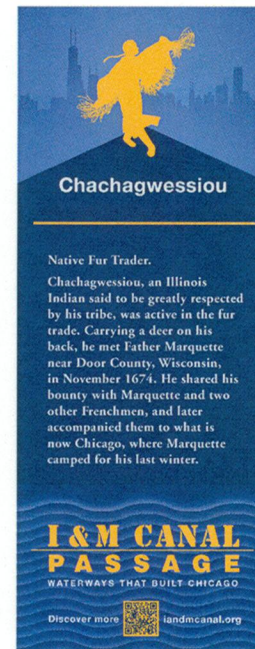
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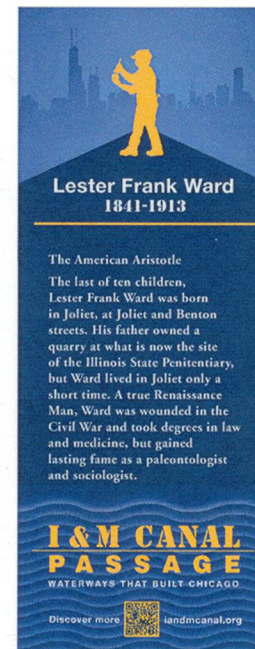
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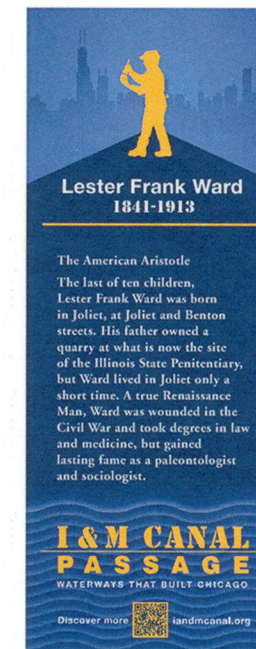
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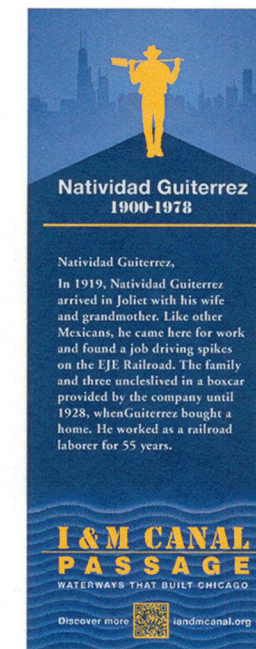
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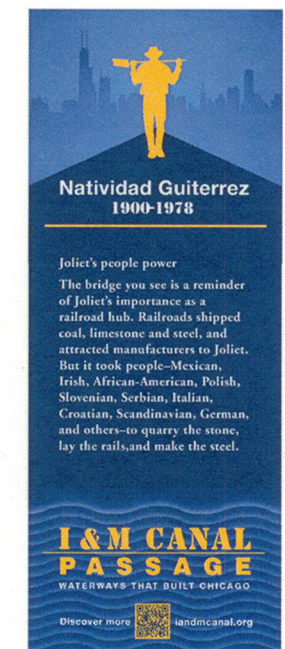
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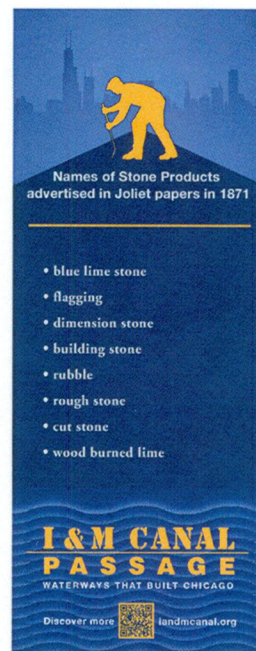
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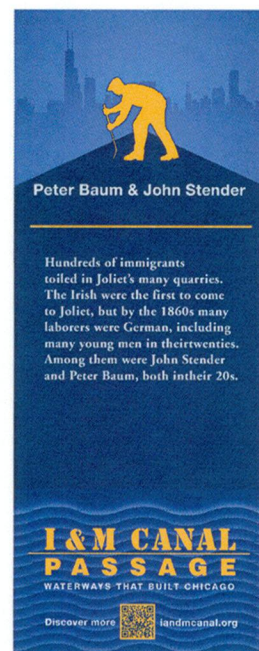
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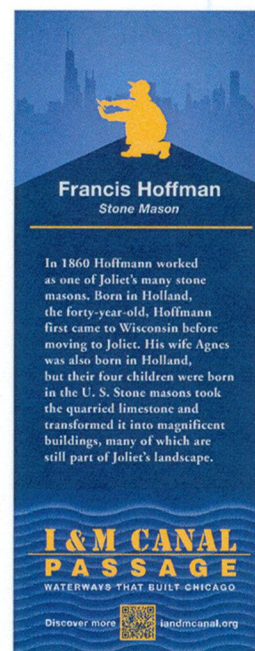
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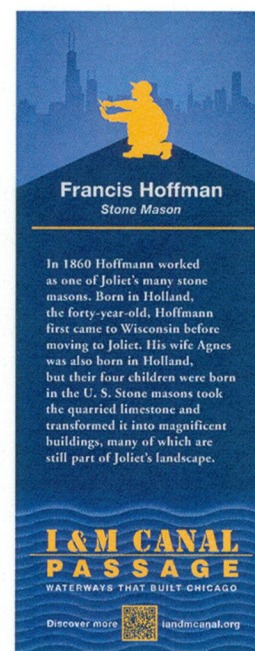
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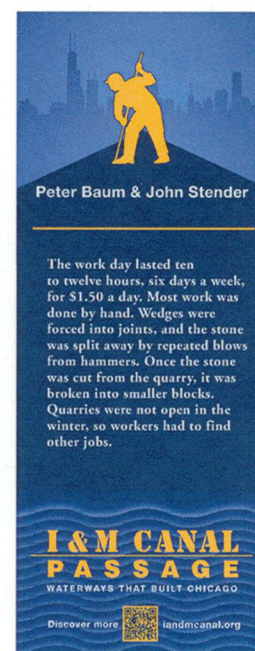
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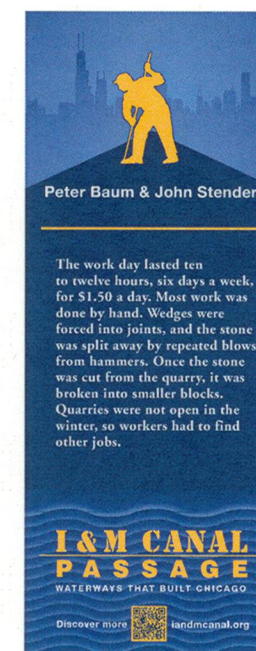
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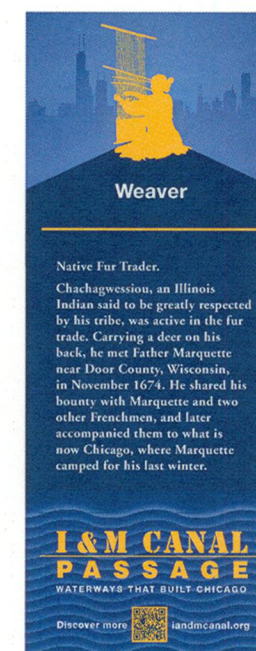
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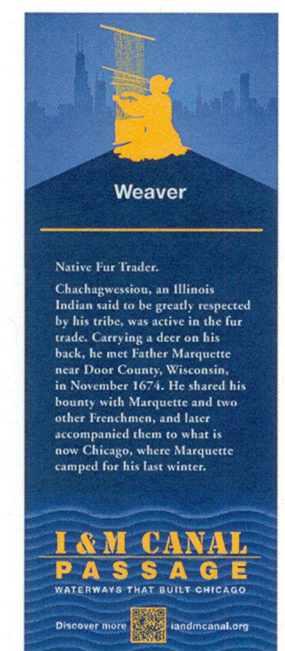
Location: 43.08a



