

PROPOSED HIGHWAY PLANS

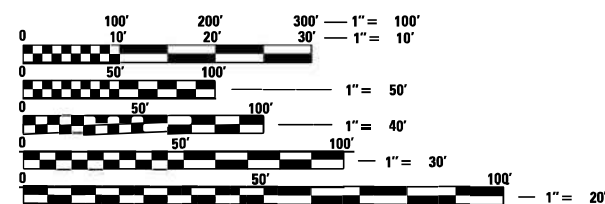
**FAU ROUTE 2714 (25TH AVENUE)
OVER INDIANA HARBOR BELT RR (IHB RR)
SECTION: 2020-203-BR
PROJECT: STP-CV3Y(313)
BRIDGE DECK OVERLAY AND JOINT REPAIR
COOK COUNTY**

C-91-001-21

END IMPROVEMENT

STA 45 + 95.59

25TH AVENUE
OVER IHB RR
SN 016-1011



BEGIN IMPROVEMENT
STA 29+52

LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH = 1,449 FT. = 0.27 MILES
NET LENGTH = 1,449 FT. = 0.27 MILES



 10/6/2020
PHILIP MINGA, P.E.
EXPIRES: 11/30/2021

SEE
11 BURLINGTON AVE., SUITE 111 - LISLE,
License No. 184 005040



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED November 13, 2020
Andy A. Rungtong 1003
REGIONAL ENGINEER
December 4, 2020
S. E. A. E. K.
ENGINEER OF DESIGN AND ENVIRONMENT
December 4, 2020
Gernot, Gun
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION ¹³

CONTRACT NO. 62M80

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OF THE STATE OF ILLINOIS

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HIGHWAY STANDARDS

STD NO.	TITLE
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
420001-09	PAVEMENT JOINTS
515001-04	NAME PLATE FOR BRIDGES
602011-02	CATCH BASIN TYPE C
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701321-18	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

1.

ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.
2.

THE AREA TO BE TACKED OR PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAY'S PRODUCTION, BUT NO MORE THAN FIVE (5) DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
3.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTING AND MAINTAINING AN ELECTRONIC LOG OF ALL STAKEOUT SURVEY THAT IS PERFORMED ON THE JOB, EITHER BY HIM/HER OR ANY SUB-CONTRACTOR PERFORMING THE STAKEOUT. UPON REQUEST, ALL LOGS SHALL BE SUBMITTED TO THE DEPARTMENT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK, BUT SHALL BE CONSIDERED INCLUDED IN THE COST FOR CONSTRUCTION LAYOUT.
4.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE LATEST EDITION.
5.

THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- EMAD ALHUSSEINI - emad.alhusseini@illinois.gov
6.

THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (1-800-892-0123) 48 HOURS PRIOR TO ANY WORK IN THE RIGHT OF WAY OR EASEMENTS TO LOCATE UTILITIES, AND CONTACT THE OWNER'S REPRESENTATIVE SHOULD PUBLIC UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENTS.
7.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
8.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
9.

THE CONTRACTOR SHALL TAKE CARE TO PROTECT ALL SIGNS ALONG THE ROUTE OF CONSTRUCTION. SIGNS SHALL BE REMOVED IF THEY ARE IN CONFLICT WITH PROPOSED WORK, AND APPROVED BY ENGINEER RED AND ALL LABOR NECESSARY TO COMPLY WITH THE RELOCATION OF SIGNS SHALL BE INCLUDED IN THE CONTRACT WITHOUT ANY EXTRA COMPENSATION ALLOWED TO THE CONTRACTOR ACCORDING TO ART. 107.25, UNLESS MARKED ON PLANS.
10.

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.
11.

ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
12.

ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
13.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 V:H.
14.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
15.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

16.

THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW OR WASTE/USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR WILL NEED TO SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
17.

ANY EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED.
18.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGES OF MELROSE PARK AND FRANKLIN PARK, AND THE INDIANA HARBOR BELT RR.
19.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

COMMONWEALTH EDISON CO.
ATTN:
1919 SWIFT ROAD
OAK BROOK, IL 60523
630-424-5704

NICOR GAS
ATTN:
1844 FERRY ROAD
NAPERVILLE, IL 60563
630-388-3830

20.

A FULL DEPTH SAWCUT OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR REQUIRED BY THE RESIDENT ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

21.

THE CONTRACTOR SHALL CONTACT THE D1 TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNON-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

COMMITMENTS:

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 GYR	QC/QA
HOT-MIX ASHPALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.5"	4% @ 70 GYR	QC/QA
HOT-MIX ASHPALT BINDER COURSE, IL-9.5, N70, 1.5"	4% @ 70 GYR	QC/QA
TEMPORARY RAMP		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.75"	4% @ 70 GYR	QC/QA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)

1.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
2.

FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
3.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

REV-SEP

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS,
GENERAL NOTES, COMMITMENTS, AND HMA MIX TABLE

SCALE: 2.0000 ' / in. SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62M80	
		ILLINOIS	FED. AID PROJECT	

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SP	CODE NO.	ITEM	UNIT	CONSTRUCTION CODE	
				BRIDGE	
				0059 80% FEDERAL 20% STATE 25th Avenue SN 016-1011	
				URBAN	
	31101100	SUBBASE GRANULAR MATERIAL , TYPE B	CU YD	1	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	446	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	351	
	40600990	TEMPORARY RAMP	SQ YD	93	
	40602985	HOT-MIX ASHPALT BINDER COURSE , IL-9.5 , N70	TON	20	
	40604062	HOT-MIX ASPHALT SURFACE COURSE , IL-9.5 , MIX "D" , N70	TON	94	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	594	
	44000161	HOT-MIX ASPHALT SURFACE REMOVAL , 3"	SQ YD	215	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	72	
	44000600	SIDEWALK REMOVAL	SQ FT	594	
	44003100	MEDIAN REMOVAL	SQ FT	1219	
	50102400	CONCRETE REMOVAL	CU YD	69.6	
	50157300	PROTECTIVE SHIELD	SQ YD	8867	
	50300225	CONCRETE STRUCTURES	CU YD	1	
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	77.8	
	50300260	BRIDGE DECK GROOVING	SQ YD	6683	
	50300300	PROTECTIVE COAT	SQ YD	7333	
	50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	25080	
	50800205	REINFORCEMENT BARS , EPOXY COATED	POUND	11070	
	50800515	BAR SPLICERS	EACH	98	
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	460	
	59000200	EPOXY CRACK INJECTION	FOOT	24	
	59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	3	
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	2	
	60255500	MANHOLES TO BE ADJUSTED	EACH	1	

SP	CODE NO.	ITEM	UNIT	CONSTRUCTION CODE	
				BRIDGE	
				0059 80% FEDERAL 20% STATE 25th Avenue SN 016-1011	
				URBAN	
	60603800	COMBINATION CONCRETE CURB AND GUTTER , TYPE B-6.12	FOOT	72	
	67000400	ENGINEER'S FIELD OFFICE , TYPE A	CAL MO	12	
	67100100	MOBILIZATION	L SUM	1	
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	195	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	390	
	70300900	PAVEMENT MARKING TAPE , TYPE IV - LETTERS AND SYMBOLS	SQ FT	296	
	70300904	PAVEMENT MARKING TAPE , TYPE IV 4"	FOOT	18779	
	70300906	PAVEMENT MARKING TAPE , TYPE IV 6"	FOOT	2418	
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	1825	
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1713	
	70600305	IMPACT ATTENUATORS , TEMPORARY (FULLY REDIRECTIVE , RESETTABLE) , TEST LEVEL 3	EACH	1	
	70600330	IMPACT ATTENUATORS , RELOCATE (FULLY REDIRECTIVE) , TEST LEVEL 3	EACH	1	
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	182	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3365	
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	873	
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	427	
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	85	
*	78008310	POLYUREA PAVEMENT MARKING TYPE II - LINE 4"	FOOT	3270	
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4	
*	78100300	REPLACEMENT REFLECTOR	EACH	111	
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	72	
	X0322102	TEMPORARY SIDEWALK RAMP	EACH	1	
	X0325749	FIBER WRAP	SQ FT	377	
	X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	113	
..	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2930	



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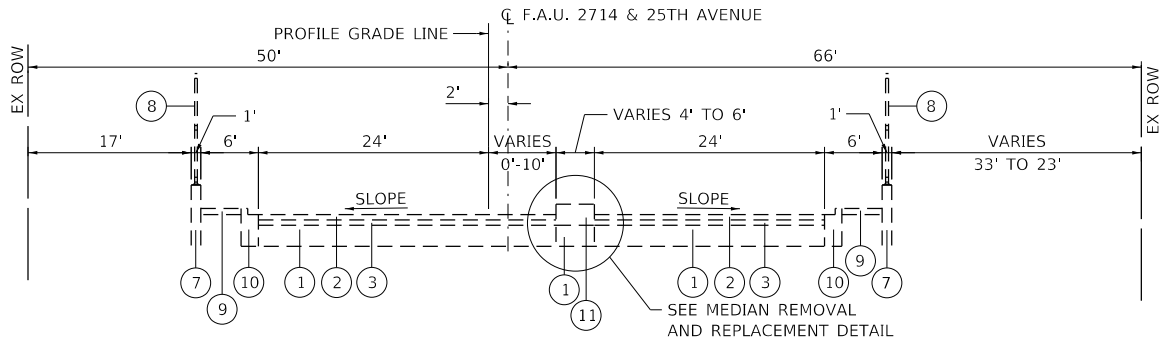
SUMMARY OF QUANTITIES

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* SPECIALTY ITEM REV-SEP

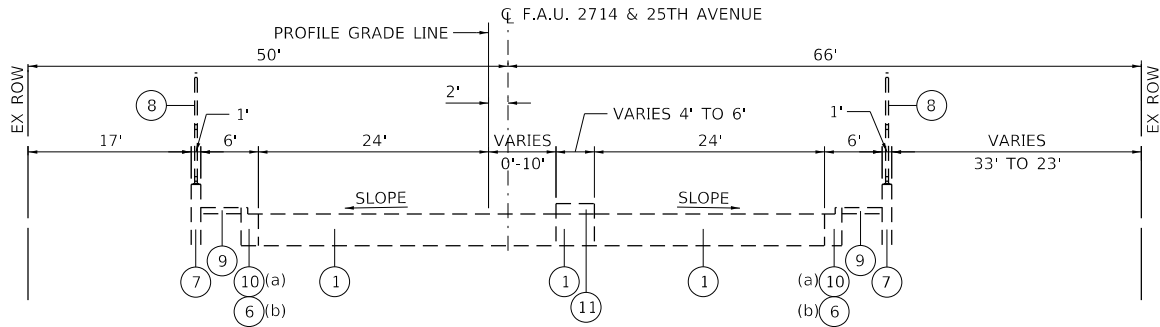
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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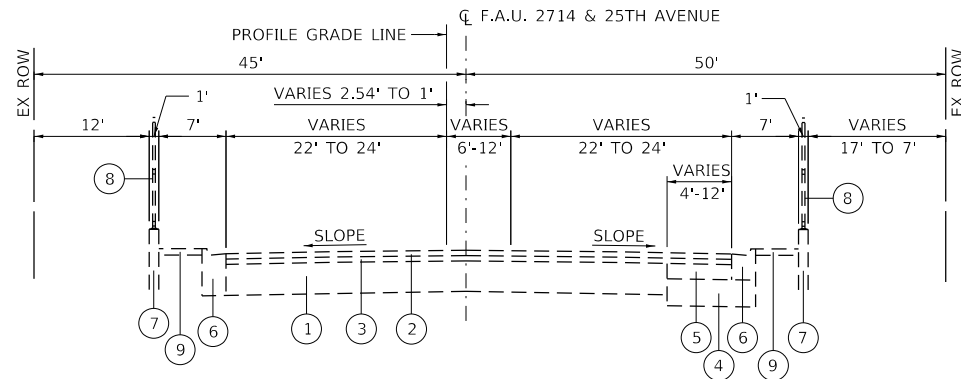
EXISTING TYPICAL SECTION

STA 29+51.8 TO STA 31+87.75



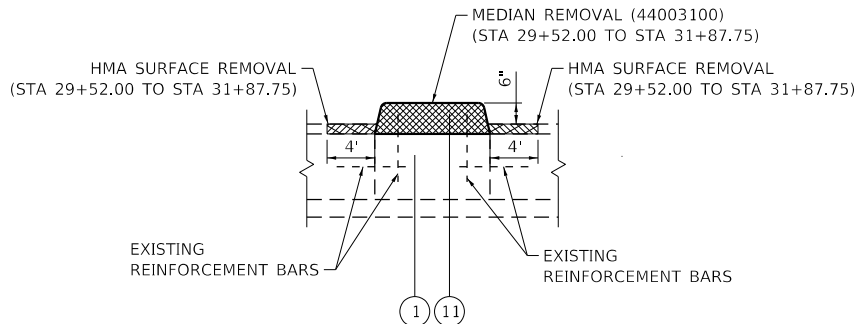
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STA. 31+87.75 TO STA. 31+93.75 (PCC CONNECTOR PAVEMENT)(a)
STA. 31+93.75 TO STA. 32+23.75 (BRIDGE APPROACH PAVEMENT)
STA. 32+23.75 TO STA. 45+29.59 (BRIDGE OMISSION)
STA. 45+29.59 TO STA. 45+59.59 (BRIDGE APPROACH PAVEMENT)
STA. 45+59.59 TO STA. 45+65.59 (PCC CONNECTOR PAVEMENT)(b)



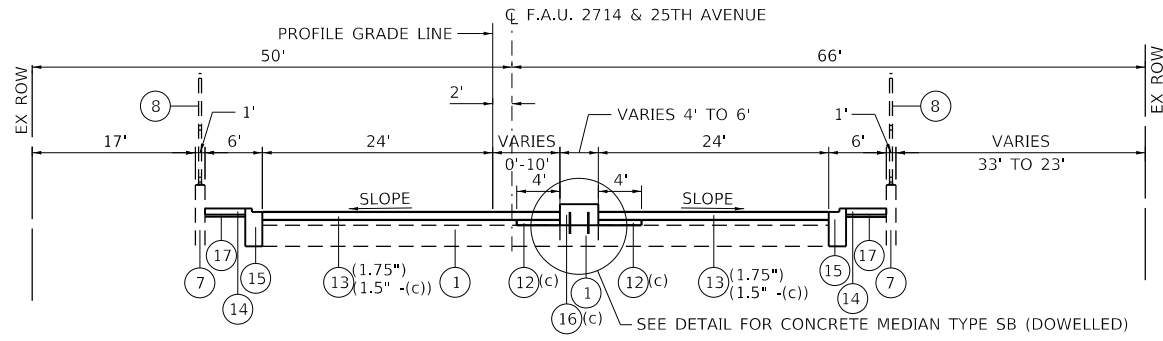
EXISTING TYPICAL SECTION

STA. 45+65.59 TO STA. 49+54.67



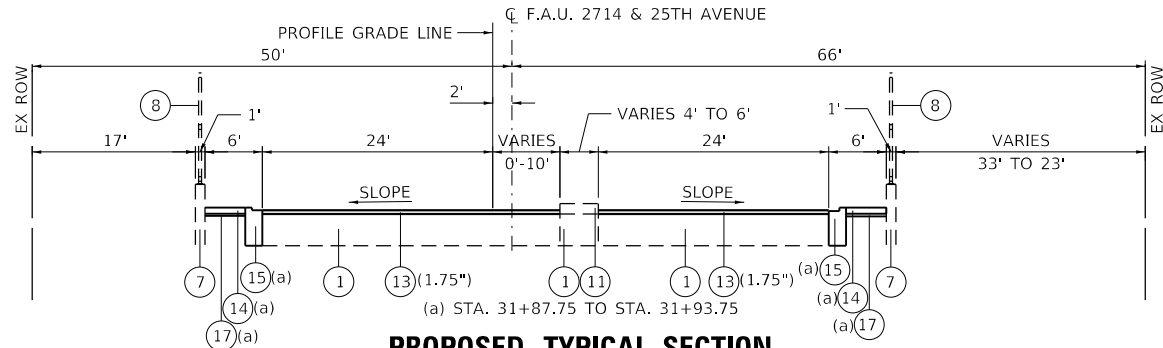
MEDIAN REMOVAL DETAIL

(SEE DETAILS FOR CONCRETE MEDIAN TYPE SB (DOWELLED)
FOR REPLACEMENT DURING POST-STAGE CONSTRUCTION)



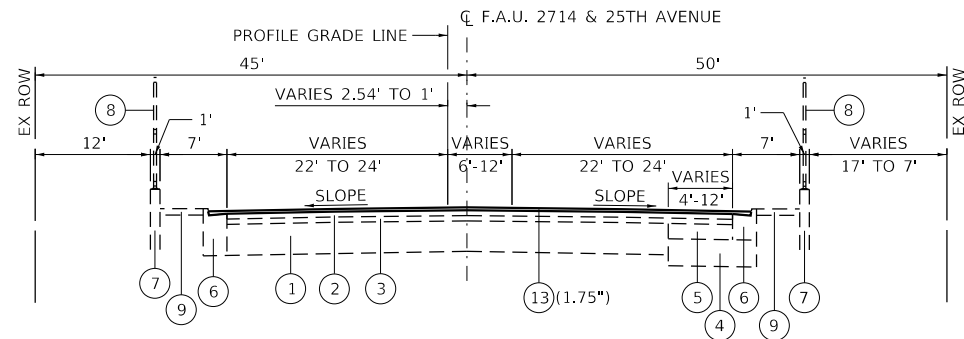
PROPOSED TYPICAL SECTION

STA 31+57.75 TO STA 31+87.75



PROPOSED TYPICAL SECTION

STA. 31+87.75 TO STA. 31+93.75 (PCC CONNECTOR PAVEMENT)
STA. 31+93.75 TO STA. 32+23.75 (BRIDGE APPROACH PAVEMENT)
STA. 32+23.75 TO STA. 45+29.59 (BRIDGE OMISSION)
STA. 45+29.59 TO STA. 45+59.59 (BRIDGE APPROACH PAVEMENT)
STA. 45+59.59 TO STA. 45+65.59 (PCC CONNECTOR PAVEMENT)



PROPOSED TYPICAL SECTION

STA. 45+65.59 TO STA. 45+95.59

LEGEND

- | | |
|---|---|
| ① EXISTING PCC PAVEMENT, 10" | ⑩ EXISTING COMBINATION CONC. CURB AND GUTTER, TYPE B-6.12 |
| ② EXISTING HMA SURFACE COURSE, 1½" | ⑪ EXISTING CONCRETE MEDIAN TYPE SB |
| ③ EXISTING HMA BINDER COURSE, 1½" | ⑫ HMA BINDER COURSE, IL-9.5, N70 (1.5 INCHES) |
| ④ EXISTING AGGREGATE SUBGRADE, 12" | ⑬ HMA SURFACE COURSE, IL-9.5 MIX D N70 (1.5 INCHES) |
| ⑤ EXISTING HMA BASE COURSE, 7¾" | ⑭ PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH |
| ⑥ EXISTING COMBINATION CONC. CURB AND GUTTER, TYPE B-6.24 | ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 |
| ⑦ EXISTING RETAINING WALL | ⑯ CONCRETE MEDIAN TYPE SB (DOWELLED) |
| ⑧ EXISTING BRIDGE RAILING | ⑰ SUBBASE GRANULAR MATERIAL-TYPE B |
| ⑨ EXISTING PCC SIDEWALK | |



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DATE - 08/28/2020

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: 20.0000 ' / in. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	5
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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DATE	-	08/28/2020

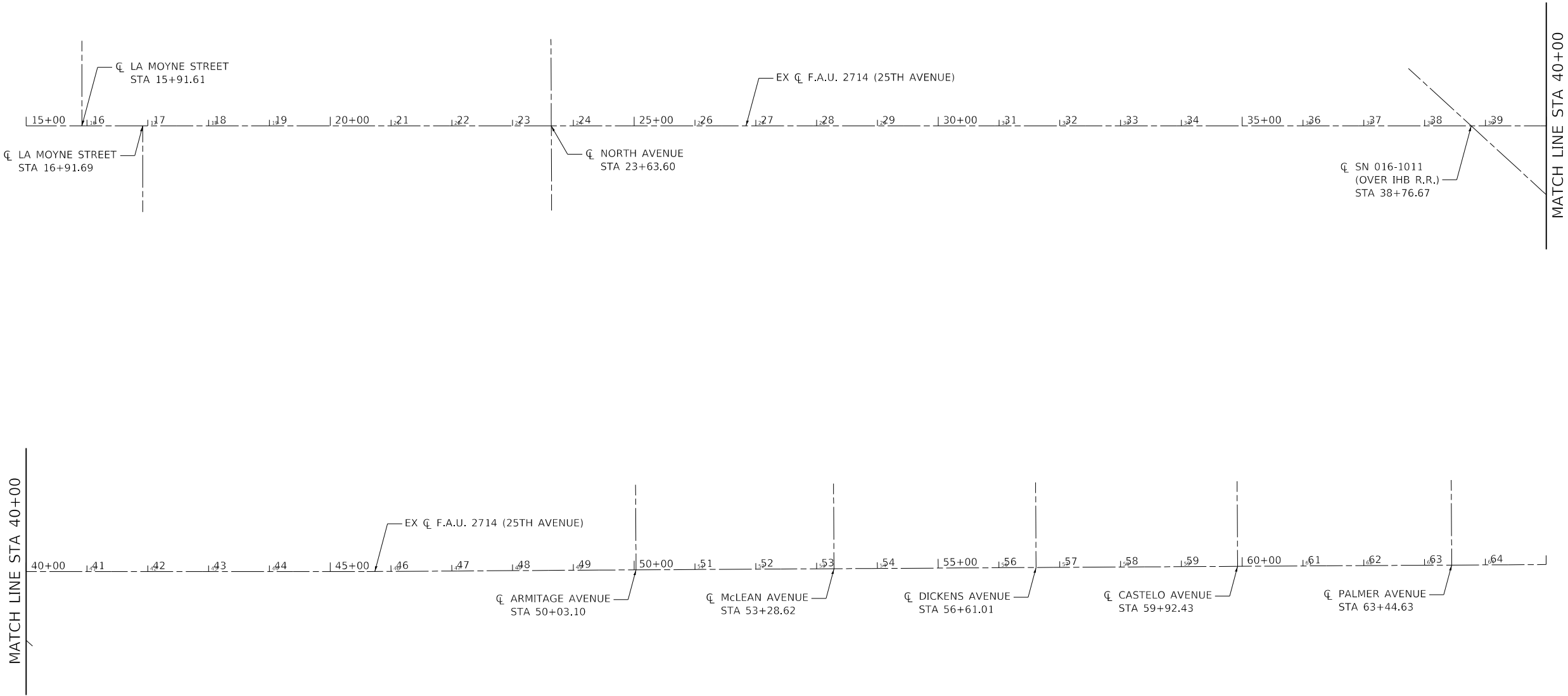
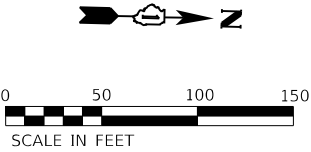
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES, AND BENCHMARKS

SCALE: 200.0000 ' / in. SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	6
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	



- BENCHMARKS (FROM SEPTEMBER 1993):
- "X" CUT ON WEST BOLT OF FIRE HYDRANT LOCATED IN N.W. CORNER OF 25TH AVENUE AND COSTELLO AVENUE
ELEV. 639.18
 - R.R. SPIKE IN SOUTH FACE OF UTILITY POLE LOCATED IN N.E. QUADRANT OF 25TH AVENUE AND ARMITAGE AVENUE INTERSECTION
ELEV. 637.23

MAINTENANCE OF TRAFFIC GENERAL NOTES

1.

TRAFFIC CONDITIONS, CRASHES, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE OF THE MAINTENANCE OF TRAFFIC ITEM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
2.

THE CONTRACTOR IS DIRECTED TO THE FACT THAT OTHER SEPARATE CONTRACTS ARE, OR MAY BE, IN FORCE THAT INTERSECT THE LIMITS OF THIS PROJECT. THE CONTRACTOR SHALL COOPERATE WITH THE OTHER CONTRACTORS IN THE PHASING AND PERFORMANCE OF THIS WORK SO AS NOT TO DELAY, INTERRUPT, OR HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY OTHER CONTRACTORS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR COMPLIANCE WITH THE ABOVE REQUIREMENTS, NOR FOR ANY DELAYS OR INCONVENIENCES RESULTING FROM THE ACTIVITIES OF OTHER CONTRACTORS. SHOULD A CONFLICT ARISE BETWEEN THE CONTRACTORS WITH RESPECT TO SEQUENCE OF CONSTRUCTION OR MAINTENANCE OF TRAFFIC REQUIREMENTS, SAID CONFLICTS SHALL BE RESOLVED BY, OR AT THE DIRECTION OF THE ENGINEER.
3.

THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL. THE ENGINEER SHALL BE INFORMED IN WRITING A MINIMUM OF 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MAINTENANCE OF TRAFFIC PLANS.
4.

THE CONTRACTOR SHALL REMOVE ALL TEMPORARY OR EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING. REMOVAL OF PAVEMENT MARKING TAPE WILL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING REMOVAL" (X7030005). REMOVAL OF PAVEMENT MARKINGS ON PERMANENT PAVEMENT WILL BE PAID FOR AS "PAVEMENT MARKING REMOVAL - WATER BLASTING" (X0327980).
5.

THE CONTRACTOR SHALL PROVIDE 48 HOURS ADVANCE NOTICE TO THE ENGINEER OF ANY CONSTRUCTION WORK THAT MAY IMPACT ANY ROADWAY LIGHTING.
6.

THE CONTRACTOR SHALL PROPERLY DISPOSE OF ANY EXISTING SIGNS REMOVED, BUT NOT RELOCATED, FROM THE PROJECT. THE CONTRACTOR SHALL PROPERLY STORE RELOCATED SIGNS AS APPROVED BY THE ENGINEER UNTIL THEY ARE PROPERLY RE-ERECTED.
7.

THE FOLLOWING IS A LIST OF MAINTENANCE OF TRAFFIC ASSOCIATED PAY ITEMS FOR WHICH ESTIMATED QUANTITIES HAVE BEEN PROVIDED. THE CONTRACTOR SHALL RECEIVE WRITTEN AUTHORIZATION FROM THE ENGINEER PRIOR TO USE AND PAYMENT FOR THESE ITEMS:

- *PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS
- *PAVEMENT MARKING TAPE, TYPE IV 4"
- *PAVEMENT MARKING TAPE, TYPE IV 6"
- *TEMPORARY CONCRETE BARRIER
- *RELOCATE TEMPORARY CONCRETE BARRIER
- *CHANGEABLE MESSAGE SIGN
- *IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- *IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 3

MAINTENANCE OF TRAFFIC STAGING DESCRIPTION

PRE-STAGE

CONSTRUCTION:

PRE-STAGE INCLUDES REMOVAL OF MEDIAN CURB AND PLACING TEMPORARY PAVEMENT.

TRAFFIC:

LANE CLOSURE OF INSIDE LANE IN EACH DIRECTION.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

STAGE I

CONSTRUCTION:

BRIDGE REPAIRS, PLACE TEMPORARY RAMP, PAVEMENT MARKING ON NORTHBOUND 25TH AVENUE AND OFF-RD IMPROVEMENTS.

TRAFFIC:

LANE CLOSURE OF 1 LANE IN EACH DIRECTION AND MEDIAN CROSSOVER OF NB TRAFFIC.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

TEMPORARY CONCRETE BARRIER AND DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

TUBULAR MARKERS WILL SEPARATE OPPOSING DIRECTIONS OF TRAFFIC.

STAGE II

CONSTRUCTION:

BRIDGE REPAIRS, PLACE TEMPORARY RAMP, PAVEMENT MARKING ON SOUTHBOUND 25TH AVENUE AND OFF-RD IMPROVEMENTS.

TRAFFIC:

LANE CLOSURE OF 1 LANE IN EACH DIRECTION AND MEDIAN CROSSOVER OF SB TRAFFIC.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

TEMPORARY CONCRETE BARRIER AND DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

TUBULAR MARKERS WILL SEPARATE OPPOSING DIRECTIONS OF TRAFFIC.

POST-STAGE

CONSTRUCTION:

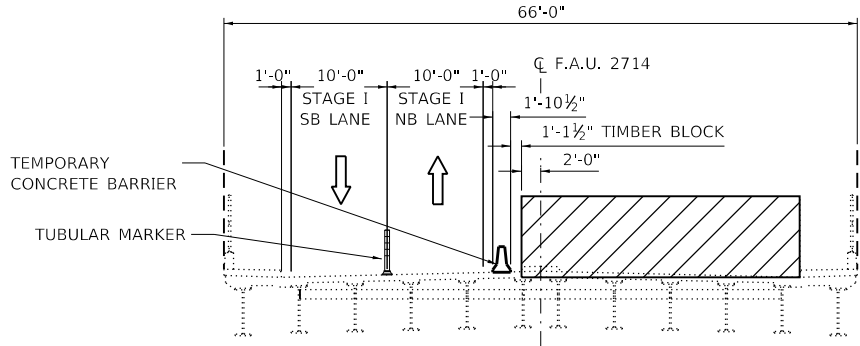
POST-STAGE INCLUDES HMA SURFACE REMOVAL, HMA SURFACE REMOVAL - BUTT JOINT, TEMPORARY RAMP REMOVAL, CONSTRUCTION OF CONCRETE MEDIAN, HMA BINDER COURSE, HMA SURFACE COURSE, AND HMA OVERLAY.

TRAFFIC:

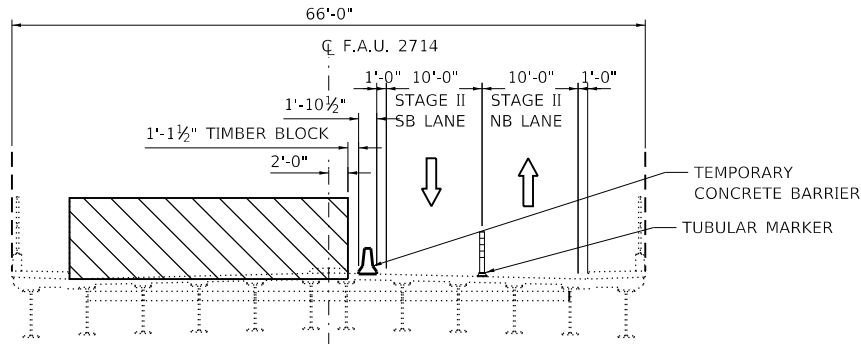
LANE CLOSURE OF INSIDE LANE IN EACH DIRECTION.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

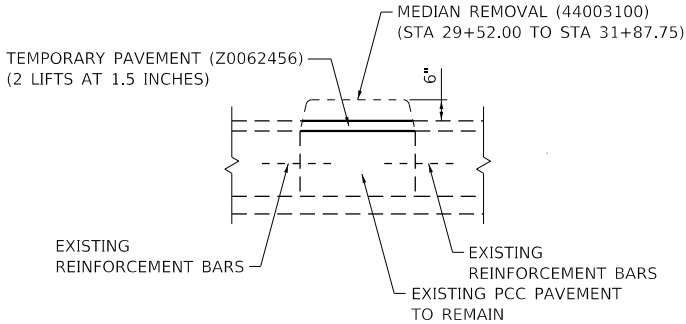
DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.



ROADWAY TYPICAL SECTION – STAGE I



ROADWAY TYPICAL SECTION – STAGE II



TEMPORARY PAVEMENT DETAIL

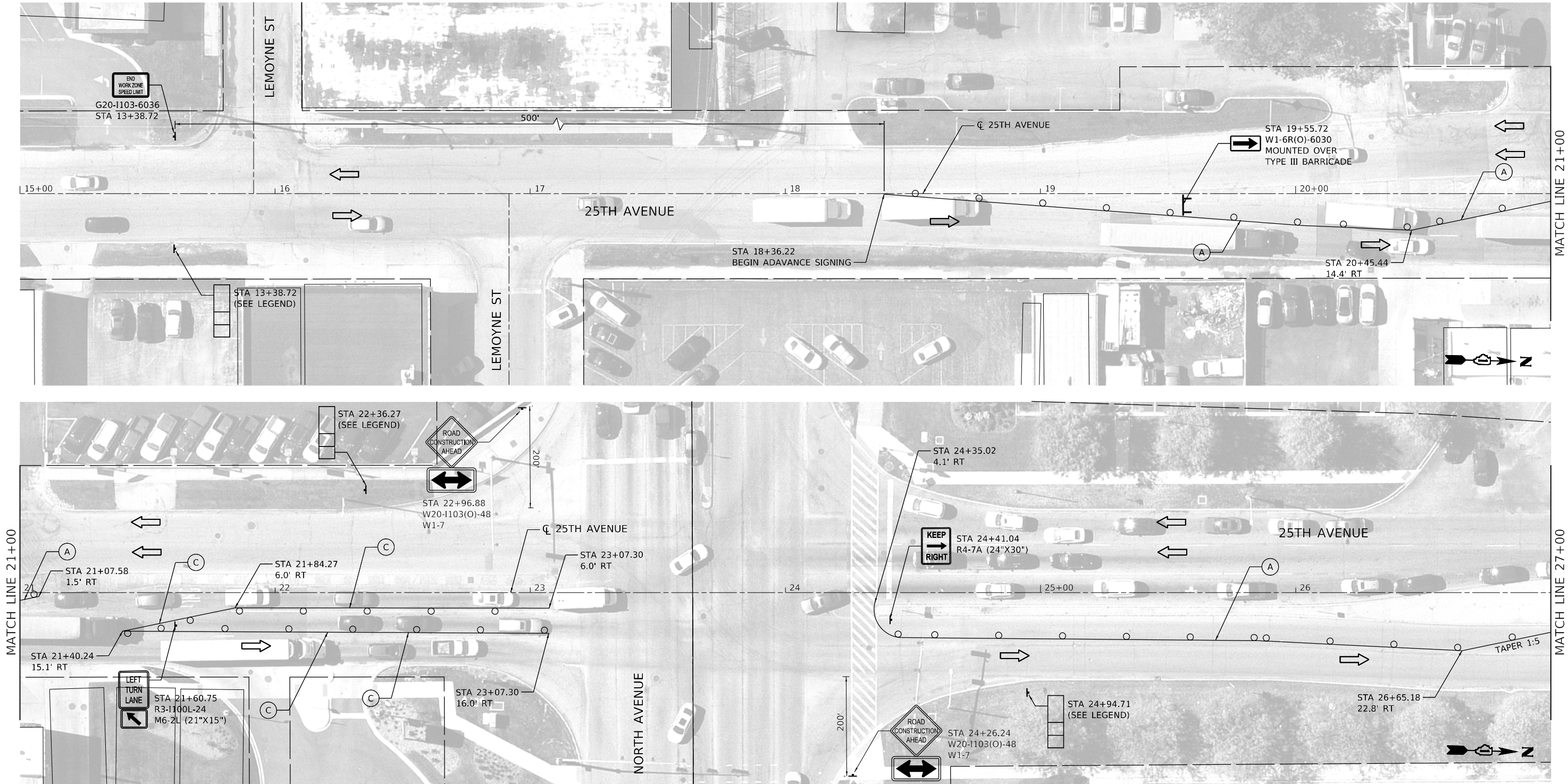
(SEE TYPICALS FOR MEDIAN REMOVAL DETAIL)

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CHECKED -	JWD	REVISED -	
DATE -	08/28/2020	REVISED -	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	7
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	

USER: idave
MODEL: Default
FILE NAME: PLOT_209_000_IDOT_PTB_190_14 Phase II Var-Var-Design\Work_Order_141CADD\CADD_Sheets\162M80-SHT-PR-POST-STAGE-P1.dgn



ADVANCE SIGNING DETAIL



* THIS SIGN SHALL BE
TAKEN DOWN WHEN
WORKERS ARE NOT
PRESENT

BEGIN SHIFT
STA 18+36.22

LEGEND



WORK ZONE



DIRECTION OF TRAFFIC



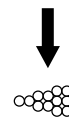
DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES



TRAFFIC SIGN



TYPE III BARRICADE WITH FLASHING
MONODIRECTIONAL LIGHTS



ARROW BOARD



IMPACT ATTENUATOR,
TEMPORARY (FULLY
REDIRECTIVE, RESETTABLE),
TEST LEVEL 3



WORK ZONE, SPEED LIMIT
PHOTO ENFORCED W/FINE
(SEE RIGHT)



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
(B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
(C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
(D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND
SYMBOLS, WHITE



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DATE - 08/28/2020

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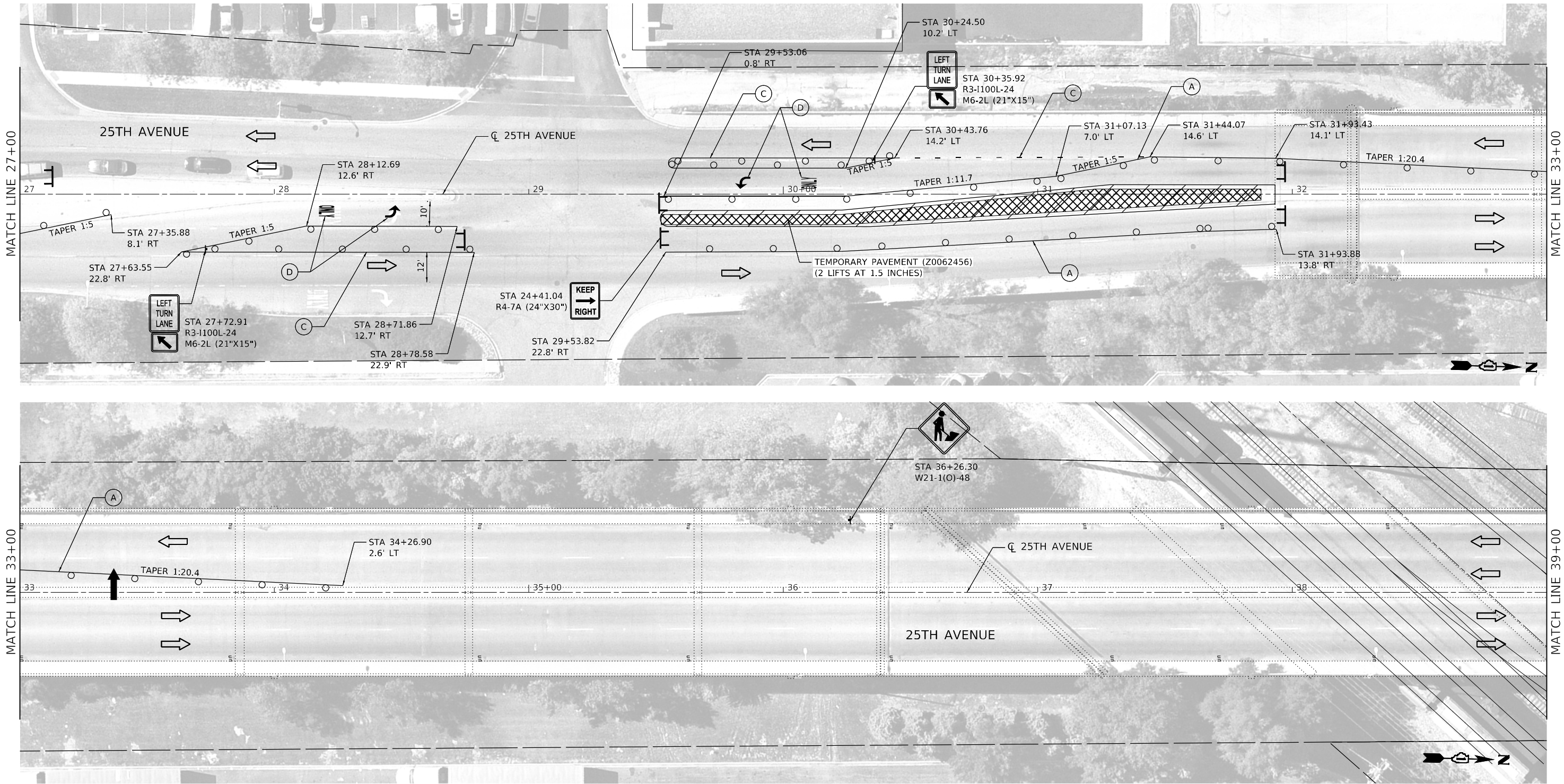
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 2 OF 14 SHEETS STA. 15+00 TO STA. 27+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	8
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: idrive
MODEL: Default
FILE NAME: PLOT_209_004_IDOT_PTB_190_14 Phase II Var-Var-Design\Work_Order_141CADD\CADD_Sheets\162M80-SHT-PRE-POST-STAGE-P2.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 3
- WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)
- WORK ZONE SPEED LIMIT 35
- PHOTO ENFORCED
- \$250 FINE MINIMUM

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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DATE - 08/28/2020

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REVISED -

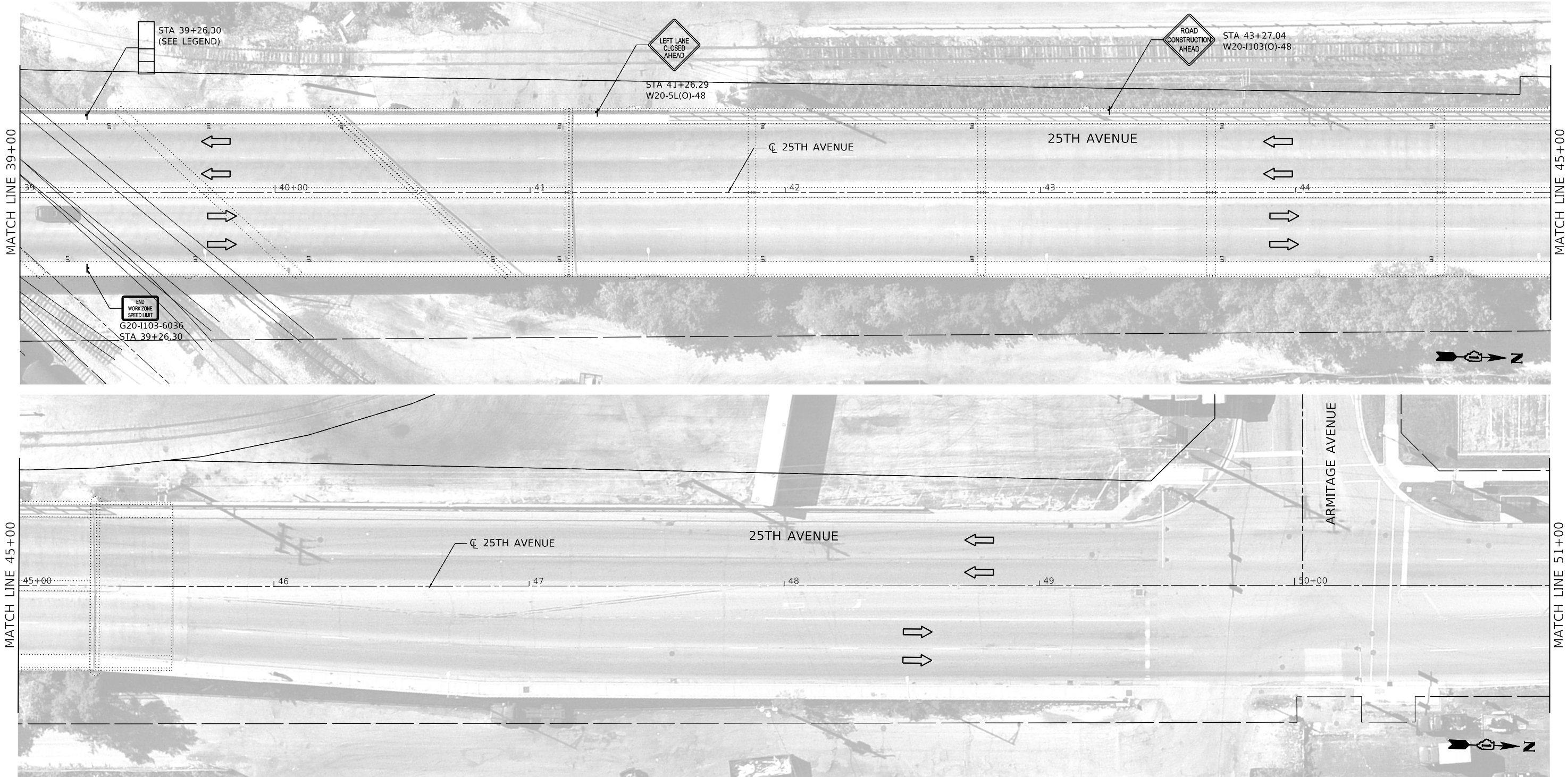
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 3 OF 14 SHEETS STA. 27+00 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	9
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- | | | | | |
|--|---|--|--|--|
| | WORK ZONE | | ARROW BOARD | |
| | DIRECTION OF TRAFFIC | | IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 3 | |
| | DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES | | WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT) | |
| | TRAFFIC SIGN | | | |
| | TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS | | | |

PAVEMENT MARKING LEGEND

- | | |
|-----|---|
| (A) | PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW |
| (B) | PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE |
| (C) | PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE |
| (D) | PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE |



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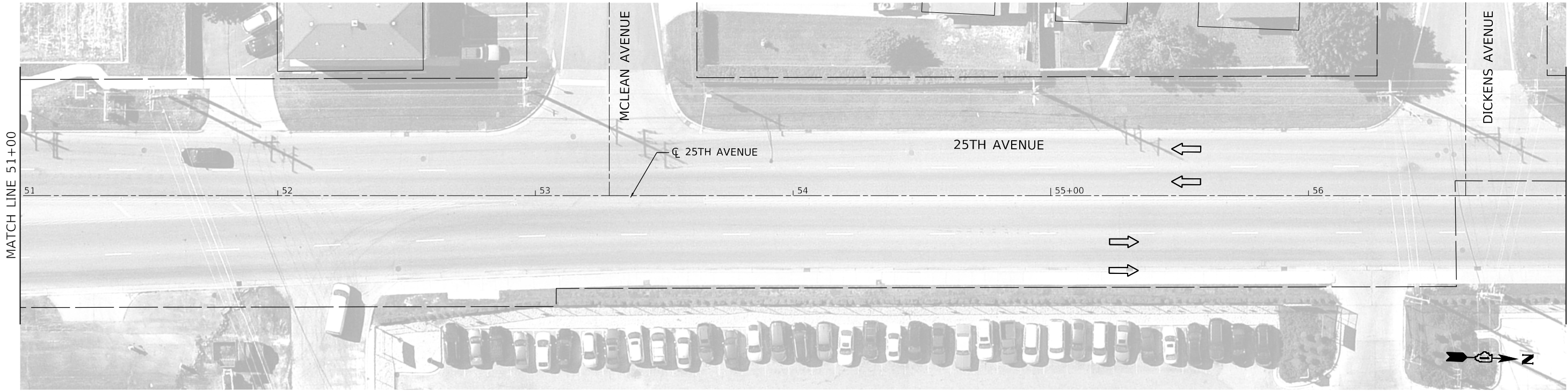
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 4 OF 14 SHEETS STA. 39+00 TO STA. 51+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	10
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND



WORK ZONE



DIRECTION OF TRAFFIC



DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES



TRAFFIC SIGN



TYPE III BARRICADE WITH FLASHING
MONODIRECTIONAL LIGHTS



ARROW BOARD



IMPACT ATTENUATOR,
TEMPORARY (FULLY
REDIRECTIVE, RESETTABLE),
TEST LEVEL 3



WORK ZONE, SPEED LIMIT
PHOTO ENFORCED W/FINE
(SEE RIGHT)



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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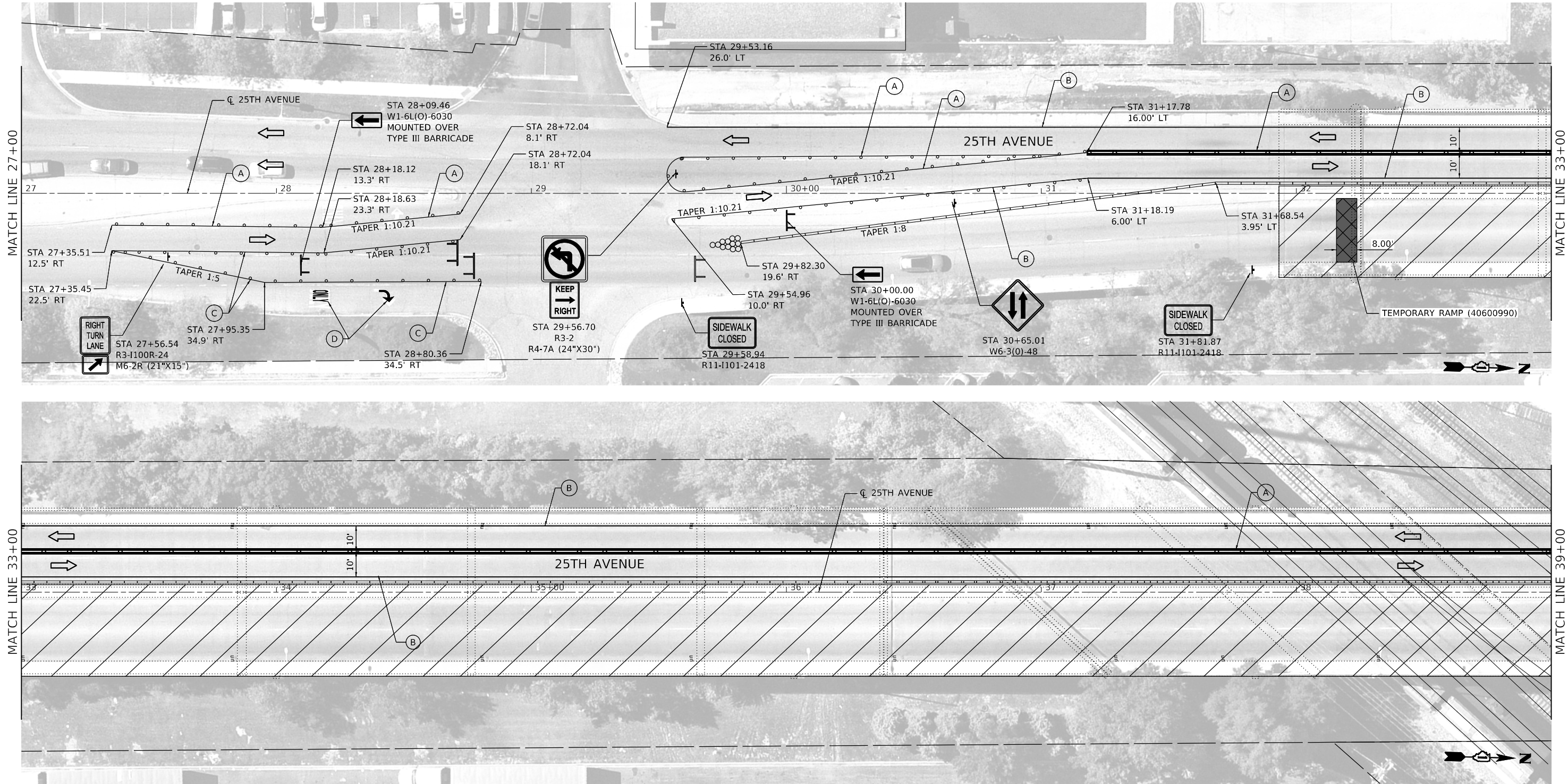
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 5 OF 14 SHEETS STA. 51+00 TO STA. 57+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	11
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TUBULAR MARKS (50' C-C-)
- ARROW BOARD
- IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 3
- WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE
SPEED LIMIT
35
PHOTO ENFORCED
\$250 FINE MINIMUM

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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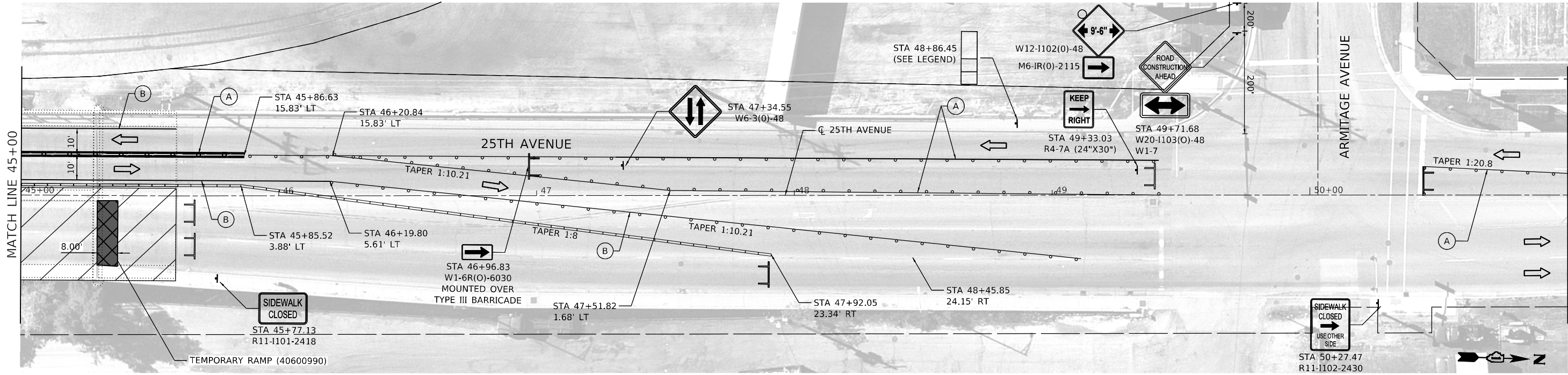
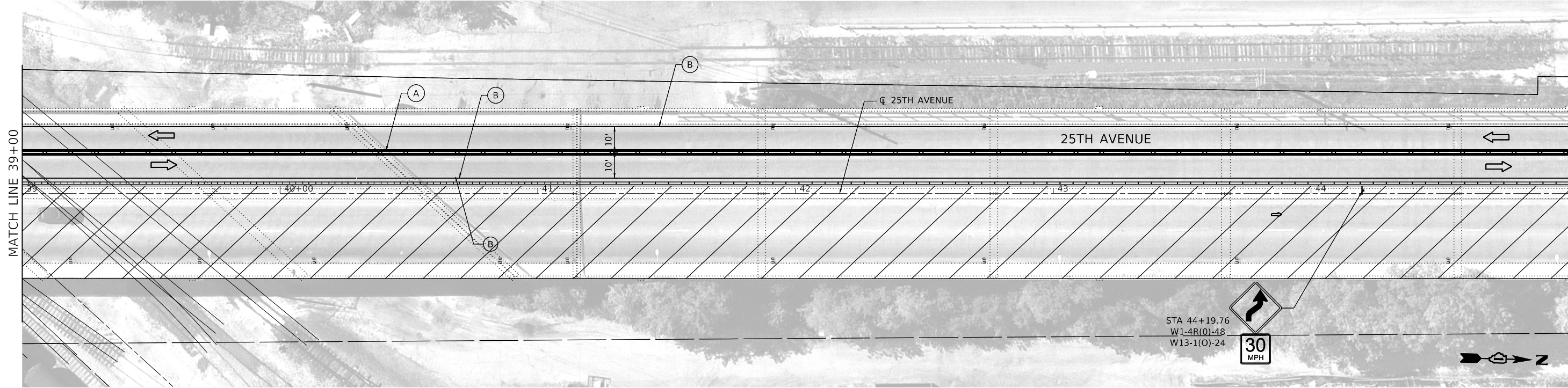
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC STAGE I

