

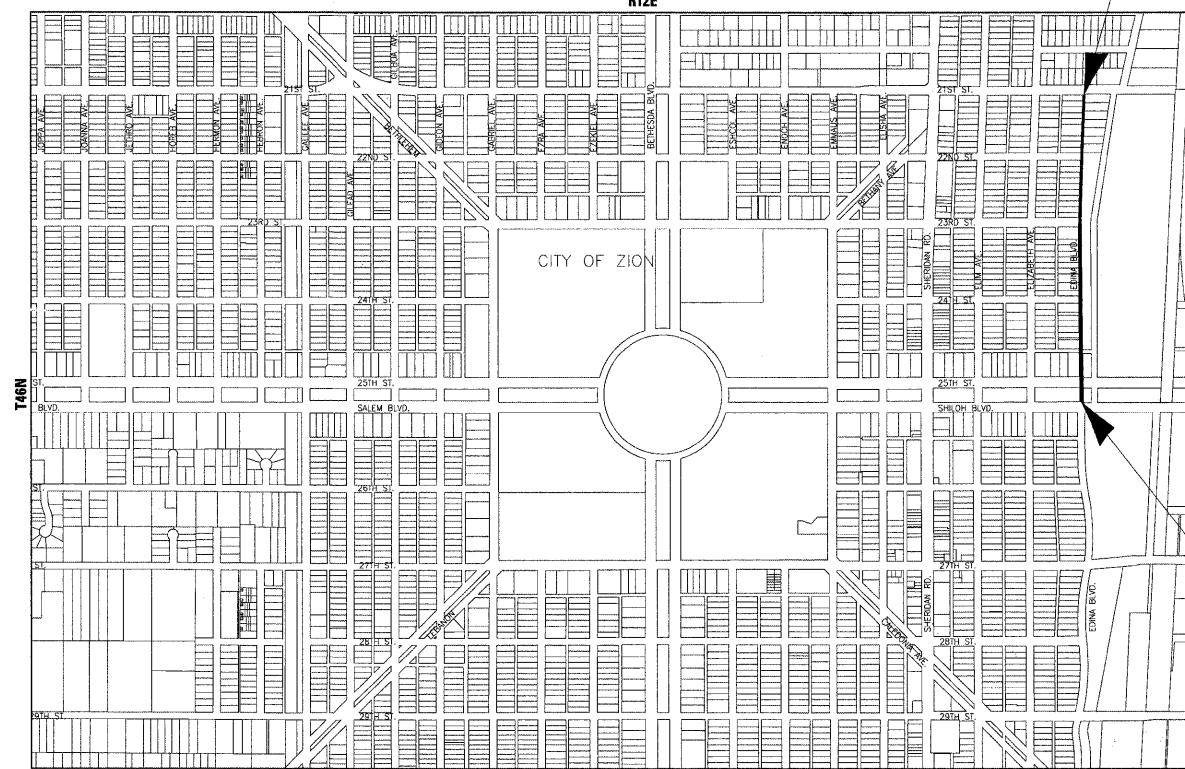
FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF ZION

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
F.A.U. ROUTE 2769 EDINA BOULEVARD
FROM 21ST STREET (F.A.U. 1202) TO SHILOH BOULEVARD (F.A.U. 1205)
PROJECT NO.: ARA-9003(442)
SECTION NO.: 09-00083-00-RS
JOB NO.: C-91-906-09
CITY OF ZION
LAKE COUNTY

LOCATION MAP
(NOT TO SCALE)
R12E

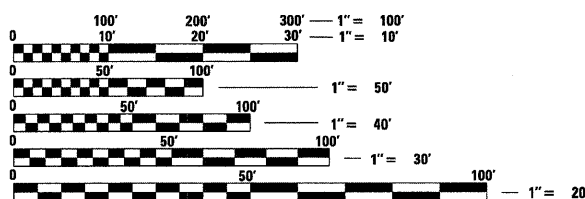


PROJECT NO. ARA-9003(442)
F.A.U. ROUTE 2769
EDINA BOULEVARD
END RESURFACING
STA. 27 + 62

PROJECT NO. ARA-9003(442)
F.A.U. ROUTE 2769
EDINA BOULEVARD
BEGIN RESURFACING
STA. 1 + 16

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LICENSE NO. - 184-001121 - EXPIRES 4/30/2011

TRAFFIC DATA
EDINA BOULEVARD
POSTED & DESIGN SPEED LIMIT = 30 MPH
2009 ADT = 175
DESIGNATION = LOCAL COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

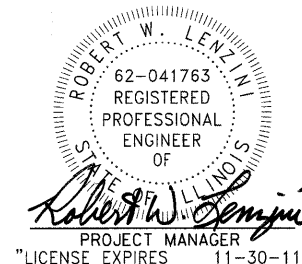
J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A2152153



CONTACT JULIE AT 811 OR 800-892-0123
WITH THE FOLLOWING:
COUNTY = LAKE
CITY-TOWNSHIP = ZION-ZION
SEC. & 1/4 SEC. NO. = WEST 1/2 SEC. 22
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 63336

ZION TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 2,646 LF OR 0.50 MILES
NET LENGTH OF IMPROVEMENT = 2,646 LF OR 0.50 MILES

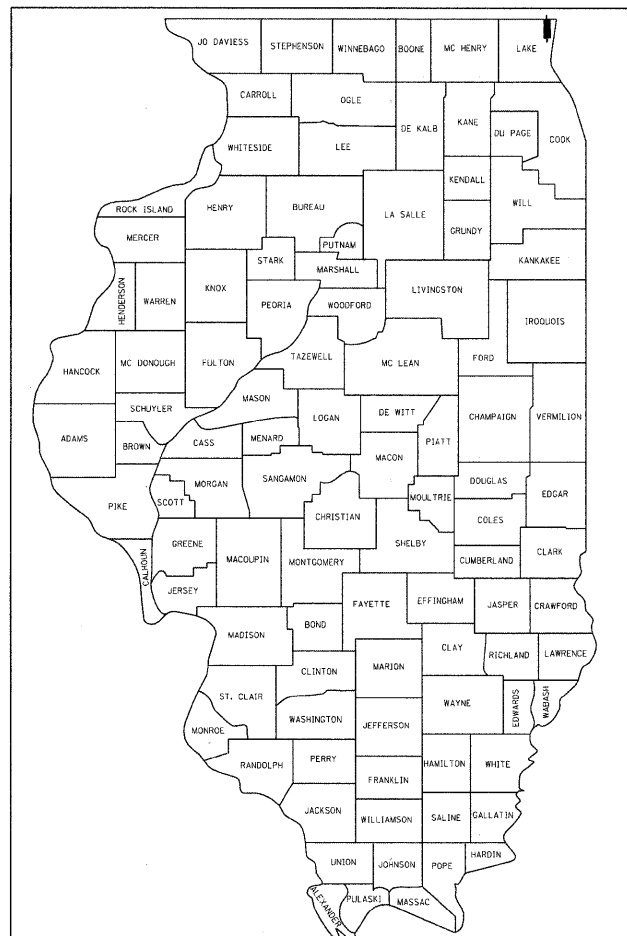


STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	<i>[Signature]</i> 10/15/09 CITY OF ZION, MAYOR
PASSED	<i>[Signature]</i> OCTOBER 27, 2009 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	OCTOBER 29, 2009 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

B&W PROJECT NO.: 090759 DATE: 10-05-09

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2769	09-00083-00-RS	LAKE	17	1
STA.	1+16	TO STA.	27+62	
FED. ROAD DIST. NO 1	ILLINOIS	FED AID PROJECT		
C-91-906-09		ARA-9003(442)		
CONTRACT NO.	63336			



LOCATION OF SECTION INDICATED THUS: [Symbol]

Burlington, Wisconsin 262.763.7834 Chicago, Illinois 312.578.0050 Crystal Lake, Illinois 815.459.1260 DeKalb, Illinois 815.787.3111 Grayslake, Illinois 847.223.5088 Itasca, Illinois 630.773.1870 Madison, Wisconsin 608.347.1542 Mokena, Illinois 708.478.2090 Plainfield, Illinois 815.609.7425
(OFFICE WHICH PREPARED PLANS)

GENERAL NOTES

3. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR CITY.
4. THE CONTRACTOR SHALL NOTIFY THE CITY'S DIRECTOR OF PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN CITY UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
5. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE CITY, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE CITY WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
6. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CITY RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF CITY WATER IF DEEMED NECESSARY.
7. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE CITY WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE CITY TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
9. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.

10. ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH CITY STANDARDS AND INCLUDED IN THE PAY ITEM FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.

11. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.

12. IN AREAS WHERE THE EXISTING DRIVEWAY, SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, DRIVEWAY PAVEMENT REMOVAL, SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL.

13. THE LOCATIONS OF CLASS D PATCHES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

14. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.

15. THE CURB SHALL BE TAPERED TO THE GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE CURB AND GUTTER TERMINATES, WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER.

16. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.

17. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.

18. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.

19. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.

20. DETECTABLE WARNINGS SHALL CONSIST OF DYED CONCRETE STAMPED WITH APPROPRIATE STAMPING TOOLS. THE PIGMENT SHALL MEET THE REQUIREMENTS OF ASTM C 979, MATCH COLOR 30166 OF FEDERAL STANDARD 595. THE COST OF DYING CONCRETE IN ADDITION TO THE DETECTABLE WARNING AREA SHOWN ON THE DETAIL SHALL BE INCLUDED IN THE COST OF DETECTABLE WARNINGS BUT THE LIMITS OF THE DYED CONCRETE SHALL NOT EXTEND BEYOND THE BACK OF THE DETECTABLE WARNING AREA.

21. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE MC-30 PRIME COAT APPLICATION RATE SHALL BE 0.3 GAL/SY.

22. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.

23. THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS POURED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. REMOVAL AND DISPOSAL OF UNSUITABLE SUB-BASE MATERIAL, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, REGARDLESS OF THE DEPTH OF MATERIAL REMOVED. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH SECTION 1081.03 RATHER THAN SALT TOLERANT SOD.

24. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).

25. THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.

26. EXISTING FRAMES AND GRATES OR LIDS THAT ARE TO BE REPLACED SHALL BE DELIVERED TO THE CITY OF ZION PUBLIC WORKS.

27. NO DAMAGE TO PARKWAYS IS PLANNED AS PART OF THESE IMPROVEMENTS EXCEPT WHERE SIDEWALK IS INSTALLED OR REPLACED, OR CURB AND GUTTER IS REPLACED. ANY AND ALL RESTORATION OF DAMAGED PARKWAYS SHALL CONSIST OF NECESSARY SUB-GRADE MATERIAL AS APPROVED BY THE ENGINEER, TOPSOIL 4-INCH AND NATIVE SOD IN ACCORDANCE WITH SECTION 1081.03 OF THE STANDARD SPECIFICATIONS. RESTORATION WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PAY ITEM WHICH CAUSED THE DISTURBANCE.

28. HIGH DENSITY POLYETHYLENE (HDPE) PLASTIC ADJUSTING RINGS WILL NOT BE ALLOWED FOR ADJUSTMENT OF FRAMES AND GRATES OR LIDS.




INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, DETAILS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
5	SCHEDULE OF QUANTITIES
6 - 8	PLAN - EDINA BOULEVARD
9	DISTRICT 1 DETAIL - BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
10	DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
11	DISTRICT 1 DETAIL - BD-24 CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
12	DISTRICT 1 DETAIL - BD-32 BUTT JOINTS AND HMA TAPER DETAILS
13	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
14	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
15	DISTRICT 1 DETAIL - TC-16 PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
16	DISTRICT 1 DETAIL - TC-18 SIGNING AND FLAGGING OPERATIONS AT WORK ZONE OPENINGS
17	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN

HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2L,2W, UNDIVIDED
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

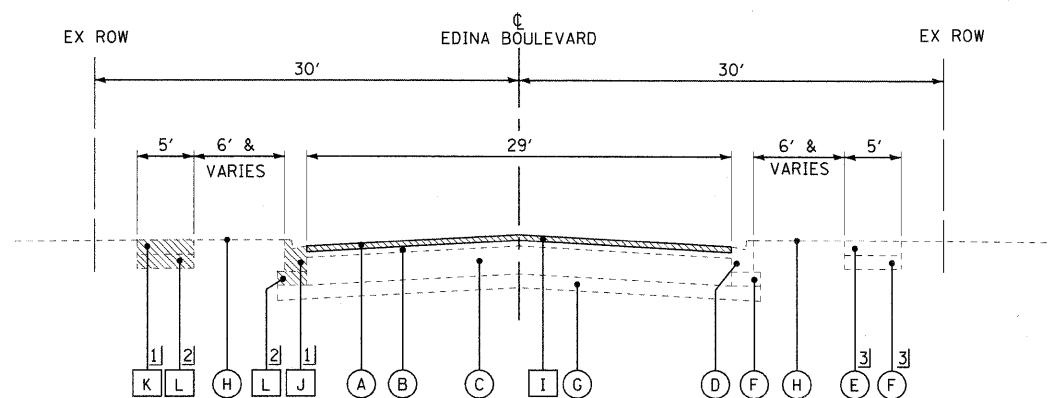
LEGEND

////////	BUTT JOINT OR DRIVEWAY BUTT JOINT
	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)
VBOX 	VALVE BOX TO BE ADJUSTED
	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

SUMMARY OF QUANTITIES

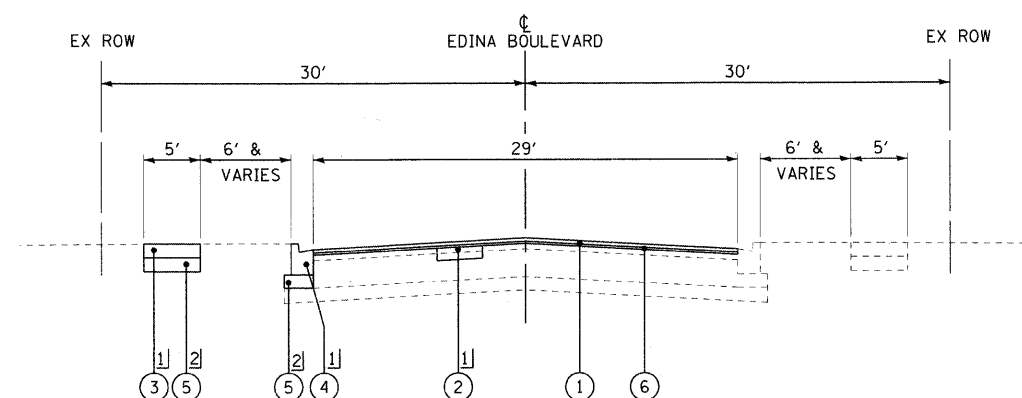
CODE NUMBER	PAY ITEM	UNIT	CONSTRUCTION CODE 1000 ROADWAY
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	206
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	935
40600300	AGGREGATE (PRIME COAT)	TON	19
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	140
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	833
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	560
42400800	DETECTABLE WARNINGS	SQ FT	210
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	9,342
44000600	SIDEWALK REMOVAL	SQ FT	560
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	647
60250200	CATCH BASINS TO BE ADJUSTED	EACH	7
60251500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	6
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1
60266610	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	2
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	10
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	308
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	114
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	155
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	538
X4420452	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	280
X4420454	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	300
X4420456	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	350

* INDICATES SPECIALTY ITEM



EXISTING TYPICAL SECTION
EDINA BOULEVARD
STA. 1+16 TO STA. 27+62

- 1) AS INDICATED ON PLANS OR AS DETERMINED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
- 2) AS REQUIRED
- 3) SIDEWALK DOES NOT EXIST ON EAST SIDE FROM STA. 1+16 TO 22+25



PROPOSED TYPICAL SECTION
EDINA BOULEVARD
STA. 1+16 TO STA. 27+62

- 1) AS INDICATED ON PLANS OR AS DETERMINED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
- 2) AS REQUIRED

HOT-MIX ASPHALT MIXTURE REQUIREMENTS NOTES:

1. CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 1 1/2"	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm); TYPE II - IV - 4-INCH	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE, 2"
- (B) HOT-MIX ASPHALT BINDER COURSE, 2"
- (C) AGGREGATE BASE COURSE, 10'±
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (E) SIDEWALK
- (F) AGGREGATE BASE COURSE
- (G) SUB-GRADE
- (H) GROUND SURFACE
- (I) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- (J) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (K) SIDEWALK REMOVAL
- (L) AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY)
- ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 1/2"
- (2) CLASS D PATCHES, TYPE I - IV, 4 INCH
- (3) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (5) AGGREGATE BASE COURSE, TYPE B 4"
- (6) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"

LEGEND NOTES:

1. SIDEWALK REMOVAL AND REPLACEMENT LOCATIONS TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.
2. CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.