Location: 43.08b



Location: 43.10a



Location: 43.10b

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Sheet
26

Silhouette Panels

Location - Trailwide

The Marx Brothers
1873-1937

Bad Farmers, great entertainers.

The Marx Brothers, one the most famous comedy teams of the 20th century, once owned a chicken farm near the I&M Canal. But, according to Groucho, they failed as farmers because they spent too much time watching baseball at Wrigley Field and not enough minding the chickens. In the 1930s, the Marx Brothers performed at the Rialto Theatre.

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Location: 43.11a

The Marx Brothers
1873-1937

A people palace

In the 1920s, the six Rubens brothers began plans for the Rialto Theatre. It was to be a "Palace for the people." Joliet was at the peak of its industrial might when the theater opened in 1926. A few years later, during the Great Depression, entertainers here brought smiles to those seeking to escape their economic worries.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 43.11b

Patrick Smith

Patrick Smith

Irish immigrants first came here to dig the I&M Canal. By the 1870s, when Joliet was a railroad hub, many, like Patrick Smith, did the back-breaking work of laying tracks. Workers used picks, shovels and sledgehammers to clear the ground and drive in spikes. They worked long hours in the warmer months but often had to find other jobs in the winter.

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Location: 43.13a

Patrick Smith

City of Churches

As immigrants came to Joliet, they created their own ethnic communities, and churches were at the heart of them. In 1884, people worshipped in 21 different churches. St. Patrick's and St. Mary's served Irish families. Serbs went to St. George's, Slovenians to St. Joseph's, Germans to St. Peter's and St. John's, and Croatians to St. Mary's Nativity.

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Location: 43.13b

Walter Olin

Walter Olin, Steelworker

Before people owned cars, or had easy access to public transportation, they often had to walk long distances to work. In the 1920s, Walter Olin rode his bicycle to his job at the Joliet Iron Works. In the winter months, he put chains on his bicycle's wheels and bundled up in his warmest clothes for the long ride to the steel plant.