SCALE: 40.0000 ' / in. SHEET 7 OF 14 SHEETS STA. 27+00 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	13
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

- TUBULAR MARKS (50' C-C-)
- ARROW BOARD
- IMPACT ATTENUATOR,
TEMPORARY (FULLY
REDIRECTIVE, RESETTABLE),
TEST LEVEL 3
- WORK ZONE, SPEED LIMIT
PHOTO ENFORCED W/FINE
(SEE RIGHT)

WORK
ZONE
SPEED
LIMIT
35
PHOTO
ENFORCED
\$250 FINE
MINIMUM

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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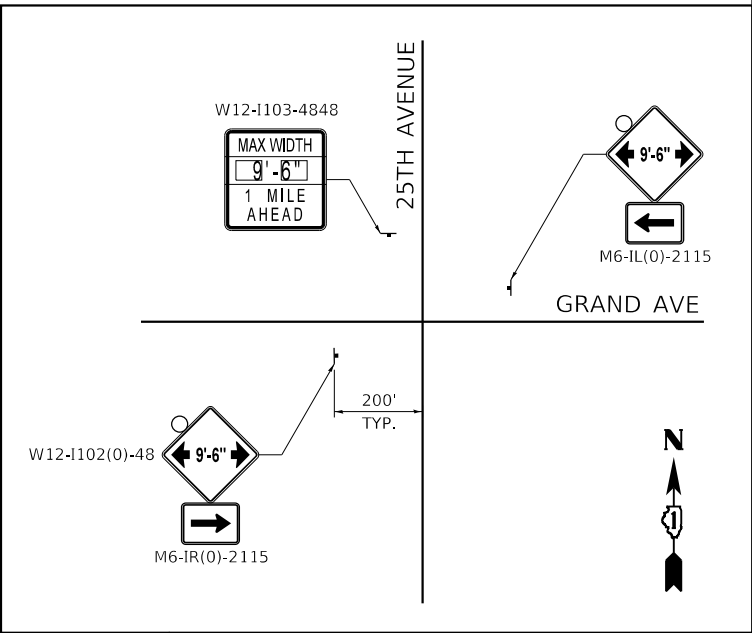
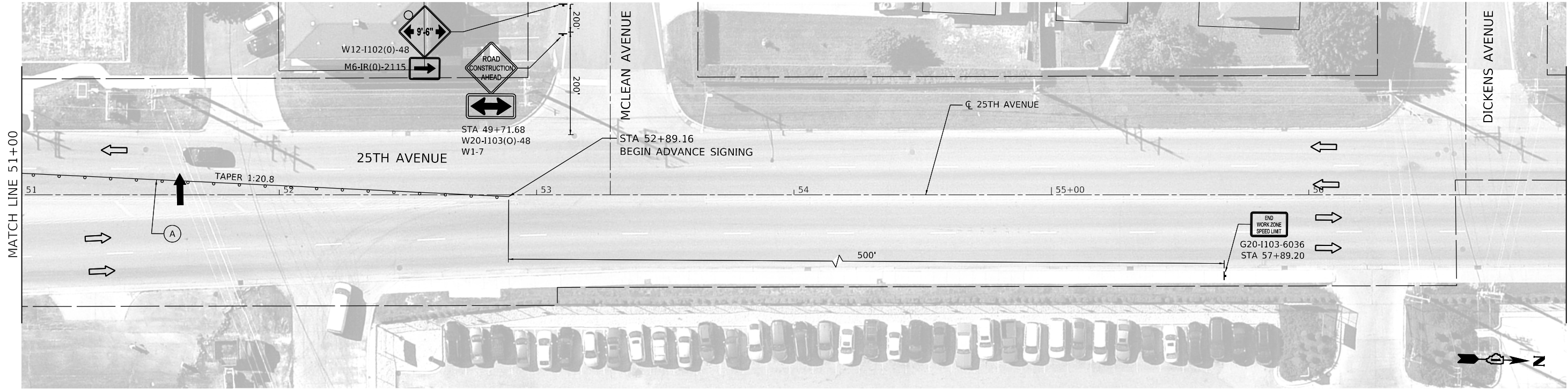
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE I

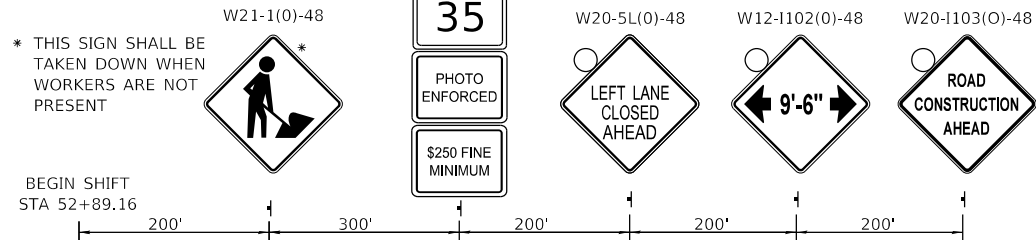
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	14
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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ADVANCE SIGNING DETAIL



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- ARROW BOARD
- IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 3
- WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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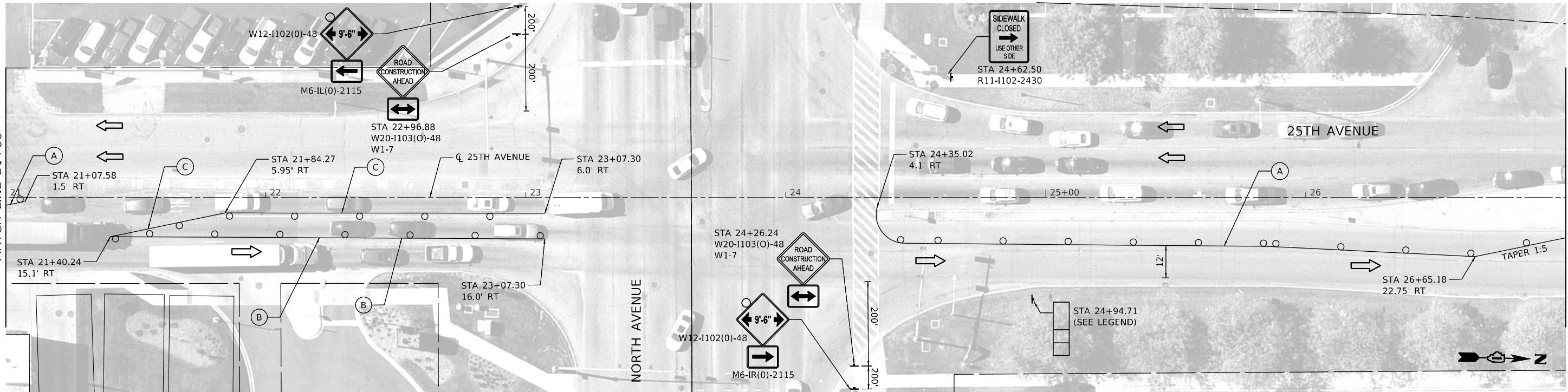
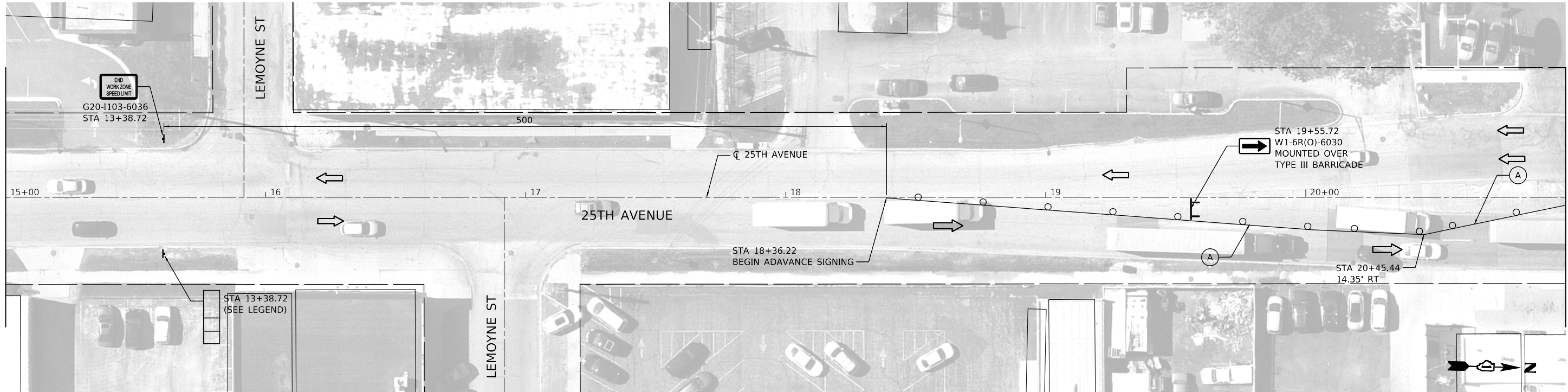
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC STAGE I

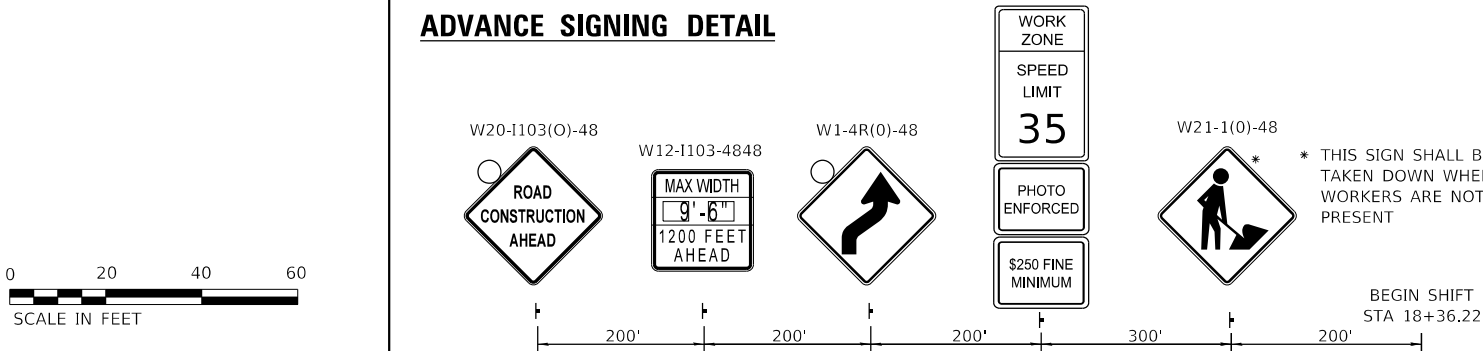
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F.A. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

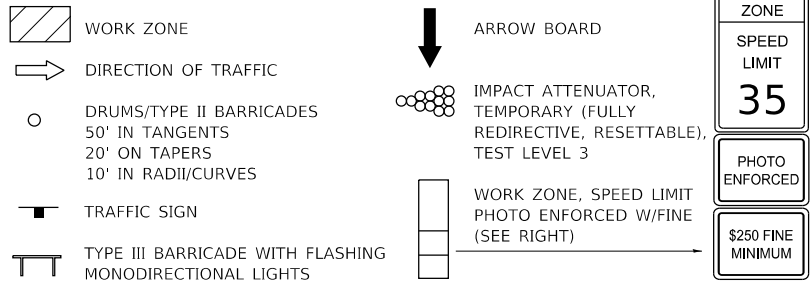
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ADVANCE SIGNING DETAIL



LEGEND



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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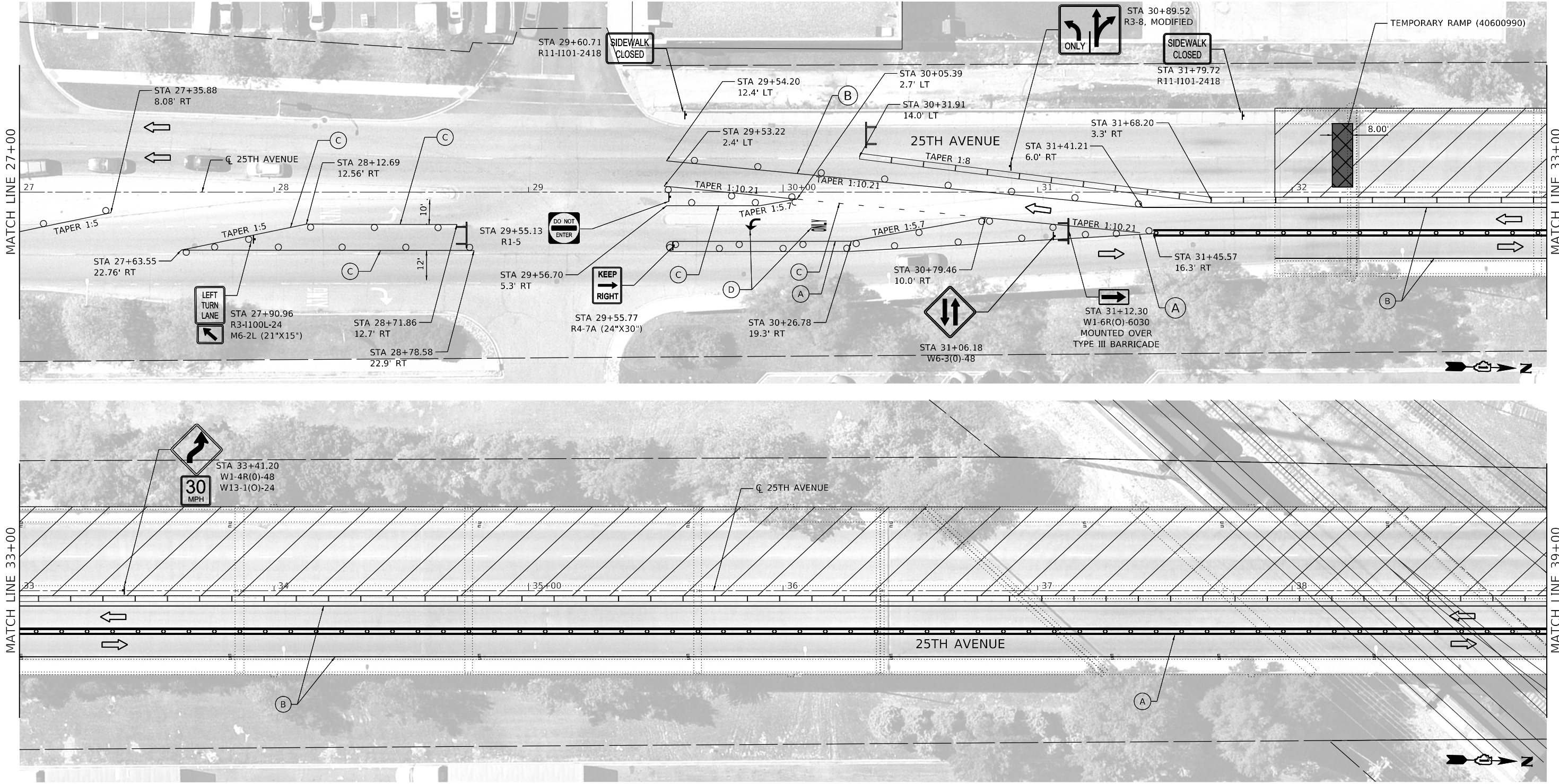
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

SCALE: 40,0000 ' / in. SHEET 10 OF 14 SHEETS STA. 15+00 TO STA. 27+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	16
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: idave
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FILE NAME: PLOT_209_00a.IDOT.PTB_190_14 Phase II Var-Var-Design\Work Order 141CADD\CADD_Sheets\161M80-SHT-STAGE03-PT2.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADI/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

TUBULAR MARKS (50' C-C-)

ARROW BOARD

IMPACT ATTENUATOR,
TEMPORARY (FULLY
REDIRECTIVE, RESETTABLE),
TEST LEVEL 3

WORK ZONE, SPEED LIMIT
PHOTO ENFORCED W/FINE
(SEE RIGHT)

WORK
ZONE
SPEED
LIMIT
35
PHOTO
ENFORCED
\$250 FINE
MINIMUM

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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DESIGNED - AWH
DRAWN - AWH
CHECKED - JWD
DATE - 08/28/2020

REVISED -
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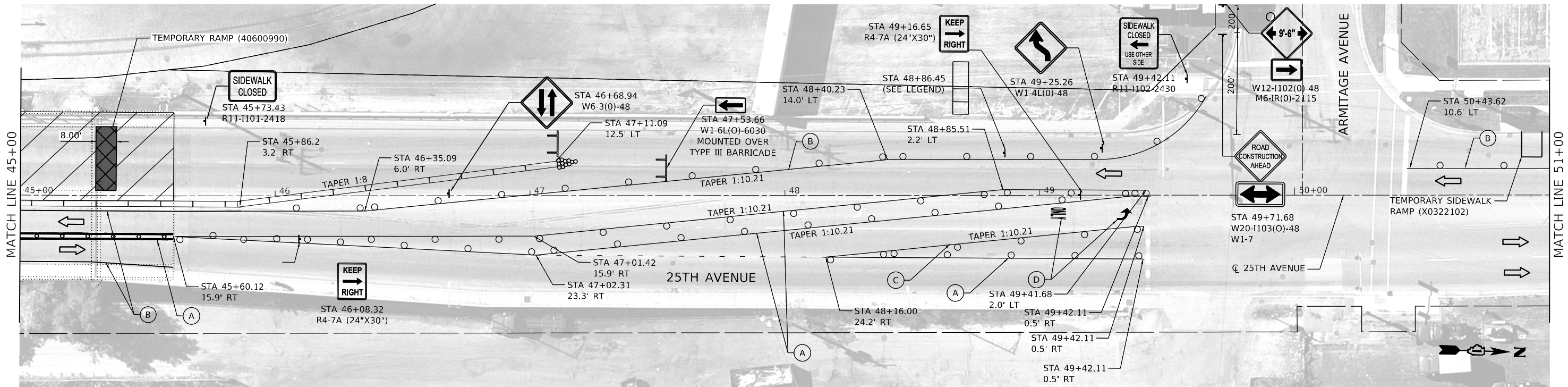
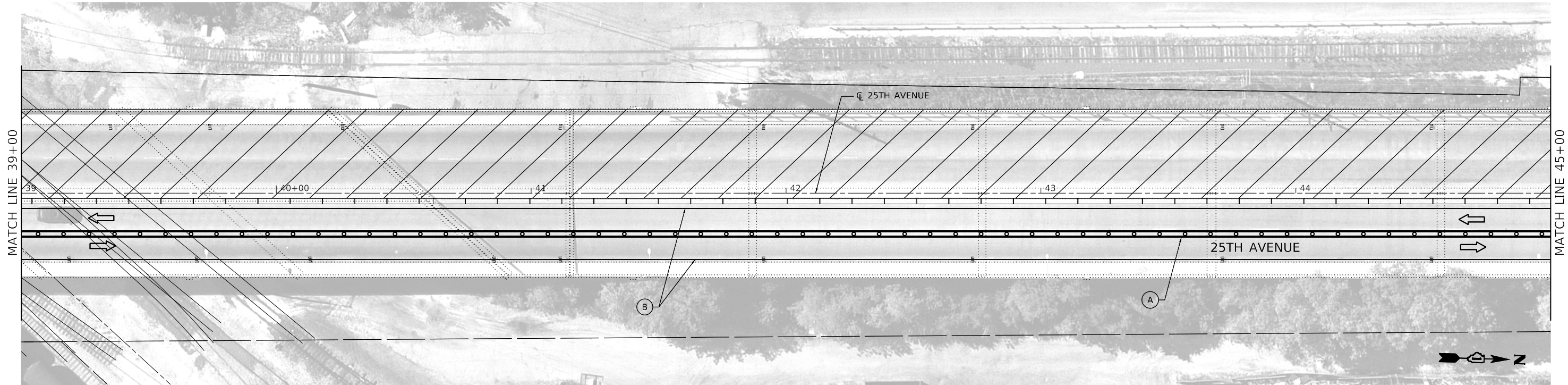
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

SCALE: 40.0000 ' / in. SHEET 11 OF 14 SHEETS STA. 27+00 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	17
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS
- TUBULAR MARKS (50' C-C-)
- ARROW BOARD
- IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 3
- WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE

SPEED LIMIT

35

PHOTO ENFORCED

\$250 FINE MINIMUM

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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DRAWN - AWH
CHECKED - JWD
DATE - 08/28/2020

REVISED -
REVISED -
REVISED -
REVISED -

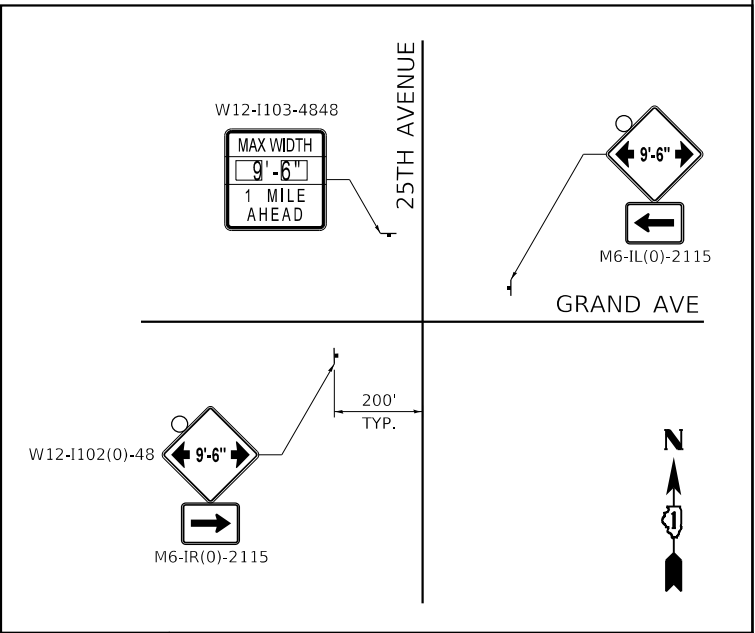
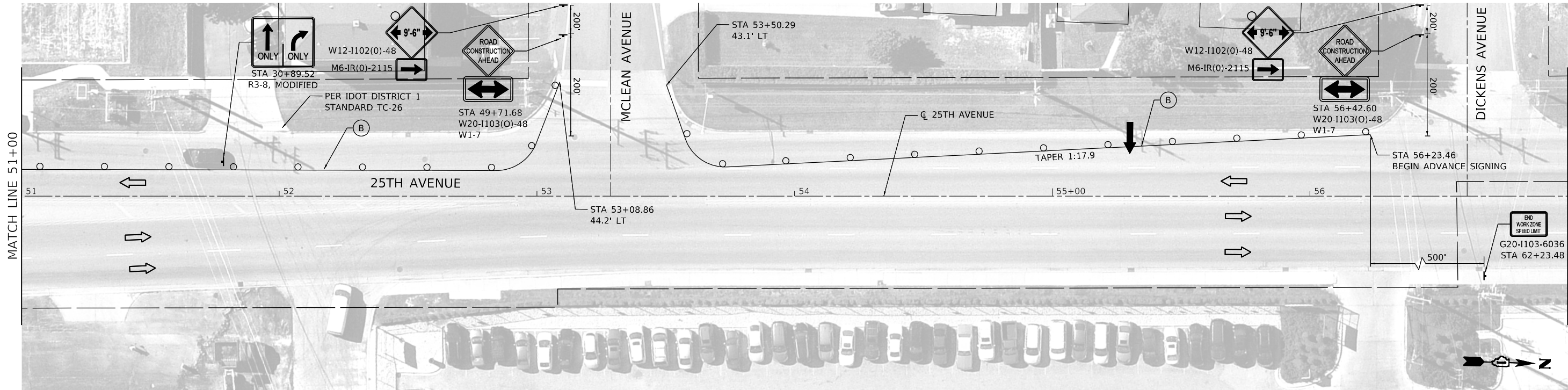
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

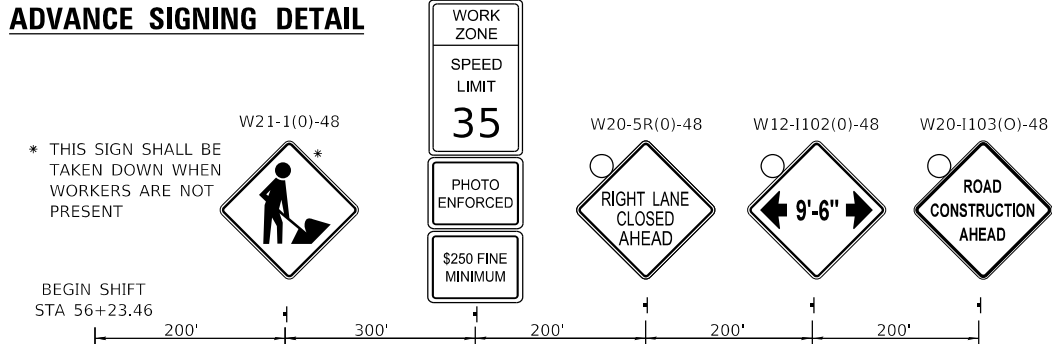
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

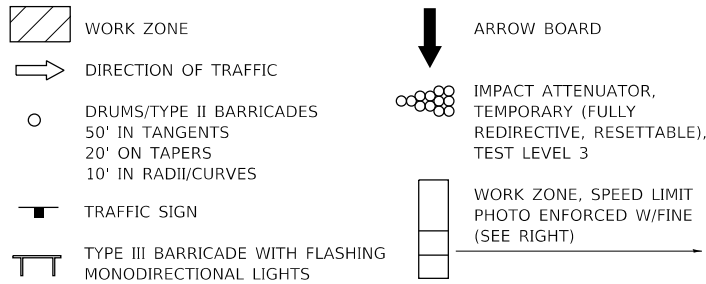
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ADVANCE SIGNING DETAIL



LEGEND



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE



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CHECKED	- JWD
DATE	- 08/28/2020

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

SCALE: 40.0000 ' / in. SHEET 13 OF 14 SHEETS STA. 51+00 TO STA. 57+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	19
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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MODEL Default
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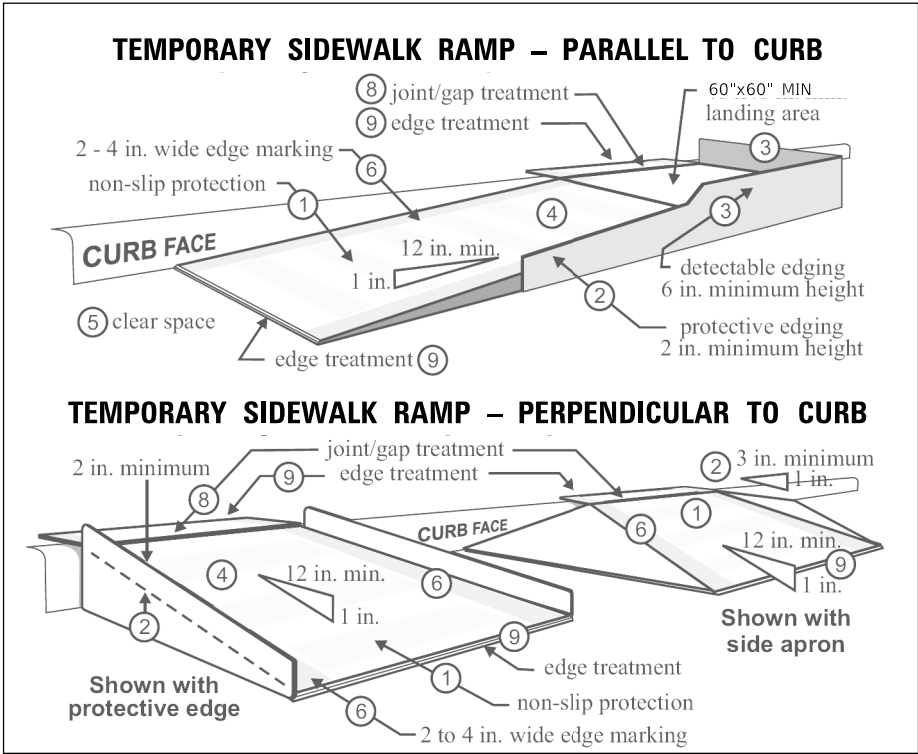
REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
TEMPORARY SIDEWALK RAMP

SCALE: N/A SHEET 14 OF 14 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	20
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				



(FIGURE FROM MnDOT TPAR)

TEMPORARY SIDEWALK RAMP

DESCRIPTION:

THIS WORK SHALL CONSIST OF INSTALLING AND MAINTAINING TEMPORARY ADA COMPLIANT RAMPS TO MAINTAIN ACCESS TO BUS STOPS DURING CONSTRUCTION AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

CONSTRUCTION REQUIREMENTS:

THIS WORK SHALL BE ACCORDANCE TO STANDARD 701801-06. TEMPORARY RAMPS TO MAINTAIN ACCESS SHALL BE COMPLAINT WITH THE ADA REQUIREMENTS AND SHALL BE USED AT LOCATIONS TO MAINTAIN A MINIMUM OF ONE ADA COMPLAINT PEDESTRIAN ACCESS PATH AROUND CONSTRUCTION ACTIVITIES AT ALL TIMES.

- TRAFFIC CONTROL DEVICES USED ADJACENT TO PEDESTRIAN ACCESS PATHS FOR CHANNELIZING PEDESTRIANS SHALL BE ADA COMPLAINT WITH A BOTTOM EDGE AT LEAST 6 INCHES HIGH FROM THE WALKWAY AND A CONTINUOUS RAIL OR SURFACE AT 3 FEET ABOUT THE WALKWAY.
- CURB RAMPS SHALL BE 8 FEET MINIMUM WIDTH WITH A FIRM, STABLE AND NON-SLIP SURFACE.
- PROTECTIVE EDGING WITH A 2 IN. MINIMUM HEIGHT SHALL BE INSTALLED WHEN THE CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6 IN. OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3 INCHES OR MORE.
- DETECTABLE EDGING WITH 6-INCH MINIMUM HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- CURB RAMPS AND LANDINGS SHOULD HAVE A 1:50 (2%) MAX CROSS-SLOPE.
- CLEAR SPACE OF 5 FT. X 5 FT. MINIMUM SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A CONTRASTING COLOR 2 TO 4 INCHES WIDE MARKING. THE MARKING IS OPTIONAL WHERE COLOR CONTRASTING EDGING IS USED.
- WATER FLOW IN THE GUTTER SYSTEM SHALL HAVE MINIMAL RESTRICTION.
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 0.5-INCH WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHOULD NOT EXCEED 0.5 INCH. LATERAL EDGES SHOULD BE VERTICAL UP TO 0.25-INCH-HIGH AND BEVELED AT 1:2 BETWEEN 0.25 INCH AND 0.5 INCH HEIGHT.

METHOD OF MEASUREMENT:

TEMPORARY SIDEWALK RAMP WILL BE MEASURED FOR PAYMENT IN PLACE ON AN EACH BASIS AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REINSTALLATION, ADJUSTMENT, OR MODIFICATION OF A TEMPORARY SIDEWALK RAMP AT THE SAME LOCATION WILL NOT BE MEASURED FOR PAYMENT.



BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "TEMPORARY SIDEWALK RAMP" AND SHALL INCLUDE REMOVAL.

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FILE NAME: PLOT_209_000_IDOT_PTB_190_14 Phase II Var-Van-Design\Work_Order_141CADD\CADD_Sheets\0162M80-CH-removals-P1.dgn



LEGEND

-  - HMA SURFACE REMOVAL 3"
-  - MEDIAN REMOVAL



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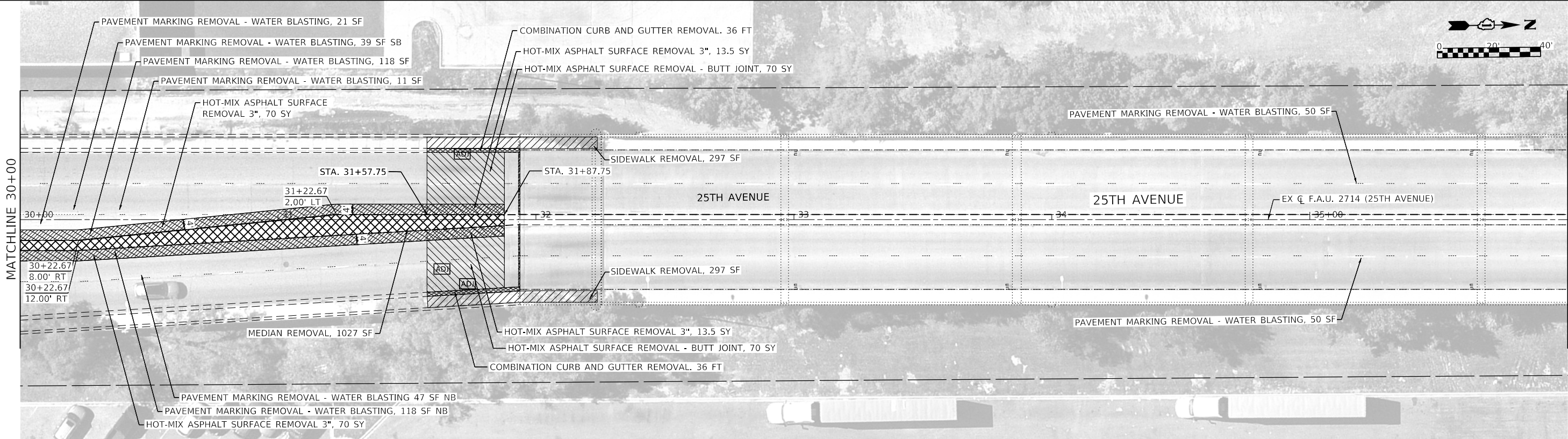
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**







**EXISTING AND REMOVAL PLAN
25TH AVENUE**

SCALE: 40.0000 ' / in. SHEET 1 OF 3 SHEETS STA. 18+00 TO STA. 30+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	21
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	



LEGEND

- | | |
|--|--|
|  - HMA SURFACE REMOVAL - BUTT JOINT (SEE DETAIL AND BD32) |  - SIDEWALK REMOVAL |
|  - HMA SURFACE REMOVAL 3" |  - MEDIAN REMOVAL |
|  - COMBINATION CURB AND GUTTER REMOVAL |  - TO BE ADJUSTED |



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CHECKED - JWD
DATE - 08/28/2020

REVISED -
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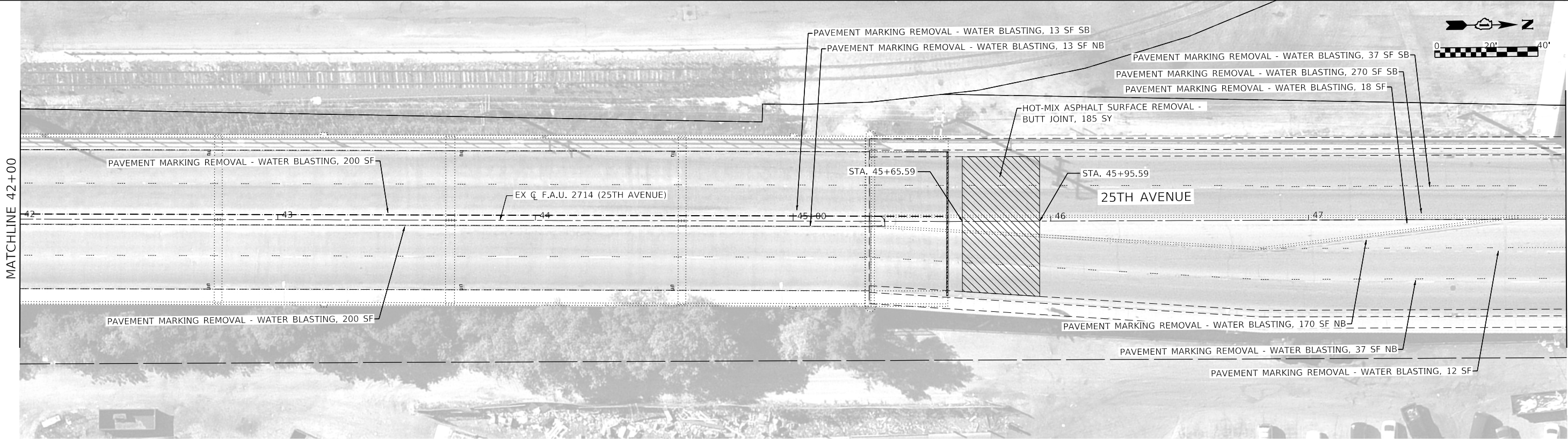
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND REMOVAL PLAN
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 2 OF 3 SHEETS STA. 30+00 TO STA. 42+00

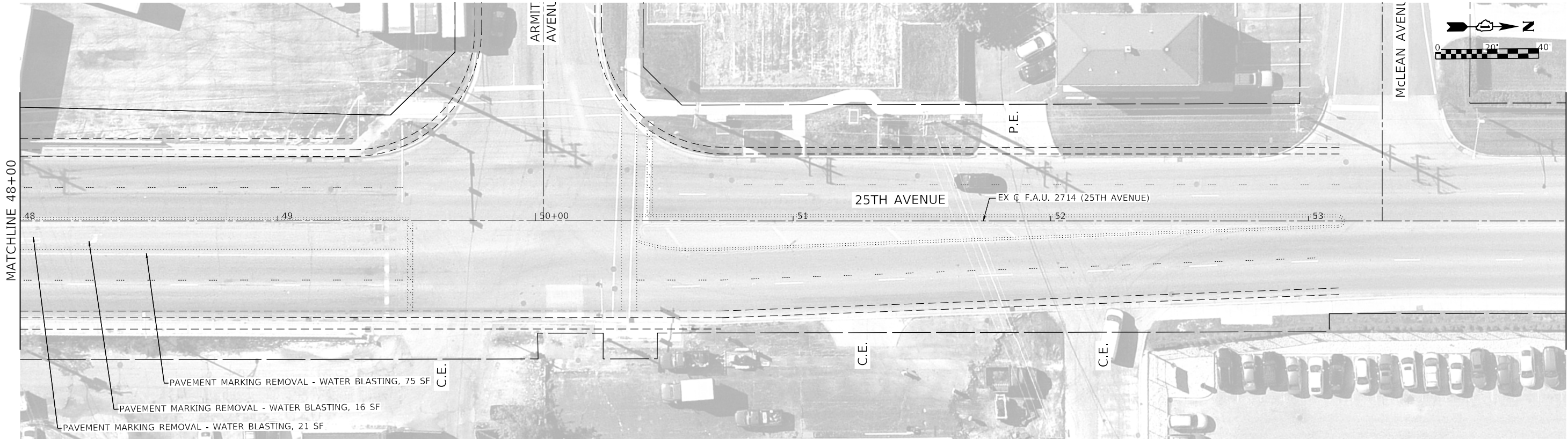
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2714	2020-203-BR	COOK	79	22
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

 - HMA SURFACE REMOVAL - BUTT JOINT
(SEE DETAIL AND BD32)



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CHECKED	-	JWD
DATE	-	08/28/2020

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND REMOVAL PLAN
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 1 OF 3 SHEETS STA. 42+00 TO STA. 54+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	23
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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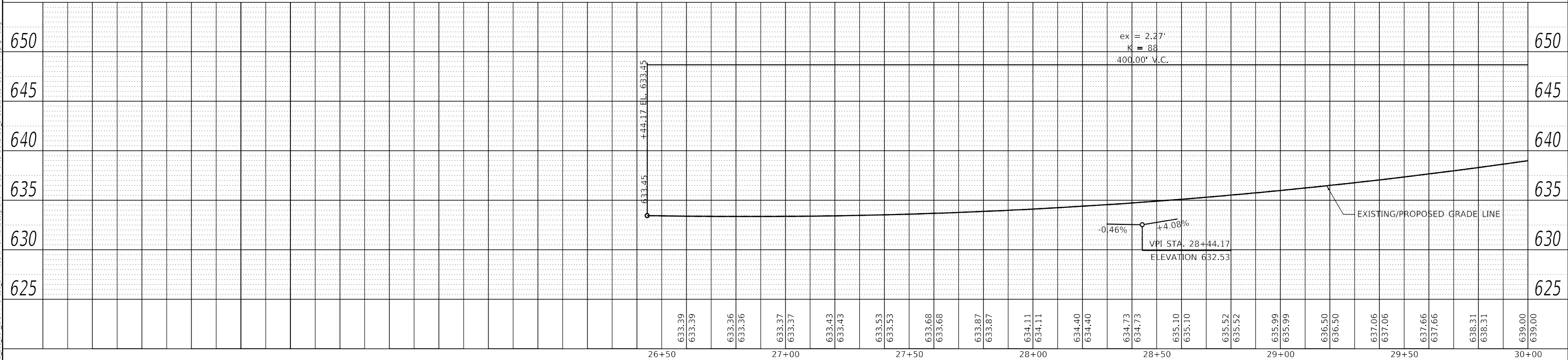
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LEGEND

- CONCRETE MEDIAN TYPE SB (DOWELLED)
- HMA SC IL-9.5 D N70
- HMA SC IL-9.5 D N70
HMA BC IL-9.5 N70
- PC CONC SIDEWALK 5"



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
25TH AVENUE

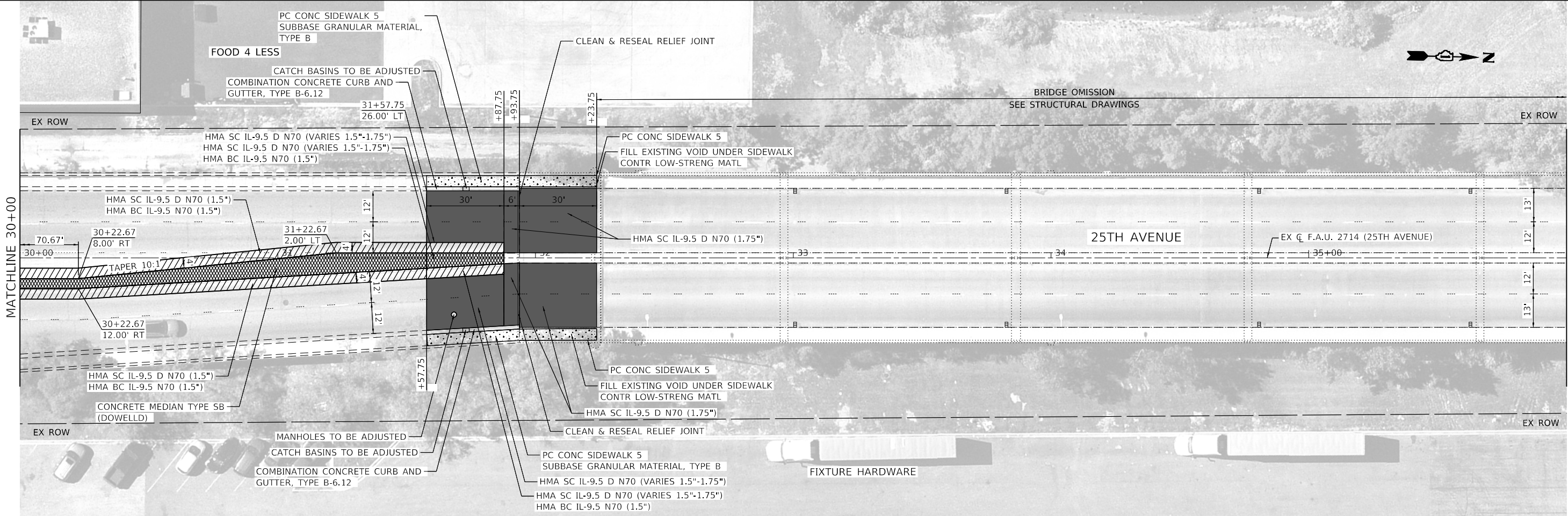
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	24
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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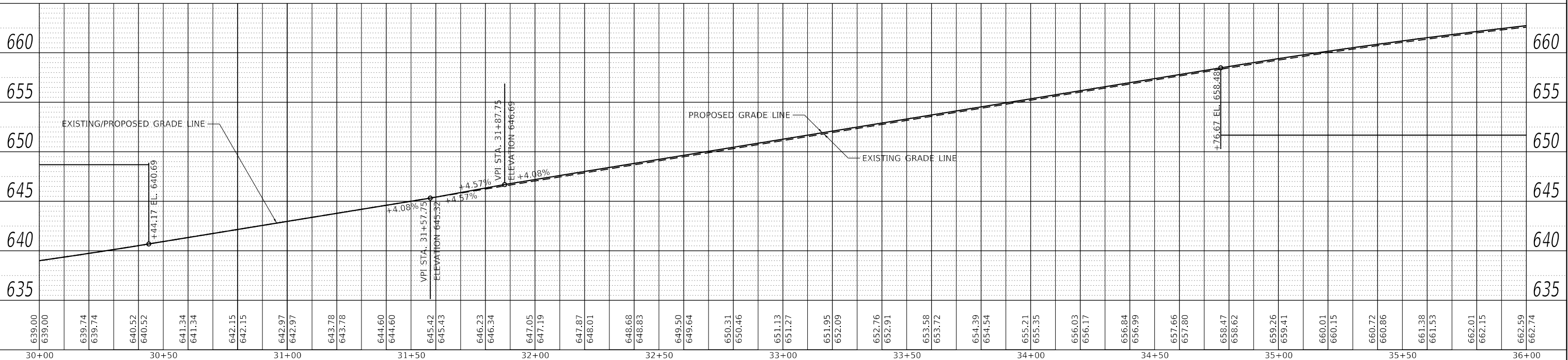
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LEGEND

- CONCRETE MEDIAN TYPE SB (DOWELLED)
- HMA SC IL-9.5 D N70
- HMA SC IL-9.5 D N70
HMA BC IL-9.5 N70
- PC CONC SIDEWALK 5"



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CHECKED - JWD	REVISED -
DATE - 08/28/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 2 OF 5 SHEETS STA. 30+00 TO STA. 36+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	25
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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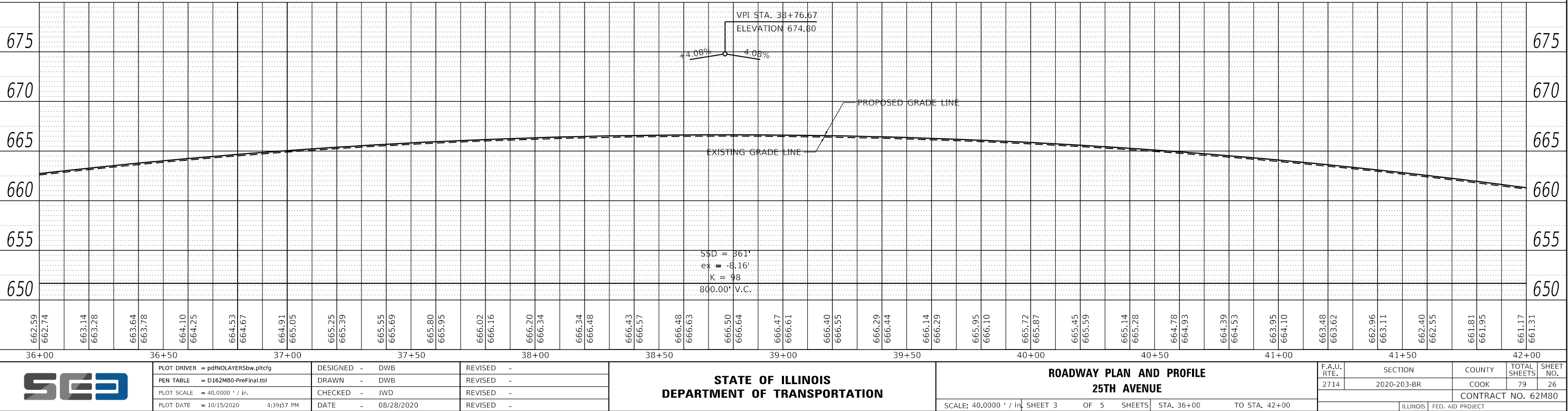
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LEGEND

- CONCRETE MEDIAN TYPE SB (DOWELLED)
- HMA SC IL-9.5 D N70
- HMA SC IL-9.5 D N70
HMA BC IL-9.5 N70
- PC CONC SIDEWALK 5"



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

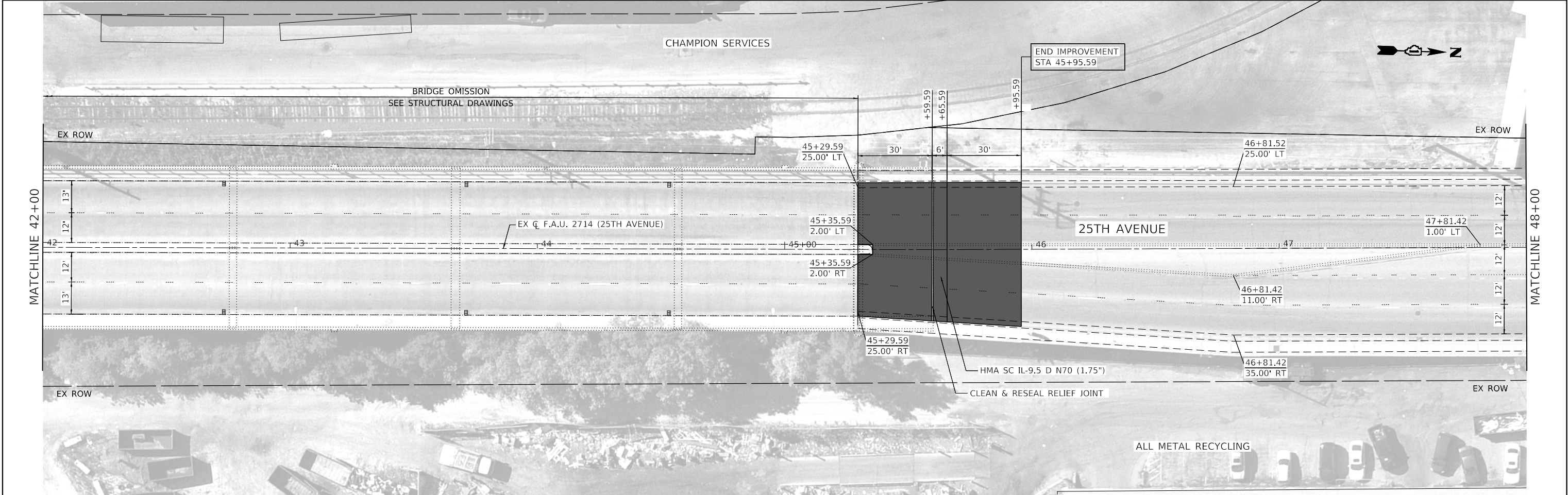
ROADWAY PLAN AND PROFILE
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 3 OF 5 SHEETS STA. 36+00 TO STA. 42+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

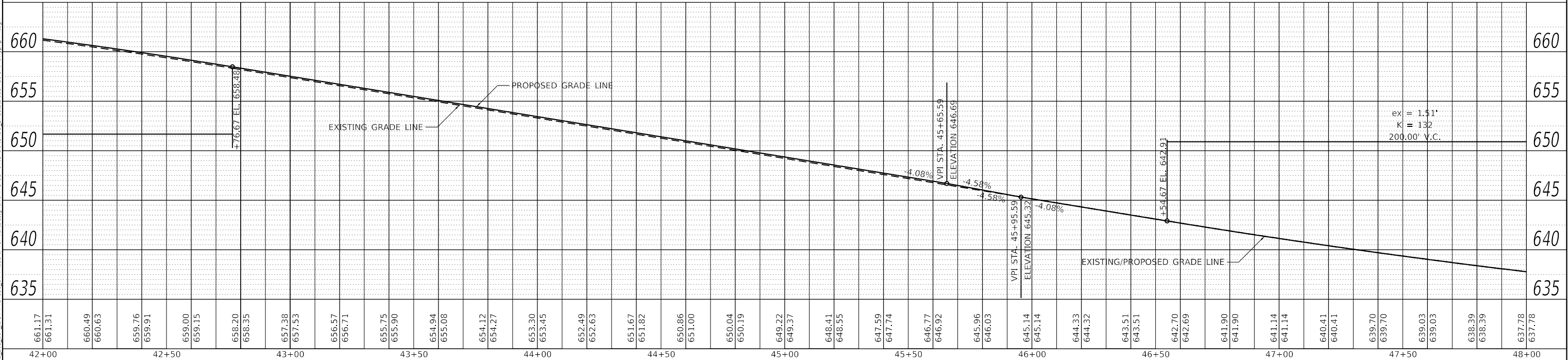
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NOTE BOOK NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHWD		
	CADD FILE NAME		



LEGEND

- CONCRETE MEDIAN TYPE SB (DOWELLED)
- HMA SC IL-9.5 D N70
- HMA SC IL-9.5 D N70
HMA BC IL-9.5 N70
- PC CONC SIDEWALK 5"



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
25TH AVENUE

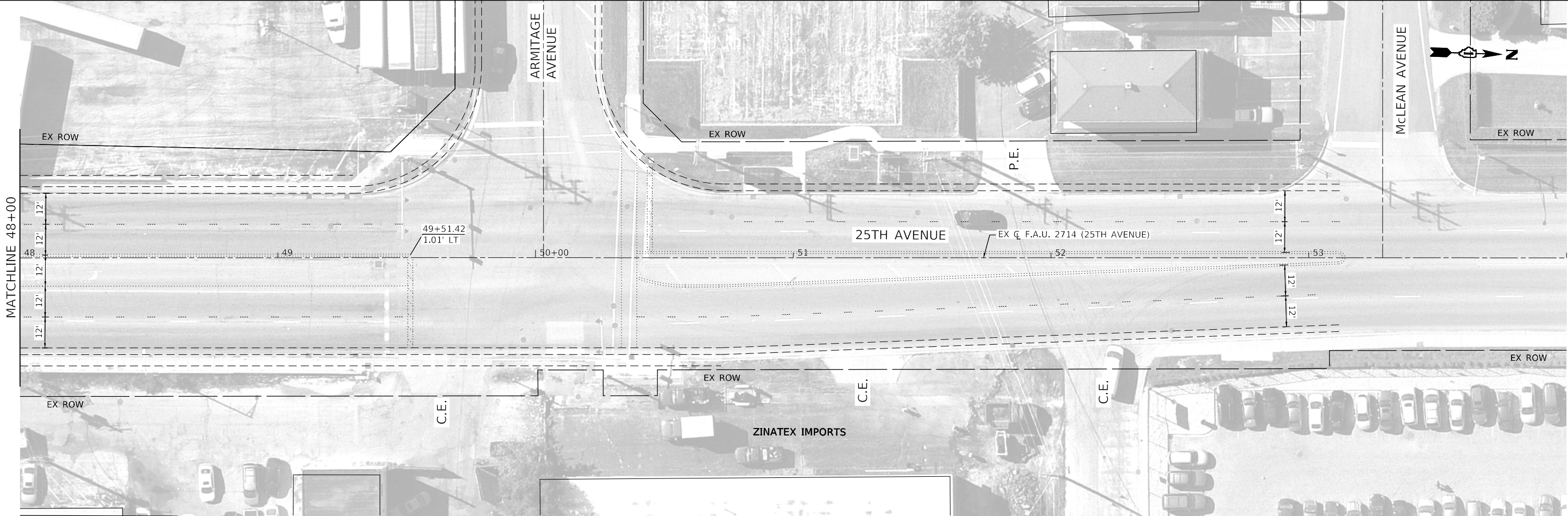
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	27
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
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	STRUCTURE NOTATIONS CHWD		
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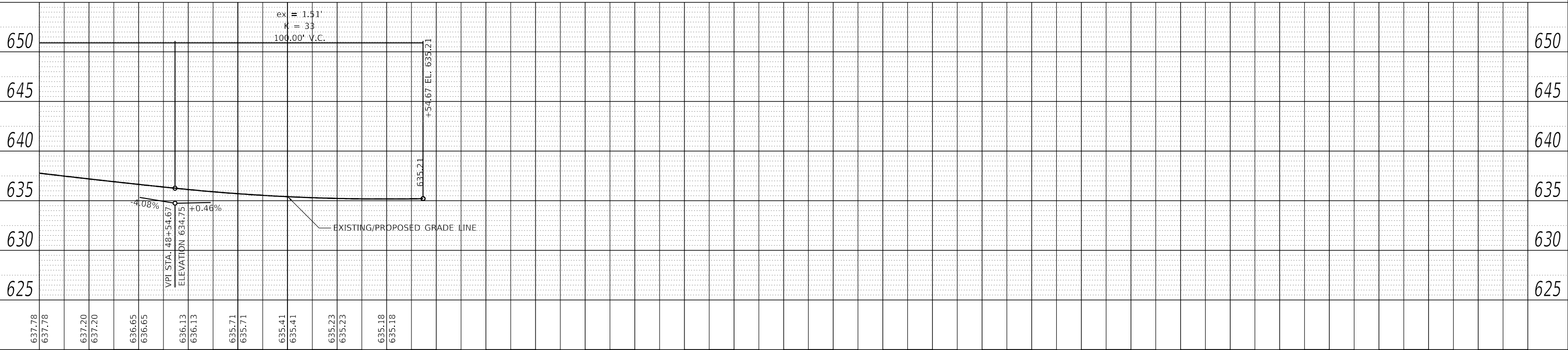
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LEGEND