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DESIGNED - SLN	REVISED - IDOT REVIEW 10/05/09
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 08-13-09	FILE - 090759-tyr sec.shx

CITY OF ZION, ILLINOIS EDINA BOULEVARD ARRA (LAPP) RESURFACING

TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS

SCALE:

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2769	09-00083-00-R5	LAKE	17	4
C-91-906-09				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(442)		

SCHEDULE OF QUANTITIES

HOT-MIX ASPHALT REMOVAL – BUTT JOINT

LOCATION	WIDTH (FT)	LENGTH (FT)	AREA (SQ YD)
SHILOH BOULEVARD (STA 1+16)	79	4.5	40
25TH STREET (STA 3+00, 98' LT)	25	4.5	13
24TH STREET (STA 9+92, 46' LT)	29	4.5	15
23RD STREET (STA 16+45, 46' LT)	29	4.5	15
22ND STREET (STA 22+07, 47' LT)	29	4.5	15
21ST STREET (STA 27+62)	84	4.5	42
TOTAL			140

COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT

LOCATION	LENGTH (FT)
NE RADIUS AT SHILOH BOULEVARD	18
NW RADIUS AT SHILOH BOULEVARD	12
STA 1+43 TO STA 4+07 (RIGHT)	264
SW RADIUS AT 25TH STREET	22
STA 4+08 TO STA 4+25 (LEFT)	17
STA 5+30 TO STA 5+52 (LEFT)	22
STA 5+41 TO STA 5+51 (RIGHT)	10
SW RADIUS AT 24TH STREET	11
NW RADIUS AT 24TH STREET	20
STA 10+63 TO STA 10+68 (LEFT)	5
STA 11+90 TO STA 12+00 (LEFT)	10
STA 11+90 TO STA 12+03 (RIGHT)	13
STA 12+45 TO STA 12+87 (LEFT)	42
SW RADIUS AT 23RD STREET	10
NW RADIUS AT 23RD STREET	36
SW RADIUS AT 22ND STREET	43
NW RADIUS AT 22ND STREET	40
STA 21+85 TO STA 22+10 (RIGHT)	25
SE RADIUS AT 21ST STREET	27
TOTAL	647

SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH

LOCATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)
SE RADIUS AT SHILOH BOULEVARD (EAST-WEST SIDEWALK RAMP)	6	5	30
SW RADIUS AT SHILOH BOULEVARD (EAST-WEST SIDEWALK RAMP)	6	5	30
SW RADIUS AT SHILOH BOULEVARD (NORTH-SOUTH SIDEWALK RAMP)	6	5	30
NW RADIUS AT SHILOH BOULEVARD (NORTH-SOUTH SIDEWALK RAMP)	5	5	25
SW RADIUS AT 25TH STREET (NORTH-SOUTH SIDEWALK RAMP)	6	5	30
NW RADIUS AT 25TH STREET (NORTH-SOUTH SIDEWALK RAMP)	6	5	30
SW RADIUS AT 24TH STREET (NORTH-SOUTH SIDEWALK RAMP)	5	5	25
NW RADIUS AT 24TH STREET (NORTH-SOUTH SIDEWALK RAMP)	14	5	70
SW RADIUS AT 23RD STREET (NORTH-SOUTH SIDEWALK RAMP)	4	5	20
NW RADIUS AT 23RD STREET (NORTH-SOUTH SIDEWALK RAMP)	6	5	30
SW RADIUS AT 22ND STREET (NORTH-SOUTH SIDEWALK RAMP)	18	5	90
NW RADIUS AT 22ND STREET (NORTH-SOUTH SIDEWALK RAMP)	19	5	95
SW RADIUS AT 21ST STREET (NORTH-SOUTH SIDEWALK RAMP)	7	5	35
SE RADIUS AT 21ST STREET (NORTH-SOUTH SIDEWALK RAMP)	4	5	20
TOTAL			560

THERMOPLASTIC PAVEMENT MARKING – LINE 6"

LOCATION	LENGTH (FT)
STA 0+57 (CROSSWALK AT SHILOH BOULEVARD)	30
STA 0+63 (CROSSWALK AT SHILOH BOULEVARD)	30
STA 1+00, 88' RIGHT (CROSSWALK AT SHILOH BOULEVARD)	34
STA 1+00, 95' RIGHT (CROSSWALK AT SHILOH BOULEVARD)	34
STA 1+00, 23' LEFT (CROSSWALK AT SHILOH BOULEVARD)	35
STA 1+00, 29' LEFT (CROSSWALK AT SHILOH BOULEVARD)	35
STA 1+31 (CROSSWALK AT SHILOH BOULEVARD)	30
STA 1+37 (CROSSWALK AT SHILOH BOULEVARD)	30
STA 3+00, 23' LEFT (CROSSWALK AT 25TH STREET)	25
STA 3+00, 29' LEFT (CROSSWALK AT 25TH STREET)	25
TOTAL	308

THERMOPLASTIC PAVEMENT MARKING – LINE 24"

LOCATION	LENGTH (FT)
STA 0+53 (AT SHILOH BOULEVARD)	14.5
STA 1+41 (AT SHILOH BOULEVARD)	14.5
25TH STREET WEST APPROACH	12.5
24TH STREET WEST APPROACH	14.5
STA 16+20 (AT 23RD STREET)	14.5
23RD STREET WEST APPROACH	14.5
STA 16+74 (AT 23RD STREET)	14.5
22ND STREET WEST APPROACH	14.5
TOTAL	114

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DESIGNED - SLN	REVISED - IDOT REVIEW 10/05/09
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 08-13-09	FILE - 090759-soq.shx

CITY OF ZION, ILLINOIS
EDINA BOULEVARD ARRA (LAPP)
RESURFACING

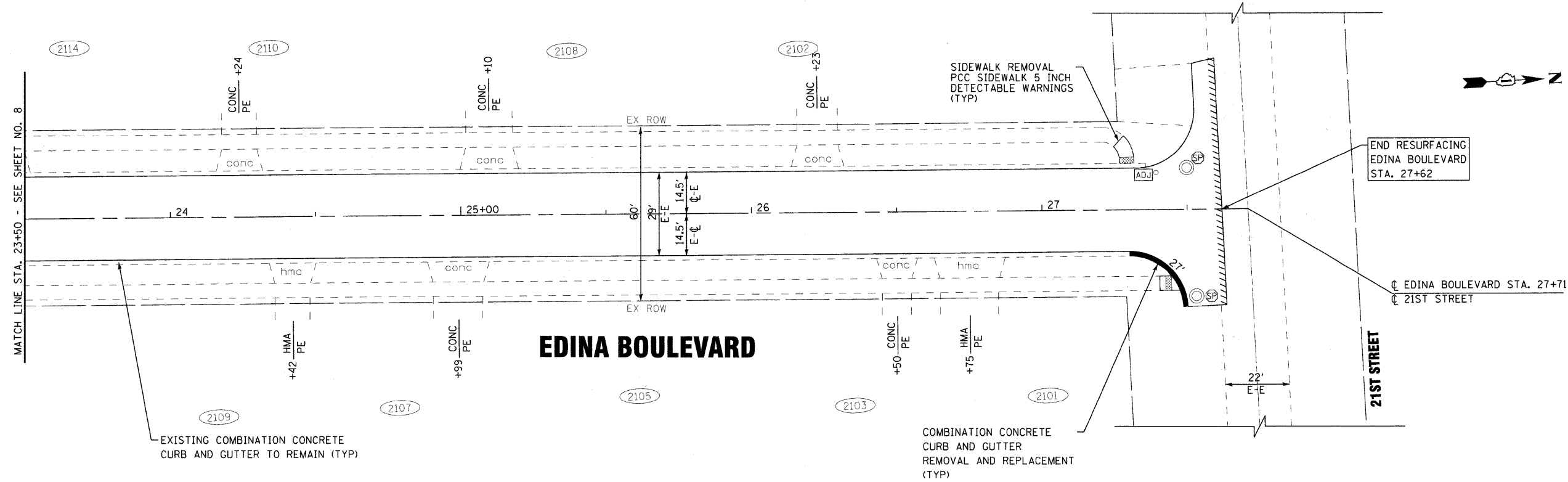
SCHEDULE OF QUANTITIES

SCALE:

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2769	09-00083-00-RS	LAKE	17	5
C-91-906-09				CONTRACT NO. 63336
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(442)		