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Location: 43.14a

Walter Olin

City of cycles?

In the late 1800s, bicycles became a popular means of transportation in Joliet. Much like today, bicycle clubs planned social rides and races. By 1895, so many people were riding bicycles, that the city passed an ordinance requiring the use of lampson bicycles at night, and established a speed limit of 6 miles per hour.

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Location: 43.14b

Mary Setina
1873-1937

Mary Setina, a Slovenian immigrant, came to Joliet in 1903. She lived in the St. Joseph's neighborhood, known as "Slovenian Row," and owned a store there. School children stopped to buy candy, ice cream, or school supplies from her, and men from the mills bought tobacco.

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Location: 43.15a

Mary Setina
1873-1937

Work at home.

Joliet's immigrant women often needed jobs to make ends meet. They were maids, seamstresses, washerwomen, cooks and shopkeepers. In addition to caring for their families, many women took in boarders - workers from local steel mills and quarries - and had to cook, clean, wash, mend and iron for as many as a dozen men at a time.

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Location: 43.15b

James Bruce
1823-1998

James Bruce, 1823-1898

Born in Scotland, James Bruce learned of the local dolomite stone as an I&M Canal construction foreman. Canal workers had to blast through the stone, and used it to build the canal's locks and walls. Bruce later used the stone to make his fortune, opening quarries in Joliet and Lockport that employed hundreds of workers.

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Location: 43.16a

James Bruce
1823-1998

City of Stone

In the 1880s, Joliet area quarries employed thousands of men, mostly immigrants from Sweden, Poland, Italy, Germany and Czechoslovakia. Quarrying was hardwork. Workers were paid only about \$1.25 per day to pry and dig out the stone in pieces. Quarry owners separated workers from different countries to keep them from unionizing.

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Location: 43.16b

George Kiser

George Kiser, an African - American, moved to Joliet from Missouri in the early 1900s. A laborer at the Joliet Iron Works, he worked in noisy, hot and dangerous conditions. The mill employed workers from all over the world - migrants from the south were joined by Poles, Swedes, Germans, Bohemians and others.

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Location: 43.17a

George Kiser

Of steel and men.

Steel was one of Joliet's major industries. Early mills were loud, dark, and poorly ventilated. Men lost their hearing and got lung disease from ore and steel dust. Blast furnace workers endured temperatures of 100-150 degrees and suffered heat stroke and exhaustion. Accidents were common.

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Location: 43.17b

John Sullivan

Many young men found jobs as canal drivers.

In 1850, Morris had nineteen canal drivers, the majority of them teenagers. Massachusetts-born John Sullivan, only fifteen-years-old. The drivers, like many in Illinois, were born elsewhere - three in Ireland, one in Canada, while others were from New York, Vermont, Ohio, Pennsylvania, and Tennessee. Canal drivers had a reputation for swearing, drinking, and stealing.

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Location: 51.02a

Old Nell

Old Nell of Morris was perhaps the most famous mule on the I&M Canal. For decades teams of mules pulled the 150-ton canal boats. Mule teams were changed every ten miles, and mule barns once dotted the landscape along the canal. Female mules were always the lead mule, as they were considered smarter than the males. Nell became well known due to her ability to find her way home untended after her shift was over.

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Location: 51.02b

Elsie Armstrong
1789-1871

"And eight I reared to manhood, and I brought to Illinois, a pleasant and a lively set of active little boys." In 1831, Elsie Armstrong and seven of her sons, aged 3 to 19, arrived on the Illinois frontier, drawn by the promise of a new life away from her drunken husband in Ohio. Plans were underway to construct the Illinois and Michigan Canal. Her sons soon helped build it. Three became founders of Morris-Armstrong Road bears their name. Elsie is buried nearby in Evergreen Cemetery.

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WATERWAYS THAT BUILT CHICAGO

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Location: 51.10a

Elsie Armstrong
1789-1871

"...How sweet the recollection, when my children were about me, then I felt I had protection, and no one dared to flout me." Elsie Armstrong wrote an epic poem that records the tragic deaths of four children. It expresses her pride in the sons who founded Morris and became community leaders. The poem is a rare record of a pioneer woman's hardships, her feelings, and a poignant reminder of the importance of families.

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Location: 51.10b

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Silhouette Panels


Location - Trailwide




Elsie Armstrong
1789-1871

Elsie Armstrong's sons illustrate the difficulties and possibilities of life on the frontier. Joel founded a bank. Jeremiah and Isaiah sought their fortunes in the California Gold Rush of 1849. Perry Armstrong ran a general store and later became a lawyer. He represented Morris in the Illinois legislature. Armstrong descendants still live in the canal corridor and have preserved the stories of Elsie Armstrong and her family.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 51.31a




Elsie Armstrong
1789-1871

Elsie Armstrong's sons illustrate the difficulties and possibilities of life on the frontier. Joel founded a bank. Jeremiah and Isaiah sought their fortunes in the California Gold Rush of 1849. Perry Armstrong ran a general store and later became a lawyer. He represented Morris in the Illinois legislature. Armstrong descendants still live in the canal corridor and have preserved the stories of Elsie Armstrong and her family.