- CONCRETE MEDIAN TYPE SB (DOWELLED)
- HMA SC IL-9.5 D N70
- HMA SC IL-9.5 D N70
HMA BC IL-9.5 N70
- PC CONC SIDEWALK 5"



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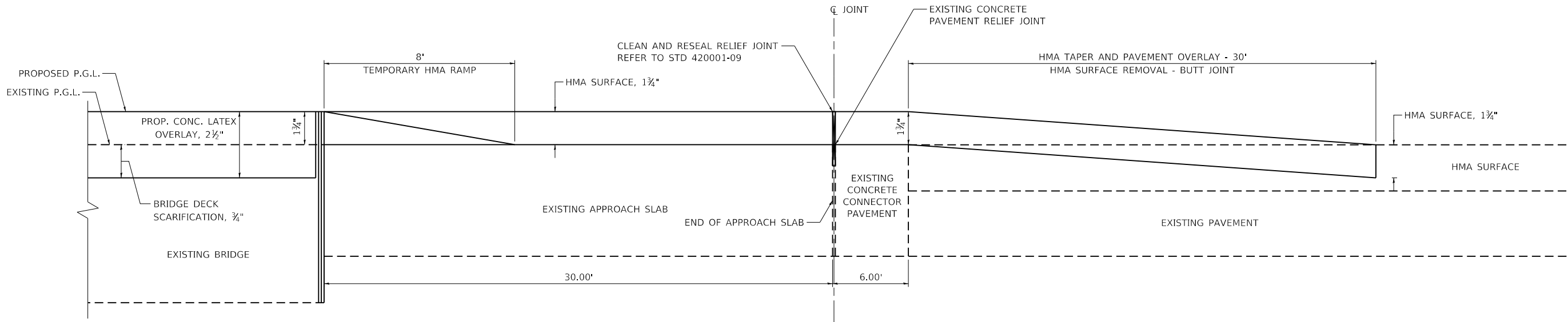
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 5 OF 5 SHEETS STA. 48+00 TO STA. 54+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	28
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	



APPROACH PAVEMENT HMA OVERLAY DETAIL

STA. 31+57.75 TO STA 32+23.75
STA. 45+29.59 to STA. 45+95.59

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**APPROACH PAVEMENT
HMA OVERLAY DETAIL**

SCALE: 24.0000 ' / in. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	29
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, 11" C-C) (78000200)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, SOLID) (78000200)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000200)

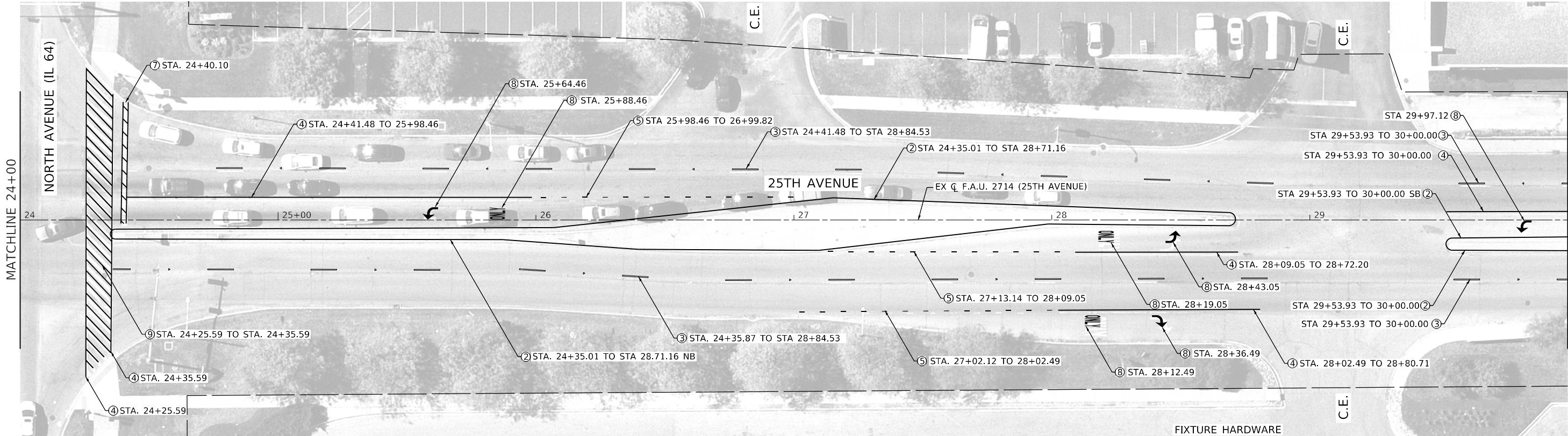
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SOLID) (78000400)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SKIP 6' - DASH 2') (78000400)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (YELLOW, SOLID) (78000600)

- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE, 24" (WHITE, SOLID) (78000650)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (78000100)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (WHITE, SOLID) (78000600)

- ⑩ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, SOLID) (78008310)
- ⑪ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, 11" C-C) (78008310)
- ⑫ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000310)

PAVEMENT MARKING TOTAL QUANTITIES STA. 18+00 TO 30+00

4" WHITE THERMOPLASTIC - 248 FT
4" YELLOW THERMOPLASTIC - 990 FT
6" WHITE THERMOPLASTIC - 644 FT
12" WHITE THERMOPLASTIC - 370 FT
12" YELLOW THERMOPLASTIC - 0 FT
24" WHITE THERMOPLASTIC - 48 FT
THERMOPLASTIC LETTERS AND SYMBOLS - 127 SQ FT
4" WHITE POLYUREA - 0 FT
4"YELLOW POLYUREA - 0 FT



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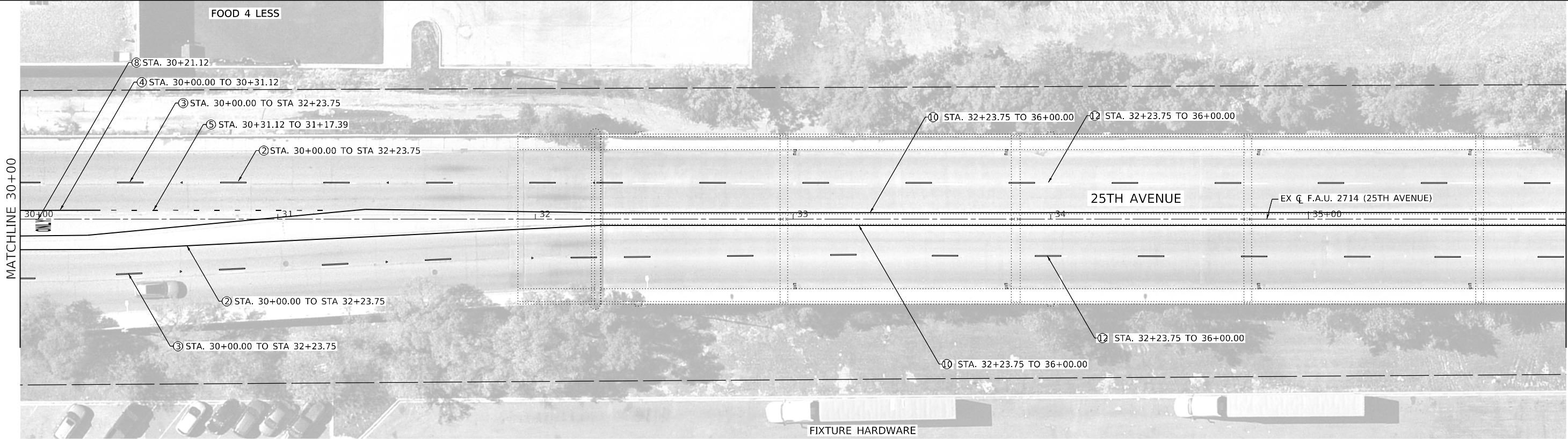
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
25TH AVENUE

SCALE: 40,0000 ' / in. SHEET 1 OF 3 SHEETS STA. 18+00 TO STA. 30+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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- ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, 11" C-C) (78000200)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, SOLID) (78000200)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000200)

- ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SOLID) (78000400)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SKIP 6' - DASH 2') (78000400)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (YELLOW, SOLID) (78000600)

- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE, 24" (WHITE, SOLID) (78000650)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (78000100)
- ⑨ THERMOPLASTIC PAVEMENT MARKING- LINE, 12" (WHITE, SOLID) (78000600)

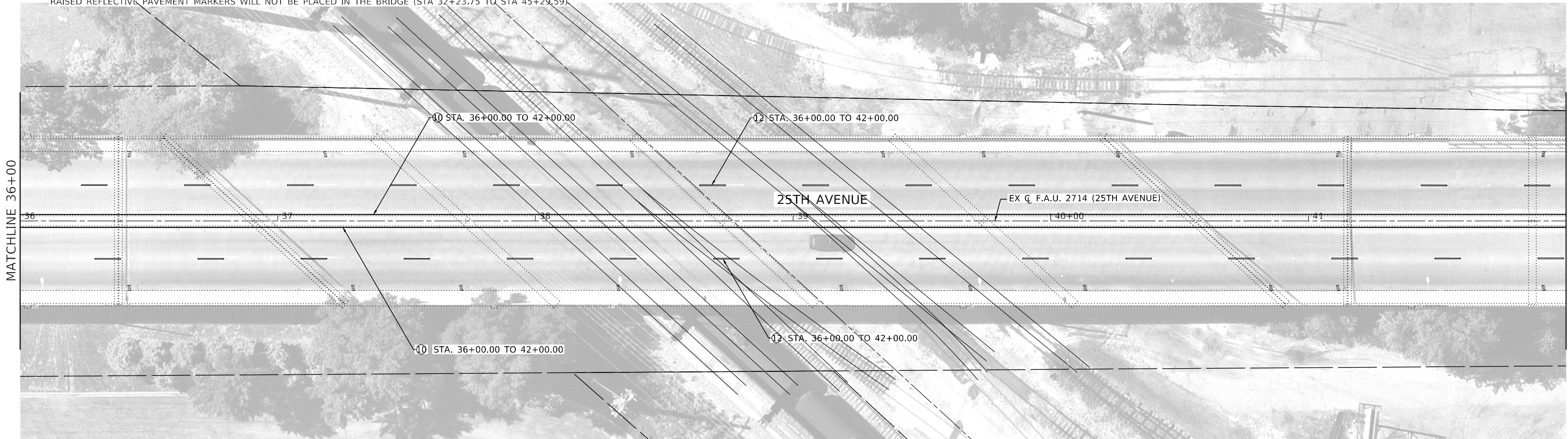
- ⑩ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, SOLID) (78008310)
- ⑪ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, 11" C-C) (78008310)
- ⑫ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000310)

PAVEMENT MARKING TOTAL QUANTITIES STA. 30+00 TO 42+00

4" WHITE THERMOPLASTIC - 112 FT
4" YELLOW THERMOPLASTIC - 450 FT
6" WHITE THERMOPLASTIC - 54 FT
12" WHITE THERMOPLASTIC - 0 FT
12" YELLOW THERMOPLASTIC - 0 FT
24" WHITE THERMOPLASTIC - 0 FT
THERMOPLASTIC LETTERS AND SYMBOLS - 21 SQ FT
4" WHITE POLYUREA - 490 FT
4"YELLOW POLYUREA - 1954 FT

NOTE:

RAISED REFLECTIVE PAVEMENT MARKERS WILL NOT BE PLACED IN THE BRIDGE (STA 32+23.75 TO STA 45+29.59).



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
25TH AVENUE

SCALE: 40,0000 ' / in. SHEET 2 OF 3 SHEETS STA. 30+00 TO STA. 42+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	31
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, 11" C-C) (78000200)

② THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (YELLOW, SOLID) (78000200)

③ THERMOPLASTIC PAVEMENT MARKING - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000200)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SOLID) (78000400)

⑤ THERMOPLASTIC PAVEMENT MARKING - LINE, 6" (WHITE, SKIP 6' - DASH 2') (78000400)

⑥ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (YELLOW, SOLID) (78000600)
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE, 24" (WHITE, SOLID) (78000650)

⑧ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (78000100)

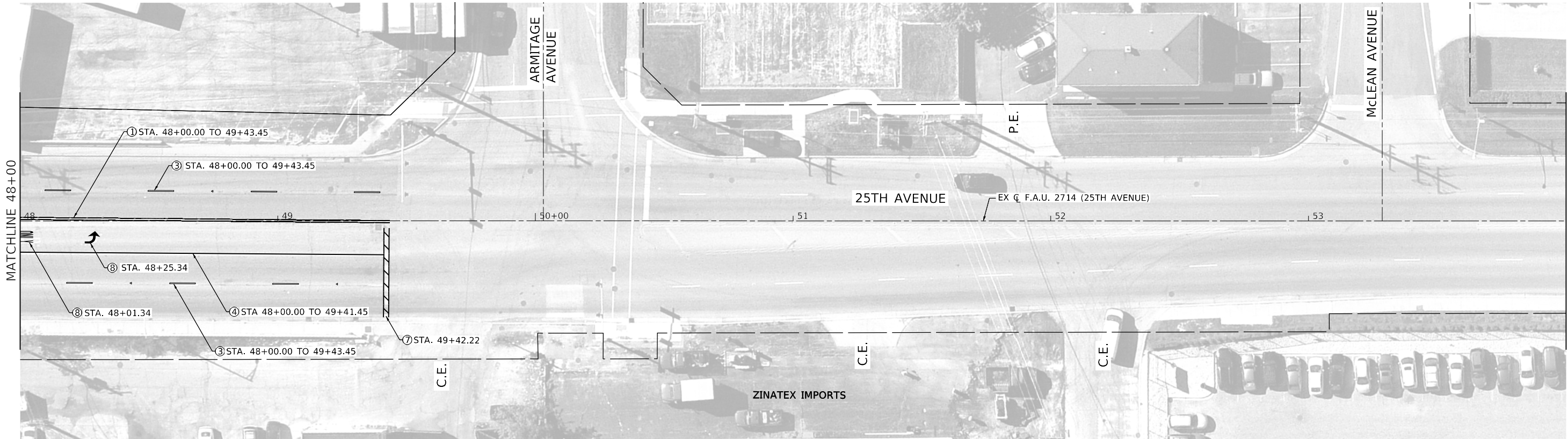
⑨ THERMOPLASTIC PAVEMENT MARKING - LINE, 12" (WHITE, SOLID) (78000600)
- ⑩ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, SOLID) (78008310)

⑪ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (YELLOW, 11" C-C) (78008310)

⑫ POLYUREA PAVEMENT MARKING TYPE II - LINE, 4" (WHITE, SKIP 30' - DASH 10') (78000310)

PAVEMENT MARKING TOTAL QUANTITIES STA. 42+00 TO 54+00

4" WHITE THERMOPLASTIC - 208 FT
4" YELLOW THERMOPLASTIC - 1357 FT
6" WHITE THERMOPLASTIC - 395 FT
12" WHITE THERMOPLASTIC - 0 FT
12" YELLOW THERMOPLASTIC - 57 FT
24" WHITE THERMOPLASTIC - 37 FT
THERMOPLASTIC LETTERS AND SYMBOLS - 37 SQ FT
4" WHITE POLYUREA - 166 FT
4"YELLOW POLYUREA - 660 FT



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
25TH AVENUE

SCALE: 40.0000 ' / in. SHEET 3 OF 3 SHEETS STA. 42+00 TO STA. 54+00

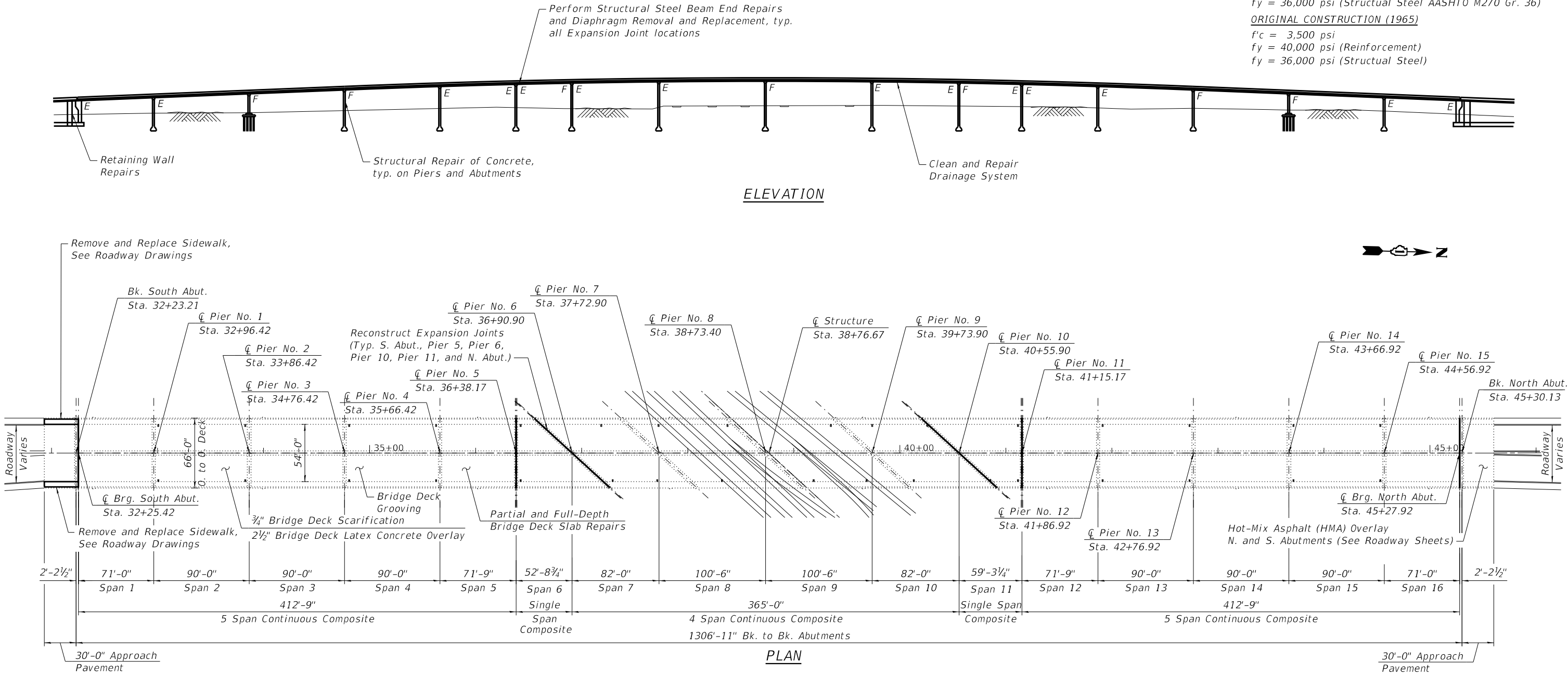
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	32
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

Existing Structure: Structure No. 016-1011, built as 25th Avenue, Section 020-1010.2MFT in 1965 is 66'-0" wide by 1302'-6' long. the existing sixteen span reinforced concrete deck and steel wide flange beams are supported on multiple column piers and closed abutments. The structure was rehabilitated in 1994 with a deck replacement, structural steel repair and substructure repairs. One lane of traffic in each direction is to be maintained utilizing stage construction.

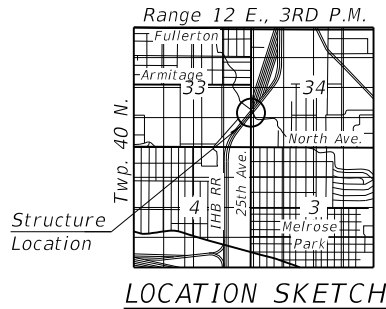
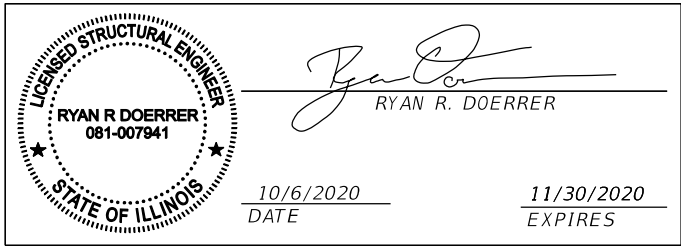
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DESIGN SPECIFICATIONS
2002 AASHTO Standard Specifications
for Highway Bridges, 17th Edition

DESIGN STRESSES
FIELD UNITS
f'c = 3,500 psi
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (Structural Steel AASHTO M270 Gr. 36)
RECONSTRUCTION (1994)
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (Structural Steel AASHTO M270 Gr. 36)
ORIGINAL CONSTRUCTION (1965)
f'c = 3,500 psi
fy = 40,000 psi (Reinforcement)
fy = 36,000 psi (Structual Steel)



NOTE:
Dimension and Clearances shown are from existing drawings.



GENERAL PLAN AND ELEVATION
25TH AVENUE OVER IHB RAILROAD
F.A.U. RTE. 2714 SECTION 1010.2R
COOK COUNTY
STATION 38+76.67
STRUCTURE NO. 016-1011

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-1011

SHEET 5-01 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	33
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to placing the bridge deck latex concrete overlay, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding ¼ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Proposed profile and elevations are based on the existing drawing profile and adjusted to account for the proposed overlay.

Fasteners shall be ASTM F 3125 Grade A352 Type 1. Fastener shall be hot dip galvanized. See Special Provision for “Hot Dip Galvanizing for Structural Steel.” Bolts ¾ in. Φ holes 1⅜ in. Φ, unless otherwise noted.

All new structural steel shall be galvanized. See Special Provision for “Hot Dip Galvanizing for Structural Steel.”

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision “Cleaning and Painting Contact Surface Areas of Existing Steel Structures.”

The existing structural steel coating contains lead. The contractor shall take appropriate precautions to deal with the presence of lead on this project. See Special Provision “Containment and Disposal of Lead Paint Cleaning Residues.”

All exposed concrete edges shall have a ¾" x 45 degree chamfer except where shown otherwise.

SCOPE OF WORK

1. Perform ¾" Bridge Deck Scarification.
2. Perform Deck slab repairs as required.
3. Reconstruct bridge deck expansion joints at the North and South Abutments and Pier 5, 6, 10, and 11. Install New preformed joints Strip Seal.
4. Perform Parapet Repair and Partial Removal/ Replacement of railing as required.
5. Apply a 2½" Bridge Deck Latex Concrete Overlay on Bridge Deck.
6. Perform Bridge Deck Grooving.
7. Perform HMA overlay on the North and South Approach Slabs (See Roadway Plans).
8. Apply Protective Coat to the top and inside faces of parapets, sidewalks, reconstructed transverse expansion joints, and surface of the new overlay.
9. Perform Structural Steel Beam End Repairs and diaphragm removal and replacement at the locations shown in the drawings.
10. Perform Structural Repair of Concrete and Epoxy Crack Injection to the abutments and Piers as shown in the drawings.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	CU YD	69.6	0	69.6
Protective Shield	SQ YD	8,867	0	8,867
Concrete Structures	CU YD	0	1.0	1.0
Concrete Superstructure	CU YD	77.8	0.0	77.8
Bridge Deck Grooving	SQ YD	6,683	0	6,683
Protective Coat	SQ YD	7,333	0	7,333
Furnishing And Erecting Structural Steel	POUND	25,080	0	25,080
Reinforcement Bars, Epoxy Coated	POUND	10,910	160	11,070
Bar Splicers	EACH	98	0	98
Preformed Joint Strip Seal	FOOT	460	0	460
Epoxy Crack Injection	FOOT	0	24	24
Fiber Wrap	SQ FT	0	377	377
Bridge Drainage System Repair	FOOT	22	0	22
Jack and Repositioning Bearings	EACH	0	24	24
Structural Steel Removal	POUND	25,860	0	25,860
Structural Steel Repair	POUND	9,960	0	9,960
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	SQ YD	7,107	0	7,107
Containment and Disposal of Lead Paint Cleaning Residues No. 1	L SUM	1	0	1
Cleaning Drainage System	L SUM	1	0	1
Bridge Deck Scarification 3/4"	SQ YD	7,107	0	7,107
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ FT	0	1,672	1,672
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	SQ FT	0	49	49
Deck Slab Repair (Full Depth, Type II)	SQ YD	10	0	10
Temporary Shoring and Cribbing	EACH	18	0	18

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S-02 General Data
S-03 Stage Construction Details (1 of 2)
S-04 Stage Construction Details (2 of 2)
S-05 Temporary Concrete Barrier for Stage Construction
S-06 Deck Repair Plan (1 of 5)
S-07 Deck Repair Plan (2 of 5)
S-08 Deck Repair Plan (3 of 5)
S-09 Deck Repair Plan (4 of 5)
S-10 Deck Repair Plan (5 of 5)
S-11 N. and S. Abut. Joint Removal and Reconstruction Plan
S-12 N. and S. Abut. Joint Removal and Reconstruction Details
S-13 Pier 5, 6, 10 and 11 Joint Removal and Reconstruction Plan
S-14 Pier 5, 6, 10 and 11 Joint Removal and Reconstruction Details
S-15 Preformed Joint Strip Seal – Sidewalk (1 of 3)
S-16 Preformed Joint Strip Seal – Sidewalk (2 of 3)
S-17 Preformed Joint Strip Seal – Sidewalk (3 of 3)
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S-19 Partial Framing Plan (2 of 2)
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S-21 Beam Repair Details, Pier 5 and 11
S-22 Diaphragm Replacement Details, Pier 6 and 10
S-23 Beam Repair Details, Pier 6 and 10
S-24 Drainage System Repair Plan
S-25 Retaining Wall Repairs
S-26 North and South Abutment Repair Details
S-27 Pier 2 and 5 Repair Details
S-28 Pier 6 and 7 Repair Details
S-29 Pier 8 and 9 Repair Details
S-30 Pier 10 and 11 Repair Details
S-31 Pier 12 and 13 Repair Details
S-32 Pier 14 and 15 Repair Details
S-33 Bar Splicer Assembly and Mechanical Splicer Details
S-34 Existing General Plan and Elevation



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 016-1011

SHEET S-02 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	34
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	

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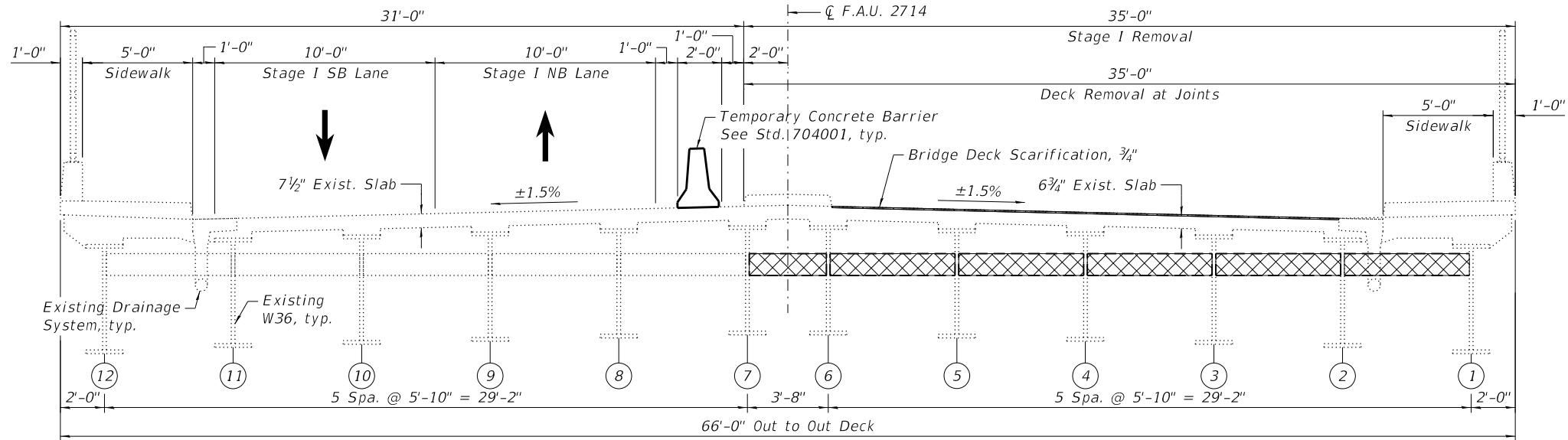
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS (1 OF 2)
STRUCTURE NO. 016-1011

SHEET 5-03 OF 5-34 SHEETS

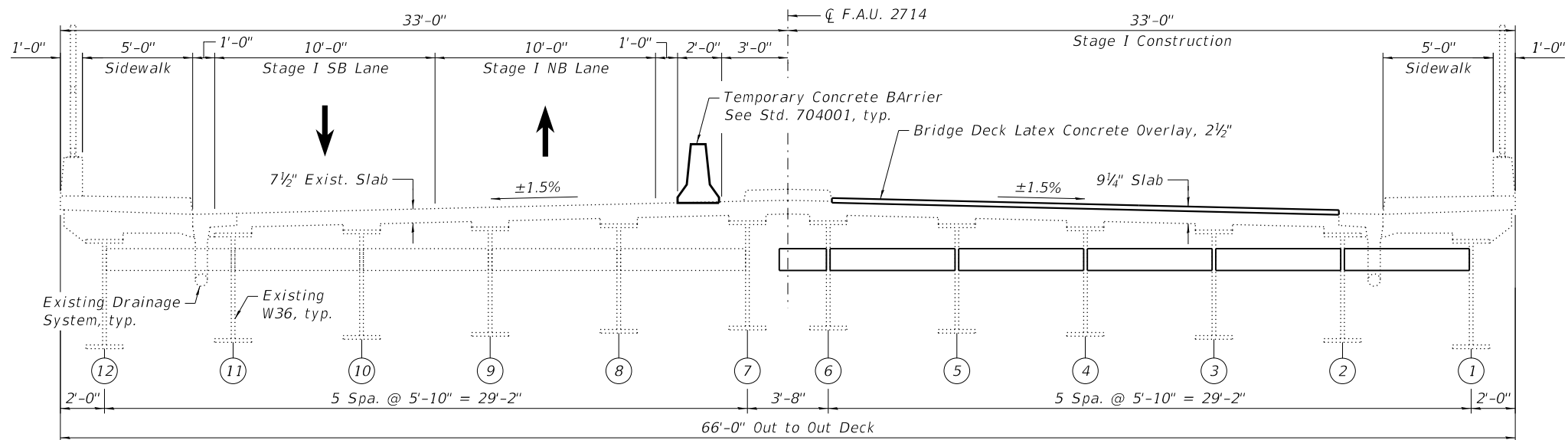
LEGEND

	Bridge Deck Scarification, 3/4"
	Structural Steel Removal



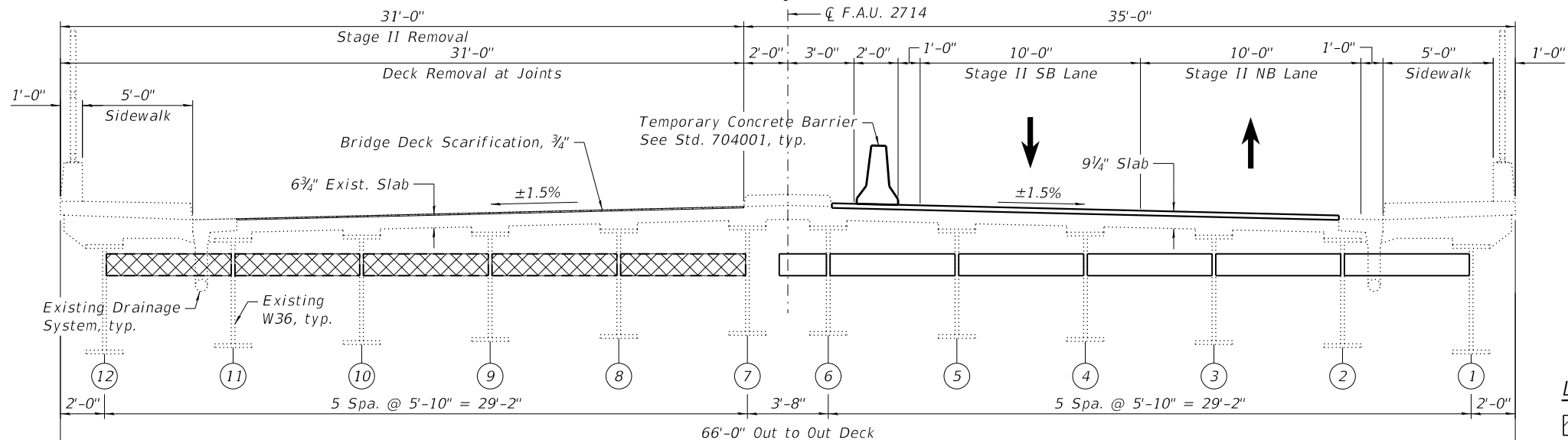
STAGE I REMOVAL

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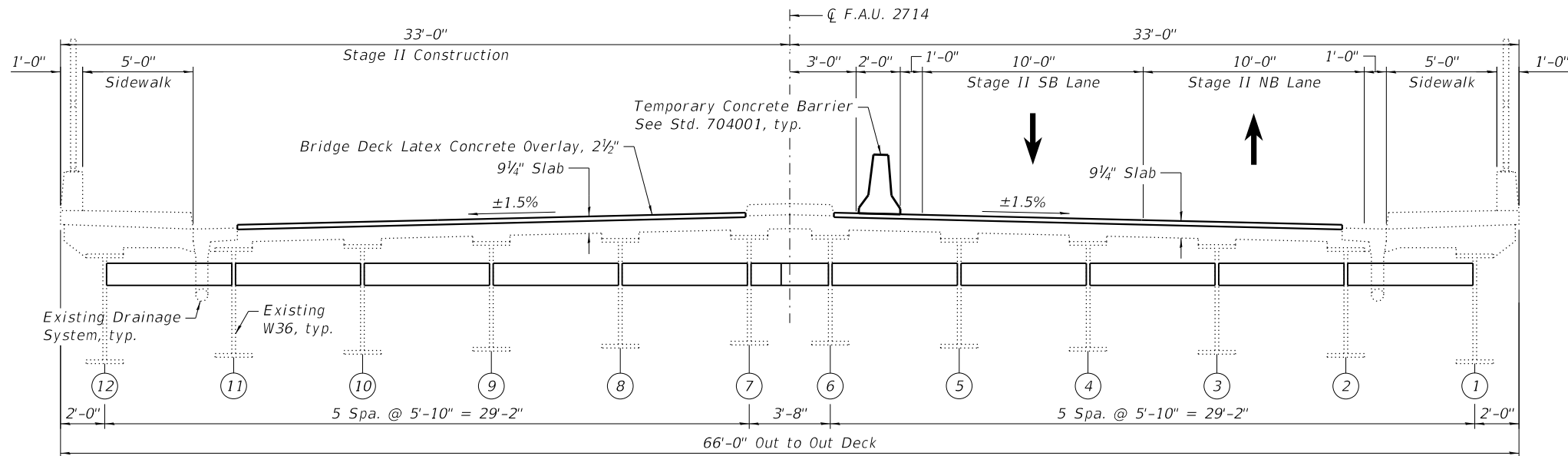
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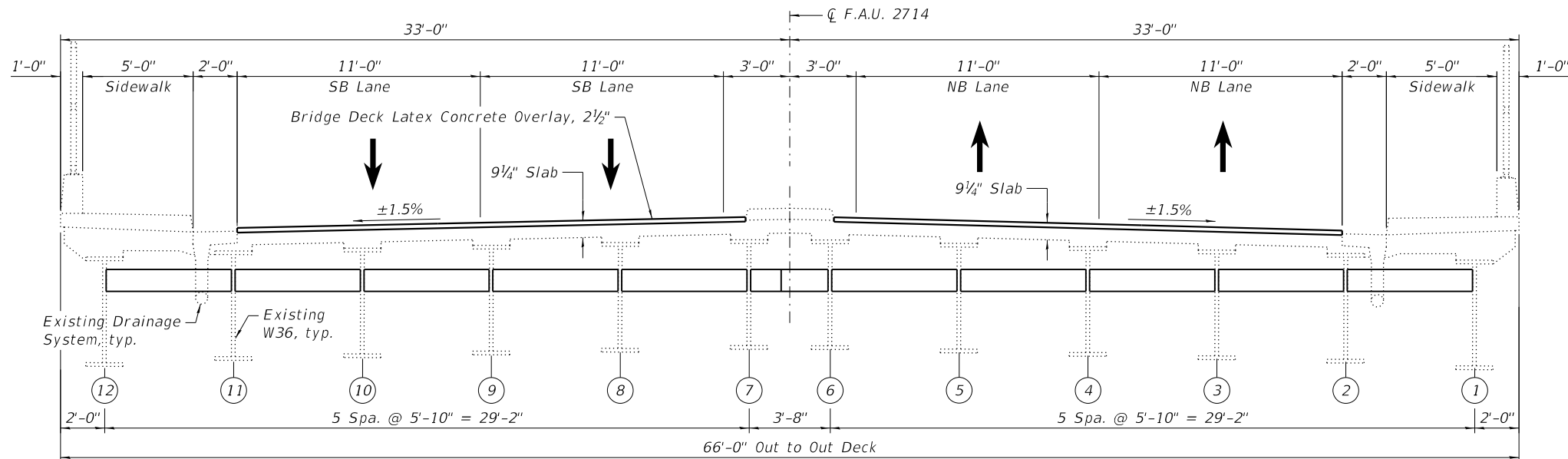


STAGE II REMOVAL

(Looking North)



STAGE II CONSTRUCTION
(Looking North)



FINAL DECK CROSS SECTION
(Looking North)

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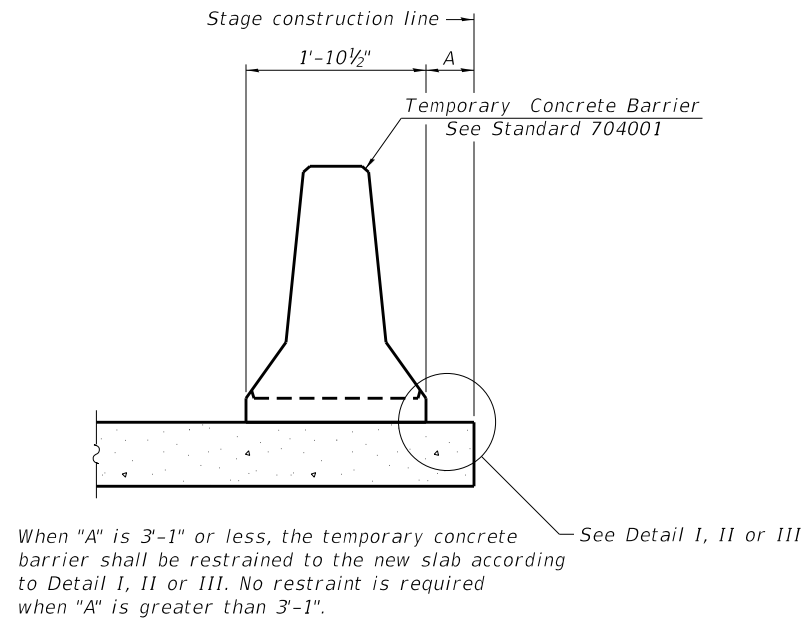
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS (2 OF 2)
STRUCTURE NO. 016-1011

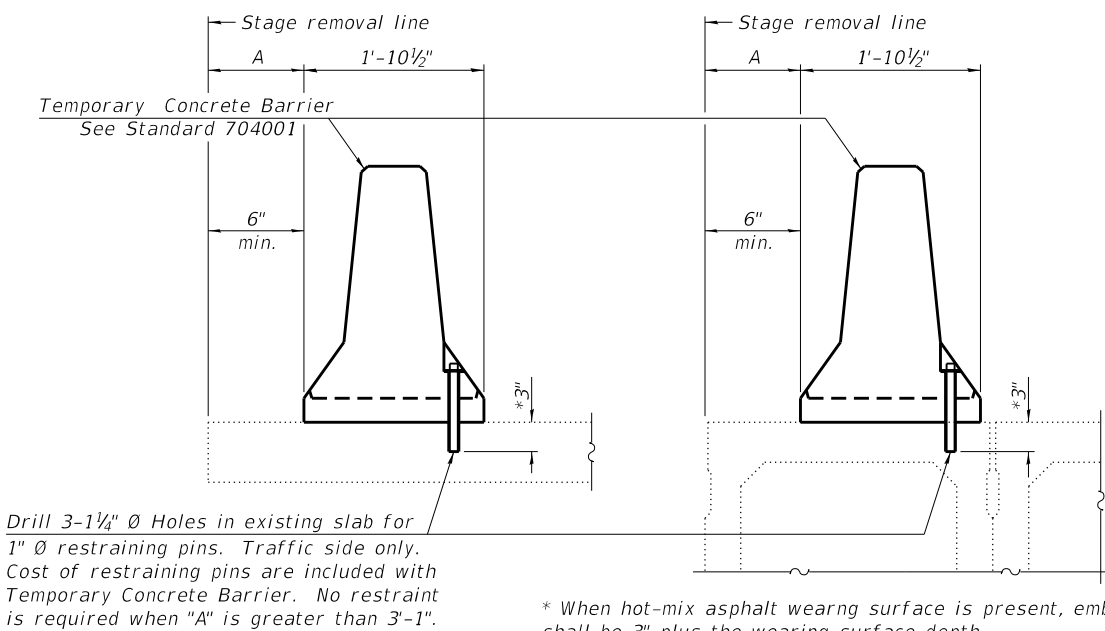
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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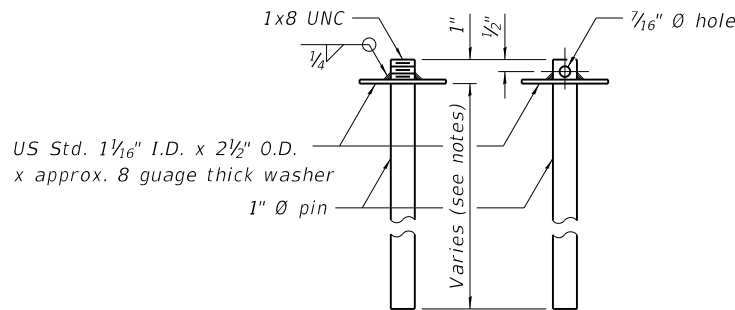


NEW SLAB OR NEW DECK BEAM



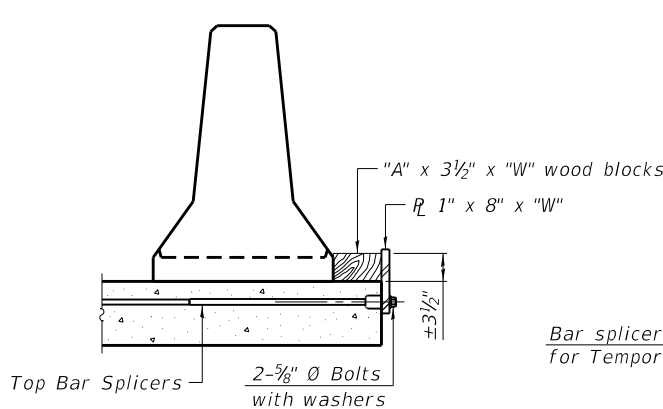
EXISTING SLAB

EXISTING DECK BEAM

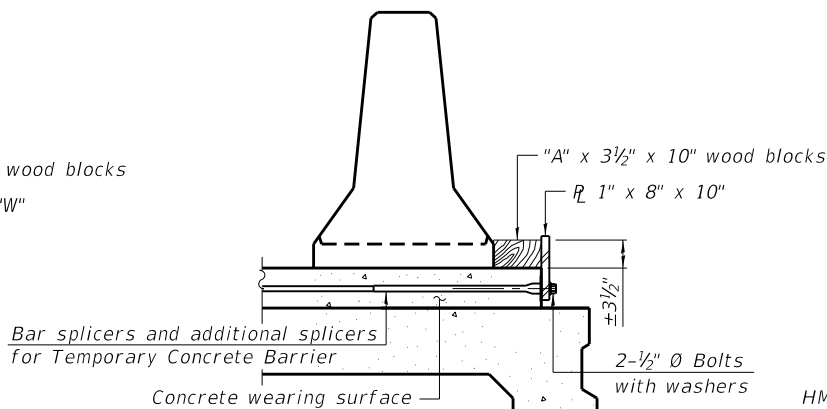


RESTRAINING PIN

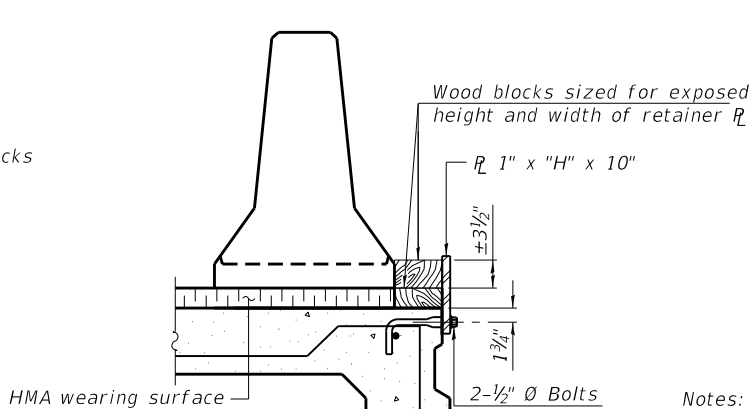
SECTIONS THRU SLAB OR DECK BEAM



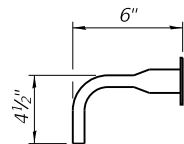
DETAIL I



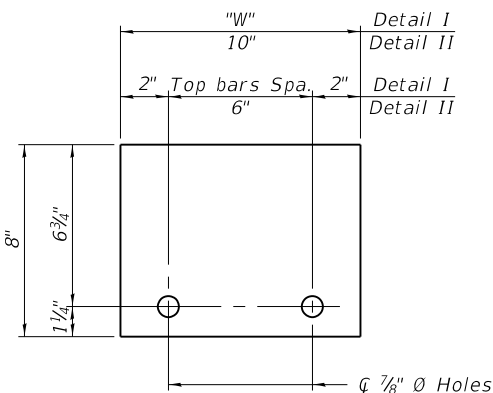
DETAIL II



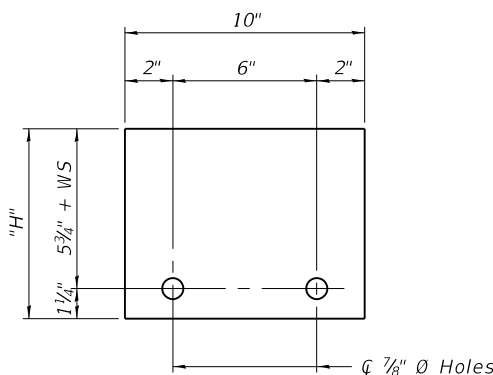
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
Cost of retainer assembly is included with Temporary Concrete Barrier.
A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27

2-17-2017



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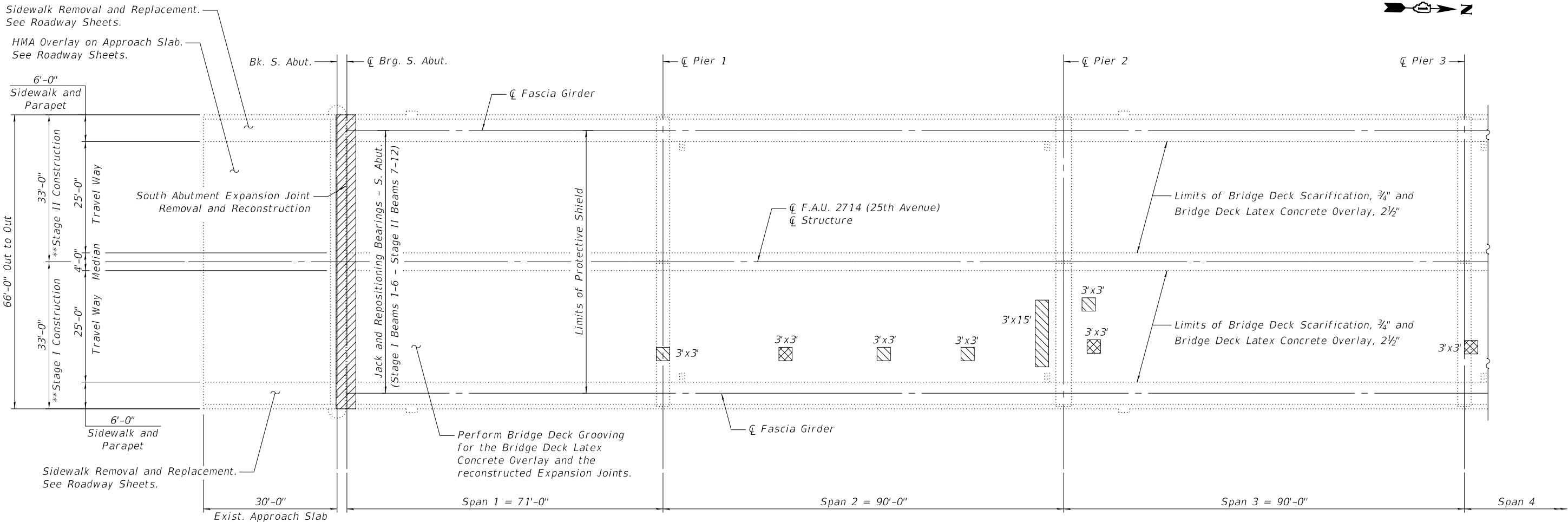
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 016-1011

SHEET 5-05 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	37
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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** Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

DECK PLAN

NOTES:

- Areas of deck repair shown are estimated. The Engineer shall determine actual locations of deck repairs at the time of construction.
- For Abutment and Pier Expansion Joint Removal and Reconstruction, see Sheets S-11 thru S-14.
- Protective coat shall be applied to top and inside face of reconstructed parapets, reconstructed sidewalks, reconstructed transverse joint areas and the surface of the new overlay.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approval bar splicer or anchorage system. Cost incidental to Concrete Removal.
- The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of their work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.
- Existing bridge fencing is to be protected during construction. Post location in the removal area for the joint replacement shall be temporally shored during the concrete removal and replacement. Existing concrete anchor shall be replaced in kind and installed into the proposed parapet. Cost included in Concrete Removal.

- Removal and disposal of the existing expansion joints will not be paid for separately, but are included in the cost of Concrete Removal.
- The Contractor must exercise extreme care with the existing conduits in the sections of the parapets to be removed and to protect and support the conduit during construction. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damaged resulting from this work. The Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- If the existing name plate fails within the limits of Concrete Removal, it shall be removed and reinstalled in its original locations in accordance with IDOT Standard 515001. Cost included with Concrete Superstructure.

JACKING LOADS

Bearing Location	Load (kips)
S. Abut. - Beam 1 and 12	22.6
S. Abut. - S. Abut. - Beam 6 and 7	22.2
S. Abut. - Beams 2 thru 5 and 8 thru 11	25.1
Pier 4 - Beam 1 and 12	73.3
Pier 4 - S. Abut. - Beam 6 and 7	71.9
Pier 4 - Beams 2 thru 5 and 8 thru 11	81.1

Service girder self-weight and deck weight prior to overlay are shown. The Contractor shall design and place jacking system to reposition the specified bearing for the stated reactions and as required in the special provision.

Loads are per Girder.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2 1/2".