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...PLOTS\090759-PP3-EDINA BOULEVARD ARR A (LAPP) CADD SURVEY\DRAWINGS\CONSTRUCTION\090759-PP3.sht
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DESIGNED -	SLN	REVISED -	IDOT REVIEW 10/05/09
DRAWN -	KAR	REVISED -	
CHECKED -	RWL	REVISED -	
DATE -	08-13-09	FILE -	090759-pp3.sht

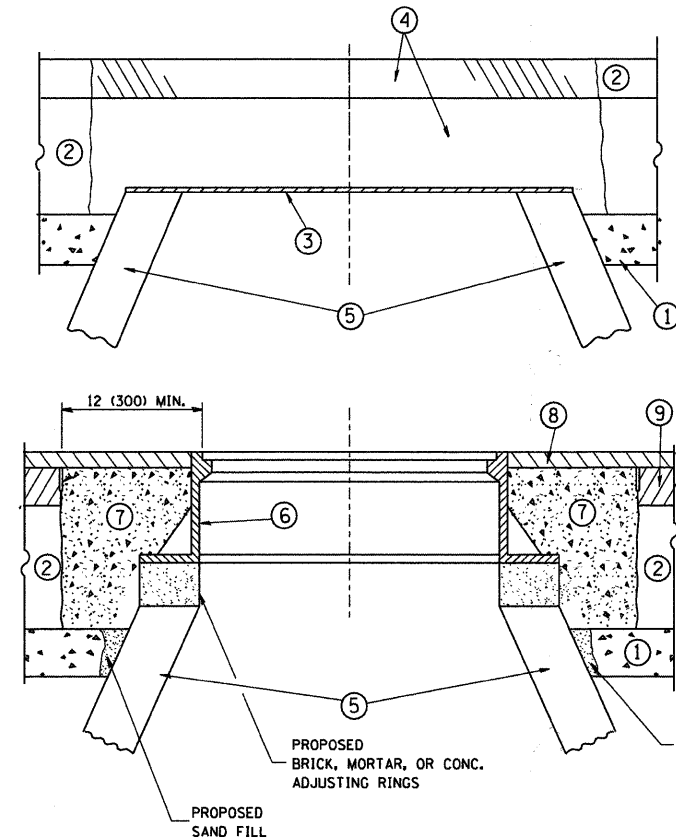
CITY OF ZION, ILLINOIS
EDINA BOULEVARD ARR A (LAPP)
RESURFACING

PLAN - EDINA BOULEVARD

SCALE: 1" = 20'

STA. 23+00 TO STA. 27+71

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2769	09-00083-00-RS	LAKE	17	8
C-91-906-09				
CONTRACT NO. 63336				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(442)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- | | |
|--|--|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

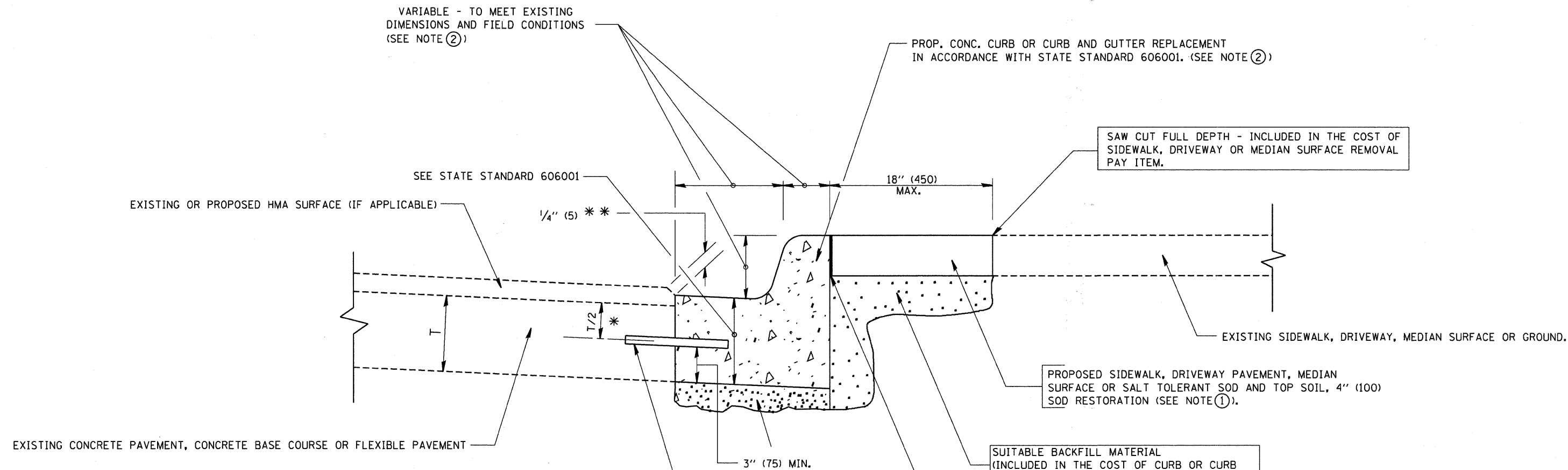
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\bd08.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97						2769	09-00083-00-RS	LAKE	17	9
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04						BD600-03 (BD-8)				
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (442)				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

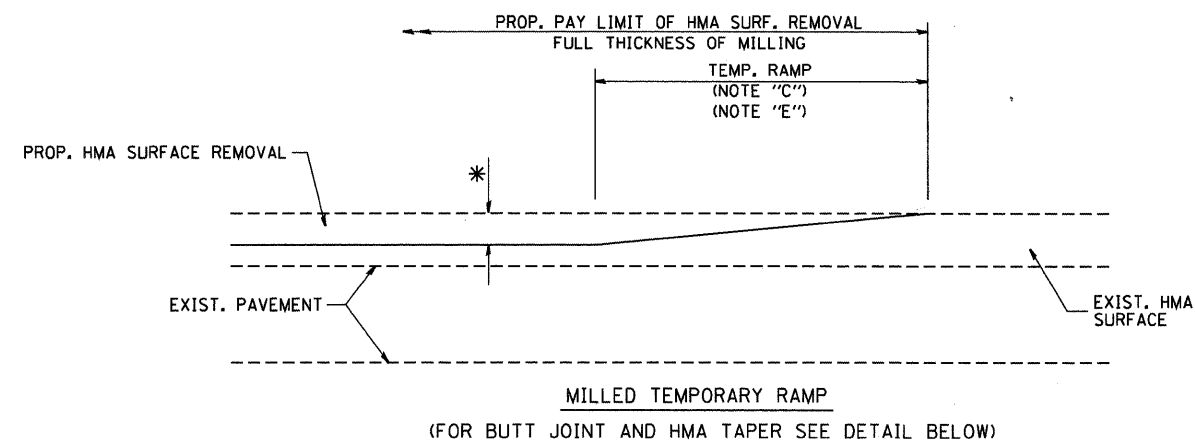
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

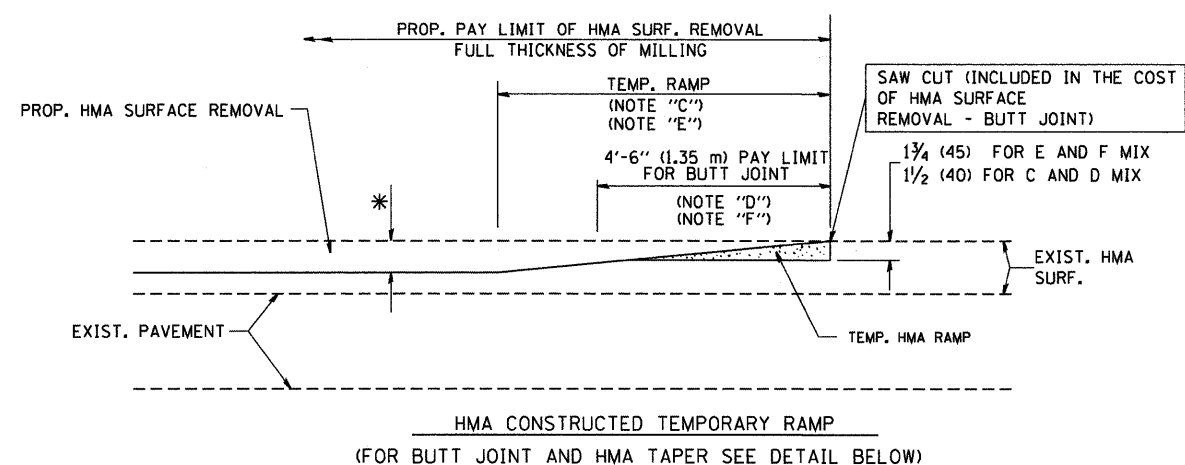
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
 UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd24.dgn	USER NAME = gaglianobt	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - A. ABBAS 03-21-97						2769	09-00083-00-RS	LAKE	17	11	
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)				CONTRACT NO. 63336					
	PLOT DATE = 1/4/2008	DATE - 03-11-94	REVISED - R. BORO 01-01-07		SCALE: NONE				SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(442)	