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Location: 51.31b




Sgt. James Sanborn
1839-1864

When the Civil War began in 1861, James Sanborn was a farmer near Marsilles. He enlisted in Company K of the 39th Illinois Volunteer Infantry. In 1863 while Sanborn was waiting for the steamer to go home on furlough from South Carolina, his company commander, Captain Joseph Woodruff of Marsilles, was killed by enemy shell fire near Charleston. Sanborn took charge of the captain's remains and brought them home to Marsilles.

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WATERWAYS THAT BUILT CHICAGO

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Location: 53.09a




Sgt. James Sanborn
1839-1864

On August 16, 1864, at the Battle of Deep Run in Virginia, Sanborn was among 97 men and 7 officers of the 39th who lost their lives. His comrades were unable to recover his body. To this day he rests on that field among the "unknown." Sanborn was one of over 250,000 Illinois men who served in the Union Army during the Civil War.

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Location: 53.09b




James Clark
1811-1888

Utica's Foremost Citizen - Clark's influence and contributions to the development of Utica are legendary. He was Utica's first postmaster, first assessor, supervisor, mayor, village president, and State Representative. Also a business man, he owned a sand business, was a grain purchaser and supplier, and owned Utica's first hotel.

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Location: 56.09a




James Clark
1811-1888

Canal Contractor - Born in England, engineer James Clark arrived in America in 1830 penniless. He found his way from New York to Ohio to Peoria and by 1835 he operated a stage coach line and was purchasing property locally. In 1837 he was contracted to dig two miles of the I&M Canal. Owner of the Utica Hydraulic Cement Co., he built the warehouse in front of you.

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Location: 56.09b




Shabbona
1775-1859

A burly bear of a man, Shabbona was a Potawatomi chief. During the War of 1812 Shabbona fought with the British against the Americans but he later became friends with early Illinois settlers. In 1832 Shabbona opposed Black Hawk's War and warned white settlers in advance of attacks. After the war nearly all Native Americans were banished from Illinois, but Shabbona given land in Dekalb County.

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WATERWAYS THAT BUILT CHICAGO

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Location: 58.07a




Shabbona
1775-1859

Imagine Chief Shabbona's astonishment during his visit to Chicago in the summer of 1852. Visiting with his old friend John H. Kinzie, the collector of I&M Canal tolls at Bridgeport, Shabbona stood in "wonder and admiration" at the remarkable changes that had taken place. In a few years Chicago had risen from a tiny military outpost to a thriving city of nearly 40,000. Shabbona died in 1859 and is buried in Morris.

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WATERWAYS THAT BUILT CHICAGO

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Location: 58.07b




Father John O'Reilly
1802-1862

Canal Passenger - Two missionaries arrived in LaSalle in 1838 to serve the spiritual needs of hundreds of young Irish canal diggers. Within weeks the priests had to face a devastating epidemic and labor violence on the canal. Father John O'Reilly arrived here in 1848 and quickly became a beloved figure among the large Irish population. Talented and energetic, O'Reilly was a compelling speaker who ministered wisely to his flock.

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WATERWAYS THAT BUILT CHICAGO

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Location: 58.08a




Father John O'Reilly
1802-1862

Canal Passenger - O'Reilly had his hands full in rough and tumble LaSalle, known for its many saloons. During his ten years in LaSalle, O'Reilly broke up countless barroom fights while helping raise the morals and manners of its citizens. He was described as a "champion of the oppressed... sworn enemy to rowdism, which he fought with forcible and scathing invectives... and did his best to cleanse the town of such questionable element."

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Location: 58.08b




John Means

Canals developed a distinctive culture, including boat captains, mule drivers, and locktenders. Little is known about most of the working class men who labored to keep the boats moving up and down the canal. Locktenders were paid \$300 a year and were on call 24 hours a day from April until December. Their slumber was often interrupted by the bleating of the boatman's horn, announcing a boat approaching the dock.

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Location: 58.09a




John Means

John Means was one of thousands who helped dig the I&M Canal, and in 1848 he was rewarded by being appointed one of the original group of 13 locktenders on the canal. He manned locks 14 and 15 here in LaSalle. During peak periods Means was constantly employed in manually opening and closing the massive lock gates, and often a locktender's entire family was involved in the process of locking boats through.

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Location: 58.09b




John W. Connett
1812-1885

Best Captain - John Connett was one of the most popular packet boat captains on the I&M Canal. Born in western New York, he piloted a boat on the Erie Canal before moving to Chicago in 1848. One testimonial to Connett characterized him as "one of the most experienced and successful navigators on the canal." An attentive and accommodating boat captain could make an uncomfortable passage more tolerable for harried passengers.