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Bridge Deck Grooving	Sq Yd	6,683
Protective Coat	Sq Yd	7,333
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq Yd	7,107
Bridge Deck Scarification, 3/4"	Sq Yd	7,107
Protective Shield	Sq Yd	8,867
Jack and Repositioning Bearings	Each	24
Deck Slab Repair (Full Depth, Type II)	Sq Yd	10



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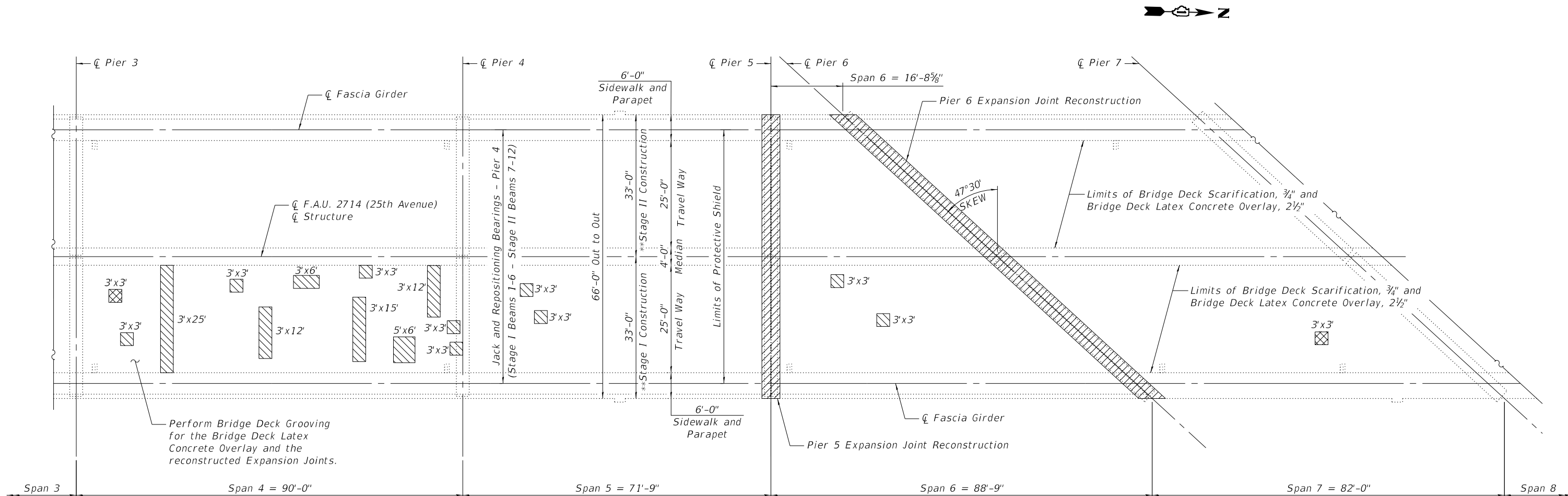
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (1 of 5)
STRUCTURE NO. 016-1011

SHEET 5-06 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	38
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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DECK PLAN

NOTES:

1. See Sheet S-06 for Bill of Material and Notes.
2. See Sheet S-06 for Jacking Loads.

**Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2 1/2".



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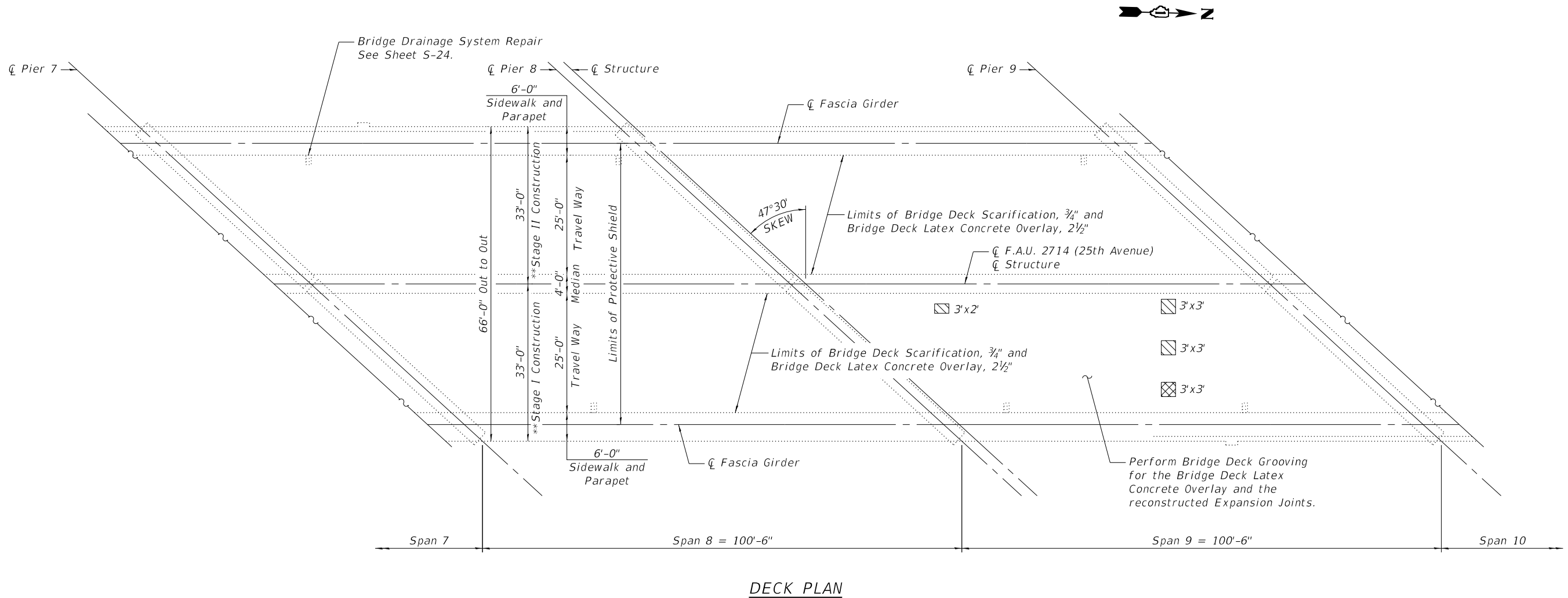
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (2 of 5)
STRUCTURE NO. 016-1011

SHEET S-07 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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NOTE:

1. See Sheet S-06 for Bill of Material and Notes.

** Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2½".



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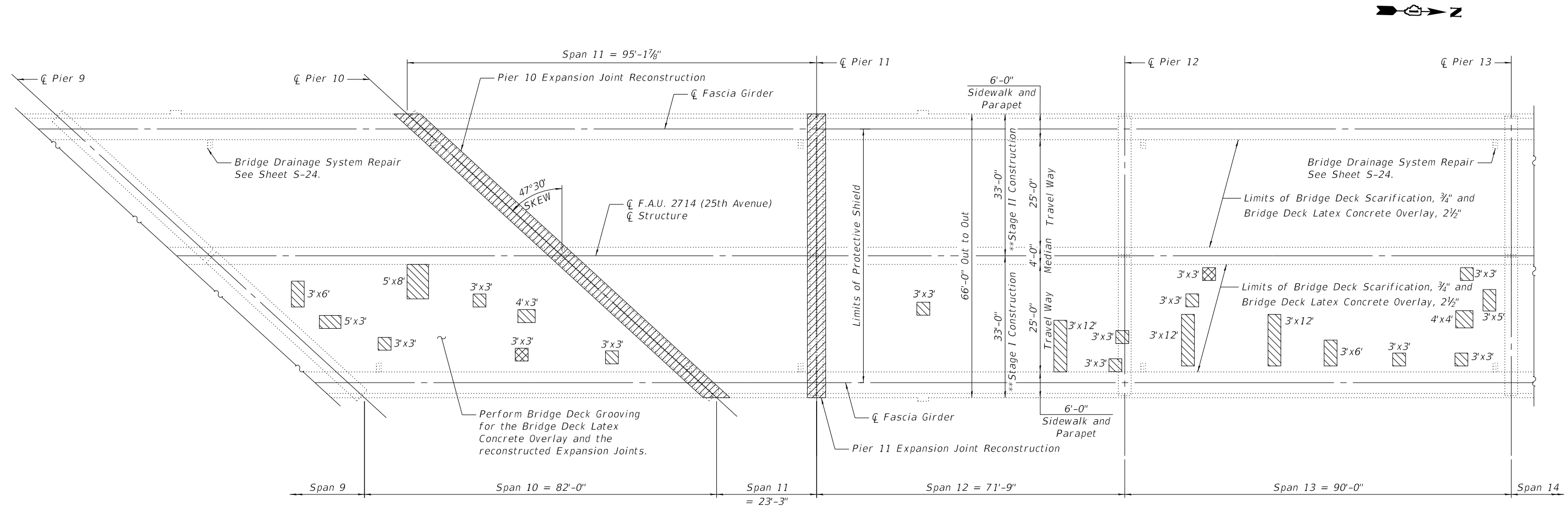
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (3 of 5)
STRUCTURE NO. 016-1011

SHEET S-08 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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DECK PLAN

NOTE:

1. See Sheet S-06 for Bill of Material and Notes.

**Deck Stage Construction limits shown.
Median construction at joints to be
constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided
for information only. Repair is included in the
cost of Bridge Deck Latex Concrete Overlay, 2 1/2".



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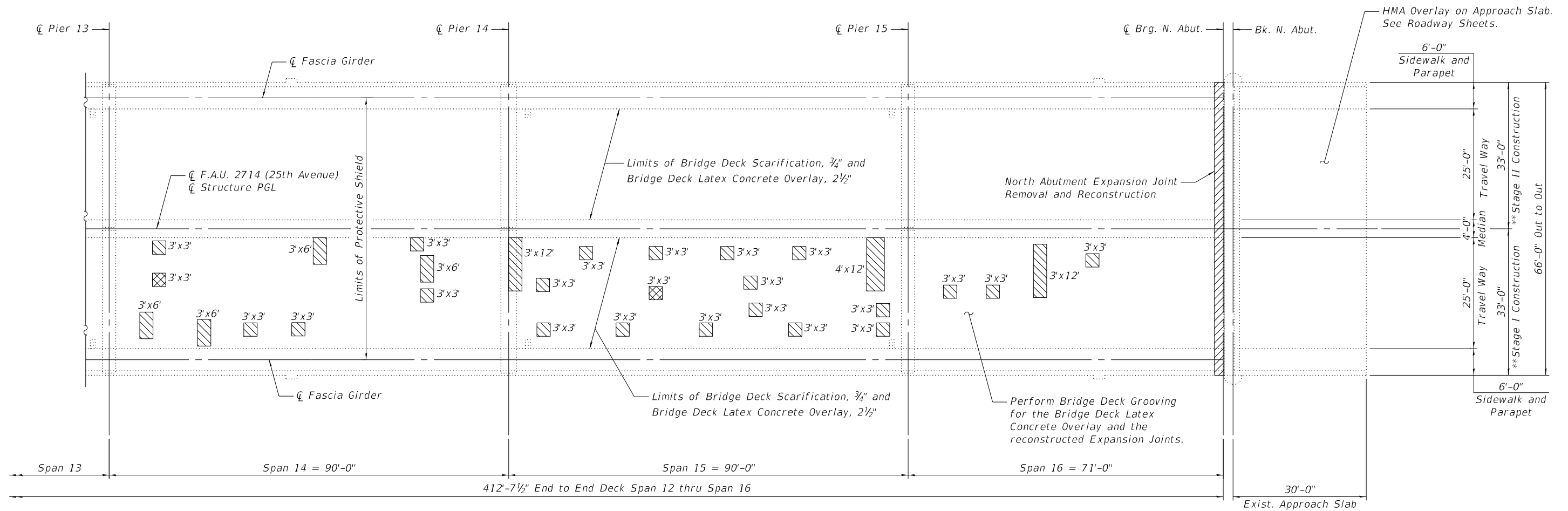
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (4 of 5)
STRUCTURE NO. 016-1011

SHEET S-09 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

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DECK PLAN

NOTE:

- See Sheet S-06 for Bill of Material and Notes.

** Deck Stage Construction limits shown.
Median construction at joints to be
constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided
for information only. Repair is included in the
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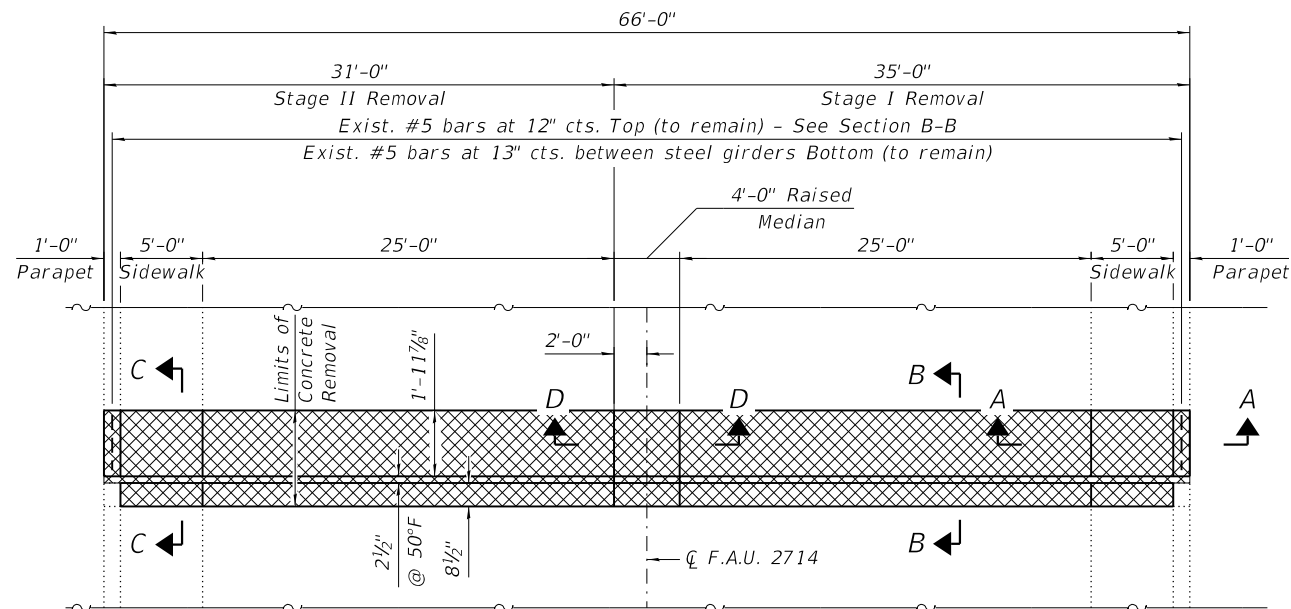
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

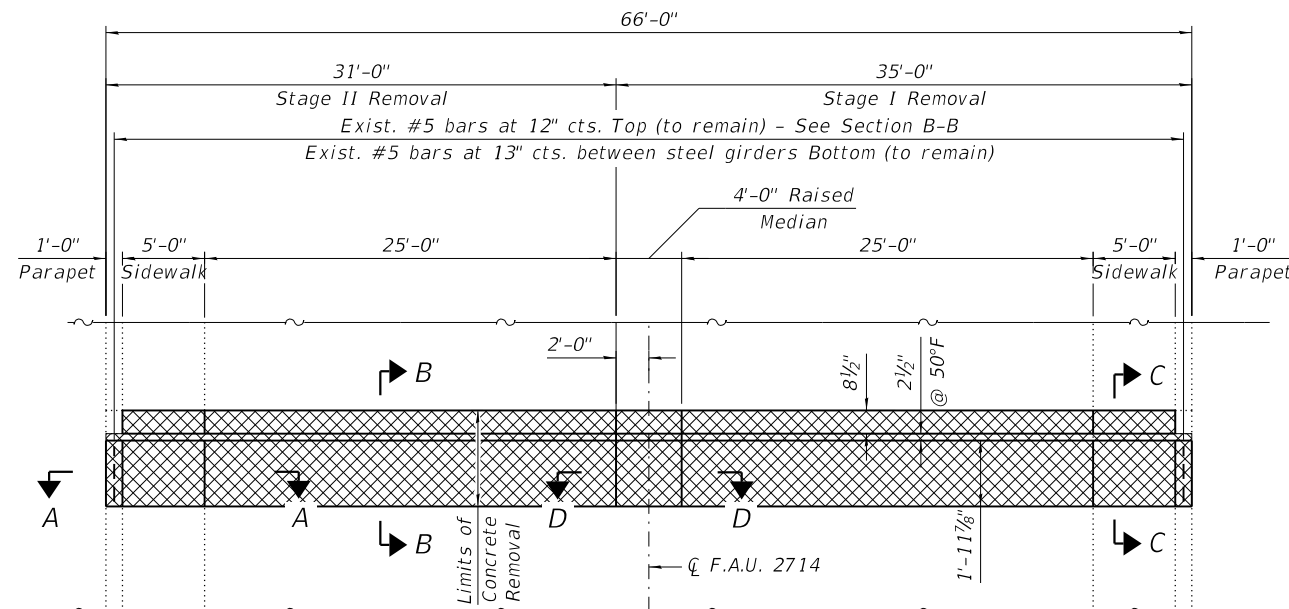
DECK REPAIR PLAN (5 of 5)
STRUCTURE NO. 016-1011

SHEET 5-10 OF 5-34 SHEETS

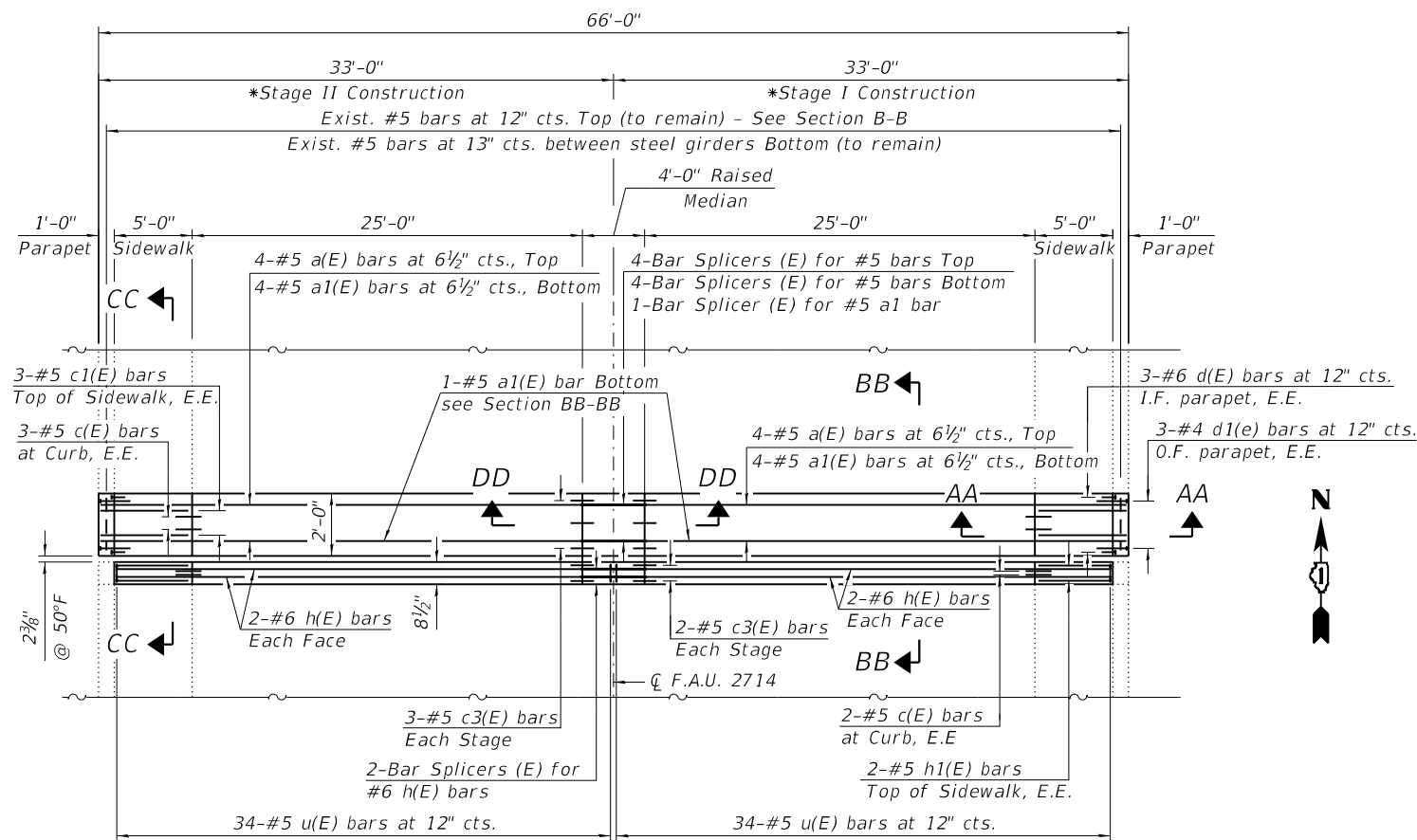
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		



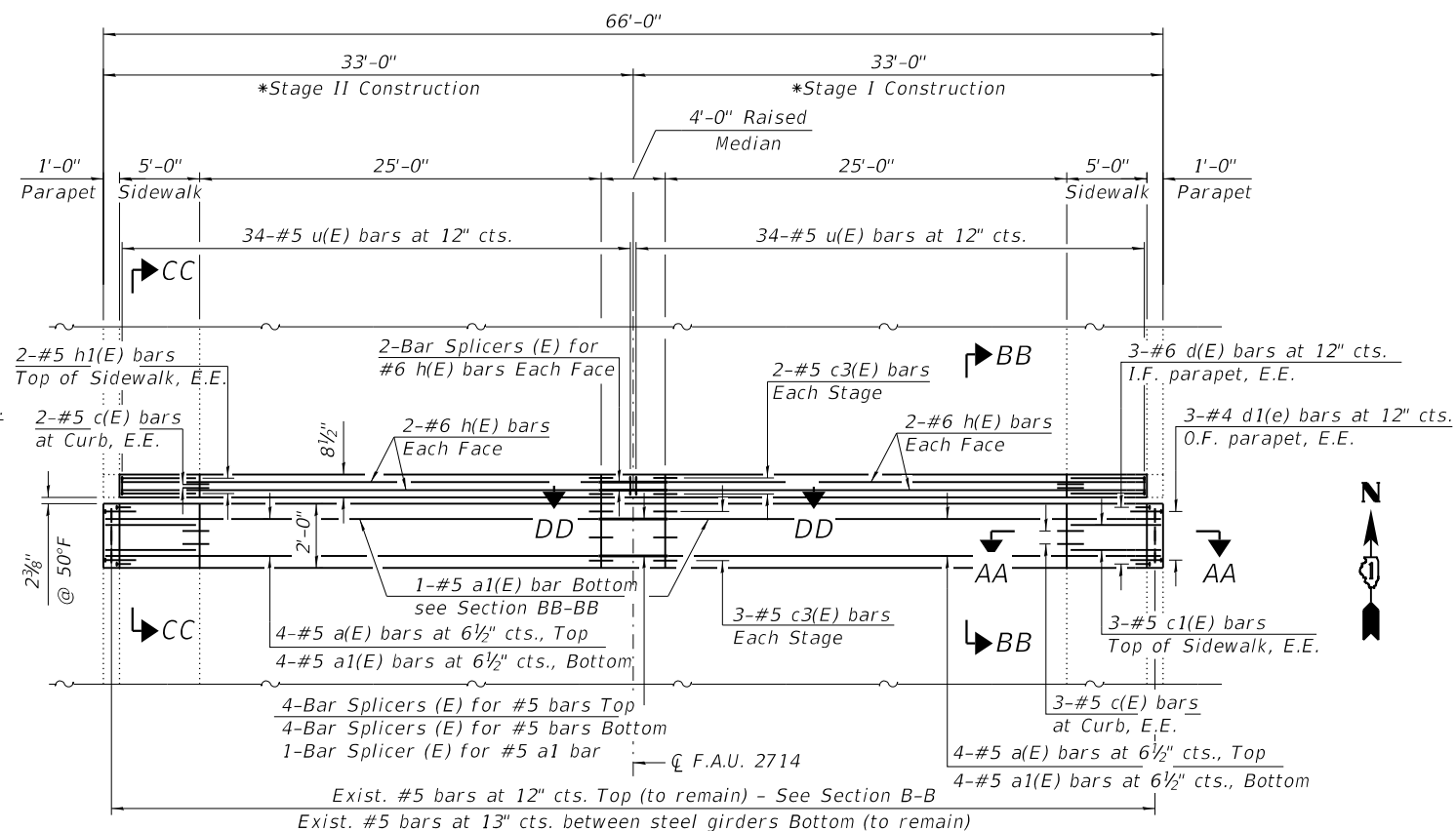
SOUTH ABUTMENT JOINT REMOVAL PLAN



NORTH ABUTMENT JOINT REMOVAL PLAN



SOUTH ABUTMENT JOINT RECONSTRUCTION PLAN



NORTH ABUTMENT JOINT RECONSTRUCTION PLAN

NOTES:

1. For preformed joint strip seal details, see Sheet S-15 thru S-17.
2. For Bar Splicer Assembly details, see Sheet S-34.
3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
4. See Sheet S-12 for Sections and Bill of Material.
5. Raised Median and c2(E) bar to be constructed in Stage II.

*Deck construction limits shown.
Median to be constructed in Stage II.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**N. AND S. ABUT. JOINT REMOVAL AND RECONSTRUCTION PLAN
STRUCTURE NO. 016-1011**

SHEET 5-11 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

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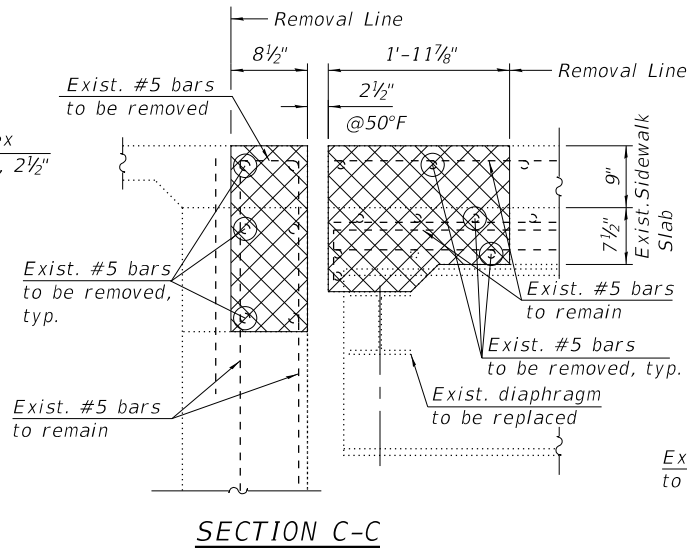
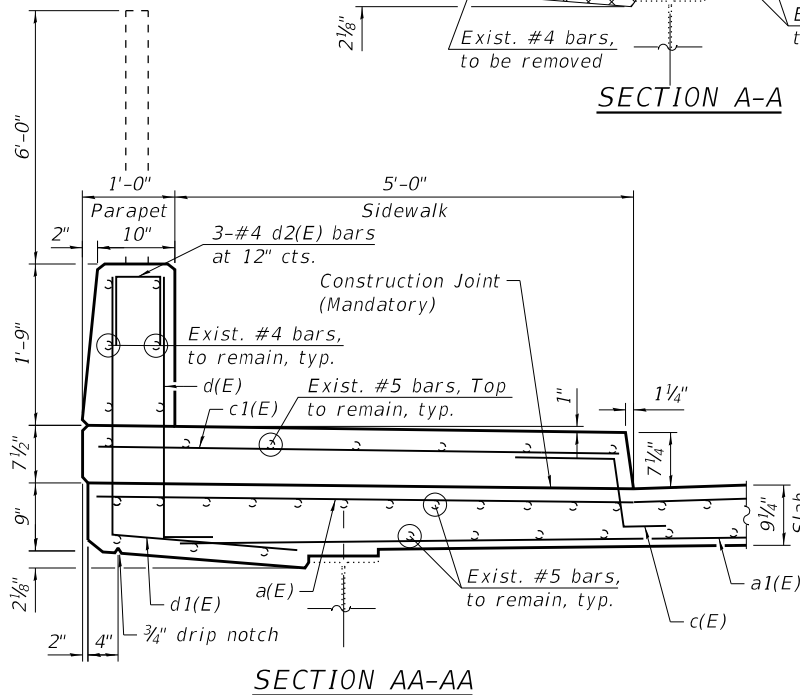
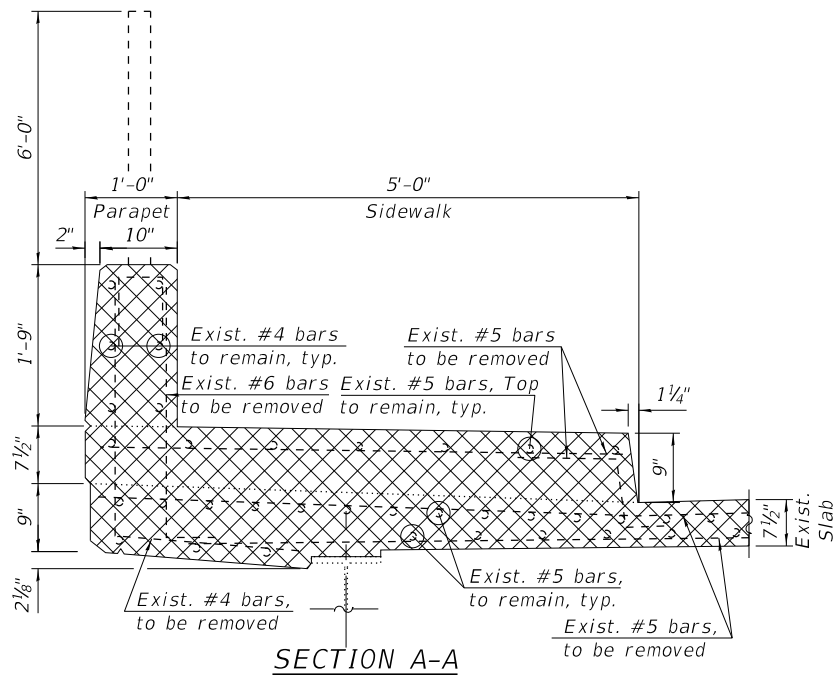
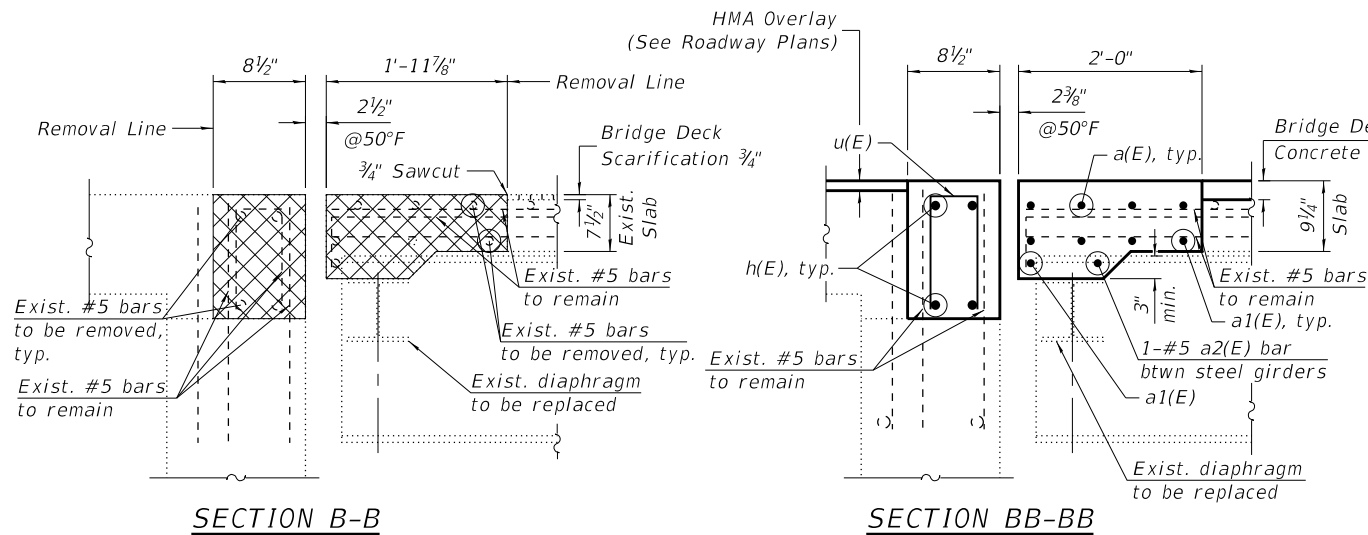
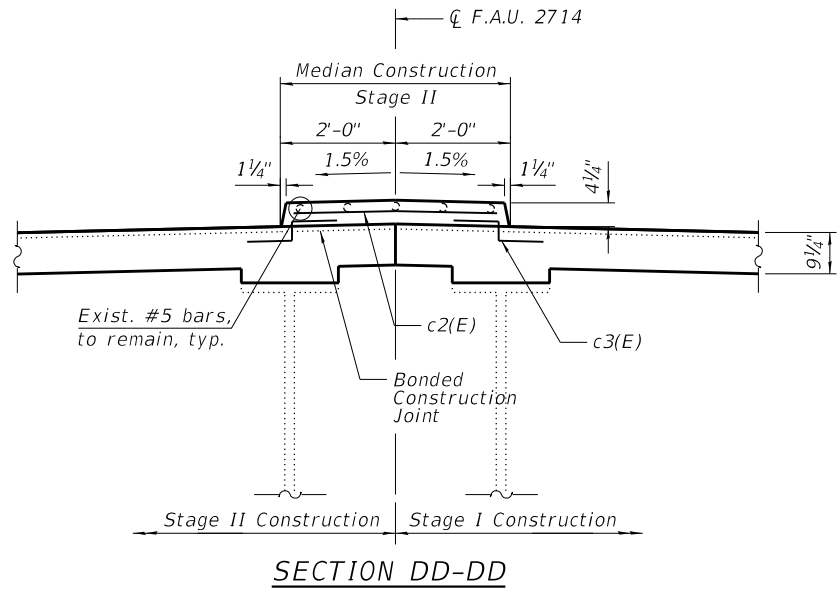
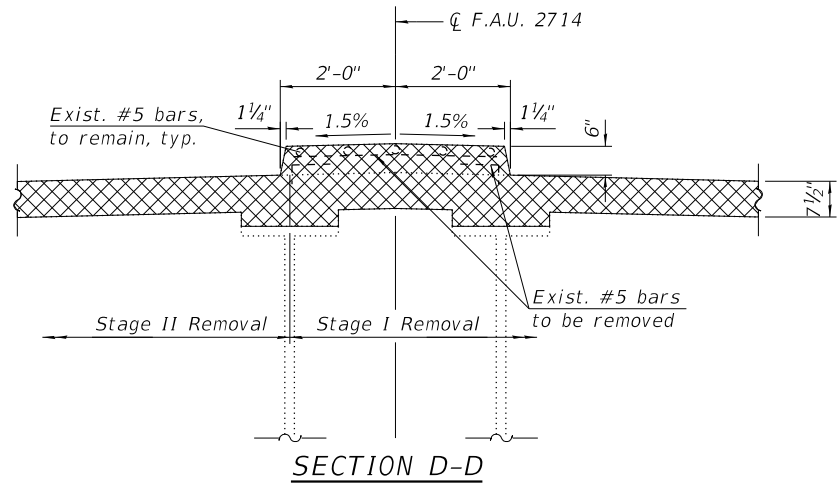
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DATE	-	10/06/2020

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

N. AND S. ABUT. JOINT REMOVAL AND RECONSTRUCTION DETAILS
STRUCTURE NO. 016-1011

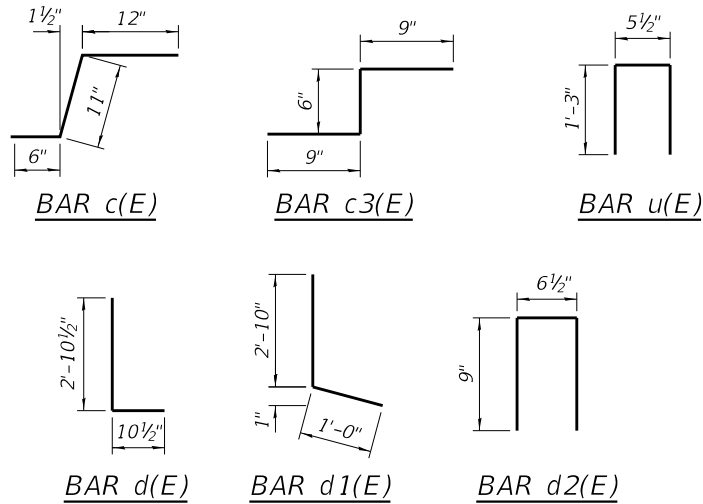
SHEET 5-12 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		



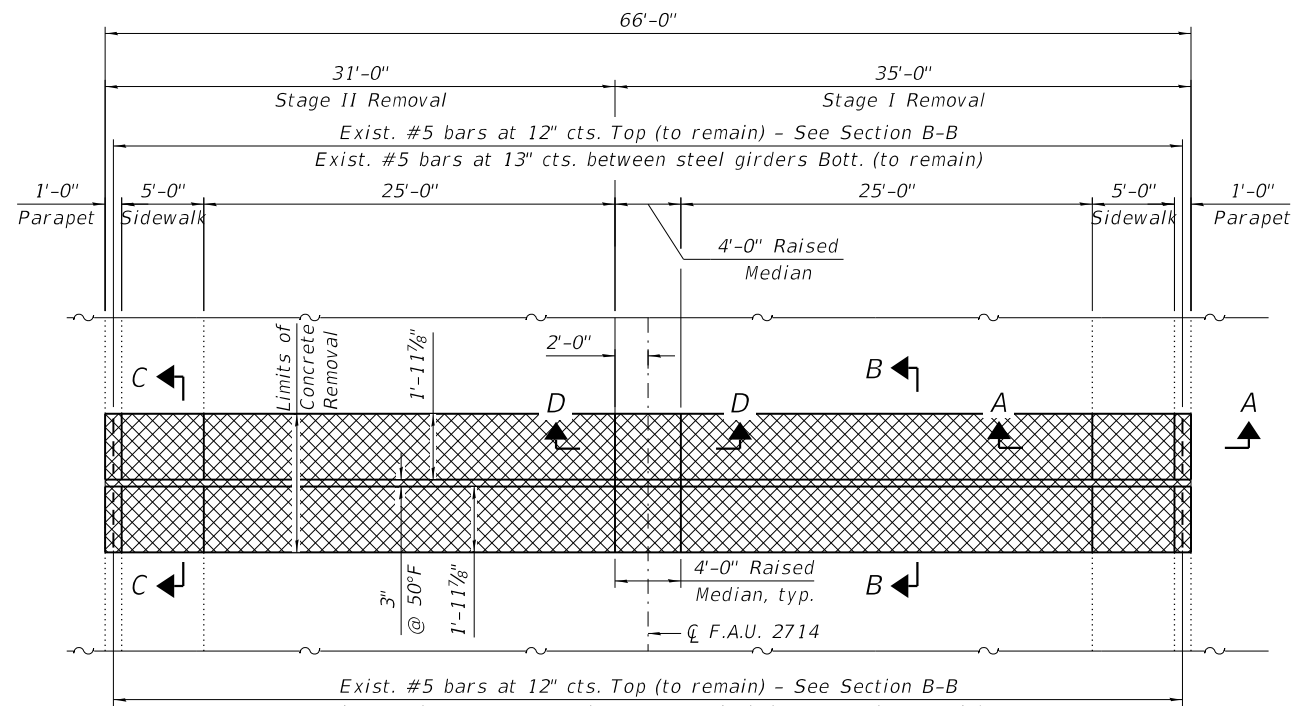
NOTE:

1. Cut a2(E) bars to fit between girders 6 and 7.

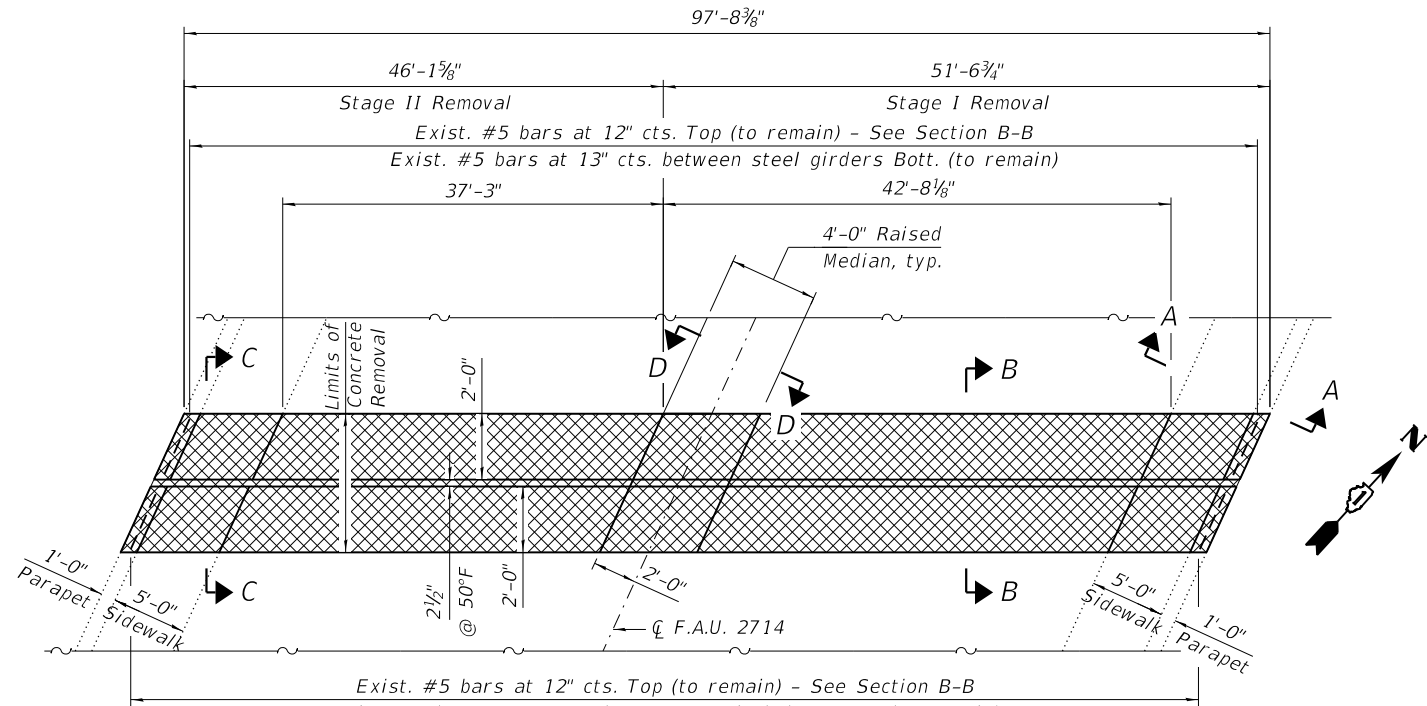


BILL OF MATERIAL

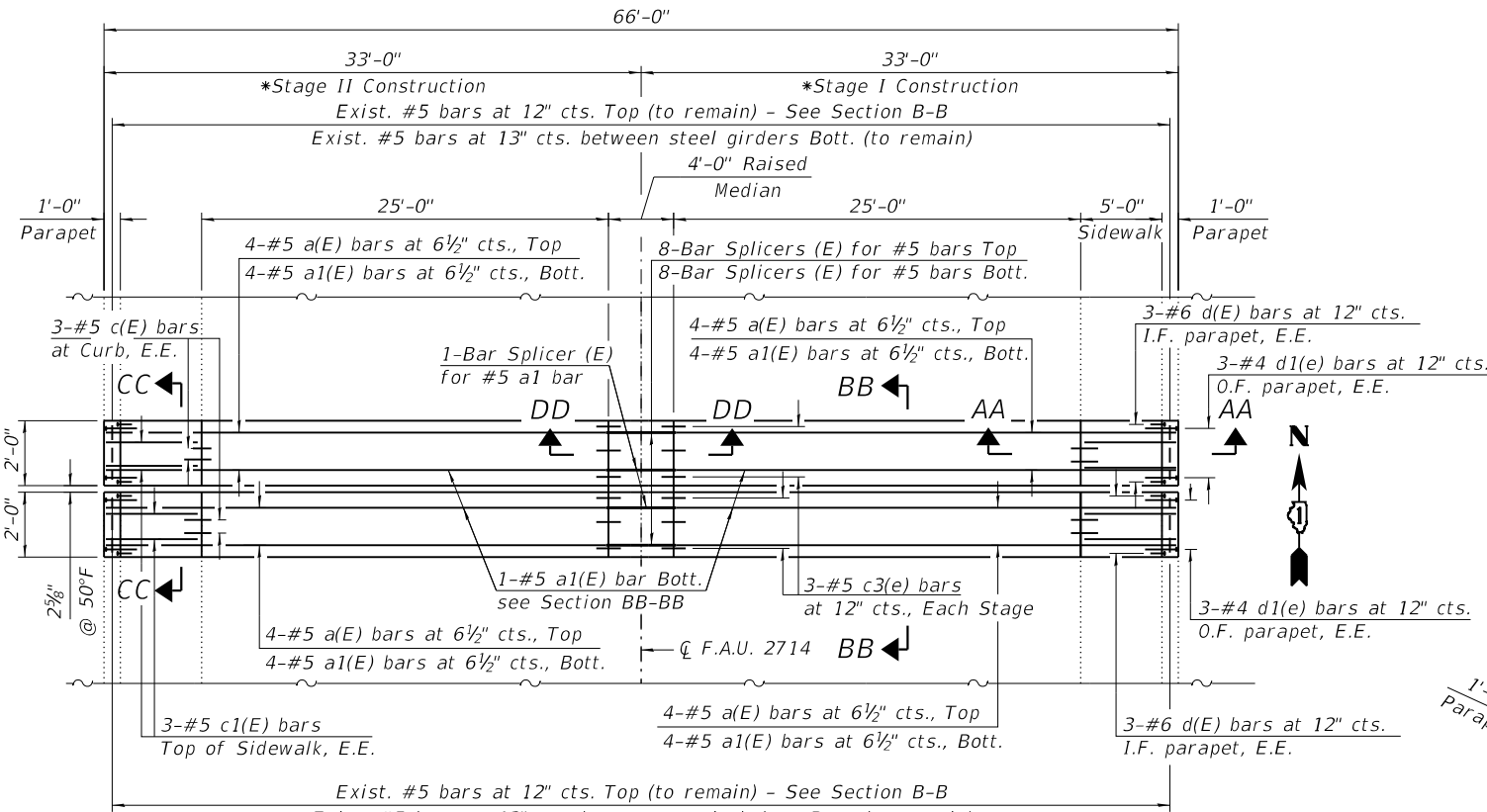
Bar	No.	Size	Length	Shape
a(E)	16	#5	32'-6"	
a1(E)	20	#5	31'-8"	
a2(E)	22	#5	5'-5"	
c(E)	20	#5	2'-5"	
c1(E)	12	#5	5'-6"	
c2(E)	10	#5	3'-6"	
c3(E)	20	#5	2'-0"	
d(E)	12	#6	3'-9"	
d1(E)	12	#4	3'-10"	
d2(E)	12	#4	2'-1"	
h(E)	16	#6	31'-8"	
h1(E)	8	#5	4'-6"	
u(E)	136	#5	3'-0"	
Concrete Removal			Cu. Yd.	17.1
Concrete Superstructure			Cu. Yd.	17.8
Reinforcement Bars, Epoxy Coated			Pound	2,870



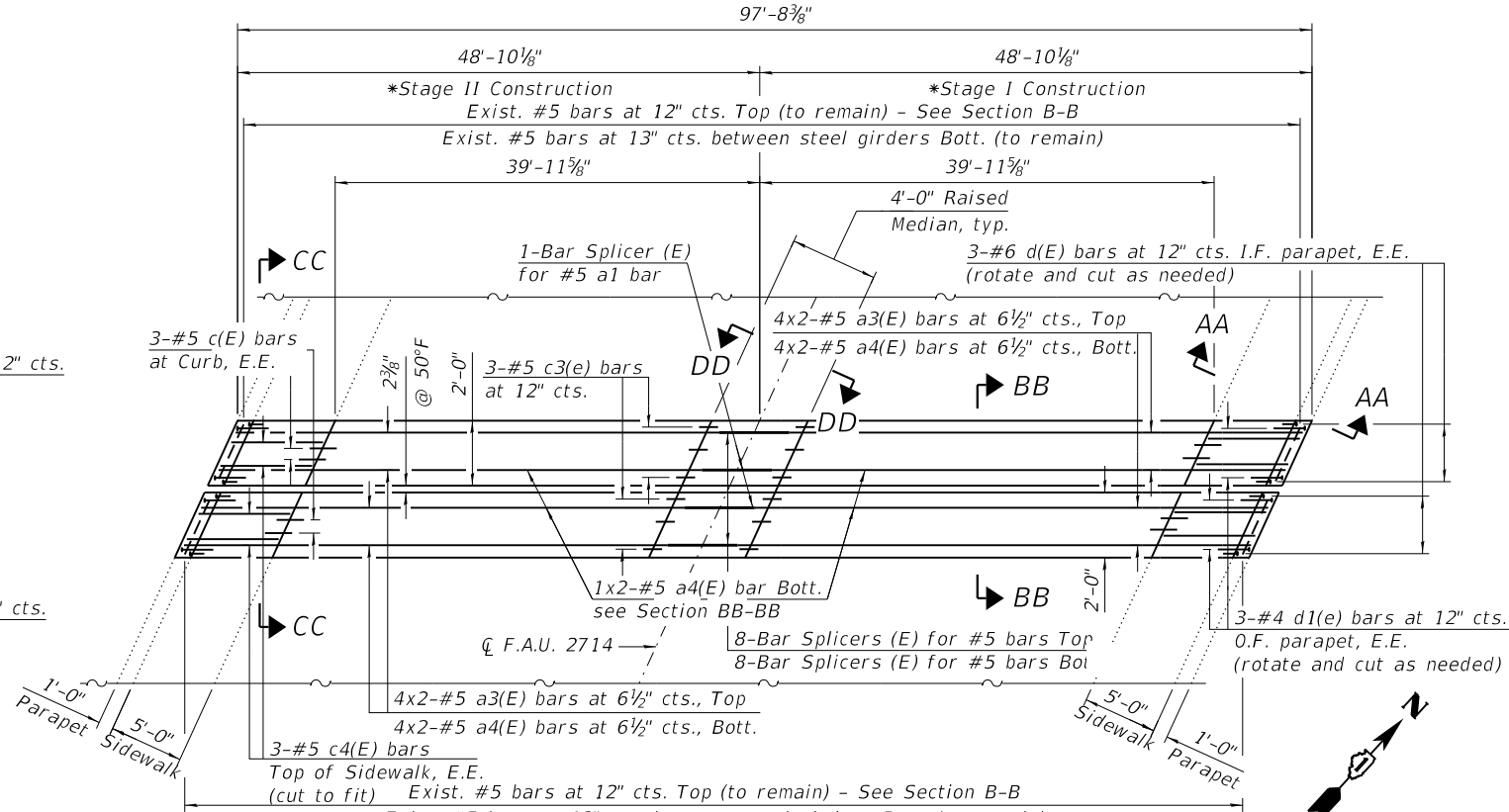
PIER 5 AND 11 JOINT REMOVAL PLAN



PIER 6 AND 10 JOINT REMOVAL PLAN



PIER 5 AND 11 JOINT RECONSTRUCTION PLAN



PIER 6 AND 10 JOINT RECONSTRUCTION PLAN

NOTES:

- For preformed joint strip seal details, see Sheet S-15 thru S-17.
- For Bar Splicer Assembly details, see Sheet S-34.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- See Sheet S-14 for Sections and Bill of Material.
- Raised Median and c2(E) bar to be constructed in Stage II.

*Deck construction limits shown.
Median to be constructed in Stage II.