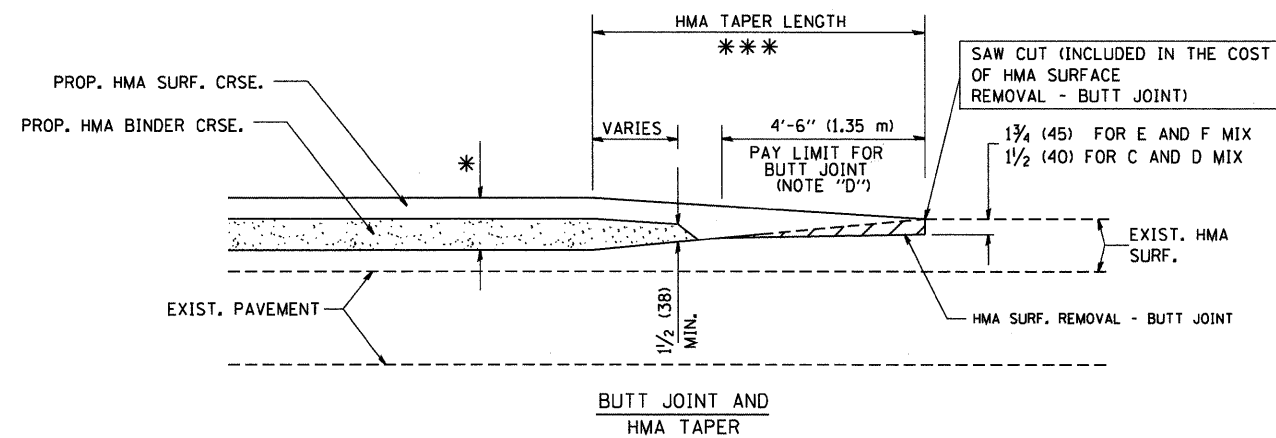


OPTION 1

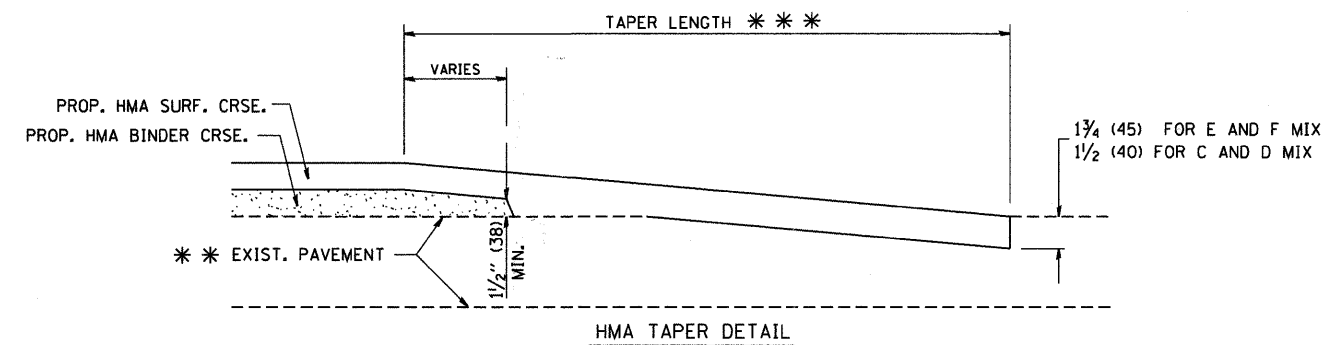
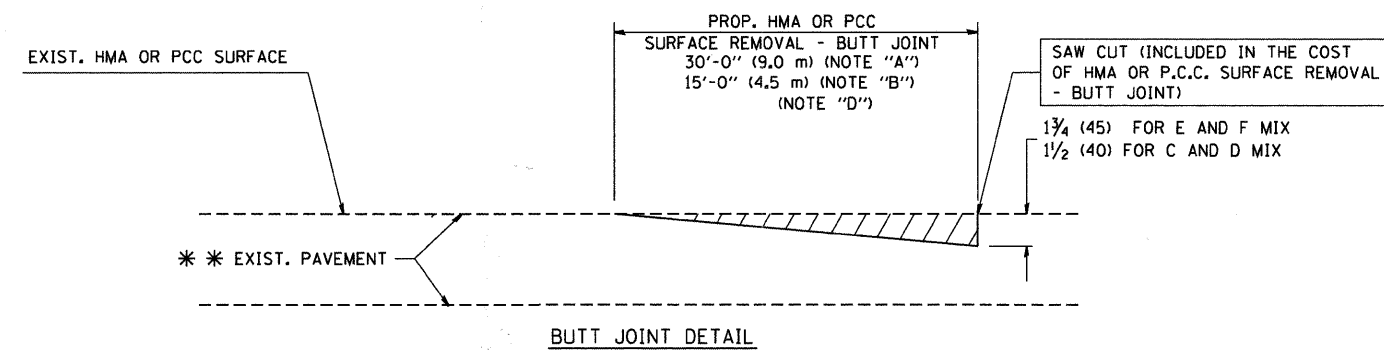


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

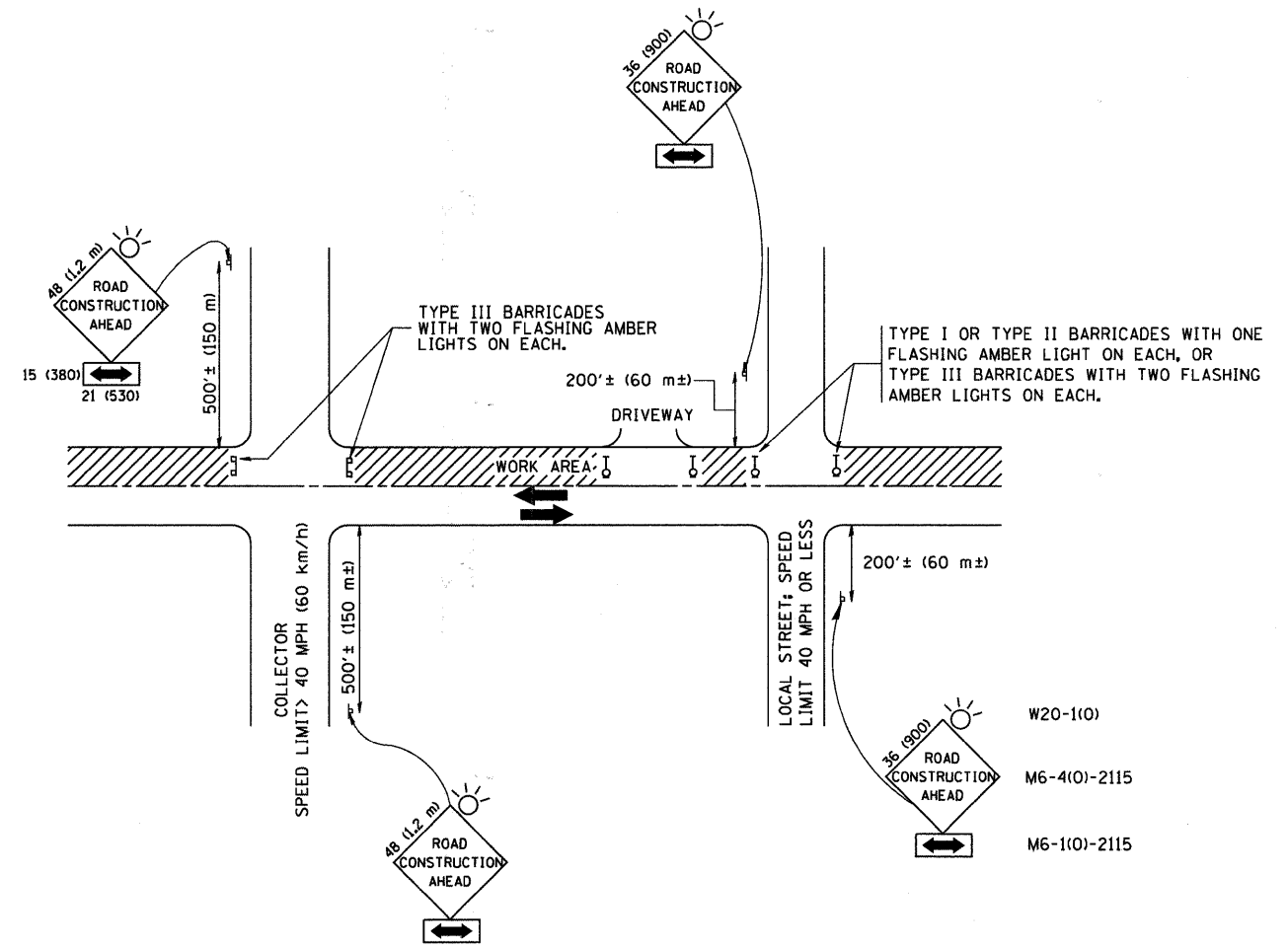
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
B: MINOR SIDE ROADS.
C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gaglianob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
W:\diststd\22x34\bd32.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97					2769	09-00083-00-RS	LAKE	17	12
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01					BD400-05 BD32 CONTRACT NO. 63336				
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (442)		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = geglrenobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\diststd\22x34\to18.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