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WATERWAYS THAT BUILT CHICAGO

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Location: 58.10a




John W. Connett
1812-1885

One of Connett's last trips on the canal was certainly memorable. During an early freeze in November 1852 his boat the Prairie Queen struck ice and sank in the canal on a cold Sunday morning. The passengers were successfully taken off and boarded another packet. After the railroads took away the canal's passenger trade in 1853, Connett quickly found work as an agent for the Michigan Central Railroad.

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Location: 58.10b




Grenville M. Dodge
1831-1916

Grenville M. Dodge gained fame as a Union General in the Civil War and as chief engineer of the transcontinental railroad. He traveled on the I&M in 1851. "I nearly all the the passengers were on deck shooting at the bull snakes that lay on the rocks through which the canal had been cut. The shooting was very bad... I thought I would try my hand... At the first shot I laid out a snake and that fixed my reputation."

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Location: 58.11a



Grenville M. Dodge
1831-1916

Arriving at LaSalle, Dodge's trip turned ugly, as he witnessed the almost casual violence of frontier Illinois. "When I got off the boat I found that Peru, the place I was destined for, was a mile away. I stepped up to Captain Wheeler to ask him how I could get there. He was talking excitedly to a man when another man stepped up and shot the man who was talking to Wheeler. It did not seem to create much excitement."

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Location: 58.11b

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Sheet
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Silhouette Panels

Location - Trailwide

Abraham Lincoln & Family
1809-1865

In the mid 1830s Abraham Lincoln, then a member of the Illinois State Legislature, voted in favor of construction of the I&M Canal. A staunch advocate of public works, Lincoln knew that the young state of Illinois desperately needed to improve its transportation system. As a member of the U.S. House of Representatives, Lincoln trumpeted the positive effects of the I&M Canal in the nation's capital.

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Location: 58.12a

Abraham Lincoln
1809-1865

During the canal's inaugural year Lincoln, his wife, Mary, and their two children, Robert Todd and Edward took a ride from Chicago to LaSalle on a canal packet. From LaSalle, the Lincolns transferred to a steamboat bound for Peoria, where they boarded a stagecoach bound for Springfield. During the Civil War, Lincoln supported the enlargement of the I&M Canal but Congress rejected the idea.

I&M CANAL PASSAGE
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Location: 58.12b

Gurdon S. Hubbard
1802-1886

Over the course of a long life and eventful life, Gurdon Hubbard helped transform Chicago from an uninhabited swamp into the fastest-growing city in the U.S. He came to Illinois as a fur trader in 1818, and Native Americans dubbed him Swift Walker. "He wore a buckskin shirt and carried a knife and tomahawk...and let his hair grow long." With the close of the fur trade, Hubbard made Chicago his permanent home in 1834.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 58.13a

Gurdon S. Hubbard
1802-1886

Hubbard served as one of the original I&M Canal commissioners, speaking at the canal's groundbreaking in 1836. A financial wizard, he made a fortune in meat-packing and real estate and had investments in many canal towns, including nearby Utica. He was among the first to see the I&M Canal corridor as a cohesive business unit. The Great Chicago Fire of 1871 bankrupted him, but Hubbard's life spanned an epoch in Chicago history.

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WATERWAYS THAT BUILT CHICAGO

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Location: 58.13b

Clara & Lydia Matteso

While canal packets moved slowly compared to river steamboats, many people enjoyed traveling via the canal. As one commentator on an eastern canal wrote, "To the lover of nature, the canal is an ideal method of travel. Rocks and trees, birds and flowers on the shore can be studied leisurely in detail, and every landscape is indelibly photographed on the memory as it slowly vanished in the distance.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 58.14a

Clara & Lydia Matteso

Women seemed to dislike packet travel more than men did. Traveling with her older sister, Clara Matteson, the nine-year-old daughter of Illinois Governor Joel Matteson, exclaimed that the voyage was "so crude that I cannot understand how people submitted to it." Another women exclaimed, "I can't tell you how pleased I was to leave the canal boat, a little, low, crowded place, moving along at a snail's pace."

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Location: 58.14b

Wild Bill Hickok
1837-1976

The most famous mule driver on the I&M Canal was James Butler (Wild Bill) Hickok. In the first recorded fight of his career, Bill tangled with a fellow canal driver who had been abusing his mules. Both men tumbled into the canal but Bill emerged victorious. Bill eventually headed west, where he gained fame as a lawman and gunslinger. At the age of 39 he was shot in the back while playing cards in Deadwood, South Dakota.

I&M CANAL PASSAGE
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Location: 58.15a

Wild Bill Hickok
1837-1976

Canal drivers were responsible for leading the mules or horses that pulled the boats along the canal towpath. They walked 10 to 15 miles per day and helped care for the animals. Drivers had to coax the notoriously stubborn mules and occasionally had to rescue horses or mules that fell into the canal. Drivers also coordinated the sometimes tricky maneuvering necessary when boats passed each other on the narrow towpath.