MINIMUM BAR LAP

#5 bar = 3'-9"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIERS 5, 6, 10 AND 11 JOINT REMOVAL AND RECONSTRUCTION PLAN
STRUCTURE NO. 016-1011

SHEET S-13 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	45
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

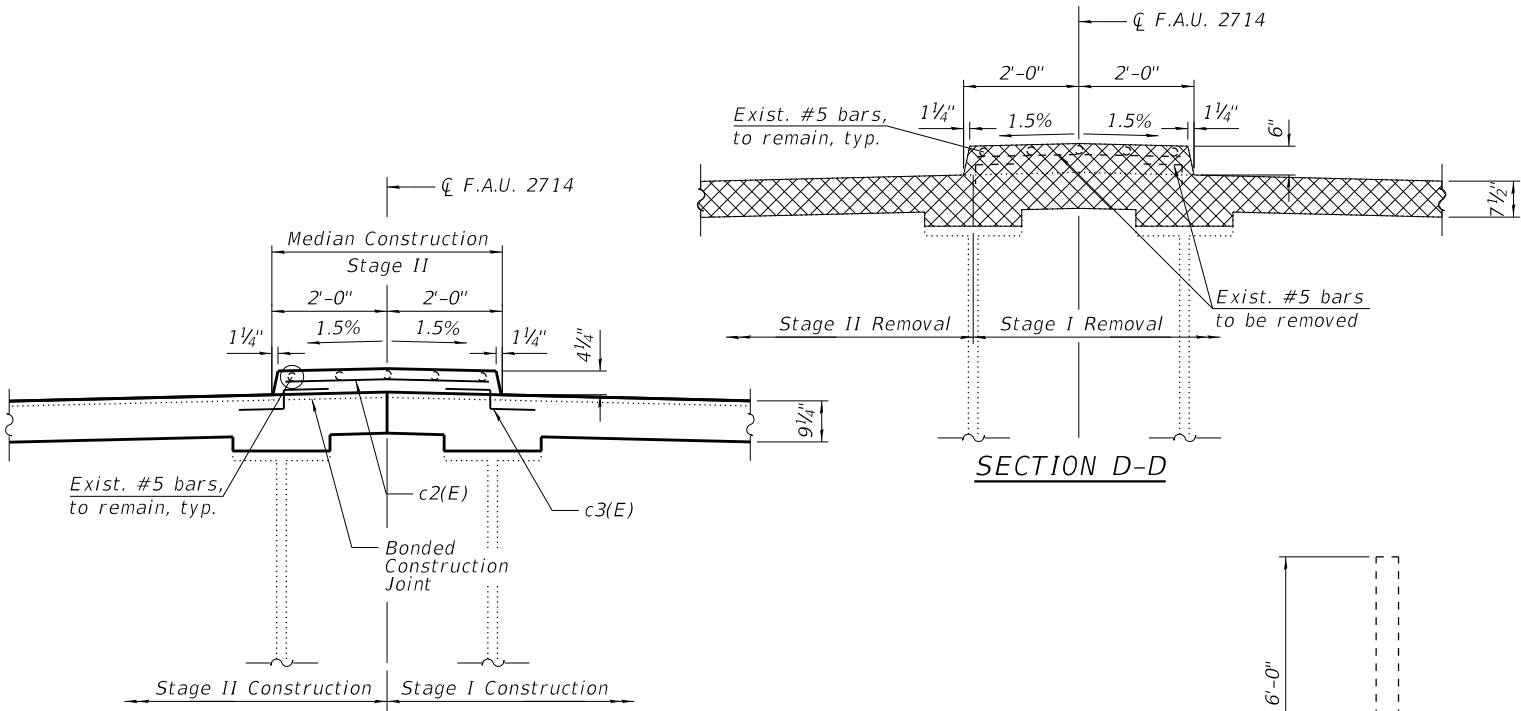
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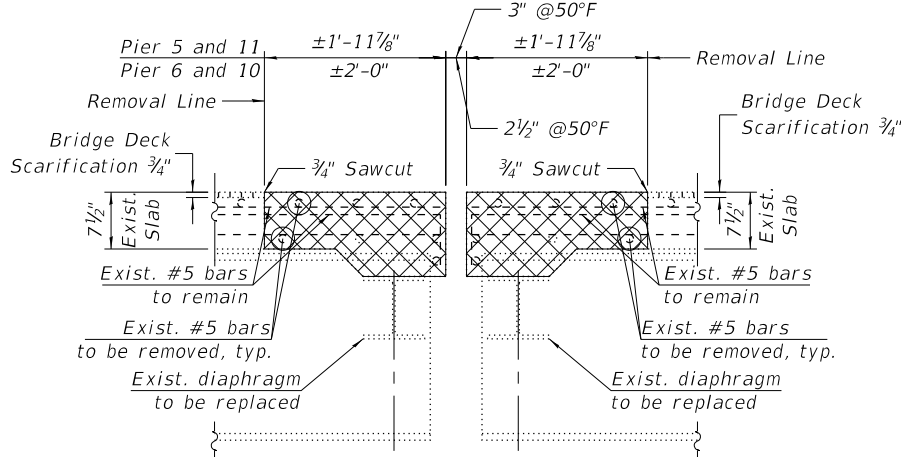
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BILL OF MATERIAL

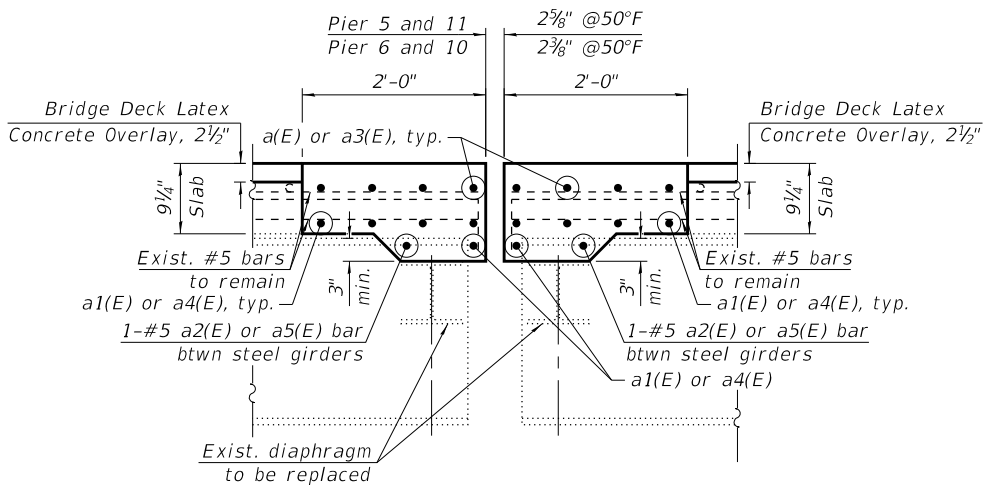
Bar	No.	Size	Length	Shape
a(E)	32	#5	32'-6"	
a1(E)	40	#5	31'-8"	
a2(E)	44	#5	5'-5"	
a3(E)	64	#5	26'-4"	
a4(E)	80	#5	25'-7"	
a5(E)	44	#5	8'-2"	
c(E)	48	#5	2'-5"	
c1(E)	24	#5	5'-6"	
c2(E)	24	#5	3'-6"	
c3(E)	48	#5	2'-0"	
c4(E)	24	#5	8'-4"	
d(E)	48	#6	3'-9"	
d1(E)	48	#4	3'-10"	
d2(E)	48	#4	2'-1"	
Concrete Removal			Cu. Yd.	52.5
Concrete Superstructure			Cu. Yd.	60.0
Reinforcement Bars, Epoxy Coated			Pound	8,040



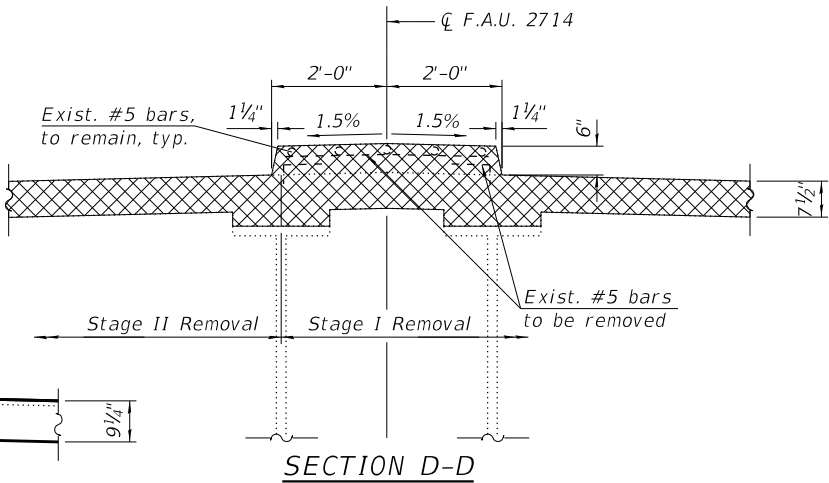
SECTION DD-DD



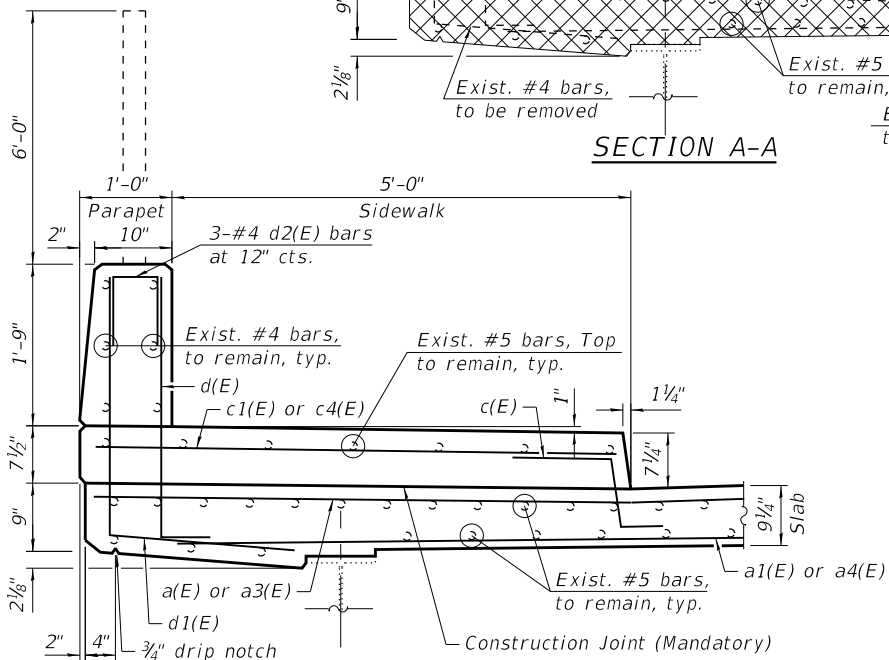
SECTION B-B



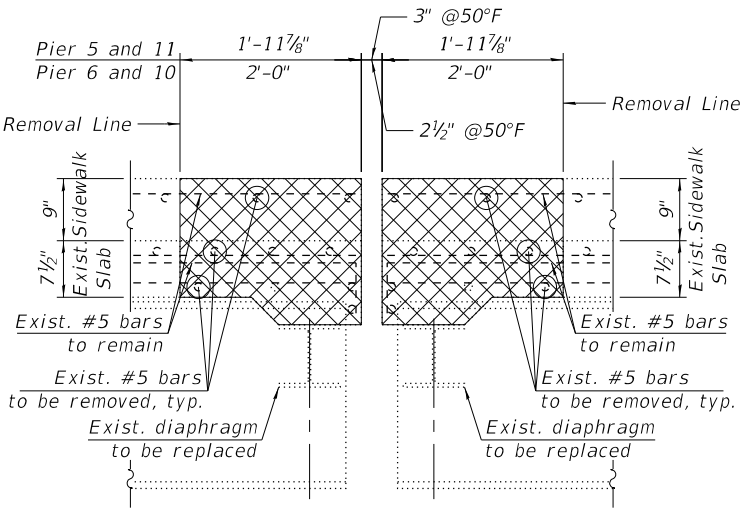
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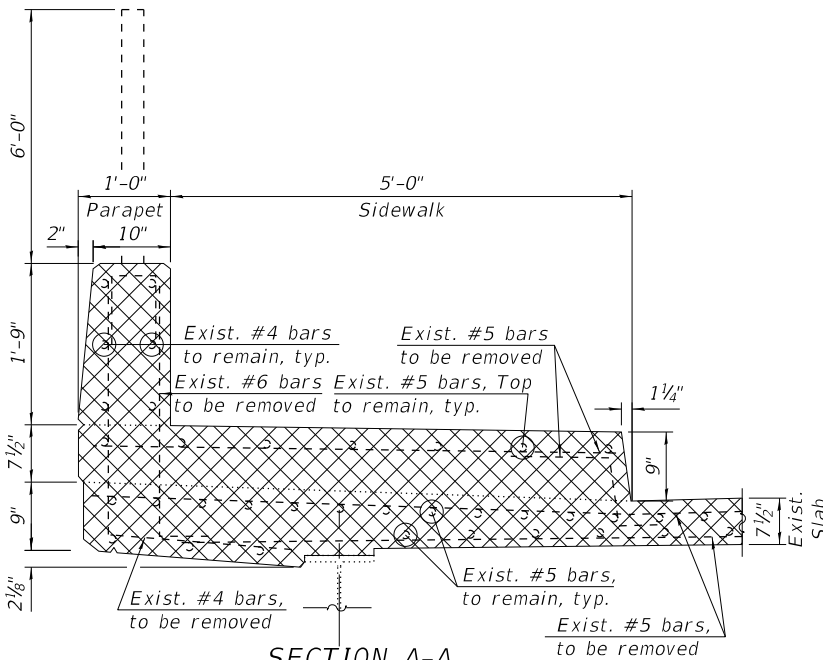
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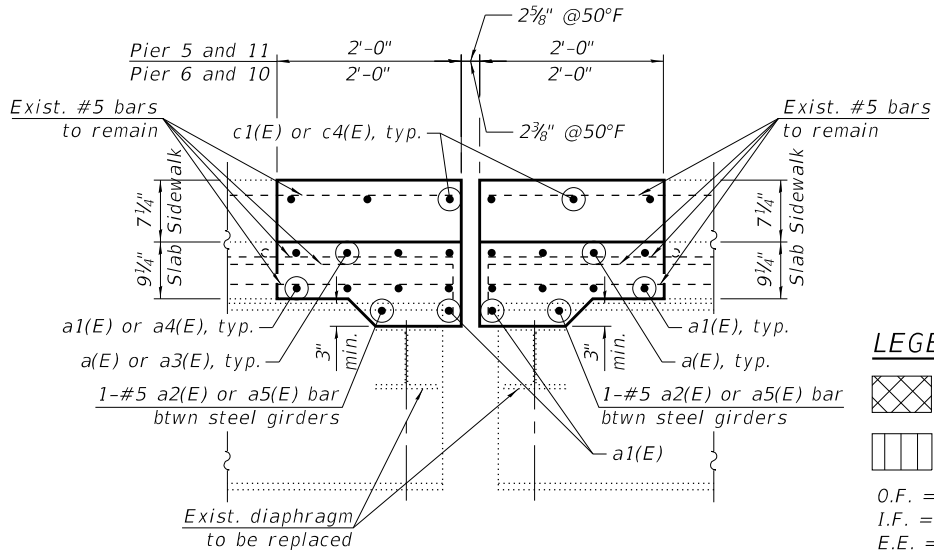
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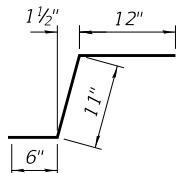
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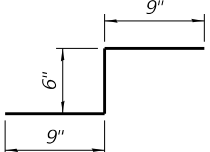
SECTION A-A



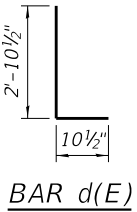
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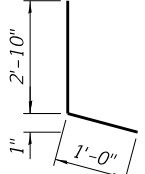
BAR c(E)



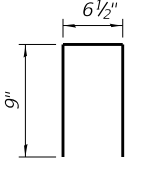
BAR c3(E)



BAR d(E)



BAR d1(E)



BAR d2(E)

NOTE:

1. Cut a2(E) a5(E) bars to fit between girders 6 and 7.

LEGEND:



O.F. = Outside Face
I.F. = Inside Face
E.E. = Each End

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 5, 6, 10 AND 11 JOINT REMOVAL AND RECONSTRUCTION DETAILS
STRUCTURE NO. 016-1011

SHEET 5-14 OF 5-34 SHEETS

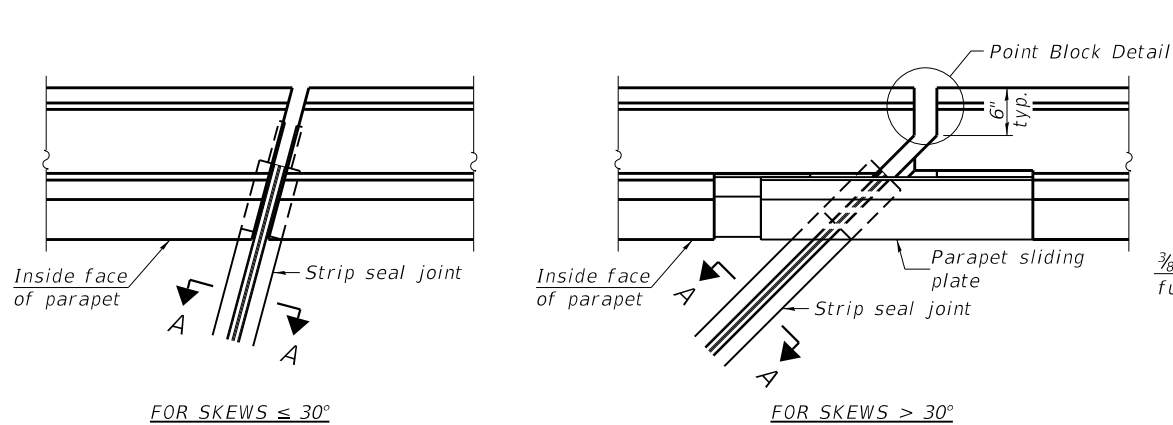
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2714	2020-161-BR	COOK	79	46
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				



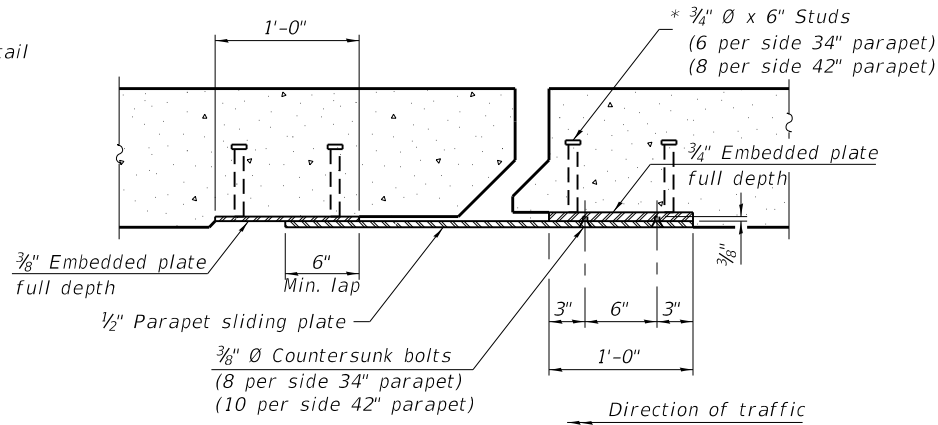
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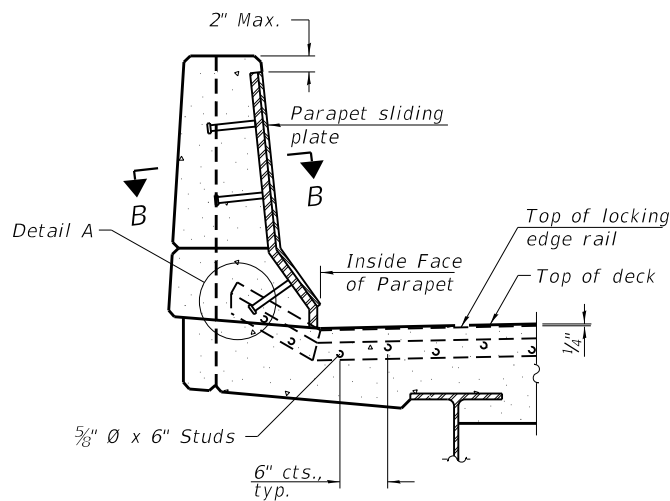
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PLAN AT PARAPET

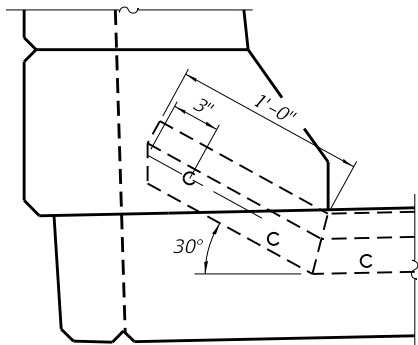


SECTION B-B



ELEVATION AT PARAPET

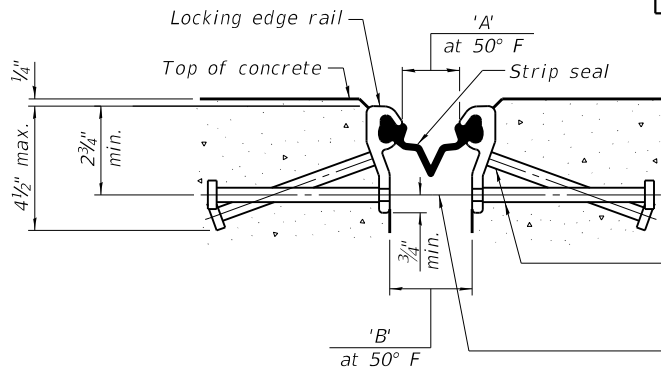
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL A

JOINT OPENING TABLE

Joint	A	B	C
S. Abut.	1 1/2"	2 3/8"	3"
Pier 5	1 3/4"	2 7/8"	3 1/4"
Pier 6	1 1/2"	2 3/8"	3"
Pier 10	1 1/2"	2 3/8"	3"
Pier 11	1 3/4"	2 7/8"	3 1/4"
E. Abut.	1 1/2"	2 3/8"	3"



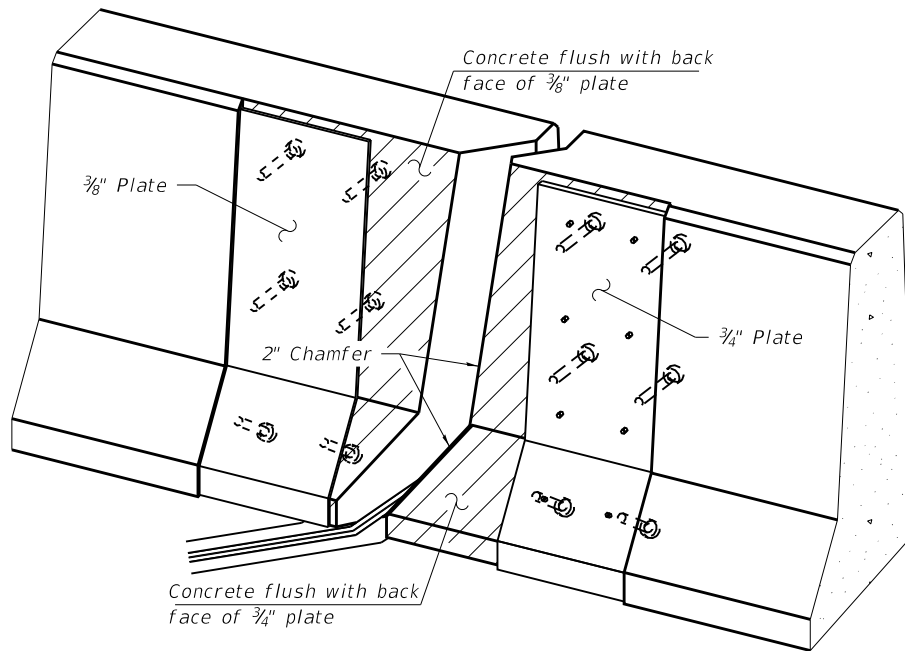
SHOWING ROLLED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

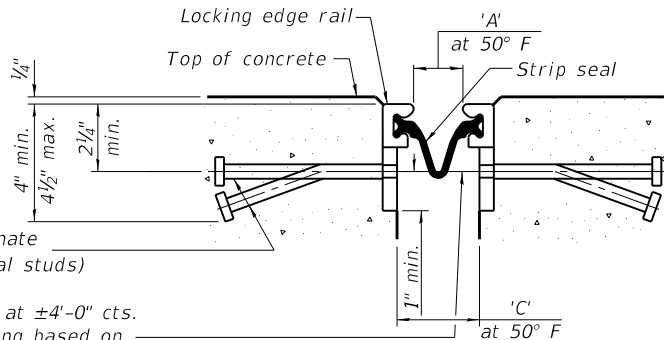
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

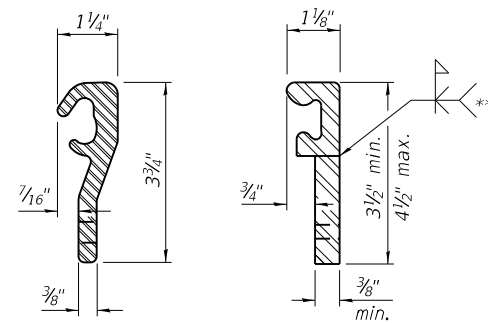


TRIMETRIC VIEW

(Showing embedded plates only)



SHOWING WELDED RAIL JOINT

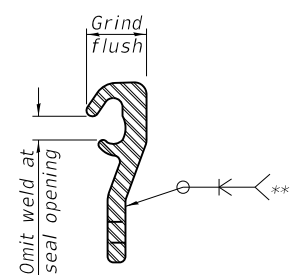


ROLLED
(EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	460



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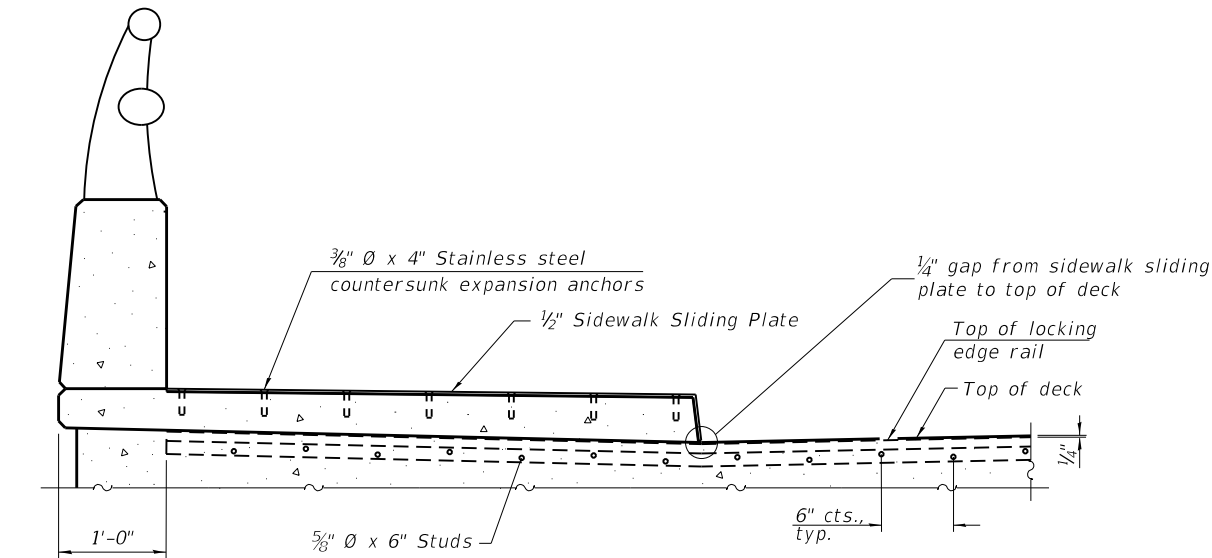
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK (1 OF 3)
STRUCTURE NO. 016-1011

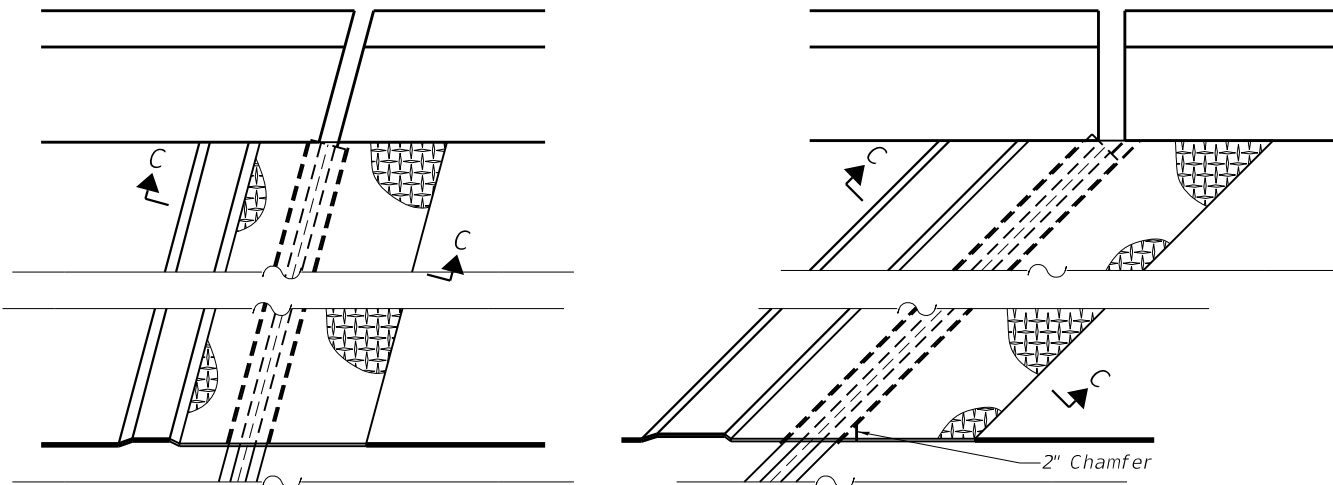
SHEET 5-15 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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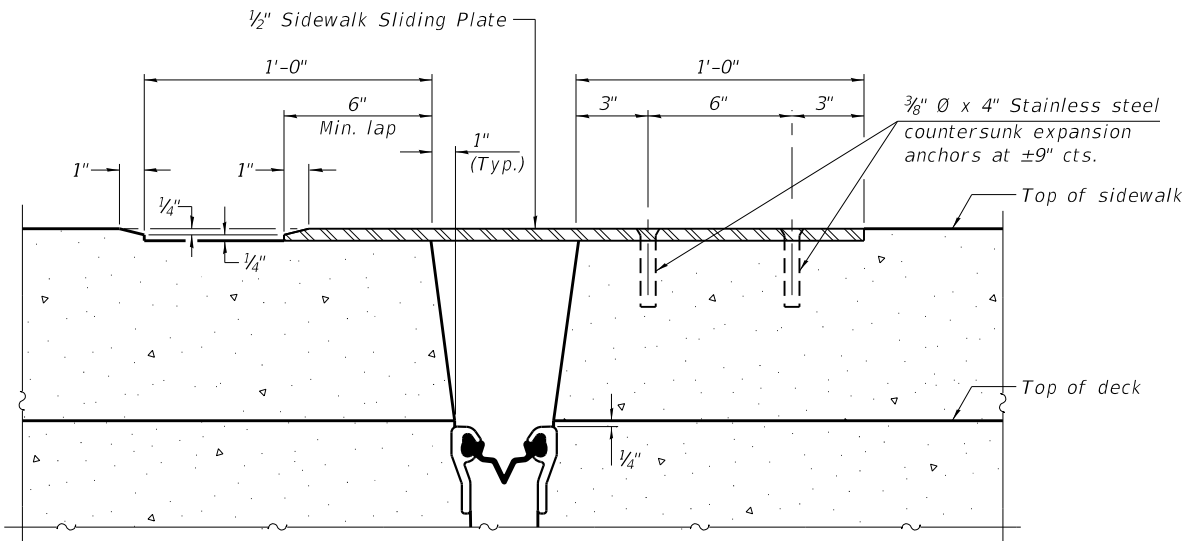
ELEVATION AT RAISED SIDEWALK



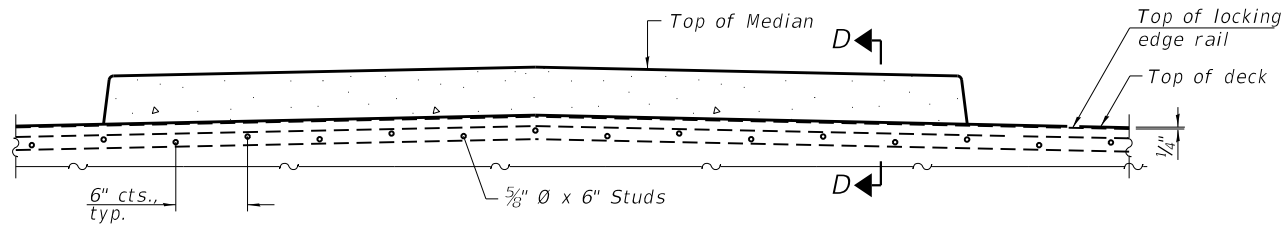
(FOR SKEWS $\leq 30^\circ$)

(FOR SKEWS $> 30^\circ$)

PLAN AT RAISED SIDEWALK

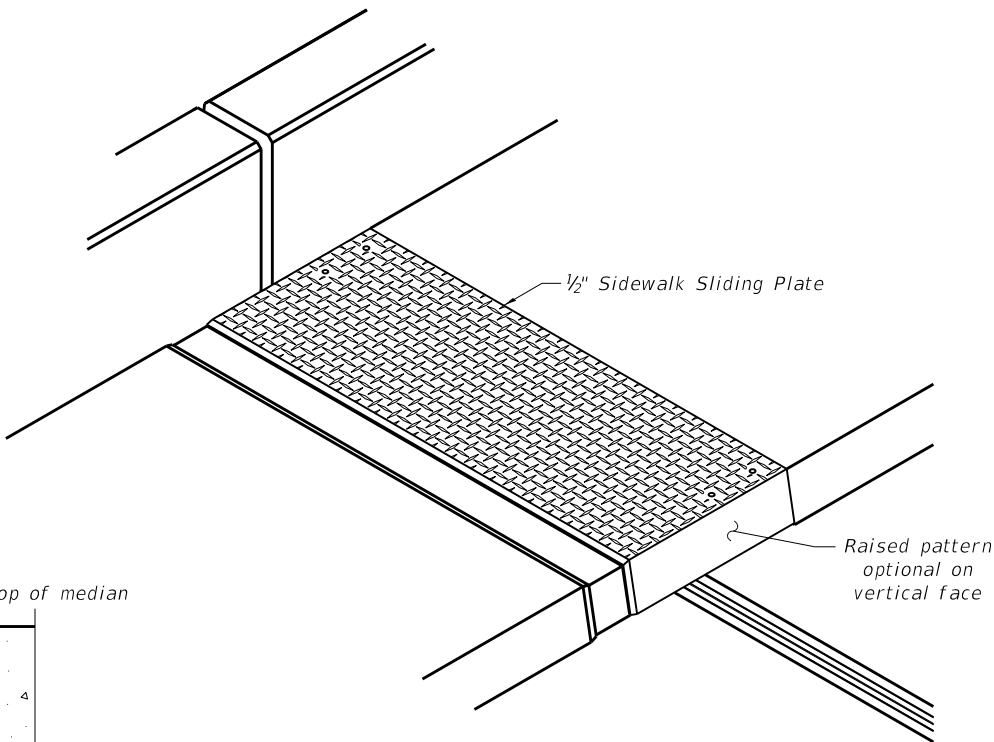


SECTION C-C

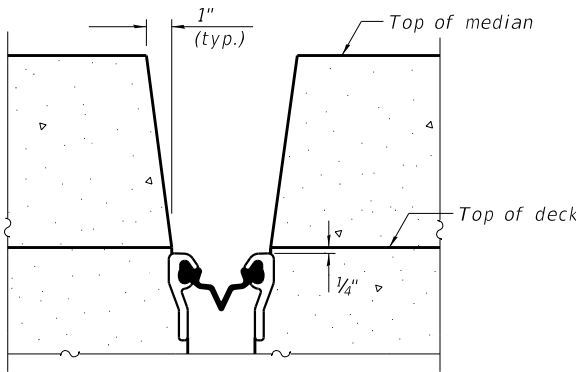


ELEVATION AT MEDIAN

For skews $> 30^\circ$, chamfer acute corners 2" similar to sidewalk.



TRIMETRIC VIEW



SECTION D-D

(at Rt. L's)

EJ-SS-S

8-11-17



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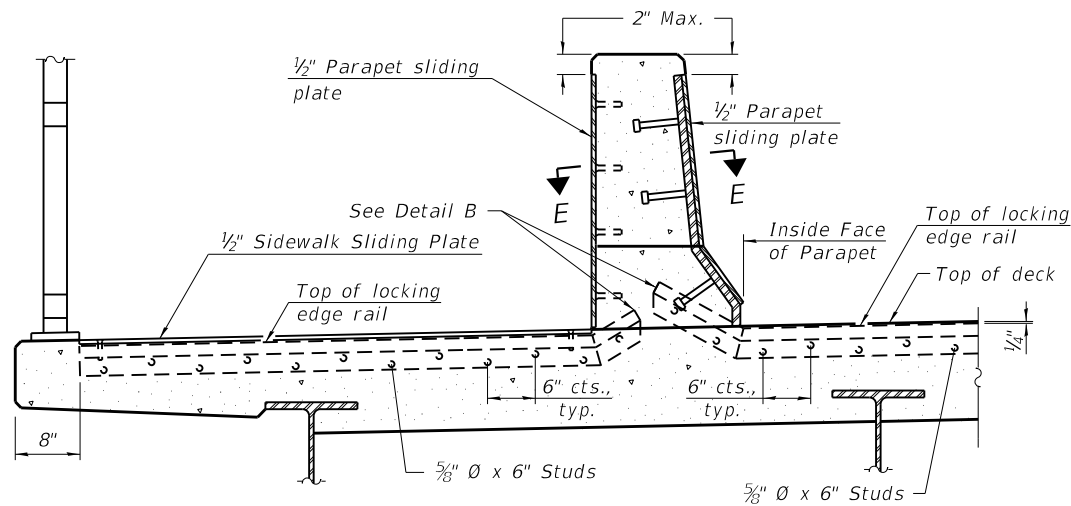
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK (2 OF 3)
STRUCTURE NO. 016-1011

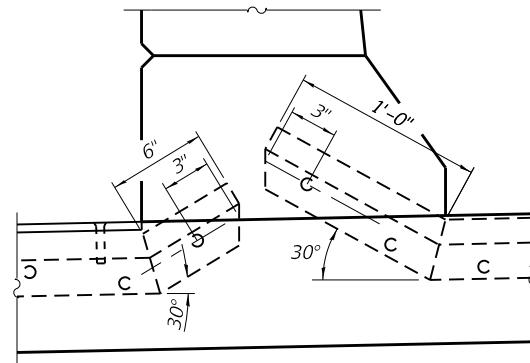
SHEET 5-16 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	48
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

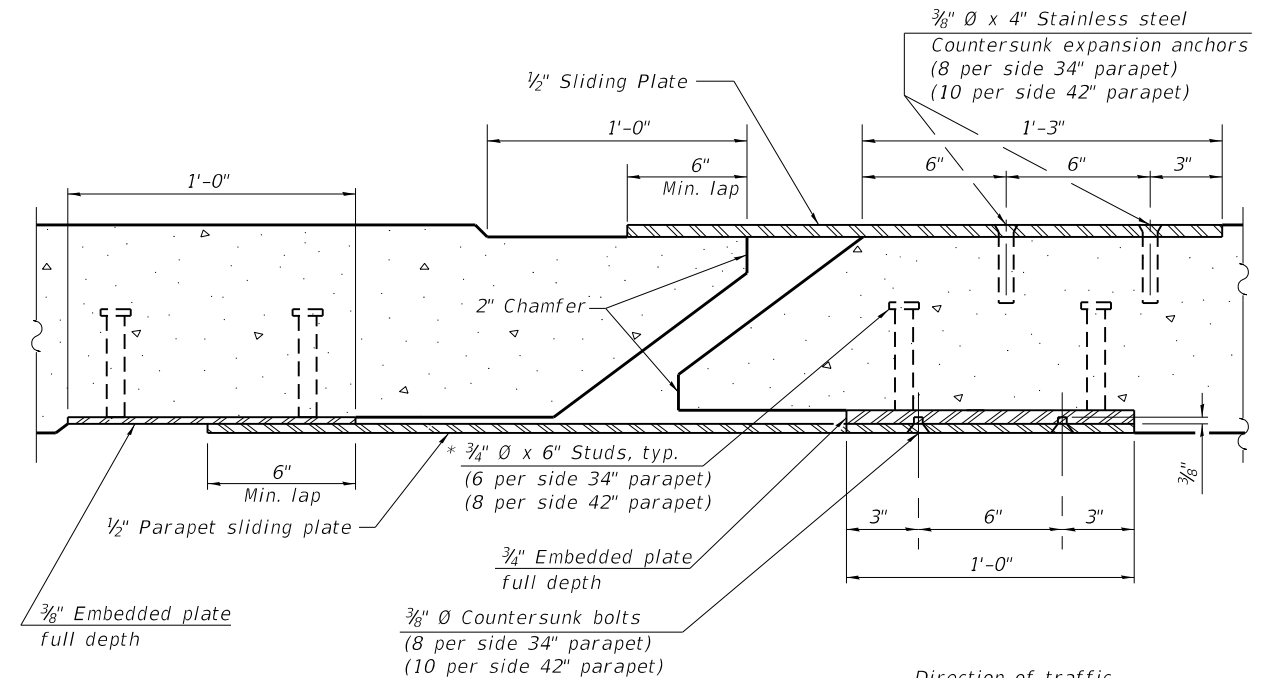
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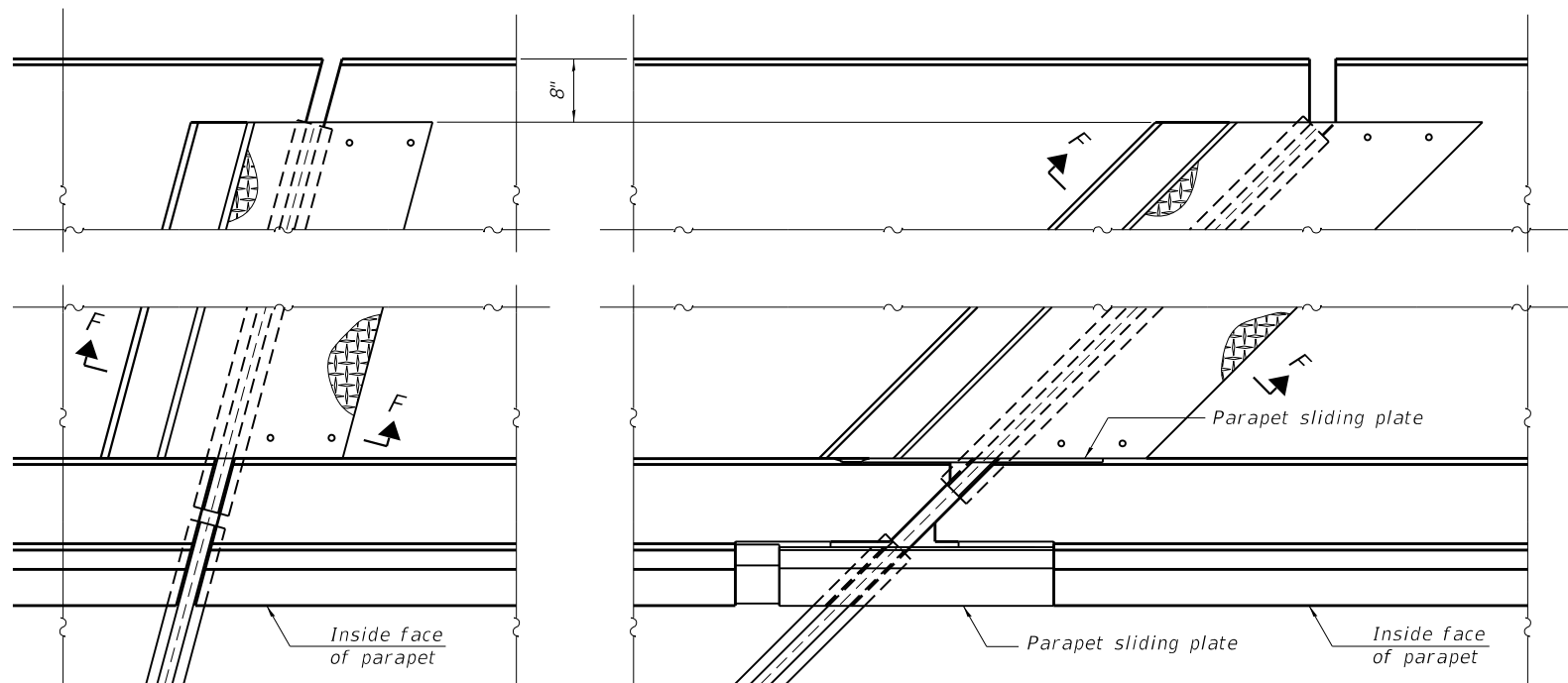
ELEVATION AT DECK LEVEL SIDEWALK
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL B



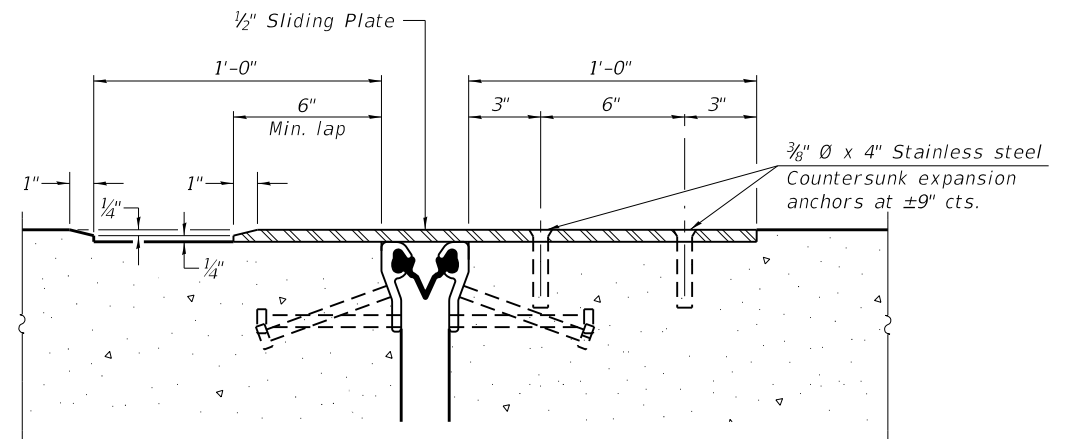
SECTION E-E



(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT DECK LEVEL SIDEWALK



SECTION F-F

EJ-SS-S

8-11-17



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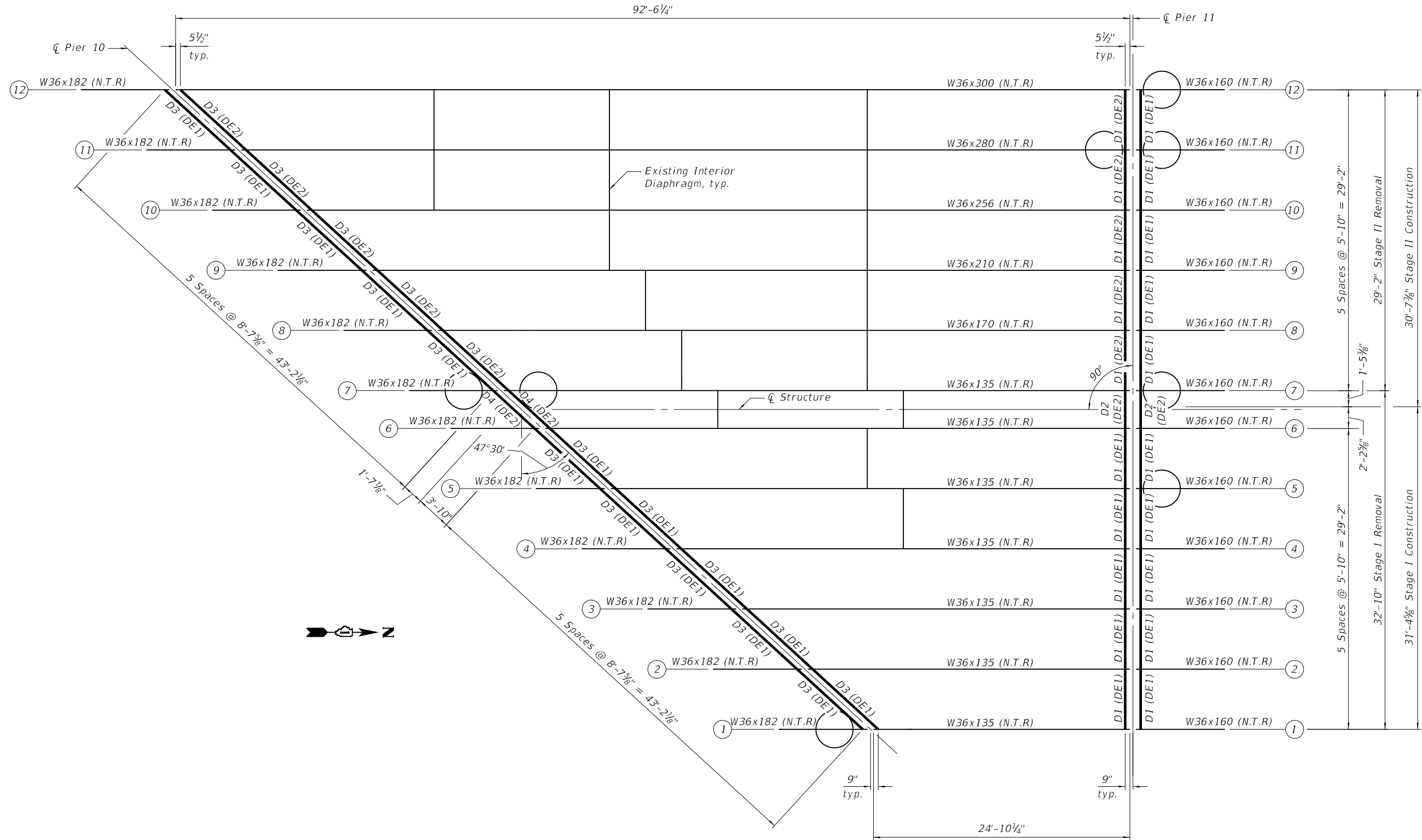
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL - SIDEWALK (3 OF 3)
STRUCTURE NO. 016-1011

SHEET 5-17 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

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NOTES:

- For Beam End Repairs, Diaphragm Removal and Replacement Details and Bill of Materials, see Sheet S-20 thru S-23.
- DE1 are W16x36 existing diaphragms and DE2 are W12x40 existing diaphragms. D1, D2, D3 and D4 are proposed C15x33.9 diaphragms.

Proposed Diaphragm Name — D1 (DE1) — Existing Diaphragm Name

LEGEND

- Perform Beam End Plating. Paid for as Structural Steel Repair.
- Remove and Replace Existing Diaphragm. Paid for as Structural Steel Removal and Furnishing and Erecting Structural Steel.



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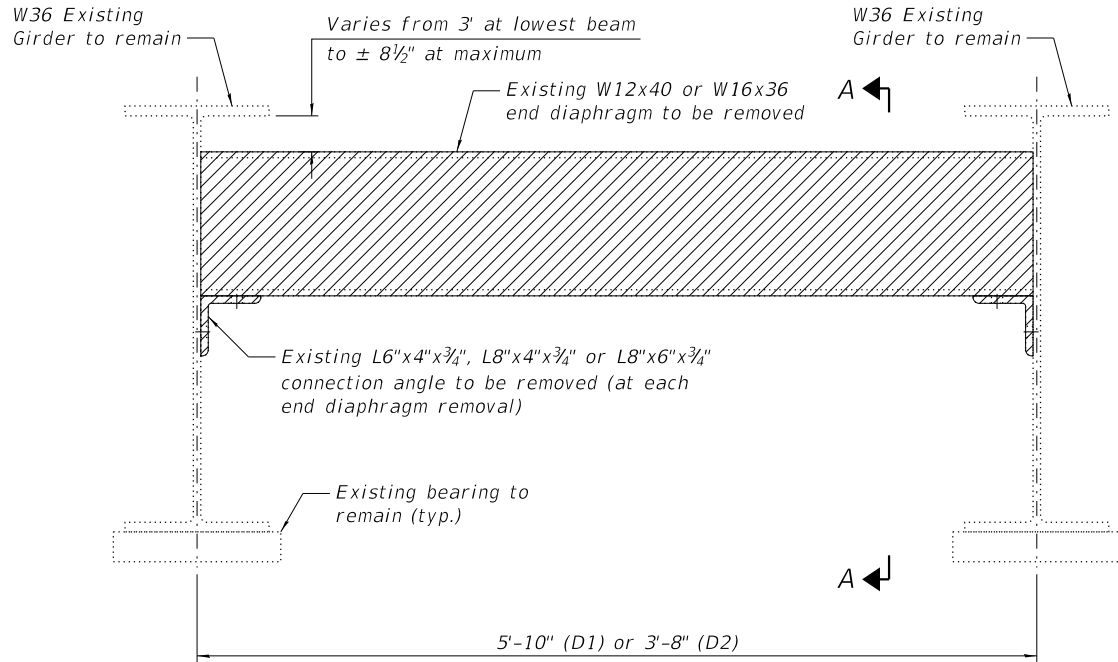
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARTIAL FRAMING PLAN (2 OF 2)
STRUCTURE NO. 016-1011

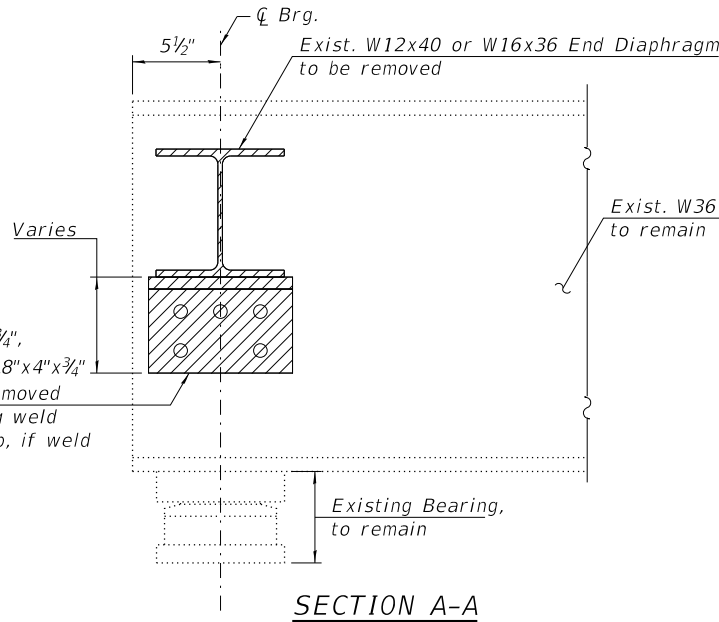
SHEET 5-19 OF 5-34 SHEETS

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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

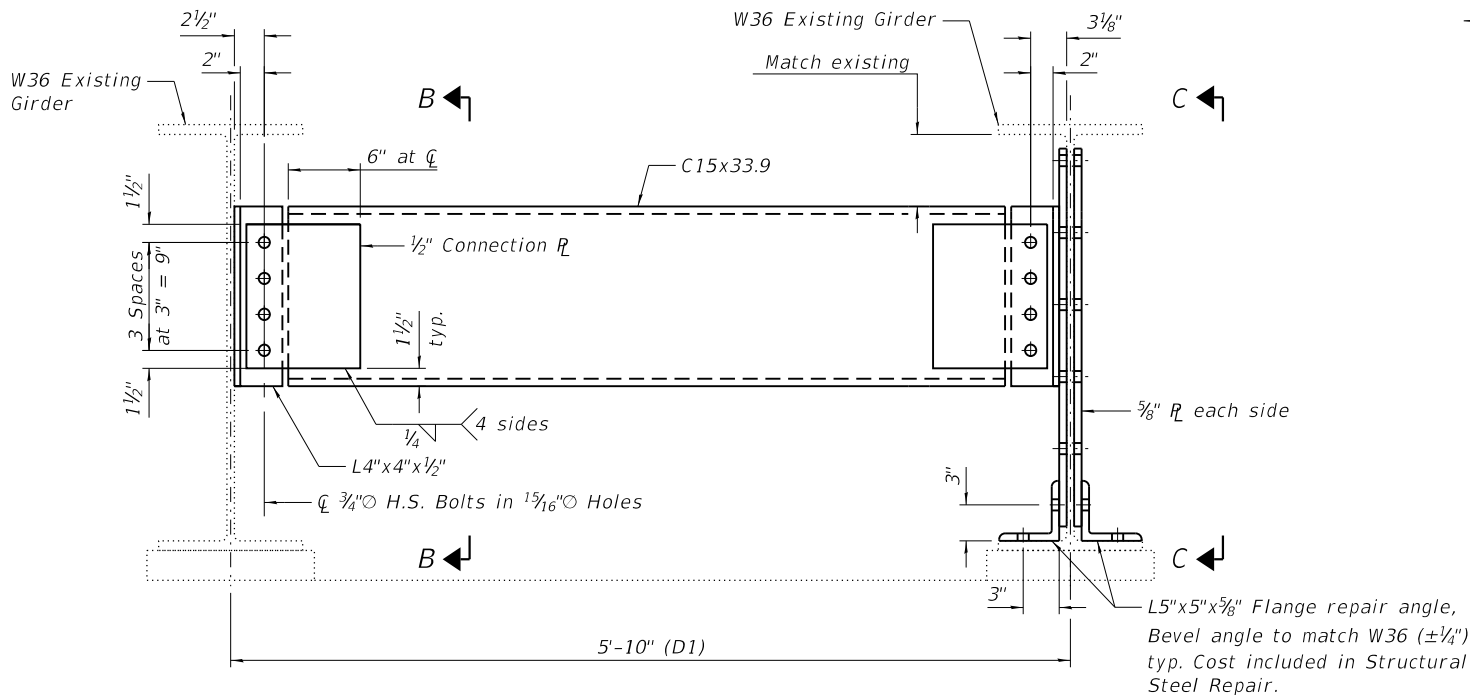
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EXISTING END DIAPHRAGM



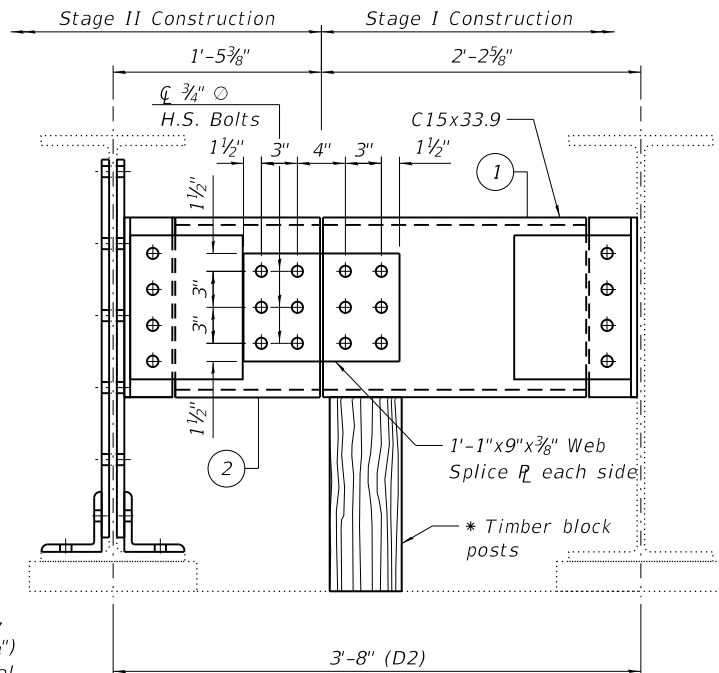
SECTION A-A



SHOWING NON-BEAM END
PLATING LOCATIONS

SHOWING BEAM END
PLATING LOCATIONS

PROPOSED END DIAPHRAGM
(Adjacent diaphragms not shown)



PROPOSED END DIAPHRAGM AT
STAGE CONSTRUCTION JOINT

(See Proposed End Diaphragm Detail
for Connection Plate and Angle details.)