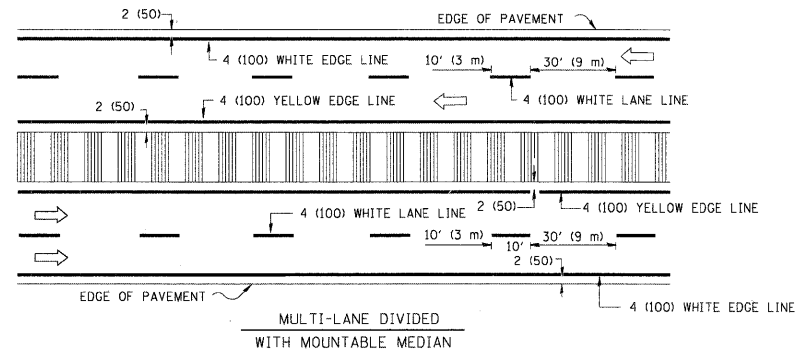
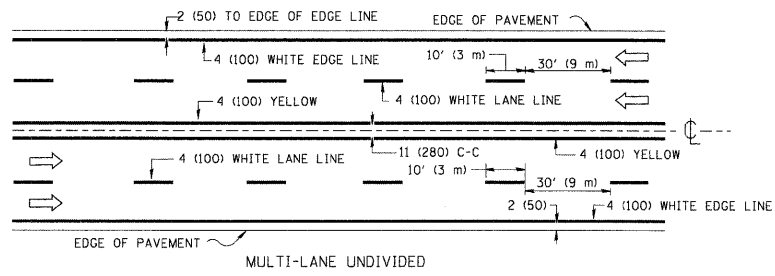
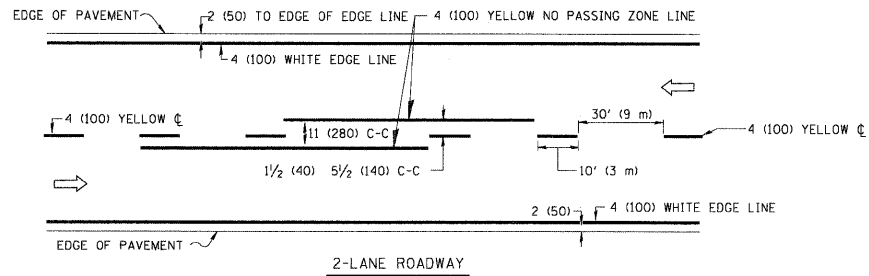
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

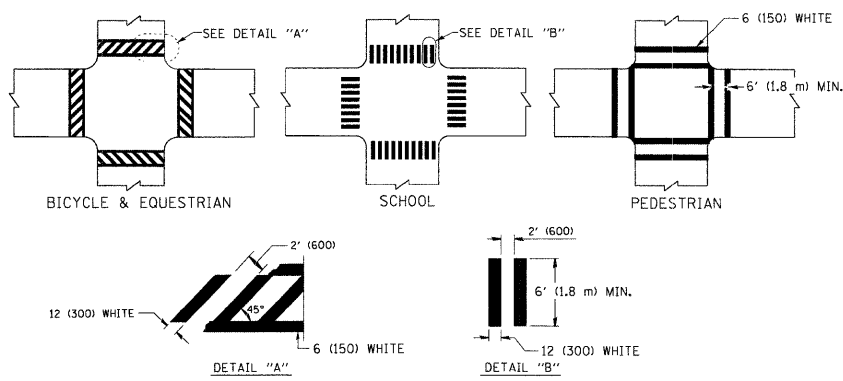
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2769	09-00083-00-RS	LAKE	17	13
TC-10		CONTRACT NO. 63336		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (442)				

C-91-906-09

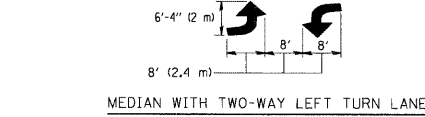
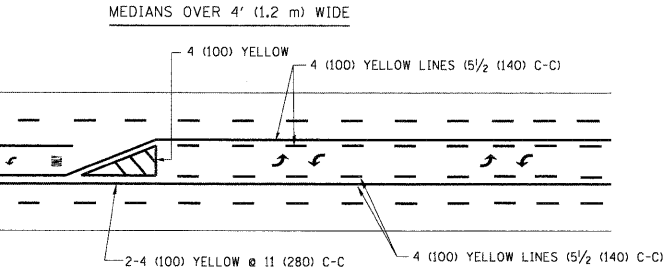
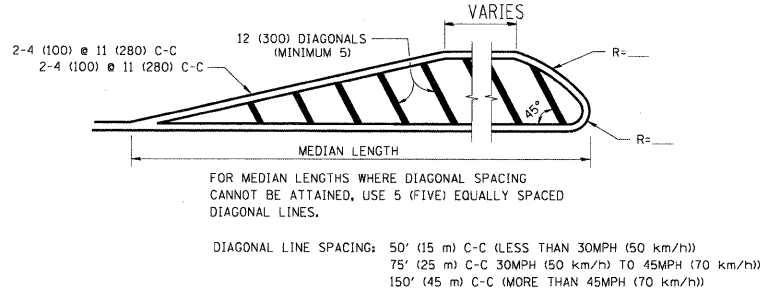
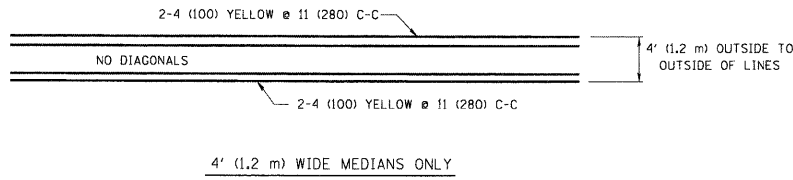


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

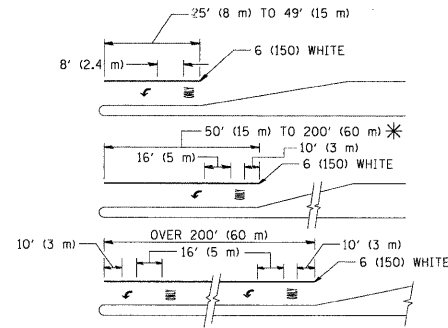
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

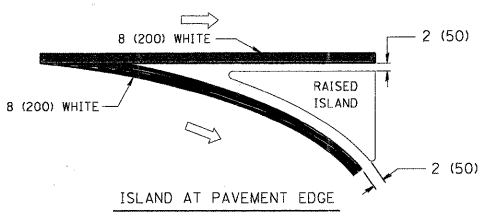
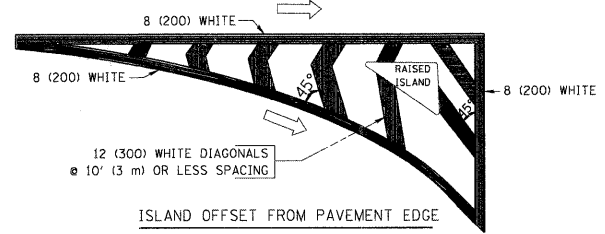