I&M CANAL PASSAGE
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Location: 58.15b

Rene-Robert Cavalier de La Salle
1643-1687

La Salle dreamed of creating a French fur trading empire in the Midwest. He came to North America in 1667 and traveled to Illinois in 1680. During the winter of 1682-83 he established Fort St. Louis atop Starved Rock for protection from the Iroquois. This intrepid French explorer was one of the most dynamic characters to have passed through what is now Illinois.

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Location: 58.18a

Rene-Robert Cavalier de La Salle
1643-1687

La Salle was most famous for his voyage of exploration down the Mississippi River to the Gulf of Mexico in 1681-82. He claimed all of these lands, later known as the Louisiana Territory, for France. For the two years before his death he fruitlessly searched for the mouth of the Mississippi River from the Gulf of Mexico. He was killed by one of his own men in what is now Texas.

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Waterways That Built Chicago

You are walking in the footsteps of people who have traveled this passageway for centuries. Native Americans first used the waterways for trade. They showed their routes to French explorers, the first to dream of building a canal.

From 1836 to 1848, immigrant workers dug the 93-mile I&M Canal by hand. It opened a water highway between New York Harbor and the Gulf of Mexico. It made Chicago our country's greatest inland port.

The canal closed in 1933, when larger man-made waterways, railroads and highways took its place. Today the Illinois and Michigan Canal National Heritage Corridor is a new kind of national park, welcoming travelers to the parks, trails, small towns and landmarks along this historic passageway. You are walking in the footsteps of people who have traveled this passageway for centuries.

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Location: 43.25

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Location: 48.02

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Location: 48.03

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Location: 48.04

Waterways That Built Chicago

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The canal closed in 1933, when larger man-made waterways, railroads and highways took its place. Today the Illinois and Michigan Canal National Heritage Corridor is a new kind of national park, welcoming travelers to the parks, trails, small towns and landmarks along this historic passageway.

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Location: 48.05

Waterways That Built Chicago

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Location: 51.01

Waterways That Built Chicago

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From 1836 to 1848, immigrant workers dug the 93-mile I&M Canal by hand. It opened a water highway between New York Harbor and the Gulf of Mexico. It made Chicago our country's greatest inland port.

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Location: 51.11

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Waterways That Built Chicago

You are walking in the footsteps of people who have traveled this passageway for centuries. Native Americans first used the waterways for trade. They showed their routes to French explorers, the first to dream of building a canal.

From 1816 to 1848, immigrant workers dug the 94-mile I&M Canal by hand. It opened a water highway between New York Harbor and the Gulf of Mexico. It made Chicago our country's greatest inland port.

The canal closed in 1933, when larger man-made waterways, railroads and highways took its place. Today the Illinois and Michigan Canal National Heritage Corridor, a new kind of national park, welcomes visitors to the parks, trails, canal towns and landmarks along this historic passageway.

Map showing the route of the I&M Canal from New York Harbor to the Gulf of Mexico, passing through Chicago.

Welcome to the I&M Canal State Trail

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Discover more: landmcanal.org

Location: 51.19

Waterways That Built Chicago

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Discover more: landmcanal.org

Location: 51.26

Waterways That Built Chicago

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Discover more: landmcanal.org

Location: 51.30

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Location: 52.04

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Discover more: landmcanal.org

Location: 54.02

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Discover more: landmcanal.org

Location: 54.06

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Location: 56.04

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Discover more: landmcanal.org

Location: 58.02

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Kiosk Panels

Location – Trailwide



Location: 58.19



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Straight Cor-Ten Panels

Location - Trailwide

Joliet Buildings

Joliet's first stone building went up in 1814-15, and the city's business district quickly grew up around it. Joliet's downtown, essentially called First Street West in the very early days of the town, where many limestone structures grew a home to the lasting beauty of this building material. They add and greatly to the character of the city, from the Joliet Prison to the many stone churches. Pick up a walking tour brochure inside the nearby Kelle Lincksecker Lumber Historical building.

Joliet's architecture is a mixture, from its early French limestone and limestone to other materials related to an Indian heritage. This mix was also called Joliet masonry, due to its ability to hold its ponds. Disturbance, a form of limestone, is one proof of calcium and magnesium carbonate. Most common calcium supplements are calcium carbonate.

Aluminum building work, 1900-1910

St. Joseph's Catholic Church, 1850-1860

Heavy stone building at Joliet Prison, 1890

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landcanal.org

Location: 43.06

Quarries Are Us

"It is wonderful to contemplate the incalculable supply of stone in the hills and knolls of the Erie-Philadelphian Valley. And the quality, too, the convenience before and hence in which it is found, and the easy means by its use the most wonderful work has been to dig out and quarry it. It seems strange that the layers of stone had never previously been used for the construction and benefit of man."