*Cost of Timber Block Posts is included with
Furnishing and Erecting Structural Steel


NOTES:

- For locations of Diaphragm Removal/Replacement and Beam End Repairs, see Sheet S-18 and S-19.
- All proposed beam end repair plates and bottom flange repair angles shall conform to the requirements of AASHTO M270 Grade 50. All proposed diaphragms and their connection angles shall conform to the requirements of AASHTO M270 Grade 36.
- Diaphragm connection holes shall be $1\frac{3}{16}$ " \varnothing for $\frac{3}{4}$ " \varnothing bolts. Two hardened washers shall be required at all diaphragm connections and all oversized holes.
- Existing diaphragm and clip angle removal shall be paid for as Structural Steel Removal.
- All proposed beam end repair plates, bottom flange repair angles, and associated bolts and fasteners shall be paid for as Structural Steel Repair. All proposed diaphragms, connection angles, connection plates and associated bolts, plate washers and fasteners shall be paid for as Furnishing and Erecting Structural Steel.
- See Sheet S-21 for Section B-B and C-C.

END DIAPHRAGM STAGE CONSTRUCTION SEQUENCE

- Order Diaphragm in two sections.
- Attach section (1) of Diaphragm to Beam
- Place Timber Block Posts between section (1) of diaphragm and abutment bearing section.
- Attach section (2) of diaphragm to both Beam and section (1) of diaphragm during Stage II Construction with splice plates.
- Remove Timber Block Posts.

LEGEND:

 Structural Steel Removal

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Furnishing and Erecting Structural Steel	Pound	25,080
Structural Steel Removal	Pound	25,860
Structural Steel Repair	Pound	9,960



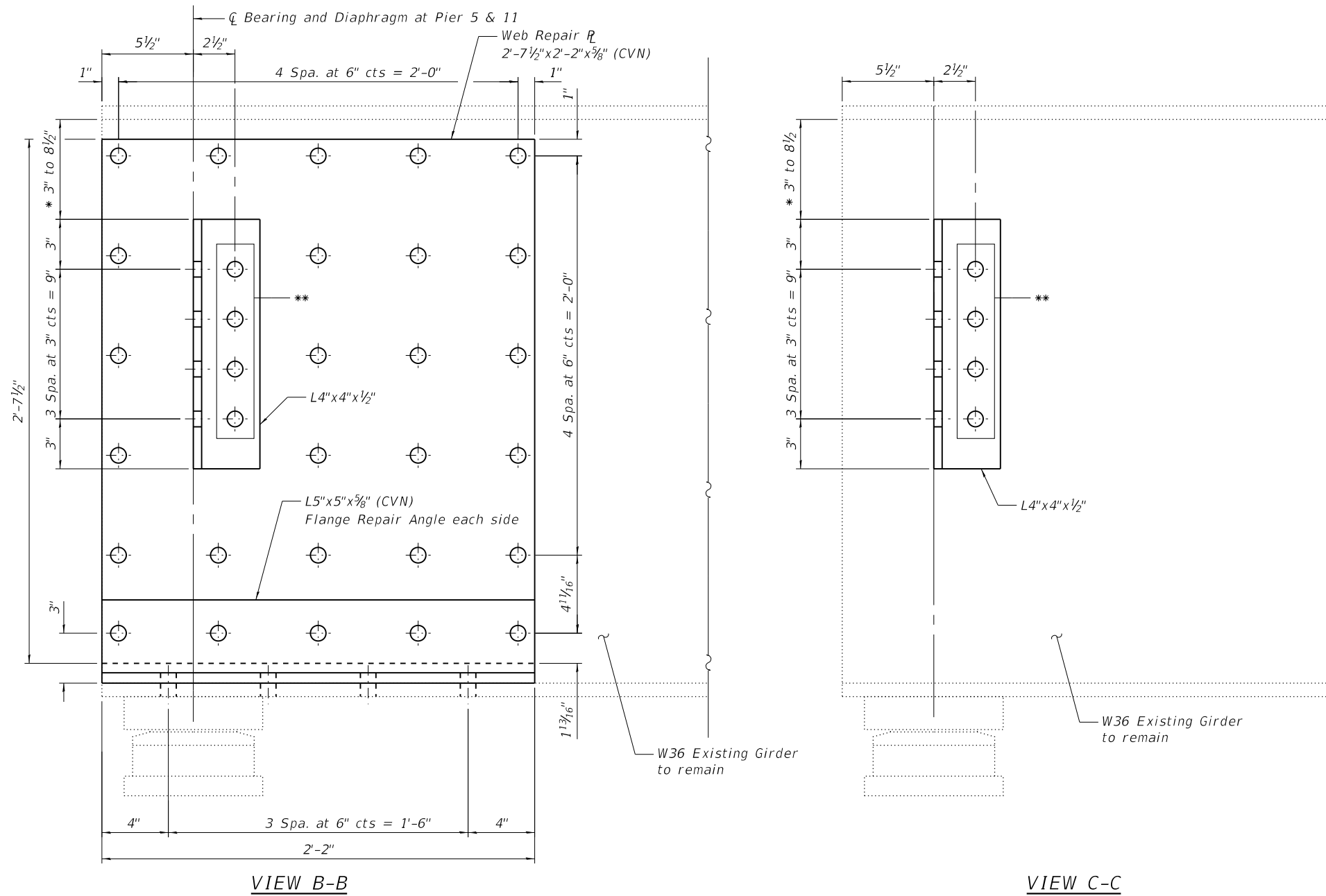
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIAPHRAGM REPLACEMENT DETAILS, PIER 5 AND 11
STRUCTURE NO. 016-1011

SHEET S-20 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

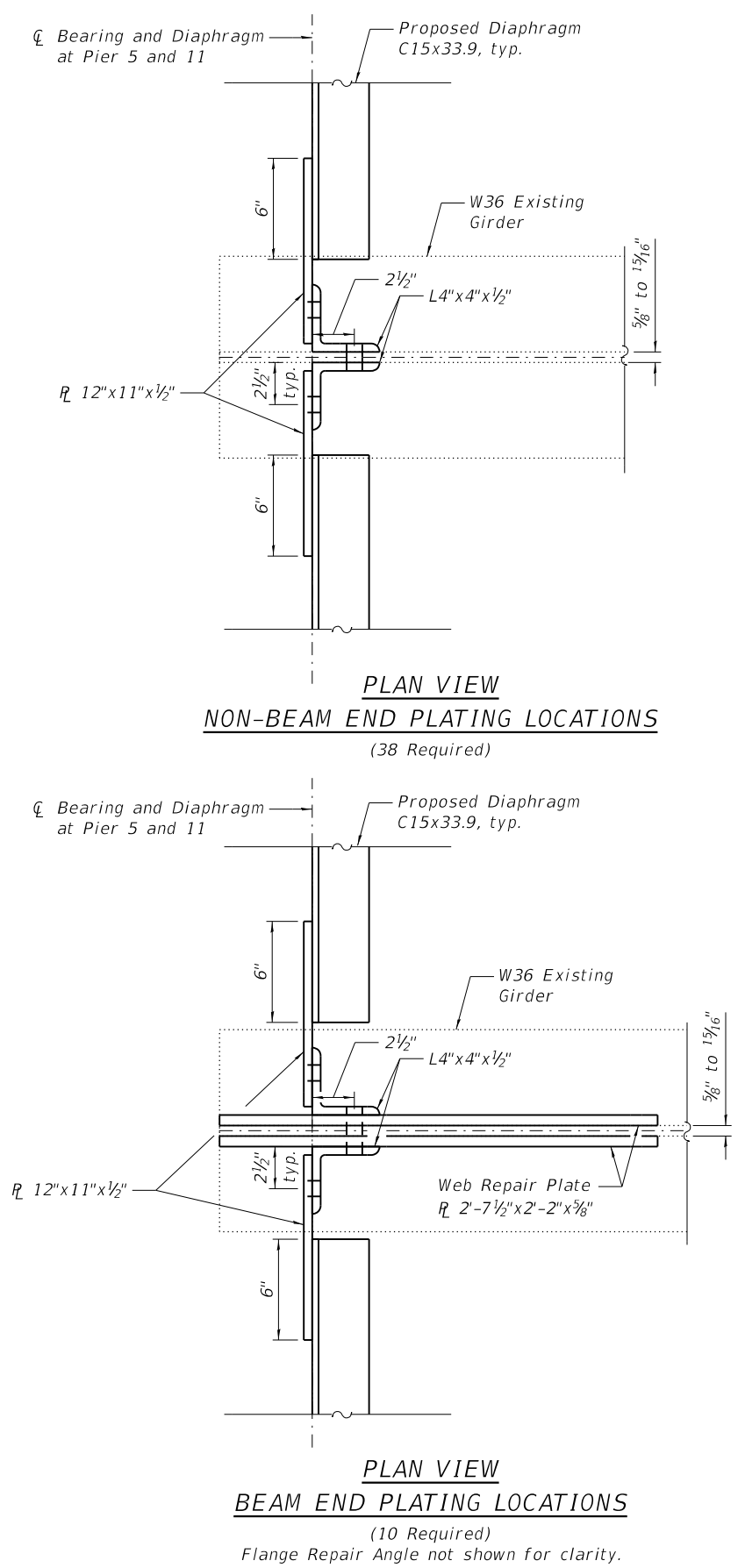


NOTES:

1. *For Notes and locations of Diaphragm Removal/Replacement and Beam End repairs, see Sheets S-18 and S-19.*
2. *The holes in the existing beam web shall be field drilled using the holes in the plate as a template. The holes in the existing beam flange shall be field drilled using the holes in the Flange Repair Angle as a template.*

* Distance from Top of Girder to Top of Proposed Diaphragm to be Field Verified prior to Ordering Material.

*** Contractor to field verify diaphragm location and bolt hole locations before ordering material. Contractor can elect to field drill holes in repair plate for proposed diaphragm connections.*



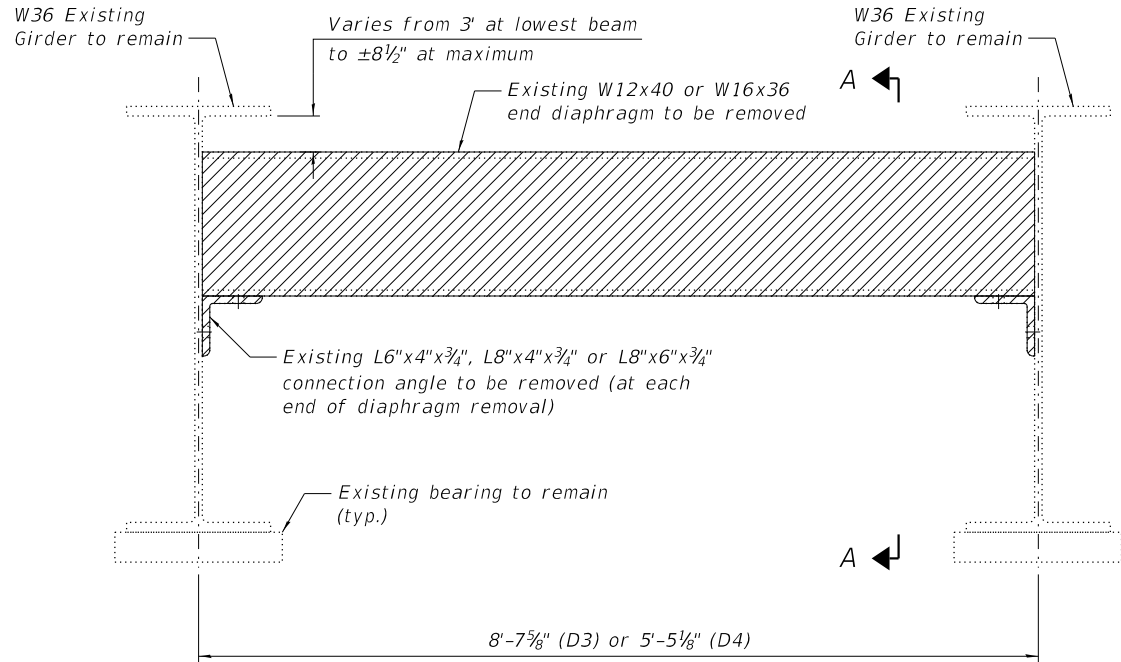
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

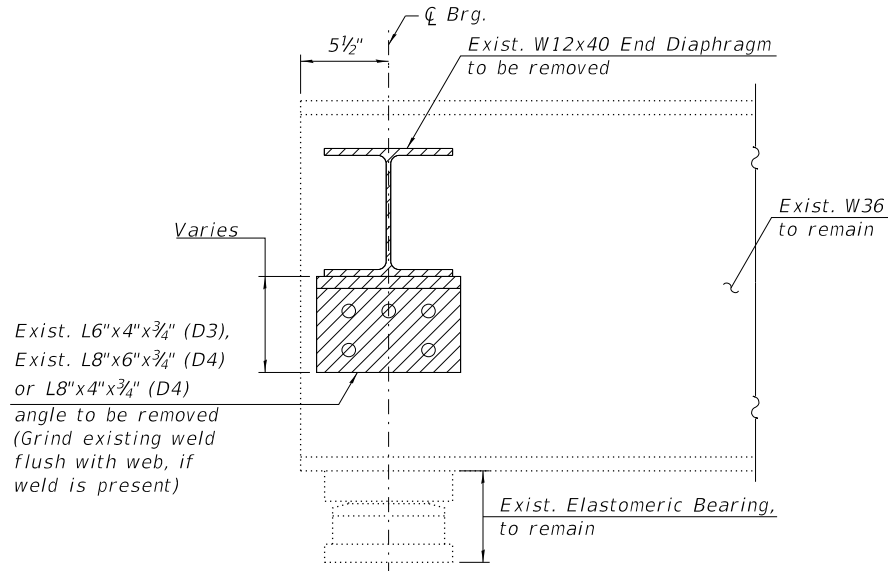
**BEAM REPAIR DETAILS, PIER 5 AND 11
STRUCTURE NO. 016-1011**

SHEET S-21 OF S-34 SHEETS

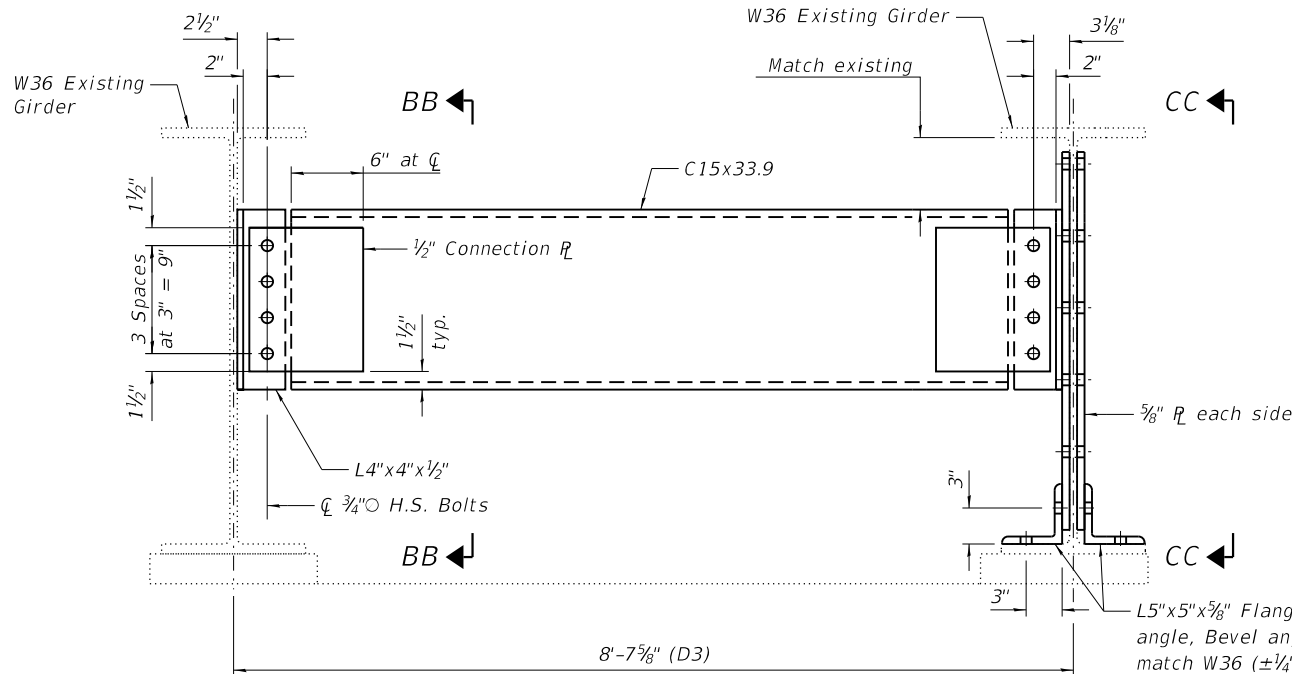
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	53
		CONTRACT NO. 62M80		
ILLINOIS		FED. AID PROJECT		



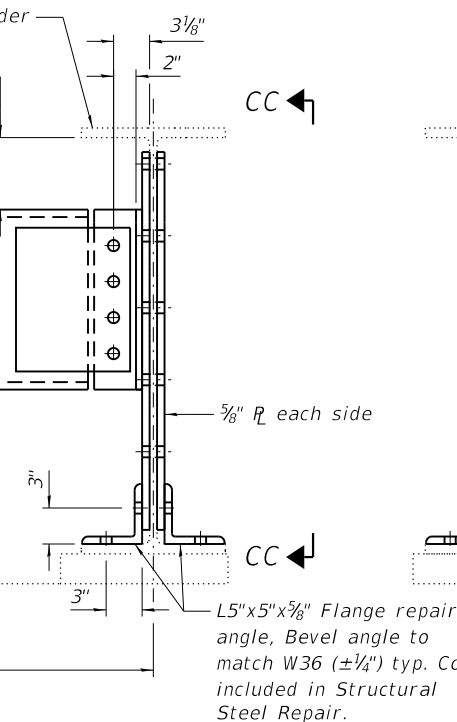
EXISTING END DIAPHRAGM



SECTION A-A

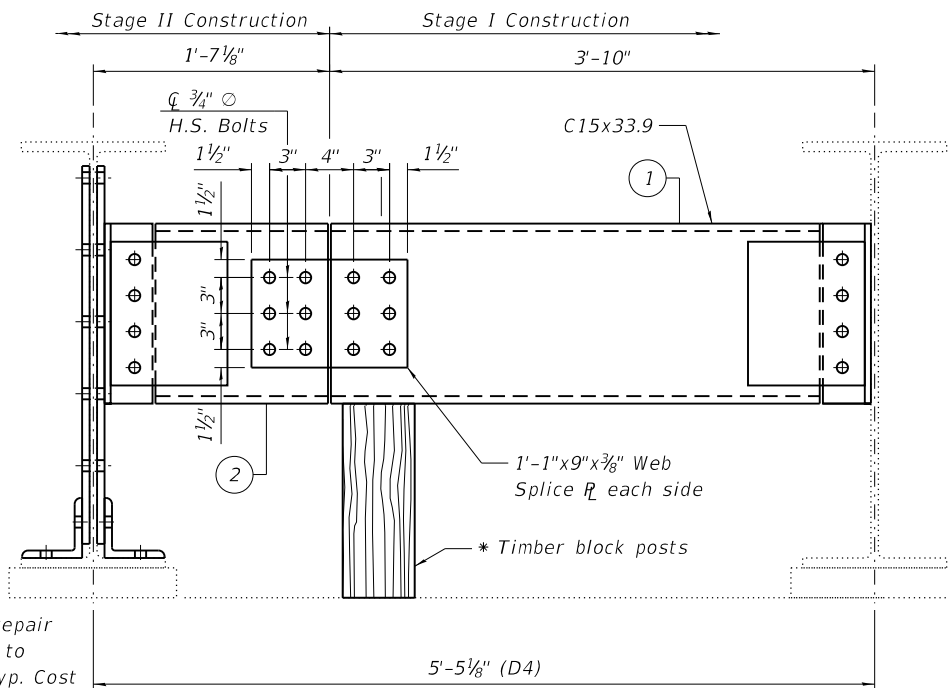


SHOWING NON-BEAM END
PLATING LOCATIONS



SHOWING BEAM END
PLATING LOCATIONS

PROPOSED END DIAPHRAGM
(Adjacent diaphragms not shown)



PROPOSED END DIAPHRAGM AT
STAGE CONSTRUCTION JOINT

(Stage II Beam Plating Shown. Stage I Beam Plating Similar.
See Proposed End Diaphragm Detail for Connection Plate and Angle details.)

*Cost of Timber Block Posts is included with
Furnishing and Erecting Structural Steel


NOTES:

1. For locations of Diaphragm Removal/Replacement and Beam End Repairs, see Sheet S-18 and S-19.
2. All proposed beam end repair plates and bottom flange repair angles shall conform to the requirements of AASHTO M270 Grade 50. All proposed diaphragms and their connection angles shall conform to the requirements of AASHTO M270 Grade 36.
3. Diaphragm connection holes shall be $1\frac{3}{16}$ " \varnothing for $\frac{3}{4}$ " \varnothing bolts. Two hardened washers shall be required at all diaphragm connections and all oversized holes.
4. Existing diaphragm and clip angle removal shall be paid for as Structural Steel Removal.
5. All proposed beam end repair plates, bottom flange repair angles, and associated bolts and fasteners shall be paid for as Structural Steel Repair. All proposed diaphragms, connection angles and associated bolts, plate washers, connection bent plates, and fasteners shall be paid for as Furnishing and Erecting Structural Steel.
6. See Sheet S-23 for View BB-BB and CC-CC.

END DIAPHRAGM STAGE
CONSTRUCTION SEQUENCE

1. Order Diaphragm in two sections.
2. Attach section (1) of Diaphragm to Beam
3. Place Timber Block Posts between section (1) of diaphragm and abutment bearing section.
4. Attach section (2) of diaphragm to both Beam and section (1) of diaphragm during Stage II Construction with splice plates.
5. Remove Timber Block Posts.

LEGEND:

 Structural Steel Removal

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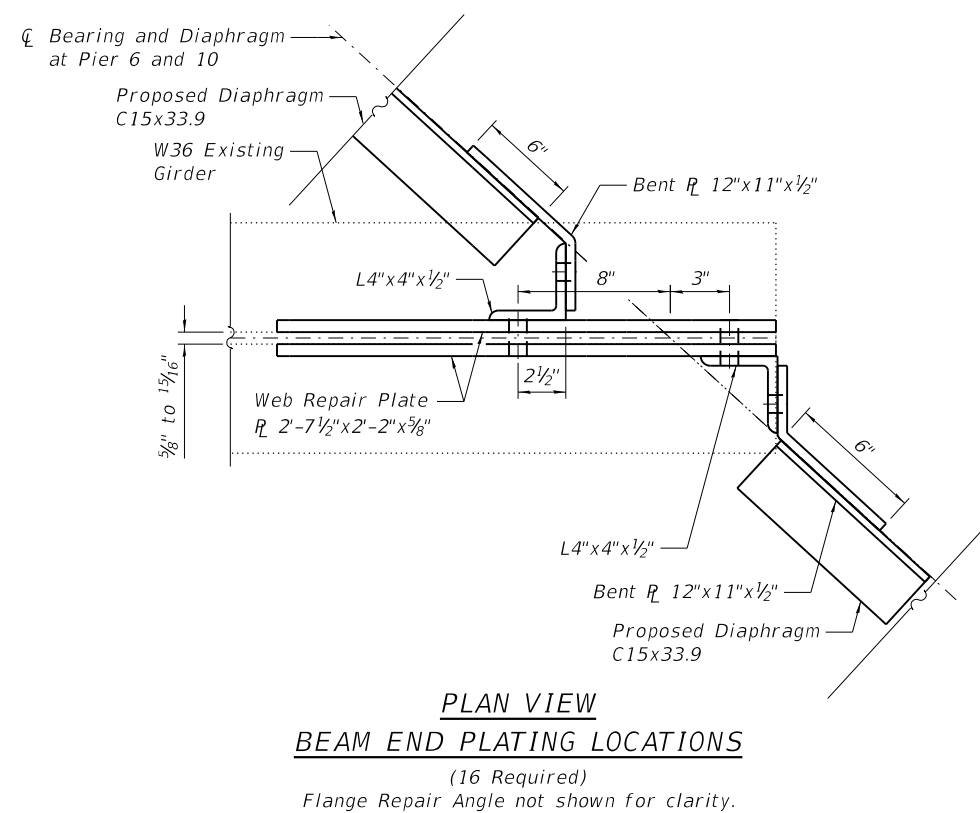
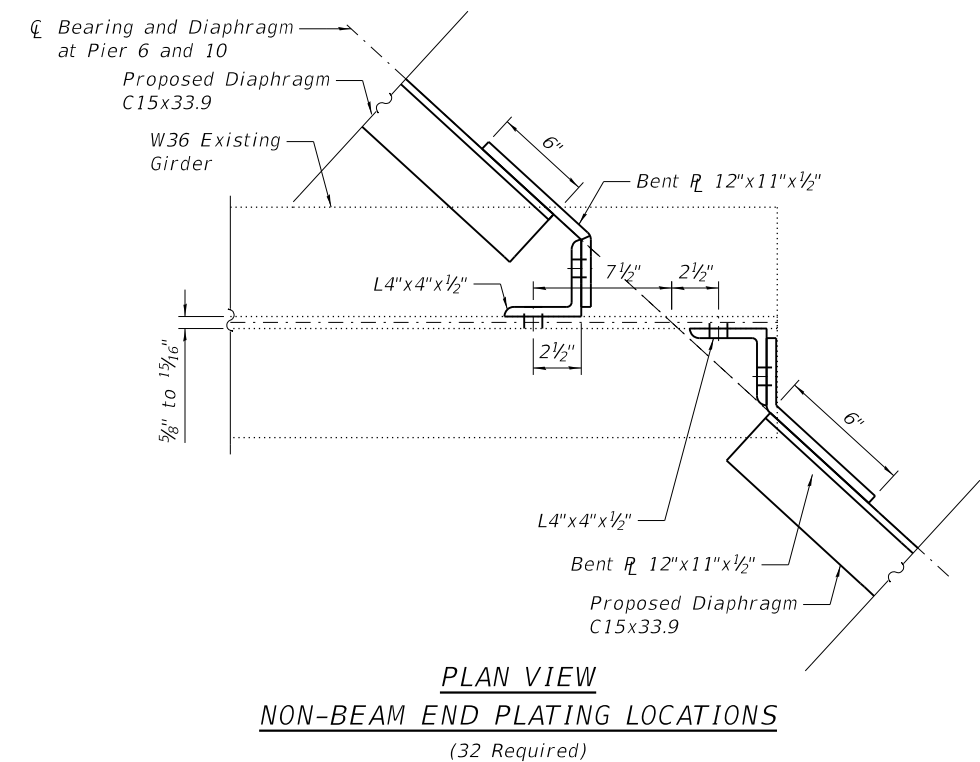
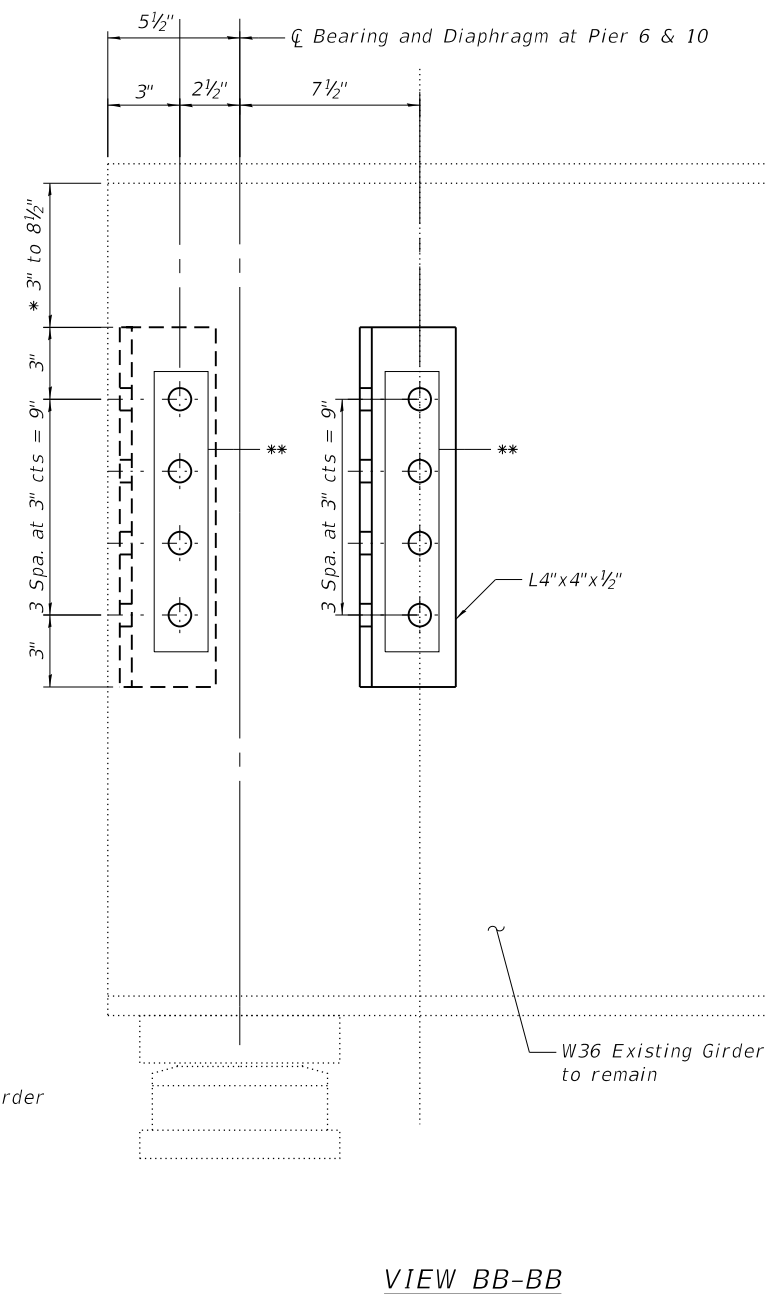
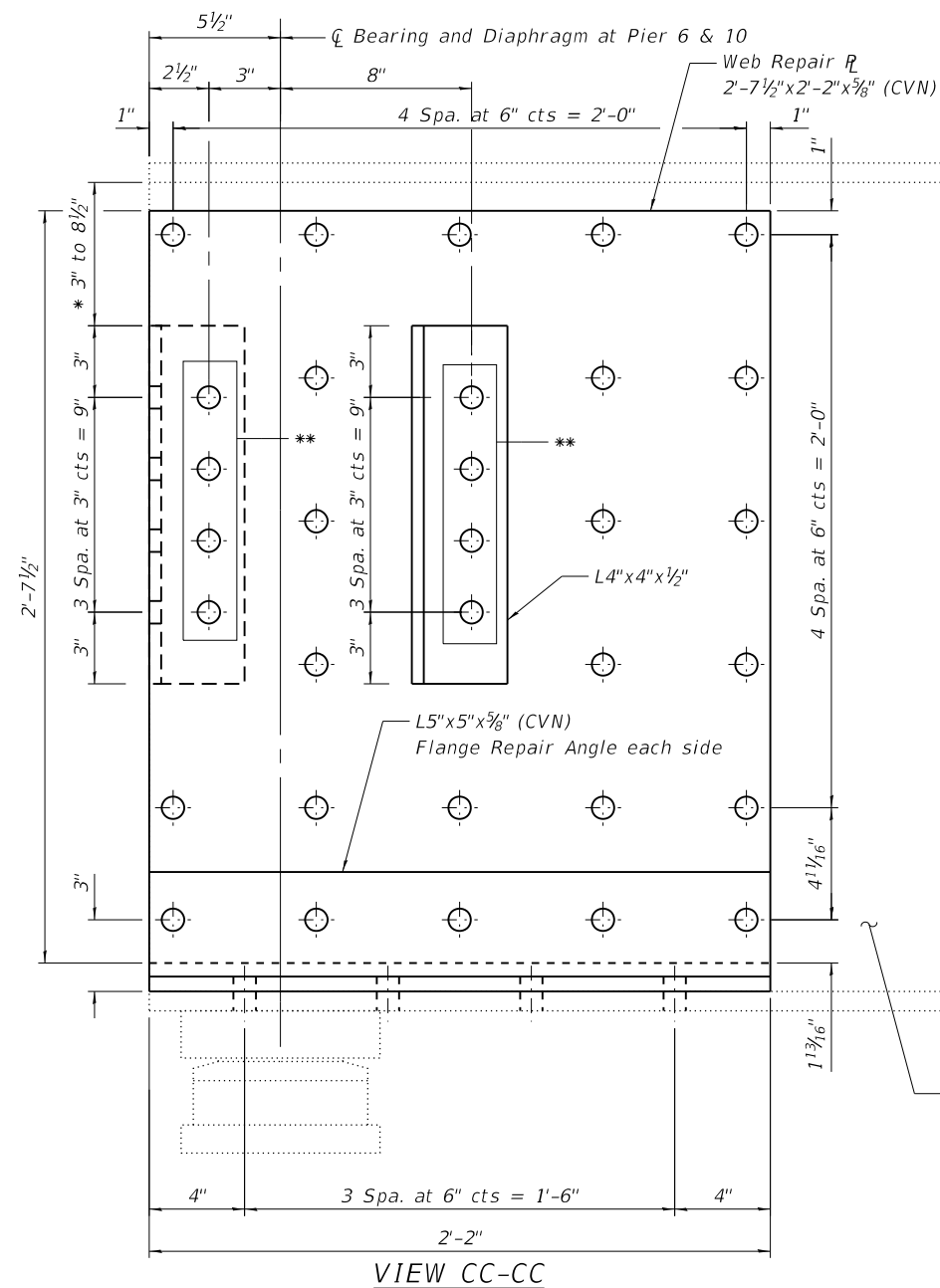
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIAPHRAGM REPLACEMENT DETAILS, PIER 6 AND 10
STRUCTURE NO. 016-1011

SHEET S-22 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	54
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		



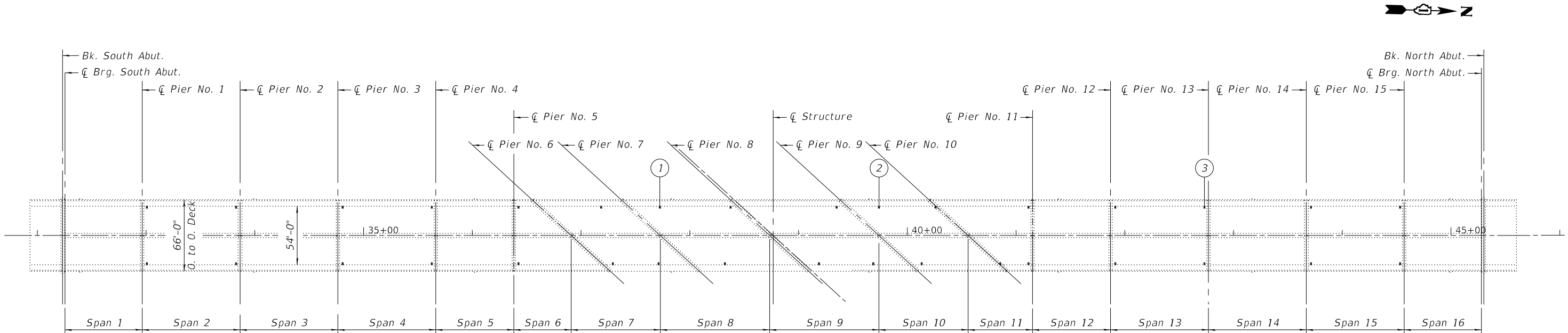
NOTES:

1. *For Notes and locations of Diaphragm Removal/Replacement and Beam End repairs, see Sheets S-18 and S-19.*
2. *The holes in the existing beam web shall be field drilled using the holes in the web repair plate as a template. The holes in the existing beam flange shall be field drilled using the holes in the Flange Repair Angle as a template.*

* Distance from Top of Girder to Top of Proposed Diaphragm to be Field Verified prior to Ordering Material.

*** Contractor to field verify diaphragm location and bolt hole locations before ordering material. Contractor can elect to field drill holes in repair plate for proposed diaphragm connections.*

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DRAINAGE REPAIR PLAN

REPAIR SUMMARY TABLE

Repair Number	Location	Repair	Quantity (Foot)
1	Span 8, Near Pier 7, West End	Broken Pipe section at drain connection.	5
2	Span 10, Midspan, West Side	Broken Pipe section at drain connection.	5
3	Pier 13, South Face, West End	Broken Pipe section at top of Pier.	12

DRAINAGE REPAIR NOTES

- The locations shown as required drainage identify the general deficiency. The drainage system is shown schematically. It shall be the Contractor's responsibility to verify the locations and details prior to ordering the material. Such variations shall not be cause for additional compensation for a change in the scope of work, however the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- The Contractor shall provide all piping, brackets, hangers, fittings, hardware and other accessories required to return the existing drainage system to a fully functional leak free system. See Special Provisions. Pipe color shall match existing.
- When existing drain pipes are wrapped with tape, the tape shall be removed, if the pipe is broken or missing a connection, the pipe shall be replaced or otherwise new tape shall be provided. The type of tape shall be approved by the Engineer. The cost shall be included in Bridge Drainage System Repairs and the length measured shall be the length of the pipe replaced or length of pipe re-taped.
- The Contractor shall wrap any joints that are leaking as directed by the Engineer. The cost shall be included in Bridge Drainage System Repairs.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Bridge Drainage System Repairs	Foot	22



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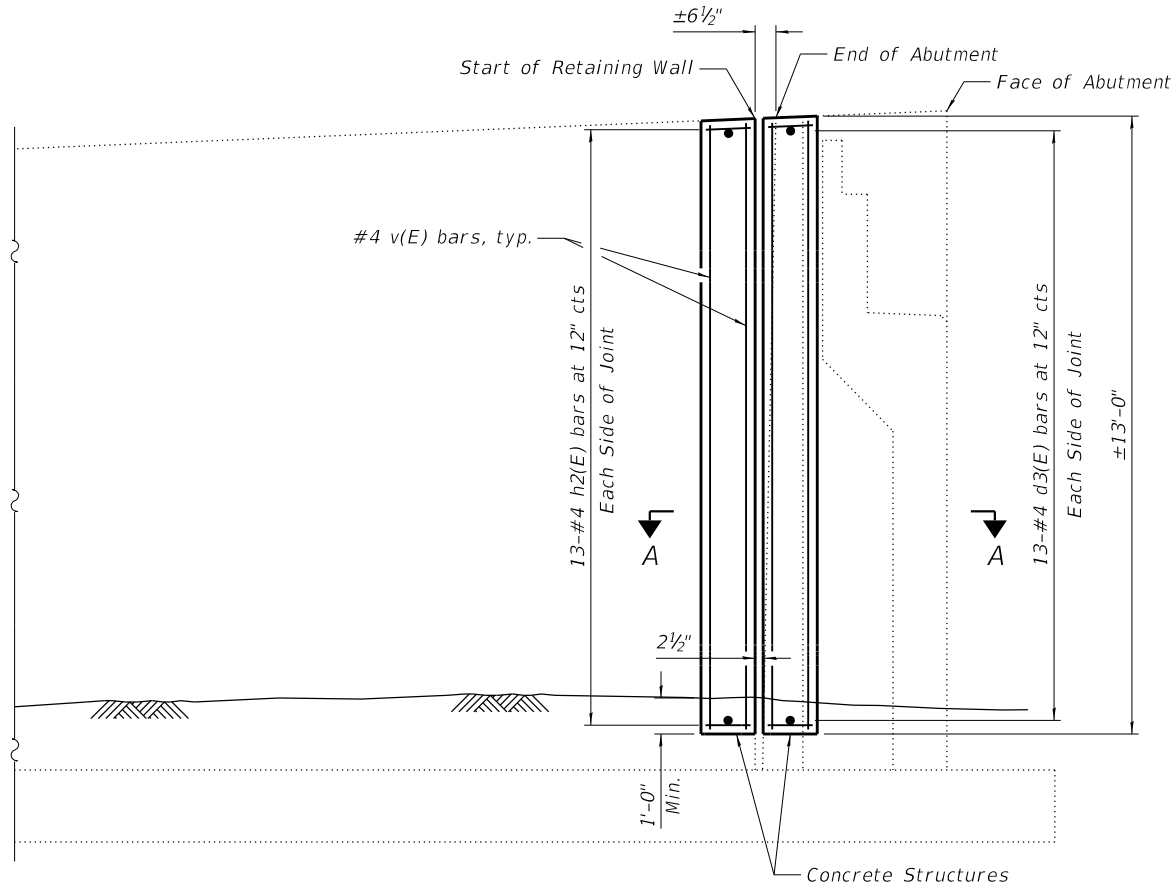
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE SYSTEM REPAIR PLAN
STRUCTURE NO. 016-1011

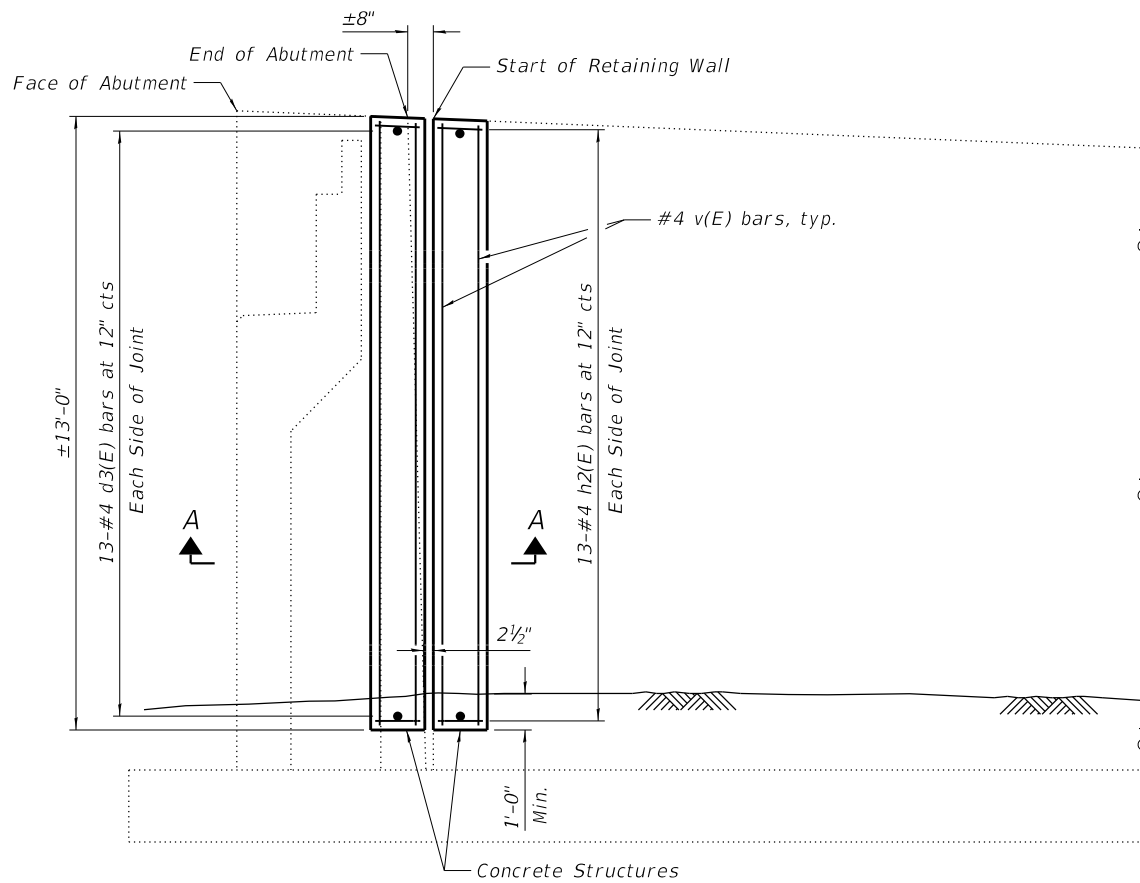
SHEET 5-24 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

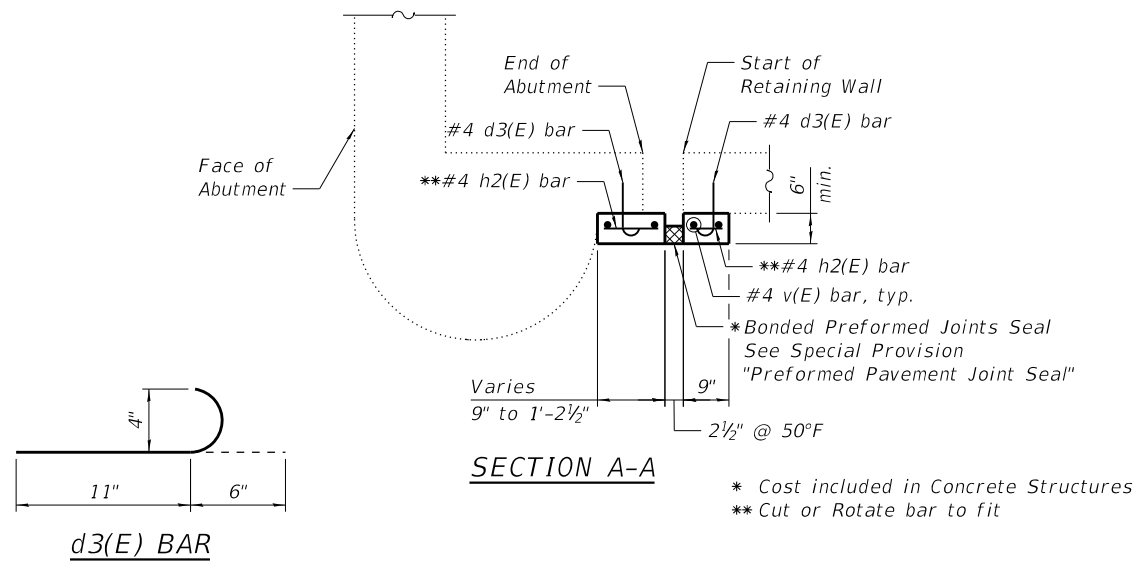
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SOUTH ABUTMENT EAST SIDE RETAINING WALL
(Looking West)



SOUTH ABUTMENT WEST SIDE RETAINING WALL
(Looking East)



NOTES:

The joint opening shall be adjusted for temperatre per Article 520.04 of the Standard Specifications.

Epoxy grout d3(E) bars in 9" min. drilled holes according to Article 584 of the Standard Specifications.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)	52	#4	1'-5"	
h2(E)	52	#4	1'-0"	
v(E)	8	#4	12'-6"	
Concrete Structures			Cu. Yd.	1.0
Reinforcement Bars, Epoxy Coated			Pound	160



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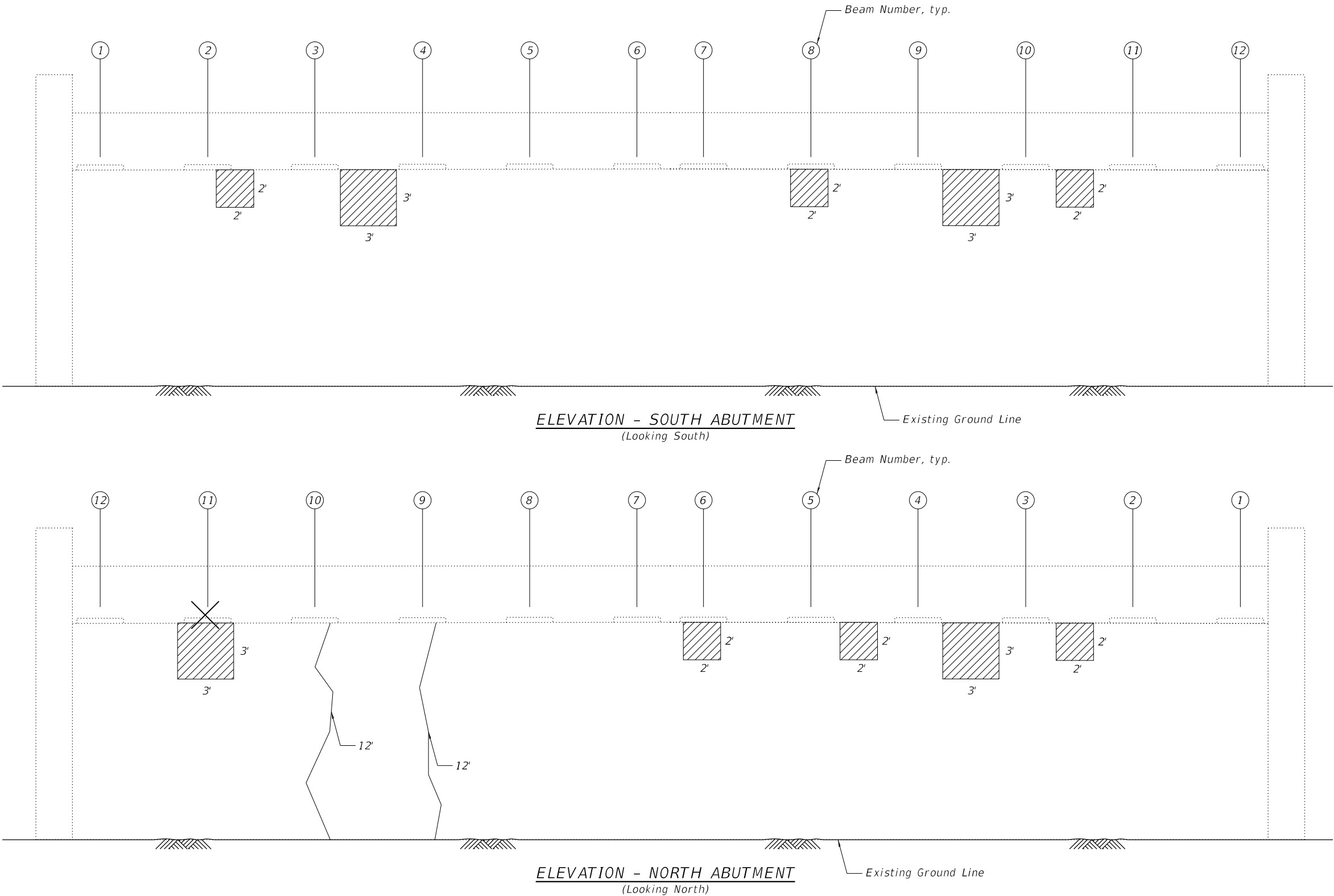
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RETAINING WALL REPAIRS
STRUCTURE NO. 016-1011**

SHEET 5-25 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		




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NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete repairs directly under Girder 2 shall not start until the Temporary Shoring is installed.
- Removal and installation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

-  Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
-  Epoxy Crack Injection
-  Temporary Shoring and Cribbing

GIRDER REACTION TABLE

North Abutment	Load (kips)
DL	25.0
LL	32.6
IM	8.3
Total	65.9

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	60
Temporary Shoring and Cribbing	Each	1
Epoxy Crack Injection	Foot	24



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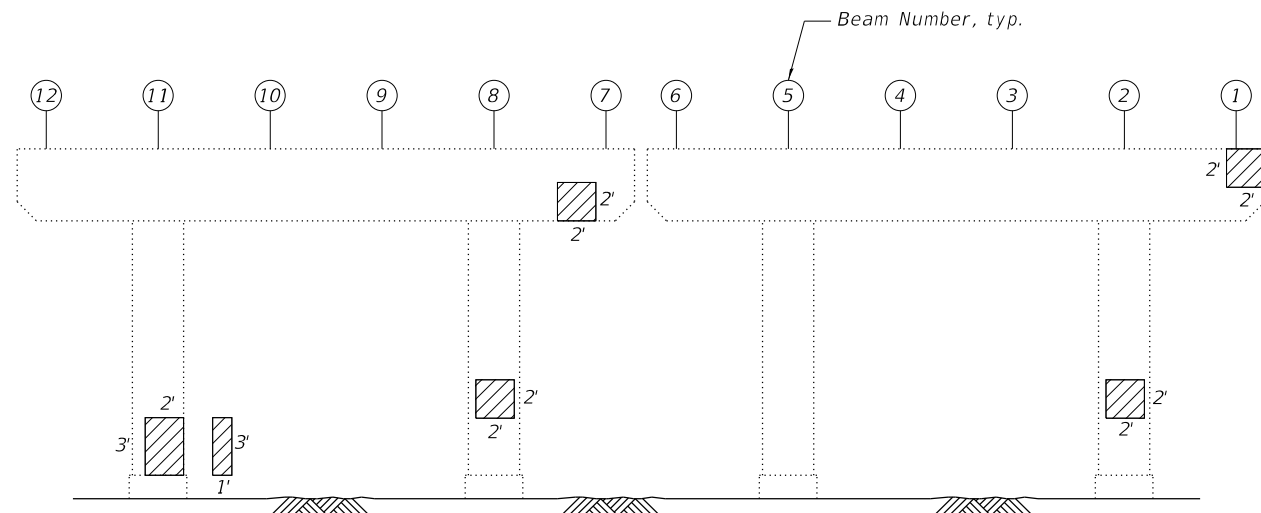
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

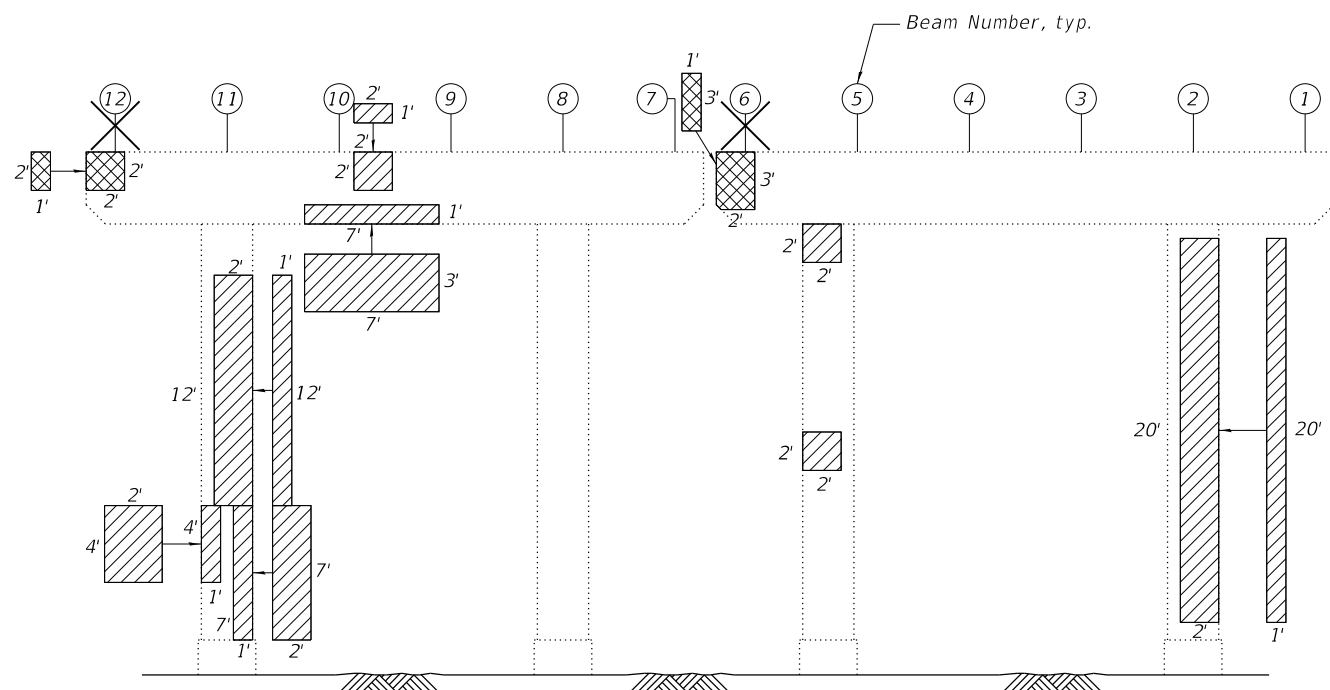
NORTH AND SOUTH ABUTMENT REPAIR DETAILS
STRUCTURE NO. 016-1011