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

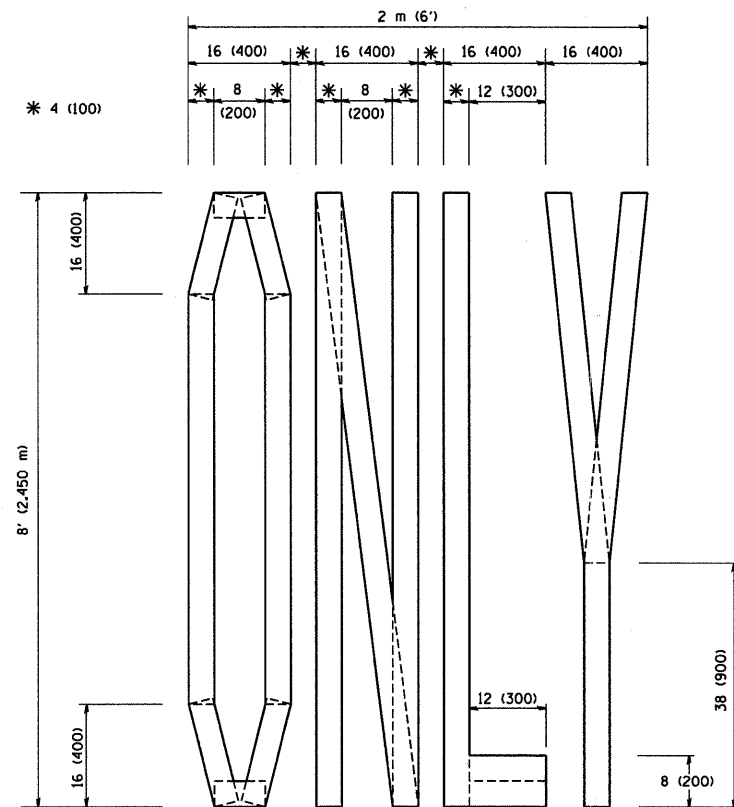


TYPICAL ISLAND MARKING

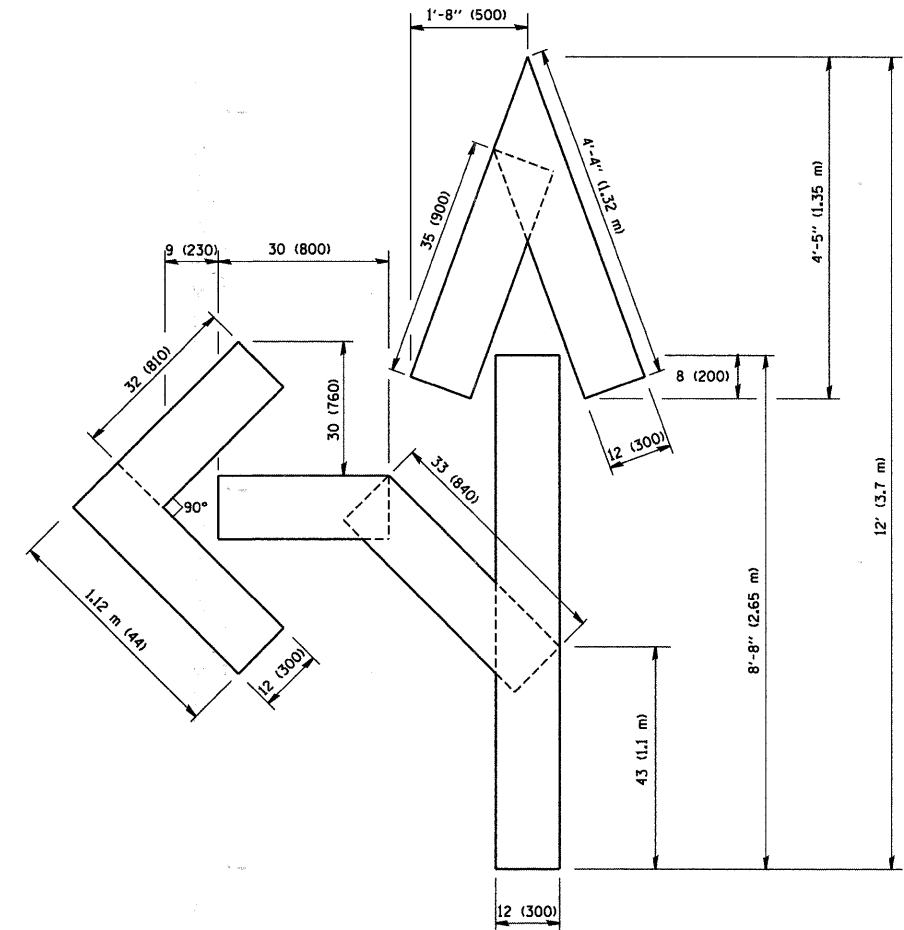
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C OVER 45MPH (70 km/h)
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

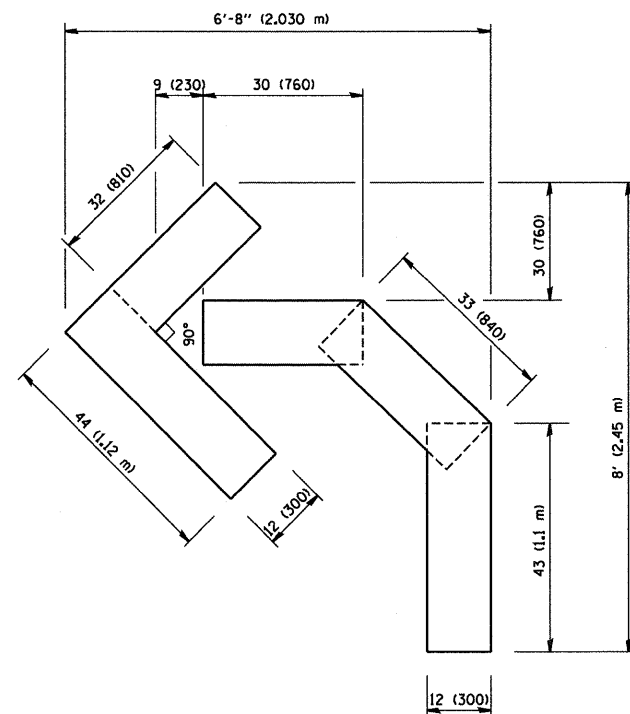
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



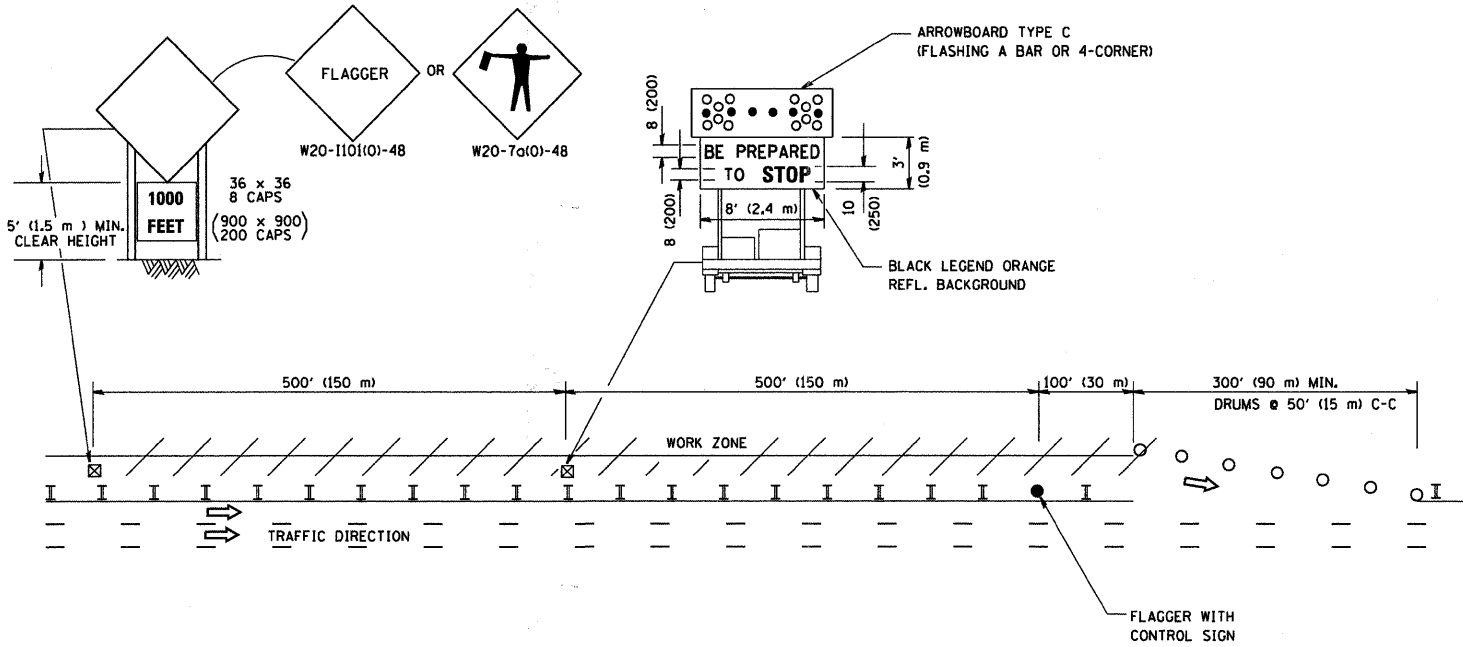
QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