George Woodruff,
WM Carey Robinson, 1876



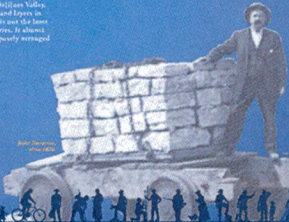

1860 and 1870s. Source: I&M CHS

While many know of Joliet as the City of Steel, the foundation of Joliet's industrial heritage rests upon limestone long before the steel industry came to epitomize Joliet's history. In the quarrying of limestone formed the basis of industry in the West. Workers during the 1830s could in Joliet in the West, and in fact, in the 1830s uncovered large quantities of a superior quality of limestone with a dramatic. The canal project had a desperate desire of transporting the stone, and within a few years a new industry was born. Quarries operated in Lemont, Lisle, Joliet and Chicago, creating thousands of new jobs. This heavy facility stone was used in bridge piers throughout the 18th Canal Corridor, including the Joliet Penitentiary and the Chicago Water Tower.

*John Ferguson
1870s*

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PASSAGE**

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Location: 43.09

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Location: 43.22

Welcome to Joliet

Discover Joliet's History
Along the Joliet Bicycle Route, look for the dimensions of just a few of the thousands of miles and centuries from all over the world whose best work helped build this town and its people.

Connect with other trails.
The Joliet Transit Bicycle Route links the 8-mile I&M Canal State Trail, which runs to Brandon Road, with trails that extend from the Joliet Interstate Historic Site north along the canal.

JOLTIET
Joliet City Center
Joliet Bicycle Route
Joliet Transit Bicycle Route
Joliet Intermodal Station

Trail link connecting again along the blue waterway

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[landmcanal.org](#)

Location: 43.23

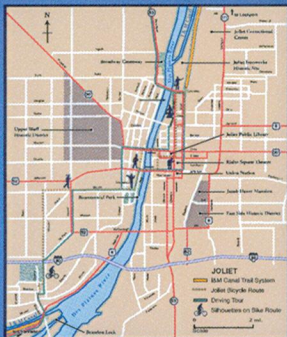
Welcome to Joliet

Discover Joliet's History

Along the Joliet Historic Route, look for the collection of just a few of the thousands of items our visitors have all over the world, where, look work beyond Joliet gone and prosper.

Connect with other roads


The Joliet Historic Route is a scenic route, the 61 miles I&M Canal State Trail, which runs to the west of Joliet, with trails that travel from the Joliet Historic Route, the south along the canal.



*Look for this sign
again along the historic route.*

I&M CANAL PASSAGE

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A silhouette illustration of a group of people in various poses, including walking, running, and pushing a stroller, set against a background of a city skyline at night.

Location: 43.24

Welcome to Joliet

Witness Joliet's History
Along the Joliet Bicycle Route, look for the silhouette of just a few of the thousands of men and women men all over the world, who have worked helped Joliet grow and prosper.

Connect with other trails
The Joliet area's bicycle routes link the 44 miles of I&M Canal State Trail, which starts at 90th Street Road, with trails that extend from the Joliet area into Winnetka, New North along the canal.

Look for silhouettes of men and women along the Joliet route.

I&M CANAL PASSAGE

Discover more.
landmcanal.org

Location: 43.26

Welcome to Canalport

Beginning in 1838, farmers from all over Grand County came to Morris to ship their produce on the I&M Canal. The opening of the Chicago and Rock Island railroad in 1853 did not diminish the shipping of bulk goods such as corn, oats, coal, and limestone via the Canal. In fact, the competition provided by the canal kept rates, freight rates lower. Many complained the canal as a more democratic form of transport, as anyone with a canal boat could engage in the trade, while railroads were criticized for being a monopoly.

While most goods shipped from Morris were destined for Chicago, oats and winter wheat were loaded onto boats bound for LaSalle, where they were transferred to steamboats for distribution in St. Louis. Corn is still an important local commodity, and you can see harvests dropped by passing trucks on Illinois Route 66 (formerly Canal St.). Since 1949 Morris has commemorated this legacy in its Successful Cere Festival. Sold every summer, Morris is one of the best preserved and most vibrant I&M Canal towns.

**I & M CANAL
PASSAGE**

Discover more.
landmcanal.org

QR code

Location: 51.03

Welcome to Morris

A Historic I&M Canal Town

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Discover more.
ianmcanal.org

Location: 51.09

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Grundy County
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Morris, IL 60450

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Straight Cor-Ten Panels

Location - Trailwide



Location: 53.10

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Cantilever Panels

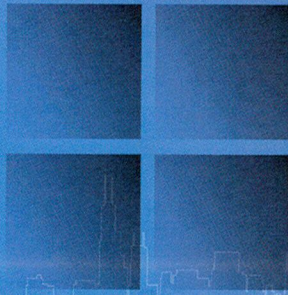
Location - Trailwide

Life in the Rocks

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Mile markers on the I&M Canal State Trail begin at mile 35 and end at mile 96. These numbers represent the approximate distance of the historic canal from its origins in Chicago to its end at LaSalle/Peru.