SHEET 5-26 OF 5-34 SHEETS

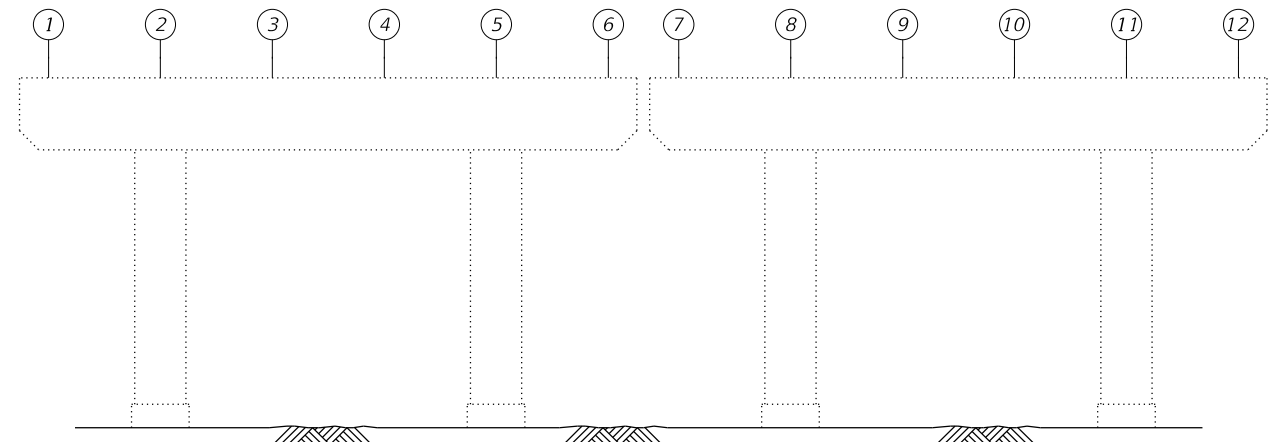
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		



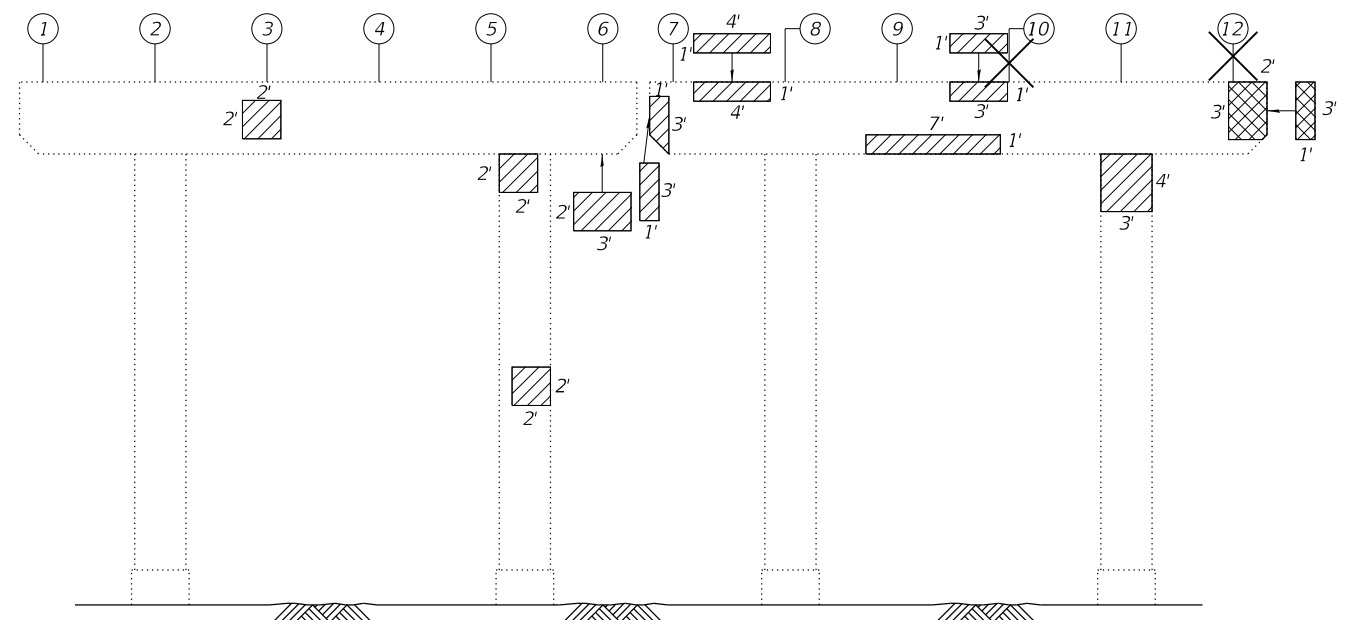
PIER 2 - SOUTH FACE
(Looking North)



PIER 5 - SOUTH FACE
(Looking North)



PIER 2 - NORTH FACE
(Looking South)






PIER 5 - NORTH FACE
(Looking South)

NOTES:

1. Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
2. Concrete repairs directly under Girder 10 and 12 Pier 5 North Face and 6 and 12 Pier 5 South Face shall not start until the Temporary Shoring is installed.
3. Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

-  Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
 -  Structural Repair Of Concrete (Depth Greater Than 5 Inches)
 -  Temporary Shoring and Cribbing

GIRDER REACTION TABLES

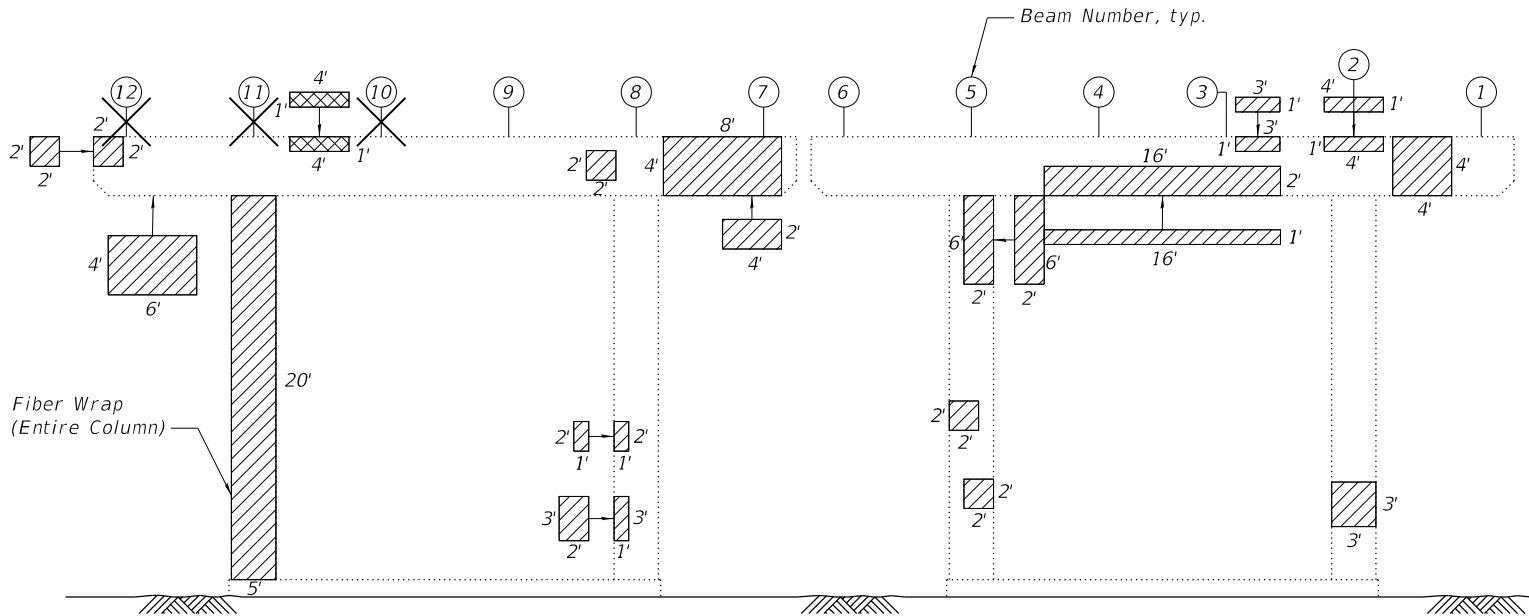
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<i>DL</i>	<i>25.0</i>	<i>DL</i>	<i>55.8</i>
<i>LL</i>	<i>32.6</i>	<i>LL</i>	<i>37.8</i>
<i>IM</i>	<i>8.3</i>	<i>IM</i>	<i>8.8</i>
<i>Total</i>	<i>65.9</i>	<i>Total</i>	<i>102.4</i>

Reactions are per Girder

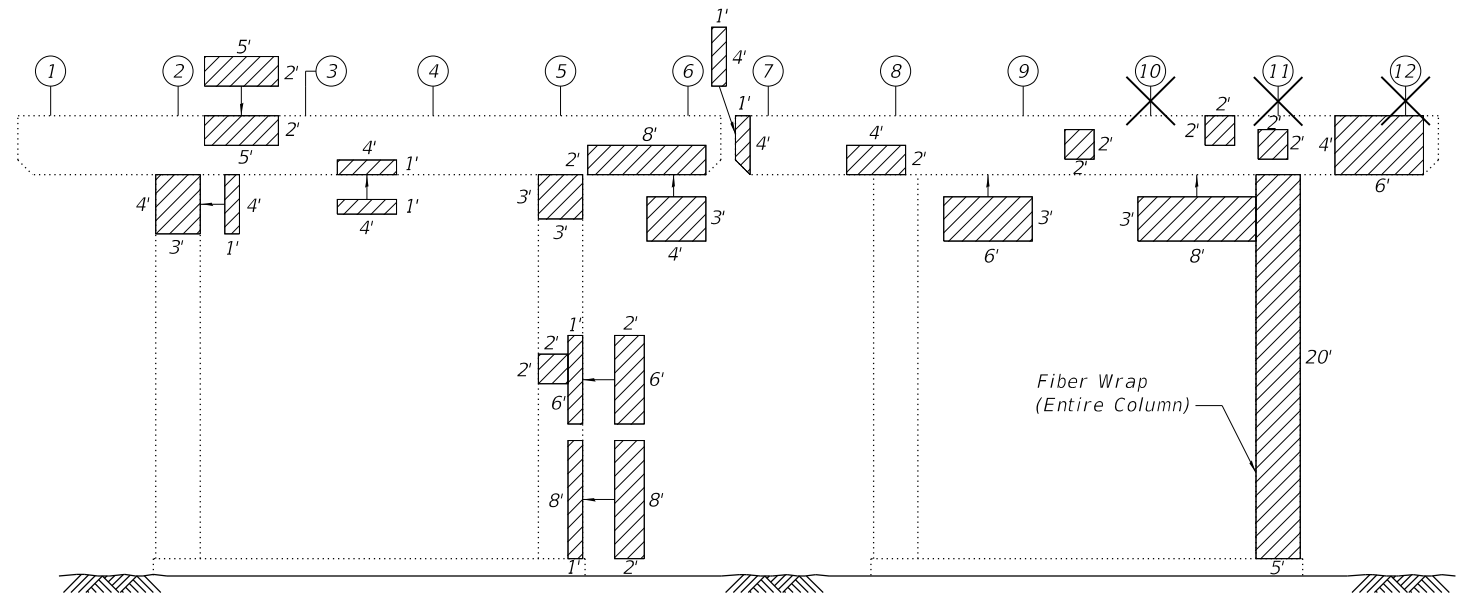
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	253
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft	24
Temporary Shoring and Cribbing	Each	4

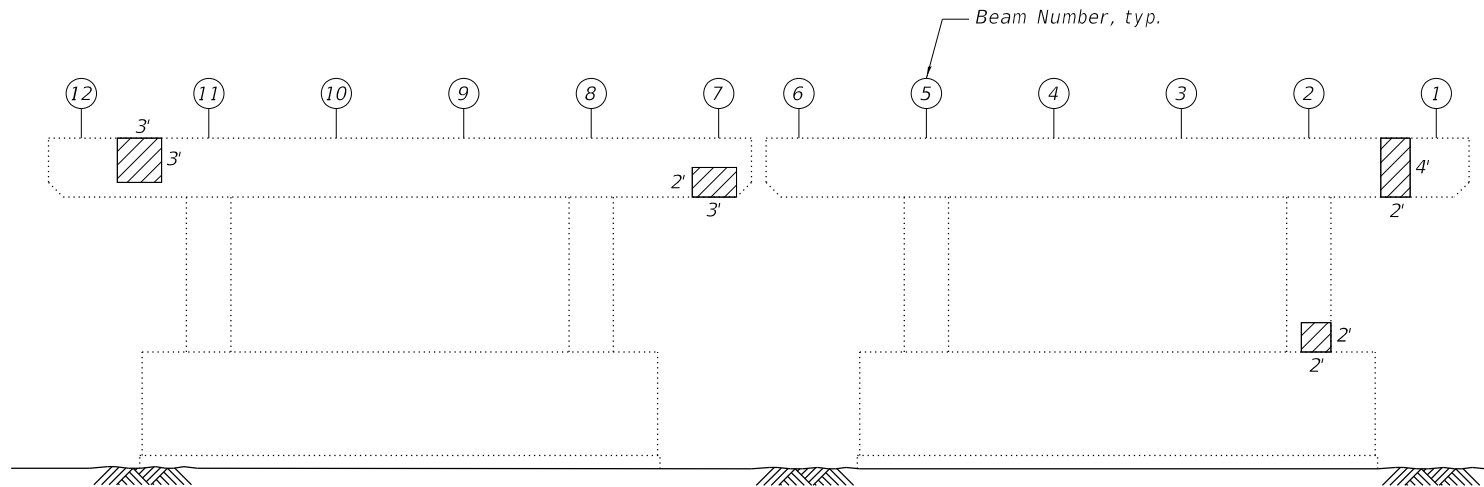
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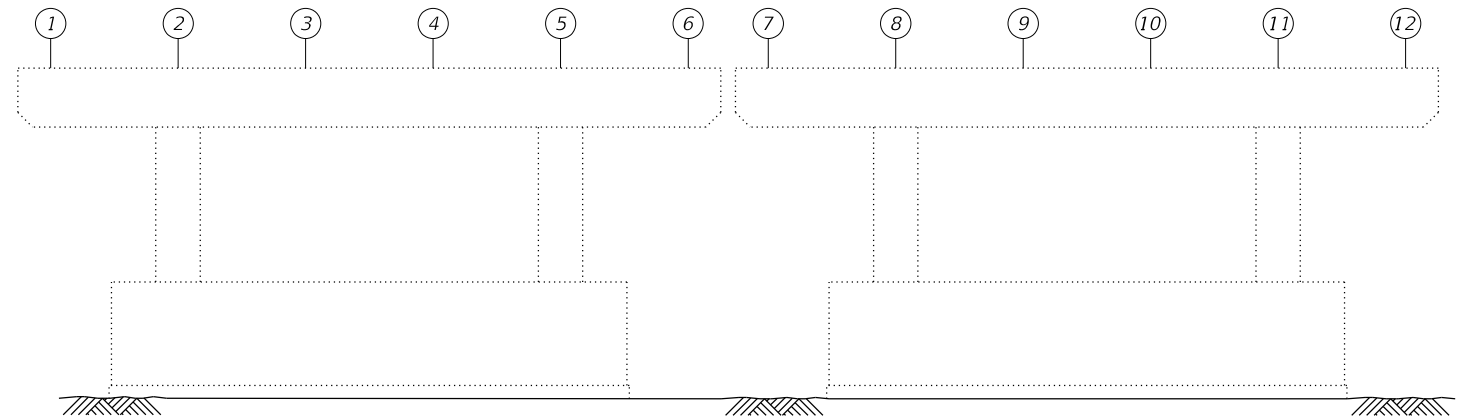
PIER 6- SOUTH FACE
(Looking North)



PIER 6- NORTH FACE
(Looking South)



PIER 7 - SOUTH FACE
(Looking North)



PIER 7 - NORTH FACE
(Looking South)

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete Repairs to the West Column and under Griders 10, 11 and 12, Each Face of Pier 6 shall not start until the Temporary Shoring is installed.
- Shoring on Each Face of Pier 6 will be required for Beam 10, 11 and 12. Each Location will be paid for separately.
- Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Structural Repair Of Concrete (Depth Greater Than 5 Inches)
- Temporary Shoring and Cribbing

GIRDER REACTION TABLES

Pier 6 S. Face	Load (kips)	Pier 6 N. Face	Load (kips)
DL	55.8	DL	29.5
LL	37.8	LL	33.4
IM	8.8	IM	8.1
Total	102.4	Total	71.0

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Fiber Wrap	Sq Ft	377
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	656
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft	8
Temporary Shoring and Cribbing	Each	6



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 6 AND 7 REPAIR DETAILS
STRUCTURE NO. 016-1011**

SHEET 5-28 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	60
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

PIER 8 - SOUTH FACE
(Looking North)

Technical drawing of a two-span continuous beam. The beam is supported by two piers, each with a rectangular base. The beam is divided into three sections: a left span, a central pier, and a right span. The dimensions are indicated by numbered circles (1 through 12) and a section cut symbol (3' and 3''). The section cut is shown as a hatched rectangle with the label 3' on the left and 3'' on the right, with a line pointing to the cut location on the beam.

PIER 8 - NORTH FACE
(Looking South)

PIER 9 - SOUTH FACE
(Looking North)

PIER 9 - NORTH FACE
(Looking South)

1. Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
2. Concrete repairs under Girder 11 on Pier 9 shall not start until the Temporary Shoring is installed.
3. Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

 Structural Repair of Concrete
(Depth Equal to or Less Than 5 Inches)

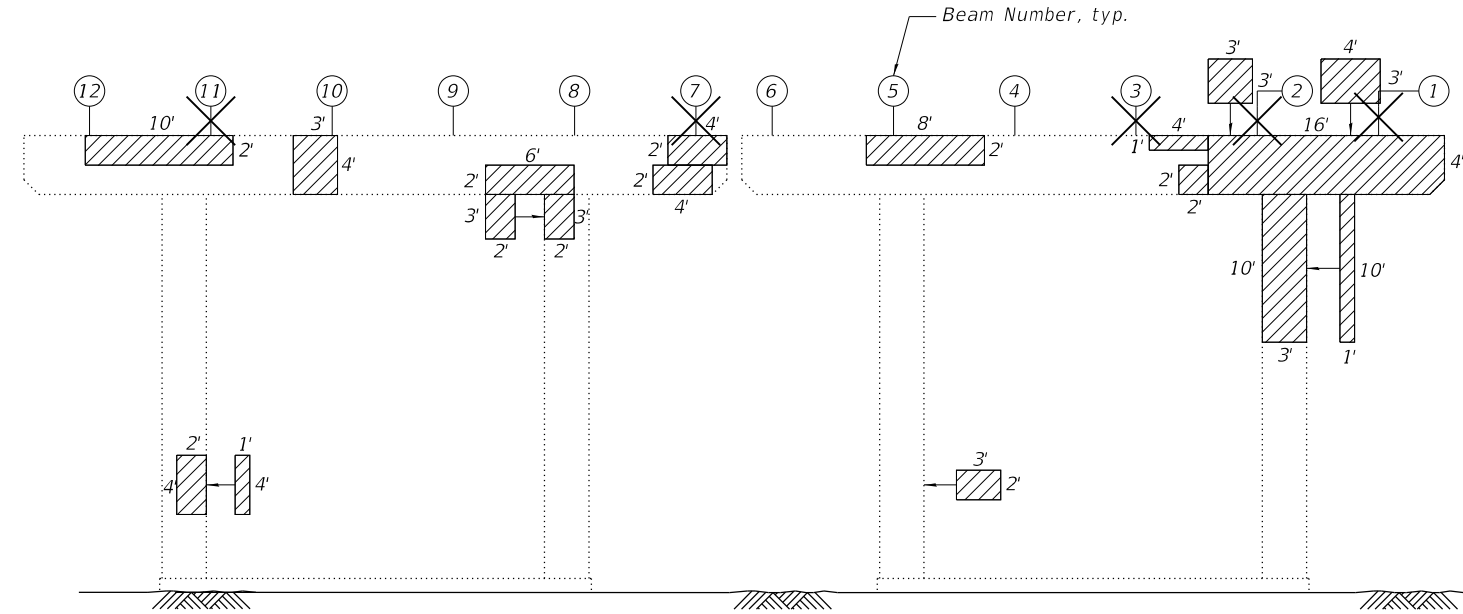
 Structural Repair Of Concrete
(Depth Greater Than 5 Inches)

 Temporary Shoring and Cribbing

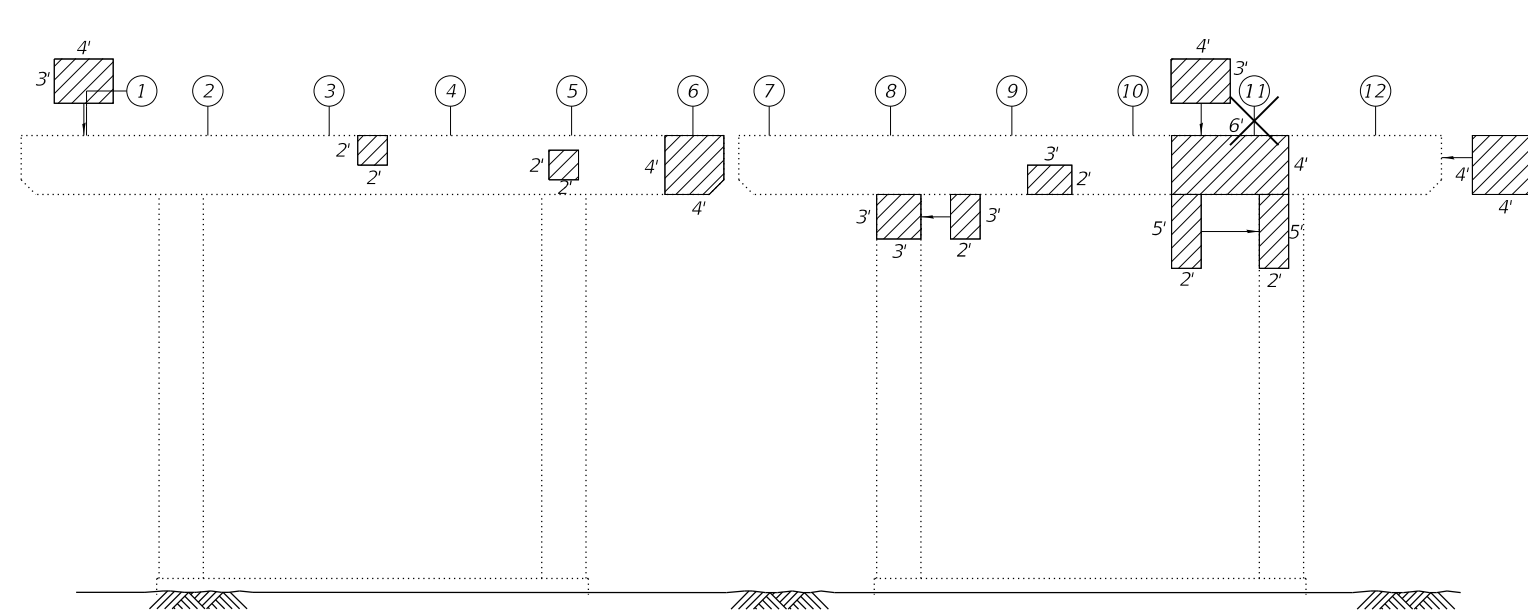
<i>Pier 9</i>	<i>Load (kips)</i>
<i>DL</i>	<i>94.8</i>
<i>LL</i>	<i>35.9</i>
<i>IM</i>	<i>8.3</i>
<i>Total</i>	<i>139.0</i>

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	52
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft	8
Temporary Shoring and Cribbing	Each	1

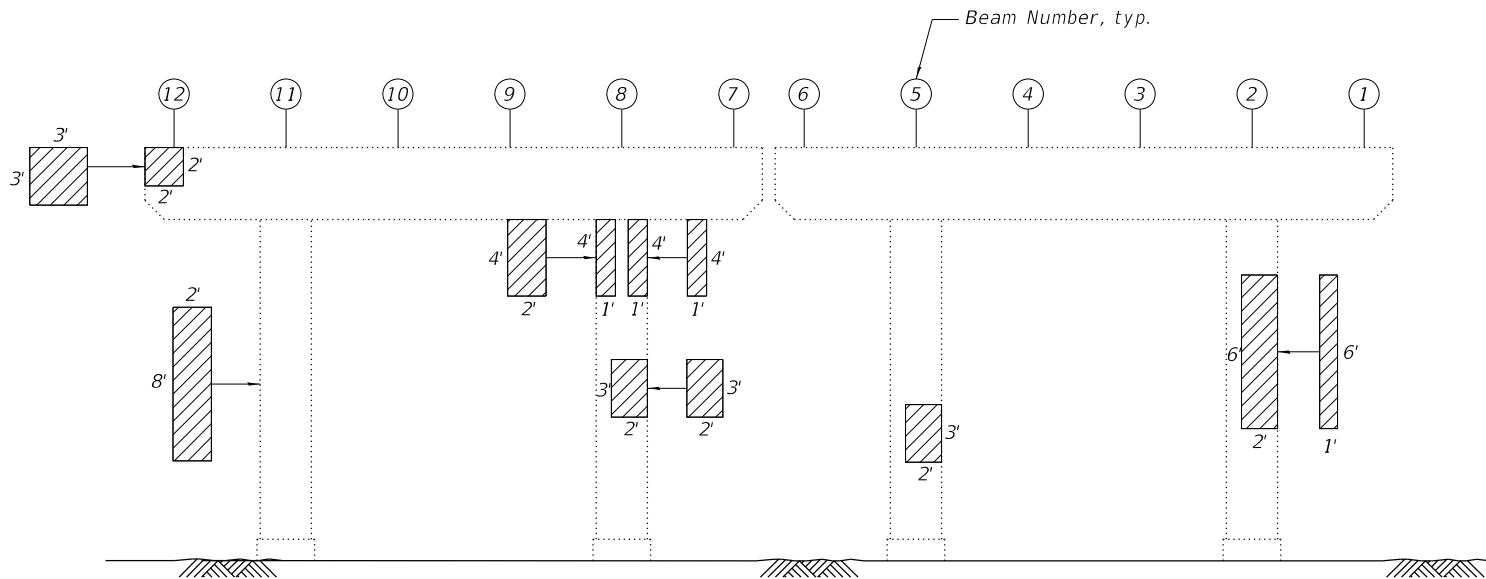
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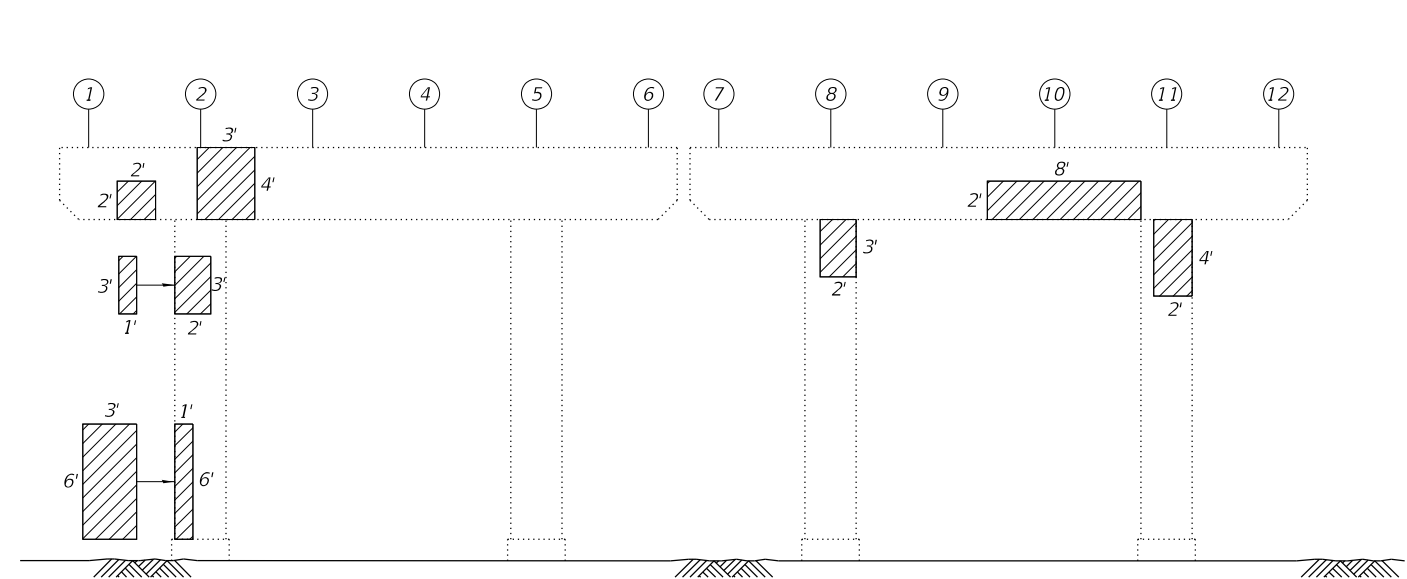
PIER 10 - SOUTH FACE
(Looking North)



PIER 10 - NORTH FACE
(Looking South)



PIER 11 - SOUTH FACE
(Looking North)





PIER 11 - NORTH FACE
(Looking South)

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete repairs under Girder 1, 2, 3, 7 and 11 on Pier 10 South Face and Beam 11 Pier 10 North face shall not start until the Temporary Shoring is installed.
- Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

-  Structural Repair of Concrete
(Depth Equal to or Less Than 5 Inches)
-  Temporary Shoring and Cribbing

GIRDER REACTION TABLES

Pier 10 S. Face	Load (kips)	Pier 10 N. Face	Load (kips)
DL	29.5	DL	55.8
LL	33.4	LL	37.8
IM	8.1	IM	8.8
Total	71.0	Total	102.4

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	532
Temporary Shoring and Cribbing	Each	6



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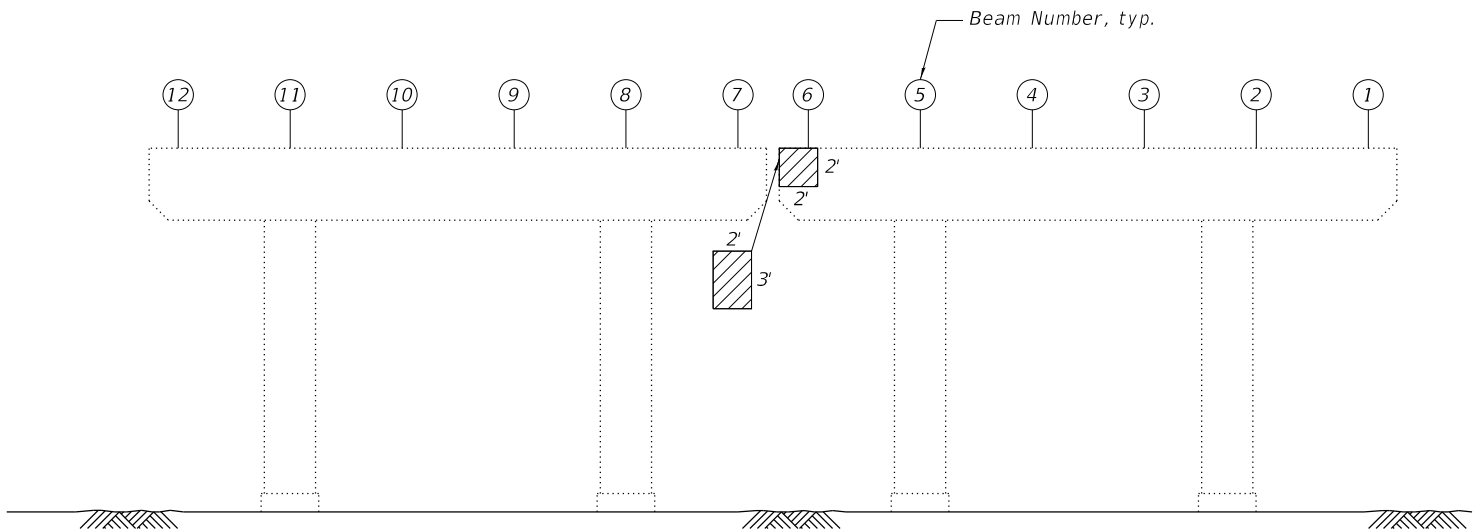
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 10 AND 11 REPAIR DETAILS
STRUCTURE NO. 016-1011**

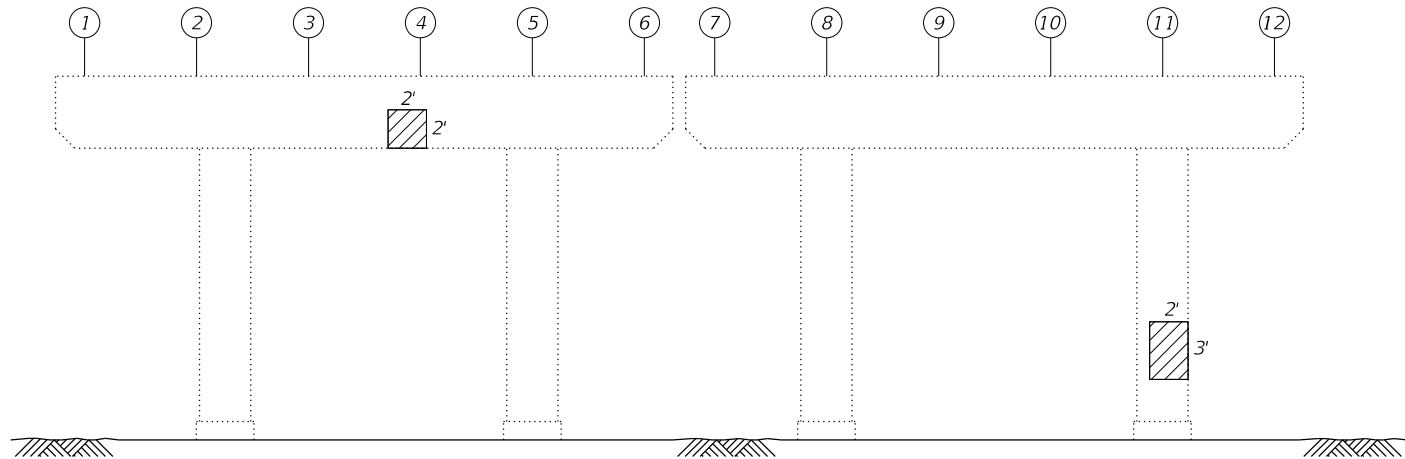
SHEET 5-30 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

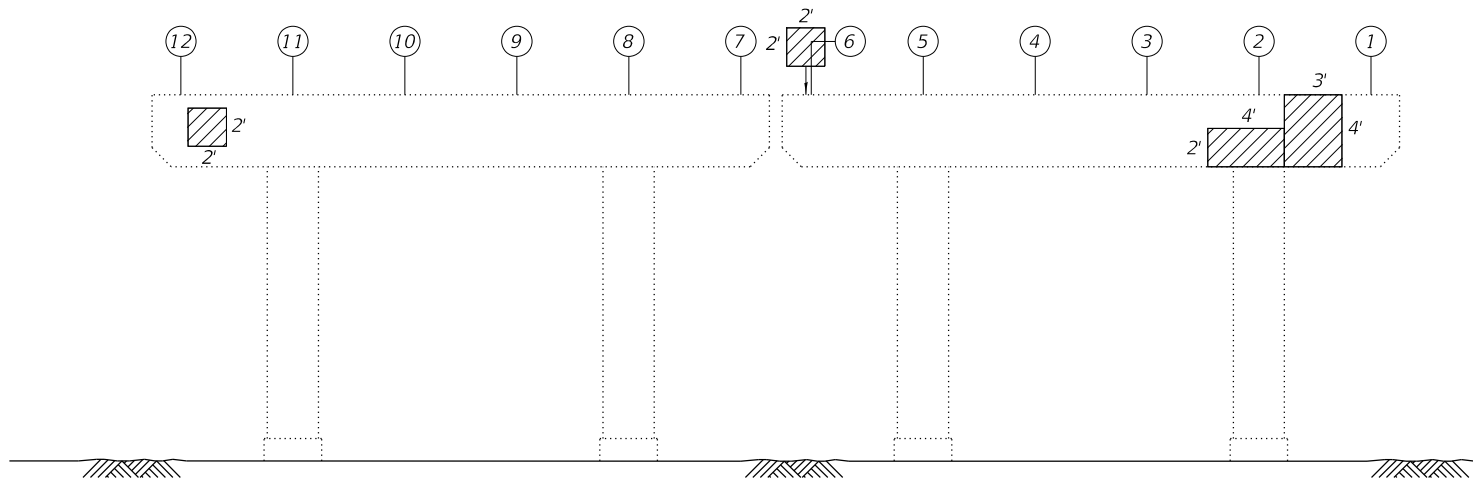
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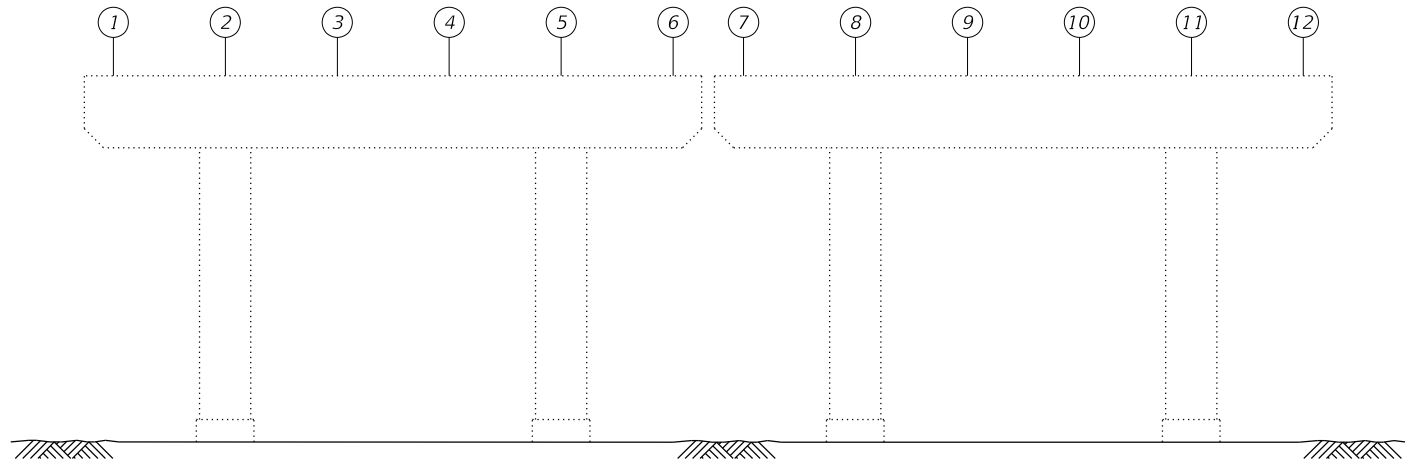
PIER 12 - SOUTH FACE
(Looking North)



PIER 12 - NORTH FACE
(Looking South)



PIER 13 - SOUTH FACE
(Looking North)




PIER 13 - NORTH FACE
(Looking South)

NOTE:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Removal and installation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

 Structural Repair of Concrete
(Depth Equal to or Less Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	48



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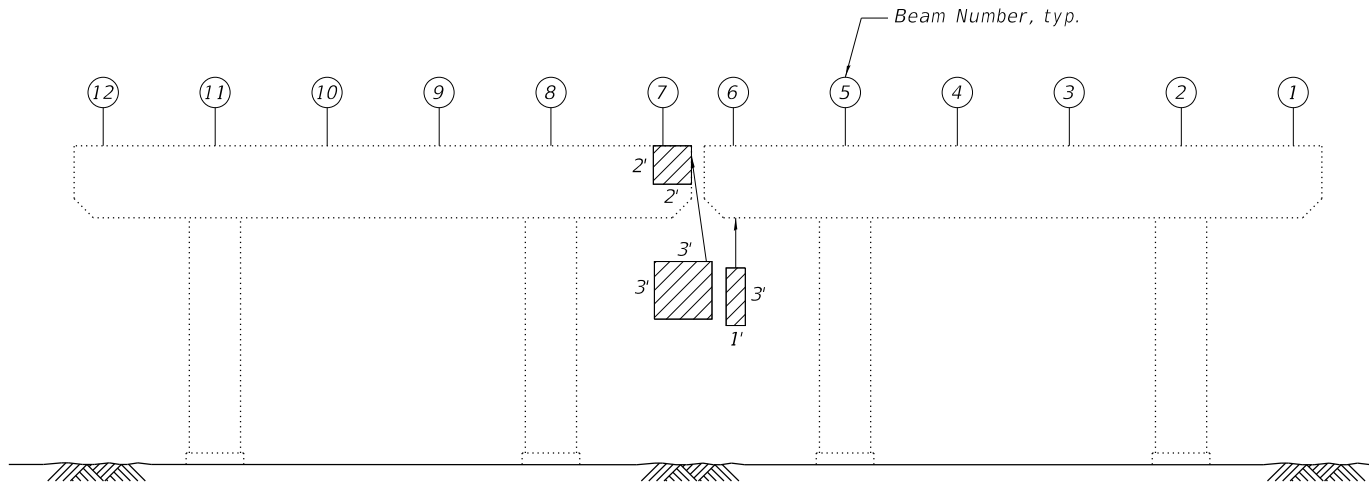
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 12 AND 13 REPAIR DETAILS
STRUCTURE NO. 016-1011**

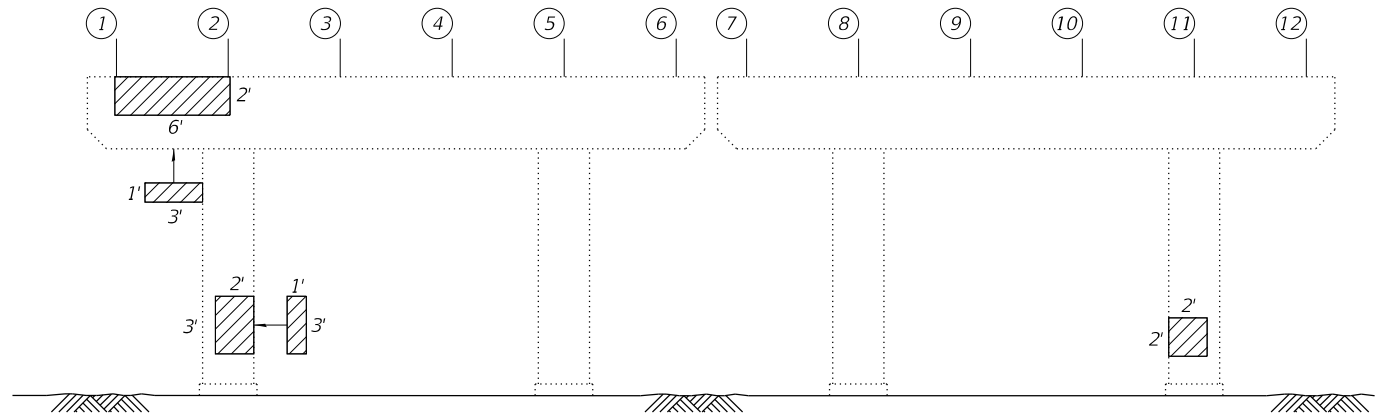
SHEET 5-31 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	63
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

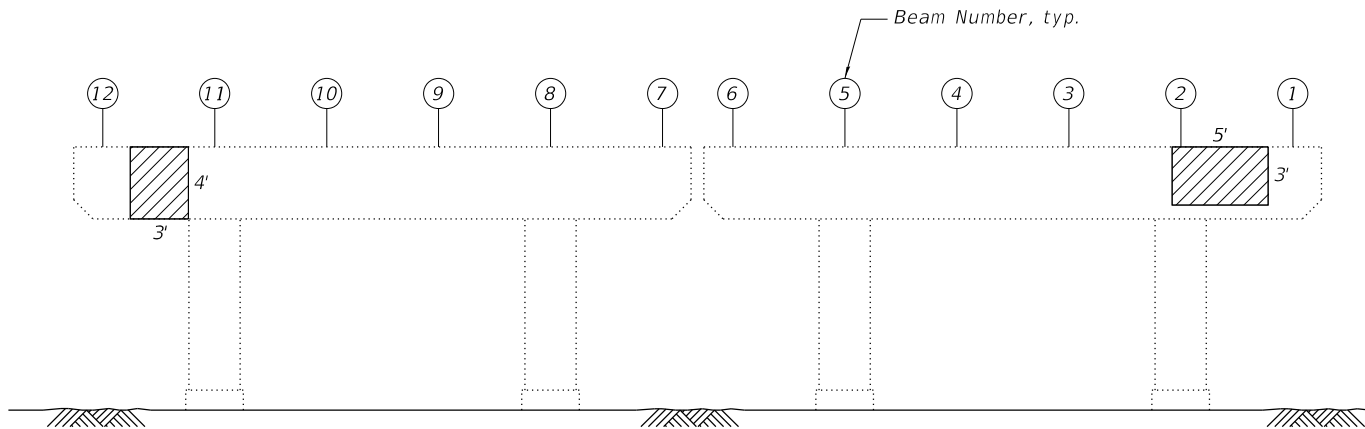
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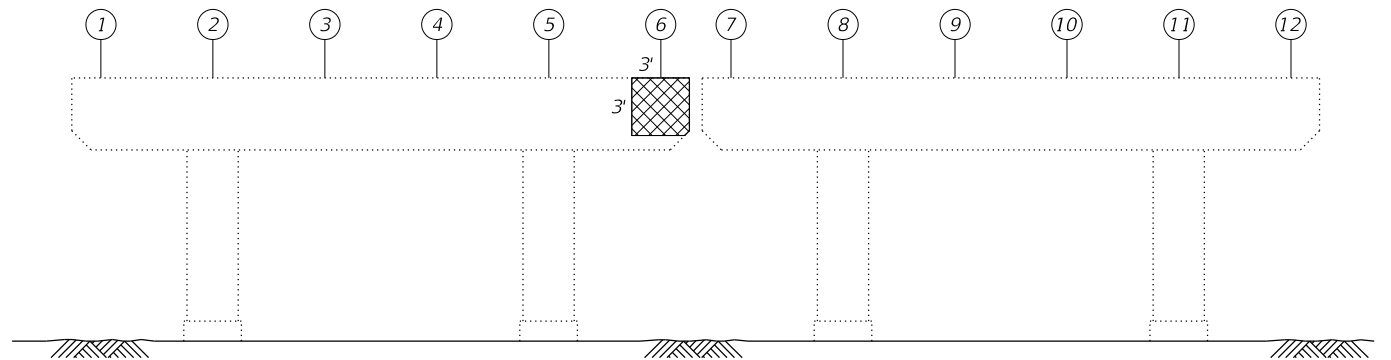
PIER 14 - SOUTH FACE
(Looking North)



PIER 14 - NORTH FACE
(Looking South)



PIER 15 - SOUTH FACE
(Looking North)





PIER 15 - NORTH FACE
(Looking South)

NOTE:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

-  Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
-  Structural Repair Of Concrete (Depth Greater Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	71
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft	9



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CHECKED	-	RRD
DRAWN	-	SVJ
DATE	-	10/06/2020

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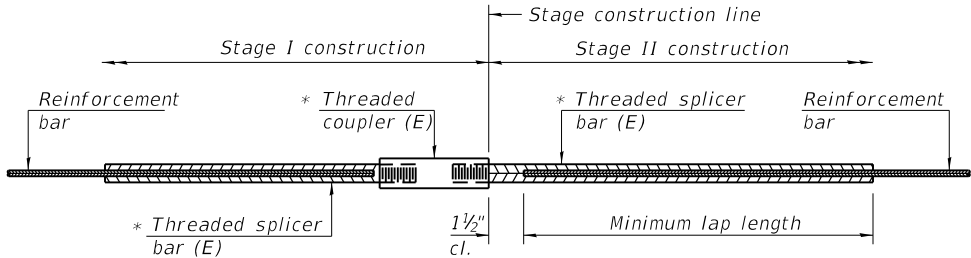
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 14 AND 15 REPAIR DETAILS
STRUCTURE NO. 016-1011**

SHEET 5-32 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	64
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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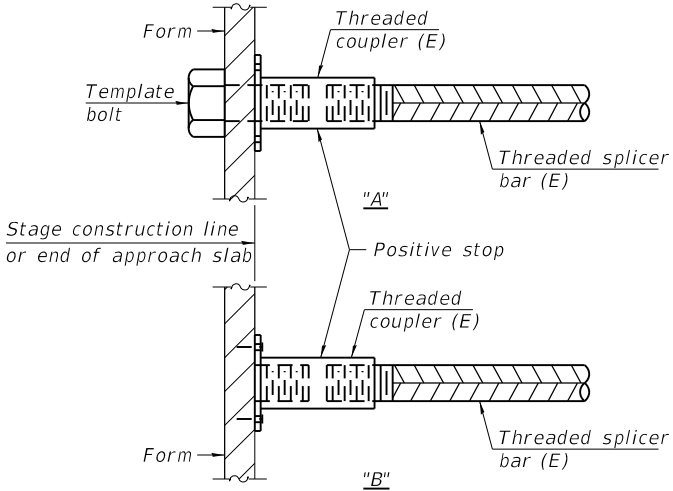


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

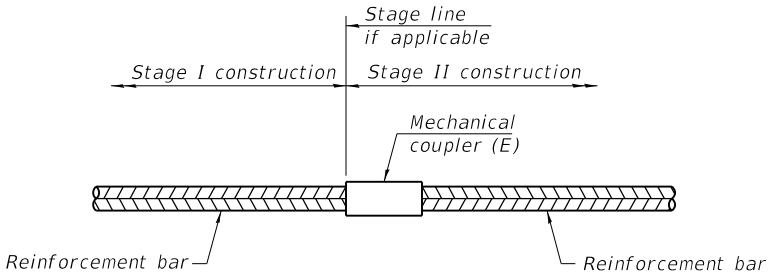
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abutment	#5	9	3'-9"
S. Abutment	#6	4	5'-2"
Pier 5	#5	18	3'-9"
Pier 6	#5	18	3'-9"
Pier 10	#5	18	3'-9"
Pier 11	#5	18	3'-9"
N. Abutment	#5	9	3'-9"
N. Abutment	#6	4	5'-2"



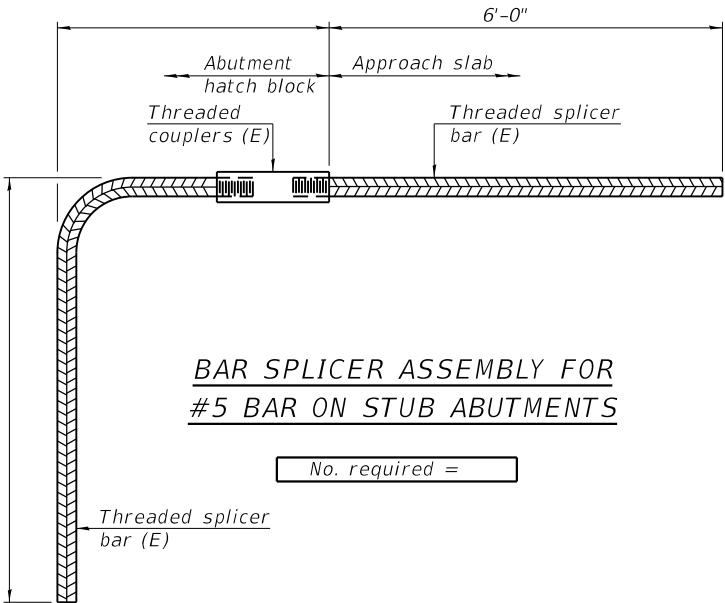
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017



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DESIGNED	-	JES
CHECKED	-	RRD
DRAWN	-	SVJ
DATE	-	10/06/2020

REVISED	-	
REVISED	-	
REVISED	-	
REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-1011

SHEET 5-33 OF 5-34 SHEETS

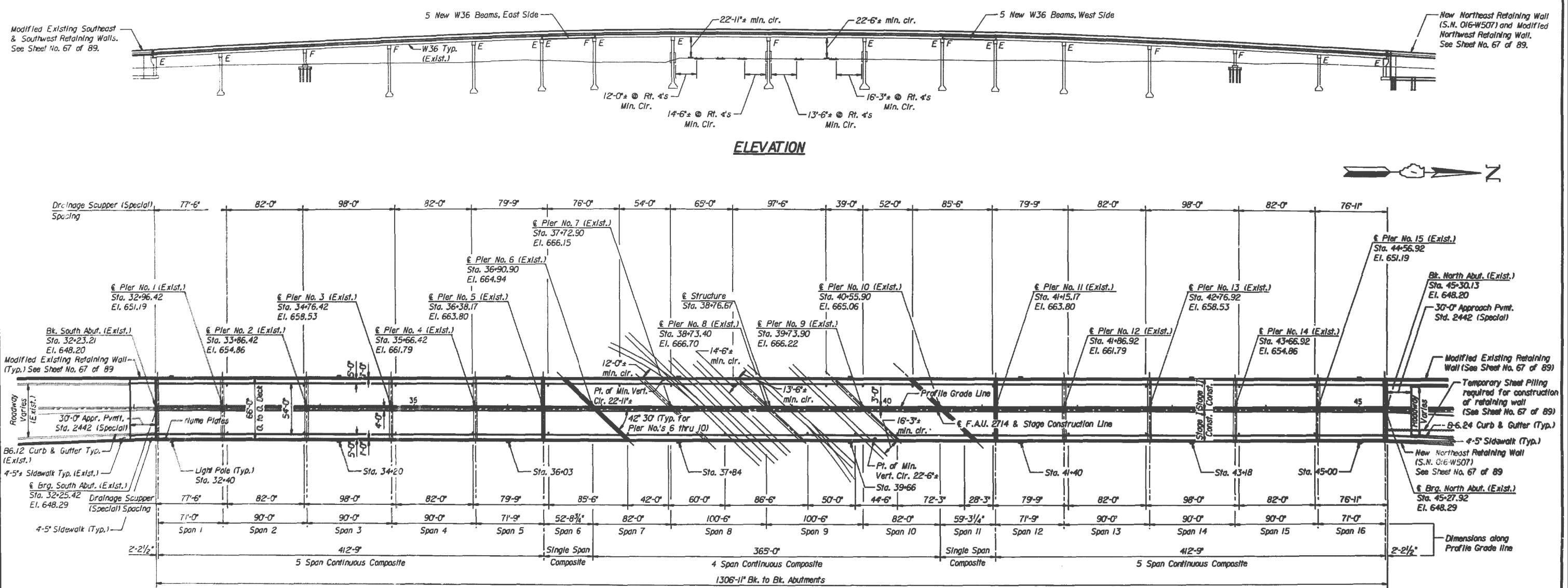
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	65
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

T.B.M.: Railroad spike in light pole in northeast quadrant of 25th Avenue and Armitage Ave. Elev. 637.23
Existing Structure: Structure No. 016-1011, built as 25th Avenue, Section 020-1010.2MFT in 1965 is 66'-0" wide by 1302'-6" long. The existing sixteen span reinforced concrete deck and steel wide flange beams are supported on multiple column piers and closed abutments. The existing concrete deck shall be removed. Five beams in Span 6 and five beams in Span 11 shall be replaced with new W36 steel beams. Existing expansion bearings below deck joints shall be replaced. Existing abutments and piers shall be repaired. A new 66'-0" wide composite concrete deck shall be constructed. Existing abutments, piers, and steel beams shall be incorporated into new construction. Traffic shall be staged during construction.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.
FAU 2714	1010.2R	COOK	89	26	1
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT				41

FOR INFORMATION ONLY



PLAN

APPROVED
FOR STRUCTURAL ADEQUACY ONLY

Ralph E. Anderson
Engineer of Bridges and Structures



Signed *Gary S. Powell*
Gary S. Powell, S.E., Ill. No. 081-004771
Expires 11-30-1994
Date 7-23-1993

Bascor, Inc.
consulting engineers and planners

DESIGNED	DAZ
CHECKED	GSP
DRAWN	SAW
CHECKED	GSP

DATE 4-30-1993
rev. 7-26-1993

DESIGN STRESSES

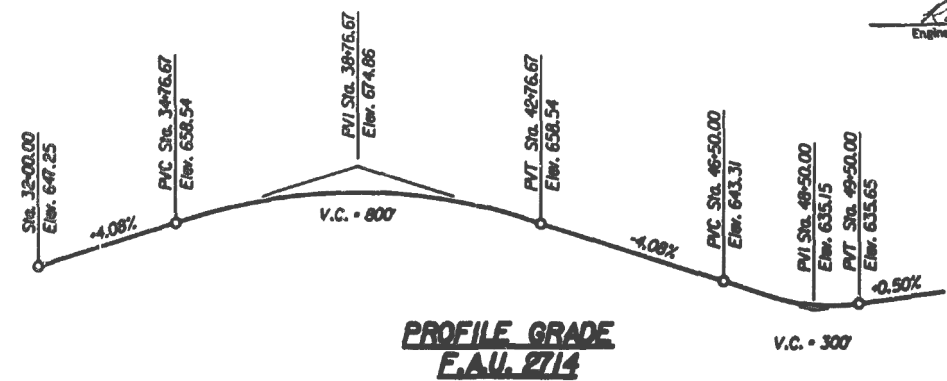
CAST-IN-PLACE CONCRETE
f'c = 3,500 psi
fy = 60,000 psi (Reinf.)
STRUCTURAL STEEL
Fy = 36,000 psi (AASHTO M270 Gr. 35)

LOADING

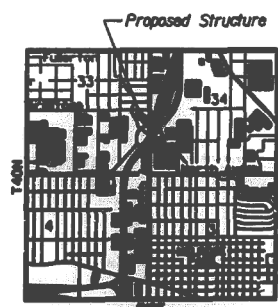
HS20-44
25 psf Future Wearing Surface

SPECIFICATIONS

Standard Specifications for Highway Bridges,
AASHTO 1992.