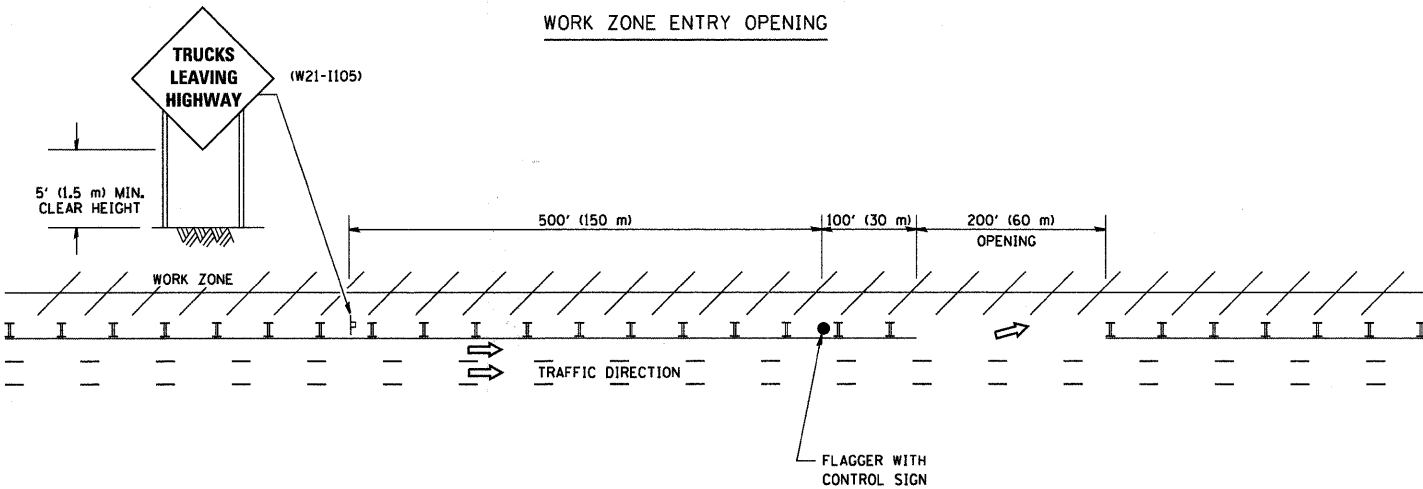
FILE NAME = W:\diststd\22x34\sc16.dgn	USER NAME = gaglianob	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			F.A.U. RTE. 2769	SECTION 09-00083-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 15
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	REVISED -T. RAMMACHER 11-04-97					TC-16		CONTRACT NO. 63336		
PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00			SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(442)		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN

FILE NAME =
W:\diststd\22x34\to18.dgn

USER NAME = gaglianobt

DESIGNED -

REVISED - D.W.S. 08-98

DRAWN -

REVISED - J.A.F. 04-03

PLOT SCALE = 50.000' / IN.

CHECKED -

REVISED - J.A.F. 02-06

PLOT DATE = 1/4/2008

DATE -

REVISED - S.P.B. 01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE

SHEET NO. 1 OF 1 SHEETS

STA.

TO STA.

F.A.D.
RTE.

2769

SECTION

09-00083-00-RS

COUNTY

LAKE

TOTAL SHEETS

17

SHEET NO.

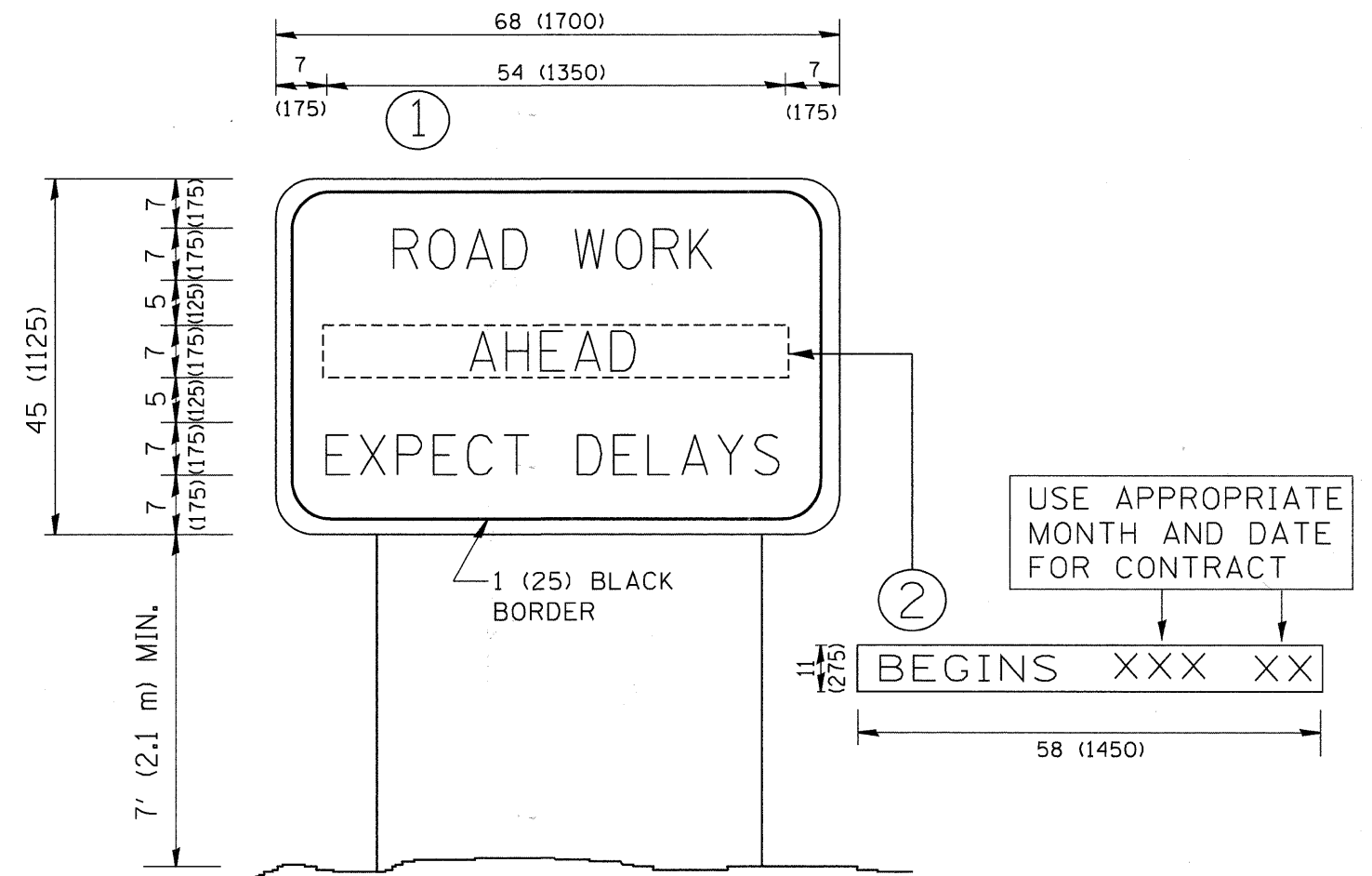
16

TC-18

CONTRACT NO. 63336

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(442)

C-91-906-09



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = geglrenobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	F.A.D. RTE. 2769	SECTION 09-00083-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 17						
		DRAWN -	REVISED - R. MIRS 12-11-97																	
		CHECKED -	REVISED - T. RAMMACHER 02-02-99																	
		DATE -	REVISED - C. JUCIUS 01-31-07																	
										TC-22 CONTRACT NO. 63336										
										FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(442)										