I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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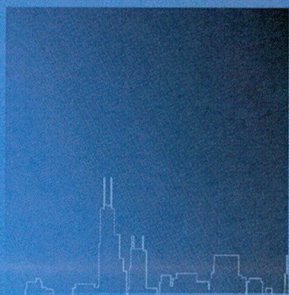
Location: 43.07

Meeting of the Waters

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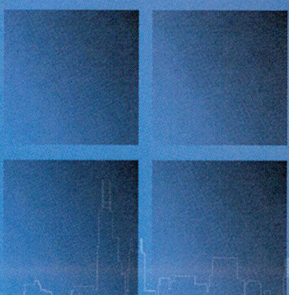
Location: 48.17

Orientation Sign (McKinley Woods)

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WATERWAYS THAT BUILT CHICAGO

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Location: 48.18

Aux Sable Village

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WATERWAYS THAT BUILT CHICAGO

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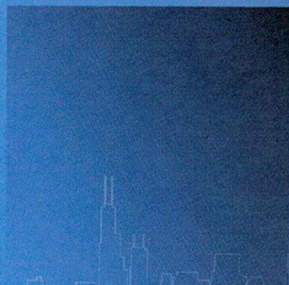
Location: 51.04

Lock 8 and Aux Aqueduct

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WATERWAYS THAT BUILT CHICAGO

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Location: 51.05

Dresden Barn

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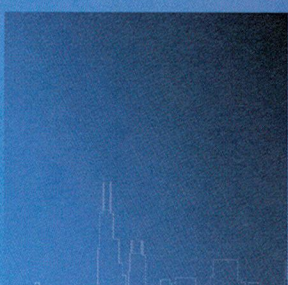
Location: 51.06

Yesterdays Canal

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I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 51.07

Welcome to Morris a Historic I&M Canal town

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I&M CANAL PASSAGE
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Location: 51.08

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Cantilever Panels

Location - Trailwide

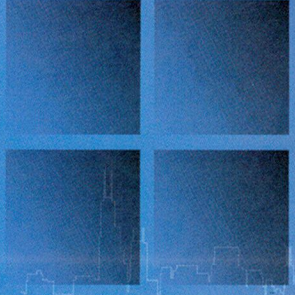
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Mile markers on the I&M Canal State Trail begin at mile 95 and end at mile 96. These numbers represent the approximate distance of the historic canal from its origins in Chicago to its end at LaSalle/Peru.



I&M CANAL PASSAGE
WATERWAYS THAT BUILT CHICAGO

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Location: 51.13

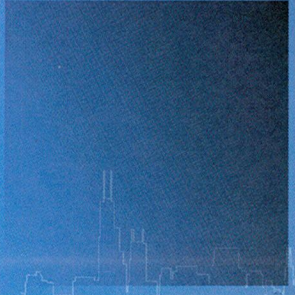
Old Grain Elevator

You are walking in the footsteps of people who have traveled this passageway for centuries. Native Americans first used the waterways for trade. They showed their routes to French explorers, the first to dream of building a canal.

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Location: 52.03

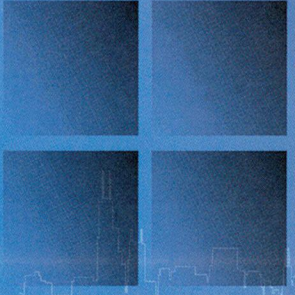
Old Swing Bridge

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Location: 53.05


Stagecoach Canal and Rail Travel

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Location: 54.01

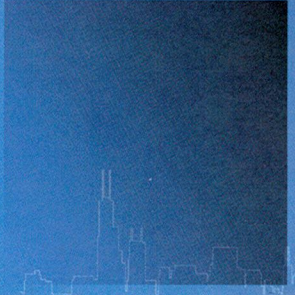
Transportation at Split Rock

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Location: 58.05

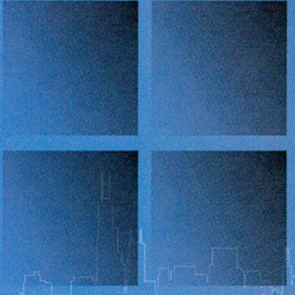
Split Rock Geology

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Location: 58.06

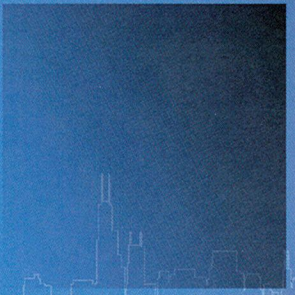
North Meets South

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Location: 58.16


Rough and Ready

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Location: 58.17

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DATE



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Sheet
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Cantilever Panels

Location - Trailwide

Little Vermillion Aqueduct

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Mile markers on the I&M Canal State Trail begin at mile 50 and end at mile 160. These numbers represent the approximate distance of the historic canal from its origins in Chicago to its end at LaSalle/Penn.

I&M CANAL PASSAGE

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Location: 58.20

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