PROFILE GRADE
F.A.U. 2714



LOCATION PLAN

GENERAL PLAN
& ELEVATION

25TH AVENUE OVER MB RAILROAD
F.A.U. RTE. 2714 SECTION 1010.2R
COOK COUNTY
STA. 38+76.67
S.N. 016-1011

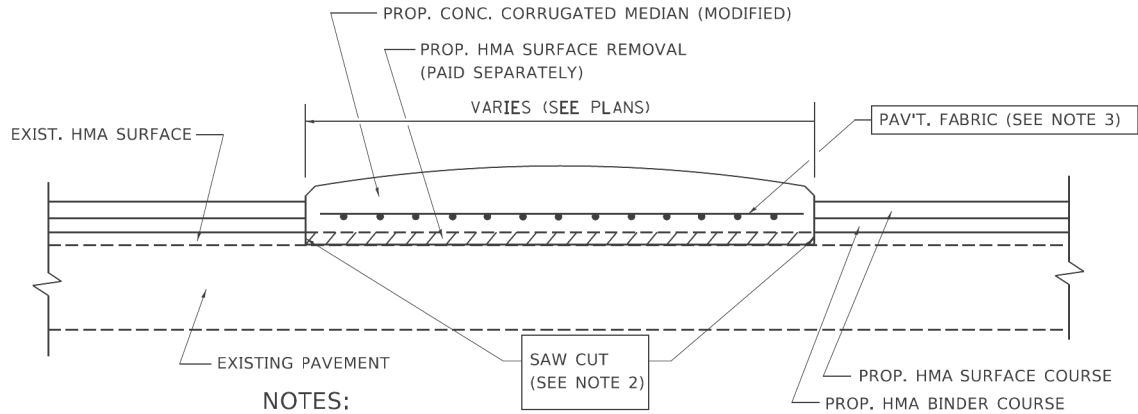
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-1011

SHEET 5-34 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	66
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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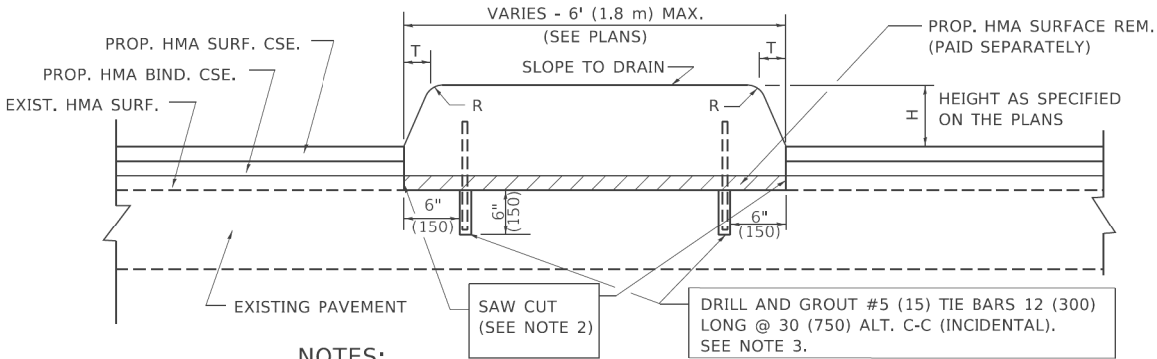


NOTES:

1. CORRUGATED MEDIAN (MODIFIED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 606 OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE PORTIONS OF STATE STANDARD 606306.
2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)
3. PAVEMENT FABRIC WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)

DETAILS FOR CORRUGATED MEDIAN (MODIFIED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CORRUGATED MEDIAN (MODIFIED)"



NOTES:

1. CONCRETE MEDIAN TYPE SB (DOWELLED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STATE STANDARD 606301 AND SECTION 606 OF THE STANDARD SPECIFICATIONS.
2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"
3. FOR MEDIAN WIDTH LESS THAN 4' (1.2 m) USE ONE ROW OF #5 (15) BARS @ 30 (750) C-C ALONG THE MEDIAN CENTERLINE. TIE BARS WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"

DETAILS FOR CONCRETE MEDIAN
TYPE SB (DOWELLED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CONCRETE MEDIAN TYPE SB (DOWELLED)"

H	R	T
6(150)	1(25)	1(25)
9(225)	1(25)	2(50)

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	DRAWN -	REVISED - R. SHAH 10-25-94
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PLOT DATE = 3/27/2019	DATE - 05-14-80	REVISED - R. BORO 01-01-07

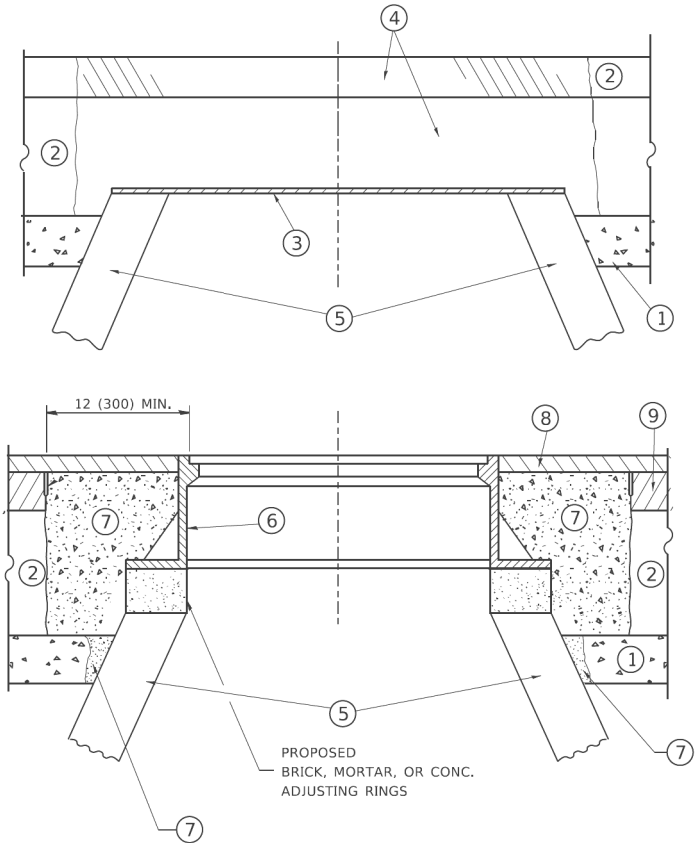
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR CONCRETE MEDIAN TYPE SB (DOWELLED) CORRUGATED MEDIAN (MODIFIED)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD600-02 (BD-5)		CONTRACT NO. 62M80		
		ILLINOIS	FED. AID PROJECT	

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PLOT DATE	= 3/27/2019	DATE	- 10-25-94	REVISED -	R. BORO 12-06-11



NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1 *CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

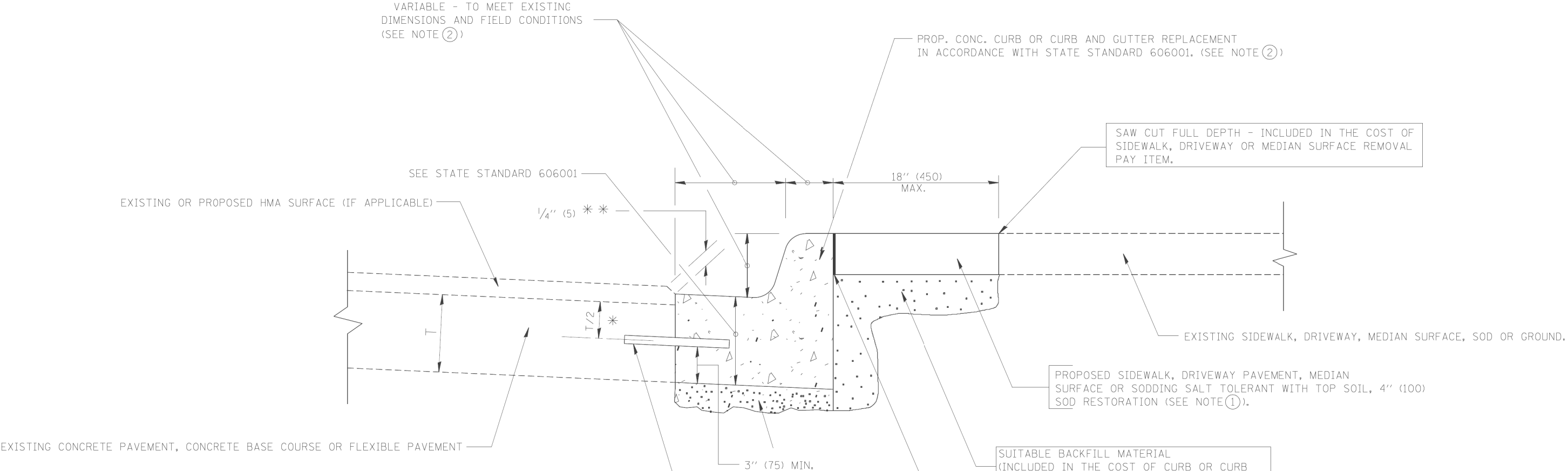
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE				2714	2020-203-BR	COOK	79	68
SHEET 1 OF 1 SHEETS				BD600-03 (BD-8)		CONTRACT NO. 62M80		
STA. TO STA.				ILLINOIS		FED. AID PROJECT		



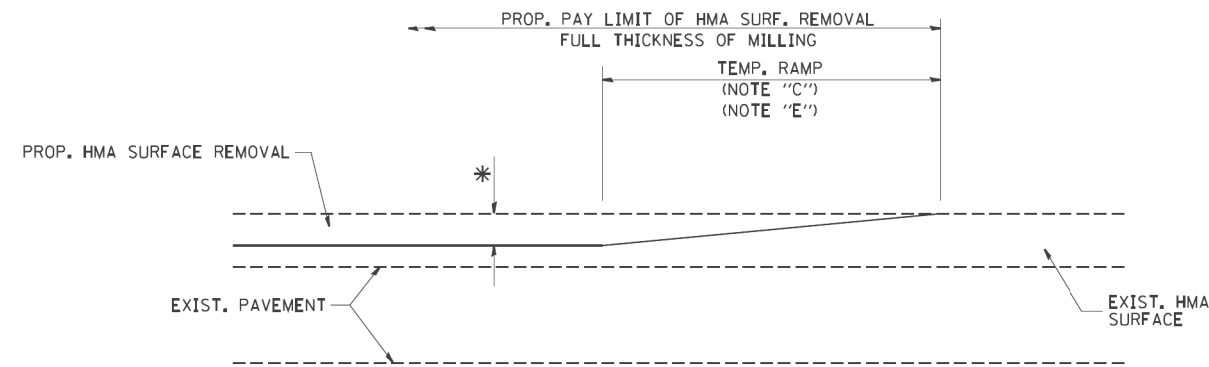
- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

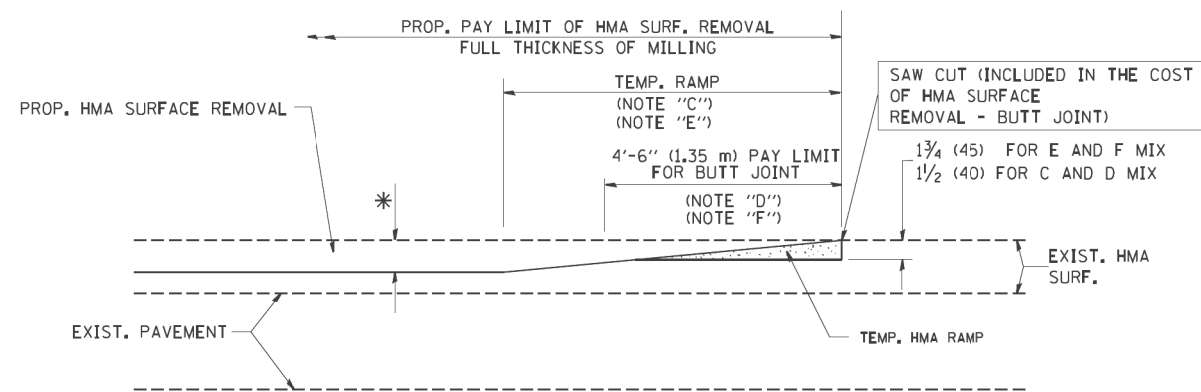
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

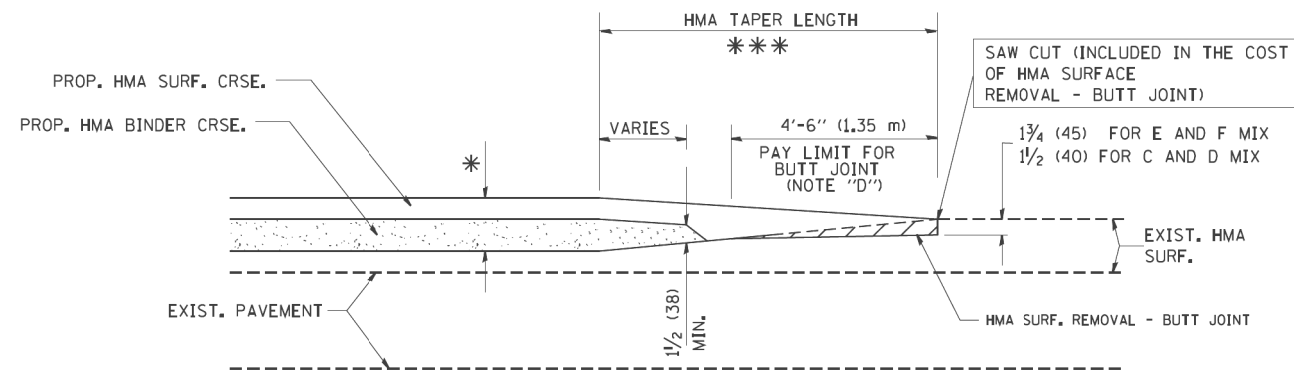
OPTION 1



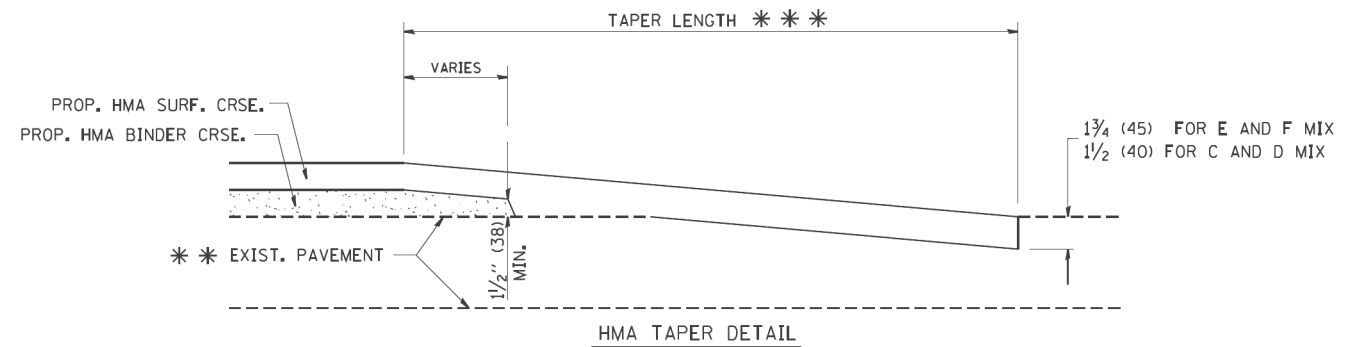
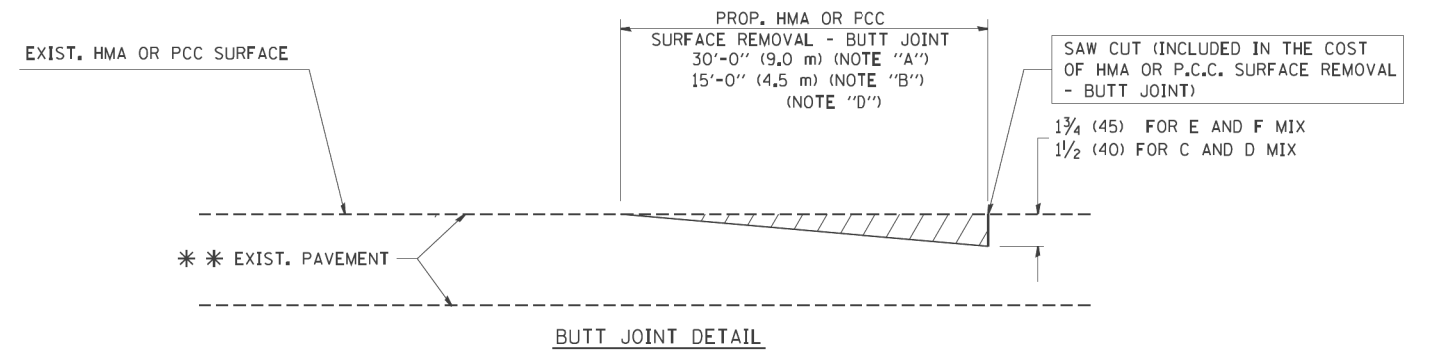
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND
HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
		DRAWN -	REVISED - A. ABBAS 03-21-97							2714	2020-203-BR	COOK	79	70			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01							BD400-05 BD32					CONTRACT NO. 62M80		
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07							ILLINOIS FED. AID PROJECT							
	SCALE: NONE					SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.							



- DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER

DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL
AND TRAFFIC BARRIER TERMINAL, OF THE TYPE
SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

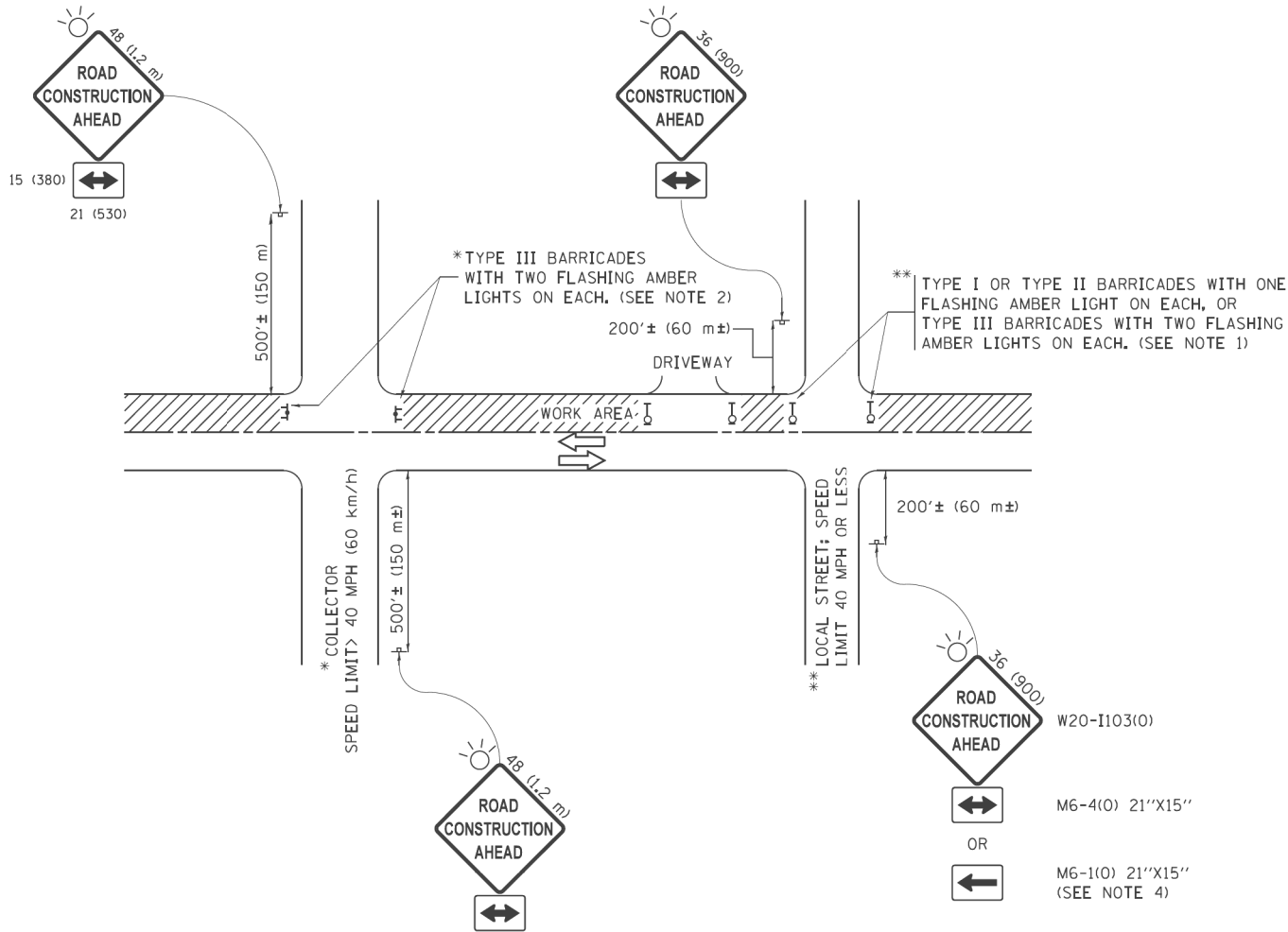
FILE NAME =	USER NAME = dravakosgn	DESIGNED - M. DE YONG	REVISED - R. BORO 12-08-2008
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	PLOT SCALE = 50,000' / 1" =	CHECKED -	REVISED - R. BORO 08-06-2012
Default	PLOT DATE = 12/21/2015	DATE = 09-22-90	REVISED - R. BORO 05-08-2015

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY.1 SPL.**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	71
BD600-10 (BD 34)		CONTRACT NO. 62M80		
ILLINOIS		FED. AID PROJECT		

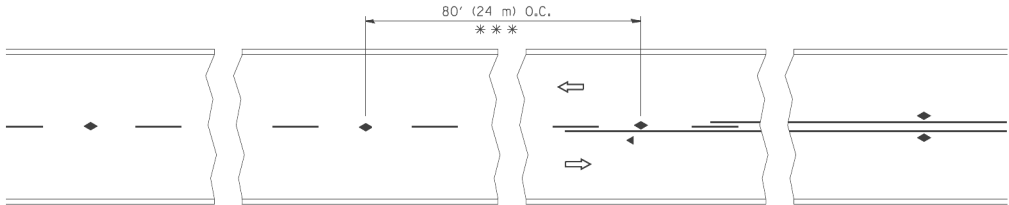


NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

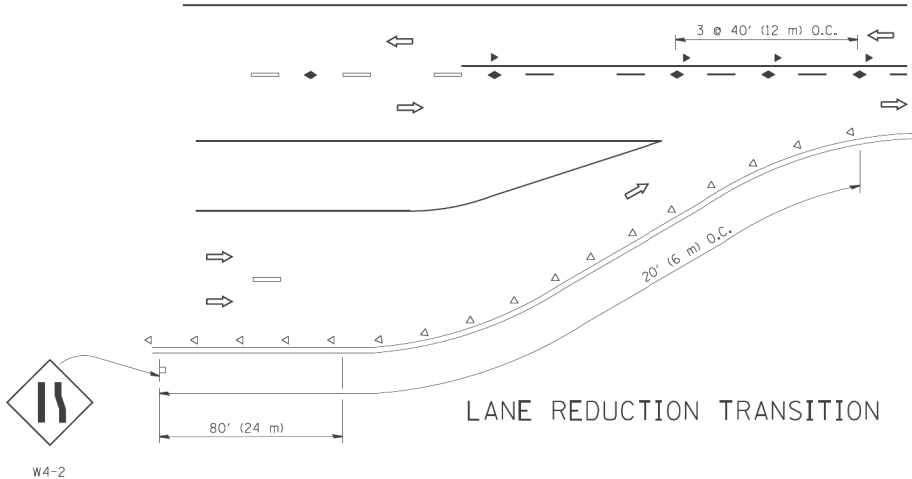
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pwt\11084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT_Offices\District_1\Projects\District_1\CADD\cadd\CADsheets\tc10.dgn Default	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	REVISED -T. RAMMACHER 01-06-00			2714	2020-203-BR	COOK	79	72
		PLOT SCALE = 50,000 ' / in.	CHECKED -			TC-10		CONTRACT NO. 62M80		
		PLOT DATE = 9/15/2016	DATE - 06-89			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS	FED. AID PROJECT

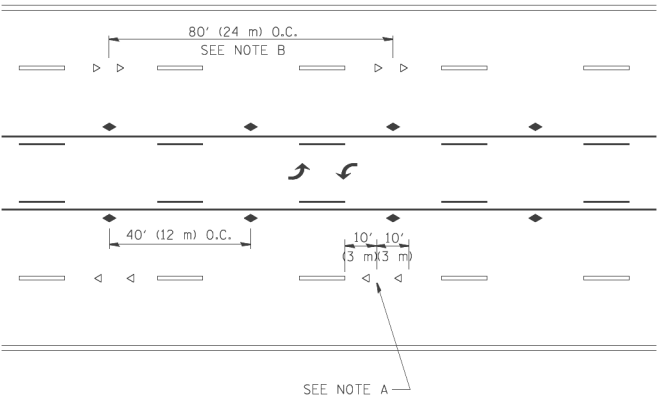


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

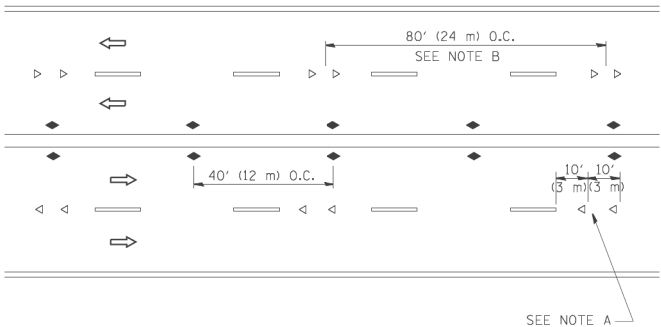
TWO-LANE/TWO-WAY



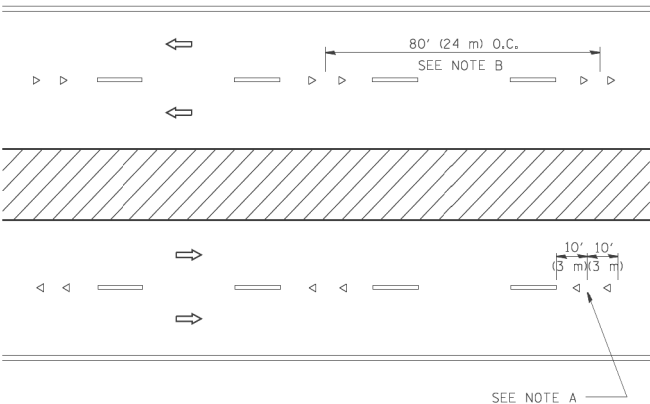
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

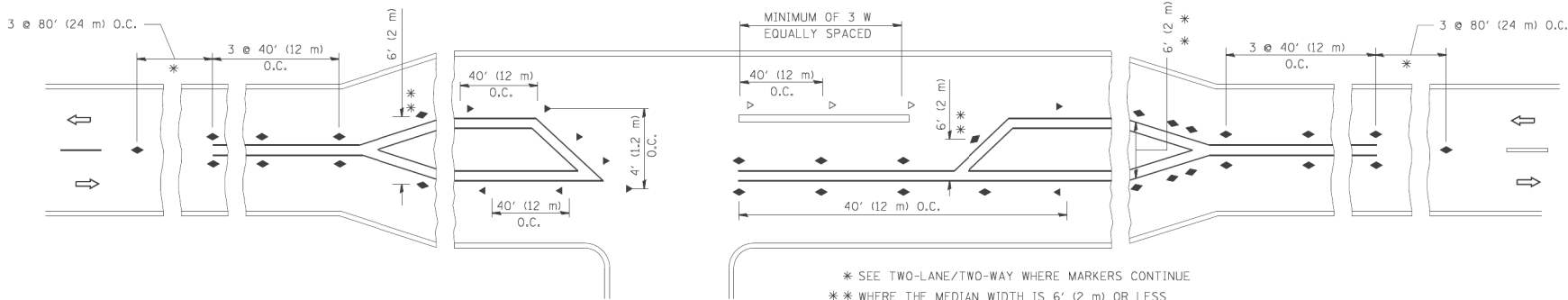
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

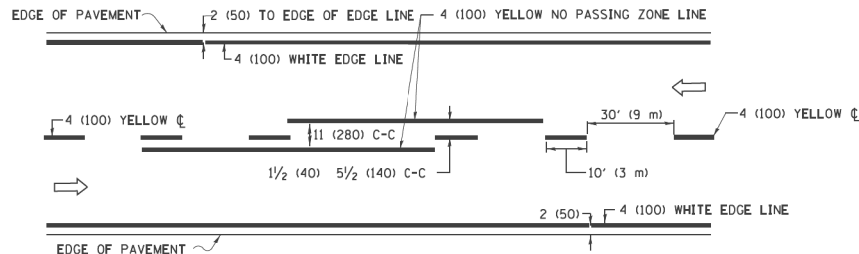
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



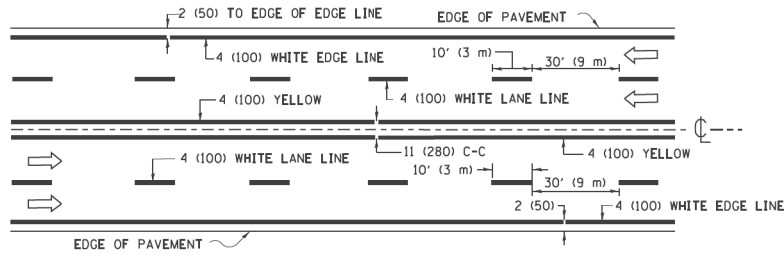
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

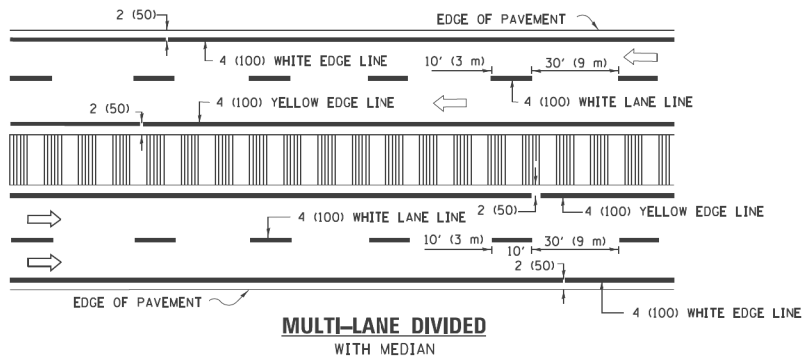
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		DRAWN -	REVISED - T. RAMMACHER 03-12-99					2714	2020-203-BR	COOK	79	73
		PLOT SCALE = 50.000' / IN.	CHECKED -		TC-11			CONTRACT NO. 62M80				
		PLOT DATE = 3/2/2011	DATE -		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



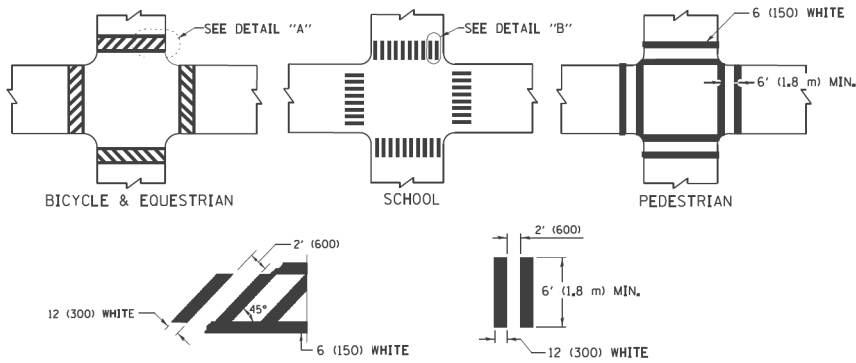
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING

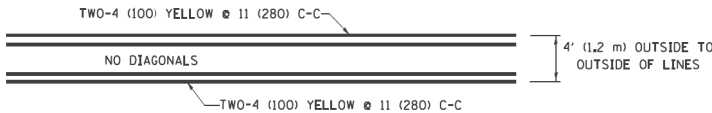


DETAIL "A"

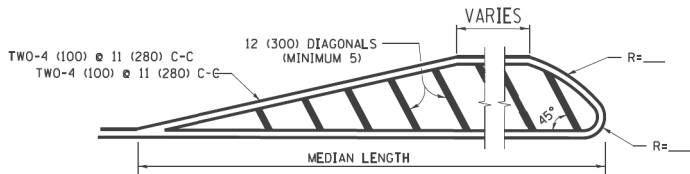
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

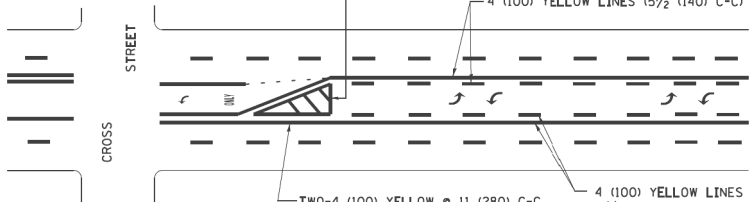


4' (1.2 m) WIDE MEDIANS ONLY

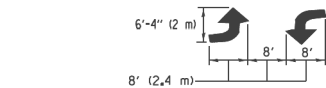


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

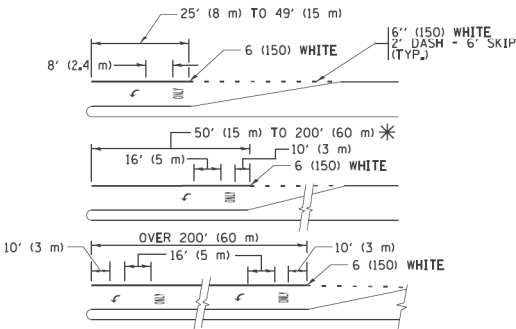


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

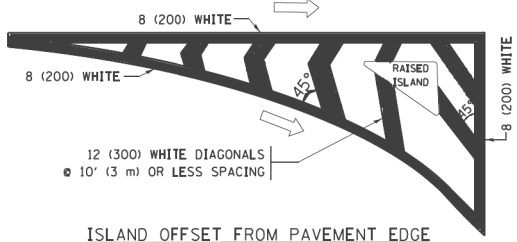


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

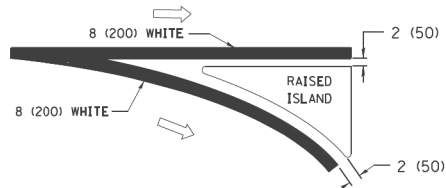
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

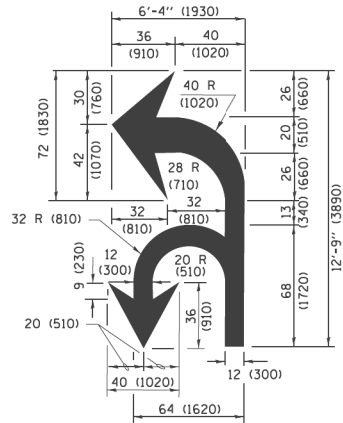


ISLAND OFFSET FROM PAVEMENT EDGE

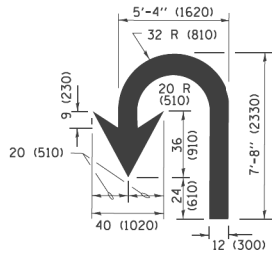


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = lrysa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
W:\dststd\22x34\tol3.dgn		DRAWN -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50,000' / 1" =	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	74
TC-13		CONTRACT NO. 62M80		
		ILLINOIS	FED. AID PROJECT	

FIGURE 1

Diagram illustrating the removal of conflicting pavement marking removal (TYP.) and the application of 6" white reflective pavement marking tape and 4" yellow reflective pavement marking tape.

Labels and Callouts:

- CONFLICTING PAVEMENT MARKING REMOVAL (TYP.)
- ONLY
- SEE DETAIL "A"
- 6" WHITE REFLECTIVE PAVEMENT MARKING TAPE
- 20' (6 m)
- 4" YELLOW REFLECTIVE PAVEMENT MARKING TAPE (REMOVE CONFLICTING WHITE SKIP-DASH LINES FIRST.)
- MEDIAN

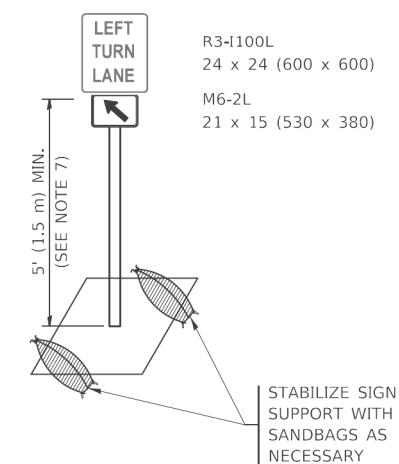
Dimensions:

- 600)
- 380)

FIGURE 2

	WORK AREA
	LANE OPEN TO TRAFFIC
	ARROW BOARD
	TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
	DRUM WITH STEADY BURN LIGHT
	SIGN ASSEMBLY
	TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

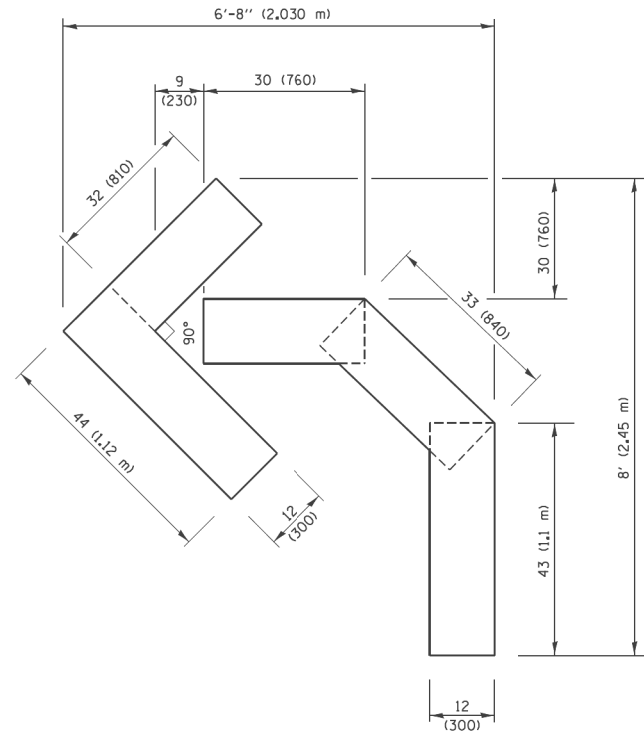
1. WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE
(AS SHOWN IN FIG. 1), USE FIGURE 1.
- B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE
OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE
SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF
28 (710) IN HEIGHT.
3. LIGHTS SHALL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY
OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED
THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN
WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN
THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER
THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600)
AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL
FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS
ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH
REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN
TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC
CONTROL STANDARDS OR ITEMS.



DETAIL A

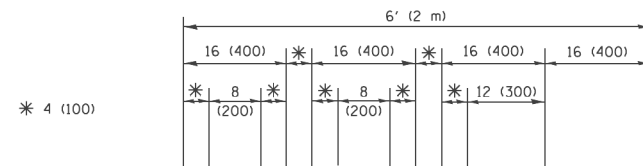
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footernj	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</div>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13						2714	2020-203-BR	COOK	79	75
PLOT SCALE = 50,0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		<div>SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.</div>				TC-14 CONTRACT NO. 62M80				
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -						<div>ILLINOIS FED. AID PROJECT</div>				



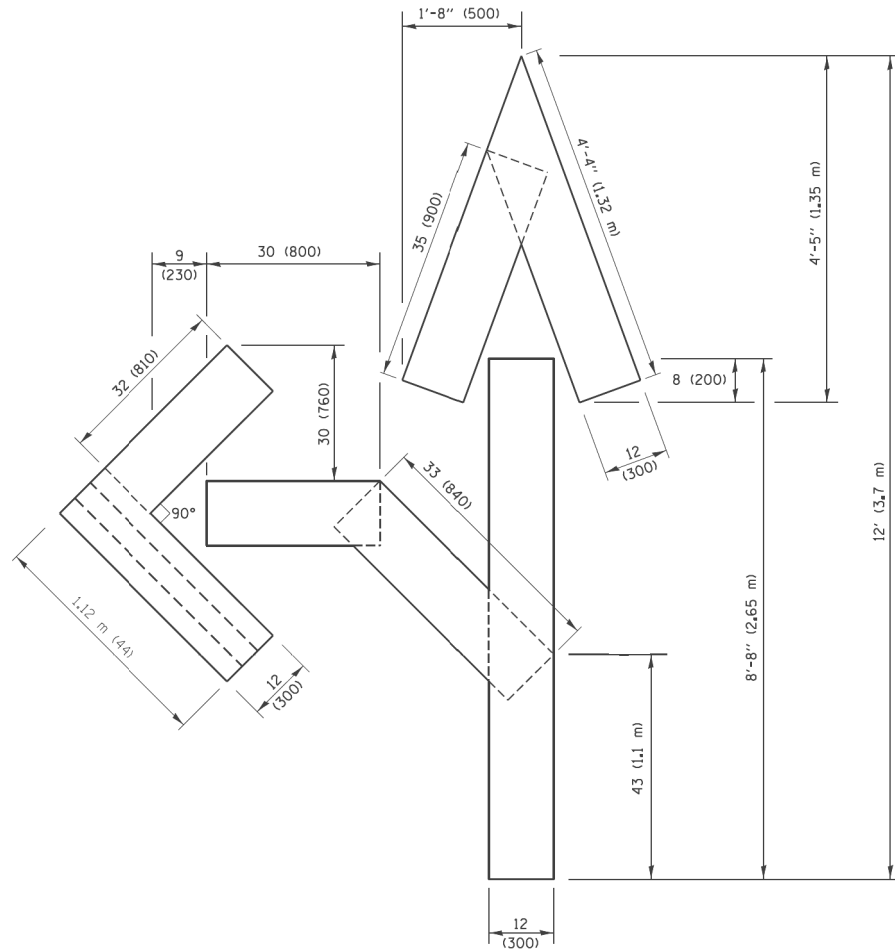
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

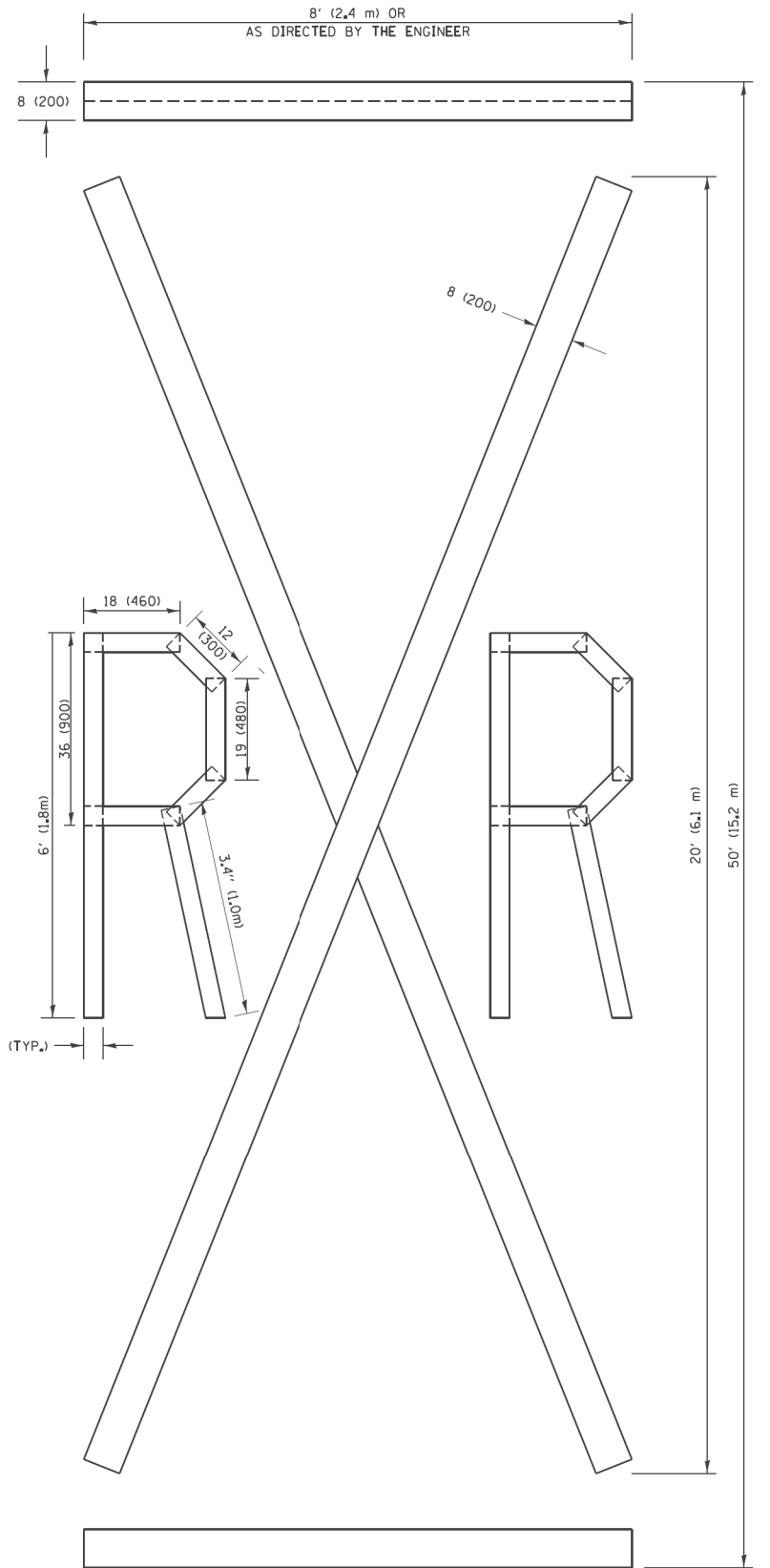


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)


All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footenj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pwt\1\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\JDOT Offices\District 1\Projects\District 1\CADData\CADsheets\tc16.dgn		DRAWN	REVISED -E. GOMEZ 08-28-00					2714	2020-203-BR	COOK	79	76
	PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00		TC-16			CONTRACT NO. 62M80				
	PLOT DATE = 9/15/2016	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				

ROUTE MARKERS

 FOR U.S. ROUTES
M1-40-2424

 FOR ILLINOIS ROUTES
M1-50-2424

 R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

 M5-1L-2115

 M5-1R-2115

 M6-1-2115

 M6-1-2115

 M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

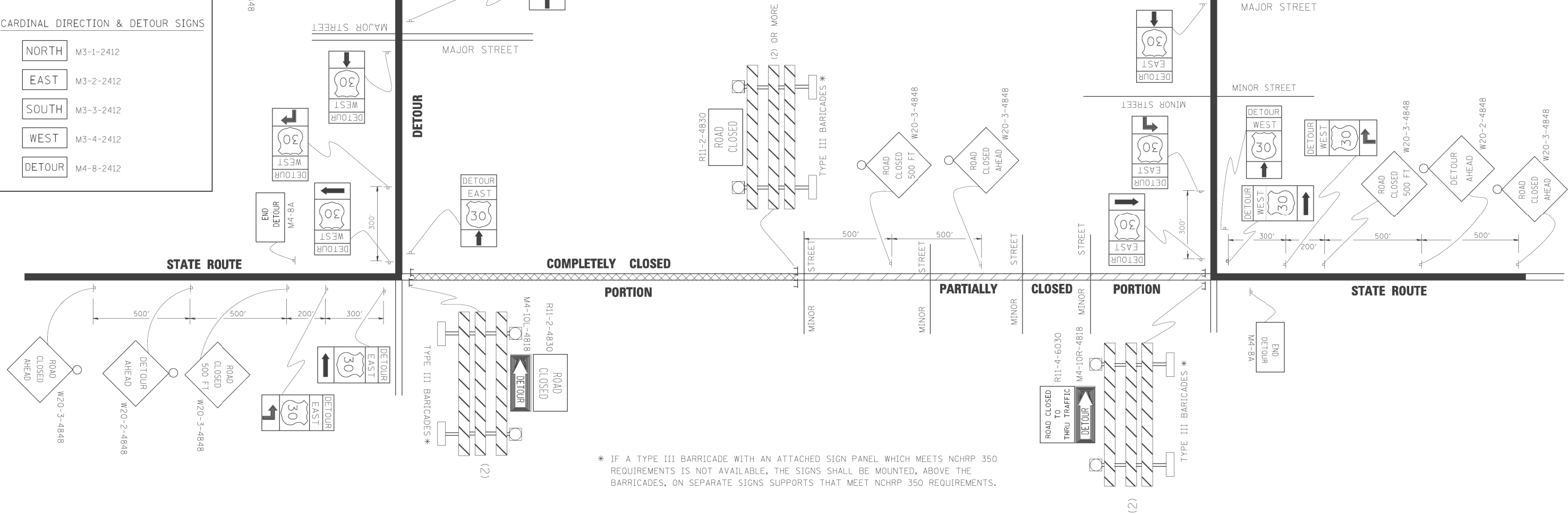
 NORTH M3-1-2412

 EAST M3-2-2412

 SOUTH M3-3-2412

 WEST M3-4-2412

 DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

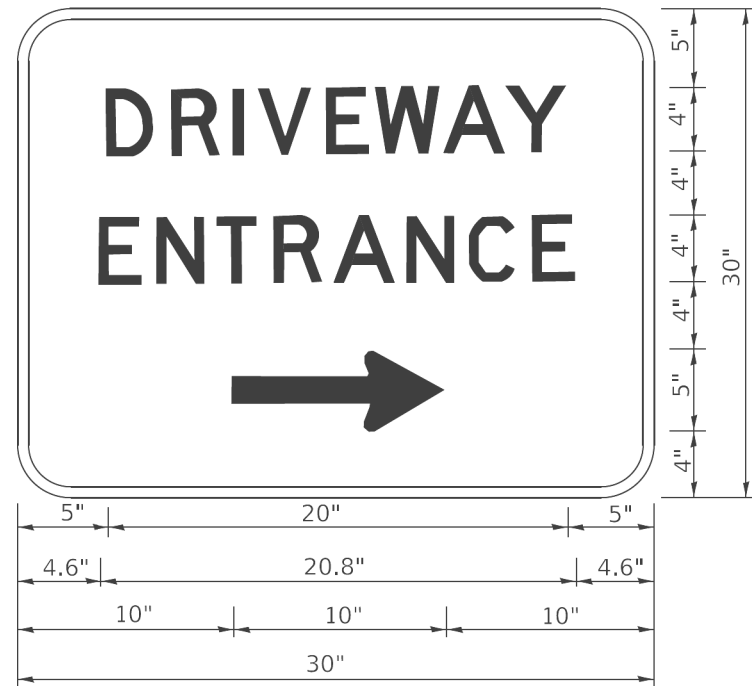
FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 10-18-02
ca:\p\work\PWIDOT\DRIVAKOSGN\d0108315\td21.dgn		DRAWN -	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/14/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	77
TC-21		CONTRACT NO. 62M80		
ILLINOIS		FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:
- 1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 - 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 - 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL Default
FILE Name: p:\u0841EID\NITEC\Illinois.gov\PHWDOT\Documents\DOT Offices\Bldgdt 1\Projects\Bldgdt\23\231\CADData\CAD\Sheet\26.dgn

	USER NAME = footemj	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					2714	2020-203-BR	COOK	79	79
	PLOT SCALE = 50,0000 ' / 1".	CHECKED -	REVISED -		TC-26			CONTRACT NO. 62M80				
	PLOT DATE = 3/4/2019	